



City of Portland, Oregon
Bureau of Development Services
Land Use Services
FROM CONCEPT TO CONSTRUCTION

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FINAL FINDINGS AND DECISION BY THE DESIGN
COMMISSION RENDERED ON October 4, 2012

CASE FILE NUMBER: LU 12-143673 DZ
Fulton Pump Station

BUREAU OF DEVELOPMENT SERVICES STAFF: Mark Walhood 503-823-7806 /
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GENERAL INFORMATION

Applicant: Benjamin Schonberger
Winterbrook Planning
310 SW 4th Ave., #1100
Portland, OR 97204

Property Owner: City of Portland Parks and Recreation
Attn.: Allan Schmidt
1120 SW 5th Ave., #1302
Portland, OR 97204-1912

Operator: Portland Water Bureau
Attn.: Dave Evonuk
1120 SW 5th Ave., Room 600
Portland, OR 97204

Site Address: Willamette Park (east of SW Macadam Boulevard near terminus of SW Nevada Street)

Legal Description: LOT A INC PT VAC STS LOT B&C, HANNAH MASONS ADD; BLOCK 6 LOT 1&3&5&7&9&11 TL 700, SOUTHERN PORTLAND; LOT 11-22 TL 300, SOUTHERN PORTLAND; TL 900 3.86 ACRES, SECTION 22 1S 1E R357100010, R780200240, R780208140, R991220400

Tax Account No.: 1S1E22A 00100, 1S1E15DC 00700, 1S1E15DC 00300, 1S1E22A 00900

State ID No.: 3730, 3630

Quarter Section:

Neighborhood: South Portland NA., contact Jim Davis at 503-248-9820.

Business District: South Portland Business Association, contact Kevin Countryman at 503-750-2984.

District Coalition: Southwest Neighborhoods Inc., contact Leonard Gard at 503-823-4592.

Zoning: **OSdqr** (Open Space base zone with Design, Greenway/River Water Quality and Greenway/River Recreational overlay zones), **Macadam Plan District**

Case Type: **DZ** (Design Review)

Procedure: **Type III**, with a public hearing before the Design Commission. The decision of the Design Commission can be appealed to City Council.

Proposal: The proposal involves the construction of a new pump station facility along the western boundary of Willamette Park. The building will contain public restrooms and storage areas for the adjacent park uses. The project includes improvements to circulation and landscaping around the building in addition to the building itself. NOTE: The larger project in the park, including overall park upgrades to the greenway trail, park landscaping, vehicle areas, and bicycle parking, as well as the necessary Zoning Map Amendment and Conditional Use Reviews required for the project, are being reviewed through a separate land use case. This separate case, LU 12-145519 CU ZC GW AD, will be going to a hearing before the Hearings Officer later in October, 2012. Approval of both the Design Review and the requests in this separate case are necessary for the project to move forward.

The existing Fulton Pump Station, which is located at 0540 SW Nevada Street, just west of SW Macadam Boulevard, is one of two pumping stations that supply drinking water to the majority of SW Portland. After 100 years of service and several upgrades between 1918 and 1960, the old Fulton Pump Station needs replacement. After a public involvement process, the Portland Water Bureau, in coordination with Portland Parks and Recreation, settled on the proposed site at the west edge of Willamette Park, near the terminus of SW Nevada Street.

The pump station building will be a cast-in-place concrete structure of 9,600 square feet in two stories, with a building height of approximately 28'-0". The ground floor of the building will include four public restrooms, storage for Parks uses, large diameter suction and discharge piping, storage for Water Bureau uses, and an enclosed loading area accessible by a garage door. The second story of the building contains the large room housing six electric water pumps, an electrical room, a mechanical room, and a room with a hatch to the ground floor loading area. The pumps and other electrical equipment are on the second floor of the building because they must be elevated above the 100-year flood plain, which is approximately 11 feet above the natural grade at this location.

Exterior materials for the building include rough stained concrete on lower portions, smooth stained concrete on the upper portions, clear and translucent/spandrel glass which has been designed to be 'bird friendly', and metal louver panels and doors. The majority of the roof surface, including an angled shed roof facing the park above the restrooms, will be provided with a vegetated eco-roof. Other features of the building include an interpretive diagram on the easterly building wall, building signage letters, and metal gutters, flashing, and safety railings for an elevated egress stair at the rear/southwest corner of the building. The restroom area is on the east face of the building, oriented to a reconfigured pedestrian walkway into the park from SW Nevada Street, and two interpretive panels are proposed adjacent to the restroom doors. The loading bay access door also faces east on the building, towards a relocated park circulation road. A rounded basalt retaining wall extends out from the building just south of the restrooms, and the area would be provided with five new bike racks and a drinking fountain.

Because the site is located in a Design overlay zone in the Macadam plan district, and with a project valuation of \$9,000,000.00, a Type III Design Review is required.

Approval Criteria: In order to be approved, this proposal must comply with the approval criteria of Title 33, Planning and Zoning. The applicable approval criteria are the *Macadam Corridor Design Guidelines*.

ANALYSIS

Site and Vicinity: The project site is in Willamette Park. The park is 26.49 acres including a boat dock, boat ramp, picnic areas, restroom, dog off-leash area, tennis courts, soccer fields, and a play ground. The boat ramp is the highest use boat launch in the city of Portland. The site proposed for the new pump station building is in the southwest quadrant of the park, near the access point in alignment with SW Nevada Street. Open grassy and play areas, driveways, parking areas and the riverbank are found to the north, south, and east of the building site.

The surrounding area is a mixture of residential, commercial, and light industrial uses. A rail corridor is located just west of the park, providing weekend and summer trolley service between the South Waterfront district and Lake Oswego. The SW Miles are just south of the park is exclusively residential, but other nearby development includes large apartment and condominium complexes, office buildings, hotels, boat-related uses, restaurants, and retail.

Zoning: The site is zoned OSdqr—Open Space base zone, with Design, River Water Quality, and River Recreational Greenway overlay zones. The site is also in the Macadam Plan District, and has a Public Recreational Trail designation.

The *Open Space base zone* is intended to preserve public and private open and natural areas to provide opportunities for outdoor recreation and a contrast to the built environment, preserve scenic qualities and the capacity and water quality of the stormwater drainage system, and to protect sensitive or fragile environmental areas. The pump station is considered a basic utility use and is regulated in the OS zone as a Conditional Use. Approval of the Conditional Use is necessary prior to operation of the facility, but this is being considered through a separate application (LU 12-145519 CU ZC GW AD).

The *Design overlay zone* promotes the conservation and enhancement of areas of the City with special historic, architectural or cultural value. The proposal requires Design Review, and the relevant guidelines are the Macadam Corridor Design Guidelines.

The *Greenway overlay zone* is intended to protect, conserve, enhance, and maintain the natural, scenic, historical, economic, and recreational qualities of lands along Portland's rivers; establish criteria, standards, and procedures for the development of land, change of uses, and the intensification of uses within the greenway; and implement the City's Willamette Greenway responsibilities as required by ORS 390.310 to 390.368 and Metro's Title 3. Greenway Review issues are being considered through a separate application (LU 12-145519 CU ZC GW AD), including a Zoning Map Amendment to change the overlay zone at the pump station site from River Recreational to River General, which is necessary to allow the pump station (a non-recreational use).

The *Macadam Plan District* contains a set of regulations designed to preserve and promote the unique character of the Macadam area. In addition to special development standards for the district, the regulations restrict auto-oriented uses and development, limit signs, allow for future light rail, and provide view corridors to the Willamette River. The standards of this plan district will have to be met and are discussed below in the Development Standards section of this report.

The Public Recreational Trail designation runs along the river's edge at the east end of the park. A recreational trail designation imposes regulations that seek to support a regional recreational trail system, increase public access along the river, support alternative modes of transportation, and help create a pleasant, aesthetically-pleasing urban environment. Trail-related issues in the larger Willamette Park project being proposed at this time are being considered through a separate application (LU 120145519 CU ZC GW AD).

Land Use History: City records indicate that prior land use reviews include the following:

- V 48-64 – Approved Street Vacation for a portion of SW California, east of SW Macadam Avenue;
- CU 68-86 – Approved Conditional Use for a landfill between SW Idaho and SW Nevada;
- CU 009-71 – Approved Conditional Use for fill on the riverbank;
- CU 060-74 – Approved Conditional Use for a picnic shelter in the park;
- DZ 4-76 – Approved Design Review for new tennis courts and plantings in the park;
- V 33-78 – Approved Street Vacation for a portion of SW Vermont, SW California and SW Miles Court east of SW Macadam Avenue;
- GP 003-82 – Approved Greenway Review for a water pipe running through the park and across the river;
- PC 7550 – Adoption of zone changes recommended by the Macadam Corridor Study for the park and other properties in the area, as approved under Ordinance #157795 in 1985;
- DZ 9-90, GP 2-90, CU 5-90 – Approved and concurrent Design, Conditional Use and Greenway Reviews for a 2-unit restroom facility and parking lot;
- LUR 97-00303 DZ GW CU – Approved and concurrent Design, Conditional Use and Greenway Reviews for significant park alterations including a replacement boat launch ramp, boarding floats, pilings, access road and parking lot, trail improvements, and with Modifications to parking lot landscape standards;
- LUR 00-00530 GW – Approved Greenway Review for new stormwater facilities for the boat ramp and parking lot, new trails, new picnic areas, and new viewpoint areas.
- LU 07-175806 GW – Approved Greenway Review for temporary re-routing of the greenway trail to SW Macadam Avenue, in order to conduct fish salvage and riparian habitat enhancement projects along the riverbank, including new interpretive signs, vegetation and other site improvements. Subject to conditions of approval; and
- LU 11-136820 GW – Approved Greenway Review for the reconstruction of the Willamette Park stormwater swale north of the boat launch area, including re-alignment of the greenway trail, and removal and replacement of planting materials. Subject to conditions of approval.

Agency Review: A “Request for Response” was mailed **August 28, 2012**. The following Bureaus have responded:

The *Bureau of Environmental Services* (BES) has reviewed the proposal and takes no exception to the Design Review application. The BES will be providing specific comments on the remainder of the project through a separate application (LU 12-145519 CU ZC GW AD). The proposed development will be subject to BES standards and requirements during the permit review process. Exhibit E.1 contains staff contact information.

The *Development Review Section of Portland Transportation* has reviewed the proposal for potential impacts regarding the public right-of-way, traffic impacts and conformance with adopted policies, street designations, relevant sections of Titles 33 and 17 of Portland City Code, and for overall potential impacts upon transportation services. There are no applicable transportation-related approval criteria in this Design Review, therefore PBOT will address transportation-related concerns in a separate pending application (LU 12-145519 CU ZC GW AD). Portland Transportation has no objection to the proposed Design Review. Exhibit E.2 contains staff contact and additional information.

The *Water Bureau* has reviewed the proposal and offered no concerns about the Design Review, and has no requested conditions of approval. Exhibit E.3 contains staff contact information.

The *Fire Bureau* has reviewed the proposal and offered general comments, but no objections to the requested Design Review. A separate building permit is required for the proposed work, and all applicable Fire Code requirements will apply at the time of building permit review. If specific requirements are not met, an appeal providing an alternative method is a potential course of action. Exhibit E.4 contains staff contact and additional information.

The *Site Development Section of the Bureau of Development Services* has reviewed the proposal and responded with no concerns about the Design Review. During permitting, documentation must be provided showing that the project complies with flood hazard and balanced cut and fill requirements of Titles 10 and 24.50 of Portland City Code, as well as the Stormwater Management Manual. Exhibit E.5 contains the original response with issues, an e-mail dialogue between the applicant and staff, a building code appeal attachment to the e-mail dialogue, the follow-up 'no concerns' response, and staff contact information.

The *Urban Forestry Division of Portland Parks and Recreation* has reviewed the proposal. Tree protection is required for existing trees to be removed. No trees may be pruned or removed without a permit from Urban Forestry. Mitigation will be required for any tree removals on an inch for inch basis. Exhibit E.6 contains staff contact information.

The *Life Safety Section of the Bureau of Development Services* has reviewed the proposal and responded with standard building code-related comments, but no objections or concerns with the Design Review. A separate building permit is required for the project, and the proposal must be designed to meet all applicable building codes and ordinances. A preliminary fire and life safety meeting is recommended. Exhibit E.7 contains staff contact and additional information.

Neighborhood Review: A Notice of Proposal in Your Neighborhood was mailed on September 6, 2012. No written responses have been received from either the Neighborhood Association or notified property owners in response to the proposal at the time of staff report publication.

ZONING CODE APPROVAL CRITERIA

33.825.010 Purpose of Design Review

Design Review ensures:

- That development conserves and enhances the recognized special design values of a site or area;
- The conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district;
- That certain types of infill development will be compatible with the neighborhood and enhance the area; and
- High design quality of public and private projects.

33.825.055 Design Review Approval Criteria

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design guidelines for the area.

It is important to emphasize that design review goes beyond minimal design standards and is viewed as an opportunity for applicants to propose new and innovative designs. The design guidelines are not intended to be inflexible requirements. Their mission is to aid project designers in understanding the principal expectations of the city concerning urban design.

The review body conducting design review may waive individual guidelines for specific projects should they find that one or more fundamental design guidelines is not applicable to the circumstances of the particular project being reviewed.

Macadam Corridor Design Goals and Guidelines

The following goals are specific to the Macadam Corridor Design Zone.

- Create and improve connections, both physical and visual, between the river, Greenway Trail, Willamette Park and the residential community west of Macadam.
- Encourage opportunities for public use and enjoyment of the waterfront.
- Promote a quality of development in this scenic entry corridor to the Downtown that complements Macadam's landscape treatment.
- Require excellence in design for projects within the Corridor, particularly by assuring that new development contributes to the formation of a rich and diverse mixture of uses and styles in scale with each other.
- Add to the scenic qualities of the river and the Greenway Trail.
- Promote compatibility of new development with the river, surrounding uses, and the neighborhood.

Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project.

Macadam Corridor Design Guidelines

1. Visual Connections. Create public views to the river, Greenway Trail and Willamette Park from Macadam Avenue and other public parks and rights-of-way west of Macadam as well as views from the river and the Greenway to the west.

- Promote physical and visual contact between the river and the area west of Macadam Avenue.
- Orient buildings, which front Macadam Avenue to preserve views of the river, Willamette Park and the Greenway.
- Integrate the east and west sides of Macadam Avenue by creating views of the river which align with streets on the east side of Macadam.
- Take particular advantage of opportunities to create and protect views, which align with Southwest Texas, Florida, Pendleton, Idaho, Nebraska, Dakota and Hamilton Streets.
- Rooftops of buildings should be carefully designed to enhance views.
- Plant on-site trees, which will grow to a sufficient height to soften new development and screen parking areas while selecting species and planting locations, which enhance view corridors to the river.

Findings: The proposed pump station site is located on the west edge of Willamette Park, with the southern edge of the building approximately 20 feet north of the north edge of the east-west alignment for SW Nevada Street. The primary east-west views of the river are and will continue to be available down the SW Nevada Street corridor from west to east and vice versa. The location of the pump station was specifically selected to respect the SW Nevada Street visual corridor from the neighborhood to the river. The existing restroom building to be demolished in the park, just south of the proposed pump station site, was located in the SW Nevada Street alignment. Therefore, the project will open up some new visual connections between the alignment of SW Nevada Street and the river, especially for pedestrians entering the park along SW Nevada Street.

The existing views to the east from nearby SW Macadam Avenue immediately west of the pump station site are already filtered by some landscaping, by satellite dishes in the Oregon Public Broadcasting surface parking lot, and by mature cedar trees in the park just west of the pump station site. The four cedar trees just west of the proposed building are proposed to remain in place and be preserved, to better screen the back side of the building from view for passersby along SW Macadam Avenue. Since the DAR, however, the building has been modified to include spandrel glazing in windows on the south and west facades, to better engage parks users when immediately adjacent to the building on the 'back' sides. The building has been effectively tucked into a corner of the park, out of alignment with a major east-west visual link and pedestrian access point in SW Nevada Avenue, and will be visually screened from the west by mature trees, and feature an earth-toned building design which blends into the surrounding environment of trees and grass. Eco-roofs on the majority of the building will enhance the rare views of the building from surrounding properties when viewed from above. Changes in grade between the grade level of the pump station site and the adjacent roadway in SW Macadam Avenue is about 15 feet, further reducing the visual impact of the building in the rare circumstance that the building is visible from the west.

Therefore, this guideline is met.

2. Physical Connections. Create a common sense of unity that ties both sides of Macadam together with each other, the river to the east and the residential area to the west. Create public walkways that physically connect the Macadam Avenue right-of-way with Willamette Park, the Greenway Trail and the Willamette River.

- Orient structures and parking areas to facilitate access for pedestrians between adjacent uses.
- Extend street tree planting west of Macadam.
- Reinforce connections for pedestrians between the Willamette River Greenway and Macadam Avenue.
- Provide safe, comfortable places where people can slow, sit and relax. Locate these places adjacent to sidewalks, walkways and the Greenway Trail.
- Provide sidewalks and pathways, through larger developments with landscaping which screens or separates these from parking and motor vehicle maneuvering areas.
- Provide walkways, which link parking areas to district-wide access systems for pedestrians.

Findings: The proposed new pump station and restroom building project includes an upgrade to the pedestrian entry sequence from SW Nevada Street, immediately southwest of the project area. The public walkway connection will remain but be slightly re-aligned to wrap around a new landscaped area with a basalt bollard perimeter treatment just south of the pump station building. The basalt bollards are low and applied as an edge treatment to the new walkway into the park, and the bollards curve in a semi-circle around to the north to the restroom entry, bike parking, and drinking fountain area in front of the building. The reconstructed path will connect to the Greenway Trail further to the west, and the landscaped area near the pump station and basalt bollards to the south of the building will provide opportunities for pedestrians and park users to stop, rest, use the restroom, and relax by sitting down on the basalt bollards themselves.

Therefore, this guideline is met.

3. The Water's Edge. Enhance the scenic qualities of the river and sites that about the riverbank to contribute to an attractive and enjoyable Greenway Trail.

- Identify natural areas of the Willamette riverbank and preserve the natural qualities of these areas.
- Screen parking, loading and vehicular movement areas from the Greenway with rich landscape plantings.
- Locate buildings to protect access to sunlight on the Greenway Trail.

Findings: No changes are proposed immediately along the water's edge with the pump station project. Changes to the trails and vegetation along the riverbank are being reviewed through a separate application, and as parks-related open space uses these alterations do not trigger Design Review.

Therefore, this guideline does not apply.

4. The Boulevard. Coordinate with and enhance Macadam's boulevard treatment and contribute to the attractiveness of this entrance to the city.

- Consider using awnings or other weather protection, street furniture, plazas, sculpture courts or other amenities for pedestrians to reinforce the boulevard design of Macadam.
- Abut pedestrian pathways with buildings or landscaping. Buffer with landscape screens, parking lots and structures, which are not oriented to pedestrians.
- Use landscaping to reinforce the boulevard character of Macadam and to provide visual connections with private property adjacent to Macadam.
- Trees interspersed with low-growing vegetation or grass should visually predominate over impervious surfaces.
- Provide frequent views from Macadam into interior ground level spaces of projects located along the Avenue.

Findings: No changes are proposed immediately abutting SW Macadam for the pump station project.

Therefore, this guideline does not apply.

5. Sub-Area Context. Enhance a site's character through designs that are compatible with features of their surroundings and contribute to the development of an attractive character in the vicinity of the project site. Pay particular attention to cases where the adjacent use is different from that which a project will house.

- Locate buildings to avoid excessive shadow on public open spaces, especially Willamette Park and the Greenway Trail.
- Isolated or independent buildings and open spaces should provide design solutions of merit, which consciously set a precedent for neighboring future developments.
- Buildings and open spaces should establish complementary relationships in terms of color, texture, scale of architectural elements, and proportions with neighboring developments.
- Provide sensitive transitions between new development and adjacent residential areas.

Findings: The overall design concept for the building is to quietly blend into the park landscape along the western edge of the park, while at the same time providing park visitors with some visual connection to the pump station functions on the interior of the building. The proposed building is 28 feet high, which is the shortest feasible height of the proposed building based on a location in the floodplain and other technical requirements for access and maintenance. In order to be fully operational in the event of a major flood, the electrically-powered pumps must be kept out of the flood plain, which is 11 feet above the natural grade at this location. The design therefore puts the primary functions of the building on the second floor, and the other uses (restrooms, Parks and Water Bureau storage, loading) on the ground floor. A two-story building also reduces the overall footprint for the project.

Following the DAR the building design was revised slightly, lowering the overall height by several feet. The ceiling height was lowered slightly in the pump room, and the rating for the overhead bridge crane was lowered to five tons, which shrank this internal equipment and allowed for a lower ceiling height. The electrical room ceiling on the west side of the building was also reduced, and the roof parapet was eliminated, except surrounding some rooftop mechanical equipment, which is now concealed within an enclosure on the northwest corner of the rooftop.

Given the significant distance between the building and the adjacent Greenway Trail, there should be no significant impacts with regards to shading or shadow on the trail system as a result of this building. The pump station is tucked in among existing trees and will feature significant new landscaping along the south, west, and north edges of the foundation, extending out to the adjacent public walkways. The building is located at the far west edge of the park, out of the SW Nevada Street east-west visual corridor, and there are no trails to the immediate north of the building that would be put into shadow at any time.

The building design seeks to strike a balance between the park setting of lawns, trails, trees, and riverbank and a modern, attractive expression of the functional uses happening within. As a basic utility and parks use, the structure is functionally distinct from all other nearby buildings, which are limited to commercial, residential, and industrial use. The primary exterior material for the building is cast concrete, with a brown-stained smooth concrete surface on the upper walls, and a textured brown-stained concrete on the lower walls. The textured surface of the lower walls integrates with the rough basalt bollards used in the landscaped area just south of the building. Both the smooth and the textured concrete will be instilled with integral pigment color to help reduce the otherwise gray color of natural concrete. The smooth areas will be further stained a darker brown to further contrast with the textured surfaces below, and to help the upper portion of the building disappear or recede into the background of trees beyond.

To avoid the appearance of a monolith, all four elevations of the building are broken up with a variety of access doors and glazing. Glazing consists of clear glass view windows in some locations and a smokey or dark spandrel glass in others. On the east façade, view windows are aligned with the five pumps inside the building, allowing some view of the functions happening inside. On the north façade a combination of clear glass and spandrel glass is proposed. Spandrel panels are located on the south and west facades, with glass intended to reduce potential bird strikes.

The roof of the pump station is almost entirely a vegetated eco-roof system. The eco-roof will extend right up to the edge of the roof, there is no parapet, and the roof is angled slightly towards different corners of the building, allowing a view of the roof for parks users. In some locations stormwater runoff is allowed to drip over the edge of the roof to cascade down the textured concrete walls. These walls will discolor, patina, age, and grow moss over time to help reinforce the idea that this building is part of the landscape.

Overall, the building design strikes a good balance between blending in with the natural surroundings of the park and river, while also expressing the function of the building interior and mitigating for its mass and placement in the park. The building is a significant distance away from the nearest homes to the south in SW Miles Place, and includes glazing and significant landscaping on the south side, reducing potential impacts to nearby homes.

Therefore, this guideline is met.

6. Signs. Keep signage consistent with and supportive of Macadam Avenue's role as a scenic boulevard while using signs to connect the activities housed by a project to the boulevard.

- The cumulative effect of signage should not create confusion for the motorist, visual clutter, or adverse visual impacts on the neighborhood.
- Signs along Macadam should not be directed to motorists on Interstate 5.

- The design scale, color and illumination of signs should be consistent with the features of adjacent buildings and activities.

Findings: There is one building identification sign proposed for the new pump station building, but is located facing east into the park above the loading access door. Smaller interpretive signage is proposed on both sides of the group of restroom doors, as well, and these signs also face east away from SW Macadam Avenue. A large interpretive representation of a water pump device is also proposed on the east façade, to be either cast into the concrete walls as a relief, or applied to the concrete as an embossed material. No signage is proposed that faces or will be visible from SW Macadam Boulevard. *Therefore, this guideline is met.*

7. Johns Landing Masterplan. Comply with the provisions of the Johns Landing masterplan¹.

Findings: The John's Landing master plan shows an area north of Willamette Park, but does not include the park itself within the master plan boundaries. *Therefore, this guideline does not apply.*

8. Future Light Rail Transit. Preserve the potential for a two-direction, light rail facility within the Macadam Corridor Design Zone as illustrated. Until the LRT facility is constructed, the required setback area may be used for parking and landscaping requirements.

Findings: The Macadam plan district imposes a special setback to either side of the centerline of the north-south rail corridor that runs along the western boundary of Willamette Park, near the proposed pump station. The plan district requires a maximum set back for 30 feet from the centerline for commercial structures, and 17 feet from the centerline for uninhabitable structures. The proposed pump station is placed 21 feet from the east property line of the park, adjacent to the 26 foot-wide rail right-of-way. The 21-foot setback and half of the right-of-way parcel width (13 feet) equals a 34 foot setback of the building from the centerline of the potential future light rail corridor, in excess of the minimum setback requirement. Area between the building and the future light rail corridor is limited to landscaping and one access stair. *Therefore, this guideline is met.*

DEVELOPMENT STANDARDS

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

The applicant must meet all applicable Building Code, Fire Code, Flood Hazard, Stormwater Management, and other relevant city regulations during the building permit review process.

CONCLUSIONS

The proposal involves the construction of a new water pump station and parks use building with restrooms and storage spaces on the west edge of Willamette Park. The proposal has been vetted through an extensive public process, and strikes a balance between making itself as discrete and 'invisible' as possible, while also offering some design cues as to the function inside the building. The placement of the building and associated walkways will open up the view corridor in alignment with SW Nevada Street as one enters the park, and the interpretive diagrams, eco-roofs, and small pedestrian gathering area with drinking fountain outside the restrooms will create a pleasant resting spot for parks users. The proposal is able to meet the applicable design guidelines and should be approved.

DESIGN COMMISSION DECISION

It is the decision of the Design Commission to **approve** the **Design Review** for a new pump station and public restroom/storage building in the southwest quadrant of Willamette Park, in the Macadam Plan District, including the immediately adjacent perimeter landscaping, basalt bollard wall, drinking fountain, and walkways as shown

¹ The masterplan may be amended through review by the Planning Commission.

on the approved exhibits C.1 through C.30 (parking areas, riverbank work and other park changes are being reviewed separately through LU 12-145519 CU ZC GW AD), and subject to the following condition:

- A. As part of the building permit application submittal, each of the 4 required site plans and any additional drawings must reflect the information and design approved by this land use review as indicated in Exhibits C.1-C.30. The sheets on which this information appears must be labeled, "Proposal and design as approved in Case File # LU 12-143673 DZ. No field changes allowed."

By: 
Guenevere Millius, Design Commission Chair

Application Filed: May 23, 2012

Decision Rendered: October 4, 2012

Decision Filed: October 5, 2012

Decision Mailed: October 16, 2012

About this Decision. This land use decision is **not a permit** for development. Permits may be required prior to any work. Contact the Development Services Center at 503-823-7310 for information about permits.

Procedural Information. The application for this land use review was submitted on May 23, 2012, and was determined to be complete on August 17, 2012.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on May 23, 2012.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period.

Some of the information contained in this report was provided by the applicant.

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. This report is the final decision of the Design Commission with input from other City and public agencies.

Conditions of Approval. This approval may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term "applicant" includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

Appeal of this decision. This decision is final unless appealed to City Council, who will hold a public hearing. **Appeals must be filed by 4:30 pm on October 30, 2012** at 1900 SW Fourth Ave. Appeals can be filed Tuesday through Friday on the first floor in the Development Services Center until 3 p.m. After 3 p.m. and on Monday, appeals must be submitted to the receptionist at the front desk on the fifth floor. Information and assistance in filing an appeal is available from the Bureau of Development Services in the Development Services Center or the staff planner on this case. You may review the file on this case by appointment at, 1900 SW Fourth Avenue, Suite 5000, Portland, Oregon 97201.

If this decision is appealed, a hearing will be scheduled and you will be notified of the date and time of the hearing. The decision of City Council is final; any further appeal is to the Oregon Land Use Board of Appeals (LUBA).

Failure to raise an issue by the close of the record at or following the final hearing on this case, in person or by letter, may preclude an appeal to City Council on that issue. Also, if you do not raise an issue with enough

specificity to give City Council an opportunity to respond to it, that also may preclude an appeal to LUBA on that issue.

Who can appeal: You may appeal the decision only if you have written a letter which was received before the close of the record at the hearing or if you testified at the hearing, or if you are the property owner or applicant. Appeals must be filed within 14 days of the decision. **An appeal fee of \$5,000.00 will be charged (one-half of the application fee for this case, up to a maximum of \$5,000.00).**

Neighborhood associations may qualify for a waiver of the appeal fee. Additional information on how to file and the deadline for filing an appeal will be included with the decision. Assistance in filing the appeal and information on fee waivers are available from the Bureau of Development Services in the Development Services Center, 1900 SW Fourth Ave., First Floor. Fee waivers for neighborhood associations require a vote of the authorized body of your association. Please see appeal form for additional information.

Recording the final decision.

If this Land Use Review is approved the final decision must be recorded with the Multnomah County Recorder. A few days prior to the last day to appeal, the City will mail instructions to the applicant for recording the documents associated with their final land use decision.

- *Unless appealed*, The final decision may be recorded on or after **October 31, 2012 – (the day following the last day to appeal)**.
- A building or zoning permit will be issued only after the final decision is recorded.

The applicant, builder, or a representative may record the final decision as follows:

- By Mail: Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- In Person: Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034

For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this approval. An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Zone Change and Comprehensive Plan Map Amendment approvals do not expire.

Applying for your permits. A building permit, occupancy permit, or development permit must be obtained before carrying out this project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed here.
- All applicable development standards, unless specifically exempted as part of this land use review.
- All requirements of the building code.
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

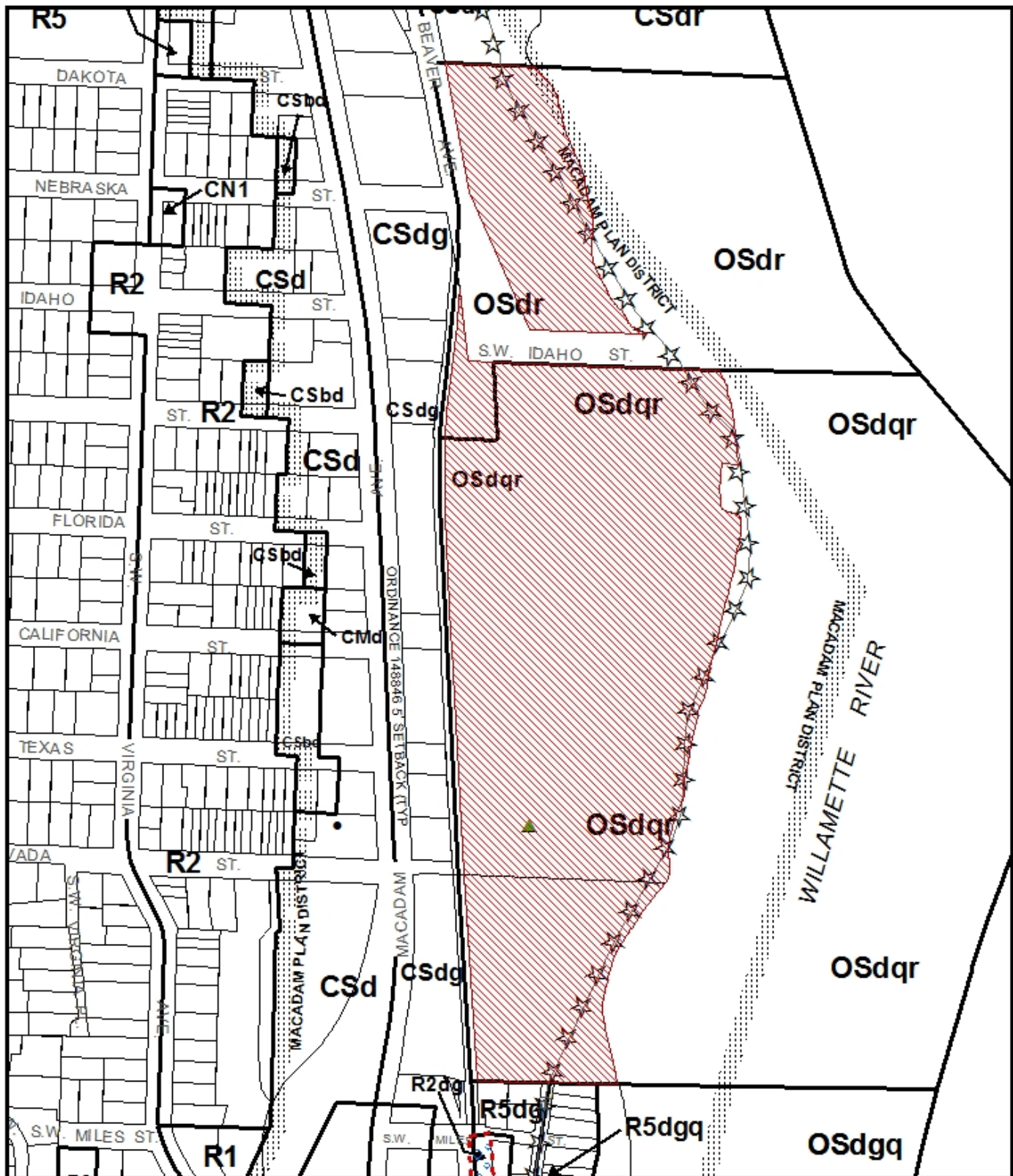
Mark Walhood
October 12, 2012

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

EXHIBITS – NOT ATTACHED UNLESS INDICATED

- A. Applicant's Statement:
 - 1. Original application narrative and attachments
 - 2. Supplemental/revised application narrative and attachments
- B. Zoning Map (attached):
 - 1. Existing Zoning
 - 2. Proposed Zoning
- C. Plans & Drawings:
 - 1. Zoning Map and Vicinity Map
 - 2. Existing conditions – overall site
 - 3. Existing conditions – pump station site
 - 4. Existing conditions – north and central parking lots REF ONLY/NOT APPROVED
 - 5. Proposed improvements – overall site
 - 6. Proposed improvements – pump station site (**ATTACHED**)
 - 7. Proposed improvements – stormwater, utility and grading plan
 - 8. Proposed improvements – north parking lot REF ONLY/NOT APPROVED
 - 9. Proposed improvements – central parking lot REF ONLY/NOT APPROVED
 - 10. Proposed improvements – south parking lot REF ONLY/NOT APPROVED
 - 11. Landscape plan – pump station site (**ATTACHED**)
 - 12. Site furnishings sections and details
 - 13. Site furnishings sections and details – 2 of 3
 - 14. Site furnishings sections and details – 3 of 3
 - 15. Landscape details
 - 16. Greenway bank planting plan REF ONLY/NOT APPROVED
 - 17. Overall site irrigation plan REF ONLY/NOT APPROVED
 - 18. Overall site construction management plan REF ONLY/NOT APPROVED
 - 19. Overall site fencing and access plan REF ONLY/NOT APPROVED
 - 20. Overall site tree removal and protection plan REF ONLY/NOT APPROVED
 - 21. Pump station floor plans
 - 22. Pump station roof plans (**ATTACHED**)
 - 23. Pump station color elevations
 - 24. Pump station black and white elevations (**ATTACHED**)
 - 25. Pump station building sections
 - 26. Pump station details – 1 of 2
 - 27. Pump station details – 2 of 2
 - 28. Rendering from North
 - 29. Rendering at restrooms
 - 30. Rendering from South
- D. Notification information:
 - 1. Request for response
 - 2. Posting letter and posting notice as sent to applicant
 - 3. Applicant's statement certifying posting
 - 4. Mailing list and mailed notice
 - 5. Mailed copy of notice
- E. Agency Responses:
 - 1. Bureau of Environmental Services
 - 2. Development Review Section of Portland Transportation
 - 3. Water Bureau
 - 4. Fire Bureau
 - 5. Site Development Review Section of Bureau of Development Services
 - 6. Urban Forestry Division of Portland Parks and Recreation
 - 7. Life Safety Section of the Bureau of Development Services
- F. Letters:
 - 1. *(none received at time of staff report publication)*
- G. Other:
 - 1. Original LUR Application
 - 2. Request for Completeness responses
 - 3. Incomplete letter from staff to applicant, sent June 14, 2012
- H. Hearing Exhibits
 - 1. Staff Report and Recommendation
 - 2. Pre-hearing staff memo to Commissioners

3. Design Advice Request Summary Memo and Drawings
4. Staff Powerpoint Presentation
5. Staff Presentation 'Cheat Sheet' for Commissioners
6. Photo of site submitted at Oct. 4, 2012 hearing



ZONING

 Site



NORTH

This site lies within the:
MACADAM PLAN DISTRICT

File No.	LU 12-143673 DZ
1/4 Section	3630,3730
Scale	1 inch = 300 feet
State_Id	1S1E22A 100
Exhibit	B (May 30,2012)

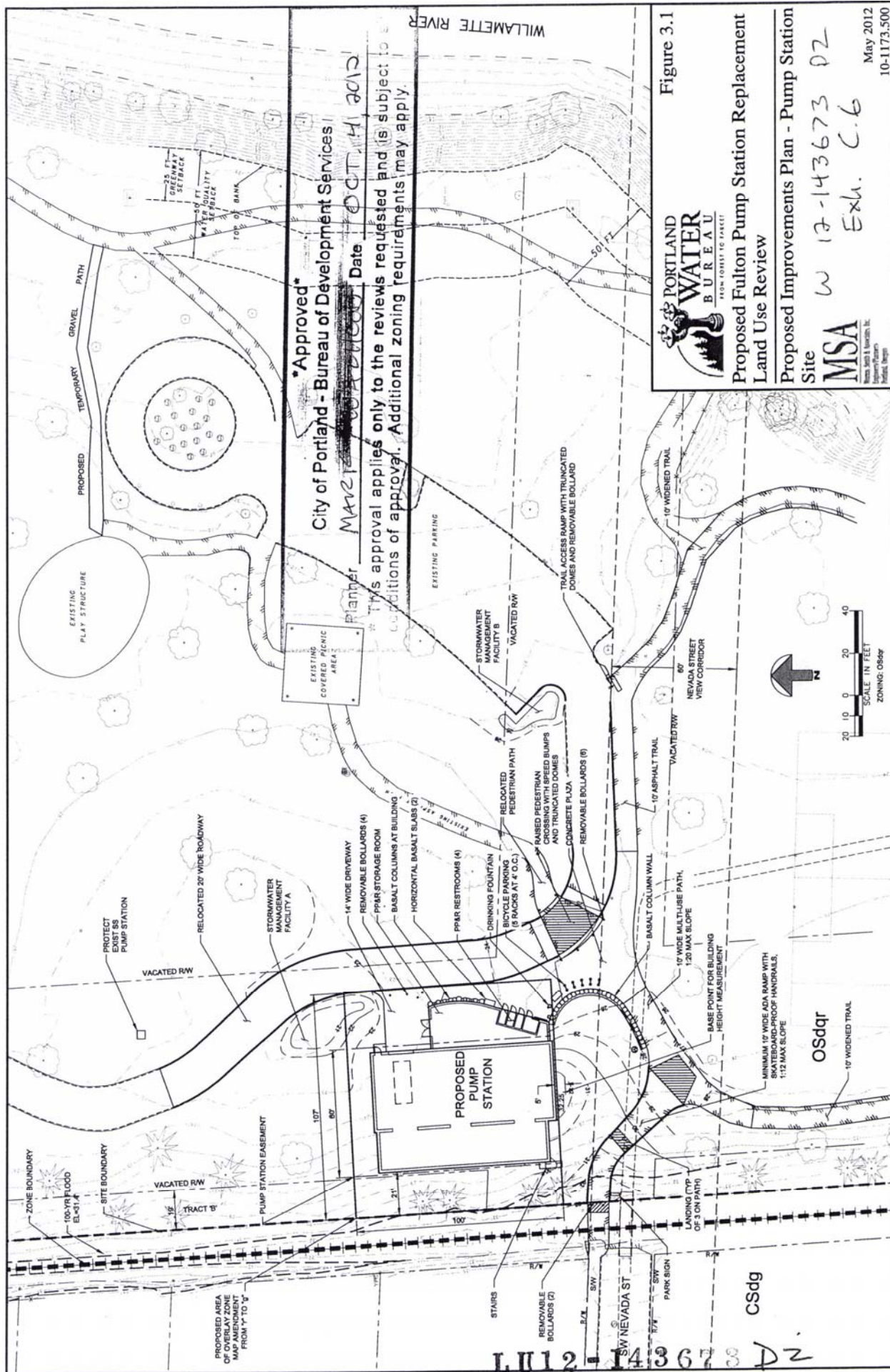


Figure 3.1



Proposed Fulton Pump Station Replacement Land Use Review

Proposed Improvements Plan - Pump Station

Site 10-143672 07

MSA

W 17-143673 02

Ex. 6

May 2012

10-1173.500

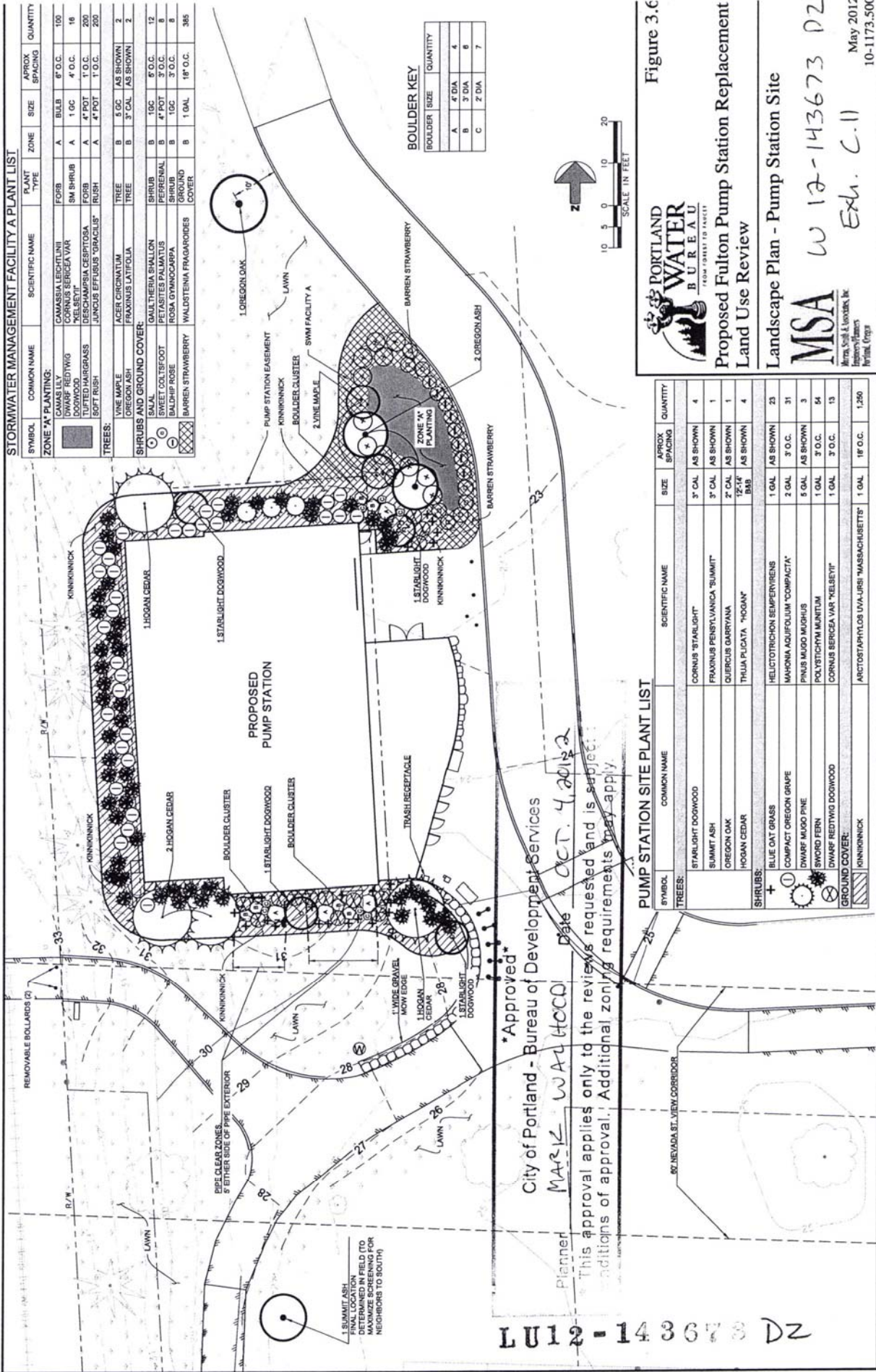


Figure 3.6



Proposed Fulton Pump Station Replacement
Land Use Review

Landscape Plan - Pump Station Site

MSA W 12-143673 DZ

Exh. C.11

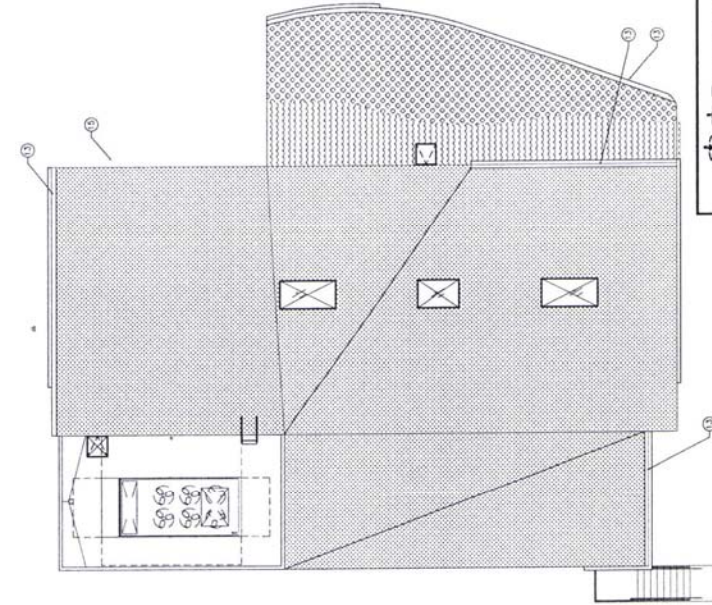
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KEYNOTES:

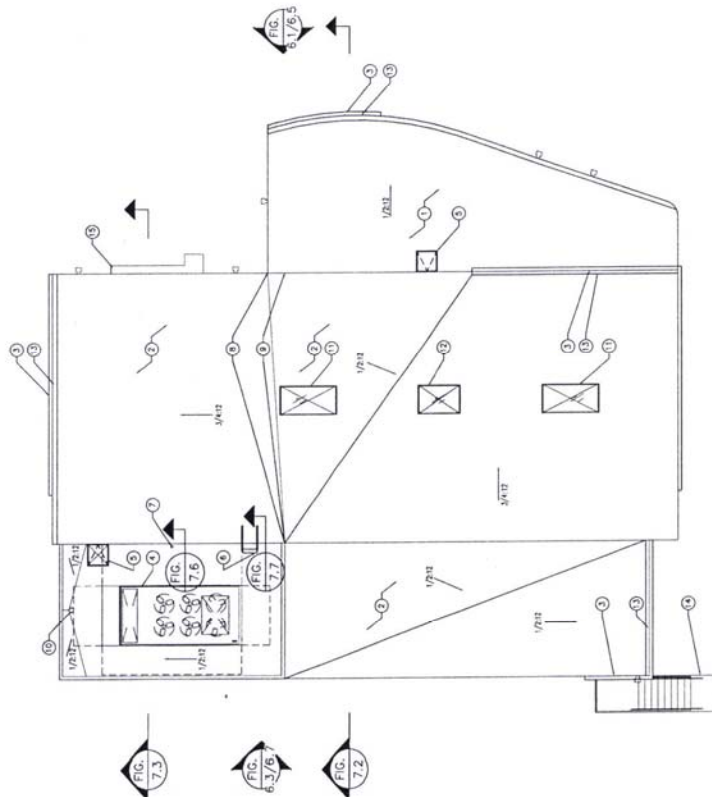
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SYMBOL LEGEND

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ECO ROOF PLANTING PLAN (FIG. 5.3)



ROOF PLAN/EXTERIOR LIGHTING PLAN (FIG. 5.2)



**Proposed Fulton Pump Station Replacement
Land Use Review**

Roof Plans

**MWA
Architects**



W 12-143673 02
Exhibit C.22

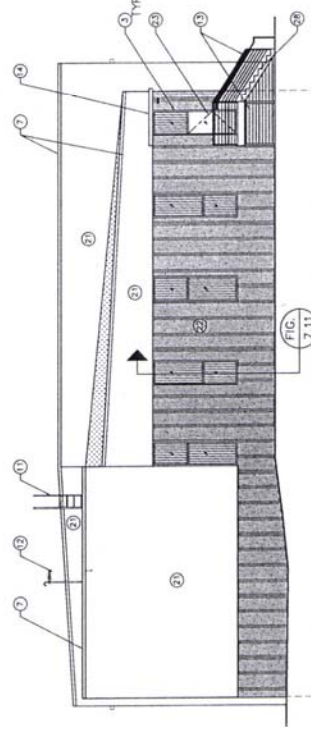
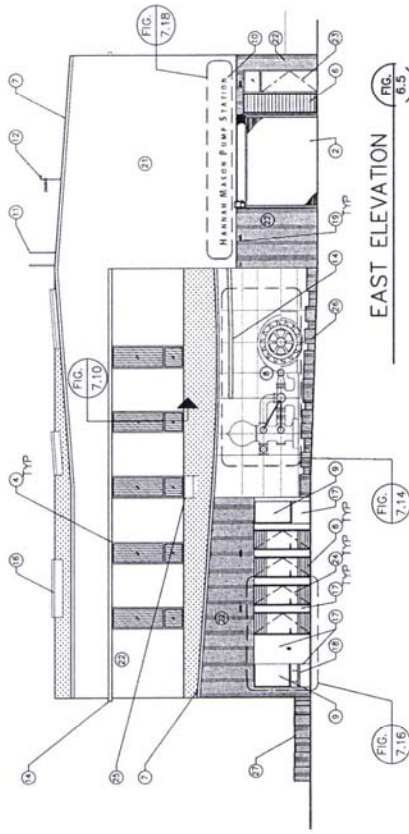
August 2012
10-1173-500

Approved
City of Portland - Bureau of Development Services
Planner MAREK WALIHO Date OCT. 4, 2012
This approval applies only to the reviews requested and is subject to conditions of approval. Additional zoning requirements may apply.

W 12-143673 02

KEYNOTES:

- 1 MET LITE
- 2 RAIL UP ROOF
- 3 PANEL CLADDING - SEE FIG. 7.10A
- 4 NEW WINDOW - SEE FIG. 7.10A
- 5 ACUSTIC HANG LINER
- 6 FLOOR LINER
- 7 METAL ROOF FLEE
- 8 INTERIOR STAIR
- 9 INTERIOR STAIR PANEL
- 10 CONCRETE RELIEF BUILDING SPACE
- 11 ROOF ACCESS LATCH, SEE FIGURE 7.7
- 12 COMMUNICATIONS ANTENNA
- 13 METAL RAILING SYSTEM
- 14 GUTTER
- 15 DOWNSPOUT
- 16 PACKAGE, SEE FIGURE 7.5
- 17 REINFORCED A/C CLADDING TO MATCH EXISTING WORK
- 18 UNFINISHED BRICK AND TROUS
- 19 EXTERIOR LIGHT FIXTURE
- 20 METAL RAILING CAP
- 21 CONCRETE - FINISHED, SMOOTH
- 22 CONCRETE - FORMER, INTERIOR
- 23 CLADDING, SEE FIGURE 7.10B
- 24 NEW DOOR - STEEL PLATE
- 25 ROOF ACCESS HATCH
- 26 ROOF ACCESS LATCH, SEE FIGURE 7.7
- 27 ROOF ACCESS LATCH, SEE FIGURE 7.8
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- 99 ROOF ACCESS LATCH, SEE FIGURE 7.8
- 100 ROOF ACCESS LATCH, SEE FIGURE 7.8



WEST ELEVATION (FIG. 6.7)

SOUTH ELEVATION (FIG. 6.6)



**Proposed Fulton Pump Station Replacement
Land Use Review**

Elevations

**MWA
Architects**

W 12-143673 02

Exh. C.24

August 2012
10-1173.500

Approved

City of Portland - Bureau of Development Services

Planner MARLE WALKER Date OCT. 4, 2012

This approval applies only to the reviews requested and is subject to conditions of approval. Additional zoning requirements may apply.

W 12-143673 02