

# Grant Park Neighborhood Association

April 5, 1980

Mr. D.E. Bergstrom  
City Traffic Engineer  
420 S.W. Main Street  
Portland, Oregon 97204

Dear Mr. Bergstrom:

Re; Proposed Traffic Plan ---  
N.E. 36th Avenue & Brazee Street

On behalf of the Grant Park Neighborhood Association, I am sending to you petitions requesting a traffic barrier to be constructed on N.E. 36th Avenue immediately north of the intersection of Brazee Street. The petitions are from the following groups of people.

1. 75+% of the residents living in effected area.  
This area was defined to us by the Bureau of Traffic Engineering and generally is bounded by Stanton on the north, Tillamook/U.S. Grant Place on the south, 35th on the west and 38th on the east. A tally sheet is attached showing neighborhood response to the plan.
2. A majority of the parents of students at Hollyrood School.
3. The entire staff at Hollyrood School.

This petition drive is the result of a general neighborhood meeting which was called by the board of directors of the Grant Park Neighborhood Association on November 19, 1979. At this meeting, citizens were asked to comment and voice their support or non-support for the traffic barriers. A motion was made and it passed by a solid majority to request your office to erect one barrier at 36th and Brazee. We were instructed by The Bureau of Traffic Engineering that the next step would be to obtain the support of the surrounding neighborhood. We have accomplished that and we are ready to move on to the next plateau.

In addition, Mr. Stan Blair, principal at Grant High School has given his support to this project.

RECEIVED

APR 16 1980

BUREAU OF  
TRAFFIC ENGINEERING

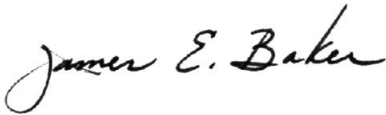
Grant Park Neighborhood Association  
3250 NE U.S. Grant Pl  
Portland, Oregon 97212



Mr. D.E. Bergstrom  
April 5, 1980  
Page 2

I would like to acknowledge that a lot of hard work went into the canvassing of the neighborhood in order to achieve 75% support. The supporters of this project trust that The Bureau of Traffic Engineering will proceed as rapidly as possible with the next step in implementing this most needed project. Please advise us as to what help we might be able to give to your office.

For the Grant Park Neighborhood Association,

A handwritten signature in cursive script that reads "James E. Baker". The signature is written in dark ink and is positioned above the printed name.

James E. Baker, President

cc: Mayor Connie McCready  
William R. White



## STANTON-S

3560	x	
3566	x	
3570	x	
3574	x	
3578	x	
3586		-
3604	x	
3614	x	
3632	x	

## MATHISON-N

3561	x	
3565	x	
3571	x	
3575	x	
3581	x	
3585	x	

## MATHISON-S

3556	x	
3570	x	
3574		-

## KNOTT-N

3565	x	
3571	x	
3577	x	
35--		(VACANT)
3585	x	
3615		-
3711	x	

## KNOTT-S

3554		-
3560	x	
3564	x	
3570	x	
3576	x	
3584	x	
3610	x	
3616	x	

## HOLLYROOD CT-N

3553	x	
3557	x	
3565	x	
3569		-
3575	x	
3581	x	

## THOMPSON NTS

3705	x	
3711	x	
3706	x	

## U.S. GRANT

3504	x	
3514	x	
3520	x	
3528	x	
3536	x	
3546	x	
3556		-
3566		-
3576	x	
3584	x	
3604	x	
3616	x	
3605		-

STREET	YES	NO	NO ANS.	TOTAL
35 Pl. W.	11	4	1	16
35 Pl. E	7	2	0	9
36 W.	4	0	0	4
36 E.	21	7	5	33
37 W.	29	2	6	37
37 E.	28	3	3	34
38 W.	30	3	5	38
STANTON	7	1	1	9
MATHISON	7	1	1	9
KNOTT	11	1	3	15
HOLLYROOD CT	2	3	1	6
THOMPSON	3	0	0	3
U.S. GRANT	10	0	3	13
TOTALS =	170	27	29	226
% =	75.2%	11.9%	12.8%	

Tally Sheet of  
Blocks for sign-  
atures-36th Ave.  
Traffic Plan



37 <sup>TH</sup> W	Y N %	37 <sup>TH</sup> E	Y N %	38 <sup>TH</sup> W	Y N %
2105	x	2104	x	2107	x
2115	x	2114	x	2115	x
2123	x	2124	x	2127	x
2135	x	2134	x	2137	x
2203	x	2204	x	2205	x
2215	-	2214	x	2217	-
2225	x	2224	x	2225	x
2235	x			2235	-

2303	x	2324	x	2307	x
2305	x	2334	x	2315	x
2323	x	2344	x	2331	x
2333	-	2404	x	2343	x
2345	x	2414	x	2405	x
2403	x	2424	x	2415	x
2415	x	2434	x	2425	x
2425	x	2444	x	2433	x
2435	x			2443	-
2443	-				

2505	-	2504	x	2503	x
2515	x	2514	x	2515	x
2525	-	2526	x	2521	x
2535	x	2536	x	2531	x
2545	x	2544	-	2535	x
2605	x	2604	x	2545	x
2615	x	2614	x	2605	x
2625	x	2624	x	2615	x
2635	x	2634	x	2623	x
2645	x	2644	x	2635	x
				2645	x

2705	x	2714	-	2705	x
2715	x	2722	x	2715	x
2721	x	2734	x	2723	-
2735	x	2744	-	2735	x
2745	x	2804	x	2743	-
2805	x	2814	x	2805	x
2815	x	2824	x	2815	x
2825	x	2834	x	2825	x
2835	-	2844	x	2835	x
				2845	x

37<sup>TH</sup> W = 29/2/6

37<sup>TH</sup> E. = 28/3/3

38<sup>TH</sup> W. = 30/3/5



35<sup>PL</sup> W Y N %35<sup>PL</sup> E. Y N %36<sup>TH</sup> W. Y N %36<sup>TH</sup> E. Y N %

2505 x -  
 2521 -  
 2525 x  
 2605 x  
 2615 x  
 2625 x

2705 x  
 2715 x  
 2725 x  
 2735 x  
 2745 x  
 2755 x  
 2801 x  
 2805 x  
 2815 x  
 2825 x

2702 x  
 2714 x  
 2724 x  
 2734 x  
 2744 x  
 2804 x  
 2814 x  
 2824 x  
 2834 x

2615 x

2725 x  
 2735 x  
 2745 x

2114 x -  
 2124 x  
 2134 x  
 2204 x  
 2214 x  
 2224 x  
 2234 x

2308 x  
 2314 x  
 2324 x  
 2336 x  
 2344 -  
 2404 x  
 2414 x  
 2424 x  
 2434 x  
 2442 x

2504 x  
 2514 -  
 2524 -\*(VACANT)  
 2534 x  
 2544 x  
 2550 x  
 2604 x  
 2616 -

2710 x  
 2724 x  
 2734 x  
 2742 x  
 2756 x  
 2804 x  
 2812 x  
 2824 x

38<sup>TH</sup> PL. W = 11/4/135<sup>TH</sup> PL. E = 7/2/036<sup>TH</sup> W. = 4/0/036<sup>TH</sup> E. = 21/7/5







May 8, 1979

Mr. D. E. Bergstrom  
City Traffic Engineer  
Bureau of Traffic Engineering  
420 S. W. Main Street  
Portland, Oregon 97204

We, the undersigned, property owners and/or residents of  
Grant Park neighborhood area as shown, hereby  
request that the City of Portland consider the request to install a  
traffic control plan, as shown on the attached map. It is our  
understanding that the City shall follow the prescribed procedures  
in considering all requests for the installation of traffic diverters.  
We also understand that a public hearing before the City Council will  
be held and as a result of this hearing, the City Council will  
determine by vote whether the control plan should be retained on a  
permanent basis.

<u>NAME</u>	<u>ADDRESS</u>	<u>DATE</u>
Mary Lou Yakym	2414 NE 36	3/31/80
Robert R. Winkler	2424 NE 36	3/31/80
Michael Murphy	2531 NE 38	"
Sandra Stofel	2815 N.E. 38th	3/31/80
David H. Cottle	2324 N.E. 37th	3/31/80
Carl F. Berrendt	2824 N.E. 37th	3/31/80
Robert Haggart	2505 NE 39th	3/31/80
John M. Miller	2806 NE 35th	3/31/80
Marie Weber	2525 N.E. 35th Pl.	"



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<u>NAME</u>	<u>ADDRESS</u>	<u>DATE</u>
Janet C. Alken	2515 NE 38 <sup>th</sup>	1-6-80
Jerry D. Wright	2503 NE 38 <sup>th</sup>	1/6/80
Bonnie W. James	2504 NE 37 <sup>th</sup>	1/6/80
Susan J. Nielson	2535 NE 38 <sup>th</sup>	3/18/80
Jean W. Hancey	2605 N.E. 38 <sup>th</sup>	3/18/80
Michael D. Mason	2615 NE 38 <sup>th</sup>	3/18/80
Anthony P. Jura	2623 NE 38 <sup>th</sup>	3/18/80
Bob McConnel	2635 NE 38 <sup>th</sup>	3/18/80
Linda Ruerman	2645 NE 38 <sup>th</sup>	3/18/80
Mrs. Harry Wood	2705 NE 38	
Mr. Ben Young	2715 NE 38 <sup>th</sup>	3/18/80
R.B. Harrison	2805 N.E. 38 <sup>th</sup>	3-18-80



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<u>NAME</u>	<u>ADDRESS</u>	<u>DATE</u>
GEDRENE Lapping	2702 NE. 35th	3/17/80
Boone & Sandra Mortenson	2744 NE 35th Place	3/17/80
Karen A. Van Giew	3560 N.E. Stanton	3-17-80
Edward Larson	3566 N.E. Stanton	288-1590
Robert A. Brann	3574 NE Stanton	281-9328
Ester Vidlund	3585 N.E. Mathison Pl.	
Yunike J. Smith	3581 NE Mathison Pl.	3/17/80
Lorise L. Gimmernan	3575 N.E. Mathison Pl.	
L. Norwood	3571 N.E. Mathison Pl.	
Bob Boal	3565 N.E. Mathison	
Dupon Paul Ferron	3561 N.E. Mathison	3/17/80
W. Mathison	3570 N.E. Mathison Pl.	



NAME

ADDRESS

DATE

Mrs. A. Cree	3576 NE Knott	3/19
Clarence D. Lamon	3616 NE Knott	
Laura M. Oien	280 N. N.E. 35 P/Ce	
A. de Jesus	2834 N.E. 35 Place	3-18-80
Val Johnson	3570 NE STANTON	
Emmett Stahly	3604 N.E. Stanton	
C. Stevenson	2824 NE 36 <sup>th</sup>	3/18
Victor C. Meyer	2812 NE 36 <sup>th</sup>	3/18
Susan Sharper	2884 NE 36 <sup>th</sup>	3/18
Virginia M. Kelley	2742 N.E. 36 <sup>th</sup>	3/18
Mrs. John A. Jackson	2734 N.E. 36 <sup>th</sup>	3/18
Shirwood Lish	2724 NE 36 <sup>th</sup>	3/18
James Hish	2724 NE 36 <sup>th</sup>	3/18
Roger Skutt	2610 NE 36 <sup>th</sup> Ave	3/18
Lydia Lotzenhiser	2725 NE 36 <sup>th</sup>	3/18
Marjellen Cole	2735 NE 36 <sup>th</sup>	
Carolyn C. Ryan	3585 NE Knott	3/18
Leila D. Ready	3577 NE Knott	3/18
Michael D. Steag	" "	"
Paul L. Smack	3571 NE Knott	M
Louette Smack	3571 NE Knott	3/18/80
Ann Marie Krager	2565 NE Knott	3/18/80
Clifford S. Landene	3564 NE Knott	3/18/80
Gail Nozies	3570 NE Knott	3/18/80
Frank J. Faheny	3584 NE Knott	3/18/80
Wm. M. Wree	3575 NE Knott	3/19/80
Uth. Nickerson	2615 NE 36	3/19/80
Mrs. Nickerson	2615 NE 36	3/19/80
Sirithy M. Hammack	2604 NE 36	3/19/80
David M. Harrison	2550 NE 36 <sup>th</sup>	3/19/80
Kathleen J. Eggs	2442 N.E. 36 <sup>th</sup>	3/19/80



NAME	ADDRESS	DATE
James D. Hulby	2545 NE 38 <sup>th</sup>	3/18/80
Wm. K. Anderson	2815 NE 38	3/18/80
Nichole Sherlock	2835 NE 38	3/18/80
Leighton W. Bryson	2735 NE 38 <sup>th</sup>	3/18/80
Elva Lee Bryson	2735 N.E. 38 <sup>th</sup>	3-18-80
Mary Lou Coulton	2644 N.E. 37 <sup>th</sup>	3-18-80
Gemini K. Phillips	2614 N.E. 37 <sup>th</sup>	3-18-80
Peter Changko	2536 NE 37 <sup>th</sup>	3-18-80
Geranna Fleming	3581 NE Hollywood Ct.	3-29-80
Dana Stadelman	3610 NE KNOTT	3-29-80
Synda Crutcher	2745 NE 36	3-29-80
Roseanna Silva	2814 N.E. 35 <sup>th</sup> pl	3-29-80
Jan Benzely	2825 N.E. 35 <sup>th</sup> pl	3-29-80
Jim Friday	2805 NE 35 <sup>th</sup> pl	3/29/80
Heleen E. Gugg	2745 NE 35 <sup>th</sup> pl	3/29/80
Alice Hartness-Reichle	2734 N.E. 35 <sup>th</sup> pl	3/29/80
Maula Heikkila	2724 NE 35 <sup>th</sup> pl	3/29/80
Virginia B. Winchester	2303 N.E. 37	3/29/80
Walter E. Bryant	2415 N.E. 38 <sup>th</sup>	3/29/80
James R. Rose	2425 NE 38	3/29/80
Thomas G. Thomas	2514 NE 37 <sup>th</sup>	3/29/80
Mary Snider	2604 NE 37	"
Sue Snider	2604 NE 37	"
Jeanne Barber	2634 NE 37	3/29/80
Ruth Seeronen	2535 NE 37	3/30/80
William M. Probst	2415 NE 37	"
Alvin T. Vanth	2114 NE 37 <sup>th</sup>	3/30/80
Cathy W. Christen	2124 NE 36	3/30/80
Barbara J. Henderson	2224 N.E. 37 <sup>th</sup>	3/30/80
Georgia Lott	3614 NE Stanton	"



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<u>NAME</u>	<u>ADDRESS</u>	<u>DATE</u>
<u>Richard Lusk</u>	<u>2123 NE 37<sup>th</sup></u>	<u>1-3-80</u>
<u>Artha Egan</u>	<u>2115 N. E. 37th</u>	<u>1-3-80</u>
<u>James A. Egan</u>	<u>2115 NE 37TH</u>	<u>1-3-80</u>
<u>Dennis Kramer</u>	<u>2124 NE 37<sup>th</sup></u>	<u>1-3-80</u>
<u>Judy Pomeroy</u>	<u>2134 NE 37<sup>th</sup></u>	<u>1-3-80</u>
<u>Frankie Paulson</u>	<u>2204 NE 37<sup>th</sup></u>	<u>1-3-80</u>
<u>Jim A. Paulson</u>	<u>" "</u>	<u>" "</u>
<u>Mr. P. L. L. L.</u>	<u>2214 NE 37<sup>th</sup></u>	<u>" "</u>
<u>Michelle L. Egreen</u>	<u>3706 NE Thompson St.</u>	<u>1-3-80</u>
<u>Ernest Stocker</u>	<u>2235 N. E. 37<sup>th</sup></u>	<u>" "</u>
<u>B. H. Miller</u>	<u>2225 N. E. 37<sup>th</sup></u>	<u>" "</u>
<u>May DeLong</u>	<u>2203 NE 37<sup>th</sup></u>	<u>" "</u>



[illegible]



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Richard Kirschner	2305 N.E. 37 <sup>th</sup>	1/3
Kathy S. Draper	2323 NE 37 <sup>th</sup>	1/3/80
A. J. Arlison	2345 N.E. 37 <sup>th</sup>	1/3/80
Joseph K. Arlison	2345 N.E. 37	1/3/80
Edith M. Anderson	2403 N.E. 37 <sup>th</sup>	1/3/80
Jack T. Sanderson	2403 NE 37	1/3/80
Jane Wright	2425 NE 37	1/3/80
Gene Haan	2435 N.E. 37	1/3/80
Bob Durand	2444 NE 37	1-3-80
Betty E. Jones	2336 N.E. 36	1-3-80
Paul B. Romans	2324 NE 36	1/3/80
Synelle T. Herber	2314 N.E. 36 <sup>th</sup>	1/3/80



NAME	ADDRESS	DATE
Louie Brunton	2314 N.E. 36 <sup>th</sup>	1-3-80
off R. K. Cuba	2314 N.E. 36 <sup>th</sup>	1-3-80
Doris Campbell	2308 N.E. 36	1-3-80
Cathy Tebock	2225 N.E. 38 <sup>th</sup>	1-4/80
Bruce Tebock	2225 N.E. 38 <sup>th</sup>	1/4/80
John M. Steele	2115 N.E. 38 <sup>th</sup>	1/3/80
Robert C. McWhite	3711 N.E. Thompson	1/3/80
Mary Anne Hillite	3711 N.E. Thompson	1/3/80
Deane C. Romano	2324 N.E. 36 <sup>th</sup> Ave	1-3-80
Larry Thomas	2404 N.E. 36	1-3-80
Richard Thomas	2404 N.E. 36 <sup>th</sup>	1-3-80



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<u>NAME</u>	<u>ADDRESS</u>	<u>DATE</u>
<u>Paula Lusk</u>	<u>2123 NE 37th</u>	<u>12/12/79</u>
<u>Blanche Wagner</u>	<u>2334 NE 37th</u>	<u>1/2/80</u>
<u>Lisa Carlshaw</u>	<u>2344 NE 37th</u>	<u>1/3/80</u>
<u>Manda L. Fitzgerald</u>	<u>2404 NE 37th</u>	<u>1/3/80</u>
<u>Fred Phillips</u>	<u>2414 NE 37</u>	<u>1/3/80</u>
<u>Dorothy W. Langdon</u>	<u>2424 NE 37</u>	<u>1/3/80</u>
<u>W. Livingston</u>	<u>2405 NE 38</u>	<u>1/3/80</u>
<u>Deborah K. Kernan</u>	<u>2343 NE 38</u>	<u>1/3/80</u>
<u>Ava Schaff</u>	<u>2315 NE 38</u>	<u>1/5/80</u>
<u>Joyane Schaff</u>	<u>2315 NE 38</u>	<u>1/5/80</u>
<u>John W. Whitlow</u>	<u>2307 NE 38th</u>	<u>1/3/80</u>
<u>Harold Whitlow II</u>	<u>"</u>	<u>"</u>



NAME	ADDRESS	DATE
Reenie Loebe	2205- <del>NE</del> NE 38 <sup>th</sup> Ave	01/03/80
Robert J. Loebe	" " " "	" "
Susan B. Harwell	2137 NE 38 <sup>th</sup> Ave	1/3/80
Charles H. Harwell	" " " "	" "
Annabel D. Cunningham	2127 NE 38 <sup>th</sup>	1/3/80
Ethel Kay Harding	2107 NE 38 <sup>th</sup>	1/3/80
Elmer Harding	2107 NE 38	1-3-80
Harold J. Kay	2433 NE 38	1-3-80
J & Dodge	3705 N. E. Thompson	1-3-80



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NAME	ADDRESS	DATE
James M. McFayson	3536 NE US Grant Pl	12-12-79
James I. Whelan Jr.	2545 NE 28 <sup>TH</sup>	12-12-79
Dorothy Holman	3504 N.E. US Grant	1/6/80
Susan J. Hicks	3514 NE US Grant Pl.	1/6/80
Bonnie C. Kiddon	3528 NE US Grant Pl.	1/6/80
Betty R. Polen	3546 N.E. US Grant	1/6/80
George G. Blakeslee	3576 NE US Grant	1/6/80
John C. Blakeslee	3576 NE US Grant Pl	1/6/80
Jane L. Blakeslee	3576 NE US Grant Pl	1/6/80
Genny Pietka	3584 NE US Grant Pl	1/6/80
David Pietka	3584 NE US Grant Pl	1/6/80
Donna Fernon	3604 NE US Grant Pl	1/6/80



[illegible]



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NAME	ADDRESS	DATE
Pam Gagnon	2625 NE 35 <sup>TH</sup> PL	Jan 6 <sup>TH</sup> '80
Dan Brumbyke	2615 NE 35 <sup>TH</sup> PL	1/6/80
Steven Schuler	2715 NE 35 <sup>TH</sup> PL	1/6/80
R. FREDRICKSON	2725 NE 35 <sup>TH</sup> PL	1/6/80
David E. Engelhardt	2755 NE 35 <sup>TH</sup> PL	1/6/80
John M. Piller	2801 NE 35 <sup>TH</sup> PL	1/6/80
Shirley L. Pletcher	2801 NE 35 <sup>TH</sup> PL	1-6-80



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420 S. W. Main Street  
Portland, Oregon 97204

We, the undersigned, property owners and/or residents of

Grant Park neighborhood area as shown, hereby

request that the City of Portland consider the request to install a traffic control plan, as shown on the attached map. It is our understanding that the City shall follow the prescribed procedures in considering all requests for the installation of traffic diverters. We also understand that a public hearing before the City Council will be held and as a result of this hearing, the City Council will determine by vote whether the control plan should be retained on a permanent basis.

NAME	ADDRESS	DATE
<u>P.B. Sidabutr</u>	<u>2545 NE 37</u>	<u>1/2/80</u>
<u>Elsie Lund</u>	<u>2605 NE 37th</u>	<u>1/2/80</u>
<u>Mrs. J. D. Walcott</u>	<u>2615 N.E. 37th</u>	<u>2/2/80</u>
<u>Mrs. Malma J. Jorgensen</u>	<u>2615 N.E. 37th</u>	<u>1/2/80</u>
<u>Mr. George H. Voss</u>	<u>2625 NE 37th</u>	<u>1/2/80</u>
<u>Charles A. Mansfield</u>	<u>2645 NE 37th</u>	<u>1/2/80</u>
<u>Susan C. Mansfield</u>	<u>2645 NE 37th</u>	<u>1/2/80</u>
<u>Richard A. Hamlen</u>	<u>2715 NE 37</u>	<u>1/2/80</u>
<u>James L. Bury</u>	<u>2721 NE 37th</u>	<u>1/2/80</u>
<u>Vernice Hanlon</u>	<u>2735 NE 37</u>	<u>1/2/80</u>
<u>Pat Fraser</u>	<u>2805 NE 37</u>	<u>1-2-80</u>
<u>Ruth Wheeler</u>	<u>2815 NE 37th</u>	<u>1-2-80</u>



[illegible]



Mr. D. E. Bergstrom  
City Traffic Engineer  
Bureau of Traffic Engineering  
420 S.W. Main Street  
Portland, Oregon 97204

We, the undersigned, staff of Hollyrood School, support the request to install a traffic control plan on 36th and Brazee.

<u>NAME</u>	<u>ADDRESS</u>	<u>DATE</u>
Helen VanBelle	2806 N.E. Dunckley	1-7-80
Judy Erickson	4024 N.E. 24 <sup>th</sup>	1-7-80
Helen J. Dobbek	4042 S.E. Yamhill	1-7-80
Julianne Winder	3026 N.E. 57 <sup>th</sup>	1-7-80
Tampa Braine	5845 N.E. 23 <sup>rd</sup>	1-7-80
Judy Wong	2904 N.E. 12 <sup>th</sup>	1-7-80
Pam Amato	2233 S.E. Grant	1-7-80
Judie Jensen	1422 N.E. Euclid	1-7-80
Phyllis Larsen	224 N.W. Skyline	1-7-80
Pam Knudsen	3043 N.E. 35 <sup>th</sup>	1-7-80
Len Arnold	1900 Tuton	1-7-80



December 14, 1979

102

Mr. D. E. Bergstrom  
City Traffic Engineer  
Bureau of Traffic Engineering  
420 S.W. Main Street  
Portland, OR 97204

We, the parents of Hollyrood children, hereby sign this petition in hopes that it will show in strength that we are behind a traffic diverter. It is our understanding that the City shall follow the prescribed procedures in considering all requests for the installation of traffic diverters. We also understand that a public hearing before the City Council will be held and as a result of this hearing the City Council will determine by vote whether the control plan should be retained on a permanent basis.

NAME	ADDRESS	DATE
Larry Kent	2504 NE 35 AVE	12-14-79
Randa Gibson	2943 NE 35th	12-14-79
Geneva Bergeson	3114 NE 35th Pl	12-14-79
Jimmy D. Duffield	3136 NE 33rd	12-14-79
Linda R. Duffield	3136 NE 33rd	12-14-79
Robert M. Allen	2414 NE 44th	12-14-79
Nancy I. Albert	2414 NE 44th	12-14-79
Debbie Moyer	3319 NE Tillamook	12-14-79
Eleanor Bell	2116 NE 52	12-14-79
Randa Kuder	3121 NE 34	12-14-79
Leila Ready	3577 NE Knott St.	12-14-79
Beggy Kuhn	2535 NE 32 CT	12-14-79
[Signature]	2535 NE 32 CT	12-14-79
Patty Hyatt	1915 NE 37th AVE	12-14-79
Harold E. McNeil	2724 NE 35 AVE	12-14-79
Nancy Dennis	2732 NE 35 Av	12-14-79
Joe Patula	2732 N.E. 35	12-14-79
Mary E. Namis	3601 NE Maltby	12-14-79
Barbara Wood	2114 NE 45th	12-14-79



Yinda Heiney

2714 NE 40<sup>th</sup>

Dec 14, 1979

Bob Heiney

2714 NE 40<sup>th</sup>

Dec. 14, 1979

Star Bahle

2305 NE 41st.

Dec 14, 1979

William R White

2605 NE 34

DEC 18, 1979



December 14, 1979

Mr. D. E. Bergstrom  
City Traffic Engineer  
Bureau of Traffic Engineering  
420 S.W. Main Street  
Portland, OR 97204

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NAME	ADDRESS	DATE
Mr. & Mrs. Charles Ketchel	2614 N.E. 44th	12-14-79
Mrs. & Mrs. Bruce Campbell	3044 N.E. 50th	12-14-79
Geraldine Malpasian	2715 N.E. 40	12/14/79
Barbara & Frank Lembrink	2725 NE 40th Avenue	12/14/79
Connie Berlinger	4534 SE 35	12/14/79
Robert Kehler	2632 NE 33RD	14 Dec 79
Kette Apao	1211 NE 27th Ave	12/14/79
Jay Helner	2544 NE 37th	12/14/79
John & Rosythe	2551 N.E. 33rd	12/14/79
Jean Mandic	3425 NE Stanton	12/14/79
Judy Noland	3519 NE Tillamook	12/14/79
Kathy Burger	2914 NE Knott	12/14/79
Sandy Ingolia	3511 NE Tillamook	12/14/79
John & Cathy Christen	2124 NE 36	12/14/79
Delbie Louett	3021 NE Knott	12/14/79
Sally Rue	3006 NE 32nd Pl.	12/14/79
Conrad J. Rue	3006 N.E. 32nd Pl.	12/14/79
Laura & Richard Lusk	2123 NE 37th Ave	12/14/79
Sam & Angelina McKinney	2525 NE 39th Ave	12-14-79
James A. Nancarrow	2704 NE 41st Ave	12/14/79



Judith C. Crary	4306 N.E. Skidmore	284-8361
Richard Metro <sup>MM/M</sup>	2635 NE 42 <sup>nd</sup>	288-2398
C. Stevenson	2824 NE 36 <sup>th</sup>	287-8503
Dick Spain	<del>35</del> 5026 NE 35 <sup>th</sup>	287-1588
Paul M. Am.	2929 NE 33 <sup>rd</sup>	281-4368
Michael Ready	3577 NE Knott	284-1091
Bl. Kelleway	3024 N.E. 35 <sup>th</sup> Ave	284-8539
G. Rosenberger	5903 NE 23 <sup>rd</sup>	284-5393
Ken Arnold	3560 NE Hollywood Ct	284-6766



December 14, 1979

Mr. D. E. Bergstrom  
City Traffic Engineer  
Bureau of Traffic Engineering  
420 S.W. Main Street  
Portland, OR 97204

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NAME

ADDRESS

DATE

Mrs. Lynne Weisfall	2815 NE 39 <sup>th</sup>	Dec. 14, 1979
Mr. & Mrs. John Mitchell	2834 N.E. 37 <sup>th</sup>	" " "
Mrs. & Mrs. John H. Wells, Jr.	2416 N.E. Halsey	Dec. 14, 1979
Mr. & Mrs. Charles R. Clinton	2012 NE 27 <sup>th</sup>	12-14-79
Mrs. Lynette Day	2923 NE 35 <sup>th</sup> Pl	12/14/79
Patricia A. White	2605 N.E. 34 <sup>th</sup>	12/14/79
Robert E. Wood	2803 NE Tillamook	12/14/79
Robert E. Wood	2426 NE 44 <sup>th</sup>	12/14/79
Mr. Ben Denzely	2525 N.E. 35 <sup>th</sup> Pl	12/14/79
Mrs. Thomas E. Johnson	3533 NE Horton	12/14/79
Mrs. John Langenhuis	1929 NE 37 <sup>th</sup>	12/14/79
Mrs. Catherine Schlemmer	2506 NE 49	12/14/79
Mrs. Linda K. Krepper	3105 N.E. 36	12-14-79
Mr. & Mrs. H. J. Brown	3015 NE 35 <sup>th</sup> Pl	12-14-79
Mrs. Mardie Nelson	2315 N.E. 40 <sup>th</sup>	12-14-79
John A. Rotmans	2315 NE 40 <sup>th</sup>	12-14-79
Jan Volmer	3809 N. Vancouver Ave	12-14-79
Prathy Lily	3827 N. Vancouver	287-2189
Patricia Dean	5434 NE Skidmore	288-8076
Carol Hamel	2814 NE 37 <sup>th</sup>	281-5866



Pat & Paul Martin	3520 NE Schuyler	Dec. 14, 1979
Mrs & Mrs S. Denkers	2404 NE 14th	12/14/79
Cathy Karson	1905 NE 28	12/14/79
Reyllis Eastman	3328 N.E. Tillamook	12-14-79
Janet Wright	2503 NE 38 <sup>th</sup>	12-14-79
Risa Horstman	2604 NE 38	12/14/79
Mrs. Lee Heimburger	4267 N.E. Laurelhurst	12-14-79
Mrs. Alan Stofil	2815 N.E. 38 <sup>th</sup>	12-14-79



December 14, 1979

Mr. D. E. Bergstrom  
City Traffic Engineer  
Bureau of Traffic Engineering  
420 S.W. Main Street  
Portland, OR 97204

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NAME

ADDRESS

DATE

Mrs. Pat Kopp	2504 NE 35th	12-14-79
Mr + Mrs Paul Seim	2325 NE 41st	12-14-79
Therese Lister	1818 NE 27th	12-14-79
Steven R. Parker	2535 NE 45th	12-14-79
Sharon Goodell	2534 NE 44th	12-14-79
Joe Ballance	3024 NE 35th Ave.	12-14-79
Mr + Mrs Roger W. Stark	2405 NE 32nd Pl.	12-14-79



December 14, 1979

Mr. D. E. Bergstrom  
City Traffic Engineer  
Bureau of Traffic Engineering  
420 S.W. Main Street  
Portland, OR 97204

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NAME	ADDRESS	DATE
Mr. & Mrs. Gary Lawrence	2136 NE 39 <sup>th</sup> Ave, Ptld	12-14-79
Mr. & Mrs. Gary Christensen	3544 N.E. Schuyler St., Ptld.	12-14-79
Mr. & Mrs. Lora Langston	3124 N.E. Knott Ptld.	12-14-79
Mr. & Mrs. David Larson	2205 NE 4 <sup>th</sup> Ave Ptld	12/14/79
Mr. & Mrs. Robert Forsythe	2551 NE 32 <sup>nd</sup> Pl Ptld	12/14/79
Mr. & Mrs. John Drake	2344 NE 39 <sup>th</sup> "	"
Julie Brinker	1914 NE 45 <sup>th</sup>	12/14/79
Mr. & Mrs. Don Bismar	3349 N.E. Schuyler Portland	12/14/79
Al Iconi	3231 N.E. Schuyler Ptld.	12/14/79
Norma Karsen	3213 NE Hayt "	12/14/79
Wendy	1000 NE Knott "	12/14/79
Penny Schneider	2730 N.E. Weidner "	12/14/79
Suzanne E. Sindelar	2316 NE 40 <sup>th</sup> Port.	12-14-79
Elith Larson	3150 SE Brooklyn 97202	12/14/79
Michael G. Jordan	3560 N.E. Hollyrood Ct 97212	12/14/79



CITY OF PORTLAND, OREGON  
DEPARTMENT OF PUBLIC WORKS

MIKE LINDBERG, COMMISSIONER  
JOHN LANG, P.E., CITY ENGINEER

---

TRAFFIC DIVERTER  
AT INTERSECTION OF  
N.E. 36th AVE. and N.E. BRAZEE ST.

JOB 6093



1981



## TABLE OF CONTENTS

NOTICE TO BIDDERS

PROPOSAL

BID BOND

SPECIAL SPECIFICATIONS

SPECIFICATIONS FOR TRAFFIC CONTROL

SPECIFICATIONS FOR LANDSCAPING

STANDARD PLANS

SIGN INSTALLATION CERTIFICATION

STREET AND LANDSCAPING PLANS



2

## NOTICE TO BIDDERS

Instructions regarding bidding, award of contract, bonding, execution of contract, and other related information are included in Division I, General Requirements, Standard Construction Specifications of the Department of Public Works, City of Portland, Oregon. The following information and check list of required actions is presented for the use and convenience of bidders. It is not guaranteed to be complete or accurate and the responsibility for compliance with all requirements remains with the bidder.

### Information for Bidder:

Federal Funds ( ) will (X) will not be used for this project.

### Bidders Check List: (Check off as completed)

1. Certification as an EEO Affirmative Action Employer  
(At least 5 days prior to bid opening). ( )
2. Certification of nonsegregated facilities. ( )
3. Bidder currently prequalified with the City of Portland  
at least 9 days prior to bid opening. ( )
4. Proposal Guarantee  
(Bid bond, certified check or cashier's check accompanying  
bid or adequate bid bond on file). ( )
5. Addenda  
(Check for receipt of all addenda before submitting bid). ( )
6. Proposal  
(Proposal clearly and accurately completed, signed and  
double checked; sealed in properly identified and  
addressed envelope; submitted before closing time for  
receipt of bids). ( )

(See following page for names, addresses and phone numbers)



3

NAMES, ADDRESSES AND PHONE NUMBERS

The following names, addresses and phone numbers are included for the convenience of the bidders and Contractor:

Certification as an EEO Affirmative Action Employer:

Contract Compliance Office  
Room 209, City Hall  
1220 S.W. Fifth Avenue  
Portland, Oregon 97204  
(503) 248-4696

Prequalification of Bidders:

Right-of-Way Services  
Room 530  
621 S.W. Alder Street  
Portland, Oregon 97205  
(503) 248-4240

Submittal of Bids (Place return address on outside of envelope):

Purchasing Division  
Room 412, City Hall  
1220 S.W. Fifth Avenue  
Portland, Oregon 97204  
(503) 248-4001

Notification of Completion (by certified mail):

Street Inspection Division  
Bureau of Street & Structural Engineering  
Room 506  
621 S.W. Alder Street  
Portland, Oregon 97205  
(503) 248-4116

Clarifications and Addendums:

HCD Engineering  
Bureau of Street and Structural Engineering  
Room 618  
621 S.W. Alder Street  
Portland, Oregon 97205  
(503) 248-4651



CITY OF PORTLAND, OREGON  
**PROPOSAL**

Page 1 of 3 Pages

Job No. 6093

Ord. No. \_\_\_\_\_

TO THE COUNCIL:

The undersigned, having full knowledge of the quality of the material and labor to be performed, hereby proposes to perform all labor and furnish all materials necessary for the improvement of:

Traffic Diverter at Intersection of N.E. 36th Avenue and N.E. Brazee Street.

for the unit prices set opposite the different items of the material to be furnished and the work to be done as herein below stated.

ITEM No's	ITEMS OF WORK AND MATERIALS	EST. QUAN.	UNIT MEASURE	UNIT PRICES	TOTAL \$
	<u>STREET ITEMS</u>				
1	Excavation, Common	60	Cu.Yds.		
2	Excavation, Concrete	3.5	Cu.Yds.		
3	Excavation, Common Trench	18	Cu.Yds.		
4	Embankment, Common Backfill Material	37	Cu.Yds.		
5	Concrete Sidewalk	31	Sq.Yds.		
6	Concrete Driveway, 6" Thick	24	Sq.Yds.		
7	Concrete Curb	89	Lin.Ft.		
8	8-inch Concrete Pipe	36	Lin.Ft.		
9	Filling Abandoned Inlet	1	Each		
10	Inlet	1	Each		
11	Aggregate Base 6" Thick (1½"-0)	18	Cu.Yds.		
12	Asphalt Conc.Pavement, Cl."C" 3" Thick	81	Sq.Yds.		
13	Skinpatch	2	Tons		
14	Install Seven Signs Only	23	Sq.Yds.		
15	Install Sign Post Only	4	Each		
16	Install Sign Brackets Only	14	Each		
17	Painting New Curb in Yellow	Lump	Sum		
	<u>LANDSCAPING ITEMS</u>				
18	ACER Rubrum 'Sunset' Red Sunset Maple 3-1½"-4" Caliper	3	Each		
19	Three Way Mix Top Soil	12	Cu.Yds.		
20	Fertile Bark Mulch	4	Cu.Yds.		
21	Cotoneaster Microphylla in 1 Gal.Containers	350	Each		
	Bidder's Signature _____				
	<b>GRAND TOTAL</b>				



CITY OF PORTLAND, OREGON  
PROPOSAL

Page 2 of 3 Pages

Job No. 6093

Ord. No.

TO THE COUNCIL:

The undersigned, having full knowledge of the quality of the material and labor to be performed, hereby proposes to perform all labor and furnish all materials necessary for the improvement of:

Traffic Diverter at Intersection of N.E. 36th Avenue and N.E. Brazee Street.

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	Bidder's Signature				
	GRAND TOTAL				



CITY OF PORTLAND, OREGON  
PROPOSAL

Page 3 of 3 Pages

Job No. 6093

Ord. No. \_\_\_\_\_

And I hereby agree that this proposal is made subject to Section 102, Proposal Requirements and Section 103, Award and Execution of Contract of the Standard Construction Specifications of the City of Portland or as amended by the Special Specifications.

I further agree that this bid shall be irrevocable for at least 30 calendar days after the bid opening date and time, and if accepted, to construct said project at the prices bid within the time specified.

The undersigned bidder hereby represents as follows: That this bid is made without connection with any person, firm or corporation making a bid for the same project, and is in all respects fair and without collusion or fraud.

Bidder \_\_\_\_\_

Licensed to do business in Oregon? Yes \_\_\_\_\_ No \_\_\_\_\_

Form of Organization \_\_\_\_\_

State of Incorporation \_\_\_\_\_

Names of Partners (if Co-Partnership) \_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

By (Signature) \_\_\_\_\_

Name Typed \_\_\_\_\_

Title \_\_\_\_\_

Date \_\_\_\_\_

Address \_\_\_\_\_

City \_\_\_\_\_ State \_\_\_\_\_ Zip \_\_\_\_\_

Phone No. ( \_\_\_\_\_ )  
include area code



BID BOND

KNOW ALL MEN BY THESE PRESENTS that we, \_\_\_\_\_

\_\_\_\_\_, as Principal, and \_\_\_\_\_  
\_\_\_\_\_, a corporation organized and existing under  
the laws of the State of \_\_\_\_\_ and duly authorized to transact a  
surety business in the State of Oregon, as surety, are held and firmly bound unto  
the CITY OF PORTLAND, a municipal corporation of the State of Oregon, in the penal  
sum of \_\_\_\_\_

Dollars (\$ \_\_\_\_\_)  
lawful money of the United States of America, for the payment whereof well and duly  
to be made, we and each of us, jointly and severally bind ourselves, our heirs, ex-  
ecutors, administrators, successors and assigns, firmly by these presents.

THE CONDITIONS OF THIS OBLIGATION ARE SUCH that whereas the Principal has  
submitted or is about to submit a proposal irrevocable for the period specified in  
the proposal to the Oblige on a contract for \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

NOW THEREFORE, in the event the Principal seeks to revoke his offer for  
any reason not authorized by law and not consented to by Oblige within the irrev-  
ocable period and if awarded the contract and the said Principal fails, neglects,  
or refuses to enter into a contract to perform said work and furnish said labor,  
equipment and/or material, and to furnish performance and labor material payment  
bonds as required within the time specified, then the amount herein stated shall  
be delcared to be forfeited and become due and payable to the City of Portland.

SIGNED, SEALED AND DATED this \_\_\_\_ day of \_\_\_\_\_ 19\_\_\_\_.

\_\_\_\_\_  
Principal

\_\_\_\_\_  
Address

BY \_\_\_\_\_

Title

\_\_\_\_\_  
Surety

BY \_\_\_\_\_

Attorney in Fact

\_\_\_\_\_  
Oregon Agent for Service

\_\_\_\_\_  
Address

CORPORATE SEAL (PRINCIPAL)

CORPORATE SEAL (SURETY)



8

NOTE

If Principal is operating under an assumed business name, there must also be set forth in the first paragraph of the bond the names of all the partners or the individual owning the business, and the bond must be executed by one of them.

If the Principal is a corporation, the bond must be executed by one of the officers authorized to execute bonds, showing his official title and the seal of the corporation.

The bond must be executed by an attorney-in-fact for the surety company, show on the face thereof the Oregon agent for service, and bear the seal of the surety company. Where the bond is executed by a person outside the State of Oregon, his authority to execute bonds should be shown.



## SPECIAL SPECIFICATIONS

### 1. SCOPE OF WORK

This Contract is for the improvement of: Traffic Diverter at Intersection of N.E. 36th Avenue and N.E. Brazee Street.

and shall consist of those items of work listed in the Proposal and as shown on the accompanying plans. The Contractor shall perform all labor, and furnish all materials, equipment and incidentals required to make each item complete.

### 2. INCORPORATION OF STANDARD CONSTRUCTION SPECIFICATIONS

The legal and general construction details of this project shall conform to the requirements of the "Standard Construction Specifications" of the Department of Public Works, City of Portland, Oregon. These "Standard Construction Specifications" may be purchased direct from the Auditor's Office, Room 202, City Hall or the City Engineer's Office, Room 530, 621 SW Alder Street, Portland, Oregon 97205. Mail orders are available only from the City Engineer's Office. The price per book is \$20 for a direct sale, or \$22 for a mail order.

### 3. TIME FOR COMPLETION

Contractor shall commence work within ten (10) calendar days and complete the entire project within 45 calendar days from the date of receipt of the Notice to Proceed but completion must occur not later than April 6, 1981. If not previously requested by the Contractor, the Notice to Proceed will be issued fifty five (55) calendar days prior to the April 6, 1981 deadline.

The written Notice to Proceed will be sent to the Contractor after the Contractor submits the signed Contract, Performance Bond and Certificate of Insurance to the City Auditor and these documents have been approved as to form by the City Attorney, signed by the Commissioner in charge and the Mayor and they have been recorded by the City Auditor.

On Federally funded projects (see Notice to Bidders) the Contractor must also submit the following forms prior to the written notice to proceed being sent:

- HUD 1421 Contractor's Certification Concerning Labor Standards and Prevailing Wage Requirements
- HUD 1422 Subcontractor's Certification Concerning Labor Standards and Prevailing Wage Requirements

### 4. PAYMENT TO AUTHORIZED PERSONS

Contractors are required to provide the Auditor's Office with a list of personnel who are authorized to personally receive contract payments.



7.

This written authorization must be signed by an officer of the Contracting company and will be placed on file in the City Auditor's Office. No payment will be released by the Disbursement Desk to an unauthorized person. If such an attempt is made, the payment will be made by mail.

#### 5. NOTIFICATION OF COMPLETION

When the Contractor considers that he has fulfilled all the requirements of the Contract, including cleanup, he shall notify the Street Inspection Division, Bureau of Street and Structural Engineering, Room 506, 621 SW Alder Street, Portland, Oregon 97205, in writing, by certified mail, that the project is completed.

#### 6. AMENDMENTS TO STANDARD CONSTRUCTION SPECIFICATIONS

The following sections of the Standard Construction Specifications have recently been revised or added :

- 102.02 Prequalification of Bidders
- 102.03 Form of Proposal
- 105.20 Access to the Work
- 108.05 Suspension of Work
- 108.06 Delays and Extensions
- 109.06 Payment for Force Account Work
- 202 Temporary Traffic Control:
  - 202.3.00 Construction
  - 202.3.01 General
  - 202.3.05 Temporary Striping
- 204 Excavation, Embankment, Bedding and Backfill:
  - 204.1.03B Common Excavation
  - 204.1.03C Concrete Excavation
  - 204.1.04 Trench Excavation
  - 204.3.08 Common Excavation and Concrete Excavation
  - 204.4.01 Common and Unclassified Excavation
  - 204.4.01D Concrete Excavation
  - 204.4.04 Embankment
  - 204.4.13 Payment
- 208.3.09 Restoring Roadway Surfaces
- 306.4.02 Payment
- 308.2.00 Materials
- 308.2.01 General
- 309 Permanent Traffic Control (Entirely New Section)
- 404.3.05 Filling Abandoned Manholes and Inlets (Construction)
- 404.4.04 Filling Abandoned Manholes and Inlets (Payment)

In addition to the above, the Standard Construction Specifications of the City of Portland are hereby amended as follows:

#### 108.01 Contractor's Construction Schedule

Revised to read as follows:



11.

Within thirty (30) days of Contract award or two (2) weeks in advance of starting work, whichever is earlier, submit for written approval a proposed construction schedule to the Engineer. If it is desirable to carry on operations in more than one location simultaneously, submit a schedule for each location two (2) weeks in advance of beginning such operations. In the event that the Contractor's proposed construction schedule does not meet the necessary construction program schedule as determined by Owner, resubmit a schedule that conforms as approved.

The schedule shall show the proposed order of work and indicate the time required for completion of the major items of work. This working schedule shall take into account the passage of handling of traffic with the least practicable interference therewith and the orderly, timely and efficient prosecution of work. It will also be used as an indication of the sequence of the major construction operations and as a check on the progress of work, but does not become a part of the Contract.

Submit, at the end of each calendar month during construction, an updated schedule and status report of the project showing any problem areas which might delay the project. Failure to provide the required schedules and updates will be considered a default of Contract in accordance with Paragraph 108.12 of the Standard Construction Specifications. Any or all payments due or that may become due to Contractor may be withheld by Owner until the schedules and updates are submitted.



EQUAL EMPLOYMENT OPPORTUNITY (EEO) CERTIFICATION

Contractors must be certified as an Equal Employment Opportunity Affirmative Action Employer. For assistance Contact:

Contract Compliance Division      Phone: 248-4696  
City Hall, Room 209  
1220 SW 5th Ave., Portland, OR      97204

PROVIDE EVIDENCE OF LIABILITY INSURANCE

Contractors shall secure liability insurance which shall hold harmless the City, other governmental agencies, and their officers and agents or employees, for any and all losses to third persons or to the City arising out of the operations of the contractor or any subcontractor in connection with this project. Work shall not be commenced until the required insurance has been obtained and approved by the City Attorney.

The minimum insurance limits shall be:

\$100,000 - injury to each person  
\$300,000 - per occurrence  
\$300,000 - for property damage  
                  or  
\$300,000 - single limit

It shall be without prejudice to coverage otherwise existing and shall carry endorsement provided by the Owner.

WORKMEN'S COMPENSATION INSURANCE

Contractors shall provide evidence of Workmen's Compensation coverage for all employed on the project. Assure that all workmen will receive the compensation for compensable injuries provide in ORS Chapter 656 either by:

- (1) Contributing to the Industrial Accident Fund as contributing employer; or
- (2) Qualifying as a direct responsibility employer as provided in ORS Chapter 656.



13

103.01 AWARD OF CONTRACT

The award will be made by Owner to the Bidder submitting the lowest acceptable bid. In determining the lowest acceptable bid, Owner may take into account, among other factors, the prices bid, discounts, if any, time of completion or delivery proposed, as between equal bids, the relative merits and performance of any item specifically proposed by the Bidder, any variation in maintenance and guaranty period specially proposed by the Bidder in excess of any minimums specified, the realistic balance of prices in the Proposals for various parts or units of work and the experience and ability of Bidder to perform the work.

While price extensions are required as a matter of convenience, in the event of error in extensions, the unit prices bid shall govern. In the event of discrepancy between the written and numerical amounts, the written prices will govern.

Determination of the lowest acceptable Bidder and award are subject to review and determination by the Attorney as to legal sufficiency of any bid submitted.

Award and tender of contract, if it be awarded, shall be made within thirty (30) calendar days after the date of opening of bids.



## CONTRACTOR NOTE:

### 105.09 PROTECTION OF PROPERTY

Protect all public and private property, insofar as it may be endangered by operations and take every reasonable precaution to avoid damage to such property.

Restore and bear the cost of any public or private improvement, facility or structure within the right-of-way or easement which is damaged or injured directly or indirectly by or on account of any act, omission, or neglect in the execution of the work and which is not designated for removal but visibly evident or correctly shown on the Plans. Restore to a condition substantially equivalent to that existing before such damage or injury occurred, by repairing, rebuilding, or otherwise affecting restoration thereof, or if this is not feasible, make a suitable settlement with the owner of the damaged property, all at no expense to Owner.

Give reasonable notice to occupants of buildings on property adjacent to the work to permit the occupants to remove vehicles, trailers and other possessions as well as salvage or relocate plants, trees, fences, sprinkler systems, or other improvements in the right-of-way which are designated for removal or which might be destroyed or damaged by work operations.

Protect all designated trees and planted areas within the right-of-way or easements. Exercise care and conduct operations so as to minimize damages to other planted areas.

Review with Engineer the location, limits and methods to be used prior to clearing work.

Clearing and grubbing shall be performed in strict compliance with all local, State and Federal laws and requirements pertaining to clearing and burning, and particularly in conformity with the provisions of ORS Chapter 477, and all subsequent amendments, which require, among other things, filing with the State Forester a general description of the right-of-way to be cleared before the start of clearing operations. Obtain the required permit from the State Forester and perform clearing work in conformance thereto.



SPECIAL SPECIFICATIONS204.1.03B COMMON EXCAVATION

Delete paragraph and replace with:

"Common Excavation is defined as removal of all material not classified as Rock Excavation or Concrete Excavation."

204.1.03C CONCRETE EXCAVATION

Add this section:

"Concrete Excavation is defined as the removal of all material composed of Portland Cement Concrete with or without reinforcement, that has not been identified in another Bid Item."

204.1.04 TRENCH EXCAVATION

Delete paragraph and relace with:

"Trench Excavation is defined as removal of all material encountered in the trench to the depths and widths as shown or as directed, and is classified as either Common, Concrete or Rock Excavation."

204.3.08 COMMON EXCAVATION & CONCRETE EXCAVATION

Delete first sentence and replace with:

"Perform all excavation defined in Sections 204.1.03B and 204.1.03C regardless of the type or nature or conditions of the material encountered."

204.4.14 CONCRETE EXCAVATION

Add this section:

"Concrete Excavation will be measured on a cubic yard bases. The quantity measured for will include only material excavated from within the limits defined."

204.4.13 PAYMENT

Add Pay Item:

"Concrete Excavation                      C.Y."



16

## REVISIONS TO STANDARD CONSTRUCTION SPECIFICATIONS

### 204. Excavation, Embankment, Bedding and Backfill

#### 204.4.00 Measurement and Payment

##### 204.4.01 Common and Unclassified Excavation

Add: Excavation required for the volume displaced by concrete curbs, driveways, sidewalks, steps and pathways will be considered incidental work and no payment will be made for removal of this material.

##### 204.4.04 Embankment

Add: No deduction in embankment pay quantities will be made for concrete curbs, driveways, sidewalks, steps or pathways within the embankment limits.

### 308.2.00 MATERIALS

#### 308.2.01 GENERAL

Materials shall conform to requirements of Section 205 MATERIALS and to additional requirements contained herein.

Concrete strength for material under this heading must have a design field strength of 3,000 psi in 28 days. Material found to test below the field design strength may be accepted at a reduced rate of 2% for each ten (10) pounds per square inch below the field design strength of the in place bid price of the material.

Any material represented by drilled cores, testing below 2,500 psi, must be removed and replaced at the contractor's expense.



17

306.4.02 PAYMENT

Payment will be made for any or all of the following items when listed as pay items in the Proposal for any particular Contract:

<u>Payment Item</u>	<u>Unit of Measure</u>
1. Asphalt concrete mixture (Specify class)	Per Ton
2. Bituminous Cement in Asphalt Concrete	Per Ton
3. Asphalt Concrete (Specify class)	Per Ton
4. Asphalt Concrete (Specify class & thickness)	Per S.Y.

A deduction of 1% of the in place price will be made for each 1% cumulative deviation from the allowable tolerance of each component of the job mix formula required by the specification, except as follows:

Deviations in asphalt cement shall be weighted 8-times, and deviations in 200-minus material shall be weighted 2-times the deviation in the other specified aggregate sieve sizes.

All materials furnished where the cumulative deviation equals or exceeds 12% shall be removed and replaced with acceptable material at no cost to the owner.

When asphalt paving materials with a cumulative deviation of less than 12% are furnished, the Owner shall notify Contractor, in writing, to remove and replace defective materials at no cost to the Owner or to pay to the Owner liquidated damages in accordance with the above deduction schedule, as determined by the Engineer.

If no in place price has been established, the price shall be equal to: (1) the price under the City's current annual supply contract for furnishing such materials, plus the cost of hauling and placing by City equipment to the area or district in which the work under the contract or permit is to be done, or (2) the Contractor's cost as established by a subcontract for such materials in place, whichever is greater.



18  
309 Permanent Traffic Control

309.1.00 Description

This section covers all work necessary to remove and reinstall existing traffic control signs or install new traffic control signs as required on a construction project.

Additional Traffic Control provisions are contained in Section 105 of the General Requirements and Section 202 of the General Technical Requirements.

309.2.00 Materials

309.2.01 Uniform Traffic Control Devices

Provide signs manufactured and built in conformance with the Manual on Uniform Traffic Control Devices (current edition), published by the U.S. Department of Transportation.

309.2.02 Signs

Use aluminum only for traffic control signs. The sign thickness shall be 0.063" when the sign is to be mounted more than 6 feet above the ground. If the sign is to be mounted 6 feet or less above the ground, the sign thickness shall be 0.125". Sign height is measured from the ground to the lowest portion of the sign.

309.2.03 Sign Posts

Provide steel 2" I.D. (2-3/8" O.D.) posts, 12 feet long or as specified. Sign posts must meet ASTM A-120-72A for welded steel pipe and ASTM A-123-71 for galvanizing. Aluminum pipe will not be allowed.

309.2.04 Sign Brackets

Provide aluminum sign mounting bracket #THDW-500-2 as supplied by Traffic Safety Supply Co. or approved equal. See Standard Plan #119-C.

309.2.05 Sign Foundation

Provide all materials required to accomplish the work as specified in Standard Plan #119-C of the Standard Construction Specifications of the City of Portland.

309.2.06 Reflective Material

Use engineering grade reflective sheeting when reflective material is specified in plans. Otherwise, use baked enamel.



#### 309.2.07 Date

All traffic control signs shall have the date of installation stamped, in a permanent manner, on the back of the sign.

#### 309.2.08 Certification

Furnish certification that signs, posts, brackets, and hardware delivered to the project site are as specified.

### 309.3.00 Construction

#### 309.3.01 Existing Signing

Remove all existing traffic signing as required on the plans. Signs that are to be re-used on the project will be stored by the Contractor until needed. The Contractor will be responsible for the replacement of any signs that are damaged while being stored. All signs, posts and hardware not to be re-used on the project will be delivered, by the Contractor, to the City of Portland at Stanton Yard. When reinstalling traffic signing, use materials specified in Sections 309.2.03 and 309.2.04 as needed.

#### 309.3.02 New Signing and Traffic Devices

Provide and install all new signing as required. New signs must be of new manufacture.

#### 309.3.03 Sign Installation Certification

The Contractor shall certify installation of all signs installed on the project. A form for this will be supplied by the City of Portland for each project.

#### 309.3.04 Street Name Signs

The only signs, posts and brackets a Contractor will not be responsible for installing will be those associated with street name signs. However, the Contractor will be responsible in removing existing street name signs and delivering these signs to the City of Portland at Stanton Yard.

### 309.4.00 Measurement and Payment

#### 309.4.01 Removal of Existing Traffic Control Signs

When listed in the proposal as a separate pay item, payment for removal of existing traffic control signs, including posts and brackets, will be made on a "per each" basis.

#### 309.4.02 Re-installation of Existing Traffic Control Signs

When listed in the proposal as a separate pay item, payment for reinstallation of existing traffic control signs, including posts



20  
and brackets, will be made on a "per each" basis.

309.4.03 Installation of New Traffic Control Signs

When listed in the proposal as a separate pay item, payment for installation of new traffic control signs will be on a "square foot" basis for the signs, and "per each" for the sign support and brackets.

309.4.04 Incidental Basis

When not listed in the proposal for separate payment, all installation of permanent traffic control signs will be considered incidental work for which no separate payment will be made.



## 207 LANDSCAPING

### 207.1.00 DESCRIPTION

This section covers the work necessary for: (A) finish grading, addition of topsoil, fertilizer and weed control, complete; (B) mulching, fertilization and planting of ground cover, establishment of nursery stock, such as trees, shrubs, and small plants and maintenance of ground cover and nursery stock, complete;

### 207.2.00 MATERIALS

#### 207.2.01 PLANTS

Names of plants conform to standardized names of the American Joint Committee on Horticultural Nomenclature. Names of varieties not included therein conform to names generally accepted in the nursery trade. Provide plants which are nursery-grown with habit of growth that is normal for the species, sound, healthy, vigorous, and free from insects, diseases, and injuries and equal to or exceeding measurements specified, measured before pruning with branches in normal position. Provide sizes and methods of handling according to the code of standards recommended by the AAN.

#### 207.2.01A PLANT LIST

3	Acer rubrum 'Sunset'	3½-4" Caliper	Balled & Burlap
Est.	Cotoneaster microphylla	1 gal. can	24" o.c.
Quan.			Plant triangular spacing

#### 207.2.01B PLANT MATERIAL INSPECTION

Contractor shall notify Park Bureau representative 36 hours prior to placement for inspection of material. All plant material rejected shall be replaced by contractor with no added cost to owner.

#### 207.2.01C PLANT MATERIAL SUBSTITUTION

All substitutions of plant material shall be approved by Park Bureau representative.

#### 207.2.04 IMPORTED TOPSOIL

Where imported topsoil is specified in the Contract Documents, provide natural, fertile, friable topsoil, representative of local productive soil, and 90 percent free of clay lumps or other foreign matter larger than 2-inch diameter, not frozen or muddy, with pH 5.0 to 7.0, and not less than 3 percent humus as determined by loss on ignition of moisture-free samples dried at 100 degrees C. Gravel portion (particles larger than 2 mm) shall not exceed 15 percent of total volume. Imported topsoil shall be free of quack grass, horsetail and other noxious vegetation and seed. Should such regenerative material be present in the soil all resultant growth, both surface and root, shall be removed by Contractor within 1 year of acceptance of the work at no expense to Owner.

#### 207.2.05 SAND

Conform to requirements of Subsection 205.2.12C for fine aggregate.



22

207.2.06 PEAT

Use a peat consisting of natural residue formed by decomposition of reeds, sedges, or mosses from freshwater site, free from lumps, roots, and stones, absorbing at least 4 times its dry weight of water, organic matter not less than 90 percent on a dry weight basis, and maximum moisture content at time of delivery of 65 percent by weight.

207.2.07 LIME

Provide a lime composed of ground dolomitic limestone not less than 85 percent total carbonates and magnesium, ground so that 50 percent passes 100 mesh sieve and 90 percent 20 mesh sieve. Coarser material will be acceptable provided the specified rates of application are increased proportionately on the basis of quantities passing 100 mesh sieve.

207.2.10 FERTILIZER

Fertilizer shall be "Agriform" plant tabs, 21-grain tablet, 20-10-5 (NPR) Agriform International Chemicals, Inc., 37650 Sycamore Street, Neward, California 94560. (or approved)

207.2.11 BARK MULCH

Organic mulch shall be medium fine, non-toxic bark mulch, STEERCO. (or approved).

207.2.12 TIE DOWNS

Use the following materials:

1. Wood stakes, 2-inch by 2-inch by 96-inch, clear straight cedar, or approved.
2. Wire for guys, or for fastening trees to stakes, of 12-gauge, pliable galvanized steel.
3. Hose for guy wire encasement, of 2-ply reinforced rubber garden hose, minimum 1/2-inch diameter new or used.
4. Wrapping material of first quality, burlap, minimum 8 ounce weight, 6 inches to 10 inches in width.

207.2.13 SOIL STERILANT

Use granular calcium cyanamide, manufactured for use as a herbicide, or other approved sterilant.

207.3.00 CONSTRUCTION

207.3.01 GENERAL

Conform to Manufacturer's and supplier's recommendations and instructions and to accepted practices in the industry.

207.3.02 SOIL TEST

If directed by Engineer, have a soil test performed before the project schedule is submitted. The test may be performed by any Oregon State University County Extension Agent or by any other approved soils testing laboratory. The soils analysis shall provide a chemical analysis of the soil and recommendations for soil improvement for the crop to be grown. The recommendations shall be used to select the particular fertilizer and soil improvement chemicals to be used prior to planting.



## 207.3.04 TREES, SHRUBS AND GROUND COVER

### 207.3.04A DELIVERY, PREPARATION AND STORAGE

Dig plants designated as Balled and Burlapped in the Contract Documents with firm, natural balls of earth of diameter and depth sufficient to encompass the fibrous and feeding root system required for full recovery of plant. Firmly wrap balls with burlap and bind with twine, cord, or wire mesh. Where necessary to prevent breaking or cracking of ball during process of planting, or where the tree exceeds 4 inches in diameter, secure ball to a platform.

If plants are not in dormant state, spray with anti-desiccant to cover foliage as recommended by Manufacturer, prior to digging plants. During shipment, protect plants with tarpaulin or other approved covering to prevent excessive drying from sun and wind.

Cover balls of Balled and Burlapped plants which cannot be planted immediately upon delivery with moist mulch to protect from drying. Water plants as necessary to prevent drying until planted. Do pruning only at time of planting.

### 207.3.04B SOIL CONDITIONING

After the specified chemical analysis report for topsoil is received, prepare topsoil mixture for plant pits and beds by thoroughly mixing approved topsoil with soil conditioner materials, fertilizer, and lime. Thoroughly mix with rotary mixer or other approved method in following proportions:

<u>Topsoil Classifica- tion by Clay Content</u>	<u>Required Mixture</u>			<u>Parts By Volume</u>	
	<u>Top Soil</u>	<u>Sand</u>	<u>Peat</u>	<u>Ferti- lizer*</u>	<u>Lime*</u>
Clay 5-10 Percent	4	0	1	( $\frac{1}{2}$ ) LB/CY	(1) LB/CY
Clay 10-15 Percent	2	2	1	( $\frac{1}{2}$ ) LB/CY	(1) LB/CY
Clay 15-25 Percent	2	4	1 $\frac{1}{2}$	( $\frac{1}{2}$ ) LB/CY	(1) LB/CY

\*Adjust in accordance with Soil Test chemical analysis report.

Store and protect topsoil mixture and other materials at designated area of the site. Protect topsoil mixture from excessive leaching by covering with tarpaulin if stored for more than 6 weeks.

### 207.3.04C PLANTING PROCEDURES

Within 20 calendar days after the date specified for the commencement of work, submit time schedule for approval indicating dates for commencement and completion of the following operations:

1. Tagging of plants in the nurseries
2. Survey and staking of plant locations
3. Delivery of topsoil and other materials
4. Digging and preparation of plant pits and beds
5. Delivery of trees to the site



6. Delivery of other plants to the site
7. Planting of trees
8. Planting of other plants
9. Guying, staking and mulching
10. Completion of work for start of guarantee period

At least 20 days before start of the guarantee period, submit a schedule of proposed maintenance operations indicating the number of man-hours contemplated for each operation by-season during autumn, winter, spring and summer.

Within three weeks of the award of contract, begin to prepare topsoil for plant pits. Thereafter conduct planting operations under favorable weather conditions during next season or seasons which are locally normal for such work.

Locate new planting where shown, except make approved adjustments where obstructions below ground are encountered or where changes have been made in the construction. Place no planting, except ground cover, closer than 18 inches to pavements and structures. Dig plant pits and have soil mixture for planting ready before plants are delivered. Excavate circular pits with vertical sides a minimum of one foot greater than the diameter of the ball. For trees, shrubs, and vines excavate pits to depth sufficient to accommodate ball or roots when plant is set to finished grade. Place 3 inches of compacted soil mixture in the bottom of pit. Set plants upright and face as approved to give the best appearance or relationship to adjacent structures. Do not pull burlap from under balls. Remove wire and surplus binding from top and sides of balls. Spread roots in normal position. Cut all broken or frayed roots off cleanly. Place prepared soil mixture and compact carefully to avoid injury to roots and to fill voids. When hole is nearly filled, add water as necessary and allow to soak away. Fill hole to finished grade and form shallow saucer around plant by placing ridge of topsoil around edge of pit 2 feet greater than diameter of ball. After ground settles, fill with additional soil to level of finished grade.

Plant trees before surrounding smaller plants and covers are placed. Position trees as shown or, where spacing dimensions or locations are not clear, as approved.

Plant shrubs on centers as shown, with spacing adjusted if required to evenly fill bed using specified quantity of plants.

Plant hedges on centers as shown. Excavate trenches a minimum of 4 inches deeper and 12 inches wider than spread of roots or diameter of balls. Make adjustments to spacing if necessary to fill trench evenly with the quantity of plants shown.

Plant ground covers in beds having minimum 6-inches of prepared soil mixture. Treat ground cover beds after preparation for planting, but before any plants are installed within bed area, with soil sterilant to destroy weed seeds. Apply according to Manufacturer's directions, delaying planting for the recommended minimum period to allow dissipation of herbicide. Space plants as shown. Mulch and water immediately after planting.

Provide trees and planting beds with 2-inch layer of organic mulch within 2 days after planting and keep at this depth throughout maintenance period. Mulch to entirely cover area of saucer around each tree.



25

Use four guys equally spaced as shown for all trees greater than 4 inches in diameter.

Use three guys equally spaced as shown for all trees 4-inches in diameter or less.

Where shown, wrap trunks of trees spirally from ground line to height of second branches. Make all wrappings neat and snug and hold material in place by raffia cord at top and bottom.

#### 207.3.04D DRAINAGE OF PITS AND BEDS

Furnish subsoil drainage where shown. Dig trenches with vertical sides and smooth bottoms a minimum of 12-inches wide and 6-inches below tree balls, or 18-inches below finished grade at highest end of drain. Bed drain tile firmly, lay true to grade with minimum slope of .008 feet per foot and connect to approved outlet or discharge at grade. Make joint gaps maximum of 1/8-inch and cover with fiberglass separator to prevent ingress of soil. Cover entire tile line with 4-inch layer of crushed stone. Cover crushed stone with fiberglass separator and backfill with well compacted soil.

#### 207.3.04E PRUNING AND REPAIR

At completion of planting work, prune and repair injuries to all plants. Limit amount of pruning to minimum necessary to remove dead or injured twigs and branches and to compensate for the loss of roots as a result of planting operations. Do not change natural habit or shape of plant. Make cuts flush, leaving no stubs. On all cuts over 3/4-inch in diameter and bruises or scars on bark, trace the injured cambium back to living tissue and remove. Smooth and shape wounds so as not to retain water. Coat with approved tree wound paint.

#### 207.3.04G PLANT GUARANTEE

Guarantee all plants for a minimum of 1 year to be alive and in vigorous growing condition at the end of guarantee period. Guarantee period shall extend 1 year from date of Acceptance of Work as defined in Section 101.01. Remove unsatisfactory plants and replace with plants of the same kind, quality and size as originally provided as specified. Guarantee all plant replacements to be alive and in vigorous growing condition 1 year after replacement. Bear all costs of replacement except for replacements resulting from removal, loss or damage due to occupancy of project in any part, vandalism, or acts of neglect on part of others. Replace plants that die, during a season unfavorable for planting, during the first month of the next favorable planting season.

#### 207.3.04H MAINTENANCE

Begin maintenance immediately after each plant is installed and continue to maintain until the end of the guarantee period defined hereinbefore.

Perform the following operations: Watering as often as required to maintain capillary water within 2 inches of the soil surface around plants, weeding of plant beds, planting saucers and plantpockets to keep free of weeds using approved selective herbicide



26

according to the Manufacturer's directions for use, and/or weeding by hand methods, mulching monthly to replenish mulch and keep at required 2-inch minimum depth, tightening and repairing guys to keep trees erect and supported without damage to bark, resetting plants to proper grades or upright position, restoration of planting saucers, seasonal spraying to control disease or insect pests that may impair plant vigor.

Replace plants required by the plant guarantee on a regular monthly basis, except during the months of December, January and February.

#### 207.3.041 FINAL ACCEPTANCE

Submit notice in writing within 20 days of the date for final inspection at the end of the maintenance period and an inspection will be arranged within 15 days of this date. Final acceptance will be made provided the terms of the plant guarantee have been met and the project site is in the condition specified in MAINTENANCE, herein.

### 208 RESTORATION AND CLEANUP

#### 208.1.00 DESCRIPTION

This section covers the work necessary to restore and clean up the site, and remove all construction equipment, refuse and unused materials of any kind resulting from project activities.

Additional requirements pertaining to site restoration and cleanup are contained in the GENERAL REQUIREMENTS, Subsection 105.18.

Lawns, planting, mulching and topsoil shall conform to the requirements of Section 207 LANDSCAPING.

#### 208.2.00 MATERIALS

Provide all materials required to accomplish the work as specified.

#### 208.3.00 CONSTRUCTION

##### 208.3.01 SURFACE DRESSING

Slopes, sidewalk areas, planting areas, and roadway shall be smoothed and dressed to the required cross section and grade by means of a grading machine insofar as it is possible to do without damaging the work or existing improvements, trees and shrubs. Supplement machine dressing by hand work as directed.

Upon completion of the cleaning and dressing, the project shall appear uniform in all respects. Grade all areas true to line and grade as shown and as approved. Where the existing planting is below sidewalk and curb, fill and dress the area to the walk regardless of limits shown. Wherever fill material is required in the planting area, make finish surface high enough to allow for final settlement.

##### 208.3.02 REMOVAL OF MATERIALS

Remove and dispose of all excavated or construction materials, equipment, and trash of all kinds resulting from the work. Where brush and trees beyond the limits of the project have been disturbed, remove and dispose of or restore same as directed, at no expense to the Owner.



21

208.3.03 CLEANING DRAINS

Clean all drainage facilities such as inlets, catch basins, culverts and open ditches of all excess material or debris which is the result of the work, as approved.

208.3.04 CLEANING PAVED SURFACES AND APPURTENANCES

Clean all pavement surfaces, whether new or existing within the limits of the project. Clean existing improvements such as curbs, gutters, walls, sidewalks, castings for manholes, monuments, water gates, lamp poles, vaults, signs, and other similar installations as approved.

Flush the street with a pressure type flusher as approved. Hand broom or flush all sidewalks as directed.

208.3.05 RESTORING PLANTED AREAS

Hand-rake and drag all former grassed and/or planted areas leaving disturbed areas free from rocks, gravel, clay, or any other foreign material and ready, in all respects, for seeding. The finished surface shall conform to the original surface, be free-draining and free from holes, rough spots, or other surface features detrimental to a seeded area.

208.3.06 RESTORING MOBILIZATION, BORROW AND DISPOSAL AREAS

Clean all properties which were disturbed during construction of the project. Dispose of all uprooted stumps, felled trees, brush, excess excavation, rock, discarded materials, rubbish and debris. Remove all plant, equipment, tools and supplies and put the property occupied in a neat, clean and orderly condition, in equal or better condition to that existing before move in.

208.3.07 REMOVAL OF SIGNS

Do not remove warning, regulatory, guide, or project signs prior to formal acceptance, except as directed.

208.3.08 RESTORING CURBS, SIDEWALKS AND DRIVEWAYS

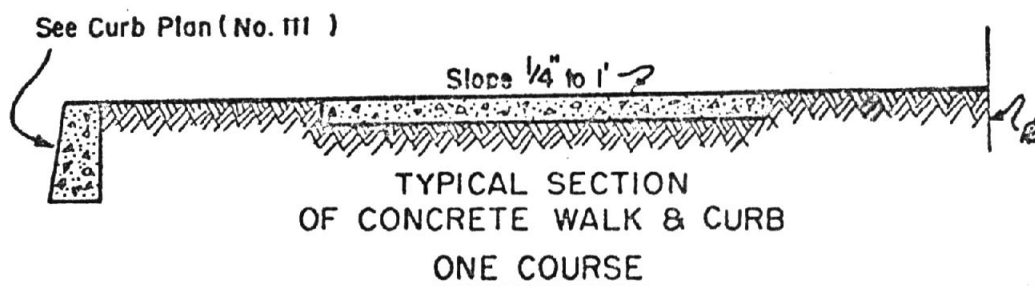
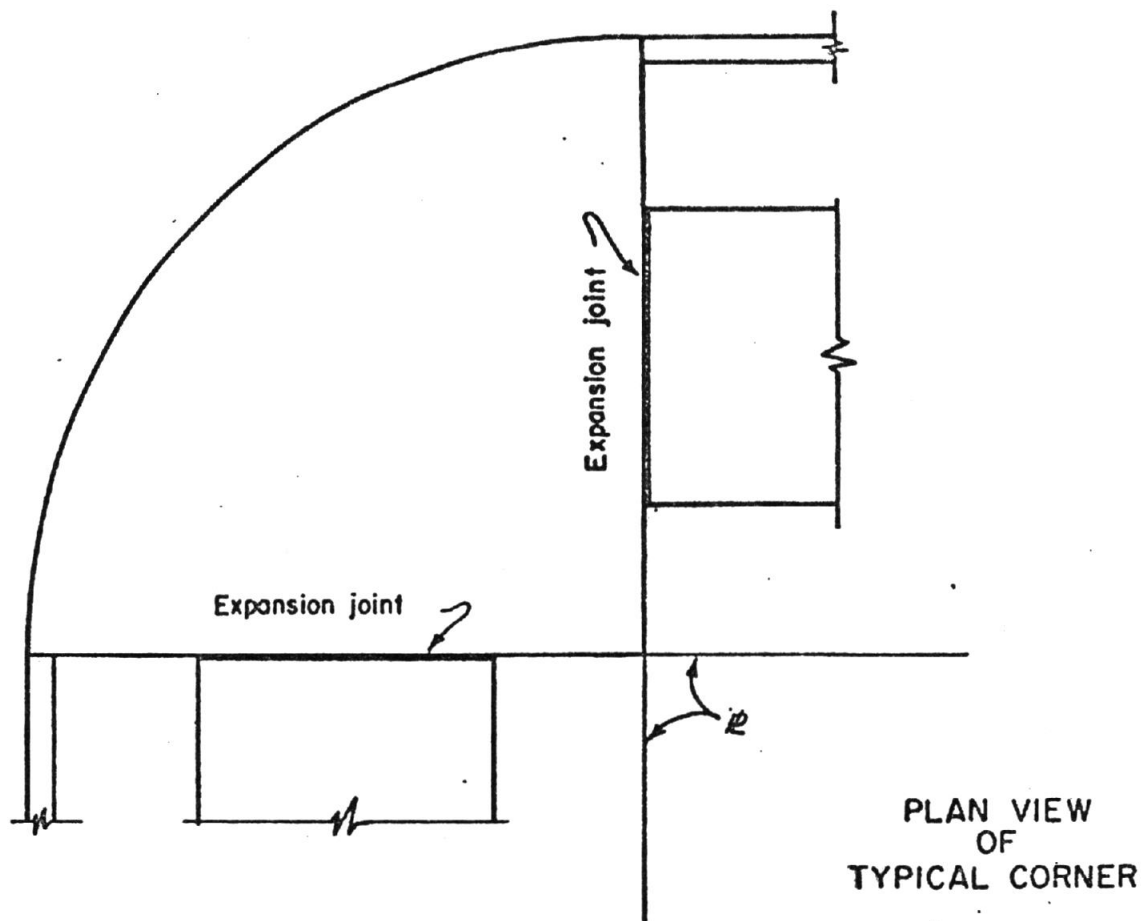
Repair or replace all curbs, sidewalks, driveways and other structures damaged during construction of the work.

208.4.00 MEASUREMENT AND PAYMENT

208.4.01 LUMP SUM BASIS

When listed in the Proposal as a separate pay item, payment for restoration and cleanup will be made on a lump sum basis.





Concrete for sidewalk and sidewalk corners shall be 3000 lb., 2" to 4" slump, 4" thick.

CITY OF PORTLAND, OREGON  
DEPARTMENT OF PUBLIC WORKS  
LLOYD E. ANDERSON COMMISSIONER

## CONCRETE SIDEWALK AND SIDEWALK CORNERS

APPROVED

*James W. Spence*  
CITY ENGINEER

DATE 3-1-70

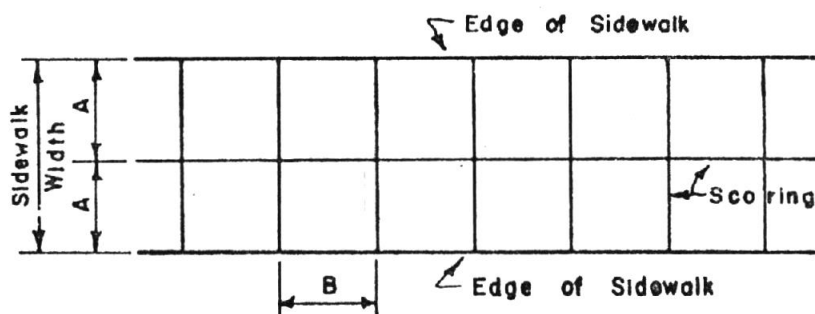
Reg Prof Engr No 3544

OCT., 1962 STANDARD PLAN NO 110

Rev. Nov. 1966

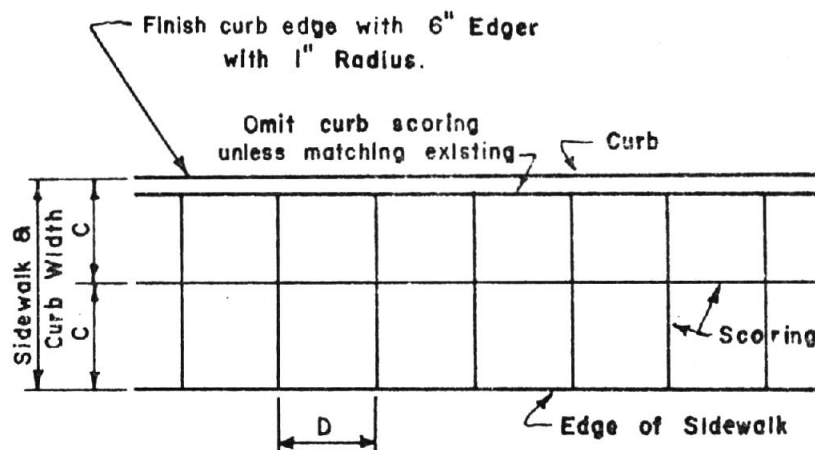


Sidewalk Width	A	B
5'	2.5'	3'
6'	3'	3'



## COMBINATION SIDEWALK & CURB

Sidewalk & Curb Width	C	D
5.5'	2.75'	3'
7'	3.5'	3.5'
8'	4'	4'
10'	5'	5'
12'	4'	4'
15'	5'	5'



Expansion Joints not to exceed 18'.

### Note:

12' & 15' Combination sidewalks & curbs require 3 tiers instead of 2.

Any variance in the marking pattern must be approved by the City Engineer 48 hours prior to commencement of work.

CITY OF PORTLAND, OREGON  
DEPARTMENT OF PUBLIC WORKS  
LLOYD ANDERSON COMMISSIONER

## SCORING PLANS FOR CONCRETE SIDEWALKS

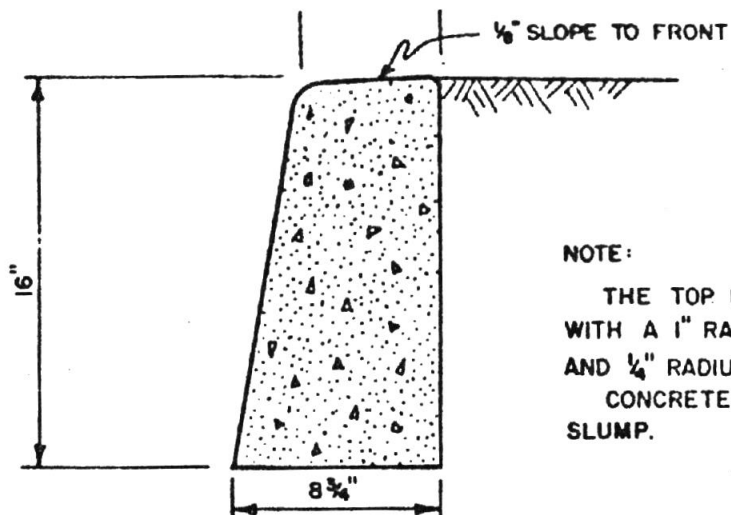
APPROVED

*Lloyd Anderson* DATE May, 1973  
CITY ENGINEER

Reg Prof Engr No 3544

STANDARD PLAN NO. 110A

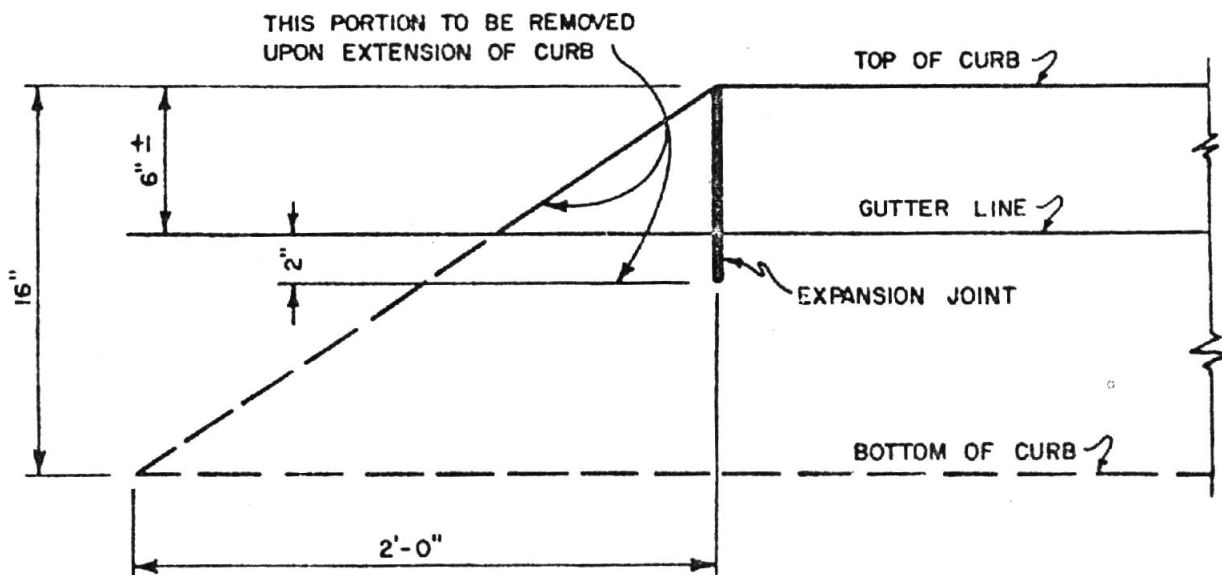




NOTE:

THE TOP EDGE TO BE ROUNDED OFF WITH A 1" RADIUS ON THE STREET SIDE AND 1/4" RADIUS ON THE PROPERTY SIDE. CONCRETE SHALL BE 3000#, 2" TO 4" SLUMP.

CONCRETE CURB SECTION



CURB ENDING

TO BE PAID FOR AS 1 ft. OF STANDARD CURB

CITY OF PORTLAND, OREGON  
DEPARTMENT OF PUBLIC WORKS  
LLOYD E. ANDERSON COMMISSIONER

STANDARD  
CONCRETE CURB

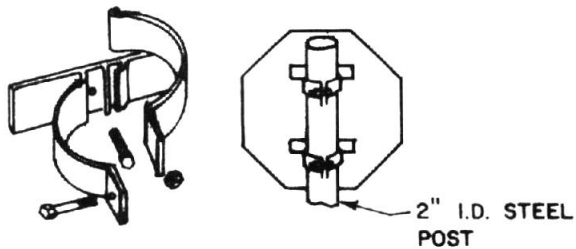
APPROVED

*Lloyd E. Anderson* DATE 3-1-70  
CITY ENGINEER

Reg Prof Engr No 3544  
OCT., 1962 STANDARD PLAN NO III



TRAFFIC SAFETY SUPPLY CO.  
NO. THDW-500-2 OR APPROVED  
EQUAL

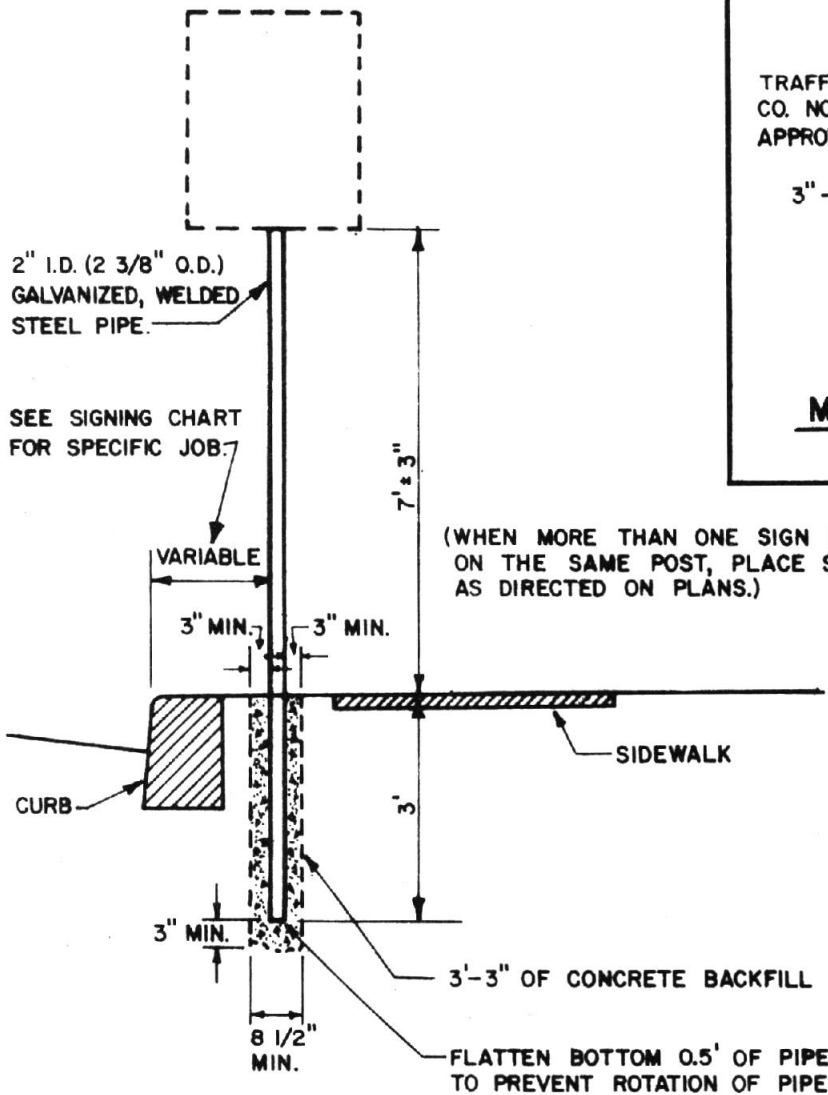


TYPICAL SIGN MOUNTING INSTALLATION

3/8" X 3" HOT DIP  
GALVANIZED LAG BOLT  
TRAFFIC SAFETY SUPPLY  
CO. NO. THDW-650  
OR APPROVED EQUAL

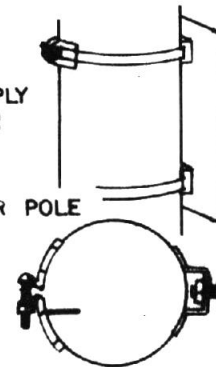


WOOD POLE MOUNTING INSTALLATION



TRAFFIC SAFETY SUPPLY  
CO. NO. THDW-100 OR  
APPROVED EQUAL

3" - 8 1/2" DIAMETER POLE



METAL POLE MOUNTING INSTALLATION

CITY OF PORTLAND, OREGON  
DEPARTMENT OF PUBLIC WORKS  
MIKE LINDBERG COMMISSIONER

## STANDARD TRAFFIC CONTROL SIGN INSTALLATION

APPROVED

*John M. Long*  
CITY ENGINEER  
Reg Prof. Engr. No. 8416

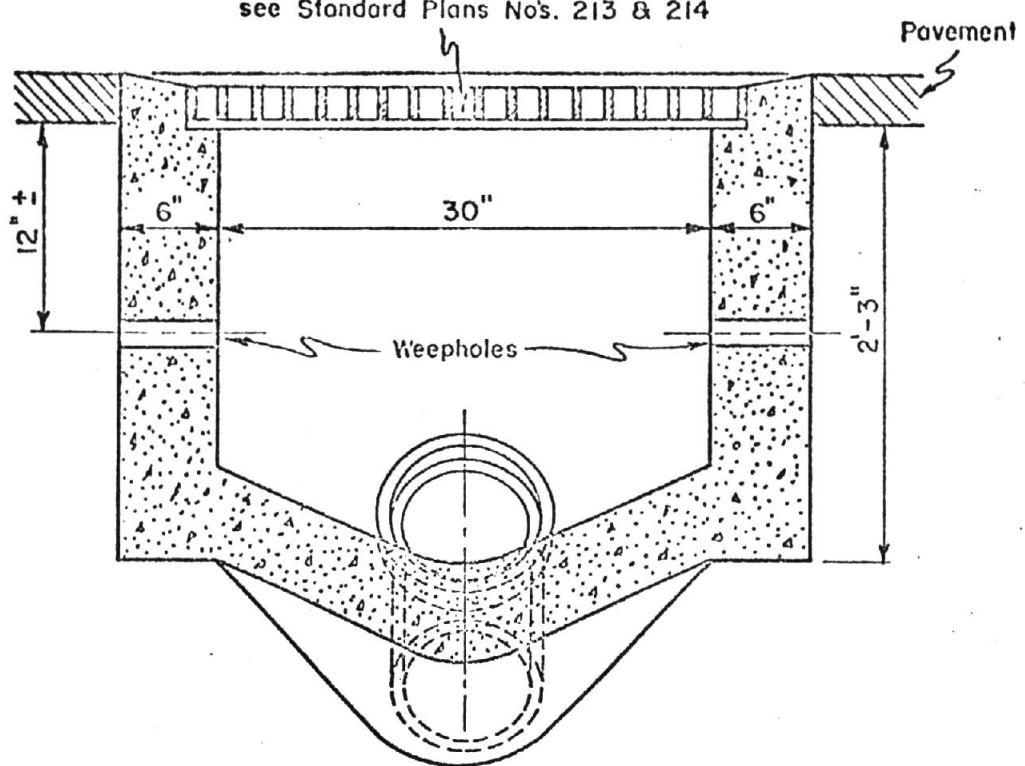
DATE 7-17-80

STANDARD PLAN NO. 119-C

SEE STANDARD PLAN NO. 119  
FOR INSTALLATIONS IN VAULTED  
& COMBINATION SIDEWALKS.



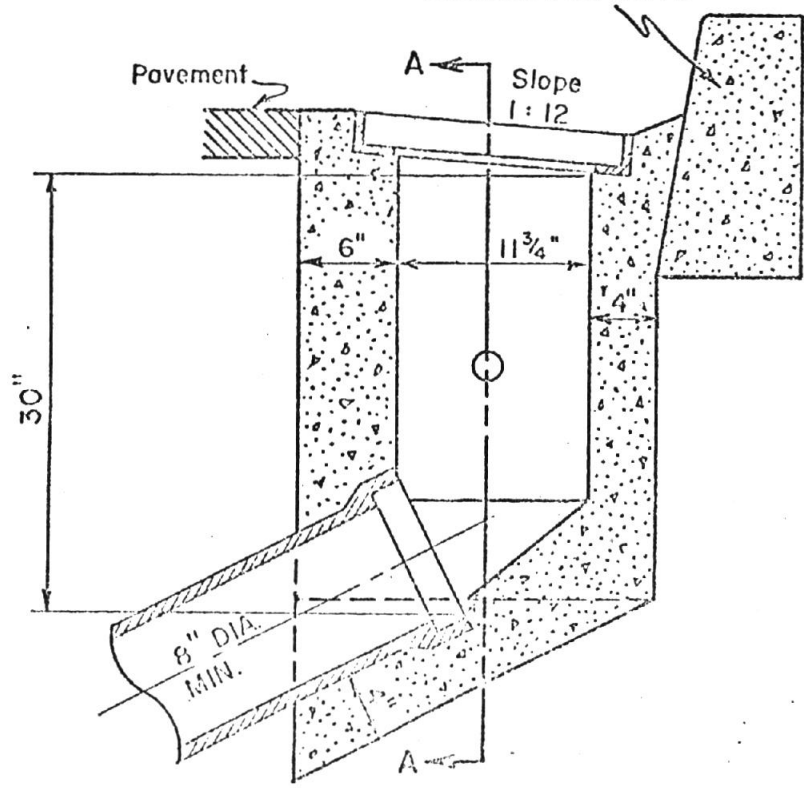
For inlet frames and gratings  
see Standard Plans No's. 213 & 214



SECTION A-A

Concrete to be 3000 lb., 3" to 5"  
slump, approx. 0.4 cu. yd.

For concrete curbs see  
Standard Plan No. III

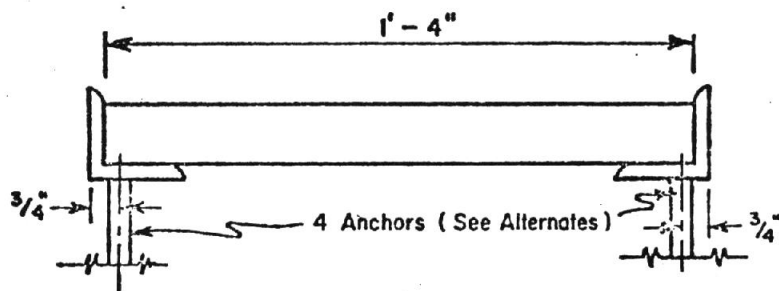
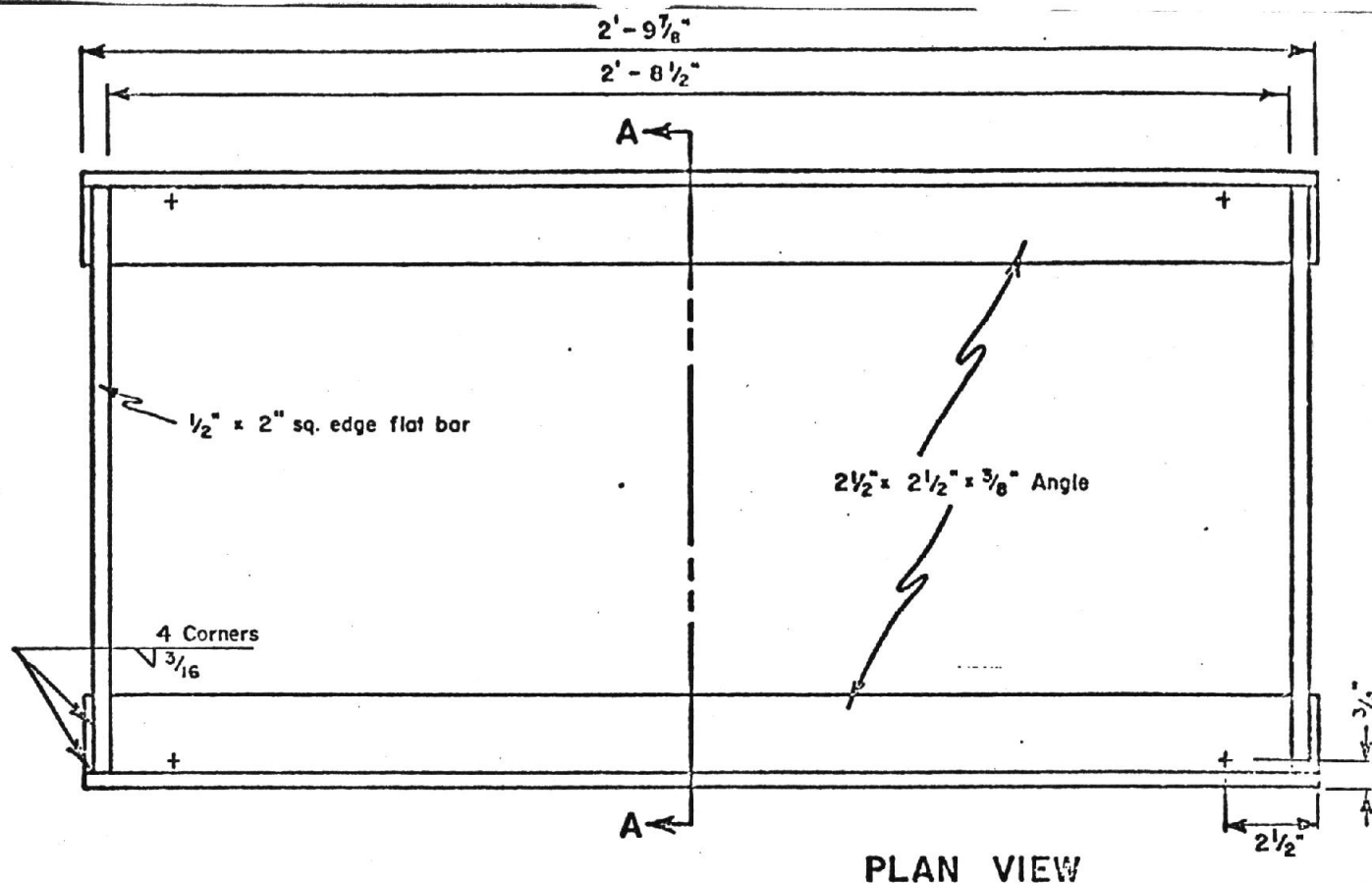


CITY OF PORTLAND, OREGON  
DEPARTMENT OF PUBLIC WORKS  
CONNIE MCCREADY COMMISSIONER

STANDARD CONCRETE INLET

APPROVED  
*Conley* *Mar 11/79* DATE *8/22/79*  
CITY ENGINEER  
REG PROF ENGR NO 8344  
STANDARD PLAN NO 211  
REVISED AUGUST 8, 1979

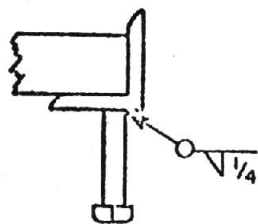




SECTION A-A

NOTE: MATERIAL TO BE NEW STRUCTURAL  
STEEL, ASTM A-7, A-36 OR A-373

### ALTERNATE ANCHORS



CITY OF PORTLAND, OREGON  
DEPARTMENT OF PUBLIC WORKS  
LLOYD E. ANDERSON COMMISSIONER

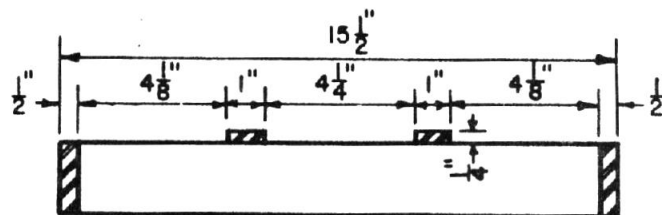
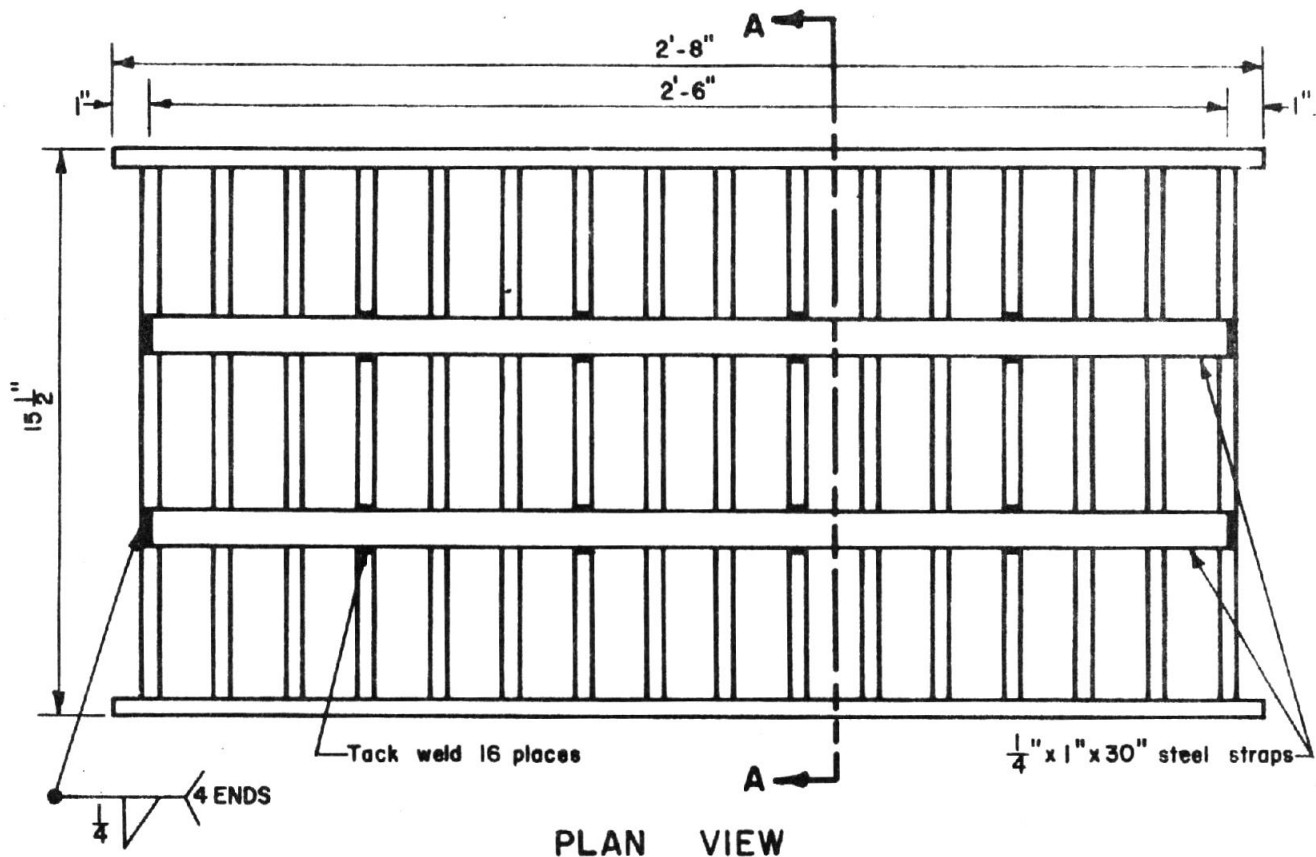
### STANDARD INLET FRAME

WEIGHT - APPROX. 55 LBS.

APPROVED

*James L. Anderson* DATE 6-26-73  
CITY ENGINEER  
Reg Prof Engr No 3544  
OCT., 1962 STANDARD PLAN NO 213  
Rev. 11-1-65





STRAP MATERIAL TO BE NEW  
STRUCTURAL STEEL, A-36 ASTM

CITY OF PORTLAND, OREGON  
DEPARTMENT OF PUBLIC WORKS  
CONNIE MCCREADY COMMISSIONER

# STANDARD INLET GRATING MODIFICATION FOR BICYCLE PROTECTION

APPROVED

*Conly Mally*  
CITY ENGINEER  
Reg Prof Engr No 8344

DATE 11/4/75

STANDARD PLAN NO 214-A



# SIGN INSTALLATION CERTIFICATION

JOB NAME: Traffic Diverter at Intersection of N.E. 36th and N.E. Brazee St.

JOB NO: 6093

PAGE 1 OF 1

TYPE OF SIGN M.U.T.C.D. NO.	SIZE	LOCATION STA. & FOOTAGE LOR R	DATE INSTALLED	TIME INSTALLED	CONTRACTOR REPRESENTATIVE'S SIGNATURE	INSPECTOR FOR CITY OF PORTLAND SIGNATURE
"Diverter ahead" City Code No. W242	30 X 30	N.E. 36th Avenue -4+80 & 20 Ft. R.		A.M. P.M.		
"STOP" R1-1	24 X 24	0+00 & 21 Ft. R.		A.M. P.M.		
"No Parking Right Arrow" R7-1	12 X 18	0+80 & 20 Ft. R.		A.M. P.M.		
"No Parking Left Arrow" R7-1	12 X 18	0+80 & 20 Ft. L.		A.M. P.M.		
"No Parking Right Arrow" R7-1	12 X 18	0 + 31 & 21 Ft. L.		A.M. P.M.		
"Dead End" W14-1	30 X 30	2 + 50 & 20 Ft. L.		A.M. P.M.		
		<u>N.E. Brazee St.</u>		A.M. P.M.		
"No Parking Left Arrow" R7-1	12 X 18	0+63 & 20 Ft. L.		A.M. P.M.		
Painting Curb Yellow		All new curbs on both sides of the diverter.		A.M. P.M.		
				A.M. P.M.		





CONNIE McCREADY  
COMMISSIONER

FRANCIS J. SARGANT  
CHIEF FIRE MARSHAL  
FIRE PREVENTION  
DIVISION

ROBERT W. BUSCHO  
FIRE MARSHAL

55 S.W. ASH STREET  
PORTLAND, OR 97204  
503/248-4363

August 23, 1979

TO: M. J. Martini, Senior Traffic Engineer  
Bureau of Traffic Engineering

THRU: Don Bergstrom  
City Traffic Engineer

FROM: Francis J. Sargent, Chief Fire Marshal  
Bureau of Fire

SUBJECT: Proposed Blockade of NE 36th at Brazee

We are opposed to the proposal to blockade NE 36th at NE Brazee. NE 36th is a primary access for emergency vehicles, specifically fire apparatus, responding to any emergency at Hollyrood Primary School. Concerning a traffic divertor being installed at NE Thompson and 36th, depending on its configuration, we would have no objections as long as our fire apparatus could negotiate the turns.

by Gerald E. Edwards  
Gerald E. Edwards  
Staff Lieutenant

FJS;GEE:mc

cc: Pauline McLaughlin  
2516 NE 41 Street  
Portland, OR 97212

RECEIVED  
AUG 24 1979  
BUREAU OF  
TRAFFIC ENGINEERING



THE CITY OF  
**PORTLAND**



**OREGON**

BUREAU OF  
POLICE

CHARLES JORDAN  
COMMISSIONER

B. R. BAKER  
CHIEF OF POLICE

222 S.W. PINE  
PORTLAND, OR. 97204

*Handwritten signature*  
October 27, 1980

MEMORANDUM

TO: M. J. Martini  
Bureau of Traffic Engineering

FROM: Captain Richard D. Walker *RDW*  
Traffic Division

SUBJECT: Traffic Diverter - N.E. 36th and Brazee

As indicated in the attached report from Lt. Coffey, the diverter has not presented a problem since its installation, and we have no problem with a permanent fixture.

RDW/rh

**RECEIVED**  
OCT 28 1980  
BUREAU OF  
TRAFFIC ENGINEERING

*N.E. 36th - Brazee*



**DATE :** September 16, 1980

**TO :** Captain Walker

**FROM:** Lt. Coffey *CC*

**SUBJ :** Traffic Diverter  
N.E. 36th Brazee



Bureau of Police

PORTLAND, OREGON

**INTER-OFFICE MEMORANDUM**

Please be advised that a temporary diverter has been at the above location for approximately six months and has posed no problem.

The purpose of the diverter is to prevent speeding traffic on N.E. 36th past Hollywood Grade School.

I staffed the proposal with the area Sgt. and District traffic unit, both of whom agreed the proposed permanent diverter was a sound idea and would not present a police problem.

CC/rh



*Shelly*

October 1, 1980

INTEROFFICE MEMORANDUM

TO: Dennis Toles  
Office of City Engineers

FROM: M. J. Martini  
Bureau of Traffic Engineering

SUBJECT: N.E. 36th and Brazee Diverter

We are enclosing copies of the reports from the agencies involved in the N.E. 36th and Brazee diverter. As you can see, the Police Bureau has no objections and the Park Bureau will need to be contacted again after the design is further along.

The Fire Bureau mentions some points with which we disagree. They want a 12' passage through the diverter which is more than they have on many city streets. We do not consider this as a fire lane, just an emergency passageway and therefore feel that 10' is sufficient for this purpose. They also don't want any obstructions in this area but without some form of flexible or breakaway post, the public can't be kept from crossing the diverter and its purpose is defeated. In addition, they also mention moving a fire hydrant. This cost should be included in your construction estimate.

MJM:TV:mc



October 1, 1980

INTEROFFICE MEMORANDUM

TO: Dennis Tolson  
Office of City Engineers

FROM: M. J. Marcini  
Bureau of Traffic Engineering

SUBJECT: W.E. 3rd and Bridge Diversion

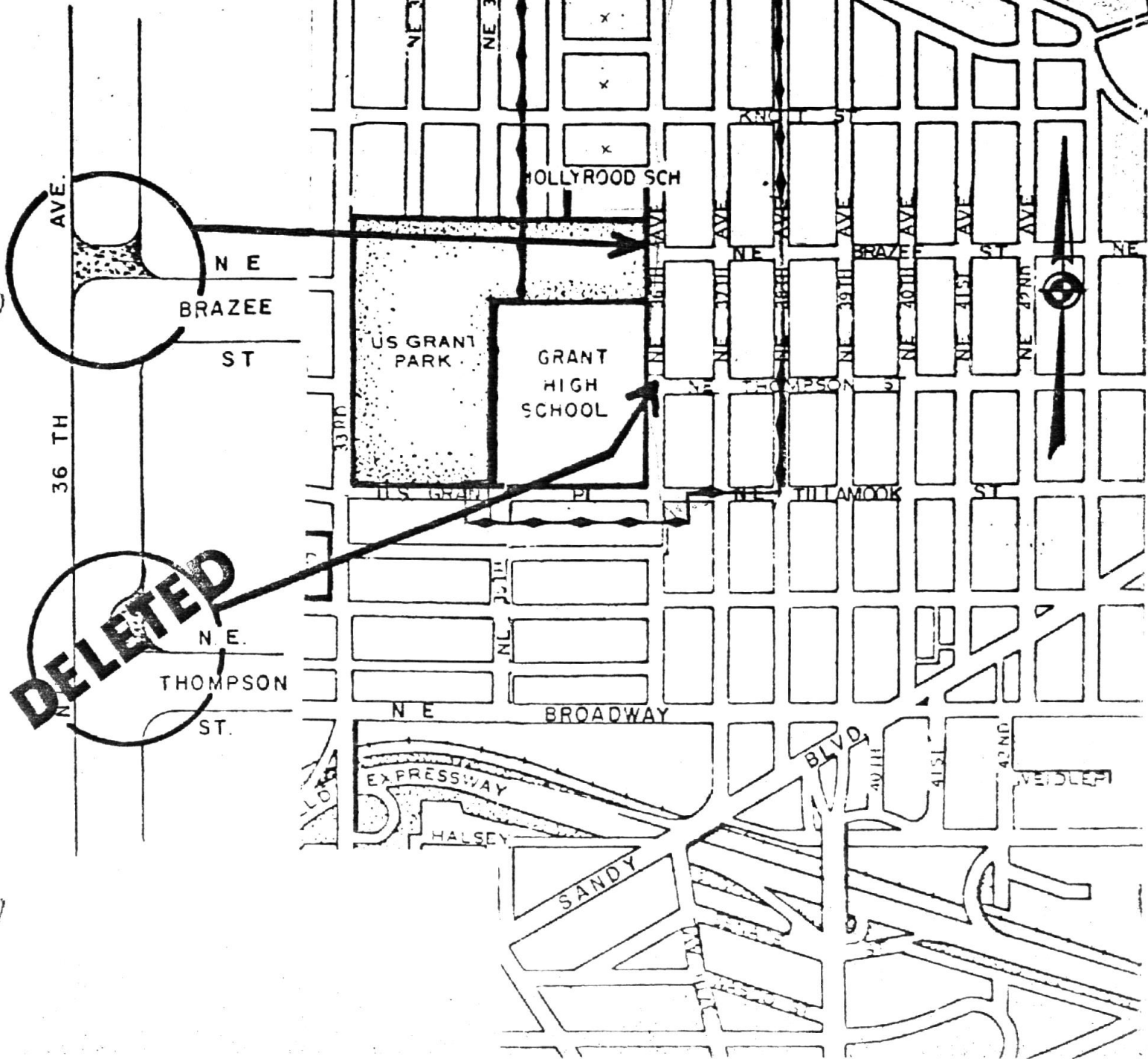
We are enclosing copies of the reports from the agencies involved in the W.E. 3rd and Bridge diversion. As you can see, the Police Bureau has no objections and the work areas will need to be contacted again after the design is further along.

The Fire Bureau mentions some points with which we disagree. They want a 12' passage through the divider which is more than they have on many city streets. We do not consider this as a fire lane, just an emergency passageway and therefore feel that 10' is sufficient for this purpose. They also don't want any obstructions in this area but without some form of flexible or breakaway post, the public can't be kept from crossing the divider and its purpose is defeated. In addition, they also mention moving a fire hydrant. This cost should be included in your construction estimate.

MJM:TV:mc



PROPOSED  
TRAFFIC DIVERTERS  
FOR  
GRANT PARK  
NEIGHBORHOOD  
5-8-79





THE CITY OF  
**PORTLAND**



**OREGON**

MIKE LINDBERG  
COMMISSIONER  
BUREAU OF FIRE

December 18, 1980

TO: N. Dinh,  
Street and Structural Engineering

FROM: Kenneth L. Owens,  
Chief Fire Marshal

SUBJECT: Curb Configuration - Fire Lane  
N.E. Brazee Diverter

KENNETH L. OWENS  
CHIEF FIRE MARSHAL  
FIRE PREVENTION  
DIVISION

55 S.W. ASH STREET  
PORTLAND, OR 97204  
(503) 248-4363

We approve the Number II design indicated on the drawing sheet dated December 12, 1980 which you submitted to us for our consideration.

We feel that the large diameter of fire apparatus can safely transverse such curbs. However, we would prefer you to 'stretch' the angle of attack a bit further if possible. Tom Neely seemed to think that is possible.

Sincerely,

KENNETH L. OWENS  
Chief Fire Marshal

By *Gerald E. Edwards*  
Gerald E. Edwards  
Staff Captain

GEE/mlw



THE CITY OF  
**PORTLAND**



**OREGON**

BUREAU OF  
POLICE

CHARLES JORDAN  
COMMISSIONER

B. R. BAKER  
CHIEF OF POLICE

222 S.W. PINE  
PORTLAND, OR. 97204

MEMORANDUM

July 8, 1980

TO: M.J. Martini, Traffic Engineering

FROM: B.R. Baker, Chief of Police

SUBJECT: Blockage of N.E. 36th Avenue at Brazee

After reviewing the proposal to block N.E. 36th Avenue at Brazee with a curbed planted divider, it is our opinion that it would present no problem. East Precinct and Traffic Division Commanders stated that the temporary closure last year presented no trouble to their officers.

We request that the dead end southbound and the detour northbound be well posted.

B. R. BAKER  
Chief of Police

BRB/JWJ/pm

**RECEIVED**

**JUL 14 1980**

**BUREAU OF  
TRAFFIC ENGINEERING**





DEPARTMENT OF  
PUBLIC AFFAIRS  
MILDRED A. SCHWAB  
COMMISSIONER

BUREAU OF PARKS AND  
PUBLIC RECREATION

SUPERINTENDENT

409 S.W. NINTH AVENUE  
PORTLAND, OREGON 97205

MEMORANDUM

December 24, 1980

TO: Tom Neeley  
Traffic Engineering

FROM: William V. Owens  
Acting Superintendent

SUBJECT: Traffic Diverter

*WVO*

The Bureau of Parks and Recreation would be happy to accept maintenance responsibility for the traffic diverter to be constructed at N.E. 36th and Brazee, once the plant materials have been established on the site.

We suggest that in order to maintain the diverter in the most cost-efficient manner, Ivy or a "low-fast" variety of Cotoneaster be used as plant materials rather than grass.

If our bureau may be of any further assistance in this matter, please don't hesitate to contact our Operations Division Manager, Bob Gustafson (4397).

WVO:MF:gr

cc: Bob Gustafson

**RECEIVED**  
**DEC 26 1980**  
**BUREAU OF**  
**TRAFFIC ENGINEERING**



4283  
RECEIVED 2204 N.E. 37th  
DEC 3 8 39 AM 1980 Portland 97212  
GEORGE H. H. AUDITOR  
CITY OF PORTLAND, ORE.  
Dec. 1, 1980

Dear Council Members,

I would like  
to express my desire  
to have a traffic  
diverter installed at  
N.E. 36th & Brazee.  
This street intersection  
is heavily travelled  
by teen-agers. It  
is also near a primary  
school where many  
small children walk.  
Anything to slow  
traffic would be  
a fine idea.

Favor Sincerely,  
J. Paulson

RECEIVED  
DEC 4 1980

BUREAU OF  
TRAFFIC ENGINEERING



City Council  
Room 202 City Hall  
Portland, Oregon 97204

RECEIVED  
DEC 3 8 39 AM 1980  
GEORGE H. ANDERSON, AUDITOR  
CITY OF PORTLAND, ORE.  
BY CA

4283  
December 2, 1980

Dear Madame and Sirs:

I am opposed to the traffic barrier and diverter planned for the corner of N.E. 36th and Brazee. My objection is based on personal knowledge and observation of traffic patterns at this intersection over many years. I have lived near this corner since 1956--before Hollyrood School was built--and have a particularly good view of the intersection and park frontage.

This is an intersection that experiences heavy vehicle and pedestrian use not confined to the school day. There are meetings, programs, and year-round use of the high school for evening classes and summer school. Until recently, the street was relatively quiet on weekends, but this is no longer true. The open area of the park near 36th now receives almost constant use for organized sports and practice games, bringing many vehicles and hundreds of children and adults. Park access is at and near this intersection. With a permanent barrier and diverter, the mix of vehicles and pedestrians at this corner promises to be lethal.

Personal observations during the trial installation period were two vehicle collisions at the corner; cars driven around the barrier, either through the park on one side or onto the sidewalk and private property on the other; emergency vehicles driven around the barrier; cars driven into the park and parked under the trees while their occupants participated in sports activities; little if any reduction in traffic speed; and rather than reduced traffic volume, there was a concentration of traffic and multiplication of hazards at this corner. The plan appears to give no consideration to the problems it will cause south of the barrier. Hollyrood children coming from the south and east will have an even more perilous intersection to cross, as will all pedestrians at this corner, because a driver intent on making a turn in a narrow lane has little attention left for pedestrians.

It is easy to understand the desire of Hollyrood parents to guarantee safety for their children. And it is not hard to understand the support for the plan by some residents on the street who expect to gain on-street parking and reduced traffic in front of their houses. But if this plan is put into effect, I am afraid that the proponents will find that they have won a bloody victory.

Sincerely yours,

*Helen S. Charneski*

Helen S. Charneski

2434 N.E. 36th Avenue  
Portland, Oregon 97212

*Personnel*  
RECEIVED  
DEC 4 1980  
BUREAU OF  
TRAFFIC ENGINEERING



Meely  
D

William Nickerson

2615 N.E. 36th Ave

Permanent Barrier @ 36th - Brayer  
281-5088

Pam

4129

Mayor's Office

248-0700

Linda Wilson 5324  
Park Bureau meeting.

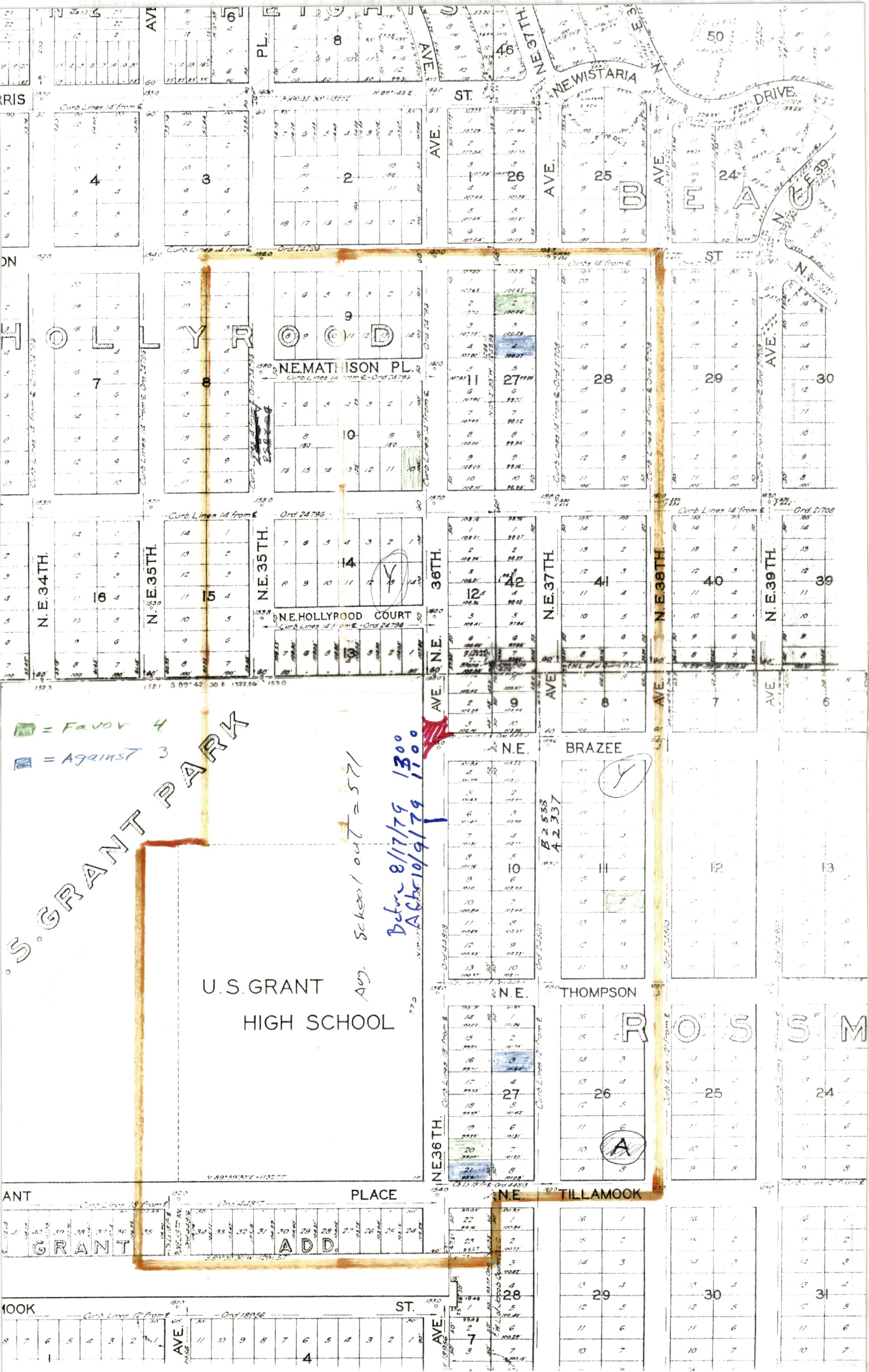


N. E. 36th & Brazee

Chronology of Events

May	1979	Grant Park Neighborhood Association decided to petition for temporary diverters at 36th and Brazee and 36th and Thompson for 60-day trial.												
June	1979	BTE requested opinions from affected bureaus.												
July	1979	Received answers from affected bureaus.												
Aug.	1979	Installed temporary diverters.												
Oct.	1979	Removed temporary diverters.												
Nov.	1979	Grant Park N. A. held meeting and decided to circulate new petition requesting only one diverter at N. E. 36th and Brazee.												
April	1980	Received new petition with 75% approval from total of <u>226</u> polled. <table border="0" data-bbox="568 1159 1169 1308"><tr><td>170</td><td>Yes</td><td>= 75%</td></tr><tr><td>27</td><td>No</td><td>= 12%</td></tr><tr><td>29</td><td>No Answer</td><td>= 13%</td></tr><tr><td><u>226</u></td><td></td><td></td></tr></table>	170	Yes	= 75%	27	No	= 12%	29	No Answer	= 13%	<u>226</u>		
170	Yes	= 75%												
27	No	= 12%												
29	No Answer	= 13%												
<u>226</u>														
Aug.	1980	Negotiated design features with Fire Bureau.												
Sept.	1980	Requested opinions from affected bureaus and began preliminary planning with City Engineers staff.												
Oct.	1980	Received answers from affected bureaus. City Engineer completed final plans and cost estimates.												
Nov.	1980	Submitted to Council.												





Green square = FAVOR 4  
Blue square = AGAINST 3

S. GRANT PARK

U.S. GRANT  
HIGH SCHOOL

Aug. School out = 5/21  
Before 8/17/79 1300  
After 10/9/79 1100

ADD

Y

A

GRANT

PLACE

TILLAMOOK

MOOK

ST.

AVE.

29

30

31



U.S. GRANT

HOLLYWOOD

36th

VEH. / M.P.H. VEH. / M.P.H.  
COUNT / SPD; COUNT / SPD

← 542 771 → 10/6/75 SCH. IN  
← 406/27+; 165/24+ → 8/17/79 SCH. OUT  
← 729/21-; 403/20+ → 10/9/79 SCH. IN  
← 518/23+; 686/24+ → 11/19-20/79 SCH. IN

TILLYMOOR

37th

THOMPSON

← 251 288 → 10/6/75  
VEH. / M.P.H. VEH. / M.P.H.  
COUNT / SPD; COUNT / SPD. 37TH

← 1912 976 → 9/17/70 SCH. IN  
← 1437 859 → 6/11/73 SCH. OUT  
← 1038/32+; 665/32- → 8/17/79 SCH. OUT  
← 1430/31- 907/31- → 10/9/79 SCH. IN  
← 1301/32; 649/32 → 11/27/79 SCH. IN

BRAZEE

← 295 399 → 10/6/75

KNOTT

W/O DIVERS (BEFORE)  
W/O DIVERS (TEMP)  
W/O DIVERS (AFTER)

→ 2



# TRAFFIC VELOCITY TAB

ON - N.E. 37th Ave N/Bd.

N - OF - N.E. Thompson St.

POSTED SPEED - 25 MPH

DATE: 11/27/79

DAY: Tuesday

WEATHER: clear - 40°

SPEED TIME	1- 15	16- 20	21- 25	26- 30	31- 35	36+ =	1 HR TOTAL								SPEED	VOLUME	PERCENT
7-8 <sup>AM</sup>	1	0	2	1	0	0	= 4								1-15	11	1.69
8-9	0	2	5	7	7	2	= 23								16-20	<sup>39</sup> 28	<sup>6.00</sup> 4.31
9-10	0	3	23	21	3	1	= 51								21-25	<sup>222</sup> 188	<sup>34.97</sup> 28.97
10-11	0	1	7	6	5	1	= 20								26-30	<sup>509</sup> 282	<sup>76.42</sup> 43.45
11-12	0	0	7	11	3	0	= 21								31-35	<sup>619</sup> 110	<sup>95.27</sup> 16.95
12-1 <sup>PM</sup>	2	0	2	12	2	1	= 19								36+	<sup>649</sup> 30	<sup>100.00</sup> 4.63
1-2	1	0	12	18	6	4	= 41										
2-3	0	2	10	20	5	3	= 40										
3-4	1	4	17	16	9	0	= 47										
4-5	2	3	19	26	10	1	= 61										
5-6	2	1	18	27	14	3	= 65										
6-7	0	3	18	39	16	5	= 81								TOTAL	649	100.0%
7-8	0	1	15	24	9	3	= 52								REMARKS:  85% SPEED BETWEEN 31 MPH. AND 35 MPH. app 32 M.P.H. -  65.03 % of vehicles exceed speed limit		
8-9	0	1	6	16	4	1	= 28										
9-10	0	2	8	11	9	0	= 30										
10-11	1	3	7	12	4	1	= 28										
11-12	0	0	4	6	1	0	= 11										
12-1 <sup>AM</sup>	0	0	4	4	2	3	= 13										
1-2	1	0	2	1	1	0	= 5										
2-3	0	0	0	3	0	0	= 3										
3-4	0	1	1	1	0	1	= 4										
4-5	0	0	1	0	0	0	= 1										
5-6	0	1	0	0	0	0	= 1										
6-7	0	0	0	0	0	0	= 0										
TOTAL	11	28	188	282	110	30	= 649										







CITY OF PORTLAND, OREGON  
BUREAU OF TRAFFIC ENGINEERING

COLLISION DIAGRAM

LOCATION NE BRAZEE ST @ 37<sup>th</sup> AVE. COMPILED BY R. EVANS  
PERIOD COVERED 1976 TO 5 mos 1979 DATE 11/13/79

NE 37<sup>th</sup> AVE.

76

IN CRWK.

NE BRAZEE ST.



76

77

COLLISION TYPE	1976				1977				1978				5 mos 1979				Fatal	Non-Fatal	Prop. Damage	Total
	Fatal	Non-Fatal	Prop. Damage	Total	Fatal	Non-Fatal	Prop. Damage	Total	Fatal	Non-Fatal	Prop. Damage	Total	Fatal	Non-Fatal	Prop. Damage	Total				
Angle		1		1			1	1												
Head-on																				
Rear-end																				
Slideswipe																				
Turning Movement																				
Parking																				
Non-collision																				
Fixed Object																				
Pedestrian		1		1																
Backing																				
Misc.																				
TOTAL		2		2		1		1				0				0				



CITY OF PORTLAND, OREGON  
BUREAU OF TRAFFIC ENGINEERING

# COLLISION DIAGRAM

LOCATION NE Thompson St. @ 37th Ave. COMPILED BY EVANS  
PERIOD COVERED 1976 TO Smos 1977 DATE 11/13/79

## LEGEND

- Person Killed
- ←--- Pedestrian Killed
- Person Injured
- ◁--- Pedestrian Injured
- ← Property Damage Only
- ←+ Collision-Rear-end
- ↔ Collision-Head-on
- ↔ Collision-Sideswipe
- ◁ Path of Pedestrian

*N.E. Thompson St.*

*NE 37th Ave.*



- ← Path of Vehicle
- ←--- Path of Animal
- ←+ Vehicle Moving
- Vehicle Stopped
- ←+ Vehicle Backing
- Properly Parked
- ▨ Improperly Parked
- ↺ Vehicle Overturned
- ↺ Vehicle Skidded

COLLISION TYPE	1976				1977				1978				Smos 1979							
	Fatal	Non-Fatal	Prop. Damage	Total	Fatal	Non-Fatal	Prop. Damage	Total	Fatal	Non-Fatal	Prop. Damage	Total	Fatal	Non-Fatal	Prop. Damage	Total	Fatal	Non-Fatal	Prop. Damage	Total
Angle																				
Head-on																				
Rear-end																				
Sideswipe																				
Turning Movement																				
Parking																				
Non-collision																				
Fixed Object																				
Pedestrian																				
Backing																				
Misc.																				
TOTAL				0				0				0				0				



# TRAFFIC VELOCITY TAB

ON - N.E. 36TH AVE, N/BND.

NORTH - OF - N.E. BRAZEE ST.

POSTED SPEED - 20 MPH - SCHOOL ZONE

DATE: Nov. 20, 1979

DAY: TUESDAY

WEATHER: OVERCAST

SPEED TIME	1- 15	16- 20	21- 25	26- 30	31- 35	36+										SPEED	VOLUME	PERCENT
7-8 <sup>AM</sup>	18	15	6	2	0	3	=	44								1-15	218	31.78
8-9	39	35	4	3	3	5	=	89								16-20	<sup>462</sup> 244	<sup>67.35</sup> 35.57
9-10	8	19	4	1	0	2	=	34								21-25	<sup>602</sup> 140	<sup>87.76</sup> 20.41
10-11	6	10	8	3	0	0	=	27								26-30	<sup>656</sup> 54	<sup>95.63</sup> 7.87
11-12	14	22	12	2	0	1	=	51								31-35	<sup>667</sup> 11	<sup>97.23</sup> 1.60
12-1 <sup>PM</sup>	11	15	13	4	0	2	=	45								36+	<sup>686</sup> 19	<sup>100.00</sup> 2.77
1-2	13	13	9	4	0	1	=	40									686	100.00%
2-3	33	21	13	2	1	2	=	72										
3-4	3	10	17	8	2	0	=	40										
4-5	3	5	8	8	0	0	=	24										
5-6	8	10	10	6	1	0	=	35										
6-7	40	31	10	4	2	0	=	87										
7-8	9	9	4	1	0	2	=	25								REMARKS:  85% speed between 21 MPH AND 25 MPH. > 24 M.P.H.  temp. After dividers, school in		
8-9	6	15	7	1	0	0	=	29										
9-10	1	1	2	1	1	0	=	6										
10-11	2	0	2	2	0	0	=	6										
11-12	1	3	3	0	1	1	=	9										
12-1 <sup>AM</sup>	1	1	0	1	0	0	=	3										
1-2	0	1	3	1	0	0	=	5										
2-3	0	0	0	0	0	0	=	0										
3-4	0	0	0	0	0	0	=	0										
4-5	0	0	0	0	0	0	=	0										
5-6	1	4	1	0	0	0	=	6										
6-7	1	4	4	0	0	0	=	9										
TOTAL	218	244	140	54	11	19	=	686										



# TRAFFIC VELOCITY TAB

ON - NE 36th Ave S/Bd. #585

N - OF - NE Thompson St.

POSTED SPEED - 20 MPH (SCHOOL ZONE)

DATE: 11/19/79

DAY: Monday

WEATHER: cloudy

SPEED TIME	1- 15	16- 20	21- 25	26- 30	31- 35	36+ =	1 HR TOTAL									SPEED	VOLUME	PERCENT
7-8 AM	22	18	11	3	2	2	= 58									1-15	180	34.75
8-9	37	38	19	1	0	0	= 95									16-20	189	36.49
9-10	7	10	5	2	0	0	= 24									21-25	105	20.21
10-11	14	15	8	3	0	1	= 41									26-30	31	5.98
11-12	16	7	4	2	0	0	= 29									31-35	8	1.54
12-1 PM	12	18	9	3	0	0	= 42									36+	5	0.97
1-2	6	5	4	0	0	1	= 16											
2-3	27	24	8	2	2	1	= 64											
3-4	14	8	7	1	0	0	= 30											
4-5	9	8	7	3	0	0	= 27											
5-6	5	2	6	3	0	0	= 16											
6-7	0	3	2	1	0	0	= 6									TOTAL	518	100.0%
7-8	1	5	0	0	0	0	= 6									REMARKS:  85% speed between 21 mph AND 25 mph.  > 23 M.P.H.  + temp - After divers School in		
8-9	3	8	1	1	1	0	= 14											
9-10	2	9	3	2	0	0	= 16											
10-11	0	5	6	3	1	0	= 15											
11-12	2	1	1	0	1	0	= 5											
12-1 AM	0	0	0	0	0	0	= 0											
1-2	0	0	0	0	0	0	= 0											
2-3	0	0	0	0	0	0	= 0											
3-4	0	0	0	0	0	0	= 0											
4-5	0	0	0	0	0	0	= 0											
5-6	1	2	3	0	0	0	= 6											
6-7	2	3	1	1	1	0	= 8											
TOTAL	180	189	105	31	8	5	= 518											



# TRAFFIC VELOCITY TAB

ON - N.E. 36th AVE. N/BD. (#505)

S - OF - N.E. BRAZEE

NONPOSTED SPEED - 25 mph. (School zone 20 mph)

DATE: 10/9/79

DAY: Tuesday

WEATHER: Clear 75°

SPEED TIME	1-15	16-20	21-25	26-30	31-35	36+	=	1 HR. TOTAL								SPEED	VOLUME	PERCENT
7-8 AM	19	3	1	0	0	0	=	23								1-15	289	71.7%
8-9	34	9	4	2	0	0	=	49								16-20	<sup>355</sup> 66	<sup>88.1</sup> 16.4
9-10	7	5	2	1	0	0	=	15								21-25	<sup>382</sup> 27	<sup>94.8</sup> 6.7
10-11	3	0	0	1	0	0	=	4								26-30	<sup>395</sup> 13	<sup>98.0</sup> 3.2
11-12	9	6	2	3	0	0	=	20								31-35	<sup>396</sup> 1	<sup>98.2</sup> 0.2
12-1 PM	20	11	7	3	0	0	=	41								36+	<sup>403</sup> 7	<sup>100.0</sup> 1.8
1-2	11	6	0	1	0	1	=	19										
2-3	30	6	1	0	0	1	=	38										
3-4	13	6	3	0	0	0	=	22										
4-5	12	1	0	0	0	0	=	13										
5-6	21	2	0	0	0	1	=	24										
6-7	35	3	2	2	0	0	=	42								TOTAL	403	100.0%
7-8	34	0	0	0	0	2	=	36								REMARKS: Count taken After Grant High School traffic Diverters were installed 8570 file 20+ M.P.H. w/diverters, (Hemp.), school in		
8-9	16	5	1	0	0	0	=	22										
9-10	17	3	3	0	0	0	=	23										
10-11	2	0	1	0	0	1	=	4										
11-12	0	0	0	0	0	0	=	0										
12-1 AM	0	0	0	0	0	0	=	0										
1-2	0	0	0	0	0	0	=	0										
2-3	0	0	0	0	0	0	=	0										
3-4	0	0	0	0	0	0	=	0										
4-5	0	0	0	0	0	0	=	0										
5-6	3	0	0	0	0	1	=	4										
6-7	3	0	0	0	1	0	=	4										
TOTAL	289	66	27	13	1	7	=	403										



# TRAFFIC VELOCITY TAB

ON - N.E. 36th AVE. S/BD. (#506)

S - OF - NE BRAZEE ST.

NON POSTED SPEED - 25 mph (School zone)

DATE: 10/9/79

DAY: Tuesday

WEATHER: Clear 75°

SPEED TIME	1-15	16-20	21-25	26-30	31-35	36+	=	1 HR. TOTAL								SPEED	VOLUME	PERCENT
7-8 AM	25	12	3	0	0	2	=	42								1-15	496	68.0%
8-9	41	12	5	1	1	2	=	62								16-20	137	18.8
9-10	17	13	1	2	0	3	=	36								21-25	49	6.7
10-11	12	6	3	2	0	1	=	24								26-30	12	1.7
11-12	21	9	4	2	0	3	=	39								31-35	6	0.8
12-1 PM	38	20	5	0	0	2	=	65								36+	29	4.0
1-2	18	8	5	0	0	1	=	32										
2-3	61	10	4	0	0	2	=	77										
3-4	35	6	3	0	0	3	=	47										
4-5	23	5	3	3	0	1	=	35										
5-6	31	7	4	0	0	2	=	44										
6-7	34	5	0	1	0	2	=	42								TOTAL	729	100.0%
7-8	23	7	4	0	0	1	=	35								REMARKS: 85% of the 21-M.P.H. w/ diversions (temp.) School in		
8-9	27	2	2	0	1	0	=	32										
9-10	75	10	3	0	3	1	=	92										
10-11	2	4	0	0	0	2	=	8										
11-12	7	0	0	0	0	0	=	7										
12-1 AM	1	0	0	1	0	0	=	2										
1-2	0	0	0	0	0	0	=	0										
2-3	0	0	0	0	0	0	=	0										
3-4	0	0	0	0	0	0	=	0										
4-5	0	0	0	0	0	0	=	0										
5-6	2	0	0	0	1	1	=	4										
6-7	3	1	0	0	0	0	=	4										
TOTAL	496	137	49	12	6	29	=	729										



# TRAFFIC VELOCITY TAB

ON - N.E. 37<sup>th</sup> AVE. N/BD. (#507)

N - OF - N.E. Thompson St.

NON POSTED SPEED - 25 mph

DATE: 10/9/79

DAY: Tuesday

WEATHER: Clear 75°

SPEED TIME	1-15	16-20	21-25	26-30	31-35	36+	=	1 HR TOTAL							SPEED	VOLUME	PERCENT
7-8 AM	1	1	9	11	2	0	=	24							1-15	45	5.0%
8-9	1	4	24	24	9	1	=	63							16-20	110	12.1
9-10	1	5	9	12	3	1	=	31							21-25	313	34.5
10-11	2	4	13	17	4	1	=	41							26-30	329	36.3
11-12	6	5	18	12	5	1	=	47							31-35	91	10.0
12-1 PM	5	11	31	29	9	2	=	87							36+	19	2.1
1-2	3	4	7	4	1	0	=	19									
2-3	2	10	25	32	7	3	=	79									
3-4	3	5	22	35	9	1	=	75									
4-5	3	7	19	29	6	3	=	67									
5-6	4	9	26	32	10	1	=	82									
6-7	4	21	35	30	5	3	=	98							TOTAL	907	100.0%
7-8	5	10	30	13	5	0	=	63							REMARKS: 857-File 31-M.P.H.  w/diverters (Temp.), School Tr		
8-9	0	5	20	12	4	1	=	42									
9-10	1	5	16	13	5	1	=	41									
10-11	0	1	3	11	4	0	=	19									
11-12	1	0	3	5	1	0	=	10									
12-1 AM	0	0	0	1	0	0	=	1									
1-2	1	1	0	2	1	0	=	5									
2-3	1	1	0	0	0	0	=	2									
3-4	0	0	1	2	0	0	=	3									
4-5	0	0	1	0	0	0	=	1									
5-6	0	1	0	2	0	0	=	3									
6-7	1	0	1	1	1	0	=	4									
TOTAL	45	110	313	329	91	19	=	907									



# TRAFFIC VELOCITY TAB

ON - N.E. 37th AVE S/Bd. (#508)  
 -OFF- N.E. Thompson St  
 NON-POSTED SPEED - 25 mph.

DATE: 10/9/79

DAY: Tuesday

WEATHER: Clear 75°

SPEED	TIME	1-6	7-8	8-9	9-10	10-11	11-12	12-1	1-2	2-3	3-4	4-5	5-6	6-7	7-8	8-9	9-10	10-11	11-12	12-1	TOTAL	PERCENT
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7-8	2	2	43	105	27	5	=	184																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																											
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## REMARKS:

85%ile 31-MPH.  
 w/diverters (temp.)  
 school in



# TRAFFIC VELOCITY TAB

ON - N.E. 36TH AVE. N/BND

SOUTH-OF- N.E. BRAZEE ST.

POSTED SPEED - SCHOOL ZONE - 20 MPH

DATE: OCT. 10, 1979

DAY: WEDNESDAY

WEATHER: CLEAR

SPEED TIME	1- 15	16- 20	21- 25	26- 30	31- 35	36+										SPEED	VOLUME	PERCENT
7-8 <sup>AM</sup>	22	5	0	0	0	1?	=	28								1-15	234	71.12%
8-9	26	12	2	3	0	0	=	43								16-20	296 62	89.96 18.84%
9-10	3	2	0	0	0	0	=	5								21-25	316 20	98.04 6.08%
10-11	3	1	0	0	0	0	=	4								26-30	321 5	97.56 1.52%
11-12	16	5	2	0	0	0	=	23								31-35	325 4	98.78 1.22%
12-1 <sup>PM</sup>	17	5	2	0	0	0	=	24								36+	329 4	100.00 1.22%
1-2	9	2	4	1	0	0	=	16									329	100%
2-3	22	6	1	0	1	1	=	31										
3-4	10	7	2	1	0	0	=	20										
4-5	12	3	2	0	1	0	=	18										
5-6	13	4	1	0	1	0	=	19										
6-7	37	2	3	0	0	1	=	43										
7-8	20	1	0	0	0	0	=	21										
8-9	11	1	0	0	0	0	=	12										
9-10	8	2	0	0	0	0	=	10										
10-11	0	2	1	0	1	0	=	4										
11-12	1	0	0	0	0	0	=	1										
12-1 <sup>AM</sup>	0	0	0	0	0	0	=	0										
1-2	0	0	0	0	0	0	=	0										
2-3	0	0	0	0	0	0	=	0										
3-4	0	0	0	0	0	0	=	0										
4-5	0	0	0	0	0	0	=	0										
5-6	1	1	0	0	0	1	=	3										
6-7	3	1	0	0	0	0	=	4										
TOTAL	234	62	20	5	4	4	=	329										

## REMARKS:

85%ile 20-M.P.H.  
w/ dividers (temp.),  
School Th



# TRAFFIC VELOCITY TAB

ON - N.E. 36th AVE N/Bd. (#388)

N - OF N.E. Brazee St.

POSTED SPEED - 20 mph.

DATE: 8/17/79

DAY: FRIDAY

WEATHER: Cloudy

SPEED TIME	1- 19	20- 24	25- 29	30- 34	35- 39	40+ =	HR. TOTAL								SPEED	VOLUME	PERCENT
7-8 <sup>AM</sup>	0	1	0	0	0	0	= 1								1-19	107	64.85
8-9	1	0	2	0	0	0	= 3								20-24	38	23.03
9-10	4	1	0	0	0	0	= 5								25-29	17	10.30
10-11	2	1	0	0	0	0	= 3								30-34	3	1.82
11-12	9	4	0	0	0	0	= 13								35-39	0	0.00
12-1 <sup>PM</sup>	9	2	0	1	0	0	= 12								40+	0	0.00
1-2	9	4	0	1	0	0	= 14										
2-3	8	3	1	0	0	0	= 12										
3-4	7	5	2	0	0	0	= 14										
4-5	10	2	2	0	0	0	= 14										
5-6	8	5	5	0	0	0	= 18										
6-7	2	1	3	1	0	0	= 7								TOTAL	165	100.0%
7-8	14	1	0	0	0	0	= 15								REMARKS:  85% speed <del>24+ M.P.H.</del> between <del>20 mph</del> and <del>24 mph</del>  Before diverters, School out		
8-9	5	2	1	0	0	0	= 8										
9-10	9	2	0	0	0	0	= 11										
10-11	3	0	0	0	0	0	= 3										
11-12	4	1	1	0	0	0	= 6										
12-1 <sup>AM</sup>	2	0	0	0	0	0	= 2										
1-2	1	1	0	0	0	0	= 2										
2-3	0	2	0	0	0	0	= 2										
3-4	0	0	0	0	0	0	= 0										
4-5	0	0	0	0	0	0	= 0										
5-6	0	0	0	0	0	0	= 0										
6-7	0	0	0	0	0	0	= 0										
TOTAL	107	38	17	3	0	0	= 165										



# TRAFFIC VELOCITY TAB

ON - N.E. 36th Ave. S/Bd. (#392)

N. - OF - N.E. Thompson St

POSTED SPEED - 20 mph.

DATE: 8/17/79

DAY: Friday

WEATHER: Cloudy

SPEED TIME	1-19	20-24	25-29	30-34	35-39	40+	=	HR TOTAL							SPEED	VOLUME	PERCENT
7-8 AM								23							1-19	199	61.61
8-9								35	} est.						20-24	55	17.03
9-10	2	1	0	1	1	0	=	5	}						25-29	43	13.31
10-11	12	5	2	2	0	0	=	21							30-34	16	4.95
11-12	16	1	4	1	0	1	=	23							35-39	5	1.55
12-1 PM	15	6	4	2	0	1	=	28							40+	5	1.55
1-2	26	7	3	1	0	1	=	38									
2-3	17	4	0	1	0	0	=	22									
3-4	18	6	3	1	0	0	=	28									
4-5	31	8	2	0	1	1	=	43									
5-6	12	2	7	1	0	0	=	22									
6-7	10	2	4	0	1	0	=	17							TOTAL	323	100.0%
7-8	9	3	2	1	0	1	=	16							REMARKS:  15 hours only - Vandals unplugged Lead hose.  85% speed between 27 + M.P.H. 25 mph and 29 mph.  Before diversions, School out		
8-9	12	2	0	1	0	0	=	15									
9-10	9	3	6	1	1	0	=	20									
10-11	4	3	1	1	1	0	=	10									
11-12	3	2	4	2	0	0	=	11									
12-1 AM	3	0	1	0	0	0	=	4									
1-2								0	} est.								
2-3								0									
3-4								0									
4-5								0									
5-6								2									
6-7								2									
TOTAL	199	55	43	16	5	5	=	323	} est.								



# TRAFFIC VELOCITY TAB

ON - N.E. 37<sup>th</sup> Ave. N/Bd. (#391)

N - OF - N.E. Brazee St

NONPOSTED SPEED - 25 mph

DATE: 8/17/79

DAY: Friday

WEATHER: Cloudy

SPEED TIME	1- 19	20- 24	25- 29	30- 34	35- 39	40+ =	HR TOTAL								SPEED	VOLUME	PERCENT
7-8 AM	0	0	1	0	0	0	= 1								1-19	82	12.33
8-9	0	3	3	0	0	0	= 6								20-24	<sup>261</sup> 179	<sup>39.25</sup> 26.92
9-10	4	5	7	3	0	0	= 19								25-29	<sup>530</sup> 269	<sup>79.70</sup> 40.45
10-11	4	12	13	7	1	1	= 38								30-34	<sup>637</sup> 107	<sup>95.79</sup> 16.09
11-12	5	7	12	4	1	0	= 29								35-39	<sup>656</sup> 19	<sup>98.63</sup> 2.86
12-1 PM	10	16	20	11	5	0	= 62								40+	<sup>665</sup> 9	<sup>100.00</sup> 1.35
1-2	2	15	11	11	2	1	= 42										
2-3	8	8	16	5	3	1	= 41										
3-4	7	12	22	13	1	0	= 55										
4-5	5	20	27	9	2	1	= 64										
5-6	15	18	32	11	2	1	= 79										
6-7	7	14	23	7	1	0	= 52								TOTAL	665	100.0%
7-8	2	11	11	6	0	0	= 30								REMARKS:  85 % speed between 25 mph and 29 mph 32 - M.P.H. Before dividers, school out		
8-9	3	16	14	3	0	1	= 37										
9-10	3	5	13	7	0	2	= 30										
10-11	2	5	8	3	1	0	= 19										
11-12	0	2	12	1	0	0	= 15										
12-1 AM	0	0	12	0	0	0	= 12										
1-2	2	2	4	3	0	0	= 11										
2-3	1	1	5	1	0	0	= 8										
3-4	0	2	2	0	0	0	= 4										
4-5	0	2	0	0	0	0	= 2										
5-6	0	0	0	2	0	0	= 2										
6-7	0	0	1	0	0	0	= 1										
TOTAL	82	179	269	107	19	9	= 665										



# TRAFFIC VELOCITY TAB

ON - N.E. 37<sup>th</sup> Ave. S/Bd. (#390)

S - OF - N.E. Brainerd St.

NON POSTED SPEED - 25 mph.

DATE: 8/12/79  
 DAY: Friday  
 WEATHER: Cloudy

SPEED TIME	1- 19	20- 24	25- 29	30- 34	35- 39	40+ 40+	=	HR TOTAL							SPEED	VOLUME	PERCENT
7-8 <sup>AM</sup>	3	2	13	5	2	1	=	26							1-19	82	7.90
8-9	6	12	17	3	0	0	=	38							20-24	<sup>364</sup> 282	<sup>35.07</sup> 27.17
9-10	13	29	9	1	0	0	=	52							25-29	<sup>823</sup> 459	<sup>79.29</sup> 44.22
10-11	5	15	32	12	3	1	=	68							30-34	<sup>995</sup> 172	<sup>95.86</sup> 16.57
11-12	3	20	33	4	2	0	=	62							35-39	<sup>1024</sup> 29	<sup>98.65</sup> 2.79
12-1 <sup>PM</sup>	4	19	32	14	1	1	=	71							40+	<sup>1038</sup> 14	<sup>100.00</sup> 1.35
1-2	3	18	25	12	3	1	=	62									
2-3	5	21	38	17	4	0	=	85									
3-4	8	15	29	12	1	0	=	65									
4-5	9	11	40	13	4	2	=	79									
5-6	4	27	33	22	2	0	=	88									
6-7	4	22	50	12	0	2	=	90							TOTAL	1038	100.0%
7-8	6	25	28	11	2	1	=	73							REMARKS: 85% speed between 30 mph. and 34 mph 32+ M.P.H. before dividers, school out		
8-9	4	12	24	10	0	0	=	50									
9-10	0	11	15	6	1	2	=	35									
10-11	0	4	12	5	2	1	=	24									
11-12	1	8	3	7	1	0	=	20									
12-1 <sup>AM</sup>	0	1	6	1	0	0	=	8									
1-2	0	0	1	2	0	1	=	4									
2-3	0	0	2	0	0	0	=	2									
3-4	0	2	1	0	0	0	=	3									
4-5	0	1	2	1	0	0	=	4									
5-6	1	1	5	0	0	0	=	7									
6-7	3	6	9	2	1	0	=	21									
TOTAL	82	282	459	172	29	14	=	1038									



# TRAFFIC VELOCITY TAB

ON - N.E. 36th AVE N/Bd. (#388)

N - OF N.E. Brazee St.

POSTED SPEED - 20 m.ph.

DATE: 8/17/79

DAY: FRIDAY

WEATHER: Cloudy

SPEED TIME	1- 19	20- 24	25- 29	30- 34	35- 39	40+ =	HR. TOTAL								SPEED	VOLUME	PERCENT
7-8 AM	0	1	0	0	0	0	= 1								1-19	107	64.85
8-9	1	0	2	0	0	0	= 3								20-24	38	23.03
9-10	4	1	0	0	0	0	= 5								25-29	17	10.30
10-11	2	1	0	0	0	0	= 3								30-34	3	1.82
11-12	9	4	0	0	0	0	= 13								35-39	0	—
12-1 PM	9	2	0	1	0	0	= 12								40+	0	—
1-2	9	4	0	1	0	0	= 14										
2-3	8	3	1	0	0	0	= 12										
3-4	7	5	2	0	0	0	= 14										
4-5	10	2	2	0	0	0	= 14										
5-6	8	5	5	0	0	0	= 18										
6-7	2	1	3	1	0	0	= 7								TOTAL	165	100.0%
7-8	14	1	0	0	0	0	= 15								REMARKS:  85% speed between 20 mph and 24 mph <24		
8-9	5	2	1	0	0	0	= 8										
9-10	9	2	0	0	0	0	= 11										
10-11	3	0	0	0	0	0	= 3										
11-12	4	1	1	0	0	0	= 6										
12-1 AM	2	0	0	0	0	0	= 2										
1-2	1	1	0	0	0	0	= 2										
2-3	0	2	0	0	0	0	= 2										
3-4	0	0	0	0	0	0	= 0										
4-5	0	0	0	0	0	0	= 0										
5-6	0	0	0	0	0	0	= 0										
6-7	0	0	0	0	0	0	= 0										
TOTAL	107	38	17	3	0	0	= 165										



# TRAFFIC VELOCITY TAB

ON - N.E. 36<sup>th</sup> Ave. S/Bd. (#392)

N. -OF- N.E. Thompson St

POSTED SPEED - 20 mph.

DATE: 8/17/79

DAY: Friday

WEATHER: Cloudy

SPEED TIME	1-19	20-24	25-29	30-34	35-39	40+	=	HR TOTAL							SPEED	VOLUME	PERCENT
7-8 <sup>AM</sup>															1-19	199	61.61
8-9															20-24	55	17.03
9-10	2	1	0	1	1	0	=	5							25-29	43	13.31
10-11	12	5	2	2	0	0	=	21							30-34	16	4.95
11-12	16	1	4	1	0	1	=	23							35-39	5	1.55
12-1 <sup>PM</sup>	15	6	4	2	0	1	=	28							40+	5	1.55
1-2	26	7	3	1	0	1	=	38									
2-3	17	4	0	1	0	0	=	22									
3-4	18	6	3	1	0	0	=	28									
4-5	31	8	2	0	1	1	=	43									
5-6	12	2	7	1	0	0	=	22									
6-7	10	2	4	0	1	0	=	17							TOTAL	323	100.0%
7-8	9	3	2	1	0	1	=	16							REMARKS:  15 hours only - Vandal's unplugged Lead hose.  85% speed between 25 mph and 29 mph. 9/2/77		
8-9	12	2	0	1	0	0	=	15									
9-10	9	3	6	1	1	0	=	20									
10-11	4	3	1	1	1	0	=	10									
11-12	3	2	4	2	0	0	=	11									
12-1 <sup>AM</sup>	3	0	1	0	0	0	=	4									
1-2																	
2-3																	
3-4																	
4-5																	
5-6																	
6-7																	
TOTAL	199	55	43	16	5	5	=	323									



# TRAFFIC VELOCITY TAB

ON - N.E. 37th Ave. N/Bd. (#391)

N - OF - N.E. Brazee St.

UNPOSTED SPEED - 25 mph.

DATE: 8/17/79

DAY: Friday

WEATHER: Cloudy

SPEED TIME	1- 19	20- 24	25- 29	30- 34	35- 39	40+ =	HR TOTAL								SPEED	VOLUME	PERCENT
7-8 AM	0	0	1	0	0	0	= 1								1-19	82	12.33
8-9	0	3	3	0	0	0	= 6								20-24	179	26.92
9-10	4	5	7	3	0	0	= 19								25-29	269	40.45
10-11	4	12	13	7	1	1	= 38								30-34	107	16.09
11-12	5	7	12	4	1	0	= 29								35-39	19	2.86
12-1 PM	10	16	20	11	5	0	= 62								40+	9	1.35
1-2	2	15	11	11	2	1	= 42										
2-3	8	8	16	5	3	1	= 41										
3-4	7	12	22	13	1	0	= 55										
4-5	5	20	27	9	2	1	= 64										
5-6	15	18	32	11	2	1	= 79										
6-7	7	14	23	7	1	0	= 52								TOTAL	665	100.0%
7-8	2	11	11	6	0	0	= 30								REMARKS:  85 % speed between 25 mph. and 29 mph av. 32		
8-9	3	16	14	3	0	1	= 37										
9-10	3	5	13	7	0	2	= 30										
10-11	2	5	8	3	1	0	= 19										
11-12	0	2	12	1	0	0	= 15										
12-1 AM	0	0	12	0	0	0	= 12										
1-2	2	2	4	3	0	0	= 11										
2-3	1	1	5	1	0	0	= 8										
3-4	0	2	2	0	0	0	= 4										
4-5	0	2	0	0	0	0	= 2										
5-6	0	0	0	2	0	0	= 2										
6-7	0	0	1	0	0	0	= 1										
TOTAL	82	179	269	107	19	9	= 665										



# TRAFFIC VELOCITY TAB

ON - N.E. 37th Ave. S/Bd. (#390)

S - OF - N.E. Bragge St.

NON POSTED SPEED - 25 mph.

DATE: 8/12/79

DAY: Friday

WEATHER: Cloudy

SPEED TIME	1- 19	20- 24	25- 29	30- 34	35- 39	40+ =	HR TOTAL								SPEED	VOLUME	PERCENT
7-8 <sup>AM</sup>	3	2	13	5	2	1	= 26								1-19	82	7.90
8-9	6	12	17	3	0	0	= 38								20-24	282	27.17
9-10	13	29	9	1	0	0	= 52								25-29	459	44.22
10-11	5	15	32	12	3	1	= 68								30-34	172	16.57
11-12	3	20	33	4	2	0	= 62								35-39	29	2.79
12-1 <sup>PM</sup>	4	19	32	14	1	1	= 71								40+	14	1.35
1-2	3	18	25	12	3	1	= 62										
2-3	5	21	38	17	4	0	= 85										
3-4	8	15	29	12	1	0	= 65										
4-5	9	11	40	13	4	2	= 79										
5-6	4	27	33	22	2	0	= 88										
6-7	4	22	50	12	0	2	= 90								TOTAL	1038	100.0%
7-8	6	25	28	11	2	1	= 73								REMARKS: 85% speed between 30 mph. and 34 mph ap. 32		
8-9	4	12	24	10	0	0	= 50										
9-10	0	11	15	6	1	2	= 35										
10-11	0	4	12	5	2	1	= 24										
11-12	1	8	3	7	1	0	= 20										
12-1 <sup>AM</sup>	0	1	6	1	0	0	= 8										
1-2	0	0	1	2	0	1	= 4										
2-3	0	0	2	0	0	0	= 2										
3-4	0	2	1	0	0	0	= 3										
4-5	0	1	2	1	0	0	= 4										
5-6	1	1	5	0	0	0	= 7										
6-7	3	6	9	2	1	0	= 21										
TOTAL	82	282	459	172	29	14	= 1038										



BUREAU OF TRAFFIC ENGINEERING  
INTRA-OFFICE ROUTE SLIP

FROM

*W. J. King*

DATE

*4-3-79*

STAFF

ABELSEN

BAKER

BAUER

BERGSTROM

BIGGS

BURDETTE

BURNS

DAVIS

DORN

EVANS

FRANKLIN

FRYE

HASSETT

JAMES

JARMAN

KNUDSON

KOENIG

☒ MAGIN

MANLY

MARTINI

MASCO

MASON

NEELY

NISHIKAWA

NOZAKI

PARKS

PHELAN

SILLETT

SMITH

SPEER

STARK

WELD

WETMORE

WILSON

CIRCULATE

INITIAL

FILE

FOR YOUR INFORMATION

FOR YOUR APPROVAL

ORDER

PREPARE ANSWER (DRAFT)

RETURN TO

SEE ME

INVESTIGATE AND REPORT

PLEASE PROCESS

PLEASE ATTEND

REVIEW AND COMMENT

*May 8th 7:30pm*

*Fernwood school*

*Mrs. Carrie McLaughlin*

*called, would like*

*someone to explain*

*& discuss 36th Ave*

*diverters etc.*

TRAFFIC SAFETY

PARKING PATROL DIVISION



✓ Hold @ 37 - Three-Knot

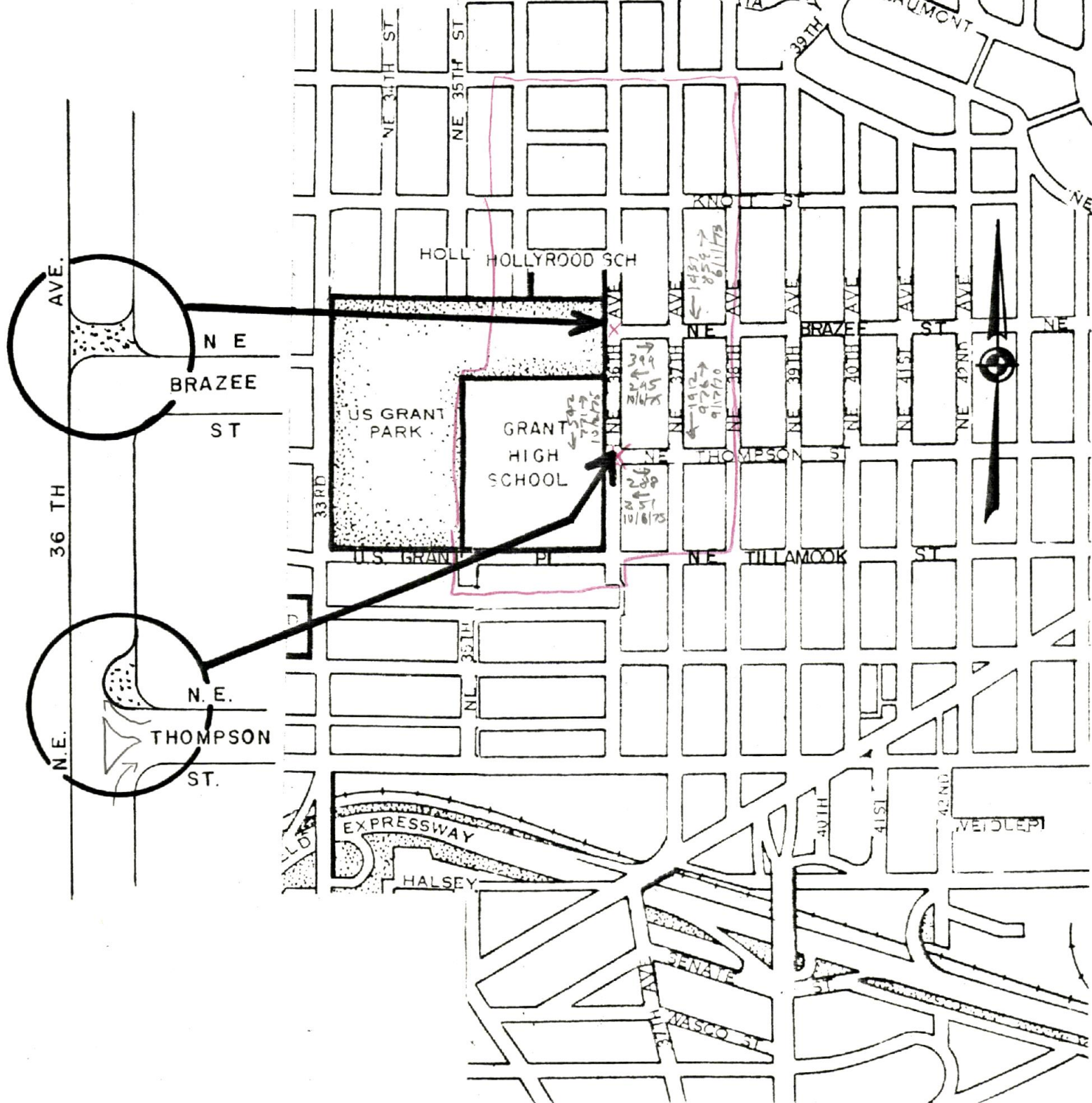
TC's 36, 37 2 cts. ea. st. + spd. cts.

LT 33/Knot pro 23  
con ~~23~~ 7

▽/Tpsu ok full karate Bze



# PROPOSED TRAFFIC DIVERTERS FOR GRANT PARK NEIGHBORHOOD 5-8-79





# PURCHASE REQUISITION

## CITY OF PORTLAND, OREGON

REQUISITION NUMBER

041

P.O. NUMBER

**February 24,** 19**78**

SUPPLY TO **Bureau of Traffic Engineering**

**VENDOR NUMBER**

**SHIPPING ADDRESS** **420 S.W. Main Street**

**Portland, Oregon 97204**

JOB NO. \_\_\_\_\_ EQUIP. NO. \_\_\_\_\_

V  
E  
N  
D  
O  
R

**TO BE COMPLETED BY PURCHASING ONLY.**

BUC NO/EXP CODE **58100340** / **638** CONTRACT NO. QUOTATION NO.

DELIVERY DATE REQUIRED \_\_\_\_\_ TERMS \_\_\_\_\_ F.O.B. POINT \_\_\_\_\_

REFER QUESTIONS ON REQ. TO **M. Nishikawa** PHONE NO. **248-4298**

**SUGGESTED VENDOR** **Custom Sprinkler Specialists** **PHONE NO. 253-4471**

ADDRESS 4827 N.E. 78th Avenue, Portland, Oregon 97218 Paul Frymark

**NOTE: IF INSUFFICIENT SPACE TO ITEMIZE, PROVIDE LIST PER FORMAT BELOW AND ATTACH 6 COPIES.**

[illegible]

I HEREBY CERTIFY THAT THE WORK, SUPPLIES, MATERIAL OR EQUIPMENT ABOVE SPECIFIED ARE NECESSARY FOR THIS A.U. AND BUDGETED FUNDS ARE AVAILABLE.

APPROVED:

TOTAL	\$380.00
-------	----------

## PURCHASING MANAGER

**BUYER**

**APPROPRIATION UNIT HEAD**

**COMMISSIONER**

R. U. FILE COPY



THE CITY OF  
**PORTLAND**



**OREGON**

Frank Ivancie

MAYOR

OFFICE OF  
PLANNING AND  
DEVELOPMENT

BUREAU OF TRAFFIC  
ENGINEERING

D.E. BERGSTROM  
CITY TRAFFIC ENGINEER

317 S.W. ALDER ST.  
PORTLAND, OR 97204  
(503) 248-4295

November 25, 1980

MEMORANDUM

TO: Mayor Frank Ivancie  
Commissioner Charles Jordan  
Commissioner Mike Lindberg  
Commissioner Mildred Schwab

FROM: Don Bergstrom *DEB*

SUBJECT: N. E. 36th and Brazee Traffic Diverter

At the Public Hearing scheduled for December 4, 1980, the Council will consider a proposed traffic diverter for the intersection of N. E. 36th and Brazee. The purpose of this memorandum is to provide the Council with background and current status of this diverter request.

BACKGROUND

The closure of N. E. 36th Avenue at Brazee is a project which began in early 1979 with a neighborhood request for two diverters to reduce traffic volume and speed created principally by young drivers attending or visiting Grant High School. (Map attached.)

A three month test period, consisting of temporary barricades was put into effect to ascertain if the problem could be solved. The results of the test show that the diverter at N. E. 36th and Brazee solved the traffic volume and speed problem. The test also showed that the diverter at N. E. 36th and Thompson could be eliminated.

As required by the Policy Memorandum for Traffic Diverters (copy attached) the Grant Park Neighborhood Association then circulated a petition requesting that the N. E. 36th and Brazee diverter be constructed.

This petition was received in mid-April of 1980 and verified that 75% of the affected residents were in favor of the single diverter. Since N. E. 36th and Brazee had been part of the original test, it was determined that no additional testing was necessary.

The diverter closes N. E. 36th Avenue on the north side of N. E. Brazee. An emergency lane is provided through the diverter at the fire Bureau's request. Access by other vehicles will be denied by bollards and traffic restricting signing.



To City Council  
November 25, 1980  
Page 2

CURRENT STATUS

Traffic Engineering has completed the necessary procedures required by the policy, which includes verification of petition signatures, comments from other affected bureaus (responses attached), preliminary design and cost estimates prepared by the City Engineer and submitted an ordinance for Council consideration.

The estimated cost of the N. E. 36th and Brazee diverter, as shown on the attached plan, is \$14,000.

RECOMMENDATION

The Bureau of Traffic Engineering recommends that the diverter at N. E. 36th and Brazee be approved and that Council appropriate funds for construction.

TVN:jjp

Enclosures: Area Map  
Policy Memorandum  
Bureau Responses  
Diverter Plan



PROPOSED  
TRAFFIC DIVERTERS  
FOR  
GRANT PARK  
NEIGHBORHOOD  
5-8-79





Diverter



DEPARTMENT OF  
PUBLIC WORKS  
CONNIE MCCREADY  
COMMISSIONER

OFFICE OF  
PUBLIC WORKS  
ADMINISTRATOR

400 S.W. SIXTH AVE.  
PORTLAND, OR. 97204

March 5, 1979

MEMORANDUM

TO: All Division and Section Supervisors

FROM: Donald Bergstrom, Chief  
Bureau of Traffic Engineering

John Lang, Chief  
Bureau of Street and Structural Engineering

SUBJECT: Policy Memorandum: Traffic Diverters

RECEIVED

MAR 2 1979

BUREAU OF  
TRAFFIC ENGINEERING

The following procedures shall be used in considering all requests for installation of traffic diverters:

- 1) Requests and justifications for all traffic diverters shall be proposed by an organized citizen's group or neighborhood association, accompanied by a formal petition signed by the owners of the majority of the property situated within two blocks in all directions of the project intersection.
- 2) The Bureau of Traffic Engineering shall be responsible for validating the petition and reviewing the diverter justification for obvious conflicts with existing City policy. Both the Bureau of Traffic Engineering and the Bureau of Street and Structural Engineering shall review the proposal from the standpoint of "state of the art" engineering considerations. This review shall include, but shall not be limited to:
  - street width
  - horizontal alignment
  - sight distance and stopping distance
  - vertical alignment
  - traffic volume

Should reason for denial arise, Traffic shall notify the requestor in writing listing reasons for such denial-- otherwise, Traffic shall distribute to all affected City agencies copies of the request and justification.

- 3) Each City agency shall review the request in terms of conformance with that agency's authorized goals and objectives. Foremost in each agency review shall be consideration of



impact on public safety, with secondary consideration given to site-specific benefit. Where major objections are noted, they shall be clearly documented together with suggested alternatives, if known.

- 4) Upon receipt of all City agency reviews, the Bureau of Traffic Engineering shall prepare a report summarizing agency comments and recommendations and forward same to the requestor, together with denial or tentative approval, as appropriate. If tentatively approved, Traffic Engineering will then notify the Bureau of Street and Structural Engineering to proceed with detailed construction plans.
- 5) The Bureau of Street and Structural Engineering, in cooperation with the Park Bureau and Maintenance Bureau, shall prepare preliminary construction and landscaping plans and cost estimates and deliver to the Bureau of Traffic Engineering.
- 6) The Park Bureau and Maintenance Bureau will prepare an estimate of the average annual cost of maintenance and a facsimile maintenance agreement specifying the life of the maintenance period and forward same to Bureau of Traffic Engineering.
- 7) Utilizing the above information, the Bureau of Traffic Engineering will prepare formal signing and signalization plans and prepare a project cost evaluation to confirm availability of funds.
- 8) The Bureau of Traffic Engineering shall arrange the construction of a temporary diverter at the project site, geometrically and functionally similar to the proposed permanent diverter installation. All signing and standard traffic control devices shall be installed with the exception of permanent lighting installations. The temporary diverter shall remain for a period of at least 3 months, during which any accidents, complaints or constructive suggestions shall be documented.
- 9) At the expiration of the 3-month trial period, the Bureau of Traffic Engineering shall conduct an evaluation of the installation with representatives of all affected City agencies and the requesting group. The requesting group shall provide proof of continued approval of the project by the owners of the majority of the property within two blocks in all directions of the project location. The Bureau of Traffic Engineering shall prepare a report to the City Council recommending approval/denial of the project, a budget impact statement and an ordinance authorizing the expenditure of funds.



- 10) Upon notification of Council approval, the City Engineer shall prepare final plans and bidding documents and request bids through the City Purchasing Agent. When bids are received, the City Engineer will prepare an ordinance authorizing award of contract to the low bidder. Maintenance agreements will then be signed by the Park Bureau and Maintenance Bureau. The City Engineer will be responsible for the project during the construction phase, while progress or final payments shall be made by the Bureau of Traffic Engineering.
- 11) An annual diverter review should be conducted by the Bureau of Traffic Engineering to identify any diverters where the risk of significant claims may have developed. Where it is apparent that necessary protective devices, visual barriers or required safety features cannot be effectively maintained resulting in increased liability exposure to the City of Portland, or where a significant accident history has developed, the Bureau of Traffic Engineering shall recommend to the City Council that the diverter be removed and that the area be restored to its original condition. At the time of the annual diverter review, representatives of the requesting group shall be asked to provide facts or opinion regarding the advisability of removing or retaining the existing diverter.

The above information shall be presented by the Bureau of Traffic Engineering to the City Council for final disposition.

TLB:li



Page

# OFFICE of FIRE MARSHAL

FIRE PREVENTION DIVISION

CITY OF PORTLAND


**From:** Kenneth Owens, Chief Fire Marshal, 130/304      **Date:** 9/29/80  
**To:** M. J. Martini, Bureau of Traffic Engineering, 133/301  
**Subject:** Your Memo dated September 9, 1980, Concerning Proposal to Install Traffic Diverter at NE 36th Avenue and Brazee Street.

The sketch you enclosed with your memo did not indicate a scale, but it appears to be 1" - 10'. If that is so, then the indicated fire lane is too narrow. Fire code requires a minimum of 12 feet for a fire lane.

None are shown, but there should be no obstructions such as posts, chains, etc., obstructing emergency vehicle access to the fire lane.

The hydrant as shown in its present location would be unsuitably located for Fire Bureau use. We recommend that it be moved to the corner of N. E. 36th and Brazee.

We are in general opposed to traffic diverters. They are restrictions to normal flow of traffic and, as such, have the potential for hampering or delaying fire suppression activities. The neighborhood association should be apprised of that fact.

By   
Gerald E. Edwards  
Staff Captain

GEE/lh

RECEIVED

SEP 30 1980

BUREAU OF  
TRAFFIC ENGINEERING





BUREAU OF  
POLICE

CHARLES JORDAN  
COMMISSIONER

B. R. BAKER  
CHIEF OF POLICE

222 S.W. PINE  
PORTLAND, OR. 97204

MEMORANDUM

July 8, 1980

TO: M.J. Martini, Traffic Engineering

FROM: B.R. Baker, Chief of Police

SUBJECT: Blockage of N.E. 36th Avenue at Brazee

After reviewing the proposal to block N.E. 36th Avenue at Brazee with a curbed planted divider, it is our opinion that it would present no problem. East Precinct and Traffic Division Commanders stated that the temporary closure last year presented no trouble to their officers.

We request that the dead end southbound and the detour northbound be well posted.

B. R. BAKER  
Chief of Police

BRB/JWJ/pm

**RECEIVED**

**JUL 14 1980**

**BUREAU OF  
TRAFFIC ENGINEERING**



THE CITY OF  
**PORTLAND**



**OREGON**

DEPARTMENT OF  
PUBLIC AFFAIRS

MILDRED A. SCHWAB  
COMMISSIONER

BUREAU OF PARKS AND  
PUBLIC RECREATION

DOUGLAS W. BRIDGES  
SUPERINTENDENT

6437 S.E. DIVISION AVE.  
PORTLAND, OREGON 97206

September 16, 1980

MEMORANDUM

TO: M.J. Martini  
Bureau of Traffic Engineering

FROM: Roland Hall *R. Hall*  
Bureau of Parks & Recreation

SUBJECT: Traffic Diverter  
N.E. 36th & Brazee Street

The Bureau of Parks' interest in the proposed traffic diverter is primarily with any horticultural plantings within this construction.

The concerns would be with the selection of shrubbery and the method of irrigating such plantings followed by the assignment of maintenance and resulting costs.

Please advise of any further progress in this project.

RH/r

RECEIVED  
SEP 24 1980  
BUREAU OF  
TRAFFIC ENGINEERING



**MAYOR**

**SEC.**

**MAYOR'S OFFICE**

Re; Proposed Traffic Plan  
N.E. 36th Avenue & Brazee Street

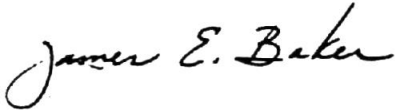
In addition, Mr. Stan Blatr, principal at Grant High School has given his support to this project.



Mr. D.E. Bergstrom  
April 5, 1980  
Page 2

I would like to acknowledge that a lot of hard work went into the canvassing of the neighborhood in order to achieve 75% support. The supporters of this project trust that The Bureau of Traffic Engineering will proceed as rapidly as possible with the next step in implementing this most needed project. Please advise us as to what help we might be able to give to your office.

For the Crant Park Neighborhood Association,

A handwritten signature in cursive script that reads "James E. Baker".

James E. Baker, President

cc: Mayor Connie McCreedy ✓  
William R. White



September 9, 1980

MEMORANDUM

TO: Bob Gustafson, Park Bureau  
Chief Sargent, Fire Bureau  
Chief Baker, Police Bureau

FROM: M. J. Martini, Bureau of Traffic Engineering

This bureau has received a request from the Grant Park Neighborhood Association to install a traffic diverter at N.E. 36th Avenue and Brazee Street. We are enclosing a sketch of the proposal and would appreciate any remarks or suggestions you might have prior to final design.

You will notice that the fire lane is angled, this is to prevent a straight line of sight through the diverter.

We would appreciate your comments by October 1st, and if you have any questions please contact T.V. Neely at 248-4290.

TVN:ah



September 9, 1930

MEMORANDUM

TO: Chief Baker, Police Bureau  
Chief Sergeant, Fire Bureau  
Bob Gustafson, Park Bureau

FROM: M. J. Martini, Bureau of Traffic Engineering

This Bureau has received a request from the Grand Park Neighborhood Association to install a traffic divider at W.E. 36th Avenue and Grand Street. We are enclosing a sketch of the proposal and would appreciate any remarks or suggestions you might have prior to final design.

You will notice that the fire lane is angled, this is to prevent a straight line of sight through the divider.

We would appreciate your comments by October 1st, and if you have any questions please contact T.V. Neely at 248-4290.

TVM:am



THE CITY OF  
**PORTLAND**



**OREGON**

DEPARTMENT OF  
PUBLIC AFFAIRS

MILDRED A. SCHWAB  
COMMISSIONER

BUREAU OF PARKS AND  
PUBLIC RECREATION

DOUGLAS W. BRIDGES  
SUPERINTENDENT

6437 S.E. DIVISION AVE.  
PORTLAND, OREGON 97206

September 16, 1980

MEMORANDUM

TO: M.J. Martini  
Bureau of Traffic Engineering

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Bureau of Parks & Recreation

SUBJECT: Traffic Diverter  
N.E. 36th & Brazee Street

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The concerns would be with the selection of shrubbery and the method of irrigating such plantings followed by the assignment of maintenance and resulting costs.

Please advise of any further progress in this project.

RH/r

RECEIVED  
SEP 24 1980  
BUREAU OF  
TRAFFIC ENGINEERING





*to Neely or  
Martini*

*Keep with  
other correspondence  
Do not lose*

OFFICE OF  
CITY AUDITOR  
GEORGE YERKOVICH  
CITY AUDITOR

1220 S.W. FIFTH AVE.  
PORTLAND, OR. 97204

November 20, 1980

Dear Property Owner/Resident:

The City Council at the meeting which will commence at 2:00 p.m., Thursday, December 4, 1980, in the Council Chamber of City Hall will consider the report of the Traffic Engineer recommending installation of a traffic diverter at the intersection of N.E. 36th and Brazee, as shown on map on back of this letter.

The Grant Park Neighborhood Association petitioned the City to install a permanent diverter (closure) to reduce traffic volume and speed at N.E. 36th and Brazee.

The neighborhood has experienced the effect of this closure during the 3-month temporary test installation last year.

If you wish to comment and do not plan to attend the hearing, you may express your opinion by writing to the City Council, Room 202, City Hall, 97204, for receipt prior to December 4.

If you have questions, please call M.J. Martini, Bureau of Traffic Engineering, 248-4295.

Yours very truly,

GEORGE YERKOVICH

Auditor of the City of Portland  
BY:

*Edna Corvera*

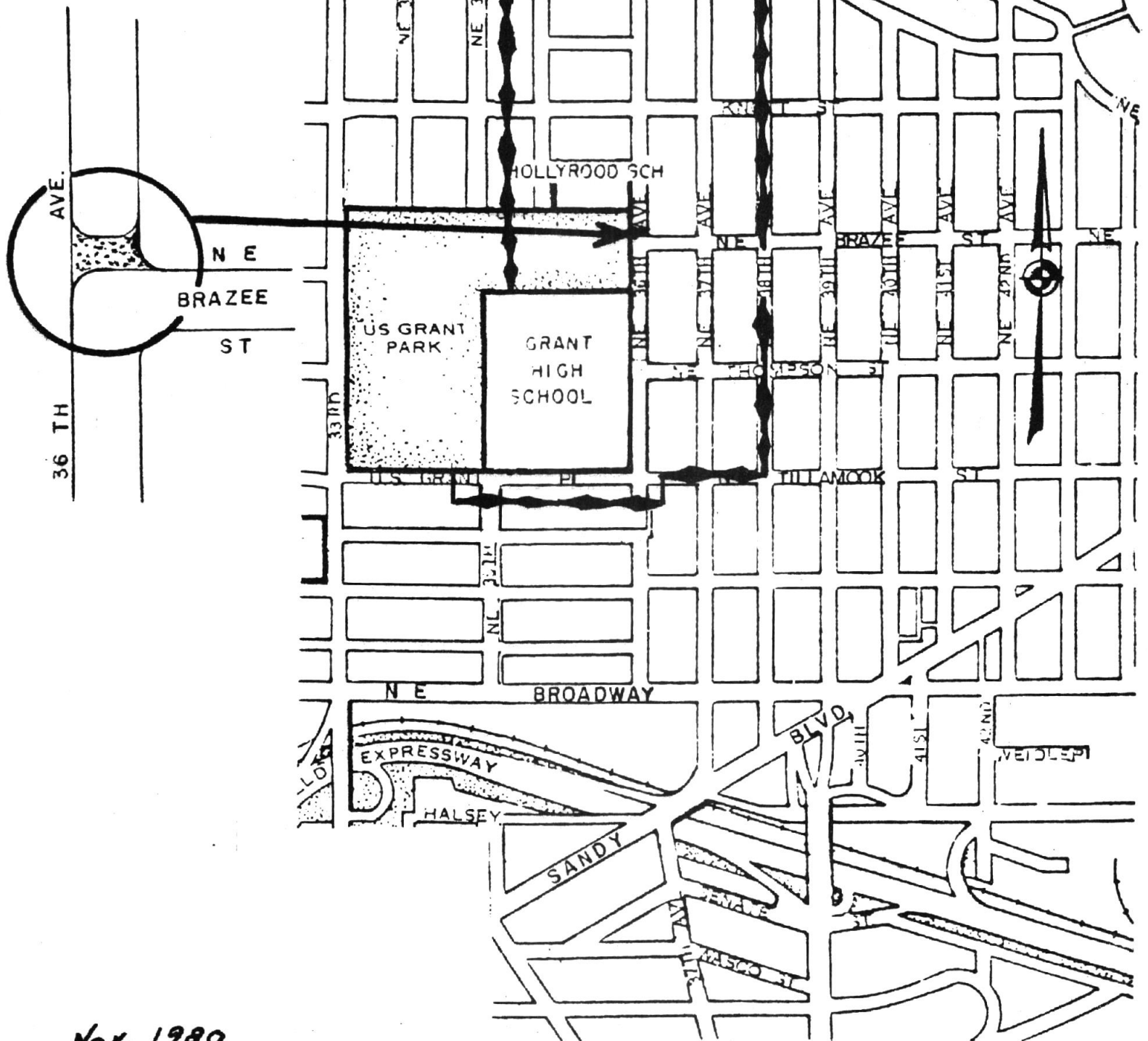
Deputy

EC:lv



# PROPOSED TRAFFIC DIVERTER FOR GRANT PARK NEIGHBORHOOD

**PETITION BOUNDARY**



Nov. 1980



231 owner  
43 res  
274

Traffic Eng

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
+ SCHOUTEN, WILLEM & SALLY J	2825 NE 35TH PL	HOLLYROOD 2	8
+ GRAVES, HAROLD S & EDITH E	2815 NE 35TH PL N 40' OF	HOLLYROOD 3	8
+ O'CONNELL, GLENDA L	2805 NE 35TH PL S 10' OF N 30' OF	HOLLYROOD 3 4	8 8
+ HOSKINS, CHARLES S & ELEANOR S	3108 NE 27 AVE S 30' OF N 10' OF	HOLLYROOD 5 6	8 8
+ GREGG, QUENTIN R & HELEN F	2745 NE 35TH PL S 40' OF	HOLLYROOD 6	8
+ MILEY, RICHARD F & DOROTHY J	2735 NE 35TH PL	HOLLYROOD 7	8
+ <del>SUCHECKI, J RICHARD</del> TO PERIGO, CLIFFORD C & CAROLE J	2725 NE 35TH PL	HOLLYROOD 8	8
+ SCHOEN, STEVEN & JANICE	2715 NE 35TH PL	HOLLYROOD 9	8
+ FITCHER, JOSEPHINE E	2801 NE 35 PLACE S 20' OF N 20' OF	HOLLYROOD 4 5	8 8
+ FILSINGER, HILDA J	2705 NE 35TH PLACE	HOLLYROOD 10	8
+ FREIMOELLER, HANS O JR & YVONNE R	4801 SE ANKENY ST	HOLLYROOD 1	8
+ RENDAR, BYRON M	3586 NE STANTON AVE	HOLLYROOD 1	9
+ SCHOELLHORN, ETHEL K	3580 NE STANTON ST	HOLLYROOD 2	9
+ BRAUN, ROBERT A & BEACH, ORALEE S	3574 NE STANTON ST	HOLLYROOD 3	9
+ VAN BRAMER, TERRY M	3570 N E STANTON E 40' OF	HOLLYROOD 4	9
+ LARSON, EDWARD J SR & DOROTHY L	3566 N.E. STANTON ST. W 10' OF E 34' OF	HOLLYROOD 4 5	9 9
+ VAN LIEW, VINCENT L & KAREN A	3560 NE STANTON W 16' OF E 26' OF	HOLLYROOD 5 6	9 9

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
+ DE JESUS, ADELINA M	2834 NE 35TH PL N 50' OF W 24' OF N 50' OF	HOLLYROOD 6 7	9 9
+ OIEN, ALVIN F	RT 1 BOX 126A-1 S 50' OF S 50' OF W 24' OF	HOLLYROOD 8 9	9 9
+ SILVA, ROSEANNA J	2814 NORTH EAST 35TH PL N 50' OF N 50' OF W 24' OF	HOLLYROOD 8 9	9 9
+ FERRON, DYSON P & JEANNE M	3561 NE MATHISON PL E 26' OF W 18' OF	HOLLYROOD 9 10	9 9



	W 12' OF	11	9
+ ZIMMERMAN, DORIS L	3575 NE MATHISON PLACE E 44' OF	HOLLYWOOD 12	9
+ SPIER, RICHARD G & JASATTIS, JENNIFER C	3581 NE MATHISON PL	HOLLYWOOD 13	9
+ VIDLUND, ESTHER M	3585 NE MATHISON PLACE	HOLLYWOOD 14	9
+ CRUTCHER, DAVID M & S LYNDIA	2745 NE 36TH AVE N 1/2 OF	HOLLYWOOD 1&2	10
+ SHUMP, NATALIE E	3574 NE MATHISON PL	HOLLYWOOD 3	10
+ NICHOLSON, WILLIAM I & ANNA L	3570 NE MATHISON PLACE	HOLLYWOOD 4	10
+ JOHNSTON, PAMELA K	3564 NE MATHISON	HOLLYWOOD 5	10
+ MORTENSEN, BOONE B & WITTKOPP, SANDRA J	2744 N.E. 35TH PLACE N 1/2 OF	HOLLYWOOD 6&7	10
+ REICHLE, DAVID S & ALICE H	2734 NE 35TH PLACE S 1/2 OF	HOLLYWOOD 6&7	10
* HEIKKILA, HENRY H & MARTLYN J BY FIRST STATE BANK	<del>1800 S MAIN ST</del> 2724 NE 35TH PL (12)	HOLLYWOOD 8	10
+ LOTZENHISER, JACK B & LYDIA C	2725 NE 36TH AVE	HOLLYWOOD 9	10
+ BAUGH, ROBERT C & BERGMAN, LINDA L	3581 NE KNOTT ST	HOLLYWOOD 11	10

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
+ NELSON, MACLAY F & RUTH-60% & SEGO, MARGARET-40%	11736 NE BRAZEE ST	HOLLYWOOD 12	10
+ SCOTT, HARRIETTE C	3571 NE KNOTT ST	HOLLYWOOD 13	10
+ BARAGER, RICHARD G & ANNE M	3565 NE KNOTT ST	HOLLYWOOD 14	10
+ WARFIELD, WALTER	324 NW 3RD AVE S 1/2 OF	HOLLYWOOD 15&16	10
+ ALDRED, LILLIAN E <del>DEFERRED TAX</del>	2714 NE 35TH PLACE N 1/2 OF	HOLLYWOOD 15&16	10
+ POTTER, GEORGIA	3614 NE STANTON ST E 1/2 OF	HOLLYWOOD 1	11
+ BAKER, GAYLE A	8200 SW ROSS W 1/2 OF	HOLLYWOOD 1	11
+ GRAY, PHILIP L BY FIRSTBANK MTGE CO	SECURITIES BLDG	HOLLYWOOD 2	11
+ MIZER, VICTOR C & DORIS R	2812 NE 36TH AVE	HOLLYWOOD 3	11
+ THOMAS, JENNET H	2756 NE 36TH AVE	HOLLYWOOD 5	11
+ SCHMEER, HENRY & SCHMEER, JOHN	1332 NE 114TH AVE	HOLLYWOOD 6	11
+ JACKSON, JOHN A & ETHEL	2734 NE 36TH AVE	HOLLYWOOD 7	11
+ GISH, SHERWOOD L & FRANCES E	2724 NE 36TH AVE	HOLLYWOOD 8	11
+ GIRTMAN, RODOLPH C & RUTH E BY NATIONAL LIFE INS CO INVESTMENT ACCOUNTING DIV	E 1/2 OF	HOLLYWOOD 9&10	11
+ STOUTT, ROGER L & LONA I	2710 NE 36TH AVE	HOLLYWOOD	



OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
<del>STADLMAN, HEIDI K</del> TO MESKEL, MILO J JR & CHERYL A	3610 NE KNOTT W 54.08' OF	HOLLYWOOD 182	12
<del>WASSERMANN, TODD &amp; SUZANNE</del>	2616 NE 3 TH	HOLLYWOOD 3	12
<del>HANMACK, DOROTHY M</del>	2604 NE 34TH AVE	HOLLYWOOD 4	12

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
<del>LARSON, DAVID M &amp; MYRTLE H</del>	2550 NE 36TH AVE	HOLLYWOOD 5	12
<del>LEIGH, DOROTHY</del>	2544 NE 36TH AVE	HOLLYWOOD 6	12
<del>BASTIAN, STEVEN T &amp; WHITE, SUSAN P</del>	2534 NE 36TH AVE EXC S 3'	HOLLYWOOD 7	12
<del>SCHOOL DISTRICT NO 1</del> <i>P.O. Box 3107</i>	631 NE CLACKAMAS NONTAXABLE	HOLLYWOOD 1-7	13
<del>LAHENEY, FRANK J &amp; RUTH S</del> <del>DEFERRED TAX</del>	P O BOX 1517	HOLLYWOOD 182	14
<del>ORIN, ALFRED &amp; BONNIE</del> <del>DEFERRED TAX</del>	3576 NE KNOTT ST	HOLLYWOOD 3	14
<del>WILLIAMS, CLARISSA D</del>	4914 NE 60TH ST	HOLLYWOOD 4	14
<del>WOODARD, MORRIS W &amp; ALICE S</del>	3560 NE KNOTT ST	HOLLYWOOD 6	14
<del>KIEL, DOROTHY</del>	3554 NE KNOTT ST	HOLLYWOOD 7	14
<del>THOMAS, ESTELLA L &amp; DOYLE, DOROTHY M</del>	3553 NE HOLLYWOOD CT	HOLLYWOOD 8	14
<del>TAYLOR, ALBERT M &amp; DIANE E</del> <del>BY WESTERN PACIFIC</del> <del>FINANCIAL CORP</del>	555 N D ST <i>3557 NE Hollywood Ct (12)</i>	HOLLYWOOD 9	14
<del>CASBON, MICHAEL A &amp; RACHEL D</del>	3565 NE HOLLYWOOD COURT	HOLLYWOOD 10	14
<del>EMBREE, DAN W &amp; GENEVIEVE</del>	3575 NE HOLLYWOOD COURT	HOLLYWOOD 11	14
<del>EMBREE, DAN W &amp; GENEVIEVE A</del>	3575 NE HOLLYWOOD COURT	HOLLYWOOD 12	14
<del>FLEMING, JAMES L &amp; GERANNA</del>	3581 NE HOLLYWOOD CRT S 1/2 OF	HOLLYWOOD 13&14	14
<del>KASSNER, KIRK &amp; JULIE L</del>	3530 NE KNOTT ST	HOLLYWOOD 1	15
<del>GAGNON, LAWRENCE P &amp; PAMELA J</del>	2625 NE 35TH PL	HOLLYWOOD 2	15
<del>BURROUGHS, DANIEL F &amp; IRENE B</del> <del>BY THE MANHATTAN LIFE INS CO</del>	<del>111 WEST 57TH ST</del> <i>2615 NE 35th Pl (13)</i>	HOLLYWOOD 3	15

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
<del>BATLEY, DEBRA K</del> TO FRANK J SUSAK, ESQ	813 SW ALDER ST #500	HOLLYWOOD 4	15
<del>MONSON, LANE S &amp; MARY E-2/3 &amp; MONSON, LANE S-1/3</del>	2521 NE 35TH PL	HOLLYWOOD 6	15
<del>POPPINGA, GERHARD G &amp; NORMA J</del>	2505 NE 35TH PL	HOLLYWOOD 7	15
<del>HARPER, JOHN H JR &amp; SUSAN, S</del>	2804 NE 36TH AVE	HOLLYWOOD 4	11



+ HARPER, DAN & DELOE	45088 McKenzie Hwy	19	16
+ BARFIELD, ARTHUR L & MARY E	2735 NE 34TH AVE S 1/2 OF	HOLLYWOOD 182	10
+ NORDGAARD, VERNON C & GERTRUDE	3571 NE MATHISON PLACE E 38' OF W 6' OF	HOLLYWOOD 11 12	9 9
+ AMEND, VIOLET	2824 NE 30TH PLACE S 50' OF W 24' OF S 50' OF	HOLLYWOOD 6 7	9 9
+ WEBER, MARIE <del>DEFERRED TAX</del>	2525 NE 35TH PLACE	HOLLYWOOD 5	15
+ NICKERSON, WILLIAM A & EVE K	2615 N. E. 36TH AVENUE N 1/2 OF	HOLLYWOOD 13814	14
+ BLANDINE, VICTOR J <del>BY LOMAS &amp; NETTLETON CO</del>	<del>700 NORTON BLVD</del> 3564 NE Knott	HOLLYWOOD 5	14
+ CAFFARELLI, ELOUISE	2524 NE 36TH AVE S 3' OF	HOLLYWOOD 7	12
+ JOHNSON, CLARENCE G & IRIS H	3616 NE KNOTT ST E 54.08' OF	HOLLYWOOD 182	12
+ PETERSON-NEDRY, HARRY P & JUDITH A	3632 NE STANTON ST	BEAUMONT 1	27
+ WEIGERT, GARY E	2835 NE 37TH AVE	BEAUMONT 2	27
+ NORTON, RUTH	2825 NE 37TH AVE	BEAUMONT 3	27
+ WHEELER, ALVIN C & RUTH L	2815 N.E. 37TH AVENUE	BEAUMONT 4	27
+ FRASER, PATRICIA A	2805 NE 37TH AVE	BEAUMONT 5	27

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
+ HANLON, DANIEL J & VERNA C	2735 NE 37TH AVE	BEAUMONT 7	27
+ KLICK, ROGER A & MARY A	2721 NE 37TH AVE	BEAUMONT 8	27
+ HAMLIN, RICHARD A & BETTY R	2715 NE 37TH AVE	BEAUMONT 9	27
+ HAGGART, ROBERT & SHIRLEY E	2705 NE 37TH AVE	BEAUMONT 10	27
+ CRAIG, ROBERT W & SHERLOCK, MICHELE L	2835 NE 38TH AVE	BEAUMONT 2	28
+ ANDERSON, WILLIAM H & IRENE	2825 NE 38TH AVE	BEAUMONT 3	28
+ STOFIEL, ALAN L & SANDRA K	2815 NE 38TH AVE	BEAUMONT 4	28
+ HARRISON, RICHARD B & MARY L	2805 NE 38TH AVE	BEAUMONT 5	28
+ BRYSON, CARLTON W & ELSA L	2735 NE 38TH	BEAUMONT 7	28
+ SZIMONISZ, GEORGE P & CAROL L	2723 NE 38TH AVE	BEAUMONT 8	28
+ KLIMP, BENE G & ADRIANA C	2715 NE 38TH AV	BEAUMONT 9	28
+ WORD, MAY S	2705 NE 38TH AVE	BEAUMONT 10	28
+ MOORE, TERRY J & CONNIE L	3711 NE KNOTT ST	BEAUMONT 11	28
+ KURTZ, PEABODY F	2722 NE 37TH AVE	BEAUMONT	



† ELSTON, CHESTER E & MARIAN L	2734 NE 37TH AVE	BEAUMONT 14	28
† HOLOCH, E D & PEARL M	2744 NE 37TH AV	BEAUMONT 15	28
† CAMPBELL, BERNICE M	2804 NE 37TH AVE	BEAUMONT 16	28
† BEHRENDT, CARL F & RUTH T	2824 NE 37TH AVE	BEAUMONT 18	28

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
† MITCHELL, JOHN E & KAY	2834 NE 37TH	BEAUMONT 19	28
† MC CREADY, GARY L & MICHELLE	2844 NE 37TH AV	BEAUMONT 20	28
† FOWLER, LLOYD J & PATRICIA A	5447 NE MASON ST	BEAUMONT 6	27
† SHEARER, DOROTHY L	2845 NE 38TH AVE	BEAUMONT 1	28
† ELBERSON, PATRICIA R	2743 NE 38TH AVE	BEAUMONT 6	28
† BURGESS, ALAN R & VIRGINIA M	2714 NE 37TH AVE	BEAUMONT 12	28
† HAMEL, FRANCIS A & CAROL R	2814 NE 37TH AVE	BEAUMONT 17	28
† RIVERMAN, CHAUNCY K & LINDA	2645 NE 38TH AVE	BEAUMONT 1	41
† LUCIA, ANTHONY P & WHITEFORD, ANN E	2623 NE 38TH AVE	BEAUMONT 3	41
† MASON, MICHAEL D & VONZEL M	2615 N.E. 38TH AVE.	BEAUMONT 4	41
† HANEY, JEAN W & WILSON, MARGARET	2605 NE 38TH AVE	BEAUMONT 5	41
† WHEELER, JAMES I JR & MARILYN L	2545 NE 38TH AVE	BEAUMONT 6	41
† WONG, TERRY B	2535 NE 38TH AVE	BEAUMONT 7	41
† CHARUHAS, PETER A & SALLY J	2536 N.E. 37TH AVE. S 8' OF	BEAUMONT 8 9	41 41
† KELNER, JUDITH	2544 NE 37TH AVE N 42' OF	BEAUMONT 9	41
† PHILLIPS, GENEVIEVE K	2614 NE 37TH AVE	BEAUMONT 11	41
† PIERSON, JAMES W & BARBARA H	2624 NE 37TH AVE	BEAUMONT 12	41
† BARBER, VIVIAN J	2634 NE 37TH AVE	BEAUMONT 13	41

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
† ATWOOD, MARYLIN E BY COULTAS WM & MARYLN	2644 NE 37TH AVE	BEAUMONT 14	41
† MANSFIELD, CHARLES A & CRISPI, SUSAN D	2645 NE 37TH AVE	BEAUMONT 1	42
† SCHLOSS, PHYLLIS G ET AL BY CARROLL L. LUTHE ET AL	2635 NE 37TH AVE	BEAUMONT	



		3	42
WALHOOD, JOHN D & MILDRED H BY SHERWOOD & ROBERTS	<del>P.O. BOX 1117</del> 2615 NE 37th Ave	BEAUMONT 4	42
PEARSON, WALTER S	2605 NE 37TH AVE	BEAUMONT 5	42
SEERONEN, JOHN R & PATRICIA K	2535 NE 37TH AVE S 9' OF	BEAUMONT 6 7	42 42
SIDEBOTTOM, CHESTER B & MILDRED A	2545 NE 37TH AVE N 41' OF	BEAUMONT 6	42
SNIDER, SUELLA M	2604 NE 37TH AVE	BEAUMONT 10	41
LA CLAIR, LARRY D	2772 SE VAN WATERS ST	BEAUMONT 2	41
MURPHY, MILDRED H	2531 NE 38TH AVE N 1/2 OF	ROSSMERE 8	8
HEIDER, MARY A	2521 NE 38TH AVE S 1/2	ROSSMERE 8	8
WRIGHT, GARY D & JANET L	2503 NE 38TH AVE	ROSSMERE 10	8
JAMES, GEORGE W & BONNIE L	2504 NE 37TH	ROSSMERE 11	8
ROBINSON, THOMAS B & CHARLENE K	2514 NE 37TH AVE	ROSSMERE 12	8
GARDNER, HAROLD M & JEANNE D	2526 NE 37TH AVE	ROSSMERE 13	8
KARMAN, RICHARD J & DOROTHY A	2515 NE 37TH AVE	ROSSMERE 9	9
BOWLER, DONALD F JR & LYNN E	2505 NE 37TH AV	ROSSMERE 10	9

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
MALLING, NEIL P & JOAN S	2443 NE 37TH AVE	ROSSMERE 1	10
HOLMBERG, LINDA S & HAAN, ANNE D-LE	2435 NE 37TH AVE	ROSSMERE 2	10
PRODEHL, WILLARD M & IRENE N	2415 NE 37TH AVE	ROSSMERE 4	10
SANDERSON, J T & EDITH M	2403 NE 37TH AVE	ROSSMERE 5	10
ATKISSON, ARTHUR A & JOSEPHINE K	2345 NE 37TH AVE	ROSSMERE 6	10
NEHER, THOMAS A & MARCIA	14239 SE HARRISON	ROSSMERE 7	10
DRAPER, JEFFREY T & YVONNE C	2323 NE 37TH AVE	ROSSMERE 8	10
KIRSCHNER, ALAN G	2305 NE 37TH AVE	ROSSMERE 9	10
WINCHESTER, EARL H & VIRGINIA B	2303 N.E. 37TH	ROSSMERE 10	10
SOUTHWICK, RAYMOND P & PHILLIPS, JOHN L	2443 NE 38TH	ROSSMERE 1	11
ROSE, JAMES R & RUTHANN	2425 NE 38TH AVE	ROSSMERE 3	11
STRONK, MICHAEL P & JERI	486 SW RIVERBEND DR	ROSSMERE 4	11



+ KERNAN, RONALD G & DEBORAH K	2343 NE 37TH AVE	ROSSMERE 6	11
+ DEARBORFF, FRANCIS A & JOYCE C	2331 NE 37TH AVE	ROSSMERE 7&8	11
+ SCHAFF, LAZARE & AVA M	2315 NE 37TH AVE	ROSSMERE 9	11
+ WHITLOW, LEONARD A II & CATHERINE C	2307 N E 38TH	ROSSMERE 10	11
+ DODGE, JOHN S & KATHLEEN M	3705 N. E. THOMPSON W 1/2 OF	ROSSMERE 11&12	11

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
+ CRABTREE, DAVID N & KLARI K	2324 NE 37TH S 10' OF	ROSSMERE 13 14	11 11
+ WAGNER, BLANCHE	2334 NE 37TH AVE N 40' OF S 5' OF	ROSSMERE 14 15	11 11
+ CAMPBELL, MARY C & LOOS, MARTHA T	2926 NE 20 N 45' OF	ROSSMERE 15	11
+ FITZGERALD, KENNETH W & WANDA L	2404 NE 37TH AVE	ROSSMERE 16	11
+ PHILLIPS, FREDRICK W & BOBBIE J	2414 NE 37TH AVE	ROSSMERE 17	11
+ LANGDON, CHARLES & DOROTHY W <del>DEFERRED TAX</del>	2424 NE 37TH AVE	ROSSMERE 18	11
+ BARNES, ROBERT A & NANCY J	2434 NE 37TH AVE	ROSSMERE 19	11
+ DURAND, ROBERT C & DYER, ROBERTA J	P O BOX 587	ROSSMERE 20	11
+ FOWLER, ALEEN J	6627 SW TERRI CRT #33	ROSSMERE 2	11
+ WRIGHT, RICHARD B & JANE M	2425 NE 37TH	ROSSMERE 3	10
+ GRIFFITH, JACQUELYN K	2525 NE 37TH AVE	ROSSMERE 8	9
+ ALKIRE, JOHN J & JANET C	2515 NE 38TH AVE	ROSSMERE 9	8
+ WILLHITE, ROBERT C & MARY A	3711 NE THOMPSON ST E 1/2 OF	ROSSMERE 11&12	11
+ TUCKER, DONALD M & LUCILLE I	2235 NE 38TH AVE N 2' OF	ROSSMERE 1 2	26 26
+ TEBOREK, BRUCE & CATHY J	2225 NE 38TH AVE S 48' OF	ROSSMERE 2	26
+ WILKINS, MARGARET B	2217 NE 38TH AVE	ROSSMERE 3	26
+ PARKHURST, ARTHUR W	3024 NE 48TH AVE	ROSSMERE 4	26

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
+ HARWELL, CHARLES H & SUSAN B	2137 NE 38TH AVE	ROSSMERE 5	26
+ CUNNINGHAM, ANNABEL C	2127 NE 38TH AVE	ROSSMERE	



+ STEELE, HELEN M	2115 NE 37TH AVE	ROSSMERE	7	26
+ YOST, DAVID J & CAROLE J	RT 1 BOX 121	ROSSMERE	9	26
+ PANTLE, THOMAS A & PANTLE, ALVIN T-LE	2114 NE 37TH AVE	ROSSMERE	10	26
+ <del>DAVIS, PATRICK J</del> TO GEARHART, ROSS E	3267 SE HAWTHORNE BLVD	ROSSMERE	11	26
+ BOUNDS, WILLIAM E & HELEN J	P O BOX 1547	ROSSMERE	12	26
+ PAULSON, JAMES A & FRANKIE L	2204 NE 37TH AVE	ROSSMERE	13	26
+ HENDRICKSON, RALPH	2224 NE 37TH AVE	ROSSMERE	15	26
+ GHEEN, E PENNOCK & MICHELLE L	3706 N.E. THOMPSON ST.	ROSSMERE	16	26
+ STOETZER, ERNEST & GERALDINE	2235 NE 37TH AVE	ROSSMERE	1	27
+ MILLER, BENJAMIN H	2225 NE 37TH AVE	ROSSMERE	2	27
+ DEKLOTZ, LEONARD A & MARY H	2203 NE 37TH AVE	ROSSMERE	4	27
+ BRIDWELL, WILLIAM J	9755 SE FRENCH ACRES DR.	ROSSMERE	5	27
+ LUSK, RICHARD J SR & PAULA I	2123 NE 37TH	ROSSMERE	6	27
+ EGAN, LEWIS R & IRETHA	2115 NE 37TH AVE	ROSSMERE	7	27
+ BIRLEW, THAYER M & EVELYN B	1544 NE 114TH AVE	ROSSMERE	8	27
+ HARDING, D'ARCY D & ETHEL K	2107 NE 38TH AVE	ROSSMERE	8	26

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
+ LA VATTA, GEORGE P & VIOLA F	2214 NE 37TH AVE	ROSSMERE 14	26
+ MC QUADE, HUBERT J & RUBY R	2215 NE 37TH AVE	ROSSMERE 3	27
+ JACOBSON, JACK & EDITH	20423 LANDER DR	U S GRANT ADD 1	
+ ROBNETT, BARBARA	2504 NE 36TH AVE	U S GRANT ADD 3	
+ CHARNESKI, HELEN S	2434 NE 36TH AVE	U S GRANT ADD 5	
+ HACKING, ROBERT R & ANNE L	2424 N. E. 36TH	U S GRANT ADD 6	
+ YAKYMI, PAUL & MARY L	2414 NE 36TH AVE	U S GRANT ADD 7	
+ THOMAS, RICHARD D & LARIE A	2404 NE 36TH	U S GRANT ADD 8	
+ LIBKE, GRACE E & LIBKE, STEVEN W	2344 NE 36TH AVE	U S GRANT ADD 9	
+ ROMANS, PAUL B & DIANE C	2324 NE 36TH	U S GRANT ADD 11	
+ GERBER, ROBERT E & LYNELL E	2314 NE 36TH AVE	U S GRANT ADD 12	



OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
BUCK, FERRY E ET AL TO BUCK, RALPH J	1722 NE CLACKAMAS ST	U S GRANT ADD 15	13
STEINHAUER, HAROLD & KATHRYN	2214 NE 36TH AVE	U S GRANT ADD 16	
SCHADWITZ, MELVIN R & LANORA M	2204 NE 36TH AVE	U S GRANT ADD 17	
AKIN, IRA B	2134 NE 36TH AVE	U S GRANT ADD 18	
FEELY, MAURICE L & HESTER E	2114 NE 36TH	U S GRANT ADD 20	
JOHNSTON, MAXINE	3605 NE US GRANT	U S GRANT ADD 21	

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
SEIDEL, BERNARD H JR & ELAINE H	3616 NE US GRANT PL E 1/2 OF	U S GRANT ADD 22&23	
PERMAN, DARRELL F & DONNA J	3604 NE US GRANT PL W 1/2 OF EXC PT TKN FOR ST ORD 71522-W 1/2 OF	U S GRANT ADD 22 23	
PIETKA, DAVID E & JENNIFER N	3584 NE US GRANT PL	U S GRANT ADD 24	
BLAKESLEE, GEORGE G & JANE S	3576 NE U S GRANT PLACE	U S GRANT ADD 25	
OWEN, LORA	3560 NE U S GRANT PL	U S GRANT ADD 27	
DAY, JEFFREY E & CAROL W	3552 NE U S GRANT PL	U S GRANT ADD 28	
POLEN, BETTY R	3546 NE U S GRANT PLACE	U S GRANT ADD 29	
NAKAYAMA, JANE M	3536 NE US GRANT	U S GRANT ADD 30	
LORENZO, A R & MARY E	3520 NE US GRANT PL	U S GRANT ADD 32	
HOLMAN, DOROTHY M	3504 NE U S GRANT PLACE	U S GRANT ADD 34	
FOX, MELVIN & JEAN H	3434 NE US GRANT	U S GRANT ADD 35	
LOTTTRIDGE, DORIS	3568 NE U S GRANT PLACE	U S GRANT ADD 26	
CHRISTEN, JOHN M & CATHY A	2124 NE 36TH AVE	U S GRANT ADD 19	
WINTER, CHARLOTTE C & WINTER, JUNE	2234 NE 36TH AVE	U S GRANT ADD 14	
EVANS, MILTON E & BETTY L	2336 NE 36TH	U S GRANT ADD 10	
EGGE, JEROME M & KATHLEEN T	2442 NE 36TH AVE	U S GRANT ADD 4	
LEESON, FRED D	2514 NE 36TH AVE	U S GRANT ADD 2	
HICKS, PATRICK L & SUSAN J	3514 NE U S GRANT PL	U S GRANT ADD 33	

OWNER / TAXPAYER NAME(S)	ADDRESS LEGAL LINE	ADDITION LOT	BLOCK
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0 SCHOOL DISTRICT NO 1

631 NE CLACKAMAS  
TL# 34 10.02 ACRES  
NONTAXABLE

SECTION 25 1 N 1 E

0 PORTLAND CITY OF

1700 SW 4TH AVE  
TL# 37 12.86 ACRES  
NONTAXABLE

SECTION 25 1 N 1 E



2605 NE 35th Pl (12)

Richard Fredrickson 2725  
Mobile Woodcraft 2755  
L M Dien 2804

M L Gwynn 2524 NE 36 (13)

Wm A Nickerson 2615  
Bill Casey 2735  
Donald Kelley 2742  
S J Fryberger 2824  
C Stevenson 2824

D Heise 2105 NE 37 (12)

M Dietrich 2124  
C Petric 2124  
L L Rossman 2134  
Joe Bridwell 2135  
William Weaver 2324  
M L Coulisshaw 2344  
R J Dyer 2444  
D L Glassman 2714  
Dr Otto Ruehle 2715  
Timothy Berry 2721  
Jim Loney 2745  
Peter Mack 2824



JM Baade 2415 NE 38 (12)

Harold Hay 2433

Susan Nielson 2535

L M Young 2535

T E McCormick 2635

A Hui 3534 NE Stanton (12)

Charlotte Miller 3586

Molly Nunn 3586

Victor J Blandine 3564 NE Knott (13)

Steven Dozier 3570 NE Knott

Michael Ready 3577

Frank Laheney 3584

C E Ryan 3585

Milo Mestel Jr 3610

David Arredondo 3552 NE US Grant Pl (12)

Michael Combx 3552

Vernon Nordgaard 3571 NE Mathison Pl (12)

Bert Taylor 3557 NE Hollyrood Ct (12)

Joseph Embree 3569

Grant Park Neighborhood Assn 2504 NE 38 (12)



Don,

I spoke with Bob Packard, president of the Grant Park Neighborhood Association, this afternoon about the 36th and Brazee diverter. He had heard that Mildred had refused to file that ordinance, and wanted to give us some additional background information about the project.

Basically, it was the number one priority item on the neighborhood needs requests for 2 years. There had been some problems due to traffic from Grant High. With the Hollyrood School so close, there had been some anxiety about the traffic. Both the PTA, with the association of Hollyrood parents, and the neighborhood association lobbied for this, and did all of the circulation of petitions to area residents to request this project. This started 2-2½ years ago. Just before the test, 1 person was killed at 36th and Knott.

Initially, two diverters had been tested, this one, and another at 36th and Tillamook. The neighborhood felt that permanent installation of this diverter only would suffice to alleviate the problem. The neighborhood, the school, and the police had all agreed that this slowed traffic sufficiently on 36th, made the park ~~safer to gain access~~ access safer, and did not disrupt the flow of traffic in the neighborhood in such a manner as to cause problems for the residents.

He reiterated that this is still a main concern for the residents of the neighborhood, and they do support this action.

A File



45  
URB 35  
3-7  
1980

January 14, 1980

RECEIVED

JAN 17 1980

BUREAU OF  
TRAFFIC ENGINEERING

City of Portland  
Bureau of traffic engineering  
M. J. Martini Sr Traffic Engineer

This letter is regarding the area of N. E. 36th ave., and N. E. U S Grant Place and N. E. Knott Streets.

This is a formal complaint and request that we have NO diverters and road closure on our Street.

The above was done on a temporary basis and found to have many faults. First, I quote traffic Engineer Bill Stark, who said "our policy states that if a proposal doesn't solve a problem, we don't institute it." It has been agreed, at the last Neighborhood Association meeting at Grant High School, that the trial diverters and road closures did not work. The plan moved more traffic around and around each block, also 95% of the local traffic went thru and around the diverters & road closures. There was more traffic going around the blocks on 37th ave., this has always been a through street and we would like to keep it that way.

The fire dep't trucks could not make their way around the barriers and people loading or unloading their children at school are just driving around and around the blocks.

It seems that 2 people at the Neighborhood Association meeting wanted to put something like this in permanently. We property owners on this street would have this burden with us all of the time because we have to go alot farther to come and go from our home. The school children have created it for 15 minutes, it was agreed at the meeting that about 15 minutes after school is out is the only time any problem might exist. It was suggested that Grant High School and Fernwood could change their closing time so they don't co inside at the same time.

We would like to suggest that a flashing Red Light be installed at the nearest intersection by Fernwood School and be placed there permanently to give the children more time to cross the street if that is necessary. We notice that the flashing red light at the intersection of N. E. 37th and Grant Place has been 100% effective in controlling the traffic in that area and it does work in solving or saving traffic accidents and slows down traffic so it would work there.

We went to local neighborhood ass'n meeting last time because a flyer was sent around by the association to discuss the diverters and closures. The majority of the people did not want the diverters and closures because it is just another BLIGHT in our neighborhood. (We already have 2 of these Blights at intersections of US Grant Place and 36th and 35th ave.) These diverters have caused many traffic problems, including cars getting their wheels & drive portions caught on them, to students wheeling around and around the intersection.

At the local meeting I also saw 2 very small children with their hands up voting with their parents on obtaining another petition. Why their votes counted on such a problem when the parents tell them to is not right.

There was NO traffic problem in front of Grant High School until diverters were placed there. Then, nearly every car going by was going around deverter to the right or left and going against traffic, creating a head on collision problem. Do we want some one killed by such a situation?

There is a lot less students going to school than ever before and also less traffic do to the energy problem, so since the trial program did not work we don't need anything done that doesn't solve problems that 2 people seem to think exists.

- continued,

+ R. 9012



January 14 1980

Continued, page 2, Sr. Traffic Engineer,

We have lived here facing the street for almost 15 years and a problem does not exist. This street is no different than any other city street.

Mr Feely will be contacting you by phone about this area which he wants to discuss with you. He is at work most of the time but will try to contact you.

Mr and Mrs Maurice Feely  
2114 N . E. 36th  
Portland, Oregon 97212  
287 7328



BUREAU OF TRAFFIC ENGINEERING  
INTRA-OFFICE ROUTE SLIP

FROM Manini DATE 12-4-80

STAFF

BAUER

BERGSTROM

BURDETTE

BUTTENHAM

CHOATE

DAVIS

DORN

ELLISON

EVANS

FRANKLIN

HASSETT

JAMES

KNUDSON

KOENIG

LOOMIS

MAGIN

MARTINI

MASCO

MASON

MUIR

NEELY

NOZAKI

PARKS

PHELAN

SCHOMANN

SPEER

STARK

THOMPSON

WEBER

WETMORE

WILSON, JIM

WILSON, MAURY

CIRCULATE

INITIAL

FILE

FOR YOUR INFORMATION

FOR YOUR APPROVAL

ORDER

PREPARE ANSWER (DRAFT)

RETURN TO

SEE ME

INVESTIGATE AND REPORT

PLEASE PROCESS

PLEASE ATTEND

REVIEW AND COMMENT

PLEASE DISCUSS WITH ME

*Letter re:*

*U.E. 36<sup>th</sup> of BNAZEE*

*Division*

PARKING PATROL DIVISION



4283  
RECEIVED  
DEC 2 1980

December 1, 1980

City of Portland  
Office of the Mayor, Frank Ivancie;  
1220 S. W. 5th Ave.,  
Portland, Oregon 97204

MAYOR'S OFFICE

Dear Sir:

This is in reply to recent notice of action on N. E. 36th Ave. Barrier in front of Grant High School.

Because we are both ill we will be unable to attend the Dec. 4th meeting at cityHall.

This is a comment on it, and we protest any barriers on this street and in this area. It was deemed unnecessary by the trial period because people drove over to all sides of the barrier which caused near-accidents. That was the only thing it resolved. People crossing the street Of N. E. 36th between Grant Place and Knott streets have plenty of time to cross the street including teachers, students and residents of the area. There is no time that it takes a person to cross N. E. 36th street in this area longer than one minute.

This Barrier, and the ones on Grant Place should be permanently removed from here. The barriers are traffic hazards, cars run over them, the busses cannot ~~turn~~ turn, the flower beds are a mud mess and an extreme accident hazard exists in these areas because they do not permit a smooth traffic flow. The residents of the area do not deserve to have our neighborhoods messed up by a lot of concrete and brick barriers. Somebody could kill themselves or someone else. We have lived ~~in~~ on this street for over 15 years and we do not believe that we need any barriers of any kind out in the streets because it is so much nicer to have the open areas like it was for so many years.

The Fire Department trucks cannot make their runs to this area in the time allowed, which is very important in saving lives, because of the barriers.

It was noted by area residents at the Association meeting we attended that <sup>there</sup> are only two times of the day that there is very much traffic in the area, and that is before school opening and after school closing for about 10 minutes total. Then traffic is a slow normal pace. If there are occasional speeders, this can be taken care of by issuing tickets restricting them from driving in the area to a second offense of a suspended driver's license, that would remove the problem.

The Neighborhood Association president was all out to push this through at local meeting without comment or alternate solutions. We don't need to spend our money like that. The cost of the barriers is too much when it can be adequately taken care of by any problem that might exist by installing stop signs or a flashing red light, they do work. We have a 4-way stop and a flashing red light situation nearby at the intersection of N. E. # 37th and Tillamook street which is adequate.

We do not want to be plagued by double-parking and head-on collisions on our street. Also we would have to drive a lot farther to get to and from our residence, creating more traffic on other neighborhood streets. There are a lot less students going to Grant High School now than there ever has been in the past so this is not a critical issue. In all the years that we have lived here it is much less traffic than it used to be, and no different than any other city street.

Please give this your Consideration, as residents of this street do not need this.

*Remona*  
287-7328

Mrs. M. L. Feeley  
2114 N E 36th Ave



4283

2203 N.E. 37 Avenue  
Portland, Oregon 97212  
November 22, 1980

RECEIVED  
NOV 25 8 22 AM 1980  
CITY CLERK  
CITY OF PORTLAND, ORE.  
City Council  
Room 202  
City Hall  
Portland, Oregon 97204

Re: Traffic diverter at N.E. 36th & Brazee

Sirs:

The objective here is to reduce traffic volume and speed.

I understand that installation of bumps is objected to by the traffic engineer because a driver may lose control and sue the city. I should think that the same objection would apply to construction of a barrier across a street.

I do not see why bumps with an advance warning sign would not be effective and much cheaper.

Or one could use "stop" signs and a flashing red light.

At the meeting I attended, the effect on access by emergency vehicles was not discussed. The minutes lost by an ambulance driver approaching the area incorrectly could prove to have serious consequences.

The schools in the area indicate that any fire alarm would be responded to by numerous vehicles. The interference with freedom of movement under such circumstances could prove to be disastrous.

Has the freedom of access to fire hydrants been investigated? Have fire and police officials been consulted regarding the effect of such a street closure on their response to an emergency at the schools, such as fire, explosion, or riot?

In conclusion, it seems to me that closure of this street is an "overkill" response to the problem. Further study may be desirable.

Very truly yours,

*L. A. DeKlotz*  
L. A. DeKlotz, P.E.

*Remont*



4283

RECEIVED

2814 N.E. 37th

DEC 1 8 13 AM 1980

Portland Ore.  
97212GEORGE W. KENNICH, AUDITOR  
CITY OF PORTLAND, ORE.

City Council

BY

Rm 202

City Hall  
Portland Ore  
97204Dear Council:

This is in reply to the  
traffic diverter at N.E. 36th  
and Bronze.

Last year with the temporary  
installation put in we discovered  
and increase of traffic on 37th  
and 39th and 42nd. This was  
mainly after Grant high school  
let out, which was the reason  
for the traffic diverter. We  
saw a near collision on 42nd  
due to kids passing between  
thompson and Knott. The kids  
are using these other streets



as their race way.  
this makes it bad because  
there are no patrols for  
the younger kids ~~at~~ these  
streets (37<sup>th</sup>, 39<sup>th</sup> + 42<sup>nd</sup>)

We have lived here  
on 37<sup>th</sup> avenue for the last  
thirteen years. We've had  
trouble with kids speeding  
up and down 37<sup>th</sup>. We feel  
the speeding will increase  
with the traffic diverter  
put in permanent.

We have children  
attending Hallwood and the  
patrol at 36<sup>th</sup> + Knott is  
very sufficient and necessary.

Thank you for your  
time to read this letter.

*Remont.*

Mr + Mrs. Francis Hamel

Note NO ON TRAFFIC Diverter



4283

November 27

To the City Council:

As a resident of NE 36<sup>th</sup>,  
I am very much in favor of the  
proposed traffic diverter.

Our son attended Hollywood  
school last year and the traffic  
was notably less and slower as  
a result of the barrier. It went  
a long way in reassuring his  
safety.

Both my husband and self  
would be very happy to answer  
further questions. Please feel  
free to write again or phone  
us. Our number is 288-7719.

Thank you,

Cathy Christen  
2124 N.E. 36

*Forced*



4283  
Nov. 26, 1980

R/E

Installing permanent divider (closure)  
to reduce traffic volume and speed  
at N.E. 36<sup>th</sup> + Bragge.

City Council -

I endorse the installation of a  
permanent divider at N.E. 36<sup>th</sup> + Bragge.  
The danger to elementary students attending  
Hollywood Grade School is very high.

Students attending Grant use N.E. 36<sup>th</sup> as a  
short road for their autos and cycles. Being  
more conscious of who is checking out their  
cars than of checking out the speed laws  
and watching for pedestrians. I have one  
child attending Hollywood and ~~am~~ also a  
DayCare Provider caring for other children that  
attend this school. We will greatly appreciate  
this divider as a show of concern for  
our children's lives.

Favor

Thank-you -  
Mrs. John E. Mitchell  
2834 N.E. 37<sup>th</sup> Ave.  
Port. Reg. 97212



4283  
RECEIVED

DEC 1 8 18 AM 1980

GEORGE J. KELLY, JR., AUDITOR  
CITY OF PORTLAND, ORE.

BY 

November 26, 1980

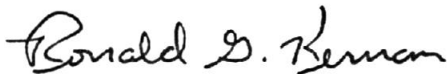
City Council  
Room 202, City Hall  
Portland, Ore.  
97204

Dear Council Members,

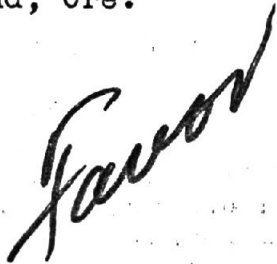
I support the installation of a traffic diverter at NE 36th and Brazee. My request is that the Council consider a diverter design which will allow bicycle traffic to continue using NE 36th. In a time when non-motorized travel is being encouraged, installing barriers without considering bicycles might set a poor precedent.

Thank you for your consideration.

Sincerely,



Ronald G. Kernan  
2343 NE 38th  
Portland, Ore.  
97212





NOV 25 5 22 AM 1980  
REC'D C.T.V.  
BY \_\_\_\_\_  
AUDITOR  
ORE.

4283

11-23-80

Dear Sir,

I am a resident on the corner of NE 36<sup>th</sup> and Knott. Since I moved into this neighborhood last February, I have had to endure the sound of squealing tires and roaring engines as juvenile drivers turn onto Knott street from 36<sup>th</sup>. Twice these cars have gone out of control and have run onto my parkway - once knocking over a neighbor's tree and barely missing his car, and once knocking down a street sign implanted in my lawn. I am tired of the noise generated by these irresponsible drivers, I am tired of the deep tire ruts in my lawn and I am fearful that someday I'll be moving my parkway and like the tree and street sign, will be struck down. The speed limit is never observed in this area, hopefully a traffic divider near Grant High School will eliminate some of the above stated problems. I wholeheartedly approve of its construction.

Ford

Sincerely,  
Carolyn E. Ryan  
3585 NE Knott



4283

RECEIVED

DEC 4 1 18 PM 1980

GEORGE YERGOVICH, AUDITOR  
CITY OF PORTLAND, ORE.

2550 N. E. 36th Avenue  
Portland, OR 97212  
December 3, 1980

BY ec

City Council  
Room 202  
City Hall  
Portland, OR 97204

Dear Council:

Since my wife and I will be at work at the time the Council has set to consider the report of the Traffic Engineer recommending installation of a traffic diverter at the intersection of N.E. 36th and Brazee, we wish to express our opinion to the Council in writing.

It is our opinion that the 3-month test installation of last year, far from being an inconvenience, was eminently successful in demonstrating that a safe environment can and should be provided for the welfare of the children who attend Hollyrood School. We therefore wish to cast our votes in favor of the Traffic Engineer's recommendation and to urge that this diverter be installed as proposed as soon as possible.

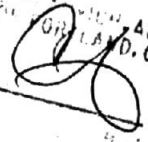
Very truly yours,

*David M. Larson*

David M. Larson

*Faced*



4283  
RECEIVED  
DEC 4 9 27 AM 1980  
GEORGE J. STEELE, AUDITOR  
CITY OF PORTLAND, ORE.  
BY 

2115 N. E. 38th Avenue  
Portland, Oregon 97212  
December 2, 1980

Portland City Council  
Room 202, City Hall  
Portland, Oregon 97204

Dear "Whoever":

In regard to the Traffic Engineer's recommendation to install a traffic diverter at the intersection of N. E. 36th and Brazee Street, I should like to register a protest for this action to be taken.

Installation of this traffic diverter would not only be an inconvenience to the residents of the area, but would create a real hazard for police, fire, or other emergency vehicles in a time of crisis.

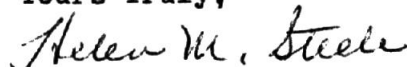
It is obvious that this action is being proposed because of the adverse behavior of the many Grant High students who drive cars to school, but it doesn't seem fair to punish the public as a whole because of the infractions of a few.

Wouldn't it make more sense to have student patrols to monitor the intersection during school hours, and institute some sort of punishment system (like not being able to drive cars to school) for those who persist in driving carelessly? Even a "bump" across the street at the intersection would be preferable to the proposed traffic diverter.

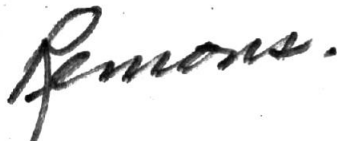
I have lived in this neighborhood for 44 years, and I am very much opposed to having any more restrictions imposed on traffic. The idiotic traffic obstacles on N. E. U.S. Grant Place are enough to have to live with all the time. It is amazing how the Grant Park Neighborhood Association has been able to get these inconveniences inflicted upon us.

Thanks for listening.

Yours Truly,



Helen M. Steele (Mrs. Walter H.)





RECEIVED

DEC 4 1 51 PM 1980

GEORGE J. H. AUDITOR  
CITY OF PORTLAND, ORE.

4283  
3581 N.E. Hollywood Ct.  
Portland, Oregon 97212  
December 4, 1980

BY *J.C.*  
Dear Council members:

We want to express our support for the proposed barricade on N.E. 36th Avenue at Braze.

We live on 36<sup>th</sup> at Hollywood Court, one short block north of Braze. We see and hear excessive speeding and noise from cars traveling 36th Avenue at all hours. During the hours of heaviest traffic — before school, at lunch time, and after school — the very young students of Hollywood School (grades K through 3) are making their way across and along 36th. Their presence does not appear to inhibit the 4-wheeled squirrels at all.

One day last winter, sometime between noon and 2:30 p.m. on a school day, a car drove over the corner curb at 36th and Hollywood, drove down the sidewalk and returned to 36th by driving across our boulevard strip, leaving 2-inch deep ruts in our lawn. The driver obviously used some speed to accomplish that. It is only good fortune that no youngsters were in the way of that car.

We have observed cars parked 4 abreast, completely blocking 36<sup>th</sup>, and then peeling off one by one down 36<sup>th</sup> toward Brant High School. We have seen cars drive up onto the sidewalk on 36th and travel for half a block or so before returning to the street.

During the late summer and early fall of this year we were obliged to call the Portland Police Department at least once each week because of raucous gatherings late at night in the northeast area of Brant Park and around the back of Hollywood School. A year ago, when the temporary barricade was in place,



we had no occasion for such complaints or concerns for the community welfare. Also, the number of out-of-state cars cruising 36th between Knott Street and the high school was drastically reduced while the barricade was up.

Not until our neighbor's husband was killed at the intersection of 36th and Knott was action taken by the City to reduce the speed of traffic on Knott street. We ask that you not again permit death to be your criterion for change.

Sincerely,  
James and Beranna Fleming

3581 N.E. Hollyrood Ct.  
Portland, Oregon 97212  
282-3182

*Fleming*



October 1, 1980

INTEROFFICE MEMORANDUM

TO: Dennis Toles  
Office of City Engineers

FROM: M. J. Martini  
Bureau of Traffic Engineering

SUBJECT: N.E. 16th and Brazee Diverter

We are enclosing copies of the reports from the agencies involved in the N.E. 36th and Brazee diverter. As you can see, the Police Bureau has no objections and the Park Bureau will need to be contacted again after the design is further along.

The Fire Bureau mentions some points with which we disagree. They want a 12' passage through the diverter which is more than they have on many city streets. We do not consider this as a fire lane, just an emergency passageway and therefore feel that 10' is sufficient for this purpose. They also don't want any obstructions in this area but without some form of flexible or breakaway post, the public can't be kept from crossing the diverter and its purpose is defeated. In addition, they also mention moving a fire hydrant. This cost should be included in your construction estimate.

MJM:TV:mc



*Handy*

**OFFICE of FIRE MARSHAL**

*Boyle*

**FIRE PREVENTION DIVISION  
CITY OF PORTLAND**

**From:** Kenneth Owens, Chief Fire Marshal, 130/304      **Date:** 9/29/80

**To:** M. J. Martini, Bureau of Traffic Engineering, 133/301

**Subject:** Your Memo dated September 9, 1980, Concerning Proposal to Install Traffic Diverter at NE 36th Avenue and Brazee Street.

The sketch you enclosed with your memo did not indicate a scale, but it appears to be 1" - 10'. If that is so, then the indicated fire lane is too narrow. Fire code requires a minimum of 12 feet for a fire lane.

None are shown, but there should be no obstructions such as posts, chains, etc., obstructing emergency vehicle access to the fire lane.

The hydrant as shown in its present location would be unsuitably located for Fire Bureau use. We recommend that it be moved to the corner of N. E. 36th and Brazee.

We are in general opposed to traffic diverters. They are restrictions to normal flow of traffic and, as such, have the potential for hampering or delaying fire suppression activities. The neighborhood association should be apprised of that fact.

*Gerald E. Edwards*  
By Gerald E. Edwards  
Gerald E. Edwards  
Staff Captain

GEE/lh

**RECEIVED**

SEP 30 1980

**BUREAU OF  
TRAFFIC ENGINEERING**





DEPARTMENT OF  
PUBLIC AFFAIRS  
MILDRED A. SCHWAB  
COMMISSIONER

BUREAU OF PARKS AND  
PUBLIC RECREATION

DOUGLAS W. BRIDGES  
SUPERINTENDENT

8437 S.E. DIVISION AVE.  
PORTLAND, OREGON 97206

September 16, 1980

MEMORANDUM

TO: M.J. Martini  
Bureau of Traffic Engineering

FROM: Roland Hall *Roland*  
Bureau of Parks & Recreation

SUBJECT: Traffic Diverter  
N.E. 36th & Brazee Street

The Bureau of Parks' interest in the proposed traffic diverter is primarily with any horticultural plantings within this construction.

The concerns would be with the selection of shrubbery and the method of irrigating such plantings followed by the assignment of maintenance and resulting costs.

Please advise of any further progress in this project.

RH/r

RECEIVED  
SEP 24 1980  
BUREAU OF  
TRAFFIC ENGINEERING





BUREAU OF  
POLICE  
CHARLES JORDAN  
COMMISSIONER

B. R. BAKER  
CHIEF OF POLICE  
222 S.W. PINE  
PORTLAND, OR. 97204

MEMORANDUM

July 8, 1980

TO: M.J. Martini, Traffic Engineering  
FROM: B.R. Baker, Chief of Police  
SUBJECT: Blockage of N.E. 36th Avenue at Brazee

After reviewing the proposal to block N.E. 36th Avenue at Brazee with a curbed planted divider, it is our opinion that it would present no problem. East Precinct and Traffic Division Commanders stated that the temporary closure last year presented no trouble to their officers.

We request that the dead end southbound and the detour northbound be well posted.

*B. R. Baker*  
B. R. BAKER  
Chief of Police

BRB/JWJ/pm

RECEIVED

JUL 14 1980

BUREAU OF  
TRAFFIC ENGINEERING





DEPARTMENT OF  
PUBLIC AFFAIRS  
MILDRED A. SCHWAB  
COMMISSIONER

BUREAU OF PARKS AND  
PUBLIC RECREATION  
DOUGLAS W. BRIDGES  
SUPERINTENDENT

6437 S.E. DIVISION AVE.  
PORTLAND, OREGON 97206

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RH/r

RECEIVED  
SEP 24 1980  
BUREAU OF  
TRAFFIC ENGINEERING





BUREAU OF  
POLICE

CHARLES JORDAN  
COMMISSIONER

B. R. BAKER  
CHIEF OF POLICE

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B. R. BAKER  
Chief of Police

BRB/JWJ/pm

RECEIVED

JUL 14 1980

BUREAU OF  
TRAFFIC ENGINEERING



*Nash*

**OFFICE of FIRE MARSHAL**

*Boyle*

**FIRE PREVENTION DIVISION  
CITY OF PORTLAND**

**From:** Kenneth Owens, Chief Fire Marshal, 130/304      **Date:** 9/29/80

**To:** M. J. Martini, Bureau of Traffic Engineering, 133/301

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*Gerald E. Edwards*  
BY Gerald E. Edwards  
Gerald E. Edwards  
Staff Captain

GEE/lh

**RECEIVED**

SEP 30 1980

BUREAU OF  
TRAFFIC ENGINEERING



URB 3-7

October 29, 1980

Memo to Files

From: T. V. Neely

On Tuesday, Oct. 28, 1980 at 7:30 P.M. I attended a meeting of the Grant Park Neighborhood Association. In addition to the chairman, Bob Packard, there were five citizens and a representative of the Park Bureau.

We discussed the progress of the 36th & Brazee diverter and the possibility of a signalized pedestrian crossing on 33rd and Brazee. There was also some philosophical discussion of the aesthetics of off street parking on Broadway.

MJM:TV



4RB3-7

September 9, 1980

MEMORANDUM

TO: Bob Gustafson, Park Bureau  
Chief Sargent, Fire Bureau  
Chief Baker, Police Bureau

FROM: M. J. Martini, Bureau of Traffic Engineering

This bureau has received a request from the Grant Park Neighborhood Association to install a traffic diverter at N.E. 36th Avenue and Brazee Street. We are enclosing a sketch of the proposal and would appreciate any remarks or suggestions you might have prior to final design.

You will notice that the fire lane is angled, this is to prevent a straight line of sight through the diverter.

We would appreciate your comments by October 1st, and if you have any questions please contact T.V. Neely at 248-4290.

TVN:ah



URB 37

Muddy

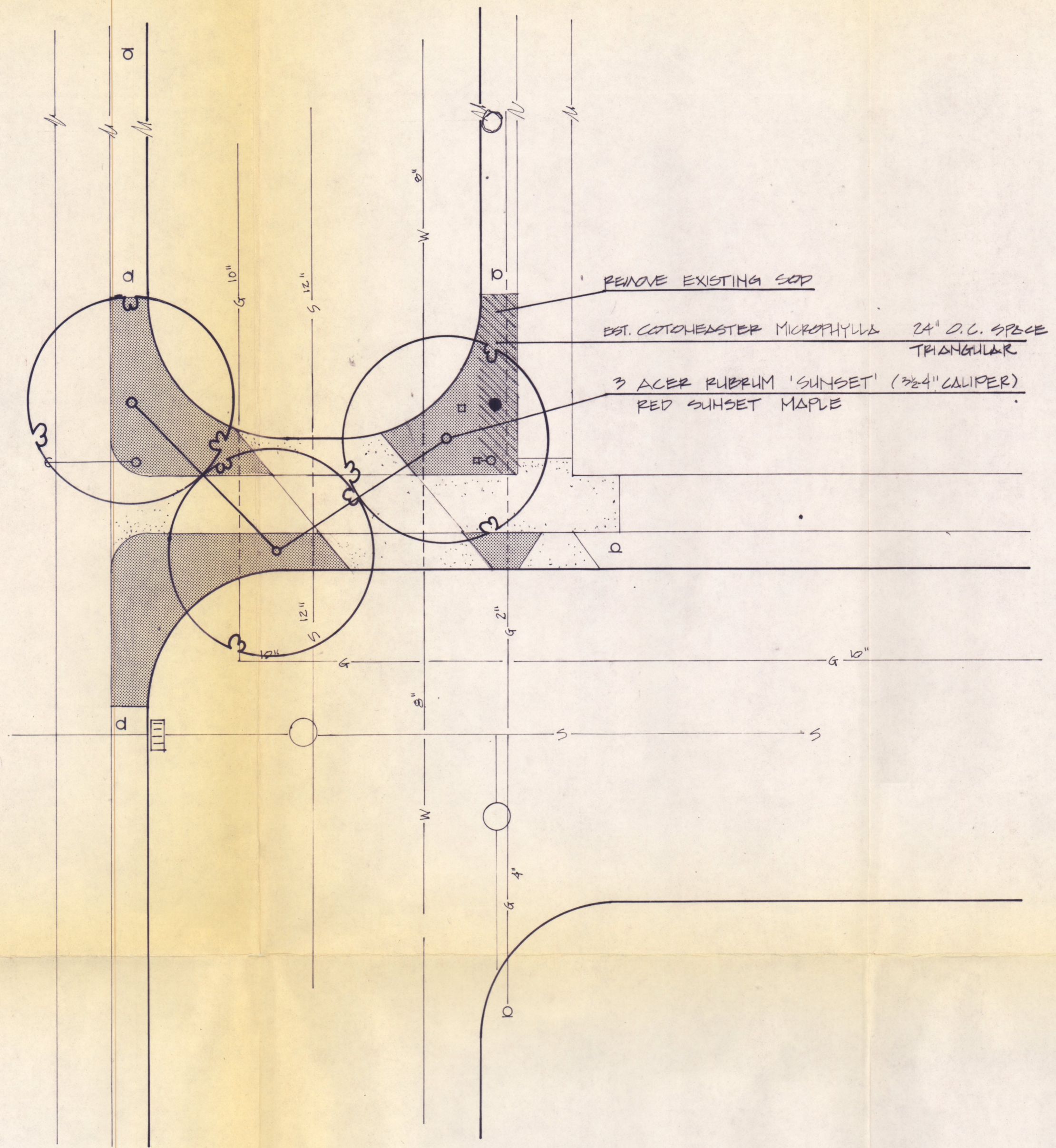
Petition

REQUEST FOR TRAFFIC BARRIER  
N. E. 36th & BRAZEE  
FROM THE  
GRANT PARK NEIGHBORHOOD  
ASSOCIATION







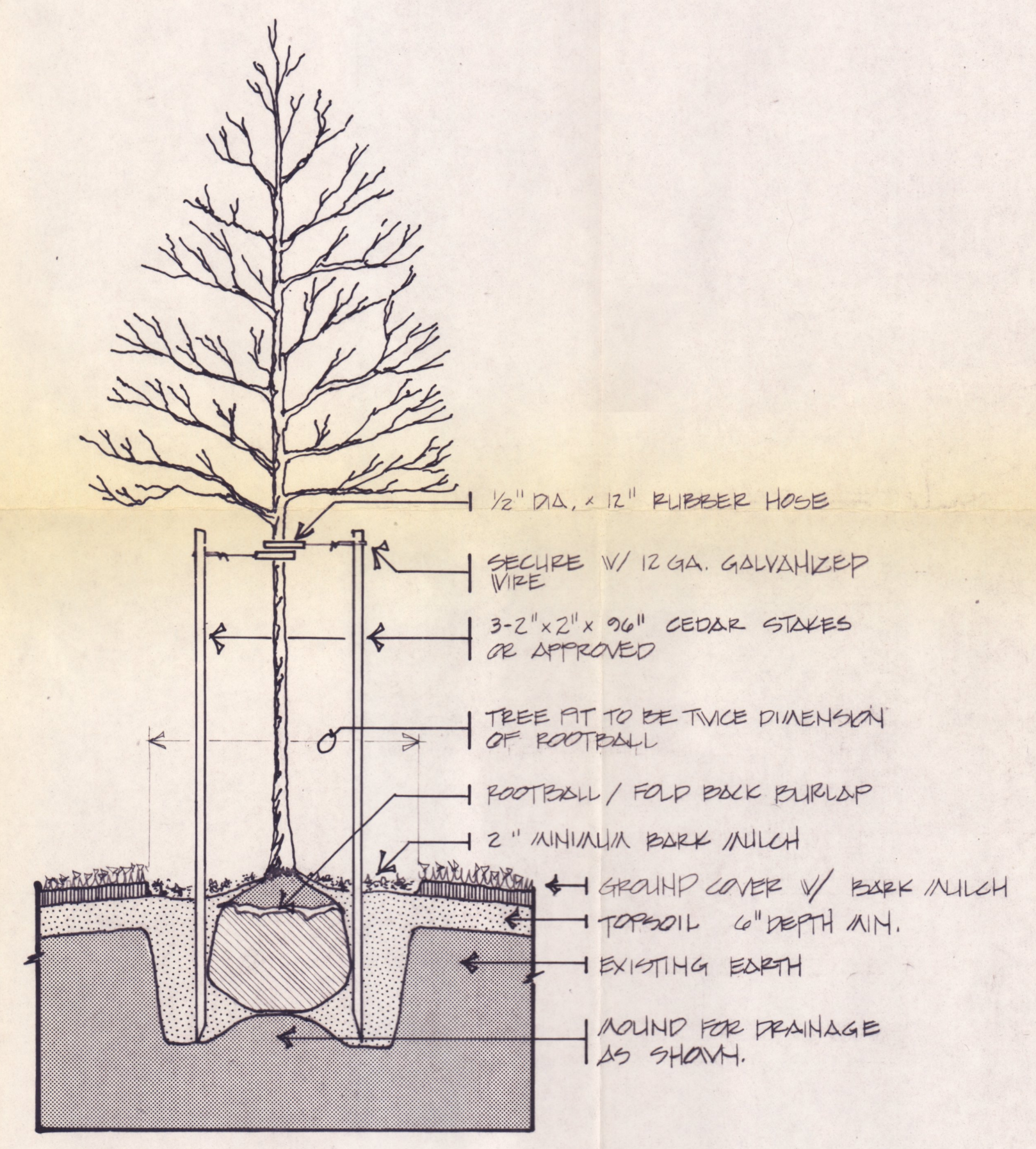


LANDSCAPE PLAN SCALE: 1" = 10'-0"

PLANTING SCHEDULE

QUANTITY	BOTANICAL NAME	SIZE	COMMENTS
2	ACER RUBRUM 'SUNSET' RED SUNSET MAPLE	3/8-4" CALIPER BALLED & BURLAP	BRANCHING @ 6' HT. MIN.
ESTIMATE	COTONEASTER MICROPHYLLA	1 GALLON CAN	24" O.C. TRIANGULAR

- GENERAL NOTES:
- # ALL PLANT MATERIAL SUBSTITUTIONS MUST BE APPROVED BY THE PARK BUREAU REPRESENTATIVE
  - # GROUND COVER AREAS ARE TO RECEIVE A MINIMUM OF TWO (2) INCHES. MEDIUM FINE NON TOXIC BARK MULCH.
  - # ALL GRADES SHALL FLOW SMOOTHLY AND PROVIDE FOR POSITIVE DRAINAGE.
  - # CONTRACTOR SHALL NOTIFY PARK DESIGNER 36 HOURS IN ADVANCE OF PLANT MATERIAL PLACEMENT FOR INSPECTION.



DECIDUOUS TREE PLANTING DETAIL

DESIGNED BY L.A.M. DATE APPROVED DEC. 29, 1953 DRAWN BY L.A. MACDONALD DIV. ENGINEER R. TASHIMA CHECKED BY S.R./N.D.H. RESOLUTION NO. FILE NO. UNDERGROUND R. W. J.			APPROVALS: BUREAU CHIEF ACTING  CITY ENGINEER REG. PROF. ENGR. 8416	CITY OF PORTLAND, OREGON <b>DEPARTMENT OF PUBLIC WORKS</b> MIKE LINDBERG JOHN LANG, P.E. COMMISSIONER CITY ENGINEER		<b>TRAFFIC DIVERTER</b> <b>at INTERSECTION of</b> <b>N.E. BRAZEE ST. and</b> <b>N.E. 36th AVE.</b>	1/4 SECTION 2834 JOB NO. 6093 SHEET NO. 2 OF 2
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NO	DATE	DESCRIPTION	APPD.
		REVISION	FINAL MAP DATA