



DAY TIME - RENDERING VIEW 1



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### ABBREVIATIONS

ADA	AMERICANS WITH DISABILITIES ACT	ETR	EXISTING TO BE REMAIN	P	PAINT
ALUM.	ALUMINUM	F.F.L.	FINISH FLOOR LINE	PNTD	PAINTED
A.B.P.	ALT. BRACED PANEL	F.C.	FIBERGLASS COMPOSITE	P.F.	PORTAL FRAME
A.P.C.	ACOUSTICAL PANEL CEILING	F.G.I	FIBERGLASS INSULATED	P.T.	PRESSURE TREATED
BD.	BOARD	F.D.	FLOOR DRAIN	PLYWD	PLYWOOD
BLDG.	BUILDING	FND.	FOUNDATION	R=	RADIUS
B.O.	BOTTOM OF	F.O.S.	FACE OF STUD	REF	REFRIGERATOR
B.P.	BRACED PANEL	FTG	FOOTING	S	STAIN
BRG.	BEARING	GALV.	GALVANIZED	SC	SOLID CORE
CPT.	CARPET	GYP.	GYPSUM	S.F.	SQUARE FOOT
C.C.	CENTER TO CENTER	GWB	GYPSUM WALL BOARD	SHO.	SHOWER
CD	CONRTOL DEVICE	I.G.	INSULATED GLASS	S.I.	SQUARE INCH
C.J.	CONTROL JOINT	INFO.	INFORMATION	SIM.	SIMILAR
CL	CENTER LINE	INSUL.	INSULATION	SH WL	SHEAR WALL
CLR	CLEAR	HRDWD	HARDWOOD	SHTNG	SHEATHING
CMU	CONCRETE MASONRY UNIT	HORIZ.	HORIZONTAL	SPKLR	SPRINKLER
COL.	COLUMN	H.B.	HOSE BIB	TEMP	TEMPERED
CONC.	CONCRETE	H.C.	HOLLOW CORE	TOI.	TOILET
C.O.W.	CENTER OF WALL	HD	HOLD DOWN	T.O.	TOP OF
CPT	CARPET	HM	HOLLOW METAL	T.O.P.	TOP OF PLATE
CRMIC	CERAMIC	LAM	LAMINATE OR LAMINATED	T.O.B.	TOP OF BEAM
CT	CERAMIC TILE	LAV.	LAVATORY	T.O.W.	TOP OF WALL
CSNK.	COUNTERSINK	MANUF	MANUFACTURER	TL	TILE
DECK'G	DECKING	MAX.	MAXIMUM	TYP.	TYPICAL
D.S.	DOWN SPOUT	MECH.	MECHANICAL	U.O.N.	UNLESS OTHERWISE NOTED
DIA.	DIAMETER	MDF	MEDIUM DENSITY FIBREBOARD	VER.	VERIFY
DW	DISHWASHER	MTL	METAL	V.I.F.	VERIFY IN FIELD
DWG.	DRAWING	MIN.	MINIMUM	VNL	VINYL
EA.	EACH	N.G.L.	NATURAL GRADE LINE	WL	WALL
EL	ELEVATOR	N/A	NONE	WH	WATER HEATER
ELEC.	ELECTRICAL	N.I.C.	NOT IN CONTRACT	WD	WOOD
ELEV.	ELEVATION	NOM.	NOMINAL	W/O	WITHOUT
EQ.	EQUAL	N.T.S.	NOT TO SCALE		
EQUIP.	EQUIPMENT	NO.	NUMBER		
EXIST'G.	EXISTING	O.C.	ON CENTER		

### DRAWING SYMBOLS

	NORTH ARROW
	BUILDING SECTION
	DETAIL SECTION
	DETAIL ELEVATION
	DETAIL PLAN
	INTERIOR ELEVATION
	REVISION
	INDICATE UNIT ENTRY
	ROOM TAG
	WALL, FLOOR, & CEILING TAG
	DOOR TAG
	WINDOW TAG
	KEY NOTES
	COLOR TAG



Development of a 4-story condominium building with five dwelling units in two buildings and parking in ground floor garages.

PROPERTY INFORMATION	
PROPERTY ID:	R182572
COUNTY:	MULTNOMAH
STATE ID:	1N1E26CD 8200
NEIGHBORHOOD:	IRVINGTON
JURISDICTION:	PORTLAND / MULTNOMAH COUNTY
SITE ADDRESS:	1806 NE 12TH AVENUE
OWNER:	MAIN STREET DEVELOPMENT INC. 5331 S MACADAM AVE #258 PMB 208 PORTLAND OR, 97239-3871
ZONING AND DISTRICT	
ZONE:	RM3 - RESIDENTIAL MULTI-DWELLING
OVERLAY:	d - DESIGN
COMP PLAN:	MULTI-DWELLING URBAN CENTER
HISTORIC DISTRICT:	IRVINGTON HISTORIC DISTRICT
NEIGHBORHOOD:	IRVINGTON
LAND INFORMATION	
Residential Land ID:	R182572
Lot area =	5,000 SF 100%
Building Footprint =	2,752 SF 55.40%
Paving =	1,310 SF 26.20%
Landscaping =	938 SF 18.76%
VERTICAL INFORMATION	
Total Floor Area breakdown is noted on Sheet C1.3	
Toal Floor Area proposed = 9,986 SF	
BASEMENT:	Not included in FAR
APPLICATION:	Type III Historic Resource Review 0.040 or project valuation (10,000 x \$150 x 0.04 = \$60,000.00 ) max fee = \$31,257.00 Combined Service Bureau Fee = \$8,737.00 Design Modification = \$1,796.00 (to setback standards) Adjustment Review = \$ 5,141.00 (to base FAR standards and minimum density)
* Fees adjusted after July 1 <sup>st</sup>	

Background  
First a quick summary about the property: The home was built on the site in 1906, and later converted into a duplex, was demolished in 2022. Its historic name was the M E Chambers House, and it was a two-and-a

half-story Foursquare style house. It was considered a dangerous structure and underwent a city ordered demolition under permit 22-200249 RS. Therefore, the site is vacant with the previous home demolished. All files relating to the demolition have been satisfied and closed. The applicant is proposing a 9,988 SF development, and therefore will go through a Type III Historic Resource Review.

The current zoning is RM3d, Residential Multi-Dwelling 3 base zone (33.120 Multi-Dwelling Zone); with both an Historic Resource Protection Overlay Zone (33.445); and a Design Overlay Zone (33.420). There is no Plan District or Pedestrian District covering this parcel.

The property has frontage on two Streets. The front lot line which is the west property is adjacent to NE 12th Street Ave, which is classified as a local service bicycle, pedestrian, transit, traffic, and design street.

The south property line and the street side-yard is Schuyler Ave, which is classified as a local service bicycle, pedestrian, transit, traffic, and design street. The property, while not considered a Civic or Neighborhood Corridor is close to transit.

Most importantly it is within the Irvington Neighborhood Plan. While the demolished structure was a contributing resource in the Irvington Historic District demolition was required by the Hearings Officer per Case 20-166470 DB. The site is no longer considered a contributing resource.

As part of the initial feasibility study the applicant reviewed the Guide to the Historic Resource Review Process prepared by the Historic Landmarks Commission, which provides guidance and expectations of the Historic Resource Review process.

The Approval of a Historic Resource Review allows for the proposed work to be built. As part of the process the applicant understands that a Certificate of Compliance will be required at the time of building permit

#### Introduction

Eric Rystadt is a local builder, who specializes in upmarket homes within the City of Portland. However, the severe headwinds now facing builders generally, and much more specifically in Portland require that homes be designed that meet current market demand. For homes in this location specific features would be baseline need. For example: a two car garage; A certain square footage; and a central location. The interior features and internal design tend to take precedent in the ability to sell a house. The task specific to this site is to marry the requirements and demands of the market, that also reflects the RM3 zoning while creating a façade that works within the Irvington Neighborhood. To that end the applicant has drawn from the history of the Irvington Historic Neighborhood.

The Irvington Community Association has provided a sound history of the neighborhood drawing from various sources, and providing a general chronological description of the neighborhood, its architects, and the many examples of homes built during its history.

The Irvington neighborhood generally bounded by Northeast 7th, Fremont, Northeast 27th, and Broadway—is primarily a residential district, characterized by Queen Anne, Period Revival Bungalow/Craftsman, and Prairie School architectural styles largely built from the 1890s through

There is a history of builders developing property in a manner that protected their value. It was understood that builders needed to protect their investment, and design homes to fit current demand.

*"In the face of largely unregulated urban development occurring throughout the United States, real estate investors and developers resorted to limited duration covenants in an effort to protect their investment until sales of lots were completed.*

*These restrictions were intended to ensure a degree of conformity to the residential architectural forms, property use, and socio-economic status of the neighborhood. This use of covenants marked a significant event in the emergence of the city's streetcar suburbs that would eventually be replicated elsewhere including Alameda Park (1909) and Laurelhurst (1909). In Irvington, these covenants were set to expire (and did) in 1916.*

*The early real estate investors of Irvington managed the risks of their real estate venture by gradually releasing parcels within the neighborhood to individual property owners or to small scale speculators who often constructed homes on individual lots rather than entire blocks."*

The text notes that the nearly half of the homes were built in the 20s

*"Of the 2,804 historic resources located within the district, only about 53 predate 1900. Between 1900 and 1910, at least 405 of the 2,804 buildings that currently exist in Irvington were constructed. The second building boom occurred in the 1920s when 1238 of the existing 2,804 buildings were erected in Irvington. By 1930, the population of Portland had expanded to nearly 300,000"*

There is also mention of the introduction of multi-family / duplex / and other non single family homes being constructed

*"Other subtle expressions of architectural illusion emerged throughout the 1910s and 1920s as non-single-family-residential buildings throughout the neighborhood were designed to look like homes."*

The construction of multifamily homes along the periphery of the neighborhood reflects its history, and lines up with the underlying zoning.

*Frederic Bowman designed the Irvington Bowman Apartments at 1825 and 1835 NE 16th in the Arts and Crafts style and the well-appointed F.E. Bowman Flats at 1624-1636 NE Tillamook. Both were conveniently close to the new streetcar lines, but on the outskirts of the Irvington Neighborhood. Several large homes near the streetcar lines in the neighborhood were actually duplexes or four-plexes consciously designed with hidden or ancillary entrances that obscured their multi-family function.*

*The pressure to build in an appropriate style during this period, therefore, came as early as the 1910s, as Irvington residents expressed the importance of residential use and architectural design.*

While architectural influence is key to understanding the history of the neighborhood it is important to note that is was developers, build design contractors, a speculators that led the design of the buildings, and hired the architects.

*Due to the large numbers of buildings erected during the building booms of the 1910s and 1920s, Irvington contains the largest, most intact collection of early twentieth century residential architecture found in Oregon. Influenced by a profound group of real estate developers, speculators, build-design contractors, as well as architects, the neighborhood exhibits an eclectic collection of early twentieth century architectural styles.*

There was no one set style though the Arts and Crafts movement seemed to be the most dominant.

*"The residences, apartment buildings, and commercial buildings erected by the architects and builders in Irvington during this period reflected the extensive influence of the Arts and Crafts movement. A reaction to the rapid industrialization of the modern world and the desire for machine-manufactured goods, the Arts and Crafts Movement centered on the inherent virtuousness of handicrafts. Beginning in Europe, the movement soon found devotees in Oregon.*

*Architects, builders, and architectural promoters in Irvington (as elsewhere in Portland) also found inspiration in the Shingle style, Spanish Mission Style, Mediterranean, Bungalow, Swiss Chalet, and Japanese architecture in addition to the Arts and Crafts Movement. The Prairie Style, largely inspired by the work of Frank Lloyd Wright, was yet another permutation of the period which sought to express simplicity, natural materials, and economy."*

The proposed build is drawn largely from the Arts and Crafts movements. The proposed development is speculative, and being led by a well-established design build contractor Eric Rystadt in partnership with a local firmly established architect Ralph Tehran.

Based upon local knowledge, years of expertise, Ralph thought it best to draw in part from the work of the arts and crafts movement. Perhaps this style is best exemplified by Ellis Lawrence given his standing within the Irvington District. Amongst his most famous buildings is the Westminster Presbyterian Church built in 1914 that reflects a size and mass common to larger multi-story structures. For example, the steep roofline with a fourth story incorporated into the roof space. The generally flat facades, and banks of windows. The use of stone / masonry to project a sense of permanence, and strength.

The Society's text in the area's history Describes Lawrence's work as follows.

*The eclectic mix of architecture in Irvington epitomizes the "harmony in diversity" that author Michael Shellenberger used to describe the work of Ellis Lawrence. Lawrence was known for his creative design flair that intermingled stylistic components with "an unerring good eye for composition and proportion." Indeed, his commissions featured the "unexpected juxtaposition of different styles and shapes, mixing traditional details with modern, and formal exteriors with informal exteriors or vice versa." Lawrence would go to form the University of Oregon's School of Architecture.*



The narrative continues:

*Setting the architectural standard in Irvington, Lawrence designed his own residence at 2201 NE 21st Avenue, a duplex, along a major road that served as a highly visible advertisement for his work. The house has been credited as being one of the earliest examples of the Arts and Crafts style in Oregon when it was erected in 1908.*

There are 22 steps and stair from the street to the front door of the Lawrence Duplex. The home rises a further three stories, with a roof peaking roughly 6-feet above that. In all the duplex commands the neighborhood rising to roughly 50-feet above the sidewalk.

Each of the duplexes is about 2800 SF in size.

The historic duplex is described as having an *asymmetrical* Fenestration. This is reflected in the arrangement, proportioning, and design of windows and doors in a building

The duplex also shows complex roof forms. Again, meant to display the craftsmanship of the architectural design in start contract to the neoclassical, or more traditional roof designs

The building is described as having a creative composition. This is not only reflected in the layout of the façade, but also in the material types

The two duplex buildings being developed at the northeast corner of Schuyler and 12th represent a similar standard of design. This begins with the use of a concrete / or masonry base. The construction of a generally flat façade. For the proposed buildings the roof design reflects the steeply pitched gable roofs, as well as the pitched cross gables on façade

Banks of multi-light sash windows are a significant feature on all facades.

#### THE PROPOSAL

The proposed development is zoned RM3 and is located in the Irvington Historic District. This project is therefore subject to 33.120 Multi-Dwelling Zone RM3d, Residential Multi-Dwelling 3 base zone. In addition it has an Historic Resource Overlay, and is therefore subject to an historic resource overlay review. Under 33.846.060, because the new structure in a historic district has a floor area that is greater than 5,000 SF, the project would be a Type III Historic Resource Review (HRR)

Under 33.420.030 for proposals with Design Overlay zoning, depicted with a ‘d’ on the official Zoning Maps, that will add at least 10,000 square feet of net building area to a site, the neighborhood contact steps of 33.705.020.C., Neighborhood Contact III, are required. However for applications submitted after January of this year the Neighborhood contact requirements have been reduced to a type I process. This is the result of changes in City code to help facilitate new housing in response to the current housing crisis.

Under the HRR The applicable approval criteria are 33.846.060.G. This is because the parcel is a non-contributing site. The former structure on the site was a contributing resource in the Irvington Historic District, but is now demolished, following a City order to demolish due to unsafe conditions. In Historic Districts, criteria #1-#5 do not apply to non-contributing resources. Criteria #6-#10 do apply to non-contributing resources. This is because the language in the criteria set was updated to be more specific about what the “resource” is and #1#5 speak to “historic” materials, features, and character. Non-contributing resources, by definition, have no historic materials, historic features, or historic character. Criteria #6-#10 speak more broadly about the compatibility with the district and the potential for unearthing significant resources.

Section 33.846.060.G is addressed later in this narrative.

In addition to addressing the zoning code and the HRR the applicant is requesting two other additional reviews: Under 33.805.040 an adjustment to the Floor Area Ratio; and under 33.846.070 a modification to the front yard setbacks along both 12th and Schuyler.

The development standards that will apply to the project are:

- 33.245 Inclusionary Housing - applies to new development with 20 or more dwelling units.
- 33.248 Landscaping and Screening
- 33.258 Nonconforming Upgrades (*The applicant does not believe this section is applicable*).
- 33.420 Design Overlay Zone
- 33.445 Historic Resource Protection Overlay Zone, Irvington Historic District
- 33.266 Parking and Loading
- 33.805.040 an adjustment to the Floor Area Ratio
- 33.846 Historic Resource Review

- Including 33.846.070 a modification to the front yard setbacks along both 12th and Schuyler.

Title 32 Sign Code – signs of any size within the Historic Resource overlay zone require a Historic Resource Review. (*No signs are proposed this section is not applicable*)

Title 11 Tree Code – Exemptions for tree density and preservation of this Title only apply to private trees on properties zoned IH, on sites with more than 85% building coverage, and sites less than 5,000 SF in area.

#### INTRODUCTION

Following an initial meeting with the Irvington Community Association Land Use Committee ICA LUC) the applicant took the changes to the design suggested by the association, and divided the initial concept into two smaller separate buildings. The parking was relocated away from the street facades, and faced to a central courtyard. In June the developer went back to the Committee having made these substantive changes.

The applicant went through the updated design and noted that in part to compensate for the loss of building area on the site created by the need for onsite maneuvering, the applicant was having to push the design outwards towards the street reducing the front yard setbacks on both streets. The committee noted that the “HLC have the ability to make setback adjustments”.

One consideration suggested by the Committee was to *‘Consider repurposing the connecting deck into enclosed living space’ and ‘potentially make the connecting deck roof compatible with the building roof i.e. ridged rather than curved’*

While it was noted in their comments that the applicant *“stated that they would be submitting the design presented this evening for review and approval rather than requesting another DAR. That none of the stated concerns raised and proposed suggestions made by the LUC would be acted upon, if ever, until after BDS submission, review and comments are made”.*

This statement does not reflect applicants comments which were that the applicant felt that City staff should be involved in future comments and review, and that any further changes, or modifications should be done with staffs involvement. The applicant made very substantive changes in the design from the first several designs to take into consideration the Boards views. There is perhaps a suggestion that after the changes are made the new design, it then becomes a new starting point to request further changes, and so on. The

applicant is always open to outside opinions, but believes the process must start through the formal application at this point.

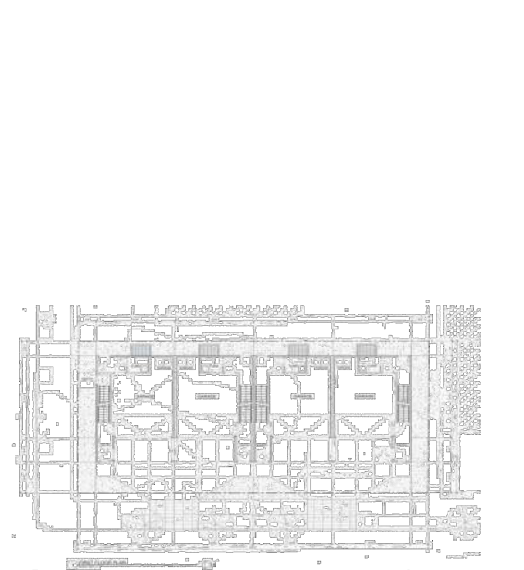
#### Initial submittal Design.

The initially submitted design needed multiple driveway design exceptions. There are driveways on two frontages as well as the driveways on the same frontage that are less than 100-ft apart (TRN 10.40.D.5, and TRN 10.40.D.2).



#### Revised Design

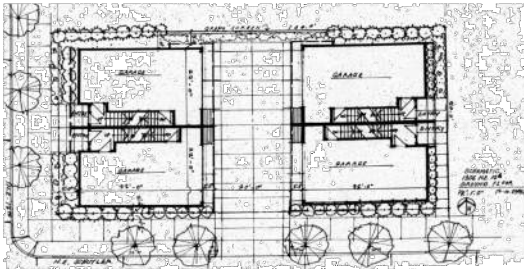
The second design reduced the number of driveways, also removing any driveway from the 12th Avenue frontage, but the most significant issue remained the dominance of garages, and driveways on the ground floor.



#### Design Revised following meetings with Neighborhood Committee, and Landmarks Commission

The applicant made a significant change

- 1) removing all garage doors from any street frontage
  - 2) Combining all access to a single central drive court
  - 3) Divided the fourplex into two separate buildings with a much smaller foot print, and façade massing
- Staff suggested that if two buildings were proposed that the applicant should explore bridging the driveway opening to create a a hidden vehicle courtyard.





33.120 Multi-Dwelling Zone RM3d, Residential Multi-Dwelling 3 base zone with Design and Historic Resource Overlays

Maximum Floor Area Ratio (33.120.210 and Table 33.120-3).

The maximum floor area ratio (FAR) for this site is 2:1. Floor area is measured to the exterior faces of a building. Floor area does not include areas where the floor elevation is 4 feet or more below the elevation of an adjacent right of way, basements, and areas where the ceiling height is less than 6 feet 8 inches. Floor area does not include porches or balconies unless they are enclosed by walls more than 42 inches in height for more than 75 percent of their perimeter.

Floor Area Bonus Option (33.120.211). More than one bonus may be used up to the overall maximum per site stated in Table 120-5. Adjustments to the maximum amount of floor area obtainable through bonuses are prohibited.

In RM3, maximum FAR with other bonuses is 3:1. Other bonuses are; Inclusionary Housing, Three-Bedroom Units, and Visitable Units bonuses; Inclusionary Housing bonus: 1:1 ;Three-Bedroom Units bonus: .5:1 and a visitable Units bonus: .5:1

The proposed floor area meets the FAR of 9,988 sf and is under the allowable area of 10,000 sf.

33.120.210 D Transfer of FAR the sending property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to send floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

For example the building along our east property line (1223 NE Schuyler ) is a contributing building. The total building area for 1223SF is 4,472 SF leaving 5528 SF of FAR remaining on the table. Per Table 120-5, and under 33.120.210 B the maximum FAR with bonus is 3:1 within a conservation district. With the transfer we would be 2.45:1

The Receiving site. The transfer must be to a site zoned RM1, RM2, RM3, RM4, RX, CM1, CM2, CM3, or CE outside of the Central City plan district. Transferring to a site zoned RMP is prohibited. Transferring to a site where a Historic or Conservation Landmark or a contributing structure in a Historic or Conservation District has been demolished within the past ten years is prohibited unless the landmark or contributing structure was destroyed by fire or other causes beyond the control of the owner, the only structure on the site that was demolished was an accessory structure, or the demolition was approved through demolition review.

This process is not a modification and is allowed outright providing we meet the applicable standards.

Alternatively: it also appears that if we save a 12" diameter tree on another site that per Table 120-4 in the RM3 zone we can transfer up to 2,000 SF to the site. Again this would be up to the 3:1 cap.

33.120.210 E Transfer of FAR the receiving property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to receive floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

Minimum Density (33.120.213 and Table 120-3).

Comment: The minimum density required for this site is 1 unit per 1,000 SF of site area. The site contains 5,000 SF and is therefore required to have a minimum density of 5 units.

The applicant is requesting an Adjustment to reduce the total number of units on site from 5 to 4 and 1 ADU.

The applicant believes this is supportable in order to ensure compatibility of scale with the historic district.

The need to break down the massing of the structure from one larger building into to mush smaller structures is in line with scale, and massing of the area. To provide them with a similar design each building is being proposed with two homes. While important to meet density there is a competing want of not trying to more closely align with the neighborhood.

33.120.210 F Transfer of FAR the receiving property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to receive floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

Height (33.120.215 and Table 120-3).

The height limit in this zone is 65 feet. Height Measurement method depends on the location of a building in relation to a street sidewalk. See the Measurements Chapter of the Zoning Code: 33.930 Measurements.

Comment: The proposed building is 50-feet in height to the peak of the highest roofline

33.120.210 G Transfer of FAR the receiving property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to receive floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

Minimum Building Setbacks (33.120.220.B and Table 120-3).

The minimum front setback is 10'. In the RM3 zone, buildings that are up to 50' tall have a minimum side and rear building setback of 5'.

Comment: The applicant is requesting an adjustment to the street side-yard from 5-feet to 2-feet, and the front yard setback being reduced from 10-feet to 5-feet

33.120.220 C Transfer of FAR the receiving property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to receive floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

33.805.040 Adjustments

A. Granting the adjustment will equally or better meet the purpose of the regulation to be modified; and

Comment: In order to make findings that the adjustment equally or better meets the purpose of the regulation(s), the applicant must first identify all relevant portions of the purpose statement must be addressed.

33.120.220 Setbacks

A. Purpose. The building setback regulations serve several purposes:

- They maintain light, air, separation for fire protection, and access for firefighting;

Comment: The adjustment to the front yard setbacks along 12th and Schuyler has no impact on the Citys ability to fight fires, nor does it decrease air circulation to adjacent structures. There is no change to the side yard setbacks next to adjacent properties

- They reflect the general building scale and placement of multi-dwelling development in the City's neighborhoods;

Comment: The proposed deployment is of similar mass, and lot coverage to other existing multi dwelling developments in the area. The adjustment request is a reflection on the need to not develop 30% of the lots

building area which is being used for onsite vehicular maneuvering . It is being requested in part to make up the buildable area at the center of the property.

- They promote a reasonable physical relationship between residences;

Comment: In

- They promote options for privacy for building residents and neighboring properties;

Comment: The adjustment makes the proposed unit 1 some 75-feet distant as opposed to 80-feet away from the home across the street. There is no impact to the homes directly east and north of the property.

- They provide adequate flexibility to site a building so that it may be compatible with the neighborhood, fit the topography of the site, allow for required outdoor areas, and allow for architectural diversity;

Comment: The proposed adjustment has freed up space in the center of the parcel which has allowed the creation of semi-private open spaces for all four units. This space has also

- Setback requirements along transit streets create an environment that is inviting to pedestrians and transit users; and

Comment: In

- They provide room for a car to park in front of a garage door without overhanging the street or sidewalk, and they enhance driver visibility when backing onto the street.

Comment: The proposed design has an interior courtyard to provide maneuvering for each of the units vehicles. These are not impacted by the reduction in the front yard setback.

B. If in a residential zone, the proposal will not significantly detract from the livability or appearance of the residential area, or if in an OS, C, E, I, or CI zone, the proposal will be consistent with the classifications of the adjacent streets and the desired character of the area; and

Comment: The adjustment is in a residential zone. The property has an historic overlay. The factors involved with the overall design massing, scale, setbacks, materials are all interrelated, and are discussed at length in other portions of this narrative. The livability, and appearance of this project have been detailed, and referenced to other contributing residential buildings in the immediate vicinity. The applicant is directing staffs attention to these elements rather than cutting and pasting these issue here again.

C. If more than one adjustment is being requested, the cumulative effect of the adjustments results in a project which is still

Comment: The applicant is requesting a single adjustment.

D. City-designated scenic resources and historic resources are preserved; and

Comment: This parcel is within the boundaries of a Historic or Conservation district. The design of the units is not dissimilar to like sized multifamily units close to the project. The adjustment is towards the street, not dissimilar to existing contributing structures. Approving this adjustment does not prevent the preservation of adjacent contributing structures.

E. Any impacts resulting from the adjustment are mitigated to the extent practical; and

Comment: The applicant is proposing a vertical L1 vegetative standard between the building, and the property line to offset the encroachment into the front yard setback

33.120.220 D Transfer of FAR the sending property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to send floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

F. If in an environmental zone, the proposal has as few significant detrimental environmental impacts on the resource and resource values as is practicable

Comment: This property does not have an environmental overlay

33.120.220 E Transfer of FAR the receiving property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to receive floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

Building Coverage (33.120.225 and Table 120-3).

The maximum building coverage for this site is a maximum of 85% of the site area.

Comment: Sheet C1.1 shows that:  
Lot area = 5,000 SF 100%

Building Footprint = 2,752 SF 55.40%

Paving = 1,310 SF 26.20%

Landscaping = 938 SF 18.76%

Façade Articulation (33.120.230.C).

In the RM3 zone the standard applies to buildings more than 45 feet high that have façade areas of more than 4,500 square feet within 20 feet of a street property line. Portions of buildings that are vertically separated by a gap of a least 10 feet in width extending at least 30 feet in depth from the street property line are considered separate façades.

Comment: The building is 50 ft. to the peak of the highest roof. Sheet C1.5 shows that the combined street facing façades of both buildings = 4,344 SF

The standard for façade articulation is at least 25% of the area of a street-facing façade within 20 feet of a street lot line must be divided into façade planes that are off-set by a least 2 feet in depth from the rest of the façade.

Comment: The proposed elevations show a number of articulations in both the west and southern façades with a total area of greater than 25%

Main Entrances (33.120.231).

The standards apply to all residential structure types. For multi-dwelling development, the standards only apply to residential structures that are located within 40 feet from a street lot line.

At least one main entrance from each structure must be within 8 feet of the longest street facing wall and either face the street, be at a 45-degree angle to the street, or open onto a porch. Multi-dwelling structures may meet the requirement with a courtyard facing entrance that is located within 60 feet of the street and the courtyard meets the following standards: the courtyard must be at least 15 feet wide, the courtyard must abut a street, and be either landscaped to the L1 standard or hard-surfaced for use by pedestrians.

Comment: Building 1 has two front doors directly facing NW 12th Avenue. The doors are setback 4-feet from the main façade. Building 2 has one front door facing NW Schuyler, and one facing the east property line.

Street Facing Facades (33.120.232)

–Windows: At least 15% of each façade that faces a street lot line must be windows or main entrance doors.

Comment: Sheet C1.5 identifies that:

SOUTH ELEVATION	WALL AREA	SF	WINDOW AREA	SF	PERCENTAGE
BUILDING 1	1,282.00	SF	269.00	SF	20.98%
BUILDING 2	1,286.00	SF	269.00	SF	20.98%
TOTAL	2,567.00	SF	538.00	SF	20.98%
WEST ELEVATION	WALL AREA	SF	WINDOW AREA	SF	PERCENTAGE
BUILDING 1	1,777.00	SF	269.00	SF	15.14%

Landscaped Areas (33.120.235 and Table 120-3).

This site requires that 15% of the site area be landscaped. Required landscaped areas must be at ground level and comply with at least the L1 standard as stated in Chapter 33.248, Landscaping and Screening (see below for more detail). The landscaped area must be at ground level or in raised planters that are used to meet minimum BES stormwater requirements However, up to one-third of the required landscaped area may be improved for active or passive recreational use, or for use by pedestrians. Examples include walkways, play areas, plazas, picnic areas, and unenclosed recreational facilities. Any required landscaping, such as for required setbacks or parking lots, applies towards the landscaped area standard.

Comment: The total landscaped area = 938 SF or 18.76%

Required Outdoor Areas. 36 square feet of outdoor area is required per unit. Where individual outdoor areas are provided, they must be designed so that a 4-foot by 6-foot square will fit entirely within it and the area must be directly accessible to the unit.

Comment: Each unit has a deck, that is 8ft wide and 11ft deep for a total square footage of 88 sf and for the ADU is 36.04 sf (see C2.1)

Screening (33.120.250)

Garbage and recycling collection areas. All exterior garbage cans, garbage collection areas, and recycling collection areas must be screened from the street and any adjacent properties. Trash receptacles for pedestrian use are exempt. Screening must comply with at least the L3 or F2 standards of Chapter 33.248, Landscaping and Screening (see below).

Comment: The applicant is proposing that each of the four units retain their garbage and recycling with their own garage space. Bins to be pushed to the curb on trash and recycling days

Mechanical equipment. Mechanical equipment located on the ground, such as heating or cooling equipment, pumps, or generators must be screened from the street and any abutting residential zones by walls, fences, or vegetation. Screening must comply with at least the L2 or F2 standards of Chapter 33.248, Landscaping and Screening, and be tall enough to screen the equipment. Mechanical equipment placed on roofs must be screened in one of the following ways, if the equipment is within 50 feet of an R zone:

Comment: All mechanical equipment will be within the roofs of individual units.

Pedestrian Standards (33.120.255).

Comment: The applicant has proposed pedestrian connections from main entrances of buildings to adjacent street, and between buildings on site within the central cobble courtyard.

33.120.255 D Transfer of FAR the sending property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to send floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

33.120.255 E Transfer of FAR the receiving property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to receive floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

Both primary buildings have connections to each unit between one main entrance and the adjacent street. This connection is less than 120% or 20 feet longer than the straight-line distance.

Because Building 1 (units 1 and 2) is located on the northwest corner of Schuyler, and 12 it has an entrance on 12th Avenue. The shared vehicular entrance is on 12th. The main pedestrian entrance will be 3-feet further away from the public right-of-way at 5-feet. The applicant requests that the City find the correct process to permit this design element.

Materials.

Comment: The proposed pedestrian circulation is hard surfaced ( concrete ) and is 3-feet wide. There is no interaction between the pedestrian entrance and access to the public street and the vehicular access.

Lighting.

Comment: The on-site pedestrian circulation access is being lighted to a level where the system can be used at night by the residents guests.

Additional Standards for Structured Parking and Garages (33.120.283).

Comment: No structured parking is proposed.

33.245 Inclusionary Housing - applies to new development with 20 or more dwelling units.

Comment: This application proposes four new homes. This section is not applicable.

33.248 Landscaping and Screening

Comment: The applicant is proposing an L1 standard landscaping around the perimeter of the property. Specifically, this will occur in the form of a hedge along the street side perimeter, and native plantings around the interior perimeter. These are noted in detail on the Landscape Plan.

33.258 Nonconforming Upgrades

- Interior or exterior improvements to a site totaling more than \$347,000 requires up to 10% of the project cost must be spent toward bringing the site into conformance with identified zoning code standards.

Comment: The applicant will be making the necessary public street improvements to meet the current half street standards for both frontages

33.266 Parking and Loading

Vehicle parking is not required at this site, but when parking is provided, the maximum parking allowances are described in 33.266.115 and Tables 266-1 and 266-2. In addition, when parking is provided, there are minimum requirements for Electric Vehicle-Ready spaces described below.

Required Electric Vehicle Charging Spaces 33.266.110.D

Comment: This standard applies only buildings with five or more dwelling units. However the applicant is proposing to add EVC to each garage in part to support the proposed modification to the standard for Setbacks

In buildings with five or more dwelling units, if parking spaces are provided for any of the dwelling units, the following standards apply:

33.266.200 Minimum Required Bicycle Parking

Comment: Bike Parking Area A: The proposed development is located our site is in Bike Parking Area A Per Map 266.1 Y. Therefore 1.5 Long term spaces per unit ( 6 spaces) and a minimum of 2 Short-term spaces are required. The applicant is proposing that each garage house two vertical bicycle parking spaces. The location of the spaces are identified on the street level floor plan. The short term bike parking rack is located on the north property line at the end of the maneuvering area between the homes.

33.266.210 Bicycle Parking Development Standards

Comment: For the proposed Bicycle Parking Spaces, Maneuvering Area, and Clearance Dimensions the proposed bike parking spaces will meet the minimum requirements for spacing as shown in Table 266-7. Specifically for the short term parking spaces in the driveway the required bicycle space is 2 feet wide, 6 feet long and 3 feet 4 inches tall. There is at least 5 feet behind all bicycle parking spaces to allow room for bicycle maneuvering.

33.266.210.C & D Long-term bicycle parking.

Comment: There will be two long term spaces within each of the four garage spaces. The details are called out in the main floor plan, that shows the location of the bike parking as well as the maneuvering area.

33.266.210 C & E Short-term bike parking

Comment: The short-term bicycle parking spaces are located on-site, outside the proposed buildings. They are at the internal driveway and can be reached by an accessible route. The short-term bike parking is

33.266.210 F Transfer of FAR the receiving property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to receive floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

Coordination with Other Agencies

a. Electric Utility Provider Coordination

The applicant is working with the Land Use Case Planner, ( and hopefully in receipt of a written acknowledgement from the Utility Provider)to place the transformer in the ROW.

b. Transportation (PBOT)

Comment: The access form the property will be from a single shared driveway on NE Schuyler, over 30-feet from the intersection with NE 12th. No garages will be facing either street. The applicant does not believe a DDE will be required. This need to be confirmed by staff

Oriel Windows. Projecting bays (aka oriel windows) are subject to the Window Projections into Public ROW standards. Note standards A through D must be met. However, Exceptions to standards E through G can be requested as part of the Historic Resource Review. The Code Guide for these standards can be found at portlandoregon.gov/bds/article/68600. A Modification fee is required for staff to consider an Exception.

Comment: No Oriel windows are proposed

Loading. If Modifications or Adjustments to Title 33 loading standards are contemplated, you must reach out early and coordinate with both PBOT and BDS Historic Resource Review staff ahead of the Land Use Review submittal. PBOT may require a Traffic Scope

33.266.210 G Transfer of FAR the receiving property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to receive floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

33.266.210 H Transfer of FAR the receiving property must be c. A site that contains a Historic or Conservation Landmark or a contributing resource in a Historic or Conservation District. Sites that are eligible to receive floor area through this transfer are allowed to transfer: (1) Unused FAR up to the maximum FAR allowed by the zone;

Approval and Loading Demand Study, which both need to be submitted before the Land Use Review submittal. More information can be found at portland.gov/transportation/development/traffic-impact-studies.

Comment: No loading is proposed or anticipated

Public Works Permit – If PBOT requires a 30% Public Works Permit approval before their response to a Historic Resource Review, it should be noted that such approvals can often take longer than the Land Use Review process. Therefore, you are highly encouraged to initiate the Public Works Permit process before you submit the Historic Resource Review.



33.846.060 Historic Resource Review

For Historic Districts, excluding Historic Landmarks, when proposals are not exempt from review as specified in Subsection 33.445.200.D.2, the review procedure is determined by Table 846-3, below:

Table 846-3 Review procedures for proposals within Historic Districts			
Proposal	Zone	Threshold	Review Type
Relocation	All	Contributing resource	Type III
		Contributing accessory structure	Type II
New structure	All	Floor area > 5,000 sq. ft.	Type III [1]
		Floor area > 800 sq. ft. and ≤ 5,000 sq. ft.	Type II [1]
		Floor area ≤ 800 sq. ft.	Type Ix

**Comment:** The proposed development has a combined floor area of greater than 5,000 SF and therefore is subject to a type III review.

F. **Approval criteria for inside the Central City plan district.** In the Central City plan district, requests for historic resource review will be approved if the review body finds that the applicant has shown that all of the approval criteria have been met. Conflicts among guidelines and criteria are resolved as specified in Paragraph F.6, below. The approval criteria for historic resource review in the Central City plan district are as follows:

d. Irvington Historic District. In the portion of the Irvington Historic District within the Central City plan district, when historic resource review is required for any resource, including Historic Landmarks and Conservation Landmarks, the approval criteria are:

- (1) The Central City Fundamental Design Guidelines;
- (2) The design guidelines for the Lloyd District subdistrict of the Central City plan district;
- (3) The criteria in Subsection G; and
- (4) If the proposal includes relocating a landmark or contributing resource, the criteria of Subsection I.

**Comment:** The applicant will be required to go through a type III land use process. Because there is no contributing structure on the property there are limited code requirements. The applicable criteria can be found under 33.846.060.G. There is no landmark or contributing resource to be relocated.

G. Other historic approval criteria.

When required by Paragraphs E. or F., the following approval criteria must be met:

**Comment:** Subsections #1 - #5 do not apply

1. Historic character.

The historic character of the landmark or contributing resource will be retained and preserved. Removal of historic materials or alteration of features and spaces that contribute to the historic significance of the landmark or contributing resource will be avoided;

**Comment:** Not applicable

2. Record of its time.

The landmark or contributing resource will remain a physical record of its time, place, and use. Changes that create a false sense of historic development, such as adding conjectural features or architectural elements from other buildings will be avoided

**Comment:** Not applicable

3. Historic changes.

Most resources change over time. Those changes that have acquired historic significance will be preserved;

**Comment:** Not applicable

4. Historic features.

Generally, deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement, the new feature will match the historic feature in design, color, texture, and other visual qualities and, where practical, in materials. Replacement of missing features must be substantiated by documentary, physical, or pictorial evidence;

**Comment:** Not applicable

5. Historic materials.

Historic materials will be protected. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials will not be used;

**Comment:** Not applicable

6. Archaeological resources.

Significant archaeological resources affected by a proposal will be protected and preserved to the extent practical. When such resources are disturbed, mitigation measures will be undertaken;

**Comment:** There is no indication that there are any archaeological resources on this parcel

7. Differentiate new from old.

New additions, exterior alterations, or new construction will not destroy historic materials that characterize a landmark or contributing resource. New work may be differentiated from the old if the differentiation does

not diminish the character, features, materials, form, or integrity of the landmark or contributing resource and, if in a Historic District, the district as a whole;

**Comment:** This property is not a contributing resource or landmark. No historic materials will be destroyed by the new construction. This is not applicable.

8. Architectural compatibility.

New additions, exterior alterations, or new construction will be compatible with the massing, size, scale, and architectural features of the landmark or contributing resource and, if in a district, the district as a whole. When retrofitting to improve accessibility for persons with disabilities or accommodate seismic improvements, design solutions will not compromise the architectural integrity of the landmark or contributing resource;

**Comment:** When looking at the proposed fourplex in the R3 zone it is difficult to find many comparable, especially ones that have been considered as 'contributing' rather than just built within the district.

For example The Bonnie Bræ apartments (1022 NE Hancock)- a contributing structure has 4 1/2 stories including the finished attic space (with windows). It has a total of 10,619 SF or finished floor area on a 5,000 SF lot. Its stands nearly 60-feet in height and is located right up against the property lines on both 11th and Hancock. Its windows hang either in singles of pairs with double hung frames.

Its third floor juts out a couple of feet from the first and second floors. From the rear it appears as if two buildings are similar in massing to the proposed pair of duplexes on the project site. There is a large 3-ft overhand around the perimeter of the building. There are multiple entrances, some with steps up. Others with several steps down. The colors are darker, heavier, and more earthen tones.

This property is on the transitional edge of the district, one block away from Broadway. The issue is whether the proposed multifamily development should be forced to conform to a single family identity or take its cues from taller historical buildings.

Overall, the building footprint is 2,752 sf of 55.04% of the parcel. The street facade add up to

SOUTH ELEVATION - LOOKING N.E SCHUYLER ST - BLDG 1 & 2 Facade massing = 2,567 sf

WEST ELEVATION - LOOKING N.E 12TH AVE - BLDG 1 Facade massing = 1,777 sf

The homes most impacted by this development of the proposed fourplex, and those homes directly adjacent to it.

Across the street, west of the proposed development sits 1803 NE 12th Avenue. This house has a total square footage of 3,724 SF. It has as a street facade massing on the corner of 2,518 SF, and a lot coverage (including overhangs ) of 2667 SF of 53% of the lot area. The applicant has included a sketch (projected mass ) where this home has been superimposed on the west face of Unit 1. While taller, the proposed unit 1 does not seem out of place in terms of facade, or lot coverage

The house directly east of the property is addressed at 1223 NE Schuyler, with a total Lot coverage of around 2,646 SF ( including overhangs) - about 53% and a side facade of 1200 SF, and a front facade of 860 SF.

The house north of the property is located at 1816 NE 12th Avenue. Has a floor area of 2300 SF ( not including the basement. The total southern facade or the house and garage equals roughly 1,750 + 300 = 2000 SF

Each of the proposed buildings have a northern facade of less than 1400 SF

9. Preserve the form and integrity of historic resources.

New additions, exterior alterations, or new construction will be undertaken in such a manner that if removed in the future, the essential form and integrity of the landmark or contributing resource and, if in a district, the district as a whole would be unimpaired; and

**Comment:** If the proposed fourplex is demolished in its entirety, there is no reasons to suspect that the form and integrity of the existing contributing resources ( homes ) would be impaired by this action.

10. Hierarchy of compatibility.

New additions, exterior alterations, or new construction will be designed to be compatible primarily with the landmark or contributing resource and, if located within a district, secondarily with contributing resources located within 200 feet and, finally, with the rest of the district. Where practical, compatibility in districts will be pursued on all three levels.

**Comment:** The four-story English style fourplex, consisting of two 'duplex' buildings in the Irvington Historic District meets the criteria for a historic review in multiple compelling ways. It exemplifies the architectural and historical significance of the neighborhood, showcasing design elements and craftsmanship of its era. Moreover, the fourplex contributes to the overall historic fabric of the district, preserving the visual character and integrity of the surrounding area.

Its historical value is evident in the architectural details, building materials, and overall design, which reflect the period in which it was constructed. Both of these buildings meet the criteria for a historic review by diligently preserving the heritage and character of the Irvington Historic District.

It has the neighborhood "DNA" making it compatible with the district. To this end the applicant has looked towards historic multi-dwelling structures and Historic Resource Review-approved contemporary multi-dwelling structures within the district to better understand the context of the district and better inform the design of the proposed building.

For example The Bonnie Bræ apartments (1022 NE Hancock)- a contributing structure has 4 1/2 stories including the finished attic space ( with windows). It has a total of 10,619 SF or finished floor area on a 5,000 SF lot. Its stands nearly 60-feet in height and is located right up against the property lines on both 11th and Hancock. Its windows hang either in singles of pairs with double hung frames.

Its third floor juts out a couple of feet from the first and second floors. From the rear it appears as if two buildings are similar in massing to the proposed pair of duplexes on the project site. There is a large 3-ft overhand around the perimeter of the building. There are multiple entrances some with steps up. Others with several steps down. The colors are darker, heavier, and more earthen tones.

A more modern take is the Irvington Garden Apartments. While not considered a contributing structure it has been designed approved and built with a similar design in mind. The use of brickwork on the ground floor. The windows grouped in twos and threes across the facades. The fourth story wrapped in the roof design. The steep pitches, and centralized arched entrance. There are hedges around the perimeter in the semi-public space to create a slightly softer buffer at the foundation of the building.

Westminster church a contributing building again four stories with an attic space. Tall roof pitches, and a plain facade located close to the property line, with some softer landscaping along the foundation of the facade The windows grouped, but in vertical lines, more muted colors, and a sense of permanence.

All of these structures have large vertical elements. These have been broken down into several planes, but are fairly sparse in detailing. IN many cases contributing Irvington multi-dwelling buildings often look like a single building.

While each of the buildings is a duplex, the size, mass, and facades of each building are not dissimilar to the adjoining properties.

The 'Mid' issues tie into the relationship between the building and the streetscape. The initial designs were hampered by the amount of garage doors, large pronounced vehicle areas and lack of entry level pedestrian use. Additionally, a significant issue of concern was the number of curb cuts proposed. Following input from both the neighborhood architectural committee, and City staff substantive changes were made by the applicant. The project architect believes that he has used his professional eye to create a sense of composition, and proportion. He has attempted to use a combination of various patterns and shapes, blending conventional materials and design with modern more formal exteriors.

This proposal now has a single curb cut on Schuyler and none on 12th Avenue. Said curb cut and driveway will service all four homes being proposed.

The ground floor street frontage has been activated using multiple entrances and significant ground level glazing. The type and style of the ground level facades are addressed under the 'micro' level notes

Ground level weather protection is provided at all four ground level entries. In addition, the central cobblestone courtyard is partially protected by both buildings and the extended outdoor covered decks that are centrally located above the courtyard.

Gas & Electric Meters have been located on the interior to the courtyard rather than facing the streetscape.

The landscaping has been built round more vertical elements creating a semi-private space between the property line, the building using black railings indicative of a more ornate history, and sense of place.

For the building itself the applicant is proposing to use high quality, durable building materials that respond to the context of the historic district, are expected. At ground level facades facing pedestrian areas, provide robust materials that ensure longevity. The applicant has attempted to design the project with 'harmony in diversity' concentrated around the early 20th century arts and crafts movement.

This begins with the central courtyard that will be lain with cobblestones. More specifically Belgian blocks. These came from the Columbia River Flood, and came into popular use with the spread of the cable car system in Portland. These cobblestones were a common site during the height of the original development of the Irvington Historical Area. This design is a callback to the original Arts an crafts movement, and reflect the underlying use of quality materials.

Sheet C3.5 through C3.8 provide specific material types, and proposed colors3

The main cladding on the second and third story will be a thicker fiber cement plank. (specs included).

The Ground floor facade is bricked in a style like other homes in the area. It provides a solid base reflected in either stone or brickwork in similar homes. He brick chosen is 8632 Velour with a rich dark chestnut color, manufactured by the Belden brick company, which its self has been around since 1885.

The upper floors will be a mix of thicker fiber cement planks, ( early American stain) cedar shingle deisgn ( early (American stain) and RGB board

The front doors mimic typical understated entries common in the area.

The Exterior lighting has been integrated into the building's overall concept to ensure a safe pedestrian condition along the adjacent sidewalk and within courtyard space on the property. To that end the Exterior light has been design to be used to highlight the building's architecture, while being suited to the neighborhood with subtle 'downlights' to respect the "dark sky" Ordinance concepts.

The garage doors are proposed as panel fiberglass with glass on all the panels to allow natural light into the space.

The windows are single hung or fixed, in grouping of twos or threes. They will be a Auraline true composite windows manufactured by Jen weld

The style of exterior lighting will be simple down lighting fixtures. These will be urban ambiance fixtures that are LED down lights manufactured by Juno

The Entry porches appear as a centralized entry but preserve individual identity.

All downspouts are not plastic, but rather 4" copper as is evident in high end residences in the area. The metal railing along the street frontage is proposed for yard protection and individual privacy for entrances.

Both of the buildings with will have a foundational masonry base. The brick type is still in process. There will be stucco board siding and a single top to reflect an arts and crafts style

The exterior landscaping is primarily vertical in nature, creating a vertical green separation. It is probable that a short hedge around the perimeter would create a softening of the foundation, and a transition from the private to public space.

H. Other conservation approval criteria.

**Comment:** No relocation is being proposed

**Other relocation approval criteria.** When required by Paragraphs E. or F., one of the following approval criteria must be met:

**Comment:** No relocation is being proposed

33.846.070 Modifications Considered During Historic Resource Review

The approval criteria for modifications considered during historic resource review are:

**Comment:** The applicant is requesting a modification to the street setbacks. Reducing the front yard setback on NE 12th from 10-feet to 5-feet, and the side yard setback on Schuyler from 5-feet to 2-feet

A. Better meets historic resource review approval criteria.

The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and

**Comment:** The applicant proposed an initial design that met the front, and street-side yard setbacks. This proposal was roundly pushed back on, with numerous issues tied to the Historical District. Namely issues tied to the garage and driveways spaces as well as massing and size issues. Responding to direction from the Historic committee who suggested that the fourplex be divided into two much smaller buildings, using a

single centralized entrance the applicant came back with a design that met the Committees' direction. However, by dividing up the building, and creating a centralized 'courtyard' the applicant lost 30% of the building envelope defined by the parcels setbacks. In attempt to try and regain a portion of this lost area the applicant proposed reducing the front yard setback by 5-feet, and the street-side yard setback by 3-feet. This meant the applicant was able get back about 1/3rd of the area lost to the change in design. The change in design allowing the project to much more closely meet the historic review criteria.

B. Purpose of the standard.

The resulting development will meet the purpose of the standard being modified; or

The preservation of the character of the historic resource is more important than meeting the purpose of the standard for which a modification has been requested.

**Comment:** The applicant believes that designing a project that only has a single driveway for four homes, no garage doors facing the street, a much smaller building footprint, and an overall façade mass for each for each building that is smaller than any adjacent contributing resource.

33.846.080 Demolition Review

**Comment:** No Demolition is proposed

ENGINEERING REQUIREMENTS

It is anticipated that the proposed project will require right-of-way improvements but is not anticipated to require dedication. This is because under 17.88.020, alterations which increase the number of occupants of a site are required to provide a standard full width improvement, including sidewalks, However no additional dedication is required to allow the full width improvement to be constructed.

Based on City GIS, there is 12-foot of right-of-way from curb to property line on both frontages. Right-of-way dedication is not anticipated on either frontage. However, ADA improvements at the intersection of Schuler and 12th are proposed.

NE Schuyler St. is in a pedestrian district, though the site is not. The standard sidewalk corridor for NE Schuyler St is a total of 12-ft consisting of a 0.5-ft curb, 4-ft furnishing zone, 6-ft sidewalk, and 1.5-ft frontage zone.

NE 12th Ave. is not in a pedestrian district at this location. The standard sidewalk corridor is 11-ft. The extra 1-ft of right-of-way should be allocated to the furnishing zone resulting in a 0.5-ft curb, 5-ft furnishing zone, 6-ft sidewalk, and 0.5-ft frontage zone.

It is anticipated that the required improvements will trigger a Public Works Permit.

For PBOT purposes, occupancy is determined based on trips rates from the Institute of Transportation Engineers Trips Generation Manual, 11th Edition. In this case, replacing a single home with four units will result in an increase in trips due to the three new dwelling units. The existing Use was a single Family Detached Dwelling. ITE Code 210- 9.43 average daily trips Proposed Use: Multi-Family Housing Low Rise. ITE Code 220- 6.74 average daily trips per unit X 4 units = 26.96 average daily trips.

Because of the RM3 zoning, the site does not qualify for consideration under TRN 1.22- Infill Development on Streets with an Existing Sidewalk Corridor.

Staff have stated that they may ask for a second light to be installed on the exiting utility pole on the NE Schuyler and 12th

The City's driveway code is TRN 10.40. Properties are allowed one driveway on the lower classified street at least 25-feet from the property corner where two streets intersect. The initial proposal included one driveway on 12th Ave. and two on Schuyler St. While an driveway exception application might have been submitted the applicant revised the proposal to cut a single access on Schuyler for all four homes.

Required Public Improvements, Dedication, and Driveways

**Comment:** The site is not within a Pedestrian District, but the NE Schuyler St. right-of-way is within a Pedestrian District. Both NE 12th and NE Schuyler St have a 60-ft right-of-way; a 36-ft paved street; a 12-ft pedestrian corridor consisting of a 0.5-ft curb; a 3.5-ft furnishing strip; a 6-ft sidewalk; and a 2-ft of clear space. The city is proposing no change to the curb location on either street, and no change to the 12-ft pedestrian corridor width. However, they are requiring a 4ft furnishing strip on NE Schuyler, and a 5-ft furnishing strip on NE 12th Avenue

At the corner at the intersection of NE 12th and Schuyler, the applicant will reconstruct the current ramp to meet current City and Federal ADA standards. A curb extension is not required. Signals and Street Lighting has stated that he 'may' ask for a second light to be installed on the exiting utility pole on the SE corner of 12th/Schuyler (oriented over NE Schuyler St) however this has NOT been confirmed by staff.

There is a single 20-ft driveway providing a central access for all four units. The access is located at the midpoint of the south property line on NE Schuyler

Public Works Permit Process

**Comment:** The Right-of-Way improvements will be designed by an Oregon licensed civil engineer and constructed under a Public Works Permit, The Concept Development Phase plans will be submitted with the Design Review Application. The PW Design Review will determine specific design elements including stormwater management, bus stop, curb-cuts, landscaping, location of signage, location of utility poles and Streetlights, as well as other design requirements.

BES

**Comment:** As part of this proposal the applicant is submitting: a site plan showing all proposed sanitary connections and stormwater systems; a stormwater report, including the results of infiltration testing, with the land use and building permit applications. It is anticipated that the required public right-of-way improvements will not trigger stormwater management requirements. However, if triggered, some or all of this area is eligible to pay an Offsite Stormwater Management Fee in lieu of building a stormwater facility.

SANITARY SERVICE

**Comment:** The is an 8-inch PVC sanitary-only sewer in NE Schuyler St (BES as-built #E10762); and an 8-inch clay (VSP) combined sewer in NE 12th Ave (BES as-built #unknown). Because a water main is located between the proposed development site and the combined and storm-only sewer mains in NE 12th Ave and the sanitary-only main in NE Schuyler St. The newly proposed sanitary laterals will cross the water main and will require a Water Utility Protection Plan. The existing lateral from the demolished house may need to be upzized.

STORMWATER MANAGEMENT



**Comment:** There is a Public 12-inch concrete (CSP) storm sewer in NE 12th Ave (BES as-built #3337). Currently stormwater from this line in public right-of-way discharges to an existing combined sewer and storm sewer.

Stormwater runoff from the public right-of-way must comply with all applicable standards of the SWMM and Sewer and Drainage Facilities Design Manual and be conveyed to a discharge point along a route of service approved by the BES Director or the Director’s designee. Additional guidance on meeting the 2020 SWMM for projects in the public right-of-way is available at <https://www.portland.gov/bes/stormwater/swmm-and-right-way-projects>.

a. Under the 2020 SWMM, sidewalk improvements behind an existing curb that create or replace 500 SF or more of impervious area are no longer exempt from stormwater management requirements unless they fall under a specific exemption described in

Section 1.2.1.2 of the SWMM. Based on the scope of PBOT requirements, the length of project frontage and existing conditions, BES does not expect that the required sidewalk improvements will exceed 500 SF of triggered impervious area. However, if the scope of work changes or future analysis shows that the triggered area will exceed 500 SF, then the applicant should note that this area is eligible to pay an Offsite

Stormwater Management Fee in lieu of building a stormwater facility through the Staff Review Special Circumstances (SRSC) process. Refer to Section 1.8.1 of the SWMM and contact BES Development Engineering staff to confirm SWMM requirements.

**Private Property Stormwater Management:**

Stormwater runoff from this project must comply with all applicable standards of the SWMM and SCM and be conveyed to a discharge point along a route of service approved by the BES Director or the Director’s designee.

Stormwater Report:

**Comment:** The applicant is submitting a stormwater report and preliminary utility plan showing stormwater management facilities sized according to SWMM standards. The applicant believes that based on criteria described in Section 2.1.2 of the SWMM, this site qualifies for stormwater facility sizing using the Simplified Approach.

**Geotechnical Engineering Requirements**

**Comment:** In order to evaluate whether the foundation design of the proposed structure complies with the Oregon Structural Specialty Code, the applicant will submit a geotechnical report at the time of building permit application.

**Shoring and Excavation Plan**

**Comment:** A complete shoring and excavation plan, including construction details and calculations, will be submitted at the time of building permit application.

**Erosion Control:**

**Comment:** Because full compliance with the erosion control requirements of Title 10, including maintenance of the erosion control elements is required during site development. An erosion control plan will be submitted at the time of building permit application.

**Water:**

**Comment:** Water is available to this site from the 8-inch Cast Iron water main in NE 12th Ave and the 6inch Ductile Iron main in NE Schuyler St. The static water pressure is estimated as 54 - 68 psi at 149 feet in elevation.

The site is currently served through a 5/8-inch meter on a 3/4-inch service.

To install new sanitary laterals to the property the applicant must cross the existing water mains in NE 12th Ave. and in NE Schuyler St. The PWB will require a Utility Protection Plan prior to construction.

The applicant proposes that the corner unit will use the existing 5/8-inch meter on SE 12th Ave. The remaining units will connect water services off the 6-inch DI in NE Schuyler. Each unit will have a sperate meter. The applicant will take advantage of PWB policy to allow separate meters on a shared service.

No sperate fire lines are proposed.

**Fire:**

**Comment:** Fire Bureau requirements are generated from the 2022 Oregon Fire Code. All current Fire Code requirements apply and are required to be met. Outside of this Design Review a separate building permit is required for this proposal. All applicable Fire Code requirements shall apply at the time of permit review and development.

**D. Heritage Trees**

**1. Heritage Trees (11.20.060):**

**Comment:** There are no heritage trees located on/adjacent to the site that is on the City of Portland’s Heritage Tree list.

**Street Tree Preservation (11.50.040)**

The proposed development parcel has approximately 100 feet of street frontage on NE Schuyler Street, and 50 feet of street frontage on NE 12th Avenue both rights-of-way are improved with pavement, curbs, planting strip, sidewalks. There are no overhead high voltage power lines. There are four street trees On Schuyler

i. Norway Maple, 28", fair

ii. Norway Maple, 23", fair

iii. Norway Maple, 20", fair

iv. Norway Maple, 21", fair

**Comment:** All street because the Norway maples are on the nuisance plant list and can be approved for removal if necessary to facilitate development They can be removed with no mitigation. Due to the species, size, and condition of trees removed, no trees are required to be planted to mitigate the loss because of this project.

**Street Tree Planting (11.50.060.C)**

The applicant has not provided a conceptual street tree planting plan. One street tree must be planted or retained for each full increment of 25 linear feet (11.50.060.C.1). Street trees must be planted at a minimum 2 caliper inches.

**Comment:** The property has 150-feet of street frontage, requiring a total of six trees. Because there is only on one entrance the applicant is proposing to retain two of the four existing trees, and to plant a further four trees, two on Schuler, and two on 12th. The trees will be Broadleaf, from the Portland Plat list with a 1.5" caliper at time of planting.

**Tree Plan Submittal Requirements (11.50.070)**

**Comment:** While not required the applicant is proposing to retain two of the four existing street trees and will take full precautionary measures during construction. This will include a tree protection plan.

**Tree Mitigation (11.50.040.C.2)**

**Comment:** Two trees are proposed to be removed on the east end of the site. The trees are a gray Birch (17-in DBH) and a Honey Locust (15-in DBH). Neither tree is native nor listed on the Nuisance Plant List. The proposed site development is a multi-unit dwelling with 5-foot lot setbacks. As such, the project will not be able to retain 1/3 of the trees on site. Therefore, under 11.50.040 Tree Preservation Standards (Table 50-1), the mitigation fee for 12 inches and less than 20 inches is the cost of (2) two-inch diameter replacement trees.

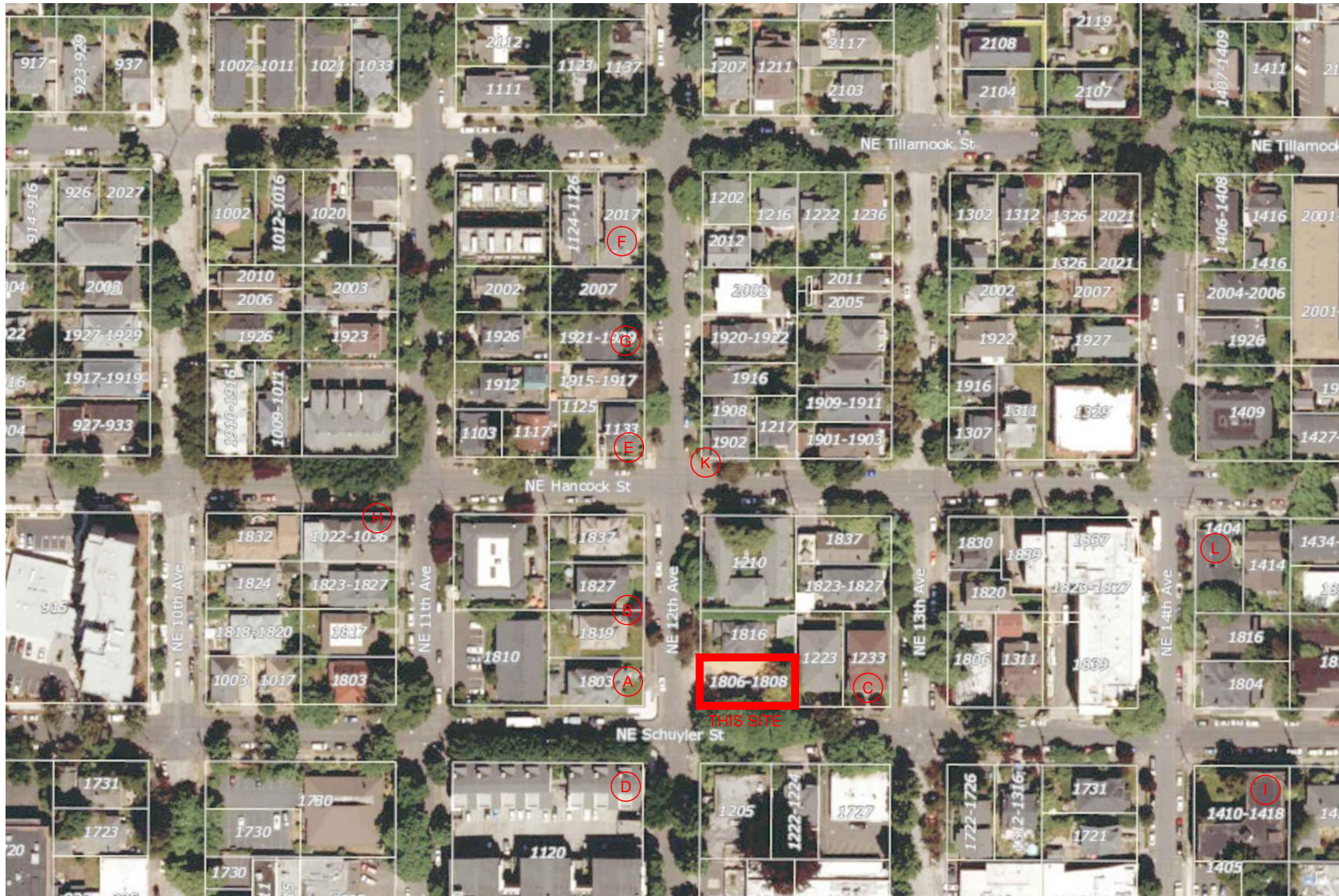
**Street Tree Planting Standards (11.50.050)**

**Comment:** The property has 150-feet of street frontage, requiring a total of six trees. Because there is only on one entrance the applicant is proposing to retain two of the four existing trees, and to plant a further four trees, two on Schuler, and two on 12th. The trees will be Broadleaf, from the Portland Plat list with a 1.5" caliper at time of planting.

**Street Tree Protection Specifications (11.60.030)**

If you elect to retain the maple trees for density tree protection is required in accordance with Title 11 Trees, Protection Methods (11.60.030). Tree protection shall follow either the Prescriptive or performance path. Protection methods must be shown on the tree plan. If using the Performance path, the alternate tree protection plan must be prepared by an arborist who has visited the site.





## LEGEND

- (A) 1803 NE 12TH AVE  
PORTLAND, OR 97212
- (B) 1819 & 1827 NE 12TH AVE  
PORTLAND, OR 97212
- (C) 1233 NE SCHUYLER ST #4  
PORTLAND, OR 97212
- (D) 1132 NE SCHUYLER ST #107  
PORTLAND, OR 97212
- (E) 1133 NE HANCOCK ST  
PORTLAND, OR 97212
- (F) 1207 & 1211 NE TILLAMOOK ST  
PORTLAND, OR 97212
- (G) 2115 & 2129 NE 12TH AVE  
PORTLAND, OR 97212
- (H) BONNIE BRAE APARTMENTS  
1022-1036 NE HANCOCK ST  
PORTLAND, OR 97212
- (I) 1418 NE SCHUYLER ST  
PORTLAND, OR 97212
- (J) IRVINGTON GARDEN APARTMENTS  
1516 NE HANCOCK ST.  
PORTLAND, OR 97212
- (K) 1902 NE 12TH AVE  
PORTLAND, OR 97212
- (L) 1404 NE HANCOCK ST  
PORTLAND, OR 97212

1 VICINITY MAP  
N.T.S.



NEIGHBORHOOD PHOTOS



D 1132 NE SCHUYLER ST #107  
PORTLAND, OR 97212



E 1133 NE HANCOCK ST  
PORTLAND, OR 97212



F 1207 NE TILLAMOOK ST AND 1211 NE TILLAMOOK ST  
PORTLAND, OR 97212



A 1803 NE 12TH AVE  
PORTLAND, OR 97212



B 1819 NE 12TH AVE AND 1827 NE 12TH AVE  
PORTLAND, OR 97212



C 1233 NE SCHUYLER ST #4  
PORTLAND, OR 97212



NEIGHBORHOOD PHOTOS



J IRVINGTON GARDEN APARTMENTS  
1516 NE HANCOCK ST.  
PORTLAND, OR 97212



K 1902 NE 12TH AVE  
PORTLAND, OR 97212



L 1404 NE HANCOCK ST  
PORTLAND, OR 97212



G 2115 NE 12TH AVE AND 2129 NE 12TH AVE  
PORTLAND, OR 97212

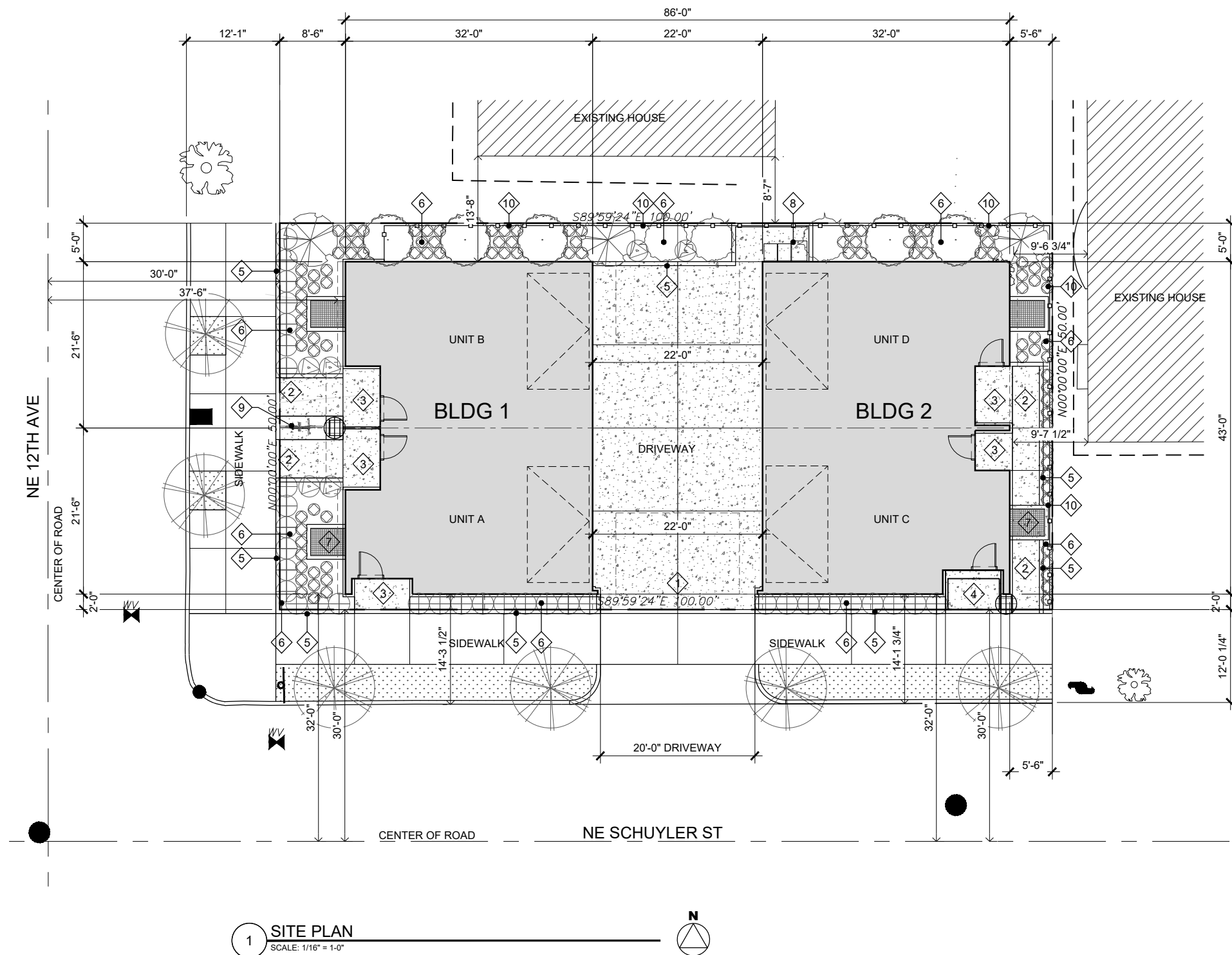


H BONNIE BRAE APARTMENTS  
1022-1036 NE HANCOCK ST  
PORTLAND, OR 97212



I 1418 NE SCHUYLER ST  
PORTLAND, OR 97212





1 SITE PLAN  
SCALE: 1/16" = 1'-0"

#### KEY NOTES

- 1 20' WIDE DRIVEWAY
- 2 WALKWAY
- 3 ENTRY PORCH
- 4 ENTRY/OUTDOOR - UNIT C
- 5 PROTECTIVE CURB
- 6 LANDSCAPE
- 7 GRATING REMOVABLE
- 8 TRASH BIN - ADU UNIT
- 9 BIKE RACK - SHORT-TERM
- 10 6' HIGH CEDAR FENCE

#### PROPERTY INFORMATION

PROPERTY ID: R182572  
 COUNTY: MULTNOMAH  
 STATE ID: 1N1E26CD 8200  
 NEW STATE ID: 1N1E26CD -08200  
 ALT ACCOUNT #: R396218820  
 MAP NUMBER: 2831  
 NEIGHBORHOOD: IRVINGTON  
 JURISDICTION: PORTLAND / MULTNOMAH

#### SITE INFO

SITE ADDRESS:  
 1806-1808 NE 12TH AVE  
 PORTLAND, OR 97212

#### OWNER:

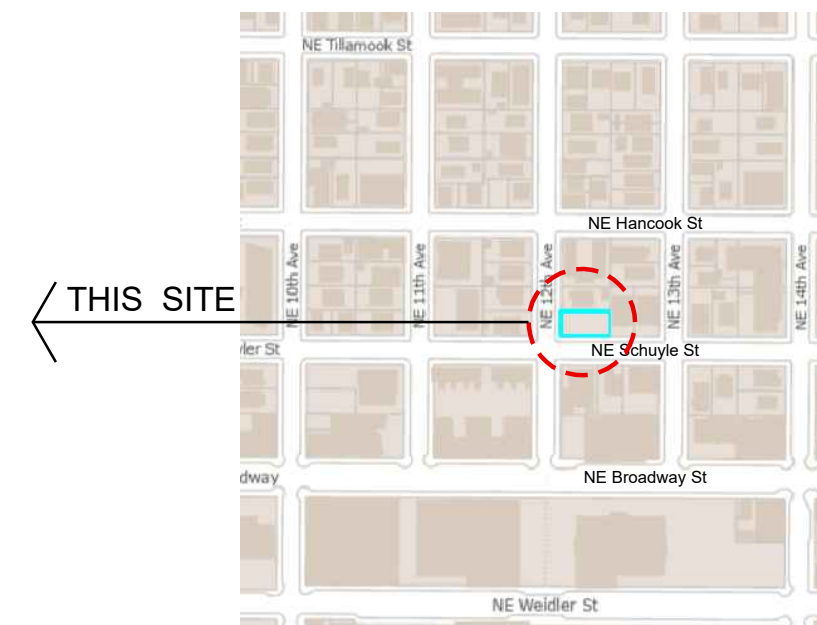
MAIN STREET DEVELOPMENT INC  
 5331 S MACADAM AVE #258 PMB 208  
 PORTLAND OR 97239-3871

#### ZONING & DISTRICTS

ZONE: RM3 - RESIDENTIAL MULTI-DWELLING 3  
 OVERLAY - d - Design  
 BASE OVERLAY COMBINATION - RM3d  
 COMP PLAN - MULTI-DWELLING - URBAN CENTER  
 COMP PLAN OVERLAY - N/A  
 HISTORIC DISTRICT IN - IRVINGTON HISTORIC DISTRICT  
 CONSERVATION DISTRICT - N/A  
 QUARTER SECTION(S) - 2831

#### LAND INFORMATION

RESIDENTIAL LAND		
PROPERTY ID: R182572		5,000 SF (0.11 ACRES)
<b>LOT AREA</b>	<b>PERCENTAGE</b>	<b>5,000.00 SF</b>
BUILDING FOOTPRINT	55.04%	2,752.00 SF
PAVING	26.20%	1,310.00 SF
LANDSCAPE	18.76%	938.00 SF
<b>TOTAL</b>	<b>100.00%</b>	<b>5,000.00 SF</b>



2 VICINITY MAP  
N.T.S.



EXISTING CONDITIONS MAP

TAX LOT 8200, MAP 1N, 1E, 26CD  
LOCATED IN THE S.W. 1/4 SECTION 26, T.1N., R.1E, W.M.  
CITY OF PORTLAND, MULTNOMAH COUNTY, OREGON  
SEPTEMBER 18, 2023 SCALE 1"=10'

SURVEY NOTES:

THE VERTICAL DATUM FOR THIS SURVEY IS BASED UPON CITY OF PORTLAND BENCHMARK NUMBER 1828, BEING A BRASS DISC ON THE TOP OF CURB AT THE S.W. CORNER OF THE INTERSECTION OF N.E. SCHUYLER STREET AND N.E. 14TH AVENUE. THE ELEVATION IS 150.129'. CITY OF PORTLAND DATUM.

A TRIMBLE S8-SERIES ROBOTIC INSTRUMENT WAS USED TO COMPLETE A CLOSED LOOP FIELD TRAVERSE.

THE BASIS OF BEARINGS FOR THIS SURVEY IS PER MONUMENTS FOUND AND HELD PER RECORD OF SURVEY RECORDED UNDER PRIVATE SURVEY NUMBER 38592, RECORDS OF MULTNOMAH COUNTY.

THE PURPOSE OF THIS SURVEY IS TO RESOLVE AND DETERMINE THE PERMETER BOUNDARY OF THE SUBJECT PROPERTY, TO SHOW ALL PERTINENT BOUNDARY ISSUES AND ENCROACHMENTS. NO PROPERTY CORNERS WERE SET IN THIS SURVEY.

NO WARRANTIES ARE MADE AS TO MATTERS OF UNWRITTEN TITLE, SUCH AS ADVERSE POSSESSION, ESTOPPEL, ACQUESCENCE, ETC.

NO TITLE REPORT WAS SUPPLIED OR USED IN THE PREPARATION OF THIS MAP.

UTILITY NOTES:

THE UNDERGROUND UTILITIES AS SHOWN ON THIS MAP ARE SHOWN BASED ON A COMBINATION OF INFORMATION, INCLUDING VISIBLE ABOVE GROUND STRUCTURES, AVAILABLE AS-BUILT AND GIS MAPPING FROM LOCAL JURISDICTIONS, AS WELL AS SURFACE MARKINGS BY ONE CALL TICKET NUMBER 232-07724 DATED AUGUST 8, 2023.

THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED.

THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES ARE IN THE EXACT LOCATION INDICATED, ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE.

THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. SUBSURFACE AND ENVIRONMENTAL CONDITIONS WERE NOT EXAMINED OR CONSIDERED AS A PART OF THIS SURVEY. NO STATEMENT IS MADE CONCERNING THE EXISTENCE OF UNDERGROUND OR OVERHEAD CONTAINERS OR FACILITIES THAT MAY AFFECT THE USE OR DEVELOPMENT OF THIS TRACT. THIS SURVEY DOES NOT CONSTITUTE A TITLE SEARCH BY SURVEYOR.

INVERT ELEVATIONS AND PIPE SIZES SHOWN ARE APPROXIMATE ONLY, BASED ON FIELD OBSERVATIONS AS WELL AS AVAILABLE AS-BUILT DATA. ALL PIPE SIZES SHALL BE FIELD VERIFIED BY THE OWNER, ENGINEER, CONTRACTOR, AND GOVERNING AGENCY PRIOR TO ANY CONSTRUCTION ACTIVITY. SURVEYOR DOES NOT WARRANT THE ACCURACY OF ANY PIPE SIZES SHOWN ON THIS SURVEY.

LEGEND:

Some Symbols shown may not be used on map.

- |  |                              |
|--|------------------------------|
| 12" DECIDUOUS TREE                                     | TRAFFIC SIGNAL POLE          |
| 24" EVERGREEN TREE                                     | UTILITY POLE                 |
| DEAD TREE  | GUY WIRE                     |
| STORM SEWER MANHOLE                                    | LIGHT POLE                   |
| CATCH BASIN  | GROUND LIGHT                 |
| CURB INLET   | ELECTRIC BOX                 |
| AREA DRAIN   | ELECTRIC METER               |
| DITCH INLET  | TRANSFORMER                  |
| SANITARY SEWER CLEANOUT                                | ELECTRIC RISER/GATE CONTROLS |
| SANITARY SEWER MANHOLE                                 | HEAT PUMP                    |
| FIRE HYDRANT   | GATE POST                    |
| WATER MANHOLE  | CABLE TV BOX                 |
| WATER METER  | CABLE TV RISER               |
| WATER VALVE  | OVERHEAD LINE                |
| HOSE BIB   | GAS LINE                     |
| IRRIGATION CONTROL VALVE                               | ELECTRICAL LINE              |
| GAS VALVE  | COMMUNICATIONS LINE          |
| GAS METER  | SANITARY SEWER LINE          |
| MAILBOX  | STORM DRAIN LINE             |
| UTILITY RISER  | WATER LINE                   |
| UTILITY BOX  | FENCE LINE                   |
| TELEPHONE MANHOLE                                      | GUARDRAIL                    |
| TELEPHONE RISER  | TRENCH DRAIN                 |
| STORM OUTFALL  | EDGE OF SHOULDER             |
| SIGN   | TREE DRIVELINE               |
| BOLLARD  | CONCRETE                     |
| FOUND MONUMENT   | GRAVEL                       |
| DOWN SPOUT TO STORM SYSTEM                             | ASPHALT                      |
| DOWN SPOUT TO SPLASH GUARD OR GROUND W/ PIPE DIRECTION | PAVERS                       |
| COLUMN   | BRICK                        |
|  | FD = FOUND                   |
|  | IP = IRON PIPE               |
|  | IR = IRON ROD                |
|  | YPC = YELLOW PLASTIC CAP     |
|  | ALC = ALUMINUM CAP           |
|  | RPC = RED PLASTIC CAP        |
|  | OPC = ORANGE PLASTIC CAP     |

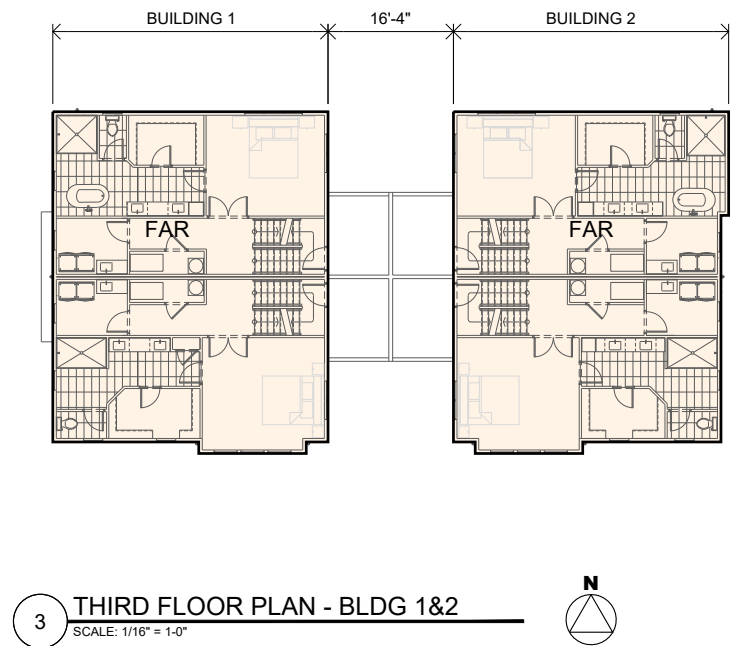
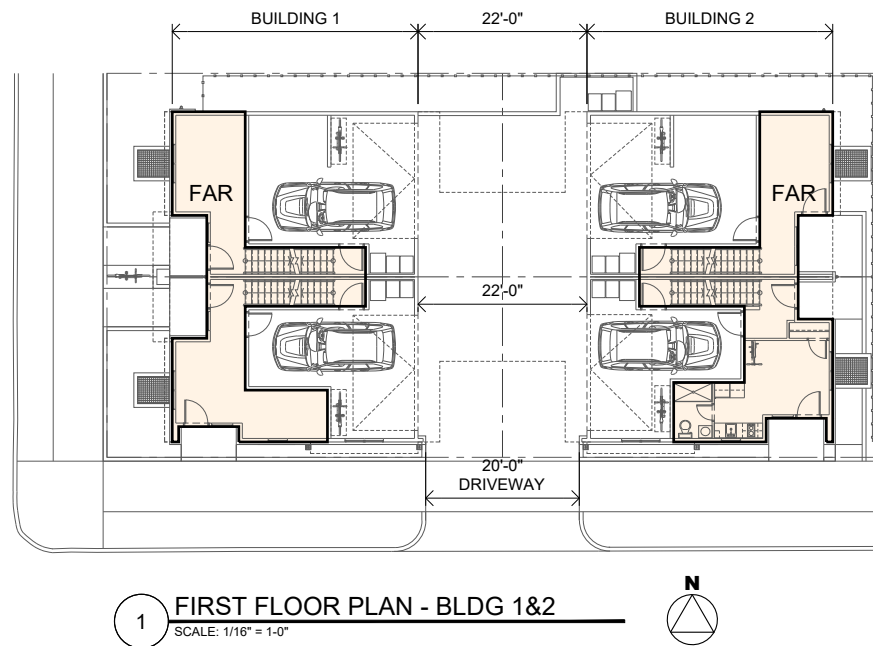
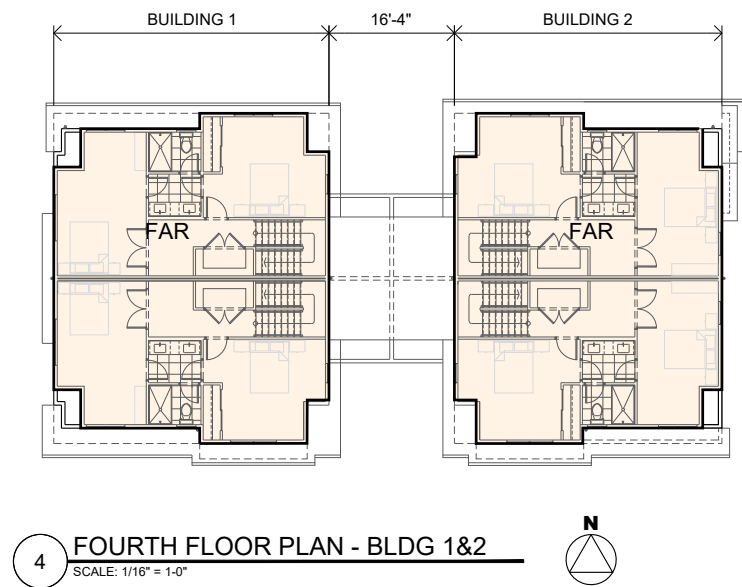
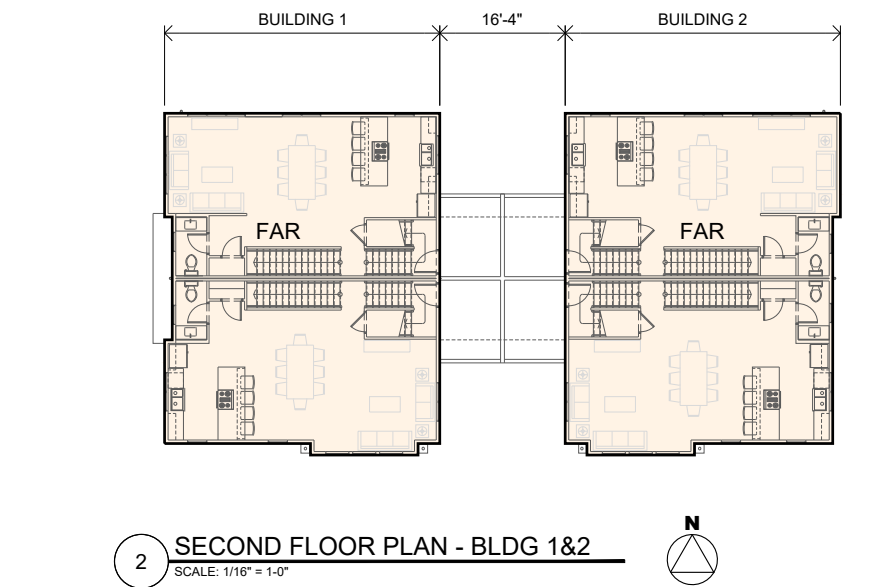
SIGNED ON: 9 SEP 23  
REGISTERED  
PROFESSIONAL  
LAND SURVEYOR  
OREGON  
JULY 13, 2004  
TOBY G. GOLDEN  
60377LS  
RENEW: DECEMBER 31, 2023

CENTERLINE CONCEPTS  
LAND SURVEYING, INC.

19376 MOLALLA AVE., SUITE 120  
OREGON CITY, OREGON 97045  
PHONE: 503.650.0188 FAX: 503.650.0189

PLOTTED: M:\PROJECTS\MAIN STREET-12TH AVE-NE-1806\DWG\ECM-C30.dwg





### 33.120 MULTI-DWELLING ZONES

TABLE 120-3

SUMMARY OF DEVELOPMENT STANDARDS IN MULTI-DWELLING ZONES

F.A.R. ALLOWED	2:1
SITE AREA	5,000 SF
ALLOWED AREA	10,000 SF

**PROPOSED FAR** **9,988 SF**

### LAND INFORMATION

RESIDENTIAL LAND	
PROPERTY ID: R182572	5,000 SF

UNIT A - AREA CALCULATION	AREA	SF
FIRST FLOOR	283.00	SF
SECOND FLOOR	783.00	SF
THIRD FLOOR	791.00	SF
FOURTH FLOOR	698.00	SF
<b>TOTAL AREA</b>	<b>2,555.00</b>	<b>SF</b>

UNIT B - AREA CALCULATION	AREA	SF
FIRST FLOOR	228.00	SF
SECOND FLOOR	762.00	SF
THIRD FLOOR	770.00	SF
FOURTH FLOOR	698.00	SF
<b>TOTAL AREA</b>	<b>2,458.00</b>	<b>SF</b>

UNIT C - AREA CALCULATION	AREA	SF
FIRST FLOOR	104.00	SF
SECOND FLOOR	769.00	SF
THIRD FLOOR	769.00	SF
FOURTH FLOOR	683.00	SF
<b>ADU - UNIT C</b>	<b>215.00</b>	<b>SF</b>
<b>TOTAL AREA</b>	<b>2,540.00</b>	<b>SF</b>

UNIT D - AREA CALCULATION	AREA	SF
FIRST FLOOR	228.00	SF
SECOND FLOOR	762.00	SF
THIRD FLOOR	762.00	SF
FOURTH FLOOR	683.00	SF
<b>TOTAL AREA</b>	<b>2,435.00</b>	<b>SF</b>

UNIT NUMBER	AREA	SF
<b>UNIT A</b>	<b>2,555.00</b>	<b>SF</b>
<b>UNIT B</b>	<b>2,458.00</b>	<b>SF</b>
<b>UNIT C</b>	<b>2,540.00</b>	<b>SF</b>
<b>UNIT D</b>	<b>2,435.00</b>	<b>SF</b>
<b>TOTAL FLOOR AREA (BUILDING 1&amp;2)</b>	<b>9,988.00</b>	<b>SF</b>



WINDOW AREA CALCULATION

SOUTH ELEVATION	WALL AREA	SF	WINDOW AREA	SF	PERCENTAGE
<b>BUILDING 1</b>	1,282.00	SF	269.00	SF	20.98%
<b>BUILDING 2</b>	1,285.00	SF	269.00	SF	20.93%
<b>TOTAL</b>	2,567.00	SF	538.00	SF	20.96%

WEST ELEVATION	WALL AREA	SF	WINDOW AREA	SF	PERCENTAGE
<b>BUILDING 1</b>	1,777.00	SF	330.00	SF	18.57%

65 FT MAXIMUM HEIGHT

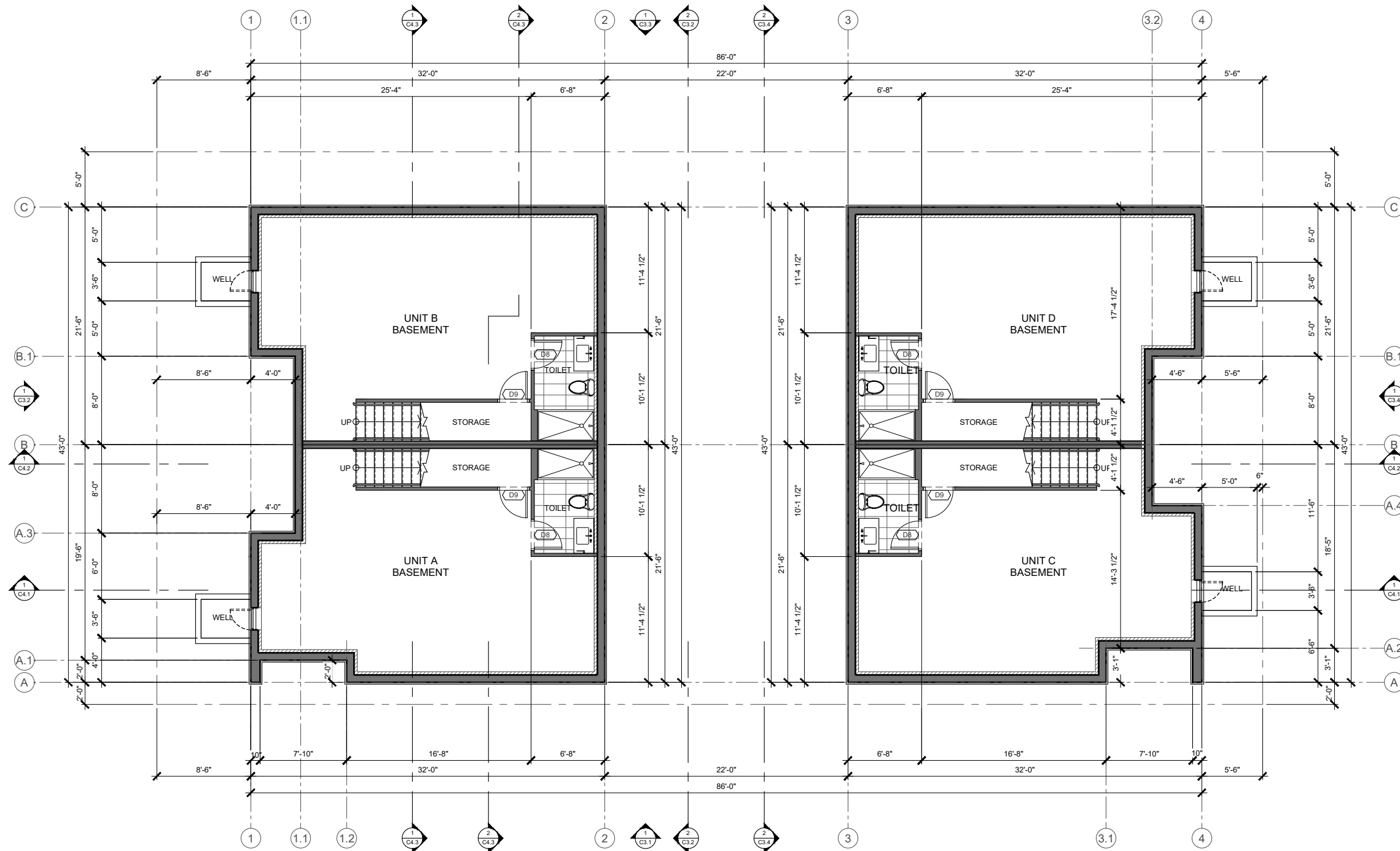


1 SOUTH ELEVATION - LOOKING N.E SCHUYLER ST - BLDG 1 & 2  
SCALE: 1/16" = 1'-0"



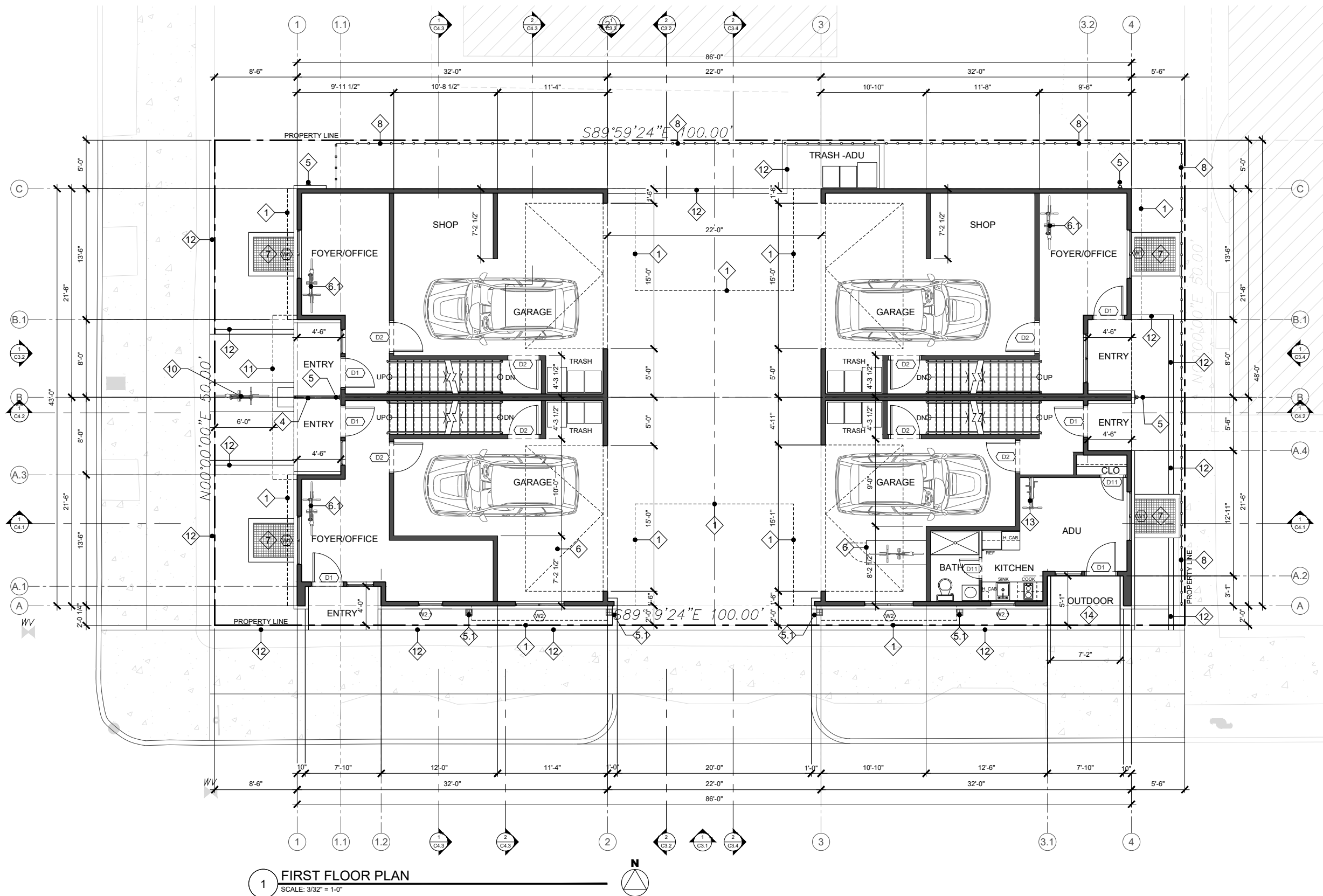
2 WEST ELEVATION - LOOKING N.E 12TH AVE - BLDG 1  
SCALE: 1/16" = 1'-0"





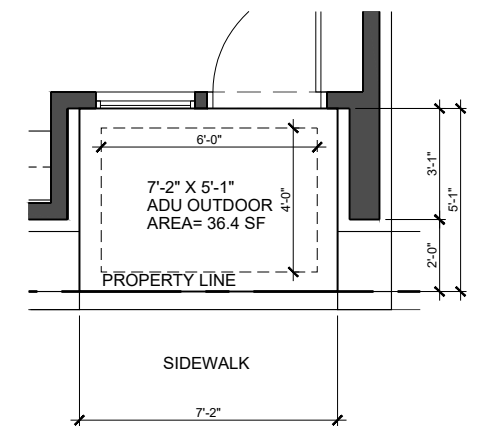
1 BASEMENT FLOOR PLAN  
SCALE: 3/32" = 1'-0"





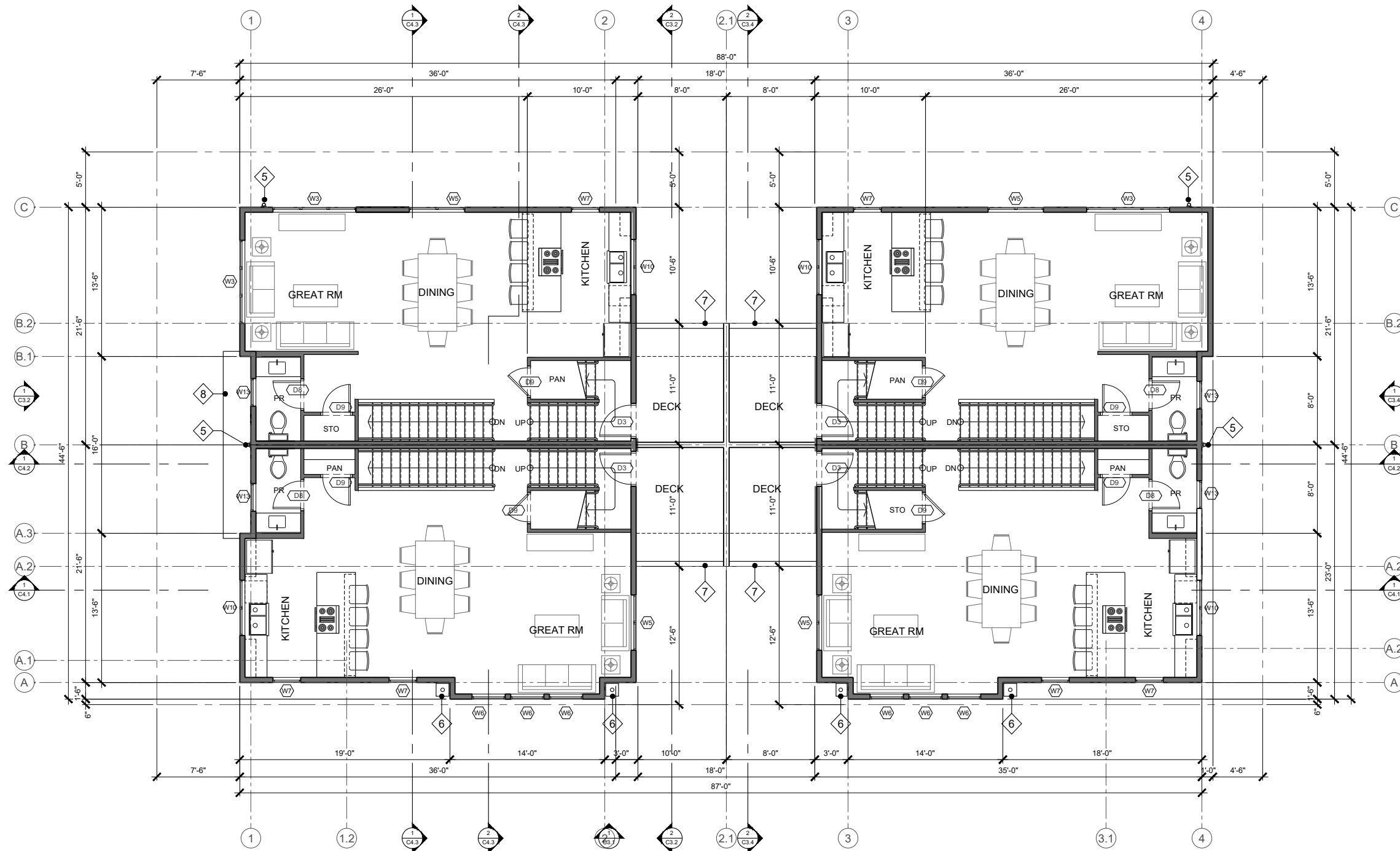
#### KEY NOTES

- 1 FLOOR ABOVE
- 2 ROOF LINE
- 3 ROOF BELOW
- 4 PRIVACY FENCE
- 5 DOWNSPOUT
- 5.1 CHAIN LINK DOWNSPOUT
- 6 24"X72" BIKE LOCKER
- 6.1 FLOOR MOUNTED BIKE RACK
- 7 GRATING REMOVABLE
- 8 6' HIGH WOODEN FENCE
- 9 NOT USED
- 10 BIKE PARKING (SHORT TIME)
- 11 CANOPY
- 12 CURB
- 13 BIKE PARKING (ADU)
- 14 7'-2" X 5'-1" ADU ENTRY/OUTDOOR  
AREA= 36.4 SF



**2 ADU ENTRY/OUTDOOR**  
SCALE: 3/16" = 1'-0"





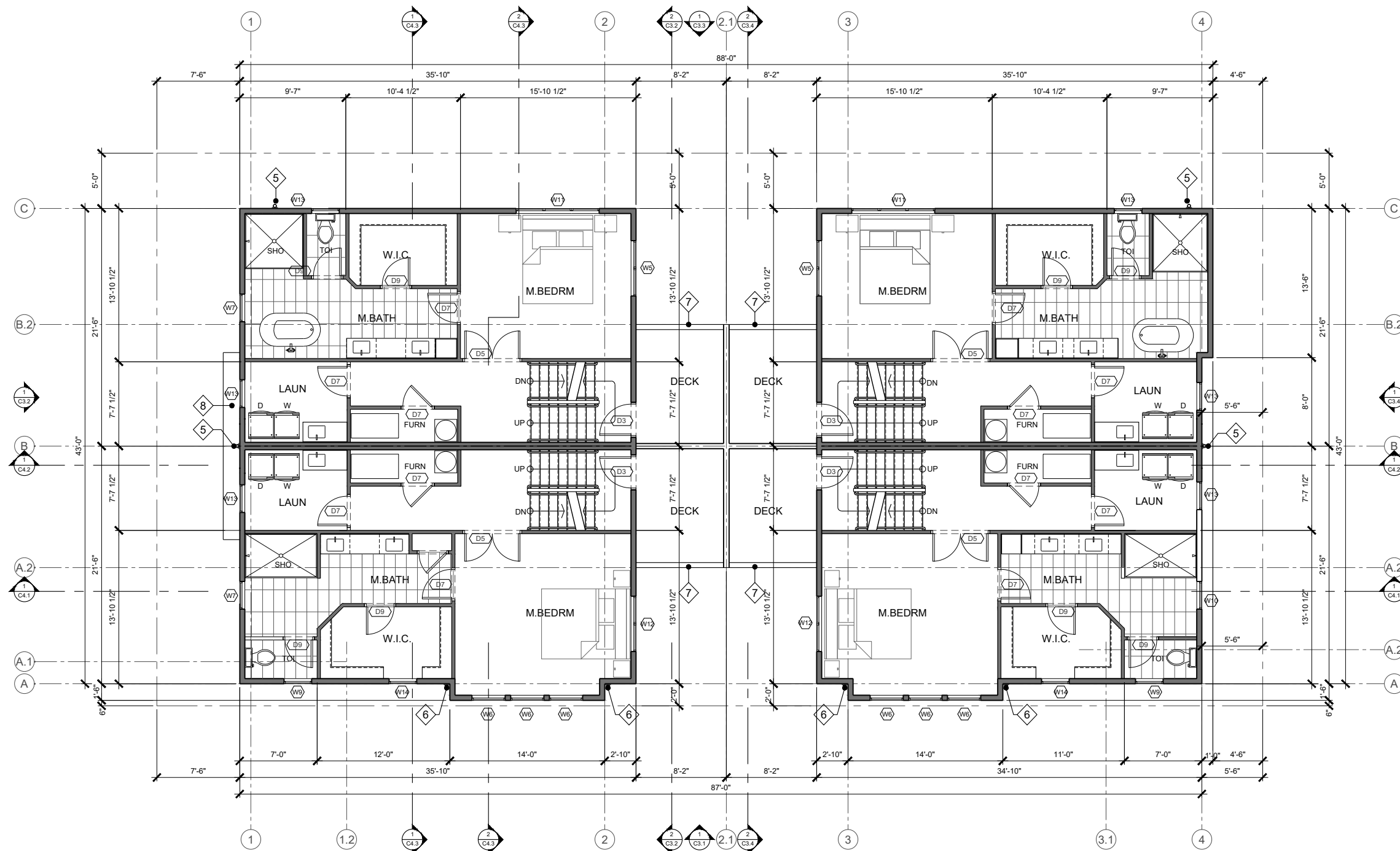
# KEY NOTES

- 1 FLOOR ABOVE
- 2 ROOF LINE
- 3 ROOF BELOW
- 4 PRIVACY FENCE
- 5 DOWNSPOUT
- 6 SCUPPER
- 7 METAL RAILING
- 8 CANOPY BELOW

1 SECOND FLOOR PLAN  
SCALE: 3/32" = 1'-0"



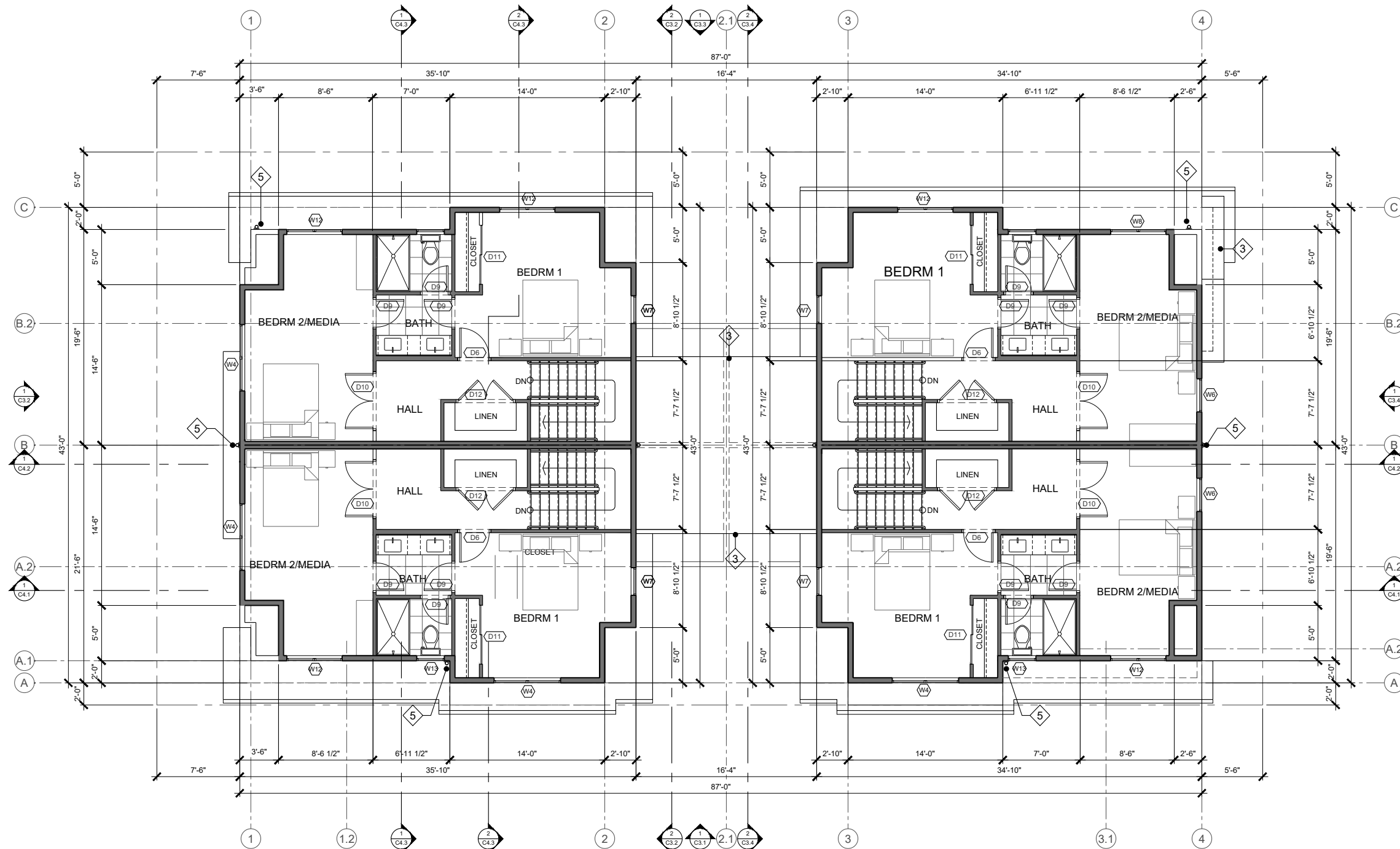




1 THIRD FLOOR PLAN  
SCALE: 3/32" = 1'-0"

- KEY NOTES**
- 1 FLOOR ABOVE
  - 2 ROOF LINE
  - 3 ROOF BELOW
  - 4 NOT USED
  - 5 DOWNSPOUT
  - 6 SCUPPER
  - 7 METAL RAILING
  - 8 CANOPY



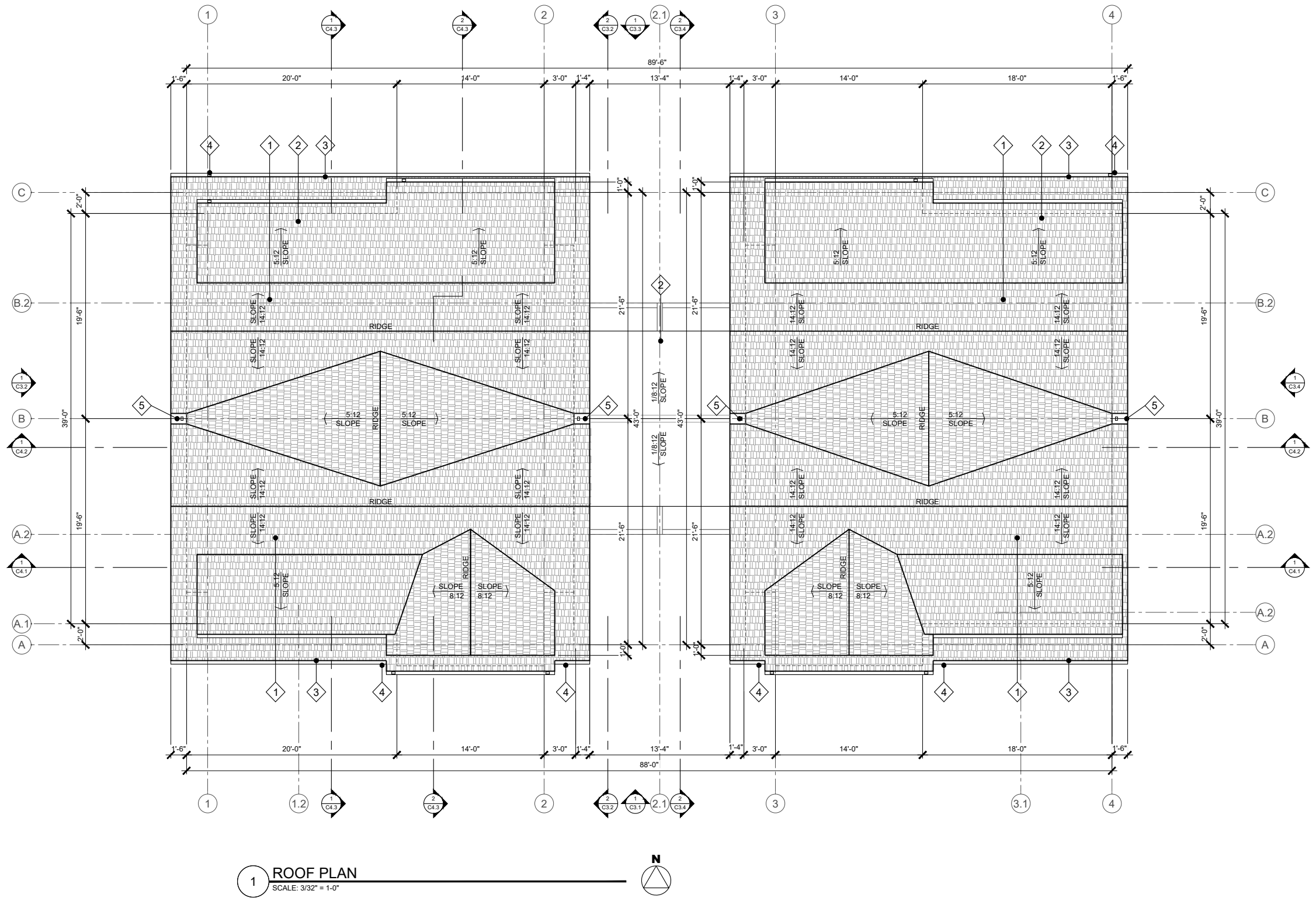


- KEY NOTES**
- 1 FLOOR ABOVE
  - 2 ROOF LINE
  - 3 ROOF BELOW PLEXIGLASS
  - 4 NOT USED
  - 5 DOWNSPOUT
  - 6 SCUPPER
  - 7 METAL RAILING

1 **FOURTH FLOOR PLAN**  
SCALE: 3/32" = 1'-0"

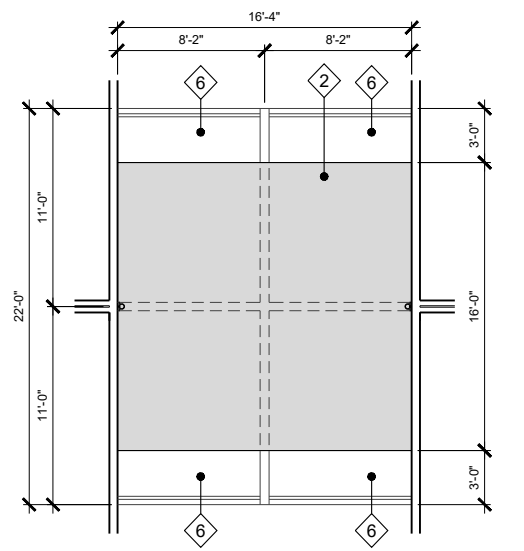






- KEY NOTES**
- 1 ARCHITECTURAL SHINGLE
  - 2 FLEXI GLASS ROOFING
  - 3 GUTTER
  - 4 DOWNSPOUT
  - 5 SCUPPER
  - 6 DECK (NO ROOF)

NOTE:  
TOTAL ROOF AREA 359.33 X .75 = 269.50 SF  
ROOF PROVIDED (PLEXIGLAS) = 261 SF



**2 DECK ROOF PLAN**  
SCALE: 3/32" = 1'-0"

▼ ELEV 65'-0"  
MAXIMUM HEIGHT

▼ ELEV 49'-0 1/2"  
T.O. ROOF

▼ ELEV 40'-6"  
4TH FLR  
T.O. PLATE

▼ ELEV 31'-6"  
4TH FLR  
T.O. SUB FLR

▼ ELEV 21'-4"  
3RD FLR  
T.O. SUB FLR

▼ ELEV 11'-2"  
2ND FLR  
T.O. SUB FLR

▼ ELEV 0'-0"  
1ST FLR  
T.O. SUB FLR

▼ ELEV -9'-5"  
BASEMENT  
T.O. CONC



## KEY NOTES

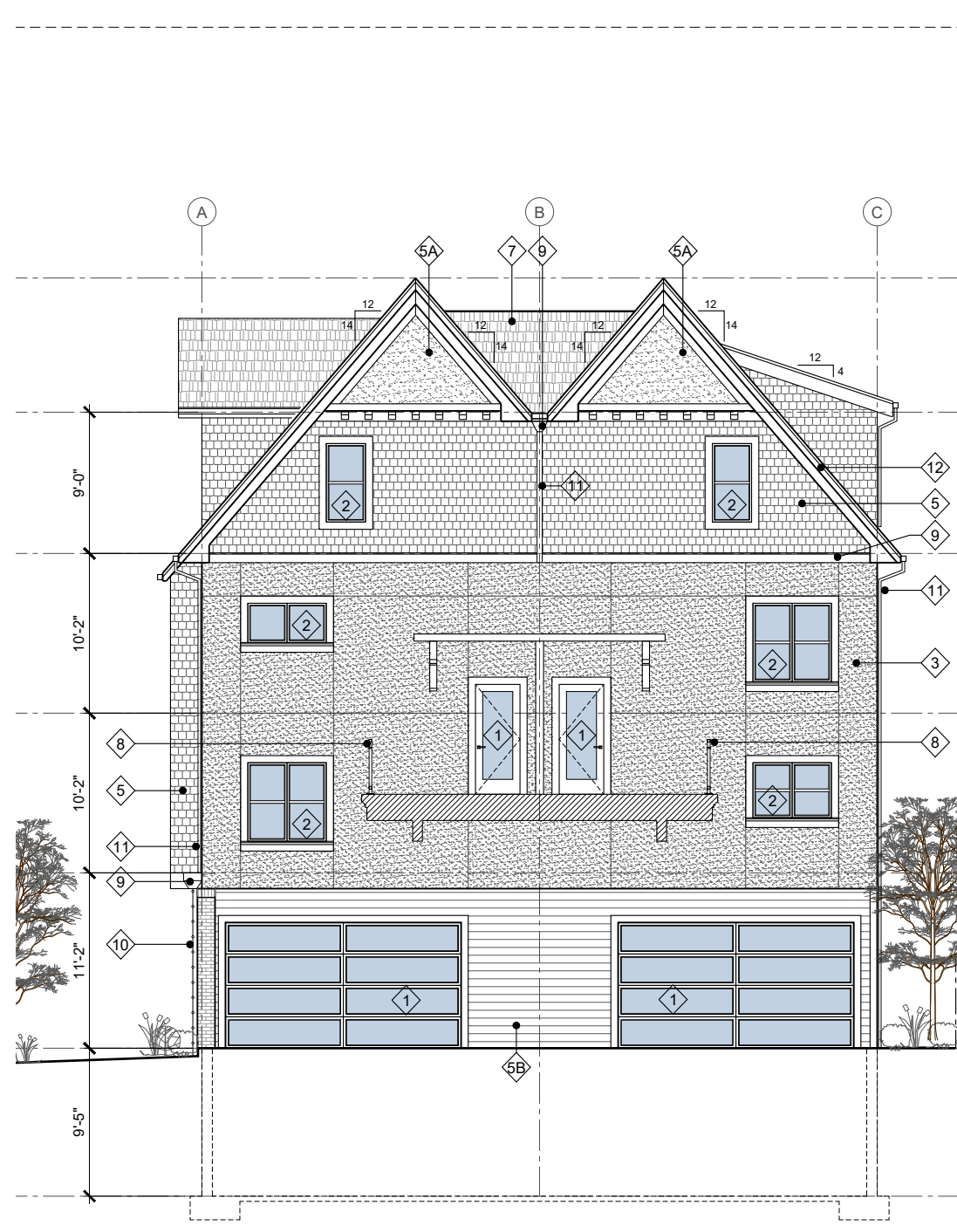
- 1 DOOR PER SCHEDULE
- 2 WINDOW PER SCHEDULE
- 3 STUCCO TYP 1  
COLOR:
- 4 BRICK  
COLOR:
- 5 FIBER CEMENT SHINGLE SIDING  
COLOR:
- 5A FIBER CEMENT PANEL SIDING  
COLOR:
- 6 HEAVY TIMBER TRELLIS WITH  
PLEXIGLAS ROOF
- 7 METAL ROOFING  
COLOR:
- 8 METAL RAILING:  
COLOR: BLACK
- 9 SCUPPER
- 10 CHAIN LINK DOWNSPOUT
- 11 3" DIA METAL DOWNSPOUT
- 12 FASCIA BOARD
- 13 ENTRY CANOPY
- 14 HEAVY TIMBER FRAME
- 15 EXTERIOR LIGHTING
- 16 WOODEN FENCE BEYOND
- 17 WROUGHT IRON RAILING BEYOND
- 18 BOXWOOD HEDGE  
SEE LANDSCAPE PLAN A6.1

1 SOUTH ELEVATION - LOOKING N.E. SCHUYLER ST - BLDG 1 & 2  
SCALE: 3/32" = 1'-0"





1 WEST ELEVATION - LOOKING N.E 12TH AVE - BLDG 1  
SCALE: 3/32" = 1'-0"



2 EAST ELEVATION - BLDG 1  
SCALE: 3/32" = 1'-0"

#### KEY NOTES

- 1 DOOR PER SCHEDULE
- 2 WINDOW PER SCHEDULE
- 3 STUCCO TYP 1  
COLOR:
- 4 BRICK  
COLOR:
- 5 FIBER CEMENT SHINGLE SIDING  
COLOR:
- 5A FIBER CEMENT PANEL SIDING  
COLOR:
- 5B FIBER CEMENT LAP SIDING  
COLOR:
- 6 WOOD SIDING  
COLOR:
- 7 ARCHITECTURAL SHINGLE  
COLOR:
- 8 METAL RAILING:  
COLOR: BLACK
- 9 SCUPPER
- 10 CHAIN LINK DOWNSPOUT
- 11 3" DIA METAL DOWNSPOUT
- 12 FASCIA BOARD
- 13 ENTRY CANOPY
- 14 HEAVY TIMBER FRAME
- 15 EXTERIOR LIGHTING
- 16 WOODEN FENCE BEYOND
- 17 BOXWOOD HEDGE  
SEE LANDSCAPE PLAN A6.1

▼ ELEV 65'-0"  
MAXIMUM HEIGHT



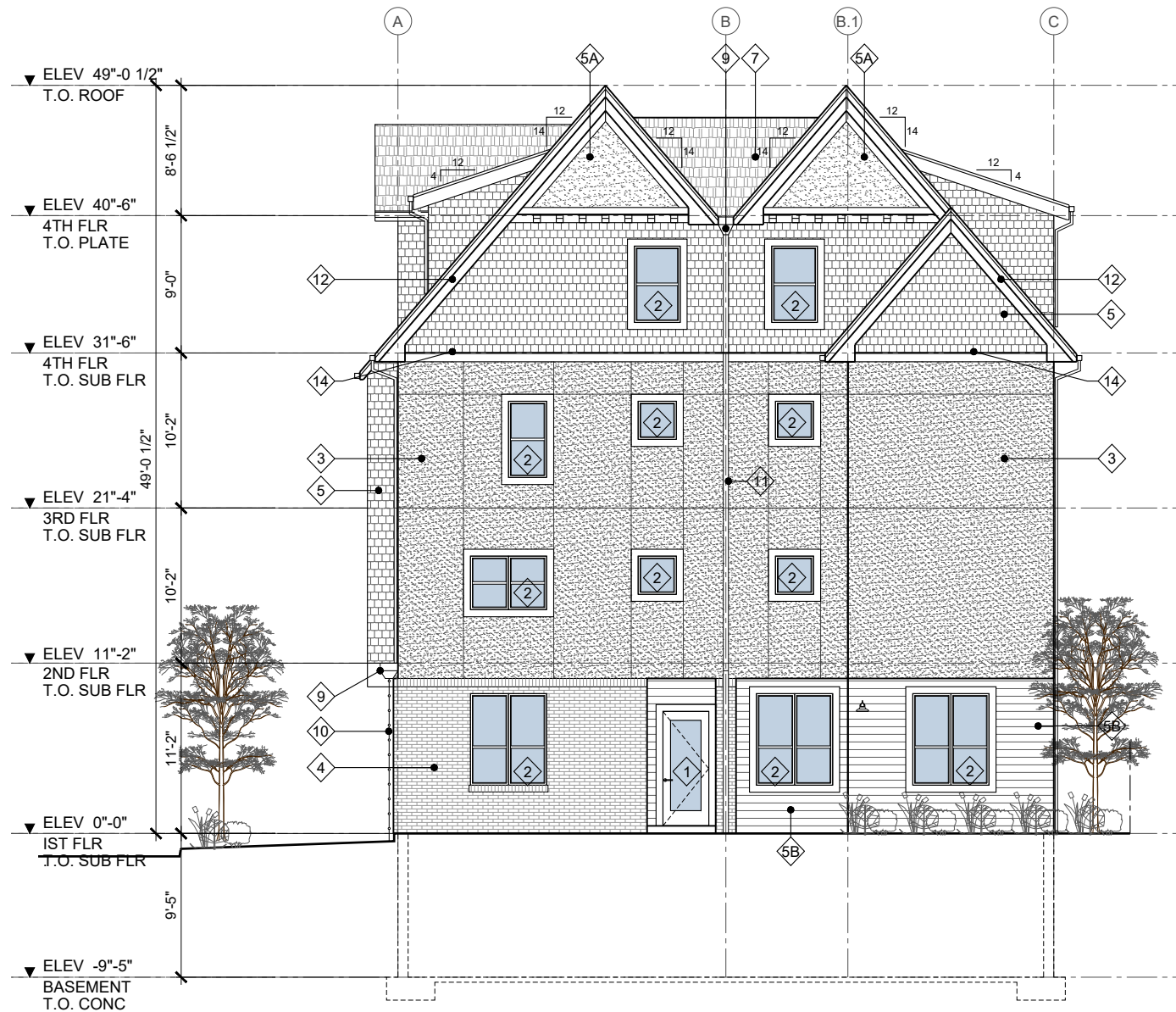
1 NORTH ELEVATION - BUILDING 1 & 2  
SCALE: 3/32" = 1'-0"

#### KEY NOTES

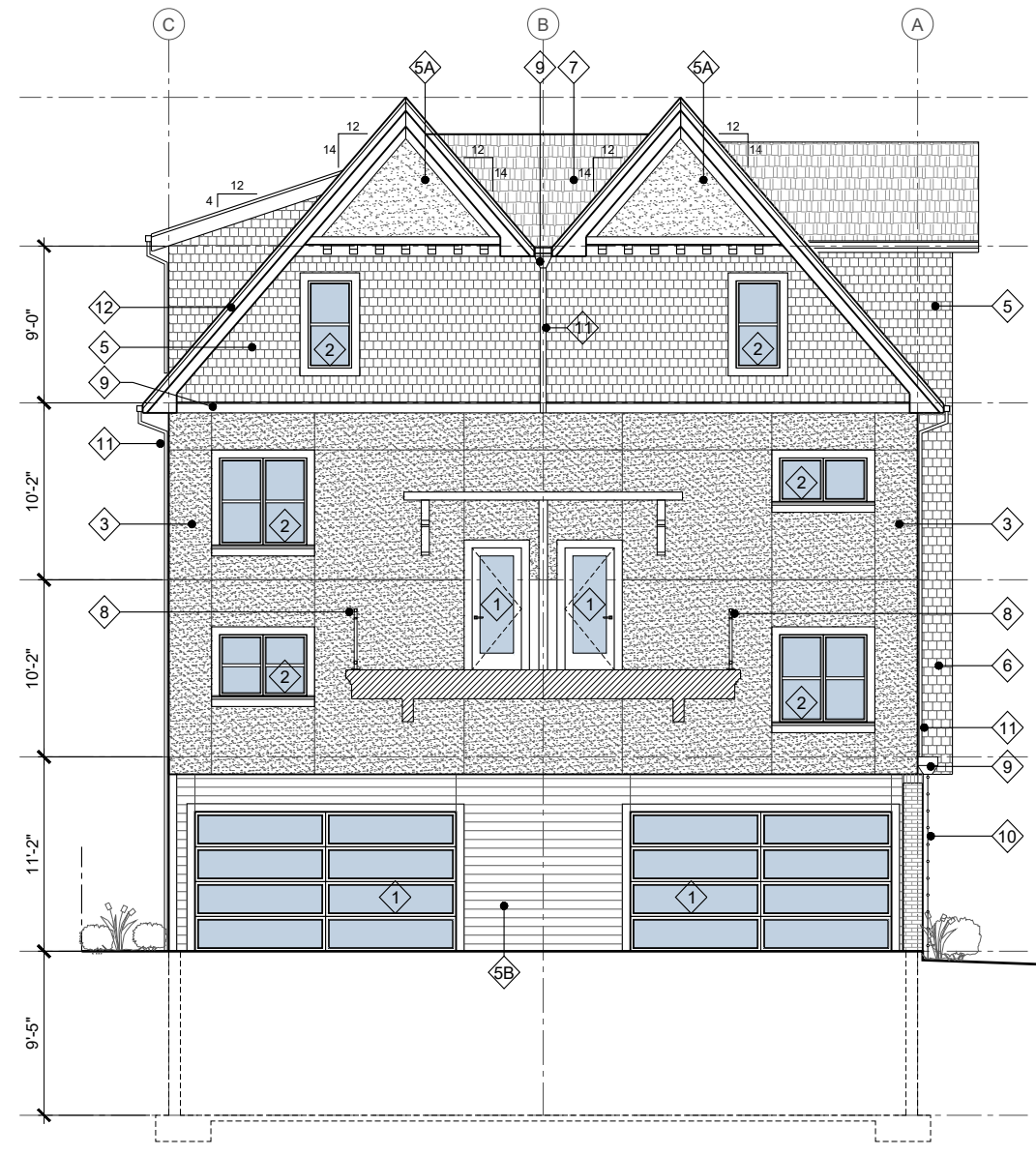
- 1 DOOR PER SCHEDULE
- 2 WINDOW PER SCHEDULE
- 3 STUCCO TYP 1  
COLOR:
- 4 BRICK  
COLOR:
- 5 FIBER CEMENT SHINGLE SIDING  
COLOR:
- 5A FIBER CEMENT PANEL SIDING  
COLOR:
- 5B FIBER CEMENT LAP SIDING  
COLOR:
- 6 HEAVY TIMBER TRELLIS WITH  
PLEXIGLAS ROOF
- 7 METAL ROOFING  
COLOR:
- 7.1 ARCHITECTURAL SHINGLE  
COLOR:
- 8 METAL RAILING:  
COLOR: BLACK
- 9 SCUPPER
- 10 CHAIN LINK DOWNSPOUT
- 11 3" DIA METAL DOWNSPOUT
- 12 FASCIA BOARD
- 13 ENTRY CANOPY
- 14 HEAVY TIMBER FRAME
- 15 EXTERIOR LIGHTING
- 16 WOODEN FENCE BEYOND



▼ ELEV 65'-0"  
MAXIMUM HEIGHT



1 EAST ELEVATION - BUILDING 2  
SCALE: 3/32" = 1'-0"



2 WEST ELEVATION - BUILDING 2  
SCALE: 3/32" = 1'-0"

#### KEY NOTES

- 1 DOOR PER SCHEDULE
- 2 WINDOW PER SCHEDULE
- 3 STUCCO TYP 1  
COLOR:
- 4 BRICK  
COLOR:
- 5 FIBER CEMENT SHINGLE SIDING  
COLOR:
- 5A FIBER CEMENT PANEL SIDING  
COLOR:
- 5B FIBER CEMENT LAP SIDING  
COLOR:
- 6 NOT USED
- 7 METAL ROOFING  
COLOR:
- 7.1 ARCHITECTURAL SHINGLE  
COLOR:
- 8 METAL RAILING:  
COLOR: BLACK
- 9 SCUPPER
- 10 CHAIN LINK DOWNSPOUT
- 11 3" DIA METAL DOWNSPOUT
- 12 FASCIA BOARD
- 13 ENTRY CANOPY
- 14 HEAVY TIMBER FRAME
- 15 EXTERIOR LIGHTING
- 16 WOODEN FENCE BEYOND

▼ ELEV 65'-0"  
MAXIMUM HEIGHT



MATERIAL BOARD  
1 SOUTH ELEVATION - LOOKING N.E SCHUYLER ST - BLDG 1 & 2  
SCALE: 3/32" = 1'-0"

#### KEY NOTES (COLOR CODE)

1 ALUMINUM GARAGE GARAGE DOOR  
COLOR: DARK BRONZE

2 2ND-3RD FLOOR  
RGB: 225,215,187



Sto 16015

67

2A FIBER CEMENT PANEL  
RGB: 225,215,187

3 TRIM, FASCIA BOARD  
RGB: 239,238,230

4 8632 VELOUR  
COLOR: BROWN  
MANUF: THE BELDEN BRICK COMPANY



6 PLEXIGLAS ROOFING  
75% MAX DECK COVERED

7 CEDAR SHINGLE  
COLOR: STAIN COLOR (EARLY AMERICAN)



7A T&G SIDING AND TIMBER FRAME  
COLOR: STAIN COLOR (EARLY AMERICAN)

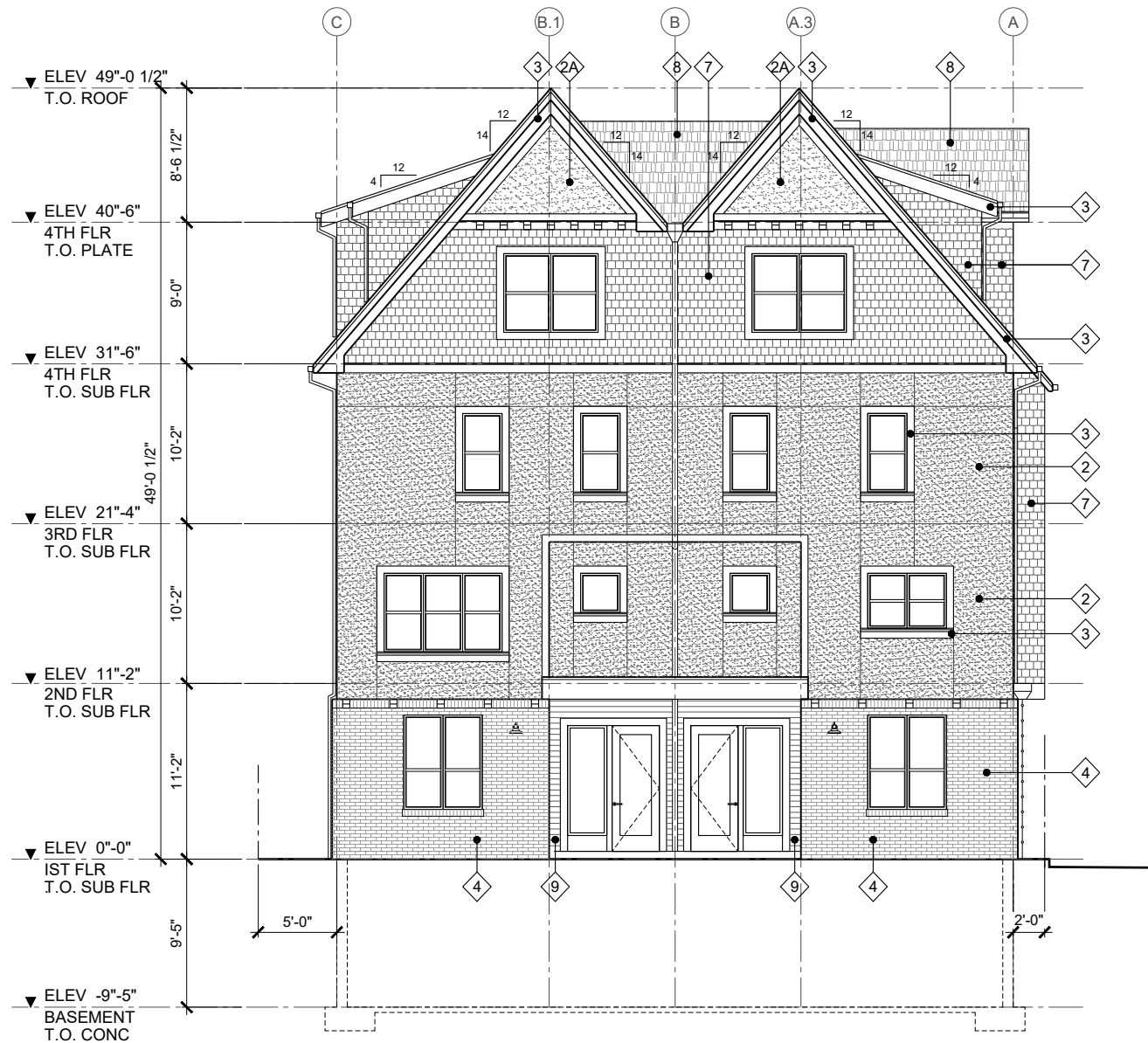


8 ARCHITECTURAL SHINGLE  
COLOR: OAKRIDGE ONYX BLACK

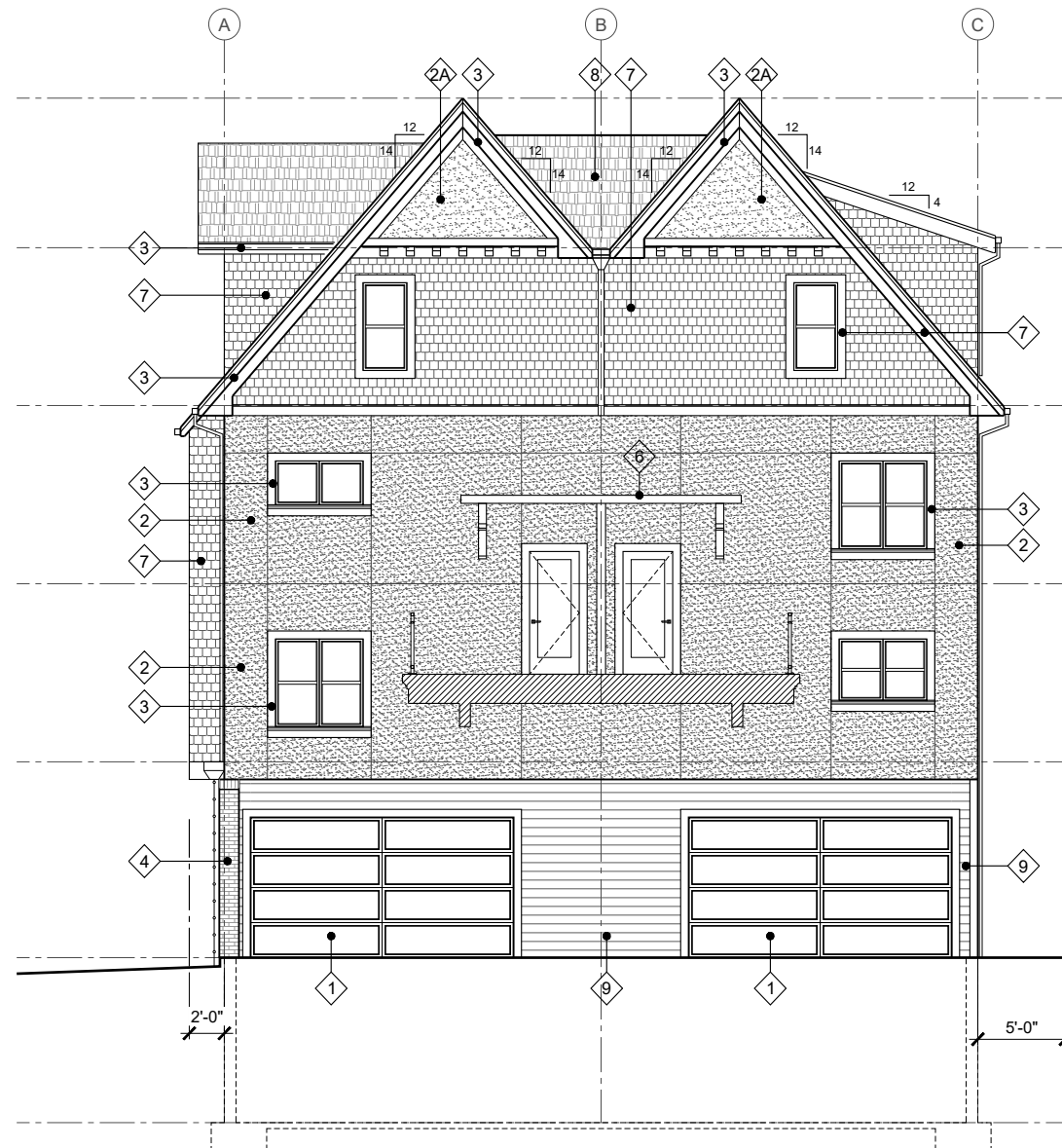
9 FIBER CEMENT LAP SIDING  
RGB: 224,198,169



▼ ELEV 65'-0"  
MAXIMUM HEIGHT



MATERIAL BOARD  
1 WEST ELEVATION - LOOKING N.E. 12TH AVE - BLDG 1  
SCALE: 3/32" = 1'-0"



MATERIAL BOARD  
2 EAST ELEVATION - BLDG 1  
SCALE: 3/32" = 1'-0"

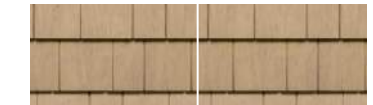
#### KEY NOTES (COLOR CODE)

- 1 ALUMINUM GARAGE GARAGE DOOR  
COLOR: DARK BRONZE
- 2 2ND-3RD FLOOR  
RGB: 225,215,187
- 2A FIBER CEMENT PANEL  
RGB: 225,215,187
- 3 TRIM, FASCIA BOARD  
RGB: 239,238,230
- 4 8632 VELOUR  
COLOR: BROWN  
MANUF: THE BELDEN BRICK COMPANY
- 6 PLEXIGLAS ROOFING  
75% MAX DECK COVERED
- 7 CEDAR SHINGLE  
COLOR: STAIN COLOR (EARLY AMERICAN)
- 7A T&G SIDING AND TIMBER FRAME  
COLOR: STAIN COLOR (EARLY AMERICAN)
- 8 ARCHITECTURAL SHINGLE  
COLOR: OAKRIDGE ONYX BLACK
- 9 FIBER CEMENT LAP SIDING  
RGB: 224,198,169

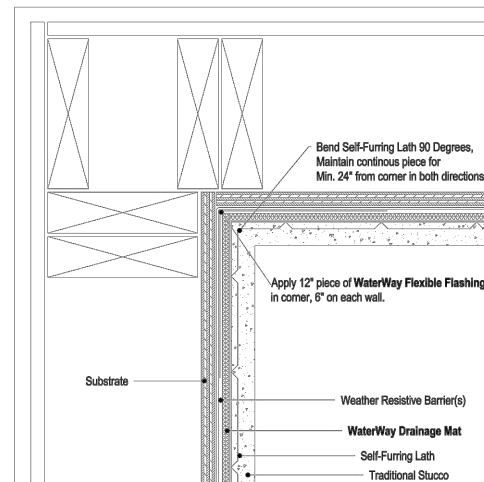


Sto 16015

67



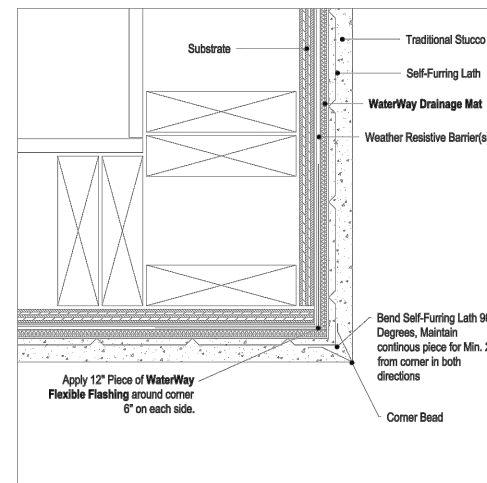
Inside Corner



**STUC-O-FLEX®**  
INTERNATIONAL, INC.  
800.305.1045  
www.stucoflex.com

Corner 1.0

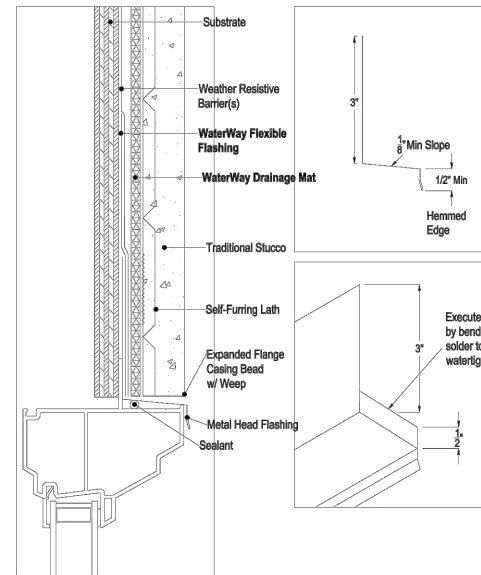
Outside Corner



**STUC-O-FLEX®**  
INTERNATIONAL, INC.  
800.305.1045  
www.stucoflex.com

Corner 2.0

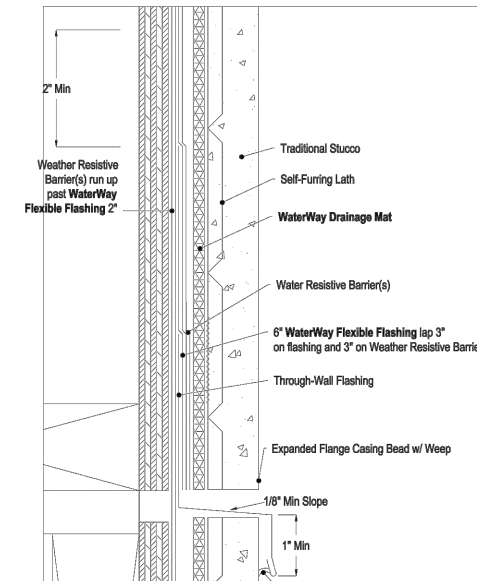
Window Head Flashing



**STUC-O-FLEX®**  
INTERNATIONAL, INC.  
800.305.1045  
www.stucoflex.com

Flashing 1.0

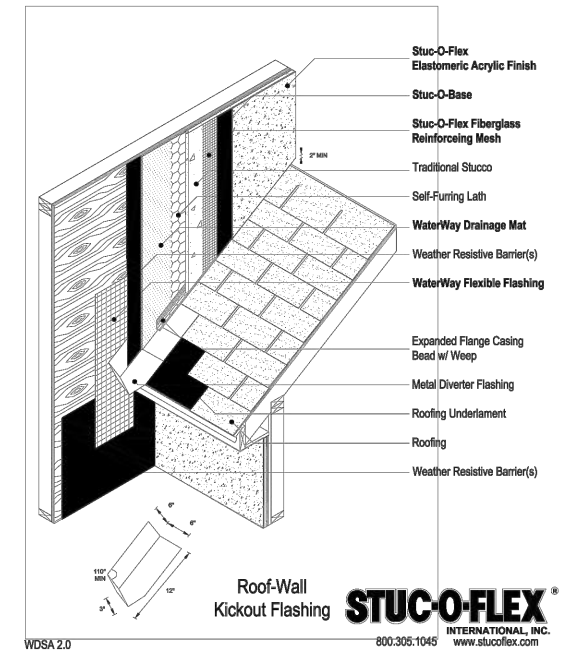
Expansion Joint



**STUC-O-FLEX®**  
INTERNATIONAL, INC.  
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Flashing 2.0

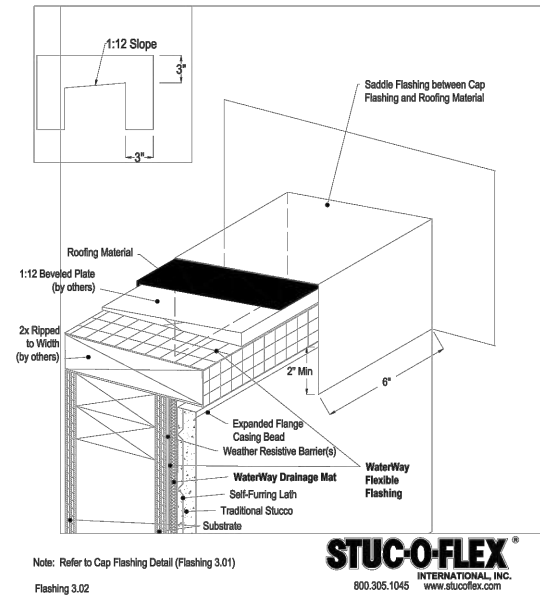
WaterWay Drainable Stucco Assembly



**STUC-O-FLEX®**  
INTERNATIONAL, INC.  
800.305.1045  
www.stucoflex.com

WDSA 2.0

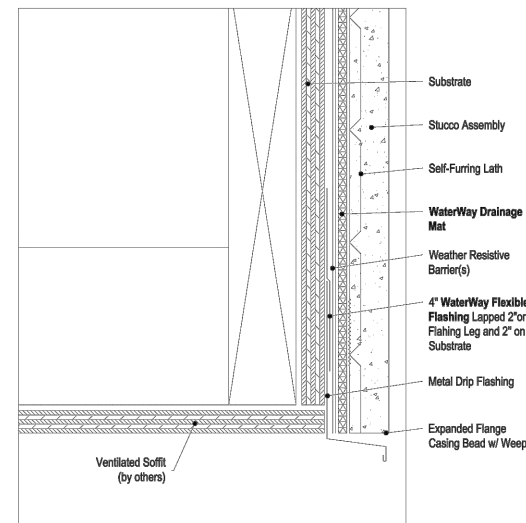
Saddle Flashing



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INTERNATIONAL, INC.  
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Note: Refer to Cap Flashing Detail (Flashing 3.01)  
Flashing 3.02

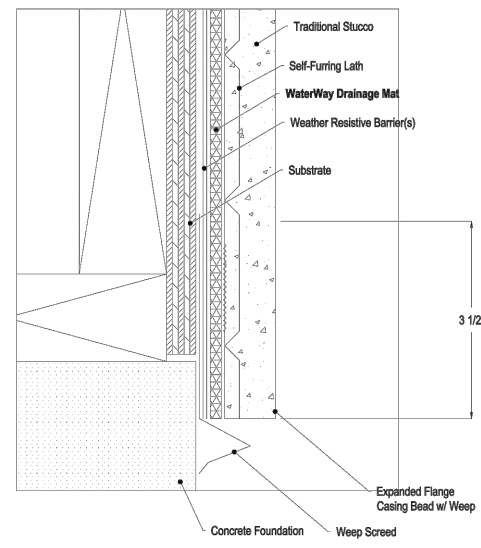
Soffit/Fascia



**STUC-O-FLEX®**  
INTERNATIONAL, INC.  
800.305.1045  
www.stucoflex.com

Soffit 1.0

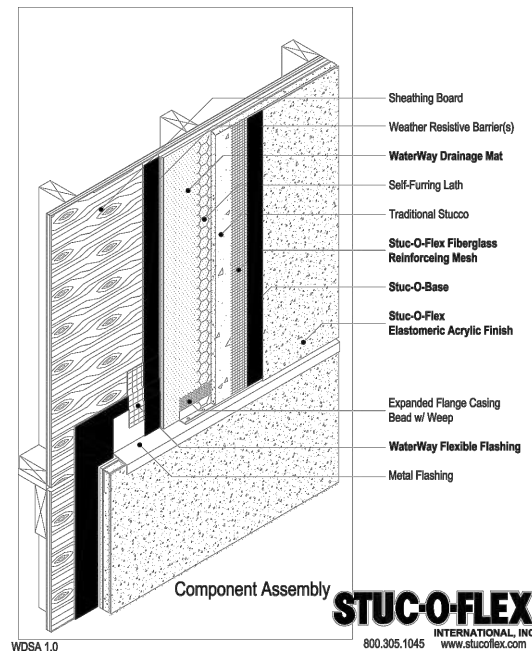
Termination At Concrete Foundation



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Terminations 1.1

WaterWay Drainable Stucco Assembly



**STUC-O-FLEX®**  
INTERNATIONAL, INC.  
800.305.1045  
www.stucoflex.com

WDSA 1.0



▼ ELEV 65'-0"  
MAXIMUM HEIGHT

▼ ELEV 49'-0 1/2"  
T.O. ROOF

▼ ELEV 40'-6"  
4TH FLR  
T.O. PLATE

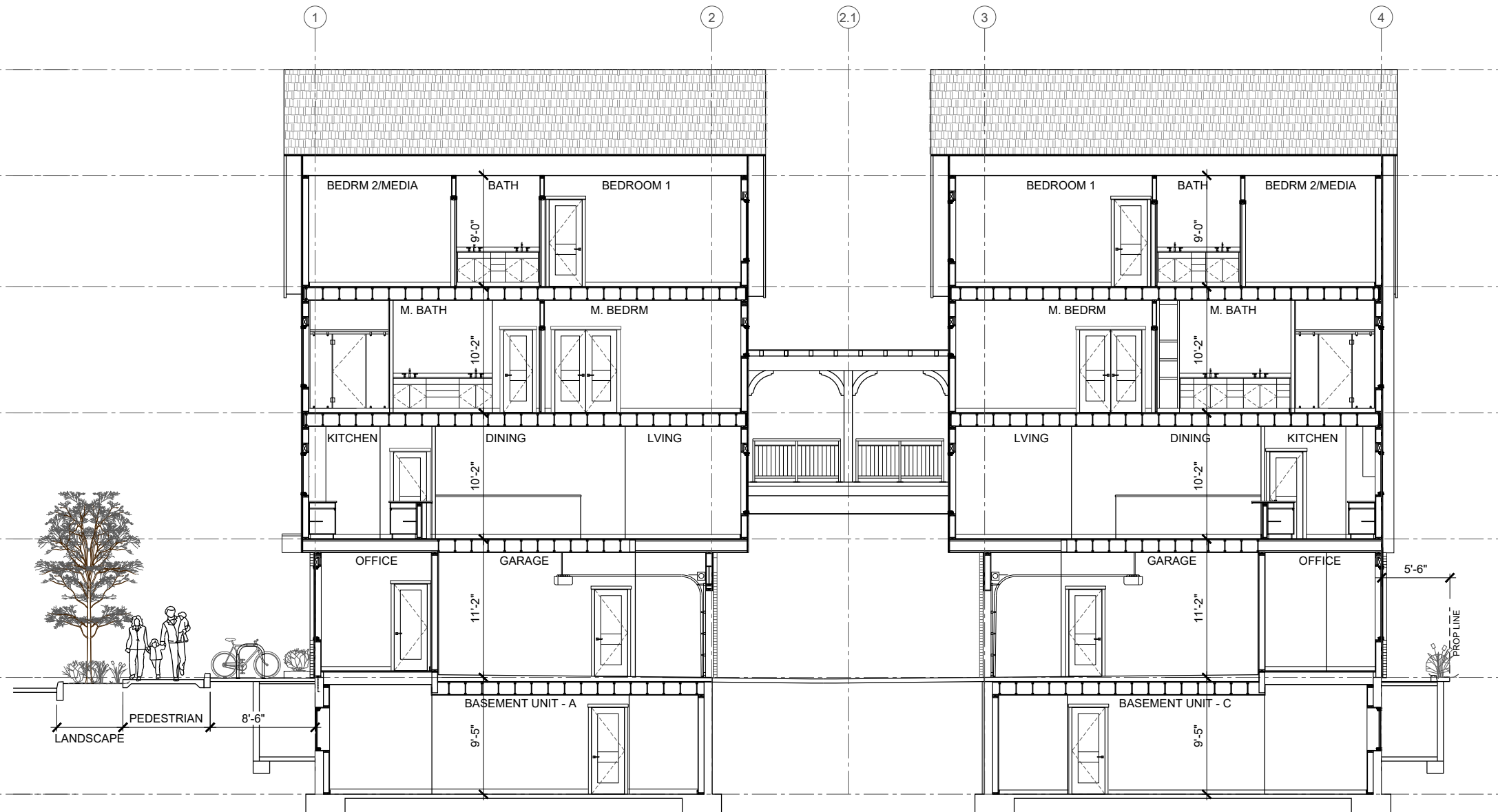
▼ ELEV 31'-6"  
4TH FLR  
T.O. SUB FLR

▼ ELEV 21'-4"  
3RD FLR  
T.O. SUB FLR

▼ ELEV 11'-2"  
2ND FLR  
T.O. SUB FLR

▼ ELEV 0'-0"  
1ST FLR  
T.O. SUB FLR

▼ ELEV -9'-5"  
BASEMENT  
T.O. CONC



1 BUILDING SECTION  
SCALE: 3/32" = 1'-0"

▼ ELEV 65'-0"  
MAXIMUM HEIGHT

▼ ELEV 49'-0 1/2"  
T.O. ROOF

▼ ELEV 40'-6"  
4TH FLR  
T.O. PLATE

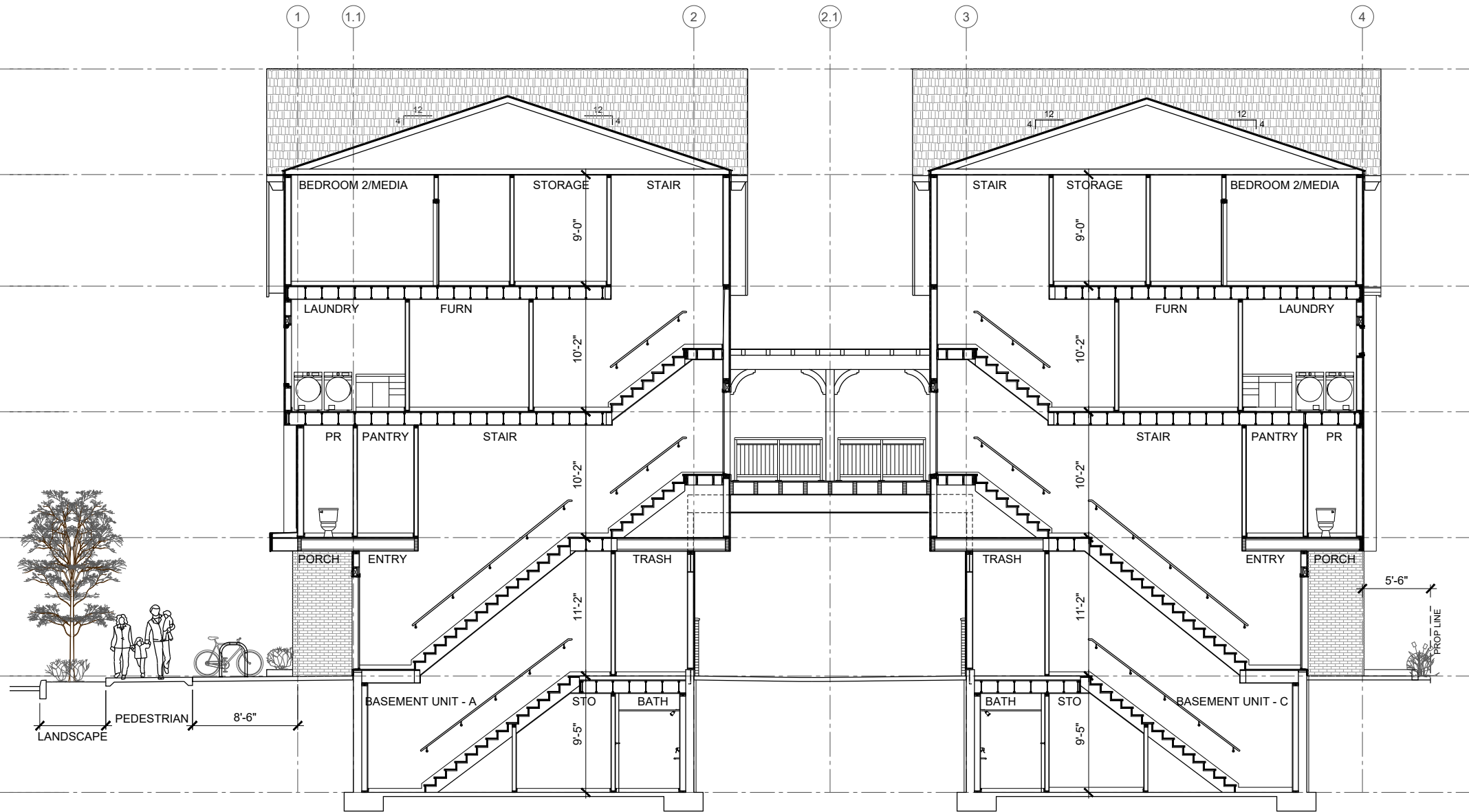
▼ ELEV 31'-6"  
4TH FLR  
T.O. SUB FLR

▼ ELEV 21'-4"  
3RD FLR  
T.O. SUB FLR

▼ ELEV 11'-2"  
2ND FLR  
T.O. SUB FLR

▼ ELEV 0'-0"  
1ST FLR  
T.O. SUB FLR

▼ ELEV -9'-5"  
BASEMENT  
T.O. CONC



1 BUILDING SECTION  
SCALE: 3/32" = 1'-0"



▼ ELEV 65'-0"  
MAXIMUM HEIGHT

▼ ELEV 49'-0 1/2"  
T.O. ROOF

▼ ELEV 40'-6"  
4TH FLR  
T.O. PLATE

▼ ELEV 31'-6"  
4TH FLR  
T.O. SUB FLR

▼ ELEV 21'-4"  
3RD FLR  
T.O. SUB FLR

▼ ELEV 11'-2"  
2ND FLR  
T.O. SUB FLR

▼ ELEV 0'-0"  
1ST FLR  
T.O. SUB FLR

▼ ELEV -9'-5"  
BASEMENT  
T.O. CONC



1 BUILDING SECTION  
SCALE: 3/32" = 1'-0"



2 BUILDING SECTION  
SCALE: 3/32" = 1'-0"



DAY TIME - RENDERING VIEW 1  
PREFER ELEVATION - OPTION A (GABLE END)





TAHRAN ARCHITECTURE & PLANNING LLC

DAY TIME - RENDERING VIEW 2  
 PREFER ELEVATION - OPTION A (GABLE END)

LOT AREA	PERCENTAGE	5,000.00 SF
BUILDING FOOTPRINT	55.04%	2,752.00 SF
PAVING	26.20%	1,310.00 SF
LANDSCAPE	18.76%	938.00 SF
<b>TOTAL</b>	<b>100.00%</b>	<b>5,000.00 SF</b>

LANDSCAPE AREA CALCULATION	5,000.00 SF
AREA 1	148.00 SF
AREA 2	425.00 SF
AREA 3	279.00 SF
AREA 4	44.00 SF
AREA 5	42.00 SF
<b>TOTAL LANDSCAPE AREA</b>	<b>938.00 SF</b>

LANDSCAPE PLAN KEYNOTES




- 1 DRIVEWAY
- 2 CONCRETE PAVING
- 3 WINDOW WELL WITH GRATING
- 4 ENTRY PORCH
- 5 ENTRY/OUTDOOR AREA - ADU
- 6 6' HIGH CEDAR FENCE
- 7 BIKE PARKING SHORT TERM
- 8 TRASH BIN - ADU

PLANTING LEGEND


TREE	CODE	COMMON NAME	CONT	CAL	QUANTITY	SIZE	HEIGHT
------	------	-------------	------	-----	----------	------	--------

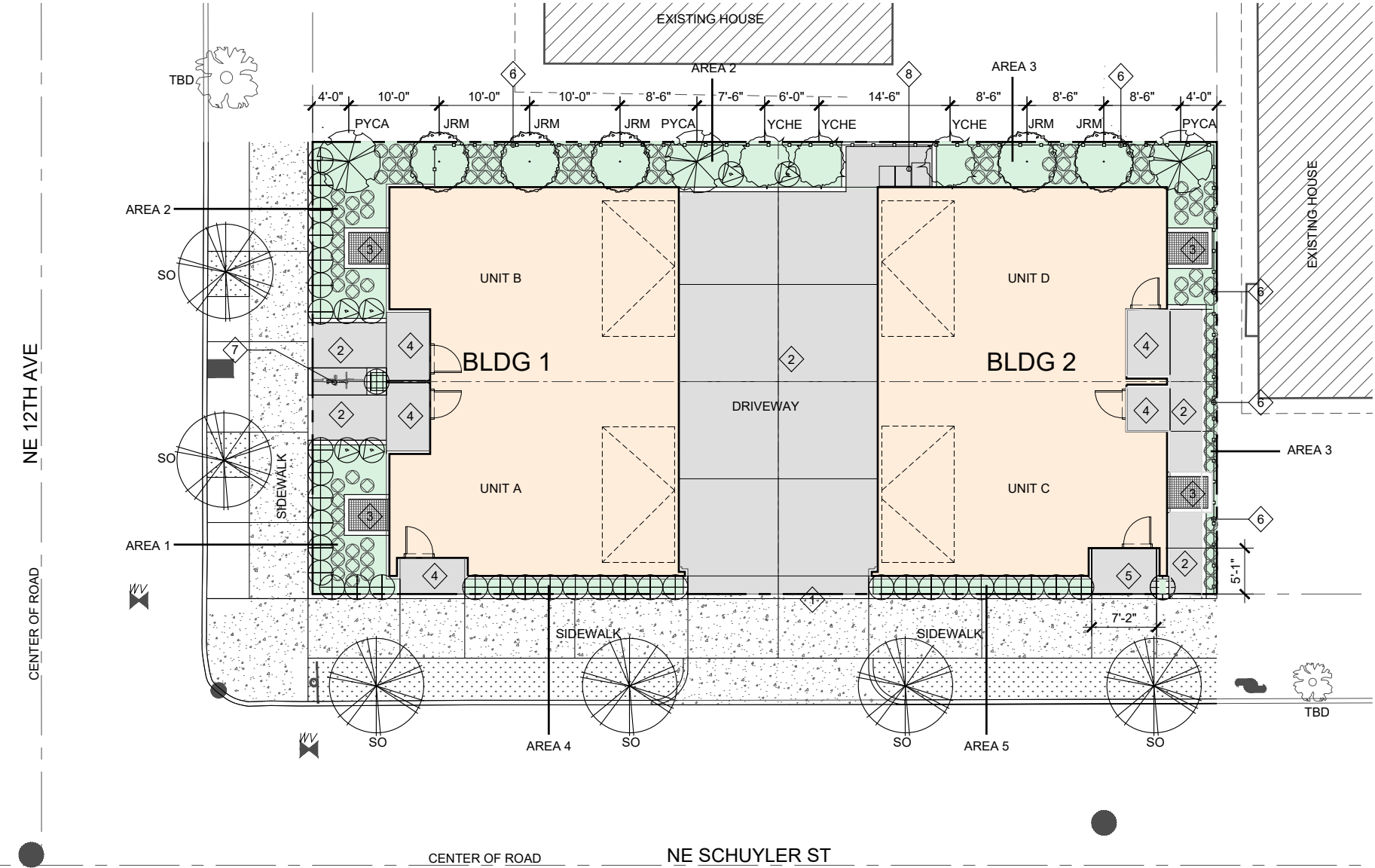
	TBD	NEW DECIDUOUSE TREE MATCH EXISTING ALONG SIDEWALK	B&B	2 CAL	X	LARGE	5FT
	SO	QUAKING ASPEN POPULUS TREMULOIDES	B&B	2 CAL	5	LARGE	5FT
	YCHE	PRUNUS X YEDOENSIS YOSHINO CHERRY	B&B	2 CAL	2	MEDIUM	5FT
	JRM	ACER X FREEMANII 'JEFFERSON RED' JEFFERSON RED MAPLE	B&B	2 CAL	5	MEDIUM	5FT
	PYCA	PYRUS CALLERYANA 'ARISTOCRAT' CALLERY PEAR	B&B	2 CAL	3	MEDIUM	5FT

SHRUBS	CODE	COMMON NAME	CONT	SPACING	QUANTITY
--------	------	-------------	------	---------	----------

	PF	POTENTILLA FRUTICOSA CVS.	3 GAL	3' O.C.	2
	DAP	DAPHNE X BURKWOODII BURKWOOD DAPHNE	3 GAL	5' O.C.	6
	BUXUS	BUXUS SEMPERVIRENS 'Graham Blandy' COMMON BOXWOOD	3 GAL	18' O.C.	TBD

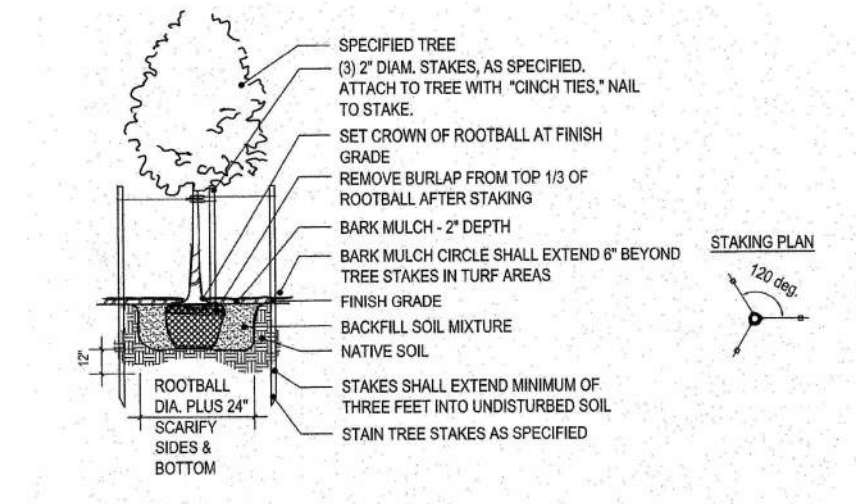
GROUND COVER	CODE	COMMON NAME	CONT	SPACING	QUANTITY
--------------	------	-------------	------	---------	----------

	FGE	FESTUCA GLAUCA 'ELIJAH BLUE'	1 GAL	12" O.C.	TBD
---	-----	------------------------------	-------	----------	-----

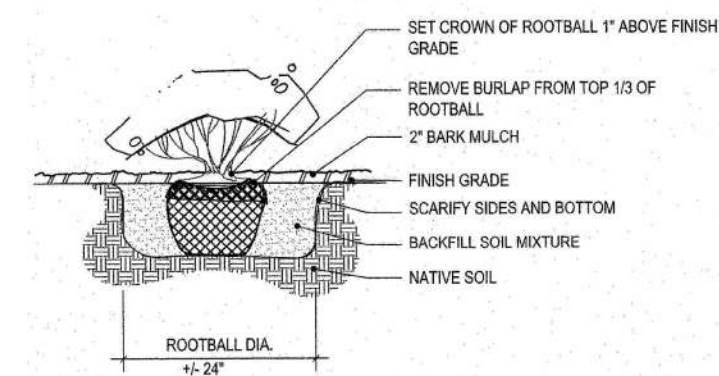


1 LANDSCAPE PLAN  
SCALE: 1/16" = 1'-0"

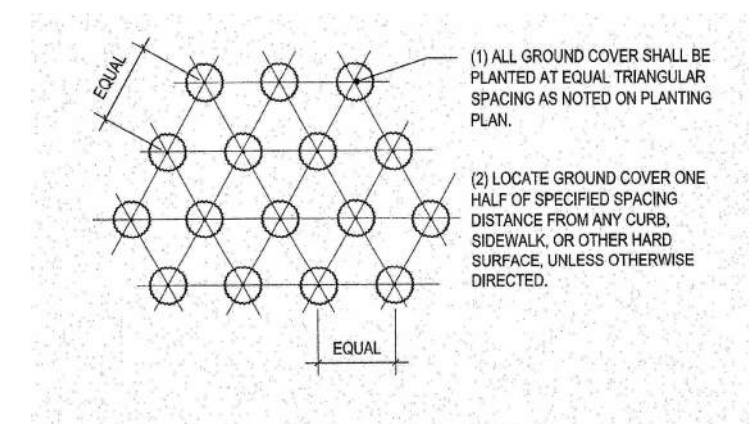




3 DECIDUOUS TREE DETAIL  
SCALE N.T.S.



2 SHRUB PLANTING DETAIL  
SCALE N.T.S.



1 GROUND COVERING PLANTING DETAIL  
SCALE N.T.S.

## PLANTING IMAGES



QUAKING ASPEN  
POPULUS TREMULOIDES



PRUNUS X YEDOENSIS  
YOSHINO CHERRY



ACER X FREEMANII  
'JEFFERSON RED'



POTENTILLA FRUTICOSA CVS.



DAPHNE X BURKWOODII  
BURKWOOD DAPHNE



BUXUS SEMPERVIRENS  
COMMON BOXWOOD

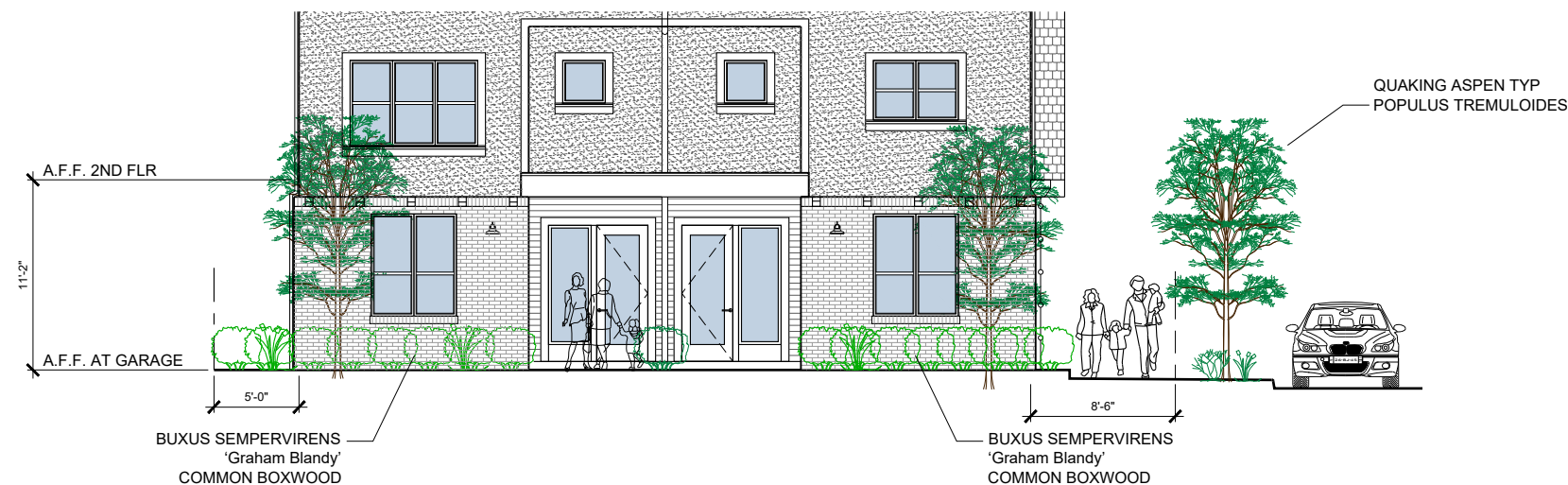


FESTUCA GLAUCA 'ELIAH BLUE'





2 BUILDING ELEVATION LOOKING NE SCHUYLER ST  
SCALE: 1/16" = 1'-0"



1 BUILDING ELEVATION NE 12TH AVE  
SCALE: 1/16" = 1'-0"



DOOR SCHEDULE

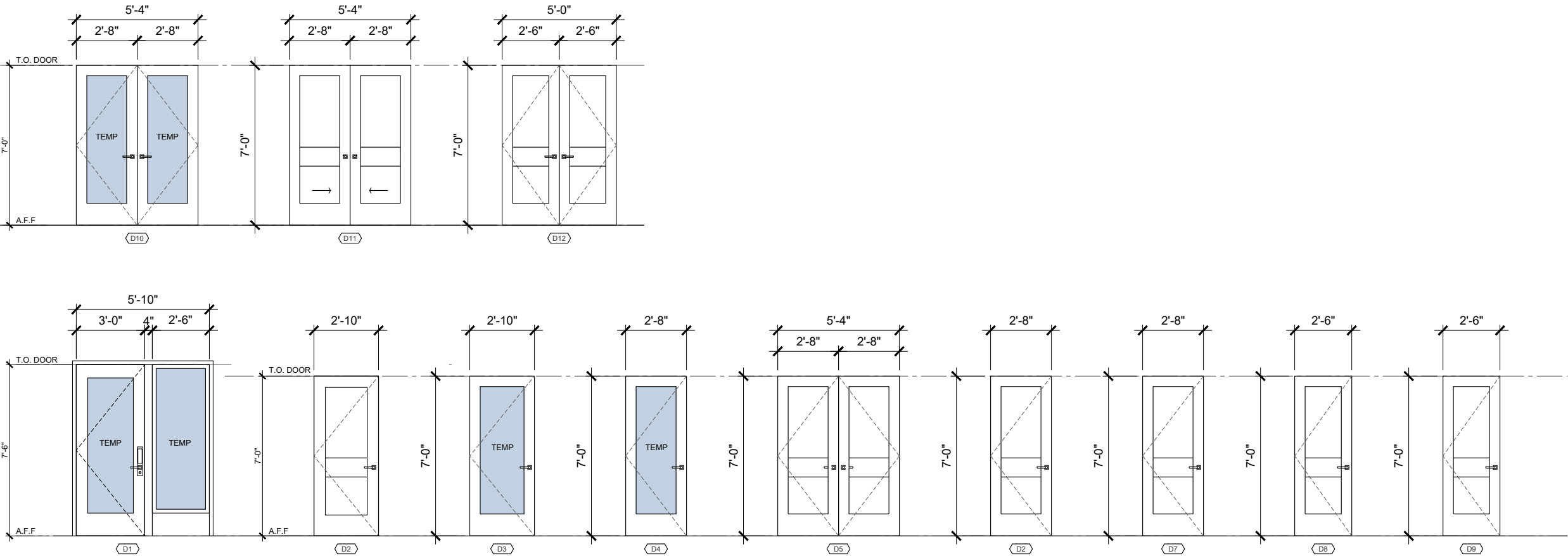
DOOR MARK	LOCATION	SIZE		NO. LEAVES	TYPE	DOOR			FRAME		DETAIL			HDWR GRP	FIRE RATING	REMARKS
		WIDTH	HEIGHT			MAT'L	CORE	FINISH	MAT'L	FINISH	HEAD	JAMB	SILL/BOTTOM			
D1	MAIN ENTRY	6'-0"	7'-6"	1	A	WD	SC	P	WD	P	.	.	.	.	1	COMPOSITE WITH TEMP GLASS
D2	GARAGE	2'-10"	7'-0"	1	A	WD	SC	P	WD	P	.	.	.	.	2	WOOD VENEER SOLID CORE
D3	PATIO	2'-10"	7'-0"	1	A	WD	F.G. I	P	WD	P	.	.	.	.	3	COMPOSITE WITH TEMP GLASS
D4	OFFICE	2'-8"	7'-0"	1	A	WD	SC	P	WD	P	.	.	.	.	5	SOLID WOOD WITH GLASS
D5	M. BEDROOM	5'-4"	7'-0"	2	A	WD	SC	P	WD	P	.	.	.	.	5	WOOD VENEER SOLID CORE
D6	BEDROOM	2'-8"	7'-0"	1	A	WD	SC	P	WD	P	.	.	.	.	5	WOOD VENEER SOLID CORE
D7	LAUNDRY/M. BATH/FURN	2'-8"	7'-0"	1	A	WD	SC	P	WD	P	.	.	.	.	6	WOOD VENEER SOLID CORE
D8	BATH	2'-6"	7'-0"	1	A	WD	SC	P	WD	P	.	.	.	.	4	WOOD VENEER SOLID CORE
D9	W.I.C/STORAGE/PANTY	2'-6"	7'-0"	1	B	WD	SC	P	WD	P	.	.	.	.	6	WOOD VENEER SOLID CORE
D10	BEDROOM 2/MEDIA	5'-4"	7'-0"	2	B	WD	SC	P	WD	P	.	.	.	.	.	SOLID WOOD WITH GLASS
D11	CLOSET	5'-4"	7'-0"	2	C	WD	SC	P	WD	P	.	.	.	.	6	WOOD VENEER SOLID CORE
D12	LINEN	5'-0"	7'-0"	1	A	WD	SC	P	WD	P	.	.	.	.	8	WOOD VENEER SOLID CORE

LEGEND

ALUM	- ALUMINUM	DOOR TYPE
HC	- HOLLOW CORE	A. SWING
INSUL	- INSULATION	B. DOUBLE SWING
SC	- SOLID CORE	C. BI-PASS
S	- STAIN	
TEMP	- TEMPERED	
F.G. I	- FIBERGLASS INSULATED	
WD	- WOOD	
FACT	- FACTORY	
P	- PAINT	
MTL	- METAL	

DOOR HARDWARE SCHEDULE

HARDWARE SET 1		HARDWARE SET 4	
EACH DOOR HAVE:		EACH DOOR HAVE:	
5	HINGE 4.5 "x4.5"	5	HINGE 4.5 "x4.5"
2	PUSH-FULL HANDLE	1	PRIVACY LOCKSET LEVER STYLE
1	ELECTRONIC SMART DOOR LOCK (RIGTH)	1	WALL STOP
1	SILL SWEEP STRIPS	HARDWARE SET 5	
1	MEETING STILES ON PAIRS	EACH DOOR HAVE:	
1	SECURITY LOCK/DEAD LOCK, ACTIVE LEAVE	5	HINGE 4.5 "x4.5"
1	DOOR BOTTOM	1	PRIVACY LOCKSET LEVER KEYED ENTRY STYLE
1	THRESHOLD FULL LENGTH	1	WALL STOP
1	WALL STOP	HARDWARE SET 6	
HARDWARE SET 2		EACH DOOR HAVE:	
EACH DOOR HAVE:		EACH DOOR HAVE:	
5	SPRING HINGE 4.5 "x4.5"	5	HINGE 4.5 "x4.5"
2	PUSH-FULL HANDLE	1	KEYLESS PASSAGE DOOR LEVER
1	ELECTRONIC SMART DOOR LOCK (RIGTH)	1	WALL STOP
1	SILL SWEEP STRIPS	HARDWARE SET 7	
1	DOOR BOTTOM	EACH DOOR HAVE:	
1	THRESHOLD 1/2" ADA FULL LENGTH	5	HINGE 4.5 "x4.5"
1	WALL STOP	1	PRIVACY LOCKSET LEVER KEYED ENTRY STYLE (RIGTH PANEL)
HARDWARE SET 3		1	DOOR LEVER INACTIVE / DUMMY
EACH DOOR HAVE:		1	FLUSH DOOR SECURITY BOLT
5	HINGE 4.5 "x4.5"	1	WALL STOP
1	PRIVACY LOCKSET LEVER STYLE	HARDWARE SET 8	
1	SILL SWEEP STRIPS	EACH DOOR HAVE:	
1	DOOR BOTTOM	1 SET SLIDING DOOR HARDWARE AND TRACK, FOR BYPASS DOORS	
1	THRESHOLD 1/2" ADA FULL LENGTH		
1	WALL STOP		



WINDOW SCHEDULE

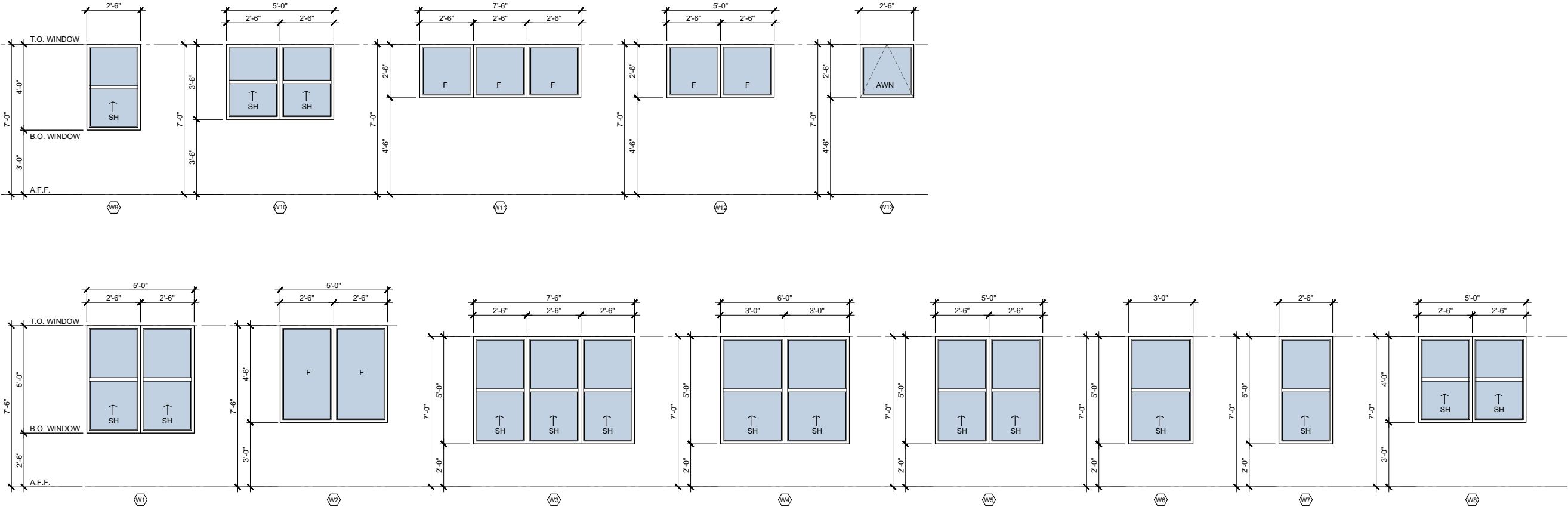
MARK	SIZE		FRAME	TYPE	GLAZING	DETAIL			STYLE	FIRE RATING LEVEL	REMARKS
	WIDTH	HEIGHT				HEAD	SILL	JAMB			
W1	5'-0"	5'-0"	FG	A	IG	.	.	.	SINGLE HUNG	NONE	.
W2	5'-0"	3'-4"	FG	A	IG	.	.	.	PICTURE	NONE	.
W3	7'-6"	5'-0"	FG	A	IG	.	.	.	SINGLE HUNG	NONE	.
W4	6'-0"	5'-0"	FG	A	IG	.	.	.	SINGLE HUNG	NONE	.
W5	5'-0"	5'-0"	FG	A	IG	.	.	.	SINGLE HUNG	NONE	.
W6	3'-0"	5'-0"	FG	A	IG	.	.	.	SINGLE HUNG	NONE	.
W7	2'-6"	5'-0"	FG	A	IG	.	.	.	SINGLE HUNG	NONE	.
W8	5'-0"	4'-0"	FG	A	IG	.	.	.	SINGLE HUNG	NONE	.
W9	2'-6"	4'-0"	FG	A	IG	.	.	.	SINGLE HUNG	NONE	.
W10	5'-0"	3'-6"	FG	A	IG	.	.	.	SINGLE HUNG	NONE	.
W11	7'-6"	2'-6"	FG	A	IG	.	.	.	PICTURE	NONE	.
W12	5'-0"	2'-6"	FG	A	IG	.	.	.	PICTURE	NONE	.
W13	2'-6"	2'-6"	FG	A	IG	.	.	.	AWNING	NONE	TEMPERED

GENERAL NOTES:


1. CONTRACTOR TO VERIFY SIZES IN THE FIELD. CONTRACTOR TO VERIFY WHERE OPERABLE SASHES OF WINDOWS ARE TO MEET CLEARANCE REQUIREMENTS AT EXHAUST OUTLETS. OPERABLE SASHES MAY HAVE TO BE ON OPPOSITE SIDES THAN SHOWN ON THESE DIAGRAMS.
2. SAFETY GLAZING REQUIRED AT THE FOLLOWING LOCATIONS:
  - GLAZING LOCATED ADJACENT TO A DOOR WHERE THE NEAREST EXPOSED EDGE IS WITHIN 24-INCH ARCH OF THE DOOR IN A CLOSED POSITION AND WHERE BOTTOM EDGE OF THE GLAZING IS LESS THAN 60 INCHES ABOVE THE WALKING SURFACE.
  - GLAZING ADJACENT TO STAIRWAYS, LANDINGS, AND RAMPS WITHIN 36 INCHES HORIZONTALLY OF A WALKING SURFACE WHERE THE EXPOSED SURFACE AT THE GLASS IS LESS THAN 60 INCHES ABOVE THE PLANE AT THE WALKING SURFACE.
3. ALL OPERABLE WINDOWS TO HAVE THE ABILITY TO HAVE OPENING LIMITERS INSTALLED AFTER WINDOW INSTALLATION.
4. ALL GLASS 3 FEET HIGH ABOVE THE FLOOR ARE ALL TEMPERED GLASS OTHERWISE NOTED.
5. SEE EXTERIOR ELEVATIONS FOR WINDOW ORIENTATION.

ABBREVIATIONS:

AW - AWNING WINDOW  
CLR - CLEAR  
CS - CASEMENT WINDOW  
HP - HOPPER WINDOW  
F - FIXED  
FG - FIBERGLASS COMPOSITE  
FS - FIXED WINDOW WITH SASH  
FW - FIXED WINDOW NO SASH  
IG - INSULATED GLASS  
SH - SINGLE HUNG WINDOW  
SL - SLIDING WINDOW  
SD - SWING WINDOW  
TEMP - TEMPERED GLASS  
TT - TILT-TURN WINDOW








# ULTRA™ SERIES

PREMIUM FIBERGLASS  
WINDOWS & PATIO DOORS



STUNNING AND MODERN WITH VISUAL IMPACT  
STRONG AND BUILT TO LAST





BUILDER MAGAZINE  
MOST USED &  
MOST RECOGNIZED  
WINDOW BRAND

2019

FIBERGLASS WINDOW  
AWARD  
WESTERN UNITED STATES

## Modern design. Stunning visual impact.

Ultra™ Series is our contemporary premium fiberglass window and patio door line that is strong and built to last.

The fiberglass material resists swelling, rotting, and warping making it the perfect choice for long-lasting durability coupled with exceptional beauty.

Awarded Most Used and Most Recognized window brand in the Western United States\*, Ultra Series offers high performing and energy efficient windows and patio doors for an upscale look that can complement any home.

\*2018 Builder Magazine Brand Choice Study

ULTRA SERIES OFFERS:

- Fiberglass frames are resistant to temperature fluctuations making them a durable, low maintenance choice
- Window hardware is attractive, secure and easy to operate
- Four exterior frame colors to enhance the look of your home
- Matching interior and exterior frames in Black Bean or Bark to create the popular dark window look
- A Full Lifetime Warranty that includes Glass Breakage coverage



MILGARD.COM 3



# Why Milgard Windows & Doors

### Quality

Milgard offers award-winning windows and patio doors built for long-lasting comfort. For over 50 years, industry experts have consistently recognized Milgard as a trusted brand, ranking us as Most Used and Most Recognized fiberglass window brand\*.

\*According to the 2019 Builder Magazine Brand Use Study in the western U.S.

### Experience

Since 1962, Milgard has designed and assembled superior custom-made windows and patio doors for replacement, remodeling and new construction. Milgard is known for their industry-leading designs, using patented technologies, that help make your home more comfortable.

### Service

We have always taken great pride in our ability to provide exemplary customer service. We believe that being close to our customers allows us to provide a better service experience. You'll find Milgard factory trained technicians near you in the Western U.S. and Canada.

### Full Lifetime Warranty

We have a dedication to quality and build our windows and patio doors to last. With Milgard, you're covered for as long as you own your home with our industry-leading Full Lifetime Warranty. See complete details at [MILGARD.COM](https://www.milgard.com)



Create the home of your dreams with the ultimate, upscale window that's exceptionally beautiful

**Single Hung**  
**HARDWARE**  
Positive Action™ Lock, Cam Lock, Sash Pull

**Casement**  
**HARDWARE**  
Folding Nesting Operator, Sash Lock

**Awning**  
**HARDWARE**  
Folding Nesting Operator, Sash Lock

**Horizontal Slider**  
**HARDWARE**  
Positive Action Lock, Cam Lock, Sash Pull

**Picture and Radius**  
**HARDWARE**  
Non-operable

**Bay and Bow**  
**HARDWARE**  
Dependent on your window selection

Positive Action Lock, Clay

Cam Lock, Bronze

Sash Pull, White

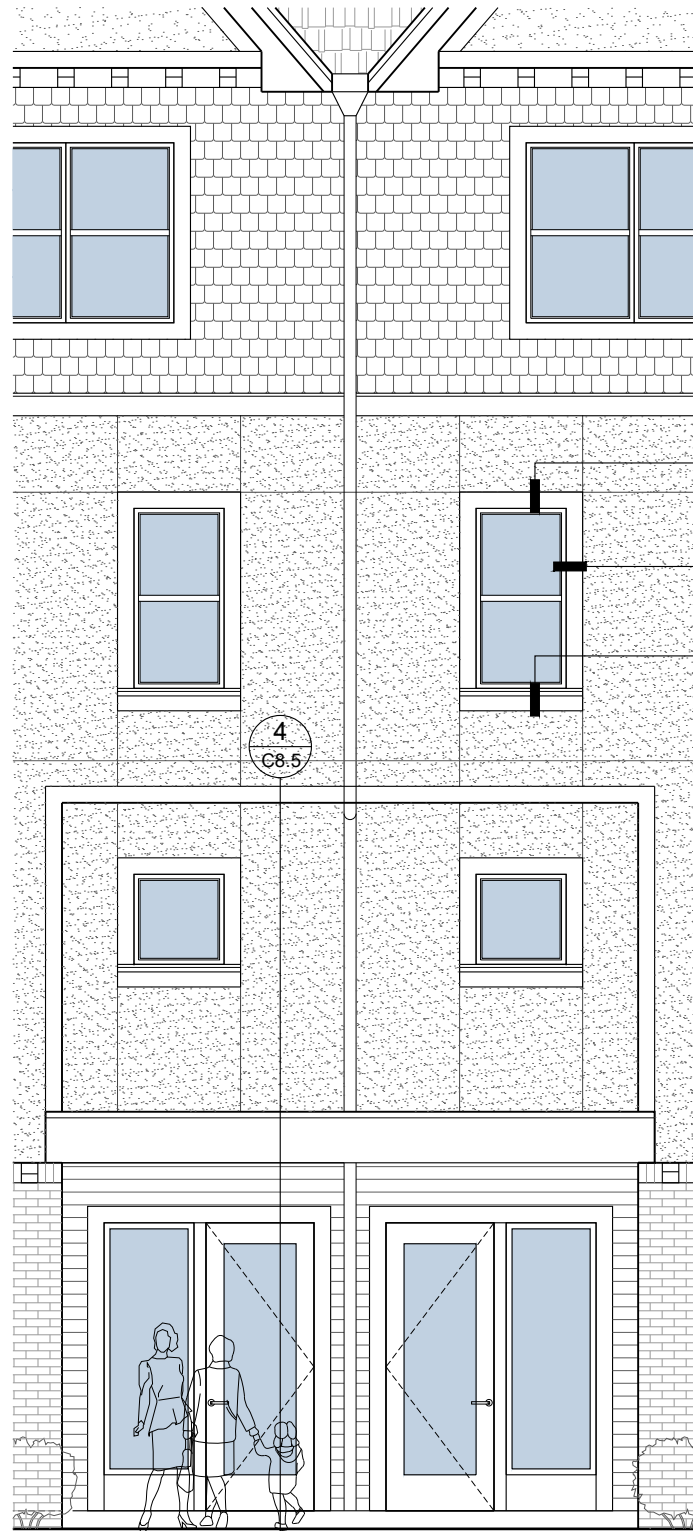
Folding Nesting Operator, Black

Sash Lock, Brushed Chrome

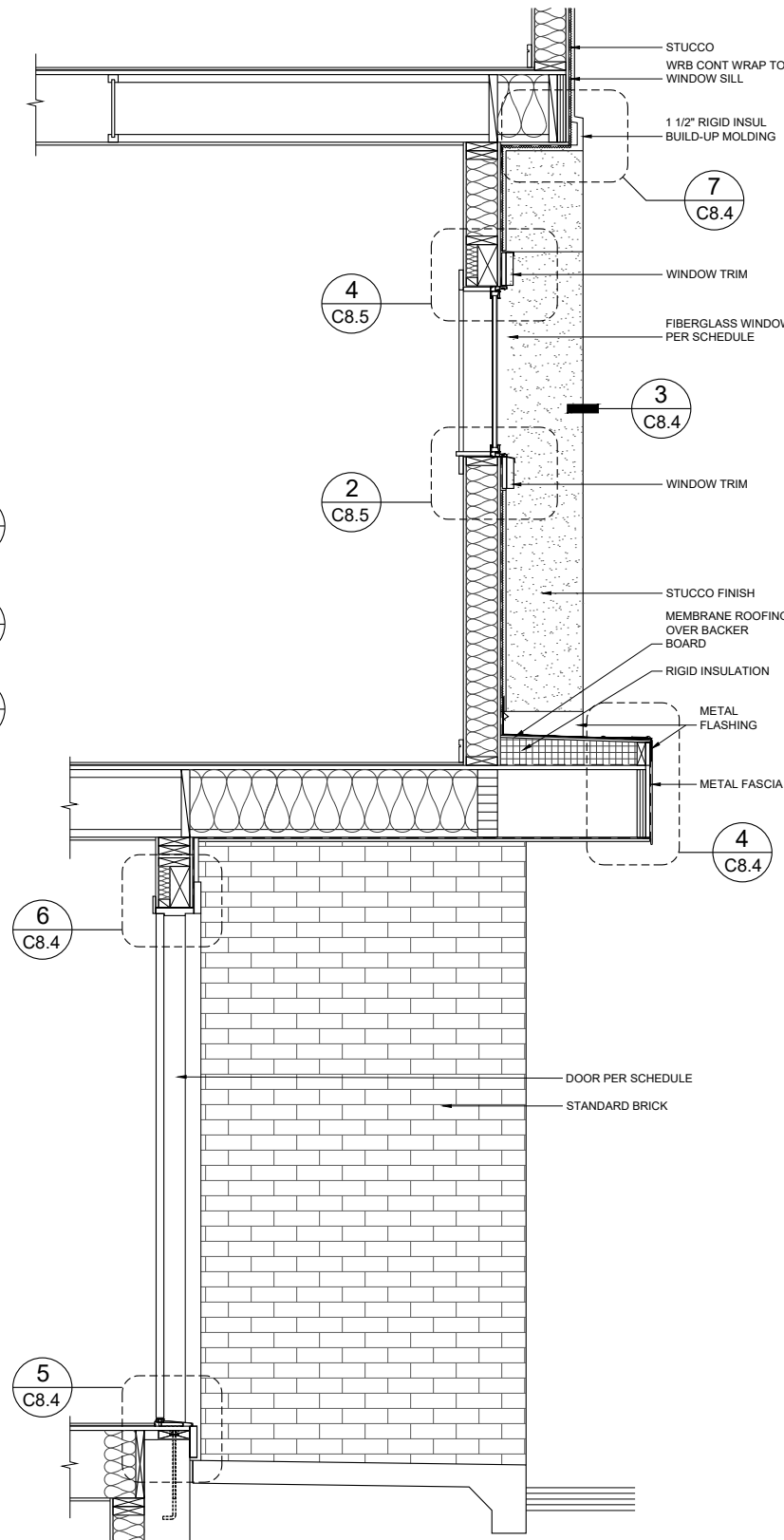
Window hardware is available in white, clay, dark bronze, black, and brushed chrome for all operating styles. Satin Nickel is available for casement and awnings only. Clay not available in all areas.



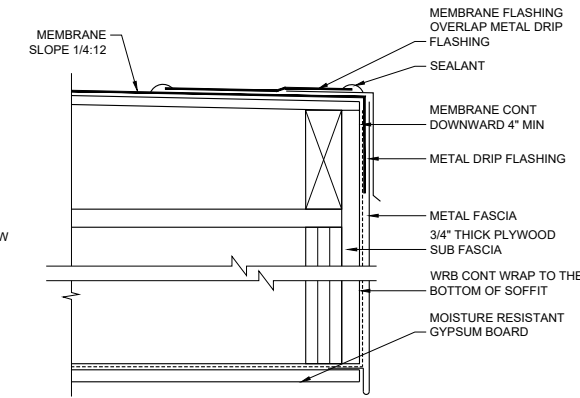




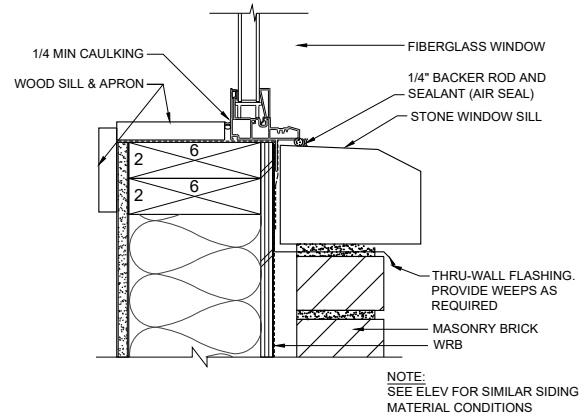
1 ENLARGE ELEVATION  
SCALE 3/16" = 1'-0"



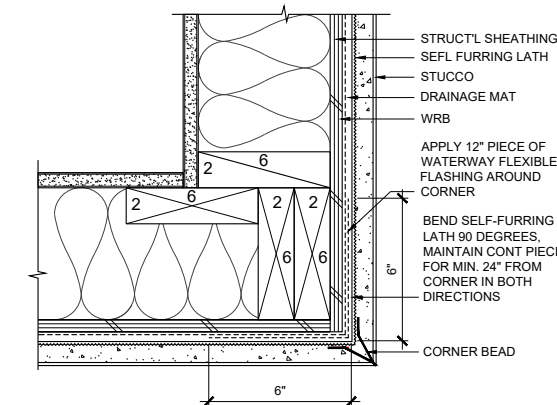
2 WALL SECTION  
SCALE 3/8" = 1'-0"



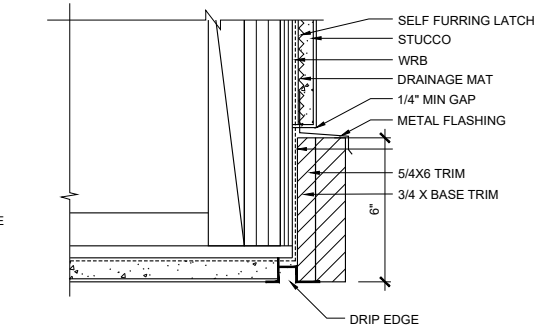
4 DETAIL SECTION  
SCALE 1 1/2" = 1'-0"



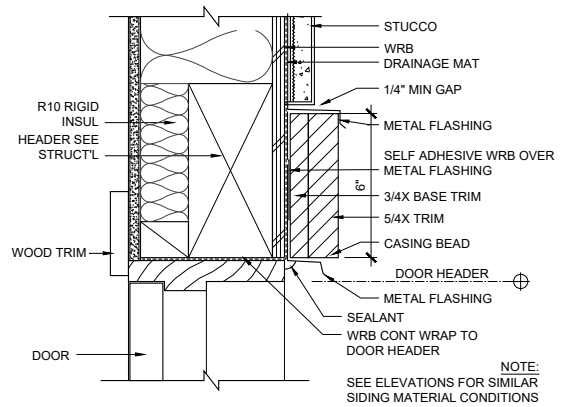
4 WINDOW SILL - BRICK  
SCALE 1 1/2" = 1'-0"



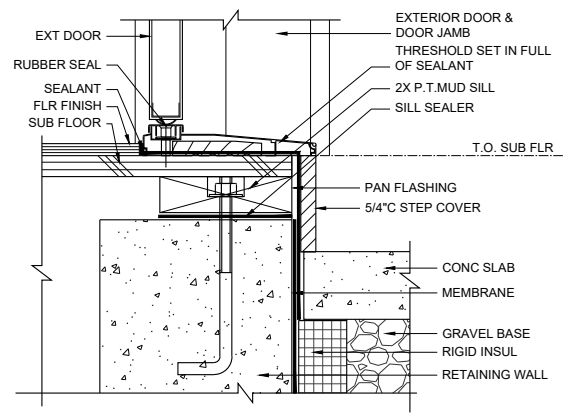
3 CORNER DETAIL - STUCCO  
SCALE 1 1/2" = 1'-0"



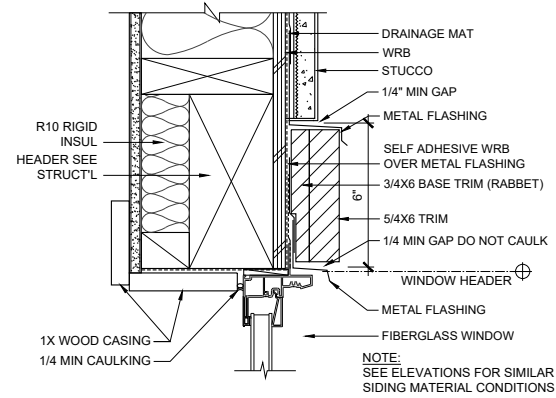
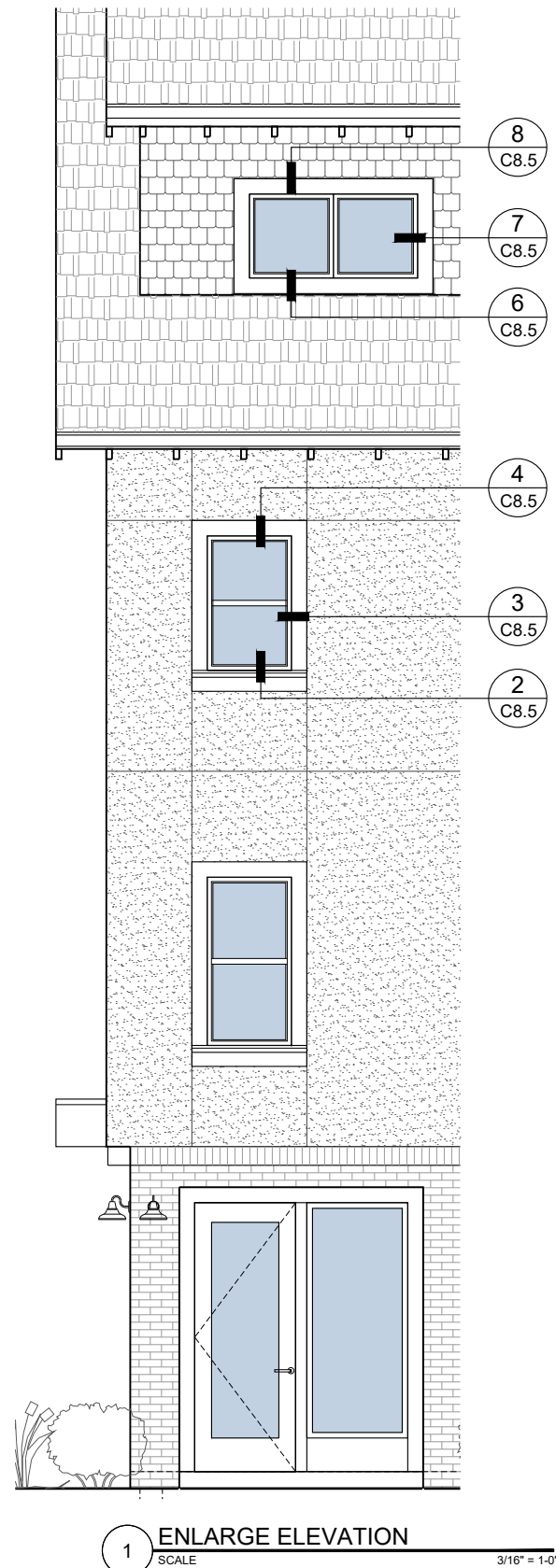
7 DETAIL SECTION  
SCALE 1 1/2" = 1'-0"



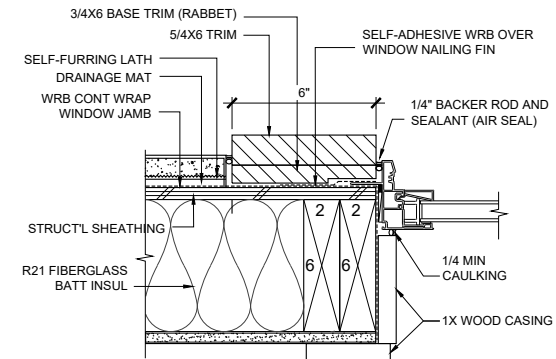
6 DOOR HEAD  
SCALE 1 1/2" = 1'-0"



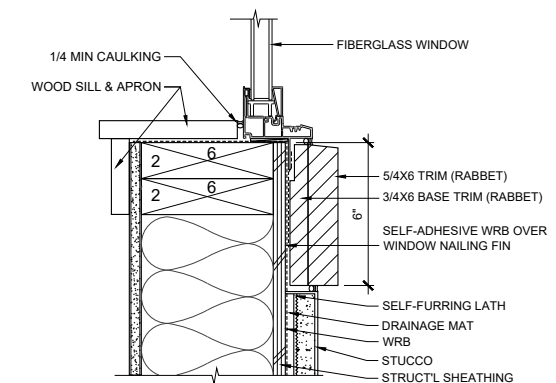
5 THRESHOLD  
SCALE 1 1/2" = 1'-0"



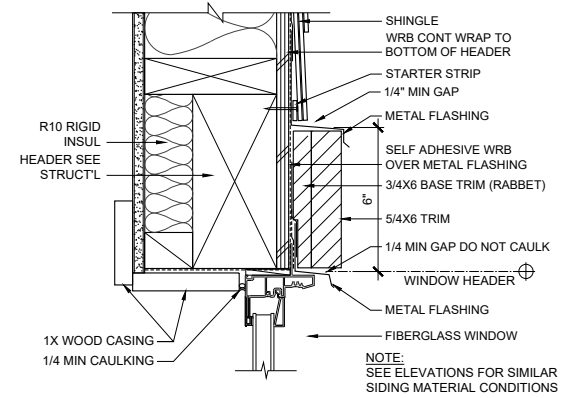
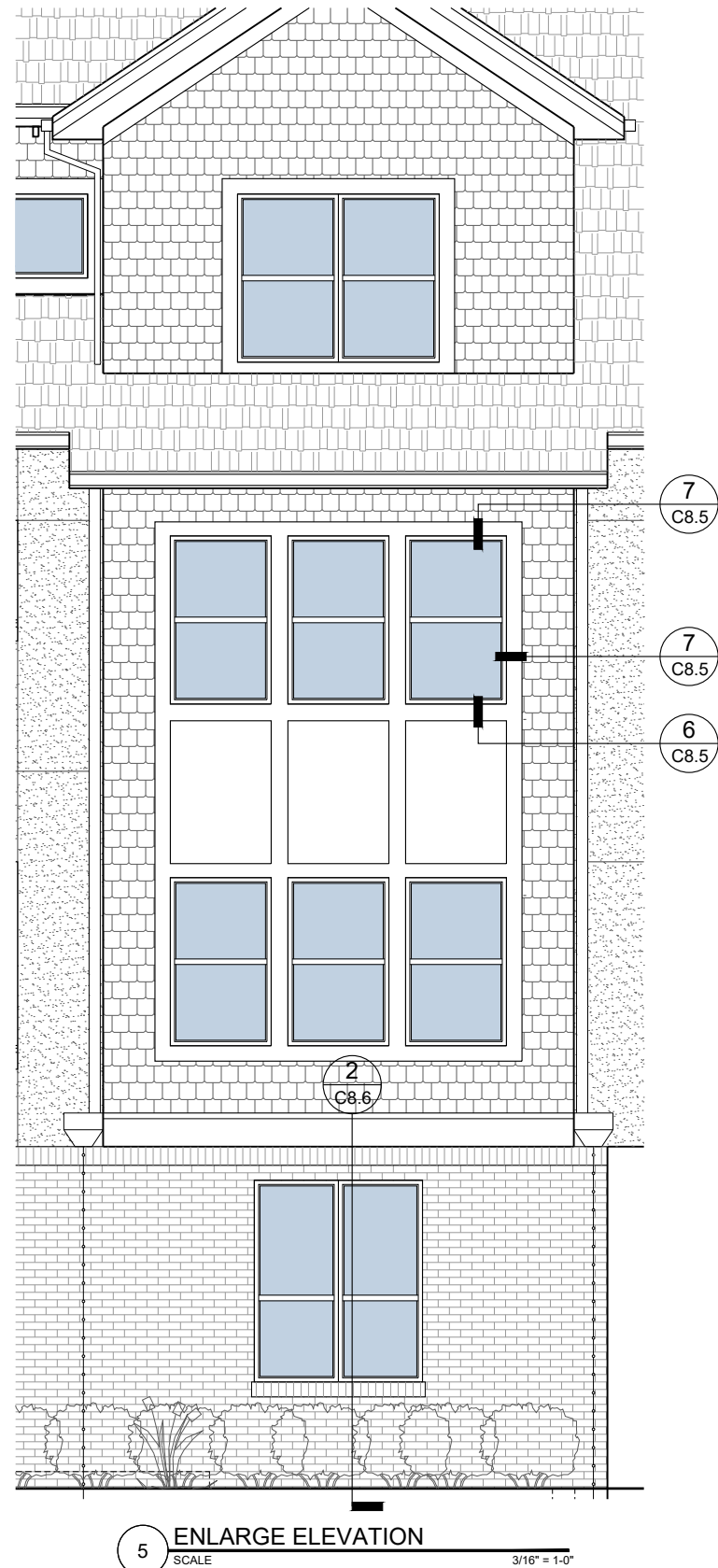
**4 WINDOW HEAD - STUCCO**  
SCALE 1 1/2" = 1'-0"



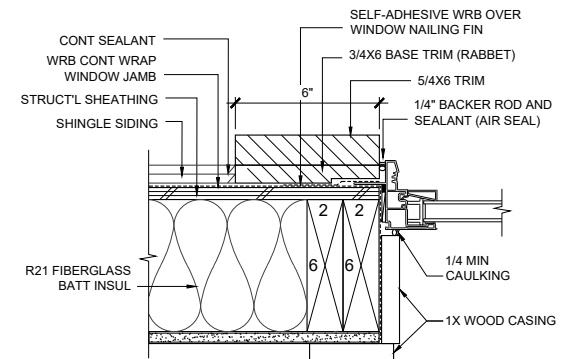
**3 WINDOW JAMB - STUCCO**  
SCALE 1 1/2" = 1'-0"



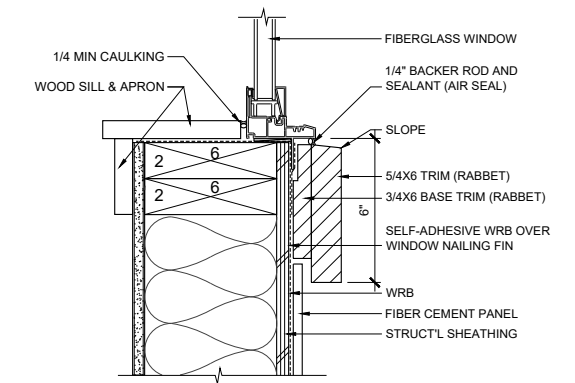
**2 WINDOW SILL - STUCCO**  
SCALE 1 1/2" = 1'-0"



**8 WINDOW HEAD - SHINGLE**  
SCALE 1 1/2" = 1'-0"

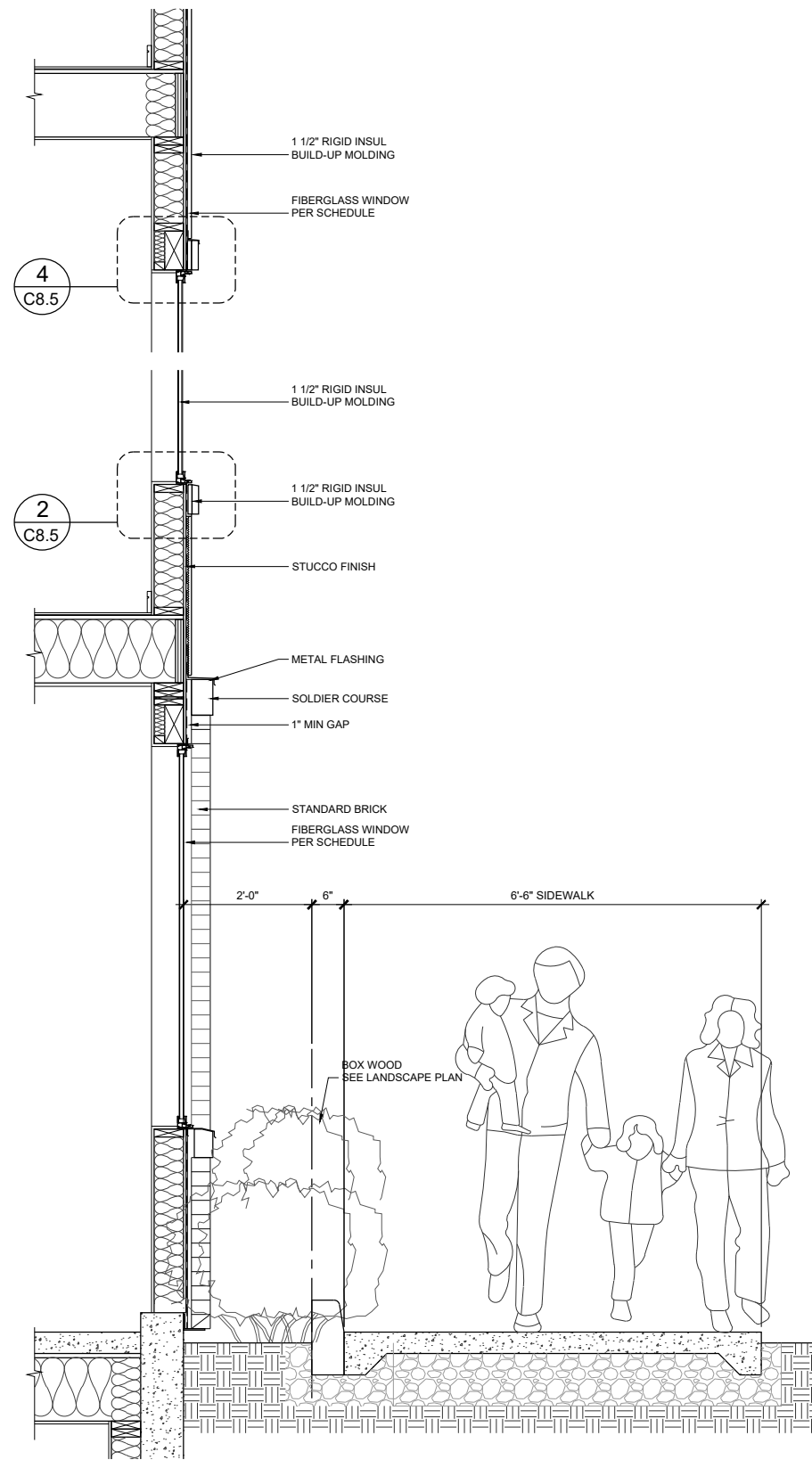


**7 WINDOW JAMB - SHINGLE**  
SCALE 1 1/2" = 1'-0"

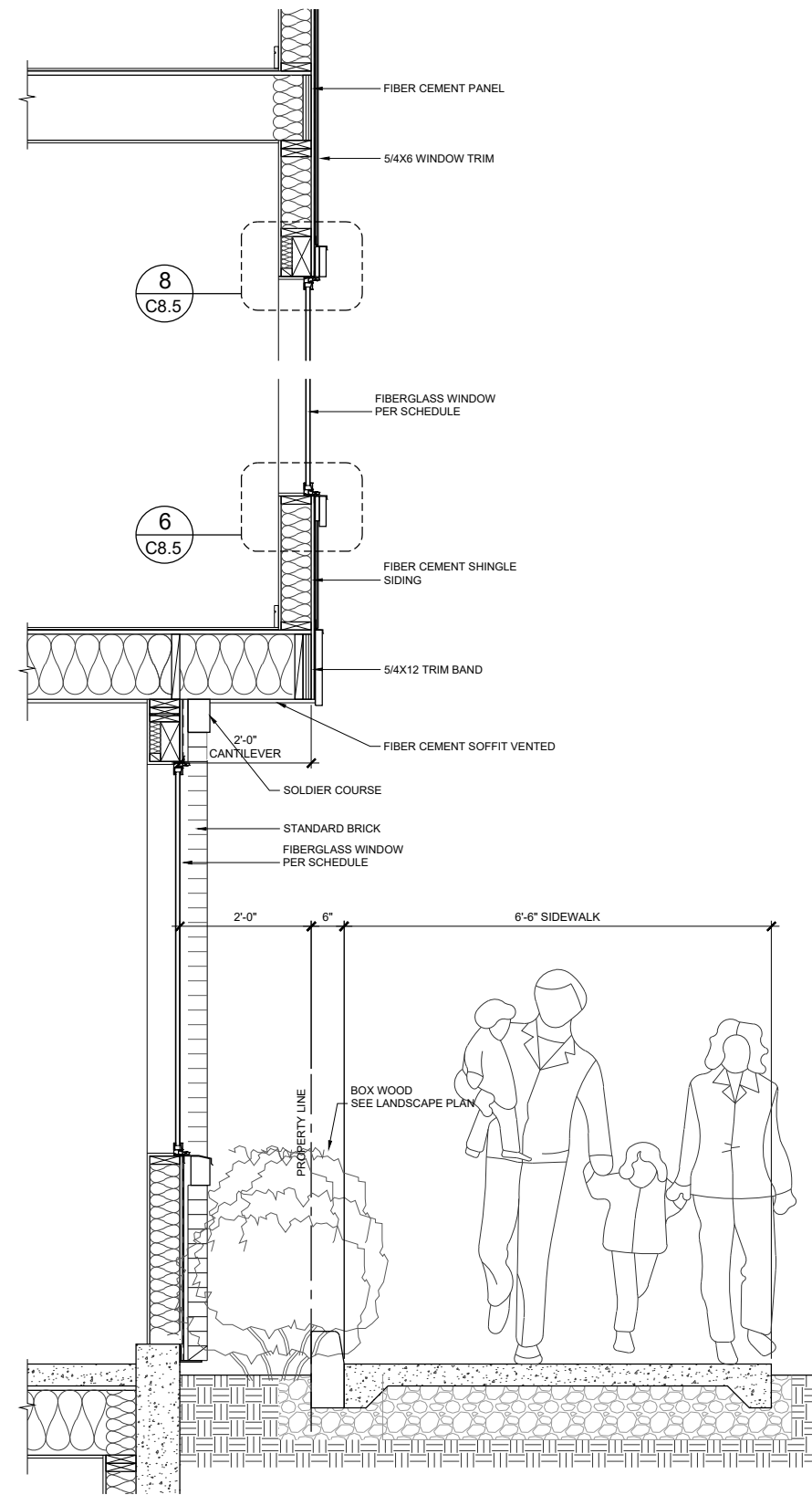


**6 WINDOW SILL - SHINGLE**  
SCALE 1 1/2" = 1'-0"

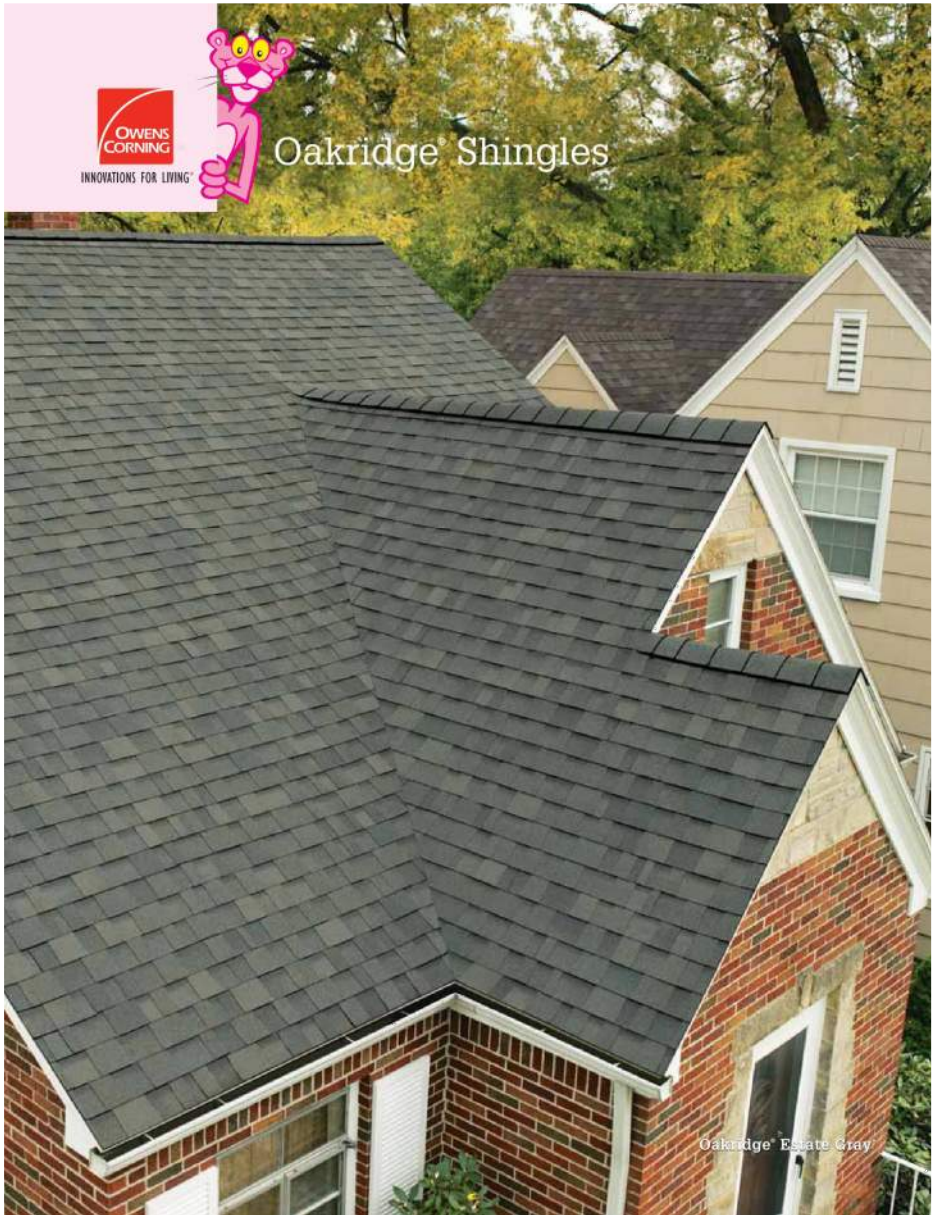




1 WALL SECTION  
SCALE 3/8" = 1'-0"



2 WALL SECTION  
SCALE 3/8" = 1'-0"



Oakridge® Shingles

Antique Silver®  
Not Available in Service Area A (see map)

Beachwood Sand®  
Not Available in Service Area A (see map)

Desert Tan®

Brownwood®

Teak®  
Not Available in Service Area B (see map)

Driftwood®

Onyx Black®

Estate Gray®

Williamsburg Gray®  
Not Available in Service Area B (see map)

Chateau Green®  
Not Available in Service Area B (see map)

Shasta White®

Oakridge® shingles have a warm, inviting look in popular colors for a step up from traditional three-tab shingles. A Limited Lifetime Warranty\*\* with 110/130\*\* MPH Wind Resistance Limited Warranty\* adds to this shingle's appeal, along with our Algae Resistance Limited Warranty\*.

**ENERGY STAR® is for roofs too**

Similar to the energy-efficient appliances in your home, roofing products can provide energy-saving qualities. Owens Corning® Oakridge roofing shingles in Shasta White can help reduce your energy bills when installed properly. These shingles reflect solar energy, decreasing the amount of heat transferred to a home's interior—and the amount of air conditioning needed to keep it comfortable. Actual savings will vary based on geographic location and individual building characteristics. Call 1-800-GET-PINK® or 1-888-STAR-YES for more information.

**Product Attributes**

<b>Warranty Length*</b>	Limited Lifetime*
<b>Wind Resistance Limited Warranty*</b>	110/130** MPH
<b>Algae Resistance Limited Warranty*</b>	10 Years
<b>Tru PROtection® Non-Prorated Limited Warranty* Period</b>	10 Years

**Oakridge® Shingles**  
**Product Specifications**

<b>Nominal Size</b>	13 3/4" x 39 3/4"
<b>Exposure</b>	5 1/4"
<b>Shingles per Square</b>	64
<b>Bundles per Square</b>	3
<b>Coverage per Square</b>	98.4 sq. ft.

**Applicable Standards and Codes**

ASTM E 108, Class A Fire	ASTM D 3462
ASTM D 3161, Class F Wind	ASTM D 228
ASTM D 7158, Class H Wind	UL 790, Class A
ASTM D 3018, Type 1	Florida Building Code

\* See actual warranty for complete details, limitations and requirements.  
\*\* 130 MPH is applicable only with 6-nail application and Owens Corning® Starter Shingle product application in eaves and rakes in accordance with installation instructions.  
† Owens Corning strives to accurately reproduce photographs of shingles. Due to manufacturing variances, the limitations of the printing process and the variations in natural lighting, actual shingle colors and granule blends may vary from the photo. The pitch of your roof can also impact how a shingle looks on your home. We suggest that you view a roofing display or several shingles to get a better idea of the actual color. To accurately judge your shingle and color choice, we recommend that you view it on an actual roof with a pitch similar to your own roof prior to making your final selection. Color availability subject to change without notice. Ask your professional roofing contractor for samples of colors available in your area.  
ENERGY STAR and the ENERGY STAR mark are registered trademarks of the U.S. Environmental Protection Agency.  
\* 40-year Limited Warranty on commercial projects.

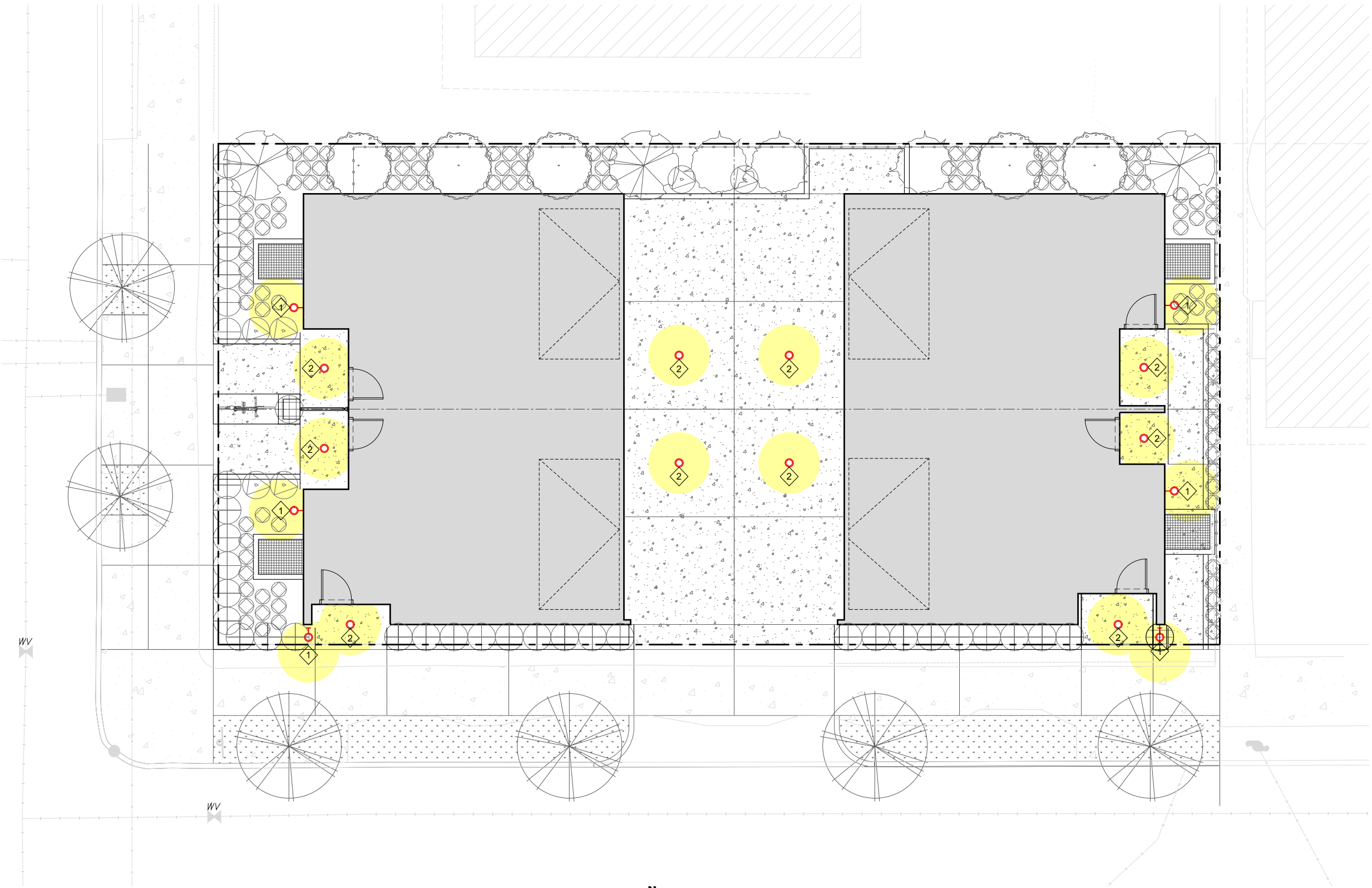
TAHRAN ARCHITECTURE & PLANNING LLC

IRVINGTON TOWN HOMES

C8.7  
DATE: 05-21-2025







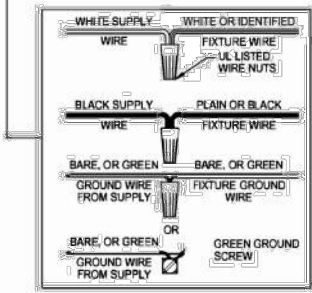
- KEY NOTES
- 1 WALL MOUNTED DOWN LIGHT  
URBAN AMBIANCE WITH LED BULD
  - 2 RECESSED LIGHTING DOWN LIGHT  
E SERIES LED DOWNLIGHT RETROFIT



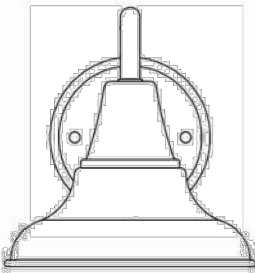
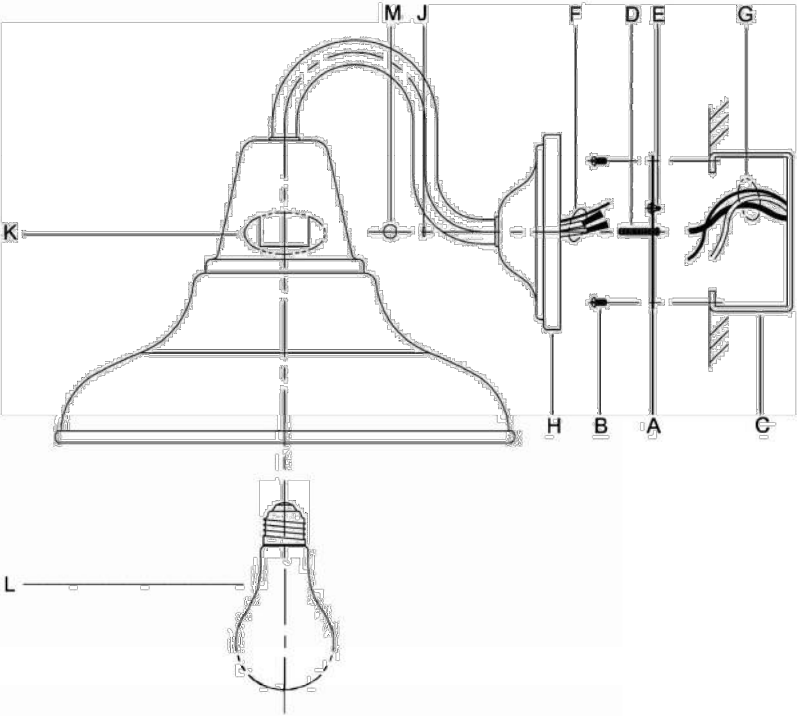
WARNING - If any Special Control Devices are used with this Fixture, Follow the Instructions Carefully to assure full compliance with N.E.C. requirements. If there are any questions, contact a Qualified Electrical Contractor.  
WARNING - This product contains chemicals known to the State of California to cause cancer, birth defects and /or other reproductive harm. Thoroughly wash hands after installing, handling, cleaning, or otherwise touching this product.  
CAUTION - All glass is fragile, use care when handling Glass component(s) and/or Lamp(s).

ASSEMBLY STEPS:  
PASOS PARA ENSAMBLAR:  
MODE D'ASSEMBLAGE:

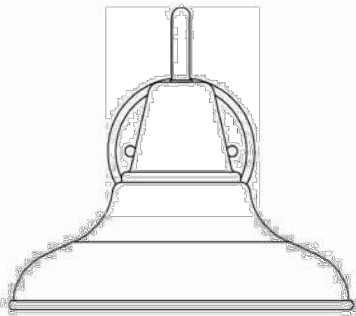
- 1. D, E → A
- 2. B, A → C
- 3. F → G
- 4. H, J, M → D
- 5. L → K



B, C, G & L  
(NOT FURNISHED)  
(NO INCLUIDA)  
(NON FOURNIE)



UHP1360/2



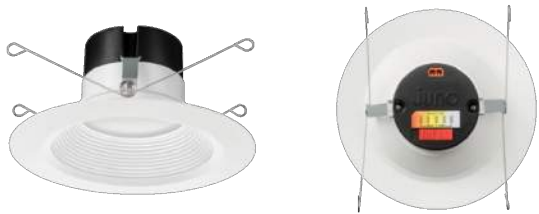
UHP1361/3

WALL MOUNTED  
LIGHTING FIXTURE TYPE 1  
N.T.S.

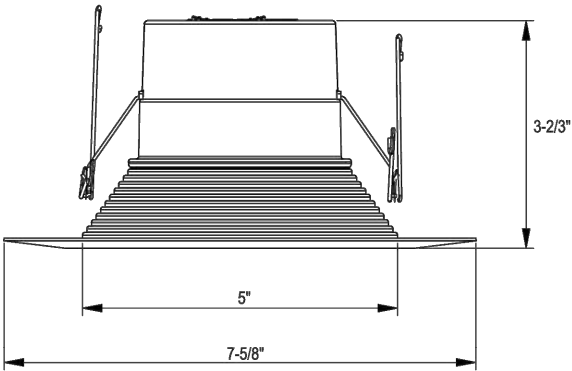


E Series LED Downlight Retrofit  
5"/6" Switchable White LED Retrofit Module

65BE/65SE SWW5 Series



Dimensions



Ships with:



E26 Adapter

65BE/65SE SWW5

Project:
Fixture Type:
Location:
Contact/Phone:

Product Features

The 5"/6" recessed LED module is the most economical means to create a well lit environment with exceptional energy efficiency and near zero maintenance. Great for retrofit into existing downlight cans or new construction and remodel applications. The LED module maintains at least 70% light output for 50,000 hours.

- 5 switchable white color temperatures to choose from via switch; 2700K, 3000K, 3500K, 4000K, 5000K CCT; 90 CRI
- Lumens: 900L, 1200L
- Adjustable torsion spring allows fitment into many 5" and 6" cans
- Baffle or smooth trim options with designer color finish trim inserts (sold separately)

Applications

- Retrofit into existing downlight cans or new and remodel construction in residential and light commercial applications
- Suitable for installation in standard and shallow-height rough-in sections
- Wet rated and air-loc: perfect for showers, bathrooms, and outdoor soffits

Performance

Delivered Lumens	Nominal 900L; 1200L at 3000K
LED Color Temperature	Switchable White (2700K, 3000K, 3500K, 4000K, 5000K) Default set at 3000K
CRI	90+
Voltage	Dedicated 120V
Dimming	Dimmable flicker-free dimming from 100% to 10%

For a list of compatible dimmers see JUNOESERIES-DIM

Specifications

Aperture: 5"  
Ceiling opening: 5.25"  
Overlap trim: 7.62"  
Height: 3.67"

RECESSED (CANTILEVER)  
LIGHTING FIXTURE TYPE 2  
N.T.S.



TAHRAN ARCHITECTURE & PLANNING LLC

NIGHT TIME - RENDERING VIEW 1





NIGHT TIME - RENDERING VIEW 2



# FOR REFERENCE ONLY



VIEW 2 - MASSING STUDY



VIEW 4 - MASSING STUDY



VIEW 1 - MASSING STUDY



VIEW 3 - MASSING STUDY



FOR REFERENCE ONLY



VIEW 1 - PREFER MASSING STUDY



VIEW 2 - PREFER MASSING STUDY



# FRONTAGE IMPROVEMENT PLAN

## NE CORNER NE SCHUYLER ST & NE 12TH AVE.

### GENERAL NOTES:

1. ERRORS AND OMISSIONS ARE THE RESPONSIBILITY OF THE "ENGINEER OF RECORD". IF ERRORS OR OMISSIONS ARE FOUND AFTER THE PERMIT HAS BEEN ISSUED, THE PERMITTEE/CONTRACTOR SHALL CONTACT THE ENGINEER OF RECORD (JOHN MIDDLETON OF ZTEC ENGINEERS AT 503-235-8795) TO HAVE THE CORRECTIONS MADE. ALL CHANGES WILL REQUIRE THE APPROVAL OF THE CITY ENGINEER PRIOR TO START OF WORK.

2. THE CONTRACTOR SHALL HAVE ON SITE AT ALL TIMES THE APPROVED CONSTRUCTION PLANS & SPECIAL SPECIFICATIONS, CITY OF PORTLAND STANDARD CONSTRUCTION SPECIFICATIONS, AND ALL OTHER APPLICABLE SPECIFICATIONS BOOKS AND MANUALS. ELECTRONIC EQUIVALENTS ARE ACCEPTABLE.

3. ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO THESE DRAWINGS AND THE APPLICABLE REQUIREMENTS OF THE CURRENT EDITION OF THE CITY OF PORTLAND STANDARD CONSTRUCTION SPECIFICATIONS AND ALL REVISIONS AND SPECIAL SPECIFICATIONS, IN PLACE AT THE TIME OF PLAN APPROVAL.

4. A PRECONSTRUCTION CONFERENCE WITH CITY STAFF AND AN APPROVED TEMPORARY TRAFFIC CONTROL PLAN (ISSUED IN CONJUNCTION WITH A STREET /SIDEWALK CLOSURE PERMIT) ARE REQUIRED BEFORE COMMENCING WORK. SEE PERMIT FOR SCHEDULING A PRECONSTRUCTION CONFERENCE AND ACQUISITION OF THE TTCP.

5. ELEVATIONS ARE BASED ON CITY OF PORTLAND DATUM FROM BENCH MARK NO. 1829 ELEVATION =150.129, LOCATED AT S.W. CORNER OF NE SCHUYLER AVE. & NE 14TH AVE.

6. EXISTING UTILITIES AND SITE FEATURES SHOWN ON THESE PLANS ARE BASED ON A SURVEY BY CENTERLINE CONCEPTS 503-650-0188 UTILITIES WERE LOCATED ON AUGUST 2023 UNDER UTILITY LOCATE TICKET NUMBER 2307724

7. THE CONTRACTOR SHALL REESTABLISH ANY SURVEY MONUMENTS DAMAGED OR DESTROYED DURING CONSTRUCTION. SURVEY MONUMENTS SHALL BE REESTABLISHED BY A LICENSED LAND SURVEYOR AT THE CONTRACTORS EXPENSE.

### UNANTICIPATED CONTAMINATED MATERIAL

8. REMOVE AND DISPOSE (AT A PROPER LOCATION OR LANDFILL) ALL MATERIALS EXCAVATED FROM WORK IN THE RIGHT-OF-WAY. FOR DISPOSAL ON PRIVATE PROPERTY, SECURE A FILL PERMIT PRIOR TO BEGINNING WORK FROM THE BUREAU OF DEVELOPMENT SERVICES (BDS). PROVIDE A COPY OF THE APPROVED FILL PERMIT TO THE STREET CONSTRUCTION INSPECTOR.

FOR UNANTICIPATED CONTAMINATED MEDIA ENCOUNTERED, THE PERMITTEE/CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS ASSOCIATED WITH THE MANAGEMENT AND DISPOSAL OF CONTAMINATED MEDIA ENCOUNTERED AND IS ALSO RESPONSIBLE FOR ALL RESULTANT DELAYS. CONTACT INSPECTOR AND REFER TO ALL APPLICABLE SPECIFICATIONS AND AS AMENDED BY SPECIAL PROVISIONS.

THE PERMITTEE/CONTRACTOR SHALL PROVIDE THE CITY (CONSTRUCTION ENGINEERING AND INSPECTION) WITH COPIES OF ALL DISPOSAL PERMITS FROM THE PERMITTED DISPOSAL FACILITY, ANALYTICAL RESULTS USED TO GAIN ACCEPTANCE OF THE CONTAMINATED MEDIA, AND DISPOSAL RECEIPTS/DAILY WEIGH SLIPS. DAILY WEIGH SLIP AMOUNTS SHALL BE CHECKED AGAINST INSPECTOR'S DAILY REPORTS. THE PERMITTEE MUST USE AN OREGON FACILITY FOR DISPOSAL OF THE CONTAMINATED MEDIA.

### UTILITIES

9. UTILITIES SHOWN ON THESE PLANS ARE FOR INFORMATION AND COORDINATION PURPOSES ONLY AND ARE NOT AUTHORIZED FOR INSTALLATION UNDER THE PUBLIC STREET IMPROVEMENT PERMIT. PRIVATE AND PUBLIC UTILITY COMPANIES ARE REQUIRED TO SECURE SEPARATE UTILITY PERMITS FROM PBOT FOR ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY.

### STREET PAVEMENT

10. ALL MANHOLE LIDS, VALVE BOXES OR OTHER LIDS SHALL BE ADJUSTED TO FINISHED STREET GRADE.

11. EXISTING ASPHALT CONCRETE PAVEMENT SECTION SHALL BE RESTORED PER CITY STD. DWG. P-505 AND/OR P-518. THE STREET INSPECTOR WILL MAKE THE FINAL DETERMINATION OF THE LIMITS OF PAVEMENT RESTORATION, INCLUDING SAWCUT LINES. THE PERMITTEE/CONTRACTOR SHALL CONSULT WITH THE STREET INSPECTOR PRIOR TO SAWCUTTING OR DEMOLISHING OF PAVEMENT.

### CURBS, SIDEWALKS, AND DRIVEWAYS

12. ALL DRIVEWAYS ARE REQUIRED TO HAVE A MINIMUM OF 3 FEET OF HARD SURFACING BEHIND SIDEWALK (SEE STD. DWG. P-536).

### TRAFFIC AND PARKING CONTROL

13. THE CONTRACTOR MUST ACQUIRE AN APPROVED TEMPORARY STREET USE PERMIT (TSUP) PRIOR TO CLOSURE OF ANY STREET, SIDEWALK, TRAVEL LANE OR PARKING LANE. THE TSUP IS ACQUIRED FROM THE PERMIT CENTER LOCATED AT 1900 SW 4TH AVENUE, OR ONLINE AT [WWW.TSUP.INFO](http://WWW.TSUP.INFO).

14. THE CONTRACTOR SHALL NOT REMOVE OR COVER ANY TRAFFIC CONTROL SIGNS, PAVEMENT MARKINGS, OR BARRICADES THAT ARE NOT IDENTIFIED ON THE APPROVED TEMPORARY TRAFFIC CONTROL PLAN.

15. THE CONTRACTOR SHALL MAINTAIN ALL NECESSARY TEMPORARY TRAFFIC CONTROL DEVICES (INCLUDING BUT NOT LIMITED TO THE FOLLOWING - SIGNS, PAVEMENT MARKINGS, AND BARRICADES) UNTIL THE PERMANENT TRAFFIC CONTROL DEVICES ARE INSTALLED.

16. WHEN WORK INTERFERES WITH THE OPERATION OF A TRIMET BUS OR BUS STOP, CONTACT TRIMET AT 503-962-4949 A MINIMUM OF 14 DAYS PRIOR TO CLOSING OR DISRUPTING TRIMET'S OPERATION.

17. THE CONTRACTOR SHALL INSTALL OR REINSTALL ALL PERMANENT TRAFFIC CONTROL SIGNING, CURB AND PAVEMENT MARKINGS, AND BARRICADES.

18. THE CONTRACTOR SHALL SUBMIT MATERIALS LIST FOR APPROVAL 14 DAYS PRIOR TO INSTALLING PERMANENT TRAFFIC CONTROL SIGNING, CURB AND PAVEMENT MARKINGS, AND BARRICADES.

19. ALL NEW SIGN MATERIALS SHALL COMPLY WITH SECTION 2910 OF THE CITY OF PORTLAND STANDARD CONSTRUCTION SPECIFICATIONS. ALL SIGNS SHALL BE ON ALUMINUM SIGN BLANKS. SIGN TYPES FOR EACH SIGN, AS SPECIFIED IN SECTION 2910.02, ARE NOTED IN THE PLANS.

20. SIGNS AND SIGN POSTS REMOVED BY THE PERMITTEE OR ITS AGENT THAT WILL NOT BE RE-INSTALLED SHALL BE PROPERLY DISPOSED OF BY THE CONTRACTOR.

21. ALL CURB AND PAVEMENT MARKING MATERIALS SHALL BE ON THE CITY'S CONSTRUCTION PRODUCTS LIST (CPL) OR THE STATE'S QUALIFIED PRODUCTS LIST (QPL). ALL MATERIALS SHALL BE INSTALLED IN CONFORMANCE WITH THE MANUFACTURER'S APPROVED APPLICATION PROCEDURE.

22. ALL LONGITUDINAL LINE WORK TO BE METHOD B (NON-PROFILE) EXTRUDED THERMOPLASTIC, 120 MILS THICK.

23. ALL TRANSVERSE LINE WORK, LEGENDS, SYMBOLS, AND ARROWS SHALL BE TYPE "B-HS" PREFORMED THERMOPLASTIC. BIKE LANE STENCILS, GREEN BICYCLE LANE MARKINGS, AND BIKE PATH RAILROAD MARKINGS SHALL BE 90 MILS THICK. ALL OTHER TRANSVERSE PAVEMENT MARKINGS SHALL BE 120-125 MILS THICK.

24. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY CURB OR PAVEMENT MARKINGS DAMAGED, WORN OUT OR REMOVED DUE TO CONTRACTOR'S OPERATION.

25. OUTSIDE METERED DISTRICTS: PARKING CONTROL SIGNING WILL BE INSTALLED BY THE CONTRACTOR. CONTACT PARKING CONTROL AT 503-823-7275 14 DAYS PRIOR TO INSTALLATION TO IDENTIFY EXACT SIGN LOCATIONS.

### TREES

26. ALL TREE REMOVAL SHALL COMPLY WITH THE FEDERAL MIGRATORY BIRD TREATY ACT. SEE THE SPECIAL PROVISIONS FOR REQUIREMENTS PRIOR TO CUTTING OF ANY TREE.

27. ADDITIONAL INSPECTION REQUIRED PRIOR TO CUTTING STREET TREE ROOTS. APPLY FOR A ROOT PRUNING PERMIT FROM URBAN FORESTRY ([HTTPS://DEV.HUB.PORTLANDOREGON.GOV](https://dev.hub.portlandoregon.gov)) ONCE ROOTS ARE EXPOSED. CONSULTATION WITH THE URBAN FORESTER IS REQUIRED BEFORE CUTTING OF ROOTS.

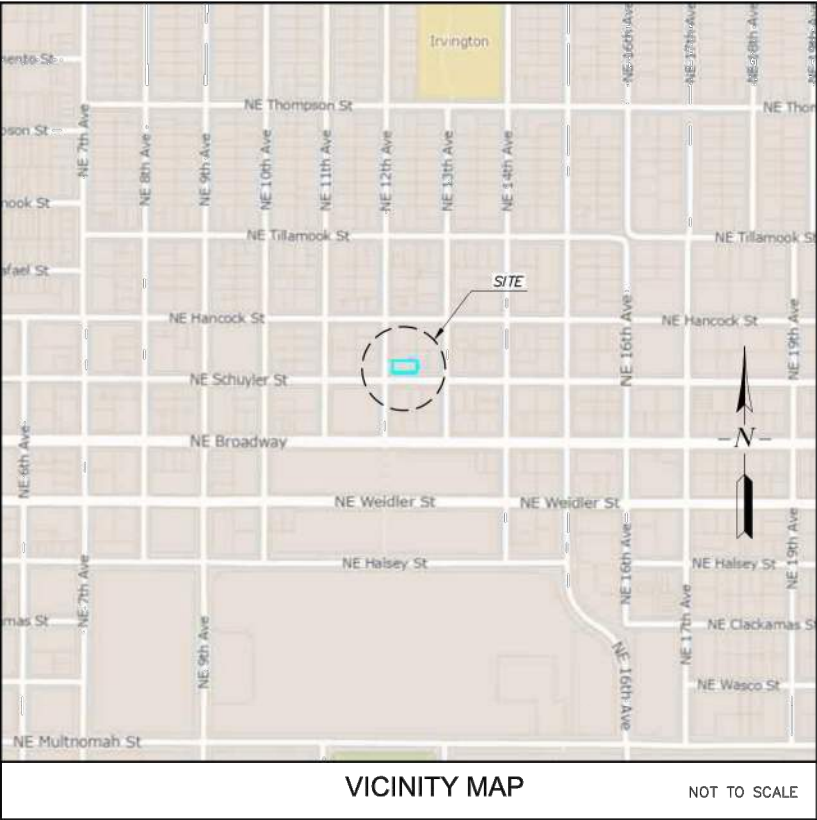
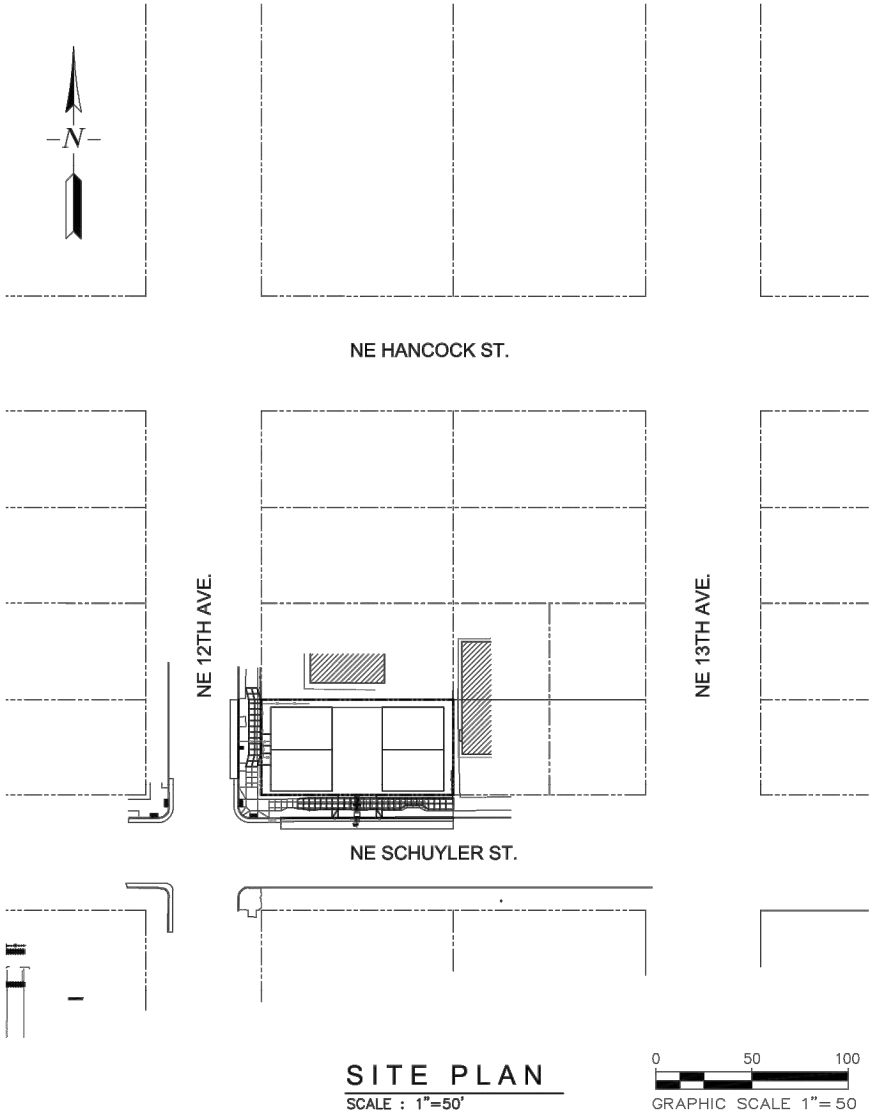
28. FOR ALTERNATE TREE SPECIES OR ALTERNATE TREE PLANTING LOCATION APPROVAL (PRIOR TO PLANTING), CONTACT URBAN FORESTRY AT 503-823-8733; TO LEAVE A MESSAGE FOR THE TREE INSPECTOR PRESS 5.

29. STREET TREES NOT APPROVED FOR REMOVAL THROUGH THIS PERMIT MUST BE PRESERVED AND PROTECTED. DO NOT DAMAGE THE TRUNK, BRANCHES, OR ROOTS. DO NOT STORE MATERIALS OR DRIVE EQUIPMENT WITHIN THE TREE'S ROOTING AREA.

### STORMWATER NARRATIVE:

PUBLIC STREET: PUBLIC STORMWATER REQUIREMENTS ADDRESSED BY INSTALLING STREET TREES FOR ADDITIONAL IMPERVIOUS AREA MITIGATION. EXISTING STORMWATER RUNOFF FLOW BY GUTTER LINE NORTH ON NE 12TH AVE. TO CATCH INLET AT SW CORNER OF NE HANCOCK ST. & NE 12TH AVE. CATCH BASIN CONNECTS TO PUBLIC STORM SEWER THAT EVENTUALLY DISCHARGES TO RIVER. EXISTING STORM WATER RUNOFF FLOWS BY GUTTER FLOW WEST ON SCHUYLER ST. TO CATCH INLET AT NE CORNER OF NE 12TH AVE. & SCHUYLER ST., DISCHARGE TO PUBLIC COMB. M.H. IN INTERSECTION.

PRIVATE LOT: ON-SITE STORMWATER RUNOFF DIRECTED TO ON-SITE SUBSURFACE DISPOSAL SYSTEM.



### NOTICE TO EXCAVATORS:

ATTENTION: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER. (NOTE: THE TELEPHONE NUMBER FOR THE OREGON UTILITY NOTIFICATION CENTER IS 503-232-1987).

### POTENTIAL UNDERGROUND FACILITY OWNERS

**Dig Safely.**

Call the Oregon One-Call Center  
DIAL 811 or 1-800-332-2344

### EMERGENCY TELEPHONE NUMBERS

NW NATURAL GAS	
M-F 7am-6pm	503-226-4211 Ext.4313
AFTER HOURS	503-226-4211
PGE	503-464-7777
CENTURYLINK	1-800-573-1311
CITY BUREAU OF MAINTENANCE	503-823-1700
CITY WATER	503-823-4874
VERIZON	1-800-483-1000

### LEGEND

- EXISTING POWER POLE
- EXISTING LIGHT POLE
- EXISTING TREE
- PROPOSED TREE
- EXISTING WATER METER
- EXISTING WATER VALVE
- EXISTING GAS VALVE
- EXISTING AREA DRAIN
- EXISTING STORM SEWER MANHOLE
- EXISTING COMBINED SANITARY MANHOLE
- EXISTING POWER POLE WITH STREET LIGHT
- EXISTING GAS LINE
- EXISTING OVERHEAD POWER LINES
- EXISTING WATER
- EXISTING COMBINED SEWER
- EXISTING FENCE
- EXISTING CURB
- PROPOSED CURB
- PROPOSED SIDEWALK

### CONTACTS:

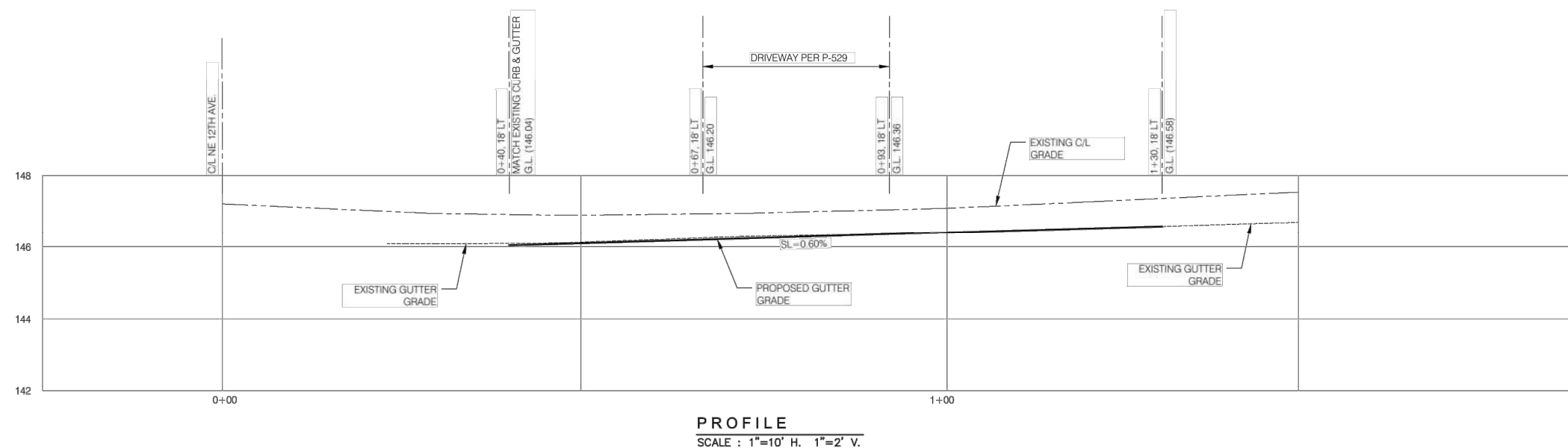
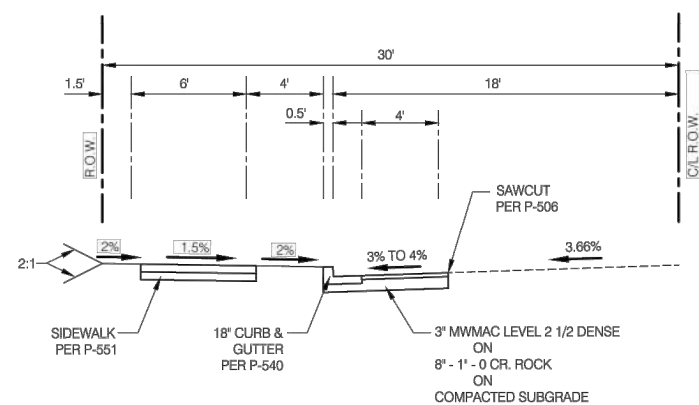
OWNER : ERIC RYSTADT  
MAIN STREET DEVELOPMENT  
5331 S MACADAM AVE. #258  
PORTLAND OR 97239  
503-422-7707  
EMAIL : ERIC@MSDPDX.COM

ENGINEER : ZTEC ENGINEERS  
JOHN MIDDLETON  
3880 SE 8TH AVE. SUITE 280  
PORTLAND OR 97202  
503-235-8795  
EMAIL : JOHN@ZTECENGINEERS.COM

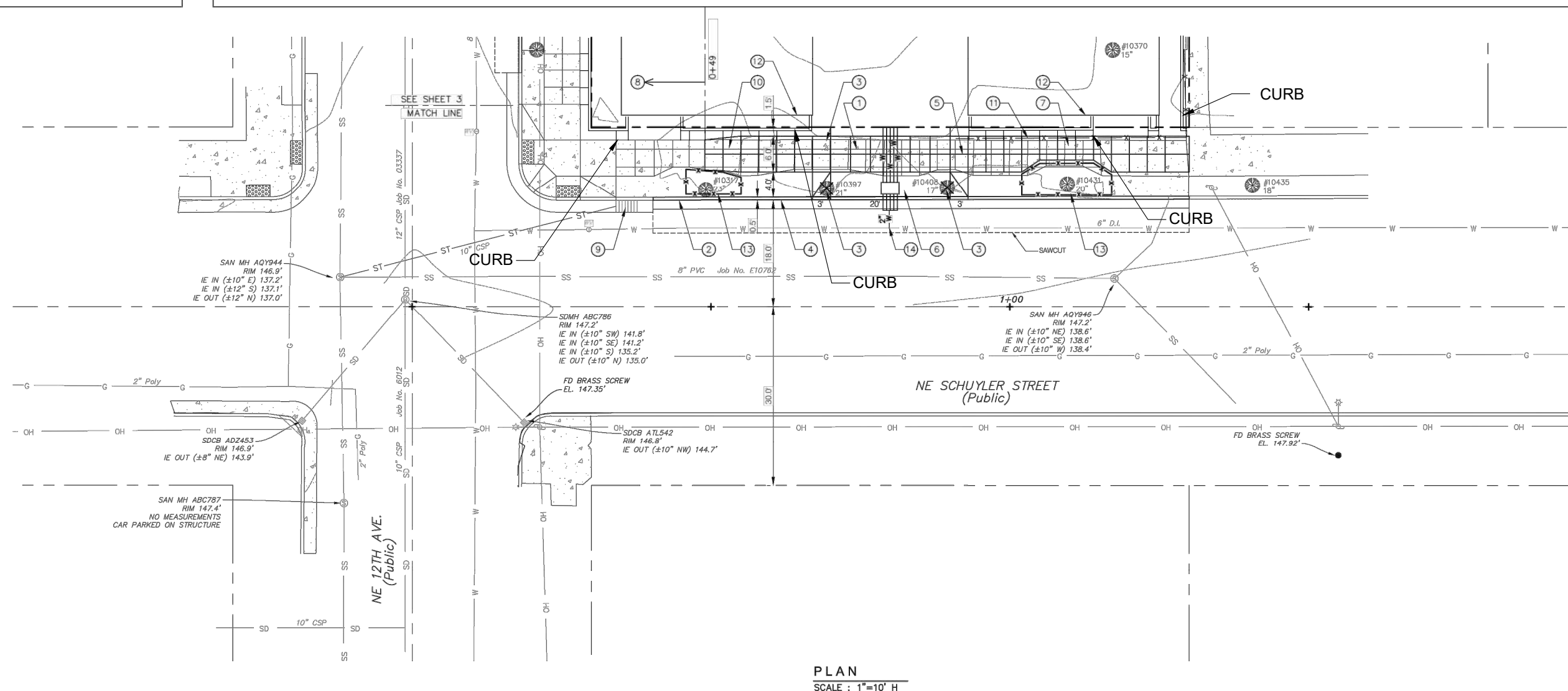
### SHEET INDEX:

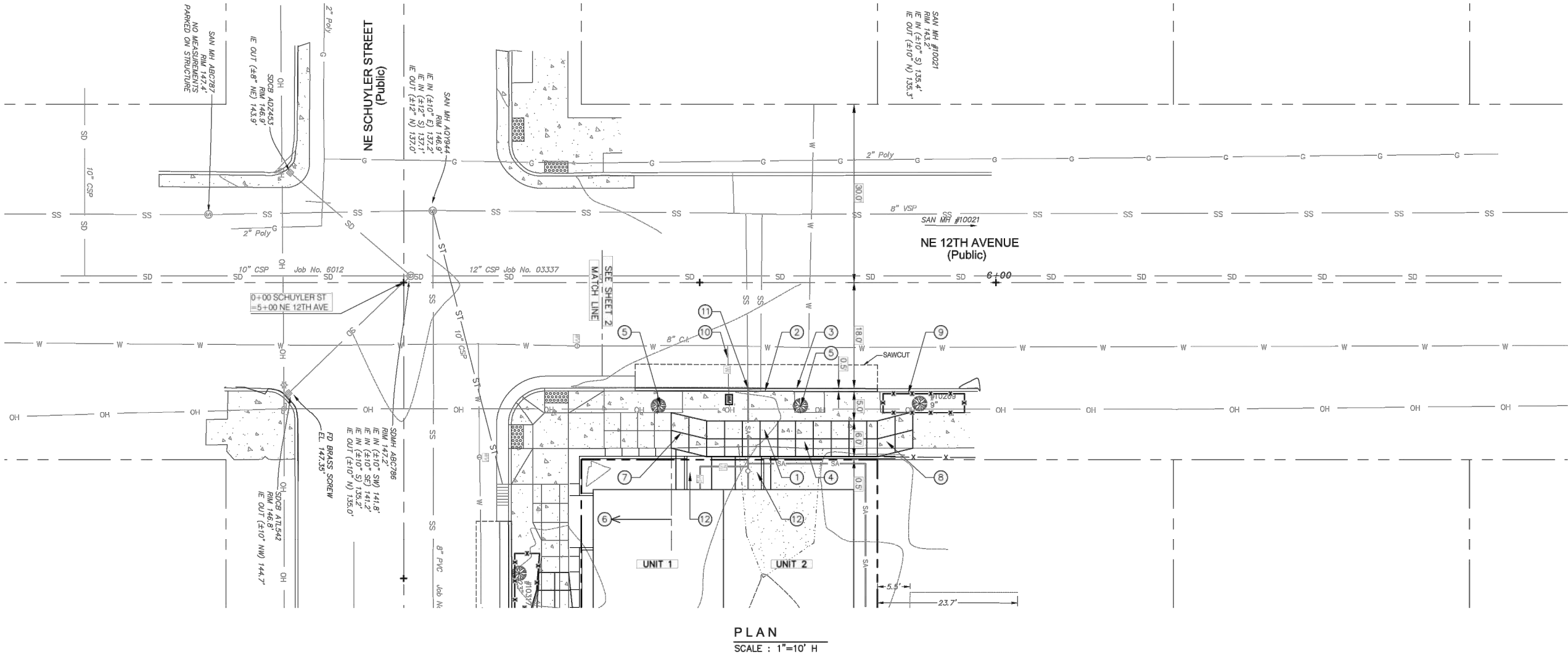
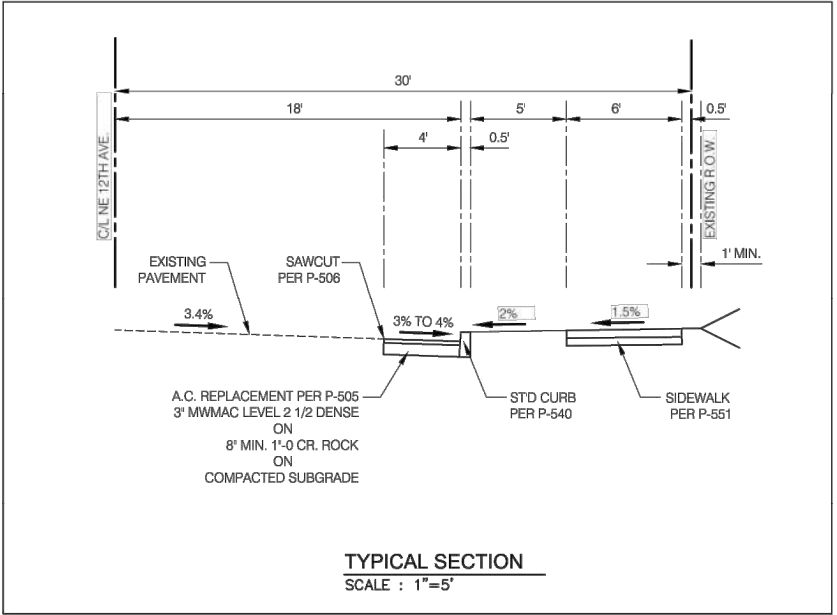
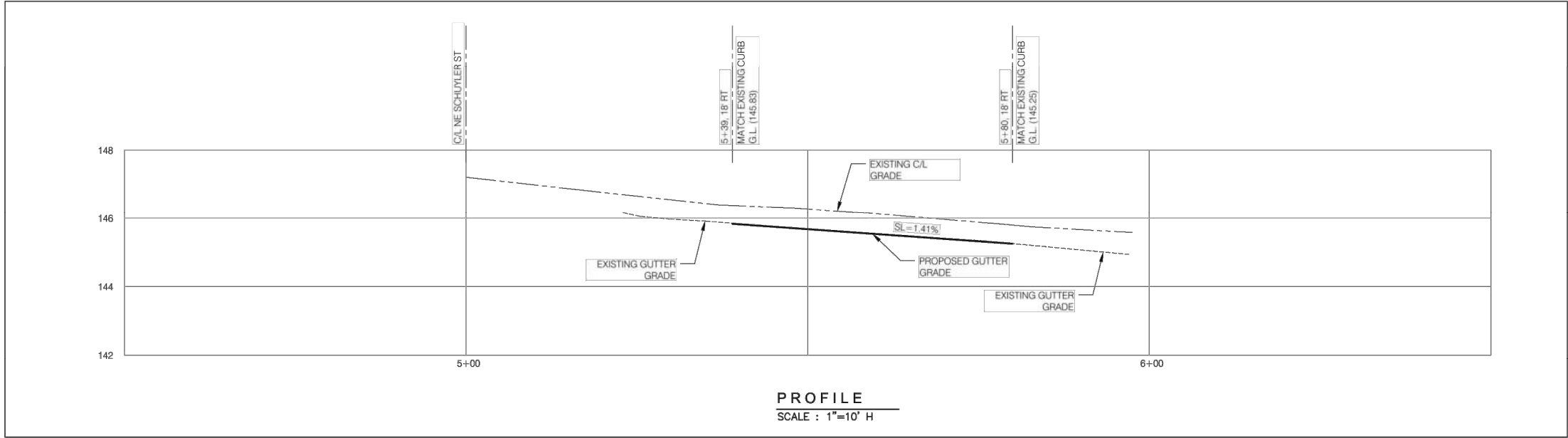
- 1 - COVER SHEET
- 2 - NE SCHUYLER ST. PLAN & PROFILE
- 3 - 12TH AVE. PLAN & PROFILE
- 4 - DRIVEWAY DETAIL



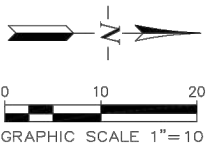


- ① 0+49 TO 1+30  
REMOVE EXISTING CONC. SIDEWALK
- ② 0+40 TO 1+30  
REMOVE EXISTING CURB
- ③ REMOVE EXISTING STREET TREE  
  
0+40 TO 1+30  
INSTALL 18" CURB & GUTTER PER P-540
- ④ 0+49 TO 1+30  
INSTALL SIDEWALK PER P-551
- ⑤ 0+80 INSTALL 20' WIDE RESIDENTIAL DRIVEWAY PER P-629 AND  
DETAIL ON SHEET 4
- ⑦ REDUCE SIDEWALK WIDTH TO 4' MIN. ADJACENT TO TREE
- ⑧ MAINTAIN EXISTING SIDEWALK, ADA RAMPS & CURB & GUTTER.  
INSTALLED BY CITY OF PORTLAND AT CORNER
- ⑨ EXISTING INLET ATL 541 TO REMAIN
- ⑩ 0+49 TO 0+55 TRANSITION, FROM EXISTING 5' WIDE SIDEWALK TO  
6' WIDE SIDEWALK
- ⑪ REMOVE FENCE IN PUBLIC R.O.W.
- ⑫ PROPOSED ON-SITE BUILDINGS
- ⑬ INSTALL TREE PROTECTION PER P-582
- ⑭ INSTALL 2" HEADER SERVICE ASSEMBLY FOR FOUR 3/4" WATER  
METERS IN A 2' x 3' BOX PER P-783 WITH H-20 RATED LID BY  
PORTLAND WATER BUREAU. APPLICANT MUST CONTACT PWB AT  
WB1STATESHED@PORTLAND.OREGON.GOV TO REQUEST  
INSTALLATION. CONTRACTOR TO CONNECT TO SHORT STUBS ON  
BACK OF METER BOX

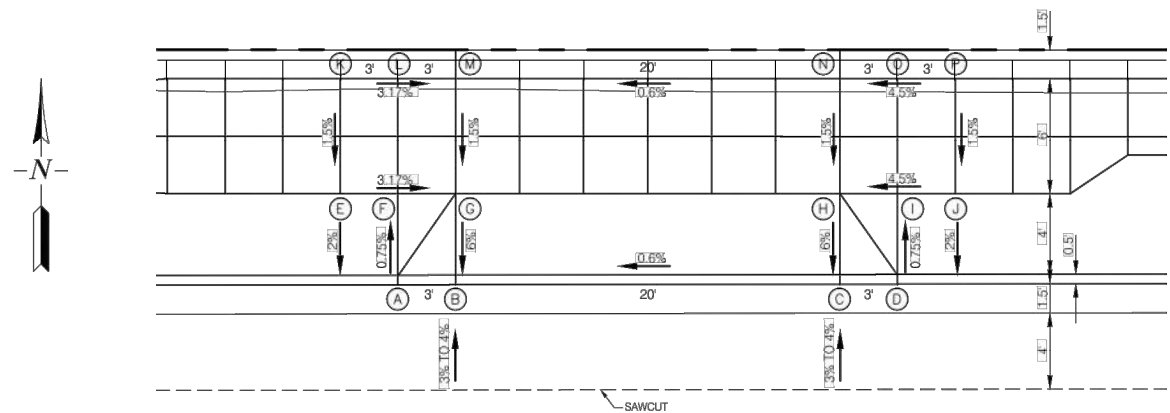




- ① 5+45 TO 5+86  
REMOVE EXISTING CONC. SIDEWALK
- ② 5+39 TO 5+80  
REMOVE EXISTING CURB
- ③ 5+39 TO 5+80  
INSTALL STD. CURB PER P-540
- ④ INSTALL SIDEWALK PER P-551
- ⑤ 0+43, 0+67, 21' RT  
INSTALL 1 1/2" DIA. STREET TREE PER P-581 SUMMER SPRITE LINDEN
- ⑥ MAINTAIN EXISTING SIDEWALK, ADA RAMPS & CURB GUTTER  
INSTALLED BY CITY OF PORTLAND
- ⑦ 5+45 TO 5+51  
TRANSITION FROM EXISTING SIDEWALK TO SIDEWALK IN  
REQUIRED LOCATION
- ⑧ 5+80 TO 5+86  
TRANSITION FROM SIDEWALK IN REQUIRED LOCATION TO  
EXISTING SIDEWALK
- ⑨ PROTECT EXISTING TREE TO REMAIN PER 5-582
- ⑩ KILL EXISTING WATER SERVICE BY PORTLAND WATER BUREAU
- ⑪ FIELD LOCATE EXISTING 6" SEWER LATERAL RECORD INVERT  
ELEVATION EXTEND TO R.O.W. LINE WITH 6" PVC @ 2% SLOPE  
UNDER SEPARATE BES PERMIT
- ⑫ 6' WIDE ENTRY CONCRETE PAD. MATCH GRADE AT BACK OF  
SIDEWALK







NE SCHUYLER STREET  
(Public)

DRIVEWAY DETAIL  
SCALE : 1"=5'

DRIVEWAY ELEVATION TABLE

	STATION	G. L. EL.	T. C. EL.	T. S. EL.
A	0+67, 18' LT	146.20	146.70	
B	0+70, 18' LT	146.22	146.30	
C	0+90, 18' LT	146.34	146.42	
D	0+93, 18' LT	146.36	146.86	
E				146.76
F				146.67
G				146.57
H				146.69
I				146.83
J				146.96
K				146.85
L				146.76
M				146.66
N				146.78
O				146.92
P				147.05