



**City of Portland, Oregon**  
**Bureau of Development Services**  
**Inspection Services - Land Use Services**  
FROM CONCEPT TO CONSTRUCTION

Dan Saltzman, Commissioner  
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[www.portlandoregon.gov/bds](http://www.portlandoregon.gov/bds)

**Date:** August 11, 2015  
**To:** Interested Person  
**From:** Puja Bhutani, Land Use Services  
503-823-7226 / [Puja.Bhutani@portlandoregon.gov](mailto:Puja.Bhutani@portlandoregon.gov)

## **NOTICE OF A TYPE I DECISION ON A PROPOSAL IN YOUR NEIGHBORHOOD**

The Bureau of Development Services has **approved** a proposal in your neighborhood. The mailed copy of this document is only a summary of the decision. The reasons for the decision, including the written response to the approval criteria and to public comments received on this application, are included in the version located on the BDS website <http://www.portlandonline.com/bds/index.cfm?c=46429>. Click on the District Coalition then scroll to the relevant Neighborhood, and case number. If you disagree with the decision, you can appeal. Information on how to do so is included at the end of this decision.

### **CASE FILE NUMBER: LU 15-174182 HR – NEW GARAGE**

#### **GENERAL INFORMATION**

**Applicant:** Bonnie Harold / BWH Design  
1705 Fern Place / Lake Oswego, OR 97034

**Owner:** Christopher J Healy  
2517 NE 21st Avenue / Portland, OR 97212

**Site Address:** 2517 NE 21ST AVENUE  
**Legal Description:** BLOCK 24 N 30' OF LOT 9, IRVINGTON  
**Tax Account No.:** R420405180  
**State ID No.:** 1N1E26DA 01400  
**Quarter Section:** 2832

**Neighborhood:** Irvington, contact Dean Gisvold at 503-284-3885.  
**Business District:** North-Northeast Business Assoc, contact Joice Taylor at 503-841-5032.

**District Coalition:** Northeast Coalition of Neighborhoods, contact [info@necoalition.org](mailto:info@necoalition.org)  
**Other Designations:** Contributing house in the Irvington Historic District  
**Zoning:** R5, Single Dwelling Residential, 5000  
**Case Type:** HR, Historic Resource Review  
**Procedure:** Type I, an administrative decision with appeal to the Oregon Land Use Board of Appeals (LUBA).

**Proposal:**

The applicant is proposing to build a new garage in the southwest corner of the subject lot. The garage is 22 feet x 20 feet in area and is setback 18 inches from the west and south property lines. It will be accessed from an existing curb cut and driveway. The proposed materials, colors and architectural details will be similar to those on the existing house. The beveled cedar siding pattern, roofing material, door and window trim, corbel details, corner boards will match those on the existing house.

Historic Resource Review is required because the proposal is for non-exempt new construction in an historic district.

**Relevant Approval Criteria:**

In order to be approved, this proposal must comply with the approval criteria of Title 33. The relevant approval criteria are:

- 33.846 Historic Resource Review
- 33.846.060 G Other Approval Criteria

## ANALYSIS

**Site and Vicinity:** The subject property, a two-story Craftsman-style house, was built in 1913 on a 3,000-square-foot lot. The primary siding is a horizontal board applied in a 2-1 pattern, with two slats at a 3-inch reveal, then one with a 6-inch reveal. The house also has a mosaic and pebble front gable. The National Register of Historic Places documentation for the Irvington Historic District identifies the building as the J.J. Armstrong House, and evaluates it as a contributing resource within the historic context.

Platted in the late Nineteenth Century, today's Irvington Historic District represents the first additions to Portland that employed restrictive covenants from the outset. These included the exclusion of most non-residential uses from the interior of the neighborhood, and where non-residential uses were allowed, such as the fire station and the telephone exchange, the buildings were purposely disguised to appear more residential in character. Other deed restrictions excluded minority groups, established uniform front setbacks, and required minimum expenditure on new buildings. The area developed generally from southwest to northeast and its growth was greatly influenced by the installation of streetcar lines that introduced an easy commuting option to downtown.

The contributing resources in Irvington range in design character from expressions of the late Victorian Era styles, especially Queen Anne, through the many Period Revival modes of the early decades of the Twentieth Century, to a few early modernist examples. There is also a wide diversity in the sizes of lots and houses. In terms of the streetscape, the numbered north-south avenues in Irvington vary dramatically in width, and they mostly form rather long block faces which the houses generally face. The named east-west street block faces are more consistent in length, almost all being traditional 200' Portland blocks. All are lined with mature street trees. Original development in many cases included garages or other accessory structures, typically facing side streets on corner lots and accessed by a variety of driveway types on mid-block sites. Garages that were added within the historic period, were sometimes built at the sidewalk and/or out of architectural character with the house.

**Zoning:** The Residential 5,000 (R5) single-dwelling zone is intended to preserve land for housing and to provide housing opportunities for individual households. The zone implements the comprehensive plan policies and designations for single-dwelling housing. Minimum lot size is 3,000 square feet, with minimum width and depth dimensions of 36 and 50 feet, respectively. Minimum densities are based on lot size and street configuration. Maximum densities are 1 lot per 5,000 square feet of site area.

The Historic Resource Overlay zone protects certain historic resources in the region and preserves significant parts of the region's heritage. The regulations implement Portland's Comprehensive Plan policies that address historic preservation. These policies recognize the role historic resources have in promoting the education and enjoyment of those living in and visiting the region. The regulations foster pride among the region's citizens in their city and its heritage. Historic preservation beautifies the city, promotes the city's economic health, and helps to preserve and enhance the value of historic properties.

**Land Use History:** City records indicate there is one prior land use reviews for this site as follows:

- LU14-175282HR- approval for exterior alterations including rear expansion and new doors and windows.

**Agency Review:** A Notice of Proposal in your Neighborhood was mailed on **July 14, 2015**. No comments were received from the service bureaus.

**Neighborhood Review:** A Notice of Proposal in Your Neighborhood was mailed on **July 14, 2015**. One written response has been received from either the Neighborhood Association or notified property owners in response to the proposal.

1. Dean Gisvold, Irvington Community Association Land Use Committee, July 28, 2015, in support of the proposal.

## ZONING CODE APPROVAL CRITERIA

### Chapter 33.846.060 - Historic Resource Review

#### **Purpose of Historic Resource Review**

Historic Resource Review ensures the conservation and enhancement of the special characteristics of historic resources.

#### **Historic Resource Review Approval Criteria**

Requests for Historic Resource Review will be approved if the review body finds the applicant has shown that all of the approval criteria have been met.

**Findings:** The site is within the Irvington Historic District and the proposal is for non-exempt treatment. Therefore Historic Resource Review approval is required. The approval criteria are those listed in *33.846.060 G – Other Approval Criteria*.

*Staff has considered all guidelines and addressed only those applicable to this proposal.*

### 33.846.060 G - Other Approval Criteria

7. **Differentiate new from old.** New additions, exterior alterations, or related new construction will not destroy historic materials that characterize a property. New work will be differentiated from the old.

**Findings:** While the new garage will match the architectural style and design of the house it will be clearly differentiated from original resource by its materials and construction methods. In order to maintain the primacy of the house, the door and window surrounds details of the garage will be simpler, but similar to those on the rear elevation of the main house. No historic material will be destroyed or removed by the garage addition. *This criterion is therefore met.*

8. **Architectural compatibility.** New additions, exterior alterations, or related new construction will be compatible with the resource's massing, size, scale, and architectural features. When retrofitting buildings or sites to improve accessibility for persons with disabilities, design solutions will not compromise the architectural integrity of the historic resource;
9. **Preserve the form and integrity of historic resources.** New additions and adjacent or related new construction will be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic resource and its environment would be unimpaired; and
10. **Hierarchy of compatibility.** Exterior alterations and additions will be designed to be compatible primarily with the original resource, secondarily with adjacent properties, and finally, if located within a Historic or Conservation District, with the rest of the district. Where practical, compatibility will be pursued on all three levels.

**Findings for 8, 9 & 10:** The proposed detached garage is located in the rear and southwest corner of the subject property and will be accessed from an existing driveway along the south property line. It has a total height of 17'8" with a covered area of 440 sf.

Both the size and location of the new garage would be consistent with the existing garages in the Irvington Historic District. It will also not entail creating new curb cuts along the street lot line.

The garage has been designed to be compatible with the primary residence's architectural style, design details color and materials. The gable roof for the garage closely matches that of the primary residence. The gable end and corbel details, as well as the gutter and associated crown pieces will be similar to that of the contributing resource. The proposed materials are of high quality and include Simpson 6 lite over beaded panel wood door, carriage house style wood garage door, and Marvin metal clad wood window. The proposed window and door will closely replicate the original windows in their profile, shadow lines and material. Architectural elements like the corbels, corner boards, beveled cedar siding, doors and windows trims also match those of the existing house. In order to maintain the primacy of the house, the door and window surrounds of the garage will be simpler, but similar to those on the rear elevation of the main house. The proposed garage will be compatible with the original resource, adjacent properties and the rest of Irvington Historic District. *These criteria are therefore met.*

## **DEVELOPMENT STANDARDS**

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

## **CONCLUSIONS**

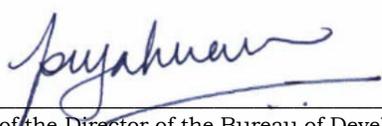
The purpose of the Historic Resource Review process is to ensure that additions, new construction, and exterior alterations to historic resources do not compromise their ability to convey historic significance. The project utilizes compatible materials and design elements that respect the character of the Craftsman-style residence and the surrounding Historic District. This proposal meets the applicable Historic Resource Review criteria and therefore warrants approval.

## **ADMINISTRATIVE DECISION**

Approval of a new garage addition, 22 feet x 20 feet in area and setback 18 inches from the west and south property lines, in the Irvington Historic District per the approved site plans, Exhibits C-1 through C-9, signed and dated 7/30/2015, subject to the following conditions:

- A. As part of the building permit application submittal, each of the 4 required site plans and any additional drawings must reflect the information and design approved by this land use review as indicated in Exhibits C.1-C.9. The sheets on which this information appears must be labeled, "Proposal and design as approved in Case File # LU 15-174182 HR."

**Staff Planner: Puja Bhutani**

**Decision rendered by:**  **on July 30, 2015**

By authority of the Director of the Bureau of Development Services

**Decision mailed: August 11, 2015**

**Procedural Information.** The application for this land use review was submitted on May 21, 2015, and was determined to be complete on **July 8, 2015**.

*Zoning Code Section 33.700.080* states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the

application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on May 21, 2015.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period.

**Some of the information contained in this report was provided by the applicant.**

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the decision of the Bureau of Development Services with input from other City and public agencies.

**Conditions of Approval.** If approved, this project may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term "applicant" includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

**This decision, and any conditions associated with it, is final.** It may be appealed to the Oregon Land Use Board of Appeals (LUBA), within 21 days of the date the decision is mailed, as specified in the Oregon Revised Statute (ORS) 197.830. Among other things, ORS 197.830 requires that a petitioner at LUBA must have submitted written testimony during the comment period for this land use review. Contact LUBA at 775 Summer St NE Suite 330, Salem, OR 97301-1283 or phone 1-503-373-1265 for further information.

The file and all evidence on this case are available for your review by appointment only. Please call the Request Line at our office, 1900 SW Fourth Avenue, Suite 5000, phone 503-823-7617, to schedule an appointment. I can provide some information over the phone. Copies of all information in the file can be obtained for a fee equal to the cost of services. Additional information about the City of Portland, city bureaus, and a digital copy of the Portland Zoning Code is available on the internet at [www.portlandonline.com](http://www.portlandonline.com).

**Recording the final decision.**

If this Land Use Review is approved, the final decision must be recorded with the Multnomah County Recorder. A building or zoning permit will be issued only after the final decision is recorded. The final decision may be recorded on or after August 11, 2015.

The applicant, builder, or a representative may record the final decision as follows:

- By Mail: Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- In Person: Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034  
For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-7617.

**Expiration of this approval.** An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

**Applying for your permits.** A building permit, occupancy permit, or development permit may be required before carrying out an approved project. At the time they apply for a permit, permittees must demonstrate compliance with:

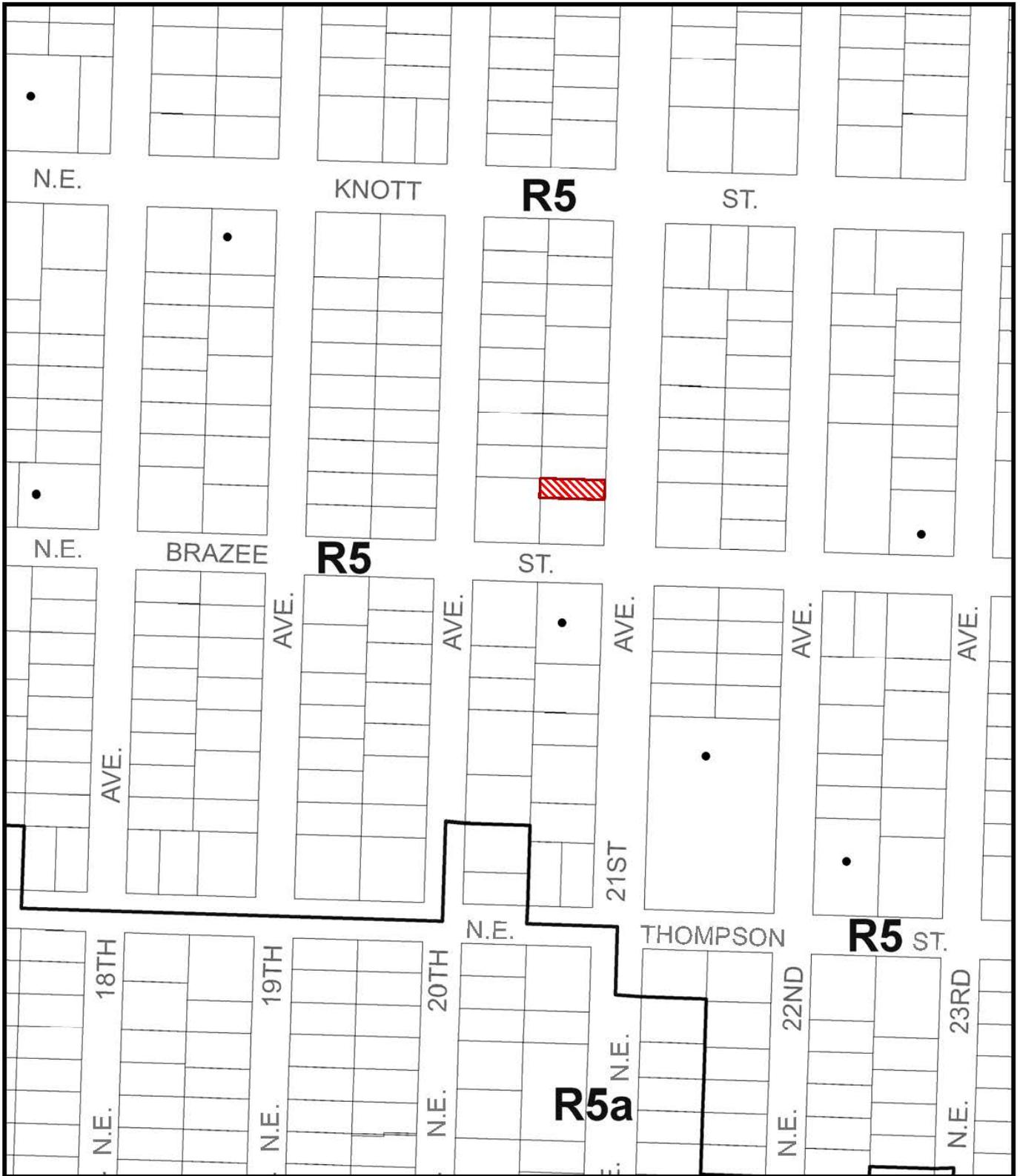
- All conditions imposed herein;
- All applicable development standards, unless specifically exempted as part of this land use review;
- All requirements of the building code; and
- All provisions of the Municipal Code for the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

#### **EXHIBITS**

NOT ATTACHED UNLESS INDICATED

- A. Applicant's Statement and photographs.
- B. Zoning Map (attached)
- C. Plans/Drawings:
  - 1. Site Plan (attached)
  - 2. Garage floor plan
  - 3. Front Elevation (attached)
  - 4. North Elevation (attached)
  - 5. Typical section through garage and house
  - 6. Section through existing and proposed window
  - 7. Simpson door manufacturers cut sheet
  - 8. Northwest Garage door manufacturers cut sheet
  - 9. Marvin Ultimate Casement, Awning and Picture manufacturers cut sheet
- D. Notification information:
  - 1. Mailing list
  - 2. Mailed notice
- E. Agency Responses: No responses were received.
- F. Correspondence:
  - 1. Dean Gisvold, ICA Land Use Committee, 7/28/5, no objections.
- G. Other:
  - 1. Original LU Application
  - 2. Incomplete letter from staff to applicant, sent 6/9/15

**The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).**



# ZONING



Site



Historic Landmark



This site lies within the:  
**IRVINGTON HISTORIC DISTRICT**

File No. LU 15-174182 HR  
 1/4 Section 2832  
 Scale 1 inch = 200 feet  
 State\_Id 1N1E26DA 1400  
 Exhibit B (May 26, 2015)

\*Approved\*

City of Portland - Bureau of Development Services

Planner P. Blumstein Date 7-30-15

\* This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

**2517 NE 21st Avenue  
Portland OR 97221**

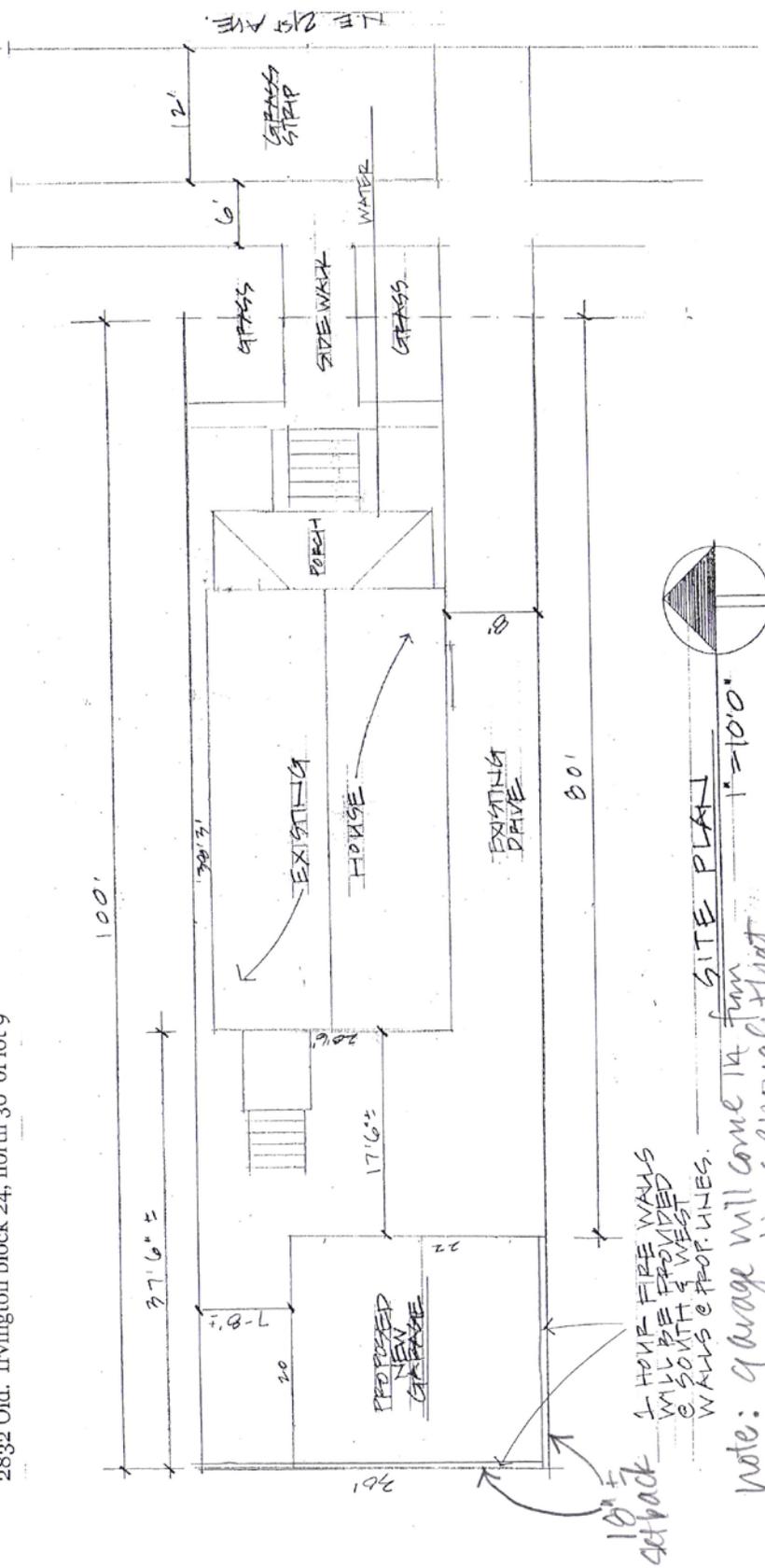
Main floor 748  
 Finished 2nd floor 680  
 Finished basement 680  
 Exist Black top drive 800  
 Covered porch 126

Property Id: R187885  
 State Id: 1N1E26DA 1400  
 Alt Acct: R40405180

Scope of work: New 440 sq. ft. garage matching architecture and details of existing home.  
 LOT: 3000 #

2832 Old. Irvington block 24, north 30' of lot 9

CLIENT: Christopher Healy 2517 NE 21st Avenue Portland OR 97212	SHEET: SITE PLAN	503-789-2770 LAKE OSWEGO OR 97034
DATE: 18 MAY 2015	REVISIONS:	

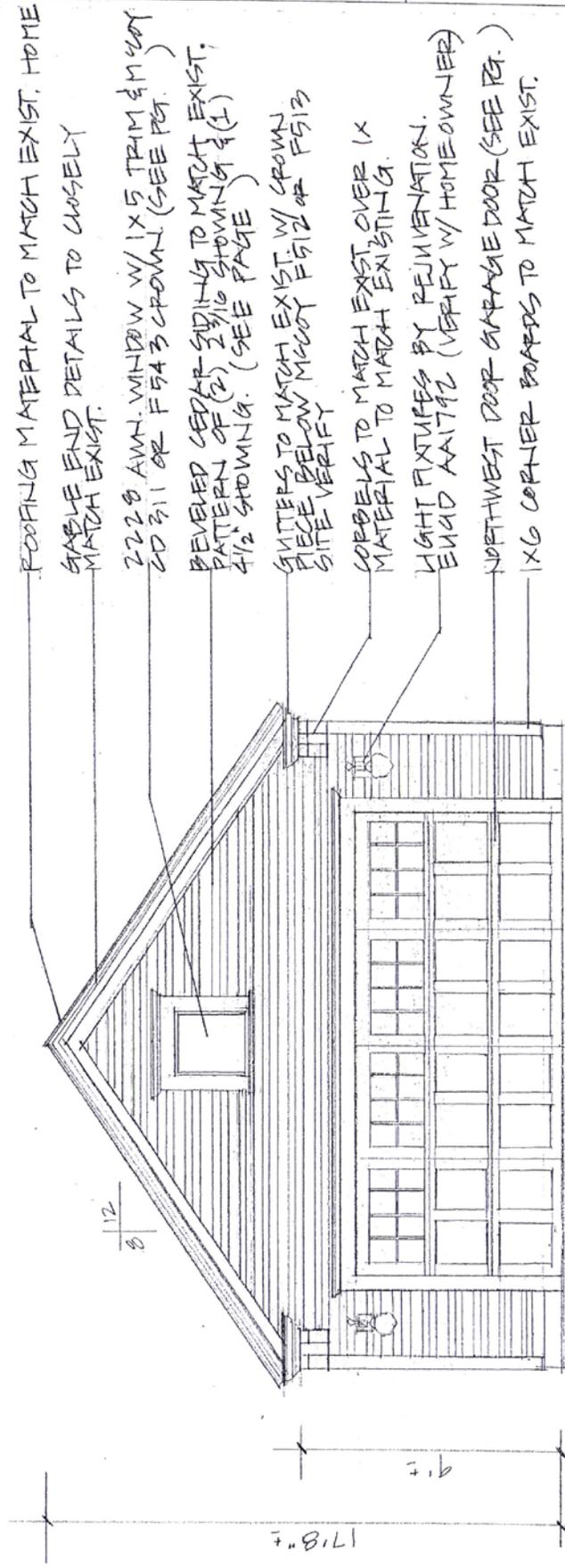


1 HOUR FIRE WALLS WILL BE PROVIDED @ SOUTH & WEST WALLS @ PROP. LINES.

Note: Garage will come 14' from property lines enough that all work can be completed on premises.

\*Approved\*  
 City of Portland - Bureau of Development Services  
 Planner: P. Buriani Date: 7/30/15

\* This approval applies only to the revisions requested and is subject to all conditions of approval. Additional zoning requirements may apply.



17'8" | 9'7" | 16'0" | 22'0" | 3'0" | 3'0"

FRONT ELEVATION  
 1/4" = 1'0"

- ROOFING MATERIAL TO MATCH EXIST. HOME
- GABLE END DETAILS TO CLOSELY MATCH EXIST.
- 2-28 AWN. WINDOW W/ 1X5 TRIM & MOUNT TO 311 OR F543 CROWN. (SEE PG.)
- BEVELLED CEDAR SIDING TO MATCH EXIST. PATTERN OF (2) 2 3/16 SHOWING & (1) 4 1/2 SHOWING. (SEE PAGE)
- GUTTERS TO MATCH EXIST. W/ CROWN PIECE BELOW MCCOY F512 @ F513 SITE VERIFY
- CORBELS TO MATCH EXIST. OVER 1X MATERIAL TO MATCH EXISTING.
- LIGHT FIXTURES BY REJUVENATION. ENJOY AA1742 (VERIFY W/ HOMEOWNER)
- NORTHWEST DOOR GARAGE DOOR (SEE PG.)
- 1X6 CORNER BOARDS TO MATCH EXIST.

CLIENT: Christopher Healy 2517 NE 21st Avenue Portland OR 97212	SHEET: FRONT ELEVATION	REVISIONS:
DATE: 18 MAY 2015		
• bwh·design BONNIE WILSON HAROLD 1705 FERN PLACE LAKE OSWEGO OR 97034 503-789-2770		SHEET 2/6

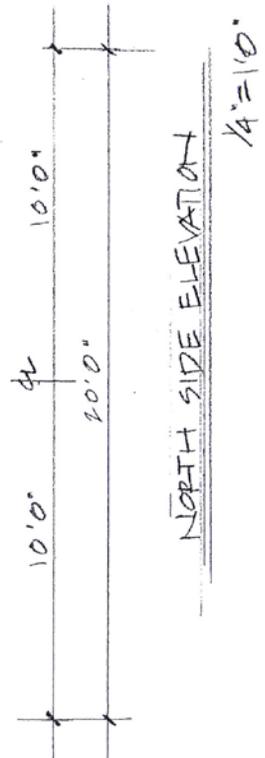
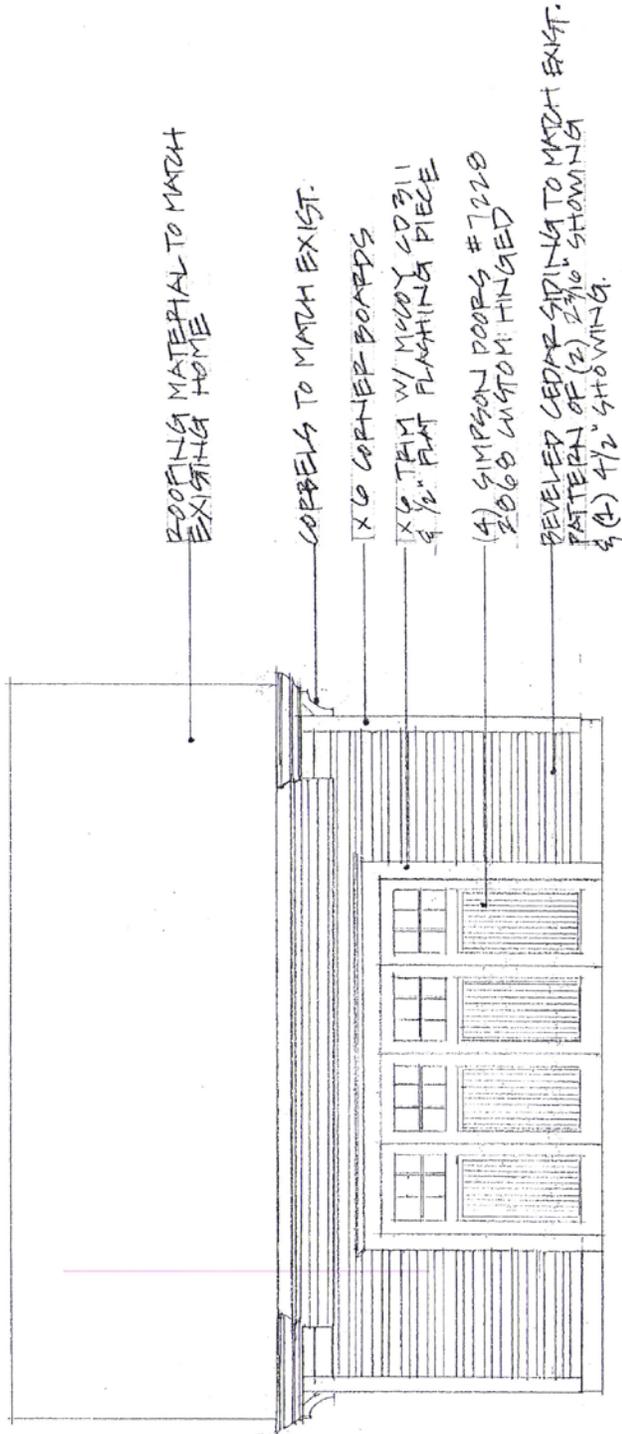
\*Approved\*

City of Portland - Bureau of Development Services

P-Burstein

Date 7-30-15

val applies only to the reviews requested and is subject to all  
of approval. Additional zoning requirements may apply.



DATE: 18 MAY 2015 REVISIONS:	CLIENT: Christopher Healy 2517 NE 21st Avenue Portland OR 97212	SHEET: NORTH SIDE ELEV. 503-789-2770 BONNIE WILSON HAROLD 1705 FERN PLACE LAKE OSWEGO OR 97034	bwh-design SHEET 3/6
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