



**City of Portland, Oregon**  
**Bureau of Development Services**  
**Land Use Services**

FROM CONCEPT TO CONSTRUCTION

Dan Saltzman, Commissioner  
Paul L. Scarlett, Director  
Phone: (503) 823-7300  
Fax: (503) 823-5630  
TTY: (503) 823-6868  
[www.portlandoregon.gov/bds](http://www.portlandoregon.gov/bds)

**Date:** December 14, 2012  
**To:** Interested Person  
**From:** Mark Walhood, City Planner  
503-823-7806 / [Mark.Walhood@portlandoregon.gov](mailto:Mark.Walhood@portlandoregon.gov)

## **NOTICE OF A TYPE II DECISION ON A PROPOSAL IN YOUR NEIGHBORHOOD**

The Bureau of Development Services has approved a proposal in your neighborhood. The reasons for the decision are included in the version located on the BDS website <http://www.portlandonline.com/bds/index.cfm?c=46429>. Click on the District Coalition then scroll to the relevant Neighborhood, and case number. If you disagree with the decision, you can appeal. Information on how to do so is included at the end of this decision.

### **CASE FILE NUMBER: LU 12-160459 DZ – SW GROVER R.O.W.**

#### **GENERAL INFORMATION**

**Applicant:** Jonathan Ledesma / Project ^ Ecological Development  
413 SW 13<sup>th</sup> Avenue, Suite 300 / Portland, OR 97205

**Architect:** Gary Blackwell / ZGF Architects LLP  
1223 SW Washington St., Suite 200 / Portland, OR 97205

**Owner Agent:** Matt French / ZRZ Realty Company  
3121 SW Moody Avenue / Portland, OR 97239

**Owner:** Z R Z Realty Company  
3121 SW Moody Ave / Portland, OR 97239-4505

**Site Address:** Block west of SW Moody NW of SW Moody and SW Grover Intersection, and SW Grover right-of-way between SW Moody and Interstate 5 ramp to west

**Legal Description:** BLOCK 101 LOT 1-4 TL 100, CARUTHERS ADD; BLOCK 101 LOT 1-3&5&6 TL 11400, CARUTHERS ADD; BLOCK 101 TL 11500, CANCEL ACCOUNT / CARUTHERS ADD, BLOCK 101 TL 11500

**Tax Account No.:** R140910080, R140910100, R140910110

**State ID No.:** 1S1E10BD 00100, 1S1E10BD 11400, 1S1E10BD 11500

**Quarter Section:** 3330

**Neighborhood:** South Portland NA., contact Jim Davis at 503-248-9820.

**Business District:** South Portland Business Assoc, Kevin Countryman at 503-750-2984.

**District Coalition:** Southwest Neighborhoods Inc., contact Leonard Gard at 503-823-4592.

**Zoning:** **CXd** (Central Commercial base zone with Design overlay zone), **Central City plan district**

**Case Type:** **DZ** (Design Review)

**Procedure:** **Type II**, an administrative decision with appeal to the Design Commission.

**PROPOSAL:** The applicant is in the process of developing a new mixed-use apartment building and associated driveway access improvements to the loading dock just south of the building at

this site. The building received approval from Design Commission recently through case file # LU 12-118988 DZM, which included an advisory note that the driveway access improvements require a separate follow-up Type II Design Review.

The proposal includes re-grading and at-grade surface improvements to what is currently the SW Grover right-of-way running west from SW Moody Avenue to the adjacent Interstate 5 access ramps immediately to the east. The design proposal includes a semi-circular driving surface with an alternating light broom and sandblasted appearance to the concrete paving, giving a striped, textural appearance. There are raised or curbed concrete stormwater planters on both the north and south sides of the driveway, ipe wood decking and stairs projecting from the south face of the building along SW Moody, and a single freestanding black metal pole light on the south side of the paving. Landscaping is provided to the west and south of the driveway paving area, except where a fenced electrical substation will be placed on the hillside, screened behind a decorative steel fence. The areas to the north of the driveway about the proposed building, and are generally treated as paved concrete pedestrian walkways, except where there are raised planters. As part of this work, in order to accommodate minimum horizontal clearances around two building transformers placed in Grover just south of the building, a small inset niche has been created at the ground floor of the recently-approved apartment building.

The proposal is being reviewed as both non-standard improvements in the right-of-way, and with consideration to the fact that the Grover right-of-way may eventually become private property, pending the outcome of a recent application for a Street Vacation at the site. In either case, the proposed alterations require a Type II Design Review either as non-standard improvements in the right-of-way, or as non-exempt exterior alterations on private property.

**RELEVANT APPROVAL CRITERIA:** In order to be approved, this proposal must comply with the approval criteria of Title 33. The relevant criteria are:

- The *Central City Fundamental Design Guidelines*; and
- The *South Waterfront Design Guidelines*.

## ANALYSIS

**Site and Vicinity:** The site is a collection of three parcels with an irregular shape, as well as the abutting right-of-way in SW Grover Street. The site is located on the west frontage of SW Moody Avenue, bound by and including SW Grover Street on the south end, and by the Ross Island Bridge to the north. The private parcels are currently vacant of buildings; gravel covers the parcels, and several large metal pipe and utility conduit materials are being stored on the gravel. Directly abutting the SW Moody right-of-way, the eastern edge of the site is defined by a continuous row of large concrete retaining wall blocks, with a chain link fence running along the top of the blocks. The site slopes up to the west, away from the street, where a landscaped hillside rises up from the west edge of the site to a ramp that connects northbound SW Hood Street to southbound SW Macadam Avenue, as part of the Ross Island bridgehead circulation system. The Grover Street right-of-way includes several above-grade electrical vault structures and an electrical substation, as well as some limited concrete sidewalk surfacing and a partially improved asphalt roadway.

The site is in a transitioning area just north of the grouping of newer medical and residential towers in the South Waterfront District, which begins approximately two blocks south of the site. The site is directly across the street from the Zidell Marine ship yard. Directly north of the site is the unimproved gravel and weedy area framed by the support piers for the Ross Island Bridge, whose main deck rises approximately 70 to 80 feet above the grade at SW Moody.

The abutting right-of-way in SW Moody has recently been improved with full right-of-way improvements, including a paved two-way roadway, curbing, and sidewalks. On the west frontage of SW Moody adjacent to the site, there is a weaving and separated bike path and

pedestrian walkway zone, creating an extra-deep paved area between the site and the streetcar tracks and roadway. New street lights, street signs, vegetated stormwater planters, decorative brick paving within the concrete, and painted identification symbols for the walkway versus bikeway complete the adjacent right-of-way improvements in SW Moody.

**Zoning:** The Central Commercial (CX) base zone is intended to provide for commercial development within Portland's most urban and intense areas. A broad range of uses is allowed to reflect Portland's role as a commercial, cultural and governmental center. Development is intended to be very intense with high building coverage, large buildings, and buildings placed close together. Development is intended to be pedestrian-oriented with a strong emphasis on a safe and attractive streetscape. The Central City plan district implements the Central City Plan and other plans applicable to the Central City area. These other plans include the Downtown Plan, the River District Plan, the University District Plan, and the Central City Transportation Management Plan. The Central City plan district implements portions of these plans by adding code provisions that address special circumstances existing in the Central City area.

**Land Use History:** City records indicate three prior land use reviews for the site:

- *PC 7550* was a large legislative zone change by the Planning Commission in 1985, changing the zoning for a large area as a result of the Macadam Corridor Study;
- *LU 11-178242 LDS* is an in-process Subdivision to divide 30 acres into 17 parcels, 14 greenway tracts, and public rights-of-way for SW Moody and SW River Parkway, SW Bond, SW Woods, SW Gibbs, and SW Grover in four stages. Because this site is already separated from adjacent sites through taxlot lines and rights-of-way, this application can proceed for new development without awaiting the outcome of the Subdivision; and
- *LU 12-118988 DZM* is the recently-approved Design Review with Modifications to approve the mixed-use apartment building on the site, including Modifications to setbacks and required building lines along SW Moody and an Oriel Window Exception. This review required the follow-up Type II Design Review for the Grover right-of-way improvements being satisfied by this review.

**Agency Review:** A "Notice of Proposal in Your Neighborhood" was mailed **November 19, 2012**. The following Bureaus have responded:

The *Water Bureau* has reviewed the proposal and provided informational comments, but no objections regarding the requested Design Review. There are currently no water services to this property, and the process to acquire water services to this location is currently in the design phase by the Portland Water Bureau. Exhibit E.1 contains staff contact and additional information.

The *Life Safety Section of the Bureau of Development Services* has reviewed the proposal and provided informational comments, but no objections or concerns regarding the requested Design Review. A complete Life Safety review is occurring with the building permit review process, which is currently underway. The Life Safety Section refers the applicant to permit-related checksheets and correspondence for further details. Exhibit E.2 contains staff contact and additional information.

The *Urban Forestry Division of Portland Parks and Recreation* has reviewed the proposal and responded with no concerns. Exhibit E.3 is a hard copy printout of their electronic 'no concerns' response.

The *Bureau of Environmental Services (BES)* has reviewed the proposal and provided informational comments, but no recommended conditions of approval. Note that this BES response does not alter BES requirements as identified under the building permit application 12-149959 CO, which is currently under review. The proposal will have to meet all applicable sanitary sewer connection requirements, as well as the City's Stormwater Management Manual. An encroachment permit must be issued by Portland Transportation along with the building permit in order to allow the improvements proposed by the applicant in the right-of-way. At

this time, BES has not received information from the applicant documenting that the proposed wall and deck will not impact the public sewer and manhole in SW Grover Street. At such time that the applicant provides additional information to BES regarding the manhole in relation to the proposal, BES will respond with any additional requirements, which could include a requirement that the wall and/or deck be reconfigured. In addition, BES will require language in the encroachment permit stating that BES must retain maintenance access to the manhole and sewer. Any private improvements impeding that access may be removed by BES; replacement and associated costs will be the responsibility of the owner. **(NOTE:** after discussions with the applicant and BES staff, the potential conflict with the manhole cover and the wood deck projecting south of the building will be addressed in the findings of this report, through a condition of approval allowing the footing design for the deck to change, and/or allowing the deck to be re-located up to 18 inches further to the north). Exhibit E.4 contains staff contact and additional information.

The *Development Review Section of Portland Transportation* has reviewed the proposal for transportation-related impacts, including traffic impacts and conformance with adopted policies, street designations, and relevant issues and standards in Titles 17 and 33. Portland Transportation has no objection to approval of the requested Design Review. Construction of the private improvements within this section of SW Grover will be done under an encroachment permit to be approved by Portland Transportation, BES, and the Water Bureau as part of the building permit review. It is anticipated that most of the SW Grover right-of-way stub west of SW Moody will be vacated in the near future. Exhibit E.5 contains staff contact and additional information.

**Neighborhood Review:** A Notice of Proposal in Your Neighborhood was mailed on November 19, 2012. No written responses have been received from either the Neighborhood Association or notified property owners in response to the proposal.

## **ZONING CODE APPROVAL CRITERIA**

### **Chapter 33.825 Design Review**

#### **Section 33.825.010 Purpose of Design Review**

Design review ensures that development conserves and enhances the recognized special design values of a site or area. Design review is used to ensure the conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district or area. Design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area. Design review is also used in certain cases to review public and private projects to ensure that they are of a high design quality.

#### **Section 33.825.055 Design Review Approval Criteria**

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design guidelines for the area.

**Findings:** The site is designated with a design (d) overlay zone, therefore the proposal requires Design Review approval. Because of the site's location, the applicable design guidelines are the Central City Fundamental Design Guidelines and the South Waterfront Design Guidelines.

### **Central City Plan Design Goals**

1. Encourage urban design excellence in the Central City;
2. Integrate urban design and preservation of our heritage into the development process;
3. Enhance the character of the Central City's districts;
4. Promote the development of diversity and areas of special character within the Central City;
5. Establish an urban design relationship between the Central City's districts and the Central City as a whole;
6. Provide for a pleasant, rich and diverse pedestrian experience for pedestrians;
7. Provide for the humanization of the Central City through promotion of the arts;

8. Assist in creating a 24-hour Central City which is safe, humane and prosperous;
9. Ensure that new development is at a human scale and that it relates to the scale and desired character of its setting and the Central City as a whole.

### **South Waterfront Design Goals**

The South Waterfront Design Guidelines and the Greenway Design Guidelines for the South Waterfront supplement the Central City Fundamental Design Guidelines. These two sets of guidelines add layers of specificity to the fundamentals, addressing design issues unique to South Waterfront and its greenway.

The South Waterfront Design Guidelines apply to all development proposals in South Waterfront within the design overlay zone, identified on zoning maps with the lowercase letter “d”. These guidelines primarily focus on the design characteristics of buildings in the area, including those along Macadam Avenue, at the western edge, to those facing the greenway and river.

The Greenway Design Guidelines for the South Waterfront apply to development within the greenway overlay zone, identified on zoning maps with a lowercase “g”. These design guidelines focus on the area roughly between the facades of buildings facing the river and the water’s edge.

### **South Waterfront Design Guidelines and Central City Fundamental Design Guidelines**

The Central City Fundamental Design and the South Waterfront Design Guidelines and the Greenway Design Guidelines for South Waterfront focus on four general categories. **(A) Portland Personality**, addresses design issues and elements that reinforce and enhance Portland’s character. **(B) Pedestrian Emphasis**, addresses design issues and elements that contribute to a successful pedestrian environment. **(C) Project Design**, addresses specific building characteristics and their relationships to the public environment. **(D) Special Areas**, provides design guidelines for the four special areas of the Central City.

*Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project.*

**A4. Use Unifying Elements.** Integrate unifying elements and/or develop new features that help unify and connect individual buildings and different areas.

**A4-1 Integrate Ecological Concepts in Site And Development Design.** Incorporate ecological concepts as integral components of urban site and development designs.

**A4-2 Integrate Stormwater Management Systems in Development.** Integrate innovative stormwater management systems with the overall site and development designs.

**A5. Enhance, Embellish, and Identify Areas.** Enhance an area by reflecting the local character within the right-of-way. Embellish an area by integrating elements in new development that build on the area’s character. Identify an area’s special features or qualities by integrating them into new development.

**Findings for A4, A4-1, A4-2 & A5:** The proposed stormwater planters that flank the north and south sides of SW Grover Street help to detain and treat stormwater runoff from the right-of-way. Located between the street and sidewalk/pedestrian areas, the planters provide a buffer between vehicular movement and pedestrians, as found elsewhere in the South Waterfront area. The use of native plants in the planters is reflective of plantings in the surrounding district, as well as along the larger streetcar corridor leading to the site. The design of the stormwater planters are well-integrated with the form and function of the vehicle area in SW Grover Street, as they frame both sides of the roadway upon entry from SW Moody Avenue. The concrete materials of the planters themselves are reflective of the street and building design overall. *Therefore, these guidelines are met.*

**B1. Reinforce and Enhance the Pedestrian System.** Maintain a convenient access route for

pedestrian travel where a public right-of-way exists or has existed. Develop and define the different zones of a sidewalk: building frontage zone, street furniture zone, movement zone, and the curb. Develop pedestrian access routes to supplement the public right-of-way system through superblocks or other large blocks.

**B1-1. Facilitate Transit Connections.** Orient the main entrances of buildings at streets served by public transit to conveniently and directly connect pedestrians with transit services.

**B2. Protect the Pedestrian.** Protect the pedestrian environment from vehicular movement. Develop integrated identification, sign, and sidewalk-oriented night-lighting systems that offer safety, interest, and diversity to the pedestrian. Incorporate building equipment, mechanical exhaust routing systems, and/or service areas in a manner that does not detract from the pedestrian environment.

**B3. Bridge Pedestrian Obstacles.** Bridge across barriers and obstacles to pedestrian movement by connecting the pedestrian system with innovative, well-marked crossings and consistent sidewalk designs.

**B4. Provide Stopping and Viewing Places.** Provide safe, comfortable places where people can stop, view, socialize and rest. Ensure that these places do not conflict with other sidewalk uses.

**B7. Integrate Barrier-Free Design.** Integrate access systems for all people with the building's overall design concept.

**Findings for B1, B1-1, B2, B3, B4 & B7:** A portion of the north sidewalk in SW Grover is designated as a terrace space, where outdoor dining, socializing, and people-watching can occur. This terrace will provide an opportunity for people to gather, interact, and enjoy the public realm, even during evening hours. The board decking material helps to emphasize a pedestrian scale, and provides a visual and physical material link to the adjacent building.

Southwest Grover Street is designed as a curbless street. As such, it provides a smooth transition for pedestrians and bicyclists traveling between the sites that are to the north and south of the SW Grover right-of-way. The concrete surface of the roadway helps to reinforce the unique character of this dead-end roadway as a combination residential plaza and driveway, while the metal bollards and raised ceramic 'pucks' provide visual and physical definition to the pedestrian through-zone for when vehicle traffic is present. Mechanical equipment on the building is located away from the main pedestrian areas along SW Moody and into the first half of the SW Grover right-of-way from Moody. The roadway improvements have been provided with a curbless design that allows for easy access, and the main accessible ramp to the building is provided directly at the corner of SW Moody and Grover, framed by the main entry steps from SW Moody and the wood deck projecting into SW Grover.

The proposed wood terrace is located partially above a sewer manhole, and staff from BES has noted that the location of this terrace and/or the footing structure for the terrace may need to change during permitting. The design of the footing would not be visible, and an L-shaped footing may address the issue and be invisible after construction is complete. Alternatively, the deck may need to be pulled back up to 18 inches further north, towards the building, in order to avoid conflicts with the manhole. A modest reduction to the size of the deck would still result in a successful outdoor terrace and pedestrian area, provided the area where the terrace is removed is provided with the matching striped concrete surface as found in the roadway. Therefore, a condition of approval will allow up to 18 inches of the terrace to be removed off the southern edge, provided that the area where the terrace was proposed be completed with the same dual-finish concrete surface as proposed for the roadway otherwise.

*Therefore, with the condition of approval regarding possible reduction in terrace size and the replacement surface finish, these guidelines can be met.*

**C2. Promote Quality and Permanence in Development.** Use design principles and building materials that promote quality and permanence.

**C3. Respect Architectural Integrity.** Respect the original character of an existing building when modifying its exterior. Develop vertical and horizontal additions that are compatible with the existing building, to enhance the overall proposal's architectural integrity.

**C10. Integrate Encroachments.** Size and place encroachments in the public right-of-way to visually and physically enhance the pedestrian environment. Locate permitted skybridges toward the middle of the block, and where they will be physically unobtrusive. Design skybridges to be visually level and transparent.

**Findings for C2, C3 & C10:** The proposed changes to the building include the creation of a small inset niche on one bay of the south façade where electrical transformers are required to be directly adjacent to the building. This area, west of the loading bay doors and at the interior edge of the site adjacent to the turnaround in SW Grover and the steep portion of the hillside, will have limited presence and visibility, and would be provided with concrete masonry block wall finishing as found elsewhere on the interior ground floors of the building.

The design of the right-of-way includes a curbless concrete roadway with two finishes applied in a striped diagonal pattern, with alternating 'broom finish' and sandblasted finish, creating visual texture to the roadway. Concrete curbing and stormwater planters, painted metal bollards and vertical 12'-0" tall street lights are durable, straightforward, contemporary materials that will complement the overall building design. The weathered steel fencing at the PGE substation at the west edge of the roadway will help to visually obscure the equipment in the right-of-way, while also integrating well with the materials and design of the adjacent apartment building. Landscape planting of grass around the perimeter of the roadway where sidewalks are not located will complement the native plants in the stormwater planters, provide a green edge to the hardscaped areas, and provide a transition to the vegetated hillside above.

All the proposed alterations in SW Grover are technically encroachments, as they are private improvements in a public right-of-way that has not been vacated. No skybridges are proposed. The proposed electrical transformers, vault cover, and substation are in the right-of-way today, but these elements have been visually incorporated into the design by placing them at the edge of the area as much as possible, and through screening methods such as the weathering steel fence.

*Therefore, these guidelines are met.*

## DEVELOPMENT STANDARDS

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

## CONCLUSIONS

The applicant has proposed alterations to a public right-of-way in SW Grover west of SW Moody, as well as minor changes to the design of the building on the adjacent lot to the north. The project is able to meet the relevant design guidelines and should be approved.

## ADMINISTRATIVE DECISION

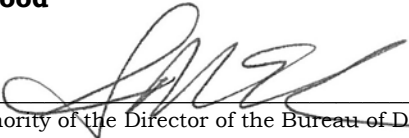
**Approval of Design Review** for alterations in and adjacent to the SW Grover right-of-way west of SW Moody Avenue, in the South Waterfront subdistrict of the Central City plan district, including the following specific elements:

- A curbless concrete roadway and pedestrian surface, provided with an alternating broom finish and sandblasted finish, to create a striped, textural pattern;
- A projecting wood terrace extending south from the building towards the roadway, which serves as a private terrace and pedestrian access from SW Moody onto SW Grover;
- Raised ceramic traffic ‘pucks’ and cylindrical painted metal bollards to define pedestrian zones in the shared pedestrian/vehicle paved areas;
- Painted metal light standards at 12’-0” tall;
- Concrete stormwater planters on both the north and south sides of the shared roadway/pedestrian surface;
- A substation, two electrical transformers, and a PGE electrical vault cover;
- Weathered steel fencing and gate at the west-facing perimeter of the substation; and
- An inset concrete wall and modified area at and just west of the loading bay on the south elevation, where the façade must be recessed to provide clearance for the electrical equipment.

This approval is granted based on the approved plans and drawings, Exhibits C.1 through C.34, all signed and dated December 11, 2012, and subject to the following conditions:

- A. As part of the building permit application submittal, each of the 4 required site plans and any additional drawings must reflect the information and design approved by this land use review as indicated in Exhibits C.1-C.34. The sheets on which this information appears must be labeled, "Proposal and design as approved in Case File # LU 12-160459 DZ."
- B. The wood terrace projecting from the southeast corner of the building into SW Grover may have its footings reconfigured, or be reduced in depth up to a maximum of 18 inches in order to prevent conflicts with the adjacent BES sewer manhole. The area where wood terrace is pulled back towards the building must be provided with a dual-textured concrete finish matching the remainder of the shared roadway/pedestrian area in SW Grover.

**Staff Planner: Mark Walhood**

**Decision rendered by:**  **on December 11, 2012.**  
By authority of the Director of the Bureau of Development Services

**Decision mailed: December 14, 2012**

**About this Decision.** This land use decision is **not a permit** for development. Permits may be required prior to any work. Contact the Development Services Center at 503-823-7310 for information about permits.

**Procedural Information.** The application for this land use review was submitted on July 11, 2012, and was determined to be complete on **November 8, 2012.**

*Zoning Code Section 33.700.080* states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on July 11, 2012.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period. Unless further extended by the applicant, **the 120 days will expire on March 8, 2013.**



**Some of the information contained in this report was provided by the applicant.**

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the decision of the Bureau of Development Services with input from other City and public agencies.

**Conditions of Approval.** If approved, this project may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term “applicant” includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

**Appealing this decision.** This decision may be appealed to the Design Commission, which will hold a public hearing. Appeals must be filed **by 4:30 PM on December 28, 2012** at 1900 SW Fourth Ave. Appeals can be filed Tuesday through Friday on the first floor of the Development Services Center until 3 p.m. After 3 p.m. and Mondays, appeals must be submitted to the receptionist at the front desk on the fifth floor. **An appeal fee of \$250 will be charged.** The appeal fee will be refunded if the appellant prevails. There is no fee for ONI recognized organizations appealing a land use decision for property within the organization’s boundaries. The vote to appeal must be in accordance with the organization’s bylaws. Assistance in filing the appeal and information on fee waivers is available from BDS in the Development Services Center. Please see the appeal form for additional information.

The file and all evidence on this case are available for your review by appointment only. Please call the Request Line at our office, 1900 SW Fourth Avenue, Suite 5000, phone 503-823-7617, to schedule an appointment. I can provide some information over the phone. Copies of all information in the file can be obtained for a fee equal to the cost of services. Additional information about the City of Portland, city bureaus, and a digital copy of the Portland Zoning Code is available on the internet at [www.portlandonline.com](http://www.portlandonline.com).

**Attending the hearing.** If this decision is appealed, a hearing will be scheduled, and you will be notified of the date and time of the hearing. The decision of the Design Commission is final; any further appeal must be made to the Oregon Land Use Board of Appeals (LUBA) within 21 days of the date of mailing the decision, pursuant to ORS 197.620 and 197.830. Contact LUBA at 550 Capitol St. NE, Suite 235, Salem, Oregon 97301, or phone 1-503-373-1265 for further information.

Failure to raise an issue by the close of the record at or following the final hearing on this case, in person or by letter, may preclude an appeal to the Land Use Board of Appeals (LUBA) on that issue. Also, if you do not raise an issue with enough specificity to give the Design Commission an opportunity to respond to it, that also may preclude an appeal to LUBA on that issue.

**Recording the final decision.**

If this Land Use Review is approved the final decision must be recorded with the Multnomah County Recorder. A few days prior to the last day to appeal, the City will mail instructions to the applicant for recording the documents associated with their final land use decision.

- *Unless appealed,* The final decision may be recorded on or after **December 31, 2012.**
- A building or zoning permit will be issued only after the final decision is recorded.

The applicant, builder, or a representative may record the final decision as follows:

- By Mail: Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- In Person: Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034  
For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

**Expiration of this approval.** An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

**Applying for your permits.** A building permit, occupancy permit, or development permit may be required before carrying out an approved project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed herein;
- All applicable development standards, unless specifically exempted as part of this land use review;
- All requirements of the building code; and
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

### EXHIBITS

NOT ATTACHED UNLESS INDICATED

A. Applicant's Statements

1. Original application narrative
2. Original drawing set – reference only – not approved
3. Request from applicant to change architect contact to Gary Blackwell, received October 11, 2012
4. Cover memo and written responses to incomplete letter, received with new drawings November 8, 2012
5. Forwarded e-mail dialogue between applicant and BES staff regarding deck location and manhole in SW Grover

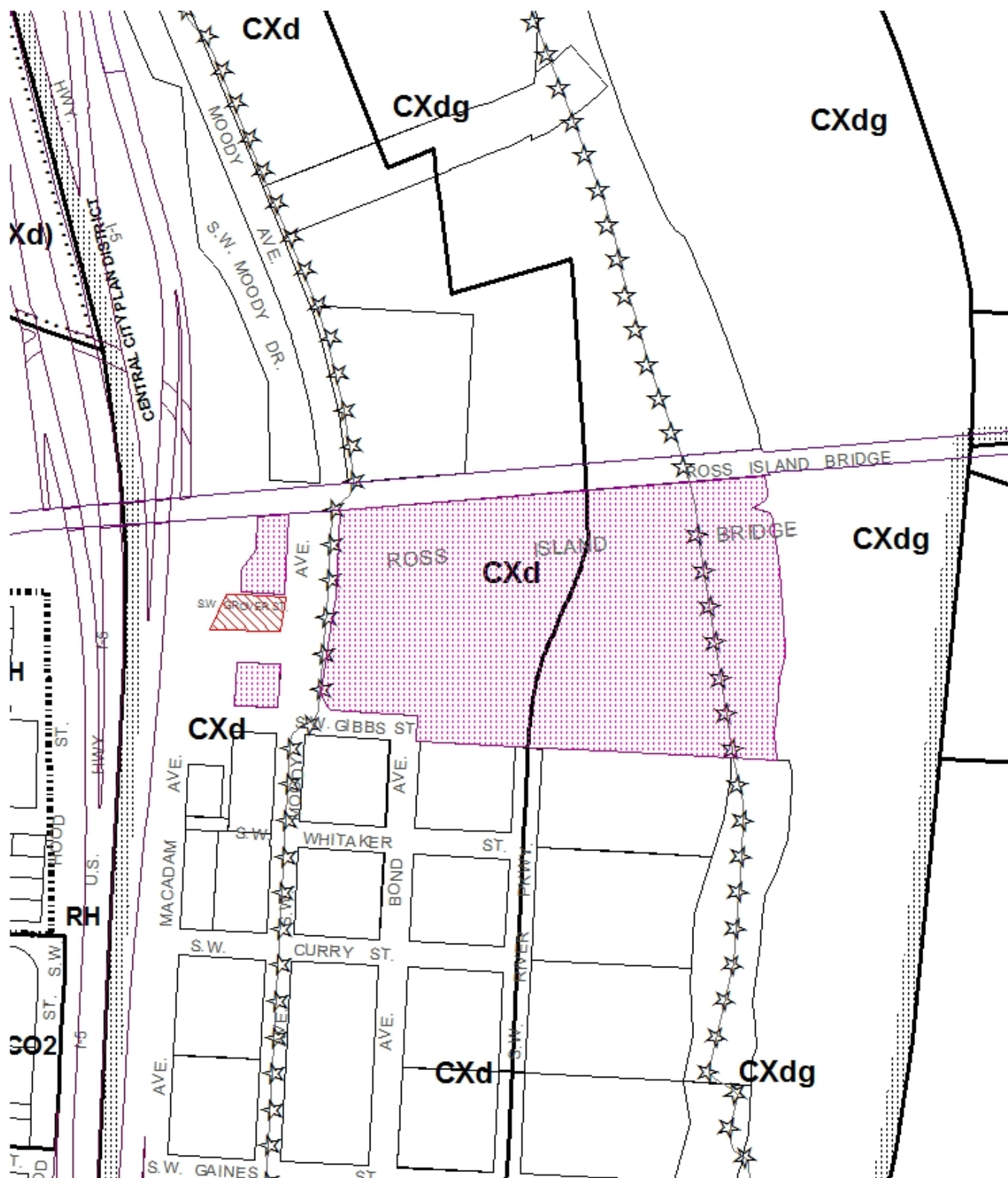
B. Zoning Map (attached)

C. Plans/Drawings:

1. Site and Layout Plan – Grover Street (attached)
2. Grover Street Site Details
3. Grover Street Site Details
4. Building Level 1 Plan
5. South and North Building Elevations (attached)
6. Enlarged Elevations and Wall Sections
7. Plan Set Cover Sheet
8. Existing Conditions/Demolition Plan
9. Demolition Plan – South Lot
10. Plan/Profile Sheet – SW Grover

11. SW Grover Utility Plan
  12. PP&L Line Plan/Profile Sheet
  13. SW Grover Detail Sheet
  14. SW Grover Detail Sheet (attached)
  15. SW Grover Detail Sheet
  16. SW Grover Planting Plan (attached)
  17. 11" x 17" Color Site Plan
  18. Large/Scalable Site and Layout Plan – Grover Street
  19. Large/Scalable Grover Street Site Details
  20. Large/Scalable Grover Street Site Details
  21. Large/Scalable Building Level 1 Plan
  22. Large/Scalable South and North Building Elevations
  23. Large/Scalable Enlarged Elevations and Wall Sections
  24. Large/Scalable Plan Set Cover Sheet
  25. Large/Scalable Existing Conditions/Demolition Plan
  26. Large/Scalable Demolition Plan – South Lot
  27. Large/Scalable Plan/Profile Sheet – SW Grover
  28. Large/Scalable SW Grover Utility Plan
  29. Large/Scalable PP&L Line Plan/Profile Sheet
  30. Large/Scalable SW Grover Detail Sheet
  31. Large/Scalable SW Grover Detail Sheet
  32. Large/Scalable SW Grover Detail Sheet
  33. Large/Scalable SW Grover Planting Plan
  34. SW Grover Street Light Specification Sheet
- D. Notification information:
1. Mailing list
  2. Mailed notice
- E. Agency Responses:
1. Water Bureau
  2. Life Safety Section of the Bureau of Development Services
  3. Urban Forestry Division of Portland Parks and Recreation
  4. Bureau of Environmental Services
  5. Development Review Section of Portland Transportation
- F. Correspondence:
1. *(no neighbor comment letters received during comment period)*
- G. Other:
1. Original LU Application Form and Receipt
  2. Incomplete letter from staff to applicant, sent July 27, 2012

**The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).**



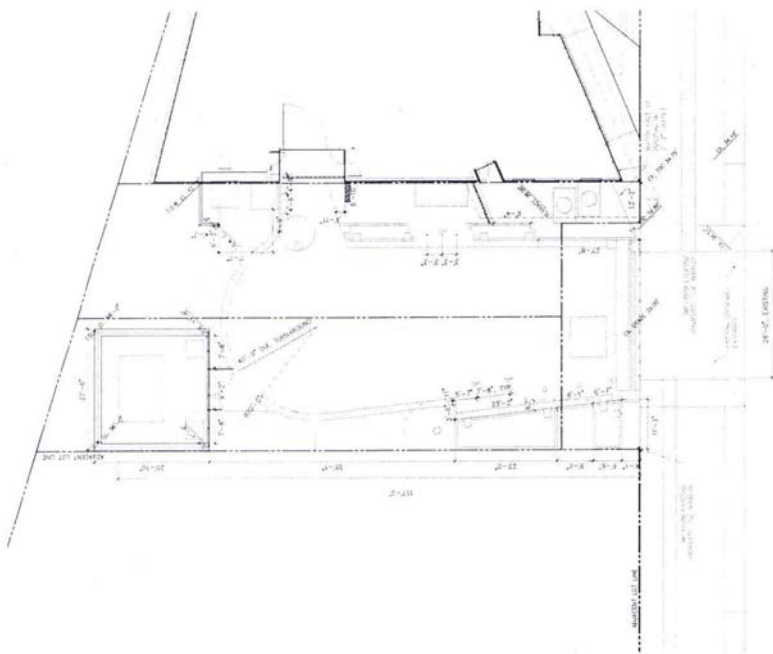
# ZONING

- Site
- Also Owned



This site lies within the:  
CENTRAL CITY PLAN DISTRICT

File No.	LU 12-160459 DZ
1/4 Section	3329,3330
Scale	1 inch = 300 feet
State_Id	1S1E10AC 200
Exhibit	B (Jul 20,2012)



SW MOODY AVENUE

1 SITE LAYOUT PLAN - GROVER ST.  
SCALE: 1" = 10'-0"



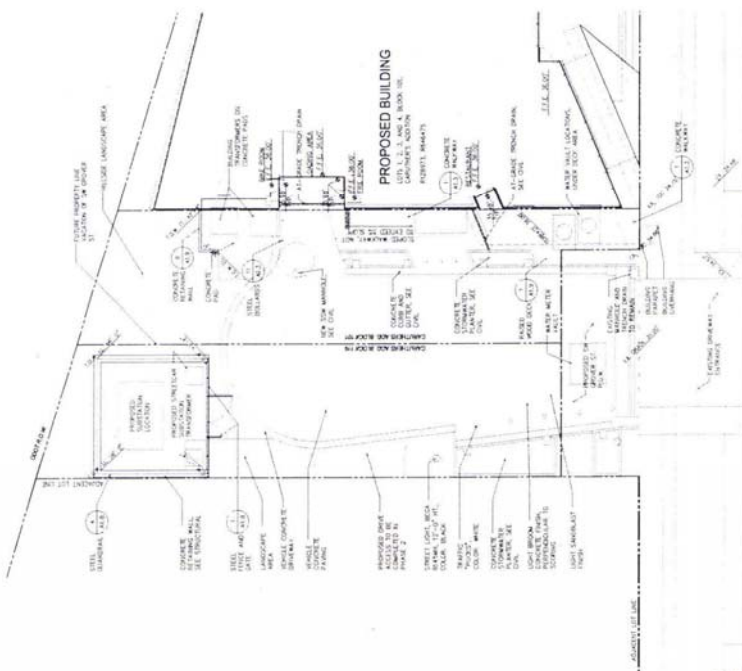
1 SITE PLAN - GROVER ST.  
SCALE: 1" = 10'-0"

\*Approved\*

City of Portland - Bureau of Development Services

Planner MARK VALLHOD Date DEC. 11, 2012

This approval applies only to the reviews requested and is subject to conditions of approval. Additional zoning requirements may apply.



SW MOODY AVENUE



1 SITE PLAN - GROVER ST.  
SCALE: 1" = 10'-0"

REMARKS:

1. FOR ADDITIONAL INFORMATION, SEE THE PLAT OF THE PROJECT AND THE PLAT OF THE PROJECT.
2. THE PROJECT IS LOCATED IN THE CITY OF PORTLAND, OREGON.
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ITEM	DESCRIPTION	DATE
1	REVISION	11/11/2012
2	REVISION	11/11/2012
3	REVISION	11/11/2012
4	REVISION	11/11/2012
5	REVISION	11/11/2012
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20	REVISION	11/11/2012

CASE NO. W 12-160459 02  
EXHIBIT C.1



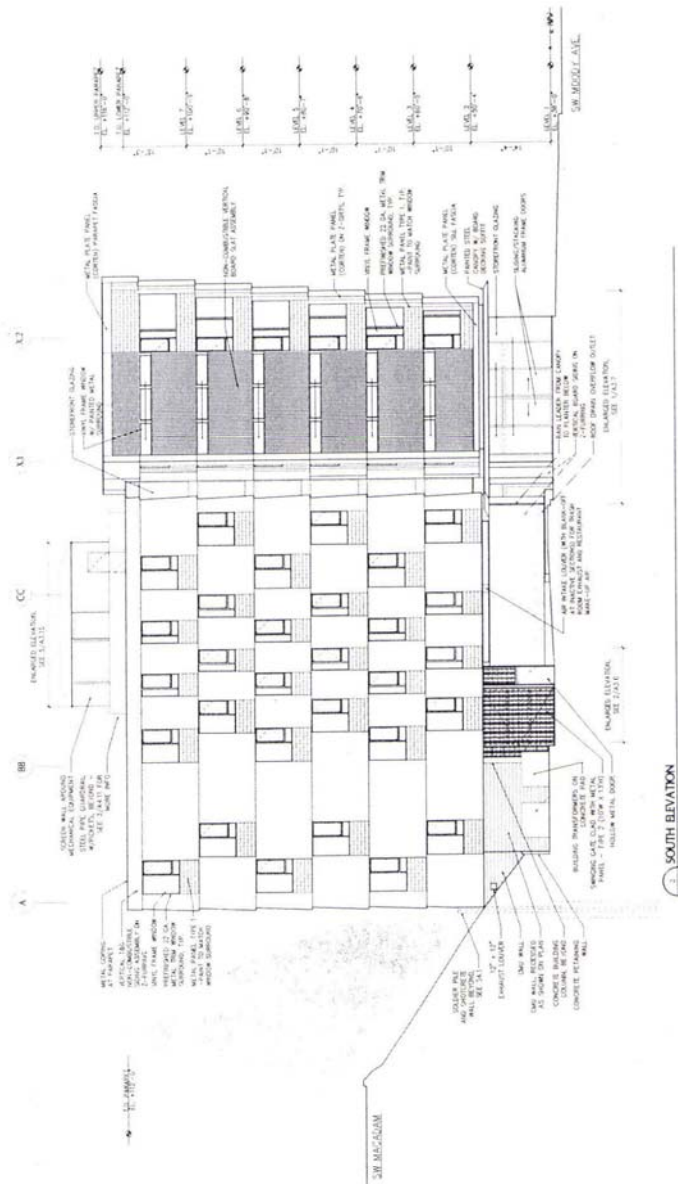


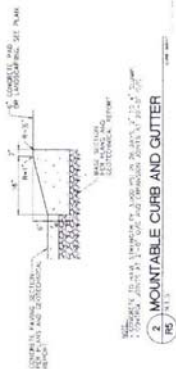
\*Approved\*

City of Portland - Bureau of Development Services

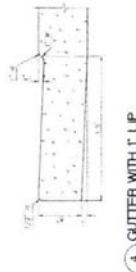
Planner MARK WALHOD Date DEC. 11, 2012  
This approval applies only to the reviews requested and is subject to conditions of approval. Additional zoning requirements may apply.

- GENERAL NOTES**
1. ALL GLAZING TYPES: SEE GLAZING AND INSULATION SECTION B
  2. ALL GLAZING TO BE TYPED: SEE B
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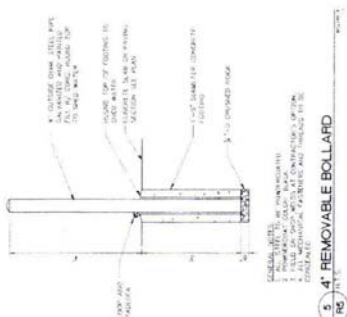
2 MOUNTABLE CURB AND GUTTER



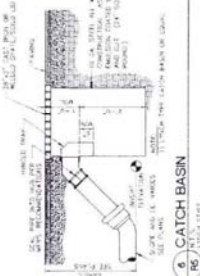
4 GUTTER WITH 1" LIP

3 DRAINAGE OPENING

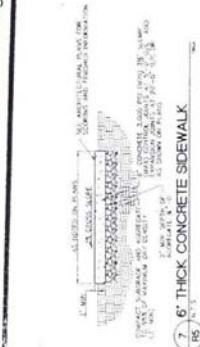
3 DRAINAGE OPENING



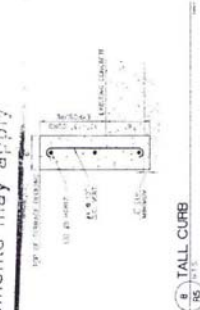
5 4" REMOVABLE BOLLARD



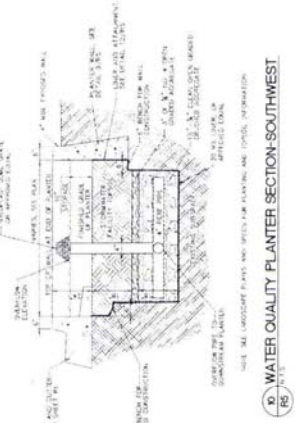
6 CATCH BASIN  
R6



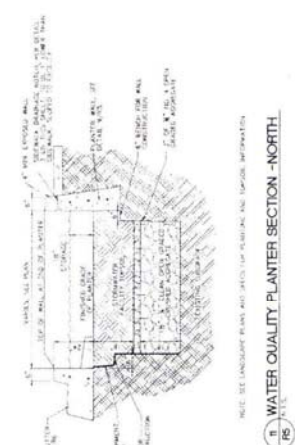
7 6" THICK CONCRETE SIDEWALK



⑧ TALL CURB



10 WATER QUALITY PLANTER SECTION-SOUTHWEST  
R5



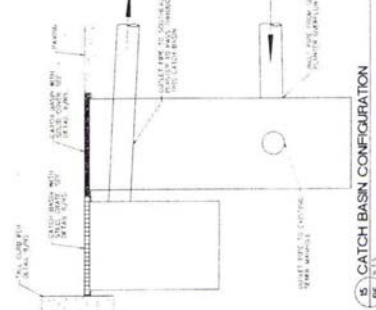
WATER QUALITY PLANTER SECTION - NORTH



12 LINER AND ATTACHMENT



14 WATER QUALITY PLANTER SECTION-SOUTHEAST



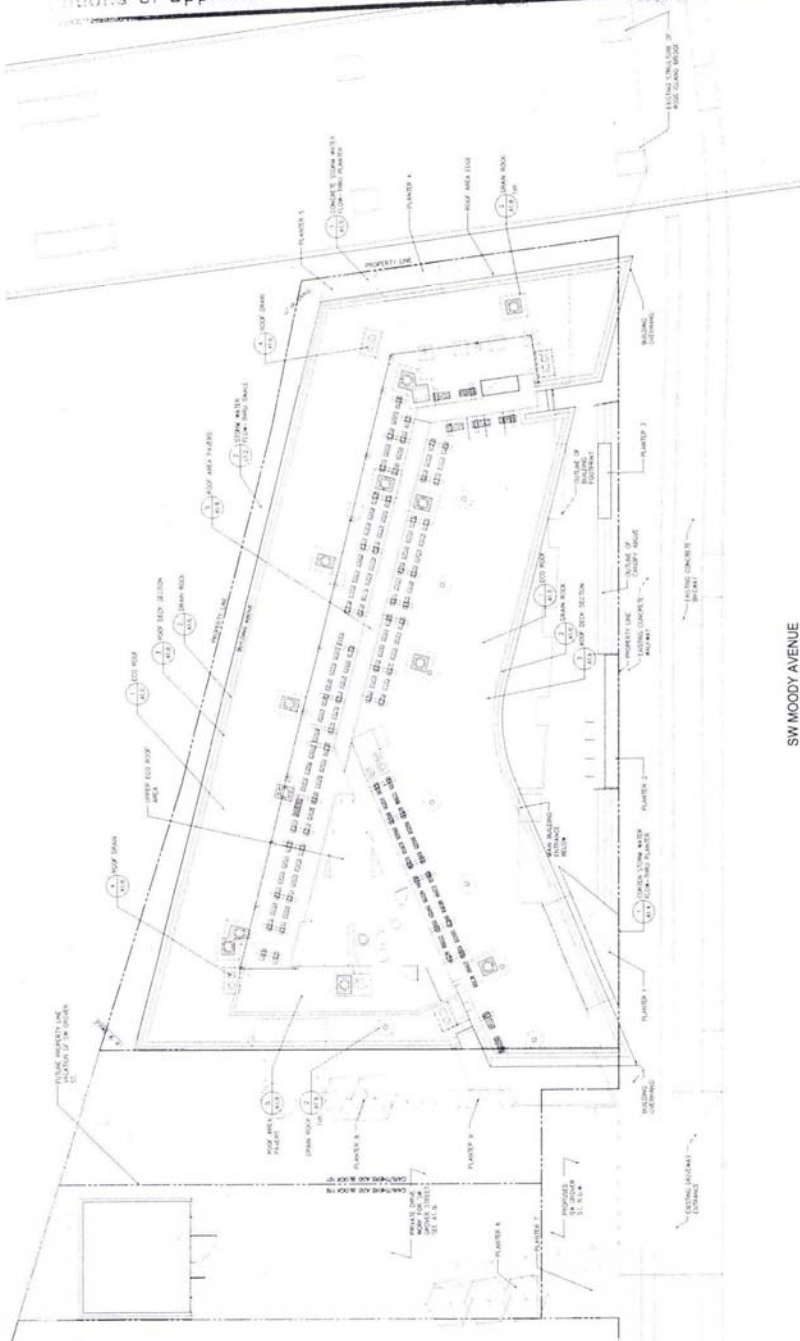
15 CATCH BASIN CONFIGURATION

\*Approved\*

City of Portland - Bureau of Development Services

Prepared by MARK WALHOOD Date DEC. 11, 2012

This approval applies only to the reviews requested and is subject to the conditions of approval. Additional zoning requirements may apply.



PLANTING PLAN  
SCALE: 1" = 10'-0"

[illegible]

<p>           1990            1991            1992            1993            1994            1995            1996            1997            1998            1999            2000            2001            2002            2003            2004            2005            2006            2007            2008            2009            2010            2011            2012            2013            2014            2015            2016            2017            2018            2019            2020            2021            2022            2023            2024            2025            2026            2027            2028            2029            2030            2031            2032            2033            2034            2035            2036            2037            2038            2039            2040            2041            2042            2043            2044            2045            2046            2047            2048            2049            2050            2051       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CASE NO. W 12-160459 D2  
EXHIBIT C16