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Bureau of Development Services
Land Use Services

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**FINAL FINDINGS AND DECISION BY THE DESIGN
COMMISSION RENDERED ON April 21, 2011**

CASE FILE NUMBER: LU 11-108266 DZM
PC # 09-100900

NE 6th & Couch Apartments

BUREAU OF DEVELOPMENT SERVICES STAFF: Chris Caruso 503-823-5747 /
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GENERAL INFORMATION

Applicant: Ryan Austin
Vallaster Corl Architects
711 SW Alder St., Penthouse Suite
Portland, OR 97205

Representative: Brad Fowler
Central Eastside Partners, LLC
5845 Jean Rd
Lake Oswego, OR 97035

Site Address: 521 & 532 NE DAVIS ST

Legal Description: BLOCK 115 N 35' OF LOT 6 LOT 7&8, EAST PORTLAND
Tax Account No.: R226507740, R226507730, R226507740
State ID No.: 1N1E35CB 04300, 1N1E35CB 04200, 1N1E35CB 04300
Quarter Section: 3031
Neighborhood: Kerns, contact Mike Whitmore at 503-233-0305.
Business District: Central Eastside Industrial Council, contact Juliana Lukasik at 503-287-5886.
District Coalition: Southeast Uplift, contact Leah Hyman at 503-232-0010.
Plan District: Central City - Central Eastside
Zoning: EXd, Central Employment with design overlay
Case Type: DZM AD, Design Review with Modifications and Adjustments
Procedure: Type III, with a public hearing before the Design Commission. The decision of the Design Commission can be appealed to City Council.

Proposal:

The applicant seeks design review approval for a four and five-story mixed-use apartment building in the Central Eastside subdistrict of the Central City Plan District. The proposed 61,327 SF building contains 11 ground floor retail spaces, 70 apartments, 23 on-site parking spaces, roof deck, and large bike room. Exterior materials are a mix of architectural concrete, vertical metal panels, fiber cement panels, wood, aluminum storefront and windows, and metal canopies, gates and balcony railings. Two existing apartment buildings on the site will be demolished by right. A Type III design review is required due to the project's total valuation and its location in the design zone of the Central Eastside of the Central City Plan District.

Modifications requested include:

1. 33.266.130.G Parking Area Setbacks and Landscaping - Reducing the width of perimeter parking lot landscaping from 5 feet to 3'-8".
2. 33.266.130.G Parking Area Setbacks and Landscaping - Reducing the amount of interior parking lot landscaping from 585 SF to 134 SF.
3. 33.140.230 Ground Floor Windows in the EX Zone - Reducing the amount of ground floor windows along the NE Davis façade.

Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of Title 33, Portland Zoning Code. The applicable approval criteria are:

- 33.805 Adjustments
- 33.825 Design Review
- 33.825.040 Modifications
- Central City Fundamental Design Guidelines
- Central Eastside Special Design Guidelines

ANALYSIS

Site and Vicinity: The 20,000 SF site is two lots comprising a half block of land bounded by NE Couch Street, NE 6th Avenue, and NE Davis Street. The area at the south end of the site was recently redeveloped into the Burnside-Couch Couplet so that NE Couch Street is now one-way going west. The southern lot contains two older apartment buildings that will be demolished as part of this project. The area around the site is developed with a combination of low-scale commercial and industrial buildings as well as single and multi-family housing. A few new mid-rise mixed-use buildings have been constructed farther to the south on East Burnside Street. The area is becoming popular with local retailers and has an active nightlife scene with multiple restaurants and bars.

The City of Portland's Transportation System Plan designates NE Couch Street a Major City Traffic Street, a Major Transit Priority Street, a City Bikeway, a City Walkway, and a Regional Main Street. Both NE Davis Street and NE 6th Avenue are designated Local Service Bikeways, Local Service Walkways. The site is within a designated freight district and is in Central Eastside Parking Sector 2.

Zoning: The Central Employment (EX) zone allows mixed uses and is intended for areas in the center of the City that have predominantly industrial-type development. The intent of the zone is to allow industrial and commercial uses which need a central location. Residential uses are allowed, but are not intended to predominate or set development standards for other uses in the area.

The Design Overlay Zone [d] promotes the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. This is achieved through the creation of design districts and applying the Design Overlay Zone as part of community planning projects, development of design guidelines for each district, and by requiring design review. In addition, design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area.

Land Use History: City records indicate that no prior land use reviews.

Agency Review: A "Notice of proposal in Your Neighborhood" was mailed **March 29, 2011**. The following Bureaus have responded with no issue or concerns:

- Site Development Section of BDS
- Life Safety Review Section of BDS
- Fire Bureau
- Water Bureau (Exhibit E-1)

- Bureau of Environmental Services (Exhibit E-2)
- Bureau of Transportation Engineering (Exhibit E-3)

The Bureau of Parks-Forestry Division responded with the following comment: *Street trees will be required on all street frontages.*

Neighborhood Review: A Notice of Proposal in Your Neighborhood was mailed on **March 29, 2011**. No written responses have been received from either the Neighborhood Association or notified property owners in response to the proposal.

EX Base Zone Development Standards (33.140)

The employment and industrial zones are for areas of the City that are reserved for industrial uses and for areas that have a mix of uses with a strong industrial orientation. The zones reflect the diversity of industrial and business areas in the City. The zones differ in the mix of allowed uses, the allowed intensity of development, and the development standards. The regulations promote areas which consist of uses and developments which will support the economic viability of the specific zoning district and of the City. The regulations protect the health, safety and welfare of the public, address area character, and address environmental concerns. In addition, the regulations provide certainty to property owners, developers, and neighbors about the limits of what is allowed.

Central City Plan District Development Standards (33.510)

The Central City plan district implements the Central City Plan and other plans applicable to the Central City area. These other plans include the Downtown Plan, the River District Plan, the University District Plan, and the Downtown Parking and Circulation Policy. The Central City plan district implements portions of these plans by adding code provisions which address special circumstances existing in the Central City area.

[Note: The proposal does not have to meet all development standards in order to be approved during this process, but will have to meet those standards (or appropriate adjustments/modifications be approved) before a building permit can be issued.]

Central Employment Zone Primary Uses (Table 140-1)

Development Standard

Household Living and Retail Sales and Service uses as proposed are allowed by right.

Floor Area Ratios (33.510.200 & Map 510-2)

Development Standard

Maximum FAR 9:1 allowed, Bonus FAR 3:1

Proposal

- The proposal has a floor area ratio of 3.07:1 based on a net site area of 20,000 SF and a building size of 61,327 SF, of which 6,995 SF are in non-residential uses. This standard is met.

Maximum Height (Table 140-3, 33.510.205 & Map 510-3)

Development Standard

Maximum height 100 feet. No bonus height available.

Proposal

- Height has been measured per Height Measurement of Base Point 1 in the PZC 33.930, Figure 930-7, at the highest abutting sidewalk elevation within a 5 foot horizontal difference to the top of the parapet. The building is 65'-0" tall. This standard is met.

Minimum and Maximum Setbacks (Table 140-3 & 33.140.215)

Development Standard

The maximum building setback is 10 feet for 100% of the ground-level street-facing facades along a Transit Street (NE Couch Street).

Proposal

- The maximum building setback is 4'-0" to the exit stair doorway. The standard is met.

Building Coverage (Table 140-3)

Development Standard

No limit.

Proposal

- Building will cover 100% of the site. This standard is met.

Minimum Landscaped Area (Table 140-3)

Development Standard

None.

Proposal

- No non-parking lot site landscaping is proposed. This standard is met.

Ground Floor Windows (33.140.230.B)

Development Standard

In the EX zone, all exterior walls on the ground level which are 20 feet or closer to a street lot line, sidewalk, plaza, or other public open space or right-of-way must have windows. The windows must be at least 50 percent of the length and 25 percent of the ground level wall area. Ground level wall areas include all exterior wall areas up to 9 feet above the finished grade. The requirement does not apply to the walls of residential units, and does not apply to the wall of parking structures when set back at least 5 feet and landscaped to at least the L2 standard.

Proposal

- Proposed ground floor windows on NW Couch Street meet the standard.
- A Modification is requested to reduce the amount of ground floor window area and length along NE Davis Street.

Pedestrian Standards (33.140.240)

Development Standard

These pedestrian standards implement the State Transportation Planning Rule. Under the pedestrian standards, the site must contain a pedestrian circulation system connecting all adjacent streets to the main entrance and provide connections to other areas of the site.

Proposal

- The building contains direct access to all entrances via the public sidewalk and the areas between the building and the street lot lines are hardscaped for use by pedestrians. This standard is met.

Transit Street Main Entrance (33.140.242)

Development Standard

All sites with at least one frontage on a transit street, and where any of the floor area on the site is in nonresidential uses, must have at least one main entrance for each tenant space facing the transit street.

Proposal

- NE Couch is the designated transit street. Each of the retail spaces fronting NE Couch Street have at least one main entrance facing the street. This standard is met.

Fences (33.140.275)

Development Standard

In EX zones, within 10 feet of a street lot line, fences 3 ½ feet tall are allowed if they are greater than 50% sight-obscuring and fences up to 8 feet are allowed if they are less than 50% sight-obscuring.

Proposal

- The 8 foot tall metal fence will be less than 50% sight-obscuring. This standard is met.

Demolitions (33.510.242 & Map 510-8)

Development Standard

In R, C, and E zones, if a building is demolished after September 1, 1994, the square footage of the ground floor of the demolished building must be replaced. Adjustments are prohibited. In the Core Area, the square footage must be replaced on the same block as the demolished building. This replacement must occur before or at the same time as any other development or redevelopment on the block, other than landscaping.

Proposal

- The new building will completely replace the ground floor square footage of the demolished building. This standard is met.

Parking (33.266 & 33.510.265)

Development Standard

There is no minimum required parking for Residential uses in the EX zone. The maximum allowed number of spaces is one per unit which would be 70 for this project.

Proposal

- 23 parking spaces are provided. This standard is met.

Parking Area Landscaping (33.266.130)

Development Standard

45 SF of interior parking lot landscaping is required for each parking space when there are more than 10 total parking spaces. 5 feet of L2 landscaping is required around the perimeter of parking lots.

Proposal

- Modifications are being requested to both of these standards.

Bicycle Parking (33.266.210 & Table 266-6)

Development Standard

The project consists of 42 apartments and 10,000 SF of Retail Sales and Service space. Bicycle Parking requirements are:

Proposal

- | | | |
|--|-----------|--|
| ▪ Residential Long-Term Spaces | 105 req'd | 140 provided (70 in bike room, 70 in units) |
| ▪ Residential Short-Term Spaces | 4 req'd | 4 provided |
| ▪ Retail Sales and Service Long-Term Spaces | 2 req'd | 2 provided |
| ▪ Retail Sales and Service Short-Term Spaces | 2 req'd | 2 provided |

This standard is met.

Loading Spaces (33.266.310 & Map 510-9)

Development Standard

One loading space at least 18 feet long, 9 feet wide and having a clearance of 10 feet is required for buildings with less than 20,000 SF of floor area in uses other than Household Living.

Proposal

- One loading space meeting the dimensional requirements is provided on site. This standard is met.

Signs (33.140.300 & Title 32) No signs are proposed for approval for this review.

Street Trees (33.140.305) Street Trees will be provided as required by the City Forester.

ZONING CODE APPROVAL CRITERIA

(1) Chapter 33.825 Design Review

Section 33.825.010 Purpose of Design Review

Design review ensures that development conserves and enhances the recognized special design values of a site or area. Design review is used to ensure the conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district or area. Design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area. Design review is also used in certain cases to review public and private projects to ensure that they are of a high design quality.

Section 33.825.055, Design Review Approval Criteria

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design guidelines for the area.

Findings: The site is designated with design overlay zoning (d), therefore the proposal requires Design Review approval. Because the site is located generally within the Central City Plan District, the applicable design guidelines are the Central City Plan Fundamental Design Guidelines. As the site is also specifically located within the Design Zone of the Central Eastside District, the Special Design Guidelines for the Design Zone of the Central Eastside District of the Central City Plan also apply.

Special Design Guidelines for the Design Zone of the Central Eastside District of the Central City Plan and Central City Fundamental Design Guidelines

The Central Eastside is a unique neighborhood. The property and business owners are proud of the district's heritage and service to the community and region. Light industry, distribution/warehousing, and transportation are important components of the district's personality. To the general public, retail stores and commercial businesses provide the central focus within the district.

The underlying urban design objective for the Central Eastside is to capitalize on and emphasize its unique assets in a manner that is respectful, supportive, creative and compatible with each area as a whole. Part of the charm and character of the Central Eastside District, which should be celebrated, is its eclectic mixture of building types and uses. An additional strength, which should be built on, is the pattern of pedestrian friendly retail uses on Grand Avenue, East Burnside and Morrison Streets, as well as portions of NE 11th and NE 12th Avenues.

The Central City Fundamental Design Guidelines and the River District Design Guidelines focus on four general categories. **(A) Portland Personality**, addresses design issues and elements that reinforce and enhance Portland's character. **(B) Pedestrian Emphasis**, addresses design issues and elements that contribute to a successful pedestrian environment. **(C) Project Design**, addresses specific building characteristics and their relationships to the public environment. **(D) Special Areas**, provides design guidelines for the four special areas of the Central City.

Central Eastside Design Goals

The following goals and objectives define the urban design vision for new development and other improvements in the Central Eastside

- Encourage the special distinction and identity of the design review areas of the Central Eastside District.
- Provide continuity between the Central Eastside and the Lloyd District.

- Provide continuity between the Central Eastside and the river, downtown, and adjacent residential neighborhoods.
- Enhance the safety, convenience, pleasure, and comfort of pedestrians.

Central City Plan Design Goals

This set of goals are those developed to guide development throughout the Central City. They apply within the River District as well as to the other seven Central City policy areas. The nine goals for design review within the Central City are as follows:

1. Encourage urban design excellence in the Central City;
2. Integrate urban design and preservation of our heritage into the development process;
3. Enhance the character of the Central City's districts;
4. Promote the development of diversity and areas of special character within the Central City;
5. Establish an urban design relationship between the Central City's districts and the Central City as a whole;
6. Provide for a pleasant, rich and diverse pedestrian experience for pedestrians;
7. Provide for the humanization of the Central City through promotion of the arts;
8. Assist in creating a 24-hour Central City which is safe, humane and prosperous;
9. Ensure that new development is at a human scale and that it relates to the scale and desired character of its setting and the Central City as a whole.

Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project.

A2. Emphasize Portland Themes. When provided, integrate Portland-related themes with the development's overall design concept.

A2-1. Recognize Transportation Modes, Produce, and Commerce as Primary Themes of East Portland. Recognize and incorporate East Portland themes into a project design, when appropriate.

A4. Use Unifying Elements. Integrate unifying elements and/or develop new features that help unify and connect individual buildings and different areas.

A5. Enhance, Embellish, and Identify Areas. Enhance an area by reflecting the local character within the right-of-way. Embellish an area by integrating elements in new development that build on the area's character. Identify an area's special features or qualities by integrating them into new development.

C3-1. Design to Enhance Existing Themes in the District. Look to buildings from throughout the district for contextual precedent. Innovation and creativity are encouraged in design proposals, which enhance overall district character.

C4. Complement the Context of Existing Buildings. Complement the context of existing buildings by using and adding to the local design vocabulary.

Findings for A2, A2-1, A4, A5, C3-1 & C4: The area historically operated as a working district with a mix of uses in buildings of varying styles and materials. The immediate area around the site also contains low-rise apartment buildings and streetcar-era commercial structures. The proposed building's diverse material palette, surface patterning, and stylized fence contribute to the eclectic mix of buildings, materials and styles seen throughout the Central Eastside, particularly in newly constructed and renovated properties. Though the adjacent buildings are currently lower structures, the proposed building massing and its location tight to the lot lines anticipates the desired height and prominence of future neighboring structures as outlined in the current zoning regulations.

The creation of the Burnside-Couch couplet which features curb extensions, stormwater facilities, and frequent transit service along NE Couch Street supports higher density development of this type that will support commerce in the area by bringing in new retail

and residential opportunities. The varied heights of the building and its overall massing do not overwhelm the site or the adjacent properties, allowing the project to complement the existing context. *These guidelines are therefore met.*

A3. Respect the Portland Block Structures. Maintain and extend the traditional 200-foot block pattern to preserve the Central City's ratio of open space to built space. Where superblocks exist, locate public and/or private rights-of-way in a manner that reflects the 200-foot block pattern, and include landscaping and seating to enhance the pedestrian environment.

A7. Establish and Maintain a Sense of Urban Enclosure. Define public rights-of-way by creating and maintaining a sense of urban enclosure.

A8. Contribute to a Vibrant Streetscape. Integrate building setbacks with adjacent sidewalks to increase the space for potential public use. Develop visual and physical connections into buildings' active interior spaces from adjacent sidewalks. Use architectural elements such as atriums, grand entries and large ground-level windows to reveal important interior spaces and activities.

Findings for A3, A7 & A8: This project occupies one half of a standard 200' x 200' Portland city block and maintains the traditional block pattern. Adjacent public rights-of-way are defined by the building walls which are placed at the lot line and maintain a sense of urban enclosure. This project responds to the surrounding scale and context by creating a dynamic pedestrian arcade that will help maintain the sense of enclosure, scale, and activity at the street level. The street level of the building along NE 6th and NE Couch and a portion of NE Davis is designed with active storefronts that have direct visual and physical connections to the adjacent sidewalk. Adjacent sidewalks are integrated into the setbacks though continuous paving for use by pedestrians. *These guidelines are therefore met.*

A5-3. Plan for or Incorporate Underground Utility Service. Plan for or Incorporate Underground Utility Service to development projects.

Findings: Where possible, the applicant plans to provide underground utility services to the building. The above ground service transformer is screened from the sidewalk by the decorative metal fence and gates. *This guideline is therefore met.*

B1. Reinforce and Enhance the Pedestrian System. Maintain a convenient access route for pedestrian travel where a public right-of-way exists or has existed. Develop and define the different zones of a sidewalk: building frontage zone, street furniture zone, movement zone, and the curb. Develop pedestrian access routes to supplement the public right-of-way system through superblocks or other large blocks.

B2. Protect the Pedestrian. Protect the pedestrian environment from vehicular movement. Develop integrated identification, sign, and sidewalk-oriented night-lighting systems that offer safety, interest, and diversity to the pedestrian. Incorporate building equipment, mechanical exhaust routing systems, and/or service areas in a manner that does not detract from the pedestrian environment.

B3. Bridge Pedestrian Obstacles. Bridge across barriers and obstacles to pedestrian movement by connecting the pedestrian system with innovative, well-marked crossings and consistent sidewalk designs.

B3-1. Reduce width of Pedestrian Crossings.

- a. Where possible, extend sidewalk curbs at street intersections to narrow pedestrian crossings for a safer pedestrian environment.
- b. Maintain large service vehicle turning radii where necessary.

C8-1. Allow for Loading and Staging Areas on Sidewalks. On local service streets, adjacent businesses may use the sidewalk area for temporary loading and staging as long as pedestrian access through it is maintained.

Findings for B1, B2, B3, B3-1 & C8-1: The existing pedestrian zones on all three street frontages will be maintained. New sidewalks as required by PBOT will be designed to provide the various activity and furnishings zones desired in this area and will match nearby sidewalk patterns. Pedestrians will be protected from vehicular movement by the complete separation of the parking area from the sidewalks by a gate that will alert pedestrians to cars when it opens and closes. Night-lighting is provided by fixtures mounted to the building that will illuminate the sidewalks, creating a safer environment. The large ground-level windows also promote “eyes on the street”. All building mechanical equipment will be on the roof and all vents will be located above the pedestrian zone. There will be no adverse impacts on pedestrians from exhaust vents or other equipment.

The newly constructed Couch Street right-of-way affords curb extensions, and accommodations for a planting strip with street trees – all of which improve the pedestrian environment and reduce the width of pedestrian crossings. New street trees along NE 6th and Davis will continue these visual and environmental improvements. The building entries are recessed along the sidewalks, which creates a successful transition from the building to the public way. Canopies and balcony projections are provided around the building and offer protection from the elements, as well as provide ambient lighting at the pedestrian level. Loading and staging space will be available along NE 6th Avenue which includes a sidewalk that is wide enough to accommodate pedestrian access together with loading. *These guidelines are therefore met.*

B6. Develop Weather Protection. Develop integrated weather protection systems at the sidewalk-level of buildings to mitigate the effects of rain, wind, glare, shadow, reflection, and sunlight on the pedestrian environment.

B6-1. Provide Pedestrian Rain Protection. Rain protection is encouraged at the ground level of all new and rehabilitated commercial buildings located adjacent to primary pedestrian routes. In required retail opportunity areas, rain protection is strongly recommended.

Findings for B6 & B6-1: Canopies and balcony projections are provided around the building. These encroachments provide weather protection for pedestrians over the public sidewalk, including above building entrances. The use of painted metal panels and wood siding minimizes any glare that would come off the building. *These guidelines are therefore met.*

B7. Integrate Barrier-Free Design. Integrate access systems for all people with the building’s overall design concept.

Findings: The proposed design is fully ADA accessible. *This guideline is therefore met.*

C1. Enhance View Opportunities. Orient windows, entrances, balconies and other building elements to surrounding points of interest and activity. Size and place new buildings to protect existing views and view corridors. Develop building façades that create visual connections to adjacent public spaces.

Findings: This project provides strong connections to views of downtown, the river, and the mountains by providing exterior balconies, windows on all sides of the upper floors, and a roof deck for residential use. *This guideline is therefore met.*

C2. Promote Quality and Permanence in Development. Use design principles and building materials that promote quality and permanence.

C5. Design for Coherency. Integrate the different building and design elements including, but not limited to, construction materials, roofs, entrances, as well as window, door, sign, and lighting systems, to achieve a coherent composition.

C10. Integrate Encroachments. Size and place encroachments in the public right-of-way to visually and physically enhance the pedestrian environment. Locate permitted skybridges toward the middle of the block, and where they will be physically unobtrusive. Design skybridges to be visually level and transparent.

C11. Integrate Roofs and Use Rooftops. Integrate roof function, shape, surface materials, and colors with the building's overall design concept. Size and place rooftop mechanical equipment, penthouses, other components, and related screening elements to enhance views of the Central City's skyline, as well as views from other buildings or vantage points. Develop rooftop terraces, gardens, and associated landscaped areas to be effective storm water management tools.

C12. Integrate Exterior Lighting. Integrate exterior lighting and its staging or structural components with the building's overall design concept. Use exterior lighting to highlight the building's architecture, being sensitive to its impacts on the skyline at night.

C13. Integrate Signs. Integrate signs and their associated structural components with the building's overall design concept. Size, place, design, and light signs to not dominate the skyline. Signs should have only a minimal presence in the Portland skyline.

C1-2. Integrate Signs.

a. Retain and restore existing signage which reinforces the history and themes of the district, and permit new signage which reinforces the history and themes of the East Portland Grand Avenue historic district.

b. Carefully place signs, sign supports, and sign structures to integrate with the scale, color and articulation of the building design, while honoring the dimensional provisions of the sign chapter of the zoning code.

c. Demonstrate how signage is one of the design elements of a new or rehabilitation project and has been coordinated by the project designer/ architect. Submit a Master Signage Program as a part of the project's application for a design review.

Findings for C2, C5, C10, C11, C12, C13 & C1-2: This project incorporates strong design principles and quality building materials that provide permanence and integrity, such as architectural concrete, glass, steel, minimum 22 gauge thick metal panels to prevent oil-canning, and wood siding. The materials are used to delineate the two distinct building masses while providing a cohesive composition through consistent window elements and panel widths. Vertical metal panels with hidden fasteners are used to accentuate the height of the five story section while horizontal wood siding runs the length of the four story portion of the building. The wood siding becomes an accent material on the five story portion of the building when used as the underside of the canopies and on the mechanical screens. The metal panels become an accent on the four story portion of the building as intermediate panels on the first two floors. A third type of panel is woven into the design as a highlight at each floor plate as well as at the top of the building. This theme is continued around the base of the building with the articulated metal fence along NE Davis Street. The entire composition is a successful one of shifting panels and window openings woven from three primary materials.

The architectural encroachments are integrated into the building in a very direct way. The design of the building's metal skin highlights these architectural projections. They reinforce the arcade effect and help provide pedestrian protection. They add visual texture and provide dynamic views of downtown and the river.

The building has integrated the necessary functional elements such as rooftop equipment, transformers, and vents into the overall design. All rooftop units are inside one of two wooden screens that are finished in the same materials as the roof deck. The equipment is located so it is not visible from the street. Smaller dwelling unit vents are incorporated into the wall panel system in a consistent pattern and will be painted to match the adjacent wall surfaces. The transformer is screened from the street by the decorative fence and gate along NE Davis Street. The project also provides a large roof deck for use by the residents. Set in the center of the building's roof, the deck is fabricated from wood slat elements that draw upon the wood siding present on the building and the mechanical screens.

The building lighting strategy defers to the architecture by highlighting the canopies and balcony projections. It also provides ample storefront lighting at the pedestrian level. Parking area lighting is accomplished with two simple pole lights that do not detract from the design. The signage is integrated into the canopy and balcony overhangs and is designed in a rectilinear manner with modern materials that complement the building. The proportions and simplicity of the signs are appropriate for this building. The signs do not disrupt or clutter the building's form. *These guidelines are therefore met.*

C7. Design Corners that Build Active Intersections. Use design elements including, but not limited to, varying building heights, changes in façade plane, large windows, awnings, canopies, marquees, signs and pedestrian entrances to highlight building corners. Locate flexible sidewalk-level retail opportunities at building corners. Locate stairs, elevators, and other upper floor building access points toward the middle of the block.

C8. Differentiate the Sidewalk-Level of Buildings. Differentiate the sidewalk-level of the building from the middle and top by using elements including, but not limited to, different exterior materials, awnings, signs, and large windows.

C9. Develop Flexible Sidewalk-Level Spaces. Develop flexible spaces at the sidewalk-level of buildings to accommodate a variety of active uses.

Findings for C7, C8 & C9: The building design provides a clear material variation between the base and the upper levels by providing large areas of glazing and solid metal panels at the ground floor and a variety of ribbed metal panels and wood siding on the residential floors. This clear delineation of the storefront level and a glazing system with a larger module than the window system above, provides a solid base for this building. The building locates active indoor uses in areas with ground floor windows adjacent to the public realm. It also provides protection at building entrances and over sidewalks adjacent to storefront windows. The ground level spaces are flexible and could be utilized in a variety of ways over time. The fully glazed corners at both ends of NE 6th Avenue provide opportunities for interactions with pedestrians. *These guidelines are therefore met.*

(2) Modification Requests (33.825)

33.825.040 Modifications That Will Better Meet Design Review Requirements:

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the design review process. These modifications are done as part of design review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through design review may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

A. Better meets design guidelines. The resulting development will better meet the applicable design guidelines; and

B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.

Modification #1: Parking Area Setbacks and Landscaping, PZC 33.266.130.G - Reducing the width of perimeter parking lot landscaping from 5 feet to 3'-8".

Purpose Statement: The development standards promote vehicle areas which are safe and attractive for motorists and pedestrians. Vehicle area locations are restricted in some zones to promote the desired character of those zones. Together with the transit street building setback standards in the base zone chapters, the setback standards:

- Improve and soften the appearance of parking areas;
- Reduce the visual impact of parking areas from sidewalks, streets, and especially from adjacent residential zones;
- Provide flexibility to reduce the visual impacts of small residential parking lots;
- Direct traffic in parking areas;
- Shade and cool parking areas;
- Reduce the amount and rate of stormwater runoff from vehicle areas;
- Reduce pollution and temperature of stormwater runoff from vehicle areas; and
- Decrease airborne and waterborne pollution.

Standard: Table 266-5, Minimum Parking Area Setbacks and Landscaping. Lot line abutting a street requires 5 feet of L2 screening. Lot line abutting a C, E, or I zone lot line requires 5 feet of L2 screening.

A. Better meets design guidelines. *The resulting development will better meet the applicable design guidelines (B2 Protect the Pedestrian & C4 Complement the Context of Existing Buildings); and*

Findings: The parking lot landscaping runs along the interior lot line and will be visible from the new building as well as the adjacent lot to the west. The total width of the landscaping is 5 feet but a portion of it is stormwater plantings in place of the required L2 landscaping. The plant materials are varied in nature and the required number of trees are provided along the property line. This diverse planting creates a visually appealing space between the lots that enhances the area and protects pedestrians from vehicles.

B. Purpose of the standard. *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

Findings: The proposed 3'-8" foot strip of L2 landscaping, combined with the additional 2 feet of stormwater planting between the parking area and the adjacent lot, will create a green buffer that reduces the amount of paved area, pollution, and stormwater runoff from the project site. The plantings will also shield views of on-site vehicles, minimizing the visual impact of surface parking on the neighborhood.

The reduced perimeter landscaping strip will continue to function as a buffer between the parking area and the adjacent lot. Landscaping will be continuous along the western interior lot line, will provide the required number of trees and will shade and cool the parking area.

The overall solution is consistent with the purpose of the design guidelines and landscape setbacks. *This Modification therefore merits approval.*

Modification #2: Parking Area Setbacks and Landscaping, PZC 33.266.130.G - Reducing the amount of interior parking lot landscaping from 585 SF to 134 SF.

Purpose Statement: The development standards promote vehicle areas which are safe and attractive for motorists and pedestrians. Vehicle area locations are restricted in some zones to promote the desired character of those zones. Together with the transit street building setback standards in the base zone chapters, the setback standards:

- Improve and soften the appearance of parking areas;
- Reduce the visual impact of parking areas from sidewalks, streets, and especially from adjacent residential zones;
- Provide flexibility to reduce the visual impacts of small residential parking lots;
- Direct traffic in parking areas;
- Shade and cool parking areas;
- Reduce the amount and rate of stormwater runoff from vehicle areas;
- Reduce pollution and temperature of stormwater runoff from vehicle areas; and
- Decrease airborne and waterborne pollution.

Standard: 33.266.130.G.3a, Interior landscaping. In all zones, interior landscaping must be provided for sites where there are more than 10 parking spaces on the entire site. At least 45 square feet of interior landscaped area must be provided for each parking space.

A. Better meets design guidelines. *The resulting development will better meet the applicable design guidelines (A4 Use Unifying Elements, B2 Protect the Pedestrian & C3-1 Design to Enhance Existing Themes in the District); and*

Findings: The proposed parking lot is quite small and can only provide 13 surface spaces plus 10 garage spaces. The parking area includes perimeter landscaping along the street edge and interior lot line, two sections of interior landscaping, as well as a decorative metal fence along NE Davis Street. The fence incorporates design elements from the building façade which add to the varied and creative design elements desired in the Central Eastside. Additional densely planted perimeter landscaping is provided between the parking area and the transformer along NE Davis Street. This also contributes to creating a visually pleasing parking area.

B. Purpose of the standard. *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

Findings: The proposed 3'-8" foot strip of L2 landscaping, combined with the additional 2 feet of stormwater planting between the parking area and the adjacent lot, will create a green buffer that reduces the amount of paved area, pollution, and stormwater runoff from the project site. The plantings will also shield views of on-site vehicles, minimizing the visual impact of surface parking on the neighborhood.

The reduced perimeter landscaping strip will continue to function as a buffer between the parking area and the adjacent lot. Landscaping will be continuous along the western interior lot line, will provide the required number of trees and will shade and cool the parking area.

The purpose of the perimeter landscaping standard is met by the significant perimeter landscaped area, and its dense planting layout, surrounding the majority of the lot. The one edge that does exhibit a minimally landscaped border incorporates a tall, durable fence planted with a dense evergreen vine. The relatively narrow dimension of the lot, and the significant perimeter landscaping, suggest that interior landscaping is not as critical as it would be in another context. The proposed additional landscaping screens views, mitigates for stormwater run-off, shades the lot, reduces airborne pollution, and visually softens the edges of the lot. The proposed decorative metal fence, and the layered, dense, and recessed nature of the landscape border, visually connects the parking lot to the historic building and its district.

The overall solution is consistent with the purpose of the design guidelines and landscape setbacks. *This Modification therefore merits approval.*

Modification #3: Ground Floor Windows in the EX Zone, PZC 33.140.230 - Reducing the amount of ground floor windows along the NE Davis façade.

Purpose Statement: In the EX zone, blank walls on the ground level of buildings are limited in order to:

- Provide a pleasant, rich, and diverse pedestrian experience by connecting activities occurring within a structure to adjacent sidewalk areas;
- Encourage continuity of retail and service uses;
- Encourage surveillance opportunities by restricting fortress-like facades at street level; and
- Avoid a monotonous pedestrian environment.

Standard: 33.140.230.B Required amounts of window area. In the EX zone, all exterior walls on the ground level which are 20 feet or closer to a street lot line, sidewalk, plaza, or other public open space or right-of-way must have windows. The windows must be at least 50 percent of the length and 25 percent of the ground level wall area. Ground level wall areas include all exterior wall areas up to 9 feet above the finished grade. The requirement does not apply to the walls of residential units, and does not apply to the walls of parking structures when set back at least 5 feet and landscaped to at least the L2 standard.

A. Better meets design guidelines. *The resulting development will better meet the applicable design guidelines (A8 Contribute to a Vibrant Streetscape, B2 Protect the Pedestrian & C5 Design for Coherency); and*

Findings: The standard calls for 25% of the ground level wall area up to 9 feet above grade to be clear glazing. The building provides 112 SF of glazing along NE Davis Street which is 16 SF less than the required 128 SF. The standard also calls for 50% of the ground floor wall length to be clear glazing. The building provides 14.5 feet of glazing along NE Davis Street which is 14.5 feet less than the required 29 feet. This portion of the building acts as the back-of-house and necessarily contains an exit stair, trash room, and parking area entry gate and fence as well as a corner of one of the retail spaces. Clear glazing has been provided into the active retail areas of the building which enhances the pedestrian realm and contributes to a vibrant streetscape by allowing views into the space. The pedestrian experience is further enhanced by the visually interesting fence and gate which uses elements from the building's design to enliven the street edge. The addition of a frosted glass overhead door into the trash room allows for the impression of an active area behind this portion of the building. The NE Davis Street façade is interesting to view and adds diversity to the neighborhood pedestrian environment.

B. Purpose of the standard. *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

Findings: Clear glazing has been provided along active building areas which connect these activities to the adjacent sidewalk along NE Davis Street by providing views into the space. The large windows at the corner of the retail space encourage its continued use by providing views from the street. The glazing in the retail area and the views provided through the fence affords surveillance opportunities for those inside. The incorporation of the highly designed fence and frosted glass overhead door, as well as the mix of materials and distinctive window patterning on the façade, avoids a monotonous

pedestrian environment by providing a number of visually stimulating elements at the sidewalk edge.

The overall solution is consistent with the purpose of the design guidelines and landscape setbacks. *This Modification therefore merits approval.*

CONCLUSIONS

The design review process exists to promote the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. The proposed building is a cohesive composition of visually interesting materials and massing elements that are appropriately oriented to the street. The building is carefully designed and detailed and will be a positive contribution to this portion of this growing area of town. The proposal meets the applicable design guidelines and modification criteria and therefore warrants approval.

DESIGN COMMISSION DECISION

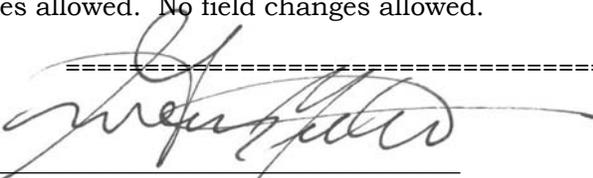
It is the decision of the Design Commission to approve Design Review for a four and five-story mixed-use apartment building containing 11 ground floor retail spaces, 70 apartments, 23 on-site parking spaces, roof deck, and bike room, to be constructed in the Central Eastside subdistrict of the Central City Plan District.

Approval of the following Modification requests:

1. Parking Area Setbacks and Landscaping, PZC 33.266.130.G - Reducing the width of perimeter parking lot landscaping from 5 feet to 3'-8".
2. Parking Area Setbacks and Landscaping, PZC 33.266.130.G - Reducing the amount of interior parking lot landscaping from 585 SF to 134 SF.
3. Ground Floor Windows in the EX Zone, PZC 33.140.230 - Reducing the area and length of ground floor windows along the NE Davis façade.

Approvals per Exhibits C.1-C.42, signed, stamped, and dated April 21, 2011, subject to the following conditions:

- A. As part of the building permit application submittal, each of the 4 required site plans and any additional drawings must reflect the information and design approved by this land use review as indicated in Exhibits C.1-C.42. The sheets on which this information appears must be labeled, "Proposal and design as approved in Case File # LU 11-108266 DZM. No field changes allowed. No field changes allowed.

By: 
Gwen Milius, Design Commission Chair

Application Filed: January 27, 2011
Decision Rendered: April 21, 2011
Decision Mailed: April 28, 2011

Decision Filed: April 22, 2011

About this Decision. This land use decision is **not a permit** for development. Permits may be required prior to any work. Contact the Development Services Center at 503-823-7310 for information about permits.

Procedural Information. The application for this land use review was submitted on January 27, 2011, and was determined to be complete on **March 10, 2011**.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on January 27, 2011.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant waived the 120-day review period (See Exhibit A-2).

Some of the information contained in this report was provided by the applicant.

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. This report is the final decision of the Design Commission with input from other City and public agencies.

Conditions of Approval. This approval may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term "applicant" includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

Appeal of this decision. This decision is final unless appealed to City Council, who will hold a public hearing. Appeals must be filed by 4:30 pm on May 5, 2011 at 1900 SW Fourth Ave. Appeals can be filed Tuesday through Friday on the first floor in the Development Services Center until 3 p.m. After 3 p.m. and on Monday, appeals must be submitted to the receptionist at the front desk on the fifth floor. Information and assistance in filing an appeal is available from the Bureau of Development Services in the Development Services Center or the staff planner on this case. You may review the file on this case by appointment at, 1900 SW Fourth Avenue, Suite 5000, Portland, Oregon 97201.

If this decision is appealed, a hearing will be scheduled and you will be notified of the date and time of the hearing. The decision of City Council is final; any further appeal is to the Oregon Land Use Board of Appeals (LUBA).

Upon submission of their application, the applicant for this land use review chose to waive the 120-day time frame in which the City must render a decision. This additional time allows for any appeal of this proposal to be held as an evidentiary hearing, one in which new evidence can be submitted to City Council.

Who can appeal: You may appeal the decision only if you have written a letter which was received before the close of the record at the hearing or if you testified at the hearing, or if you are the property owner or applicant. Appeals must be filed within 14 days of the decision. **An appeal fee of \$14,648 will be charged (one-half of the application fee for this case).**

Neighborhood associations may qualify for a waiver of the appeal fee. Additional information on how to file and the deadline for filing an appeal will be included with the decision. Assistance in filing the appeal and information on fee waivers are available from the Bureau of Development Services in the Development Services Center, 1900 SW Fourth Ave., First Floor. Fee waivers for neighborhood associations require a vote of the authorized body of your association. Please see appeal form for additional information.

Recording the final decision.

If this Land Use Review is approved the final decision must be recorded with the Multnomah County Recorder. A few days prior to the last day to appeal, the City will mail instructions to the applicant for recording the documents associated with their final land use decision.

- A building or zoning permit will be issued only after the final decision is recorded.

The applicant, builder, or a representative may record the final decision as follows:

- By Mail: Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- In Person: Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034
For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this approval. An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Zone Change and Comprehensive Plan Map Amendment approvals do not expire.

Applying for your permits. A building permit, occupancy permit, or development permit must be obtained before carrying out this project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed here.
- All applicable development standards, unless specifically exempted as part of this land use review.
- All requirements of the building code.
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

Chris Caruso
April 25, 2011

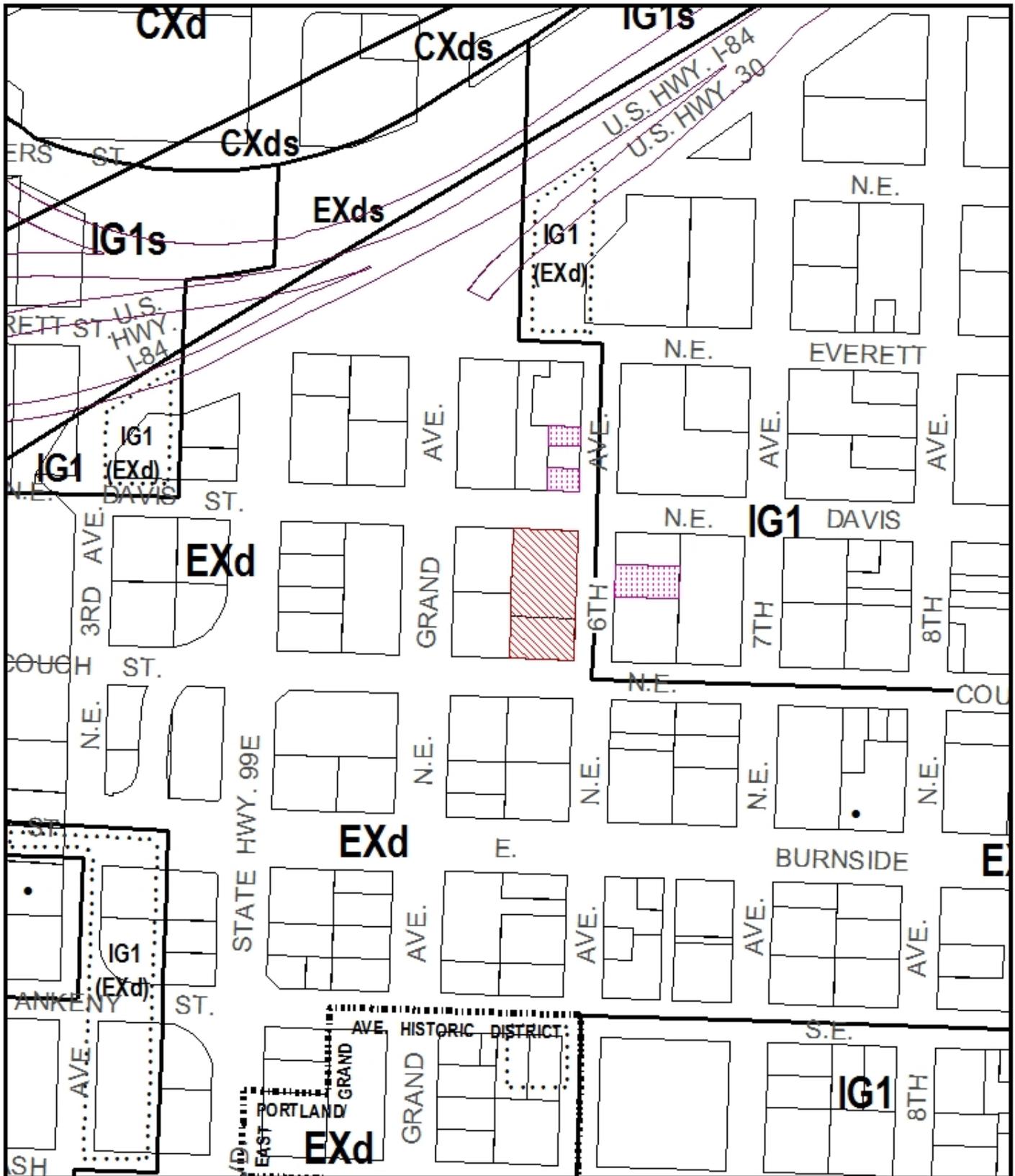
The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

EXHIBITS – NOT ATTACHED UNLESS INDICATED

- A. Applicant's Information
 - 1. Narrative
 - 2. 120-day waiver
- B. Zoning Map (attached)
- C. Plans & Drawings
 - 1. Site Plan (attached)
 - 2. Existing Conditions Plan
 - 3. Site Grading Plan
 - 4. Site Utility Plan
 - 5. Site Erosion Control Plan
 - 6. Site Details
 - 7. Site & Utility Details
 - 8. Erosion Control Details
 - 9. Building Diagrams
 - 10. Landscape Plan (attached)
 - 11. 1st Floor Plan
 - 12. 2nd Floor Plan
 - 13. 3rd & 4th Floor Plan
 - 14. 5th Floor Plan
 - 15. Roof Plan
 - 16. Elevations S & E (attached)
 - 17. Elevations N & W (attached)
 - 18. Building Sections
 - 19. Building Section
 - 20. Typ. Sections & Elevs
 - 21. Enlarged Elevations
 - 22. Enlarged Elevations & Sections
 - 23. Enlarged Elevations
 - 24. Details - Windows
 - 25. Details - Cladding
 - 26. Details – Canopies
 - 27. Balcony Details
 - 28. Vent Details
 - 29. Roof Deck
 - 30. Parking Gate & Fence
 - 31. Rooftop Mech. Screens
 - 32. Reflected Ceiling Plans
 - 33. Materials
 - 34. View – SE
 - 35. View – SW
 - 36. View – NE
 - 37. View – Roof & Parking Gate
 - 38. View – Parking Area
 - 39. Night Rendering
 - 40. Shadow Study
 - 41. Materials & Product Cutsheets
 - 42. Storm Drainage Calculations
- D. Notification information:
 - 1. Request for response
 - 2. Posting letter sent to applicant

3. Notice to be posted
 4. Applicant's statement certifying posting
 5. Mailing list
 6. Mailed notice
- E. Agency Responses:
1. Water Bureau
 2. Bureau of Environmental Services
 3. Bureau of Transportation Engineering and Development Review
- F. Letters: none received
- G. Other:
1. Original LUR Application
 2. Site History Research
 3. Incomplete Letter
 4. Pre-Application and Early Assistance Summary

cc: Applicants and Representatives
Neighborhood Associations
Those who testified, orally or in writing
City Auditor's Office
Development Services Center
BDS Staff for Bureau of Buildings
BDS Staff for Commission Book



ZONING

-  Site
-  Also Owned
-  Historic Landmark



This site lies within the:
CENTRAL CITY PLAN DISTRICT

| | |
|-------------|-------------------|
| File No. | LU 11-108266 DZM |
| 1/4 Section | 3031 |
| Scale | 1 inch = 200 feet |
| State_Id | 1N1E35CB 4300 |
| Exhibit | B (Feb 01,2011) |



VALLASTER CORP ARCHITECTS, PC
 111 SW ALDER
 PORTLAND, OR 97205
 PHONE: 503.228.0311
 FAX: 503.228.0312
 WWW: V1A1V1A.COM

OWNER:
 CENTRAL EASTSIDE
 PARTNERS LLC
 1400 NE 6TH AVE
 LAKE OSWEGO, OR 97035

PROJECT NAME:
BM & COUCH
 133 NE DAVIS
 PORTLAND, OR
 97203

ISSUE DATES:
 DESIGN REVIEW: 02/21/14
 DESIGN REVIEW: 03/07/14
 DESIGN REVIEW: 04/11/14

JOB NO.: 899
DRAWN: AMK
CHECKED: CHM
STATUS: TYPE 1B

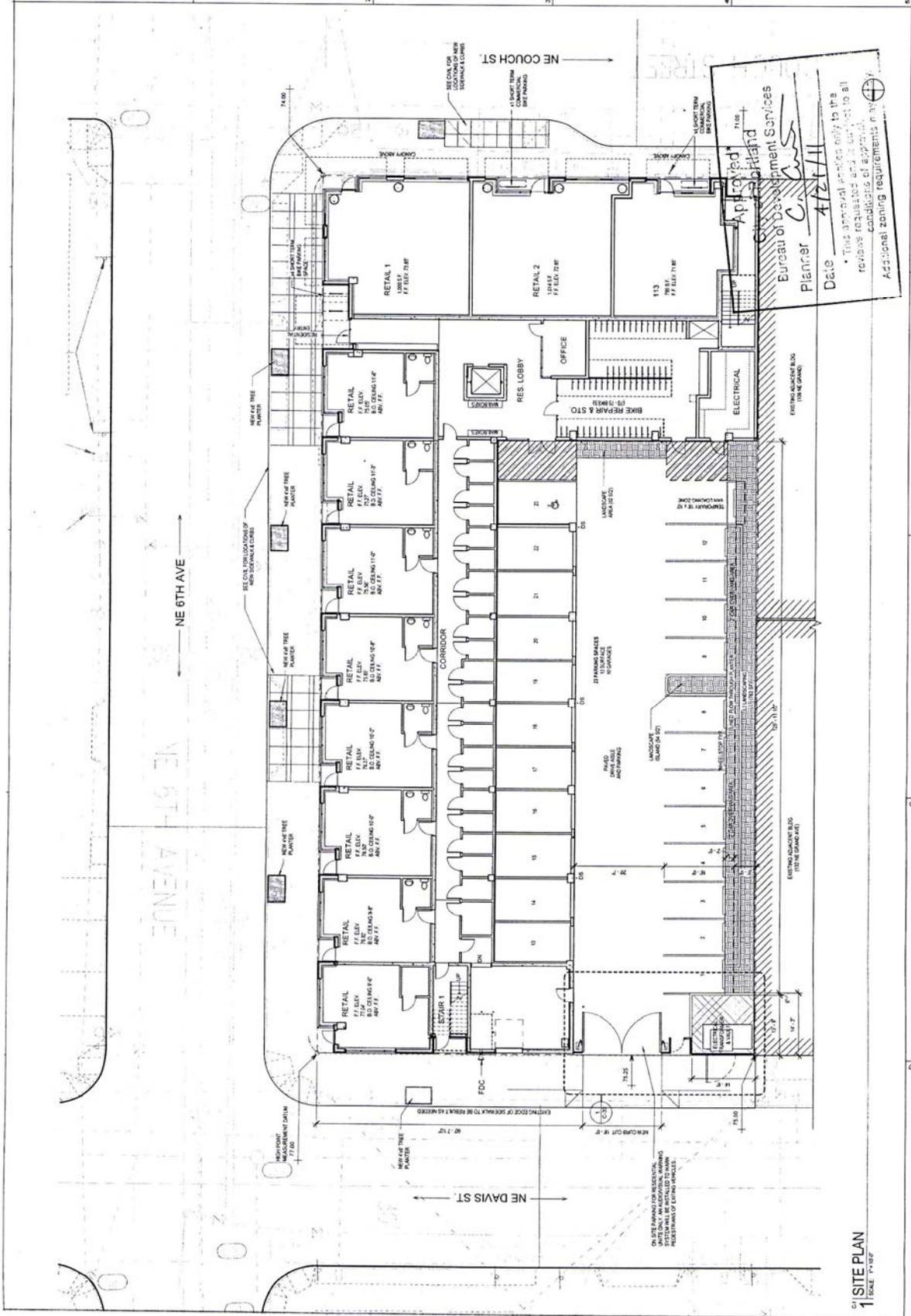
SITE PLAN

ORIGINAL SHEET SIZE: 22"X34"

EXHIBIT

C-1

LI 11-10000-000-00



SITE PLAN
1
 SCALE: 1/8"=1'-0"



VALLASTER CORP
 711 SW A L D E R
 PORTLAND ORE 97205
 PHONE: 503.228.0311
 FAX: 503.228.0314
 WWW.VC-CORP.COM

OWNER:
 CENTRAL EASTBRIE
 522 NE DAVIS
 PORTLAND, OR 97203

PROJECT NAME:
 6TH & COUCH

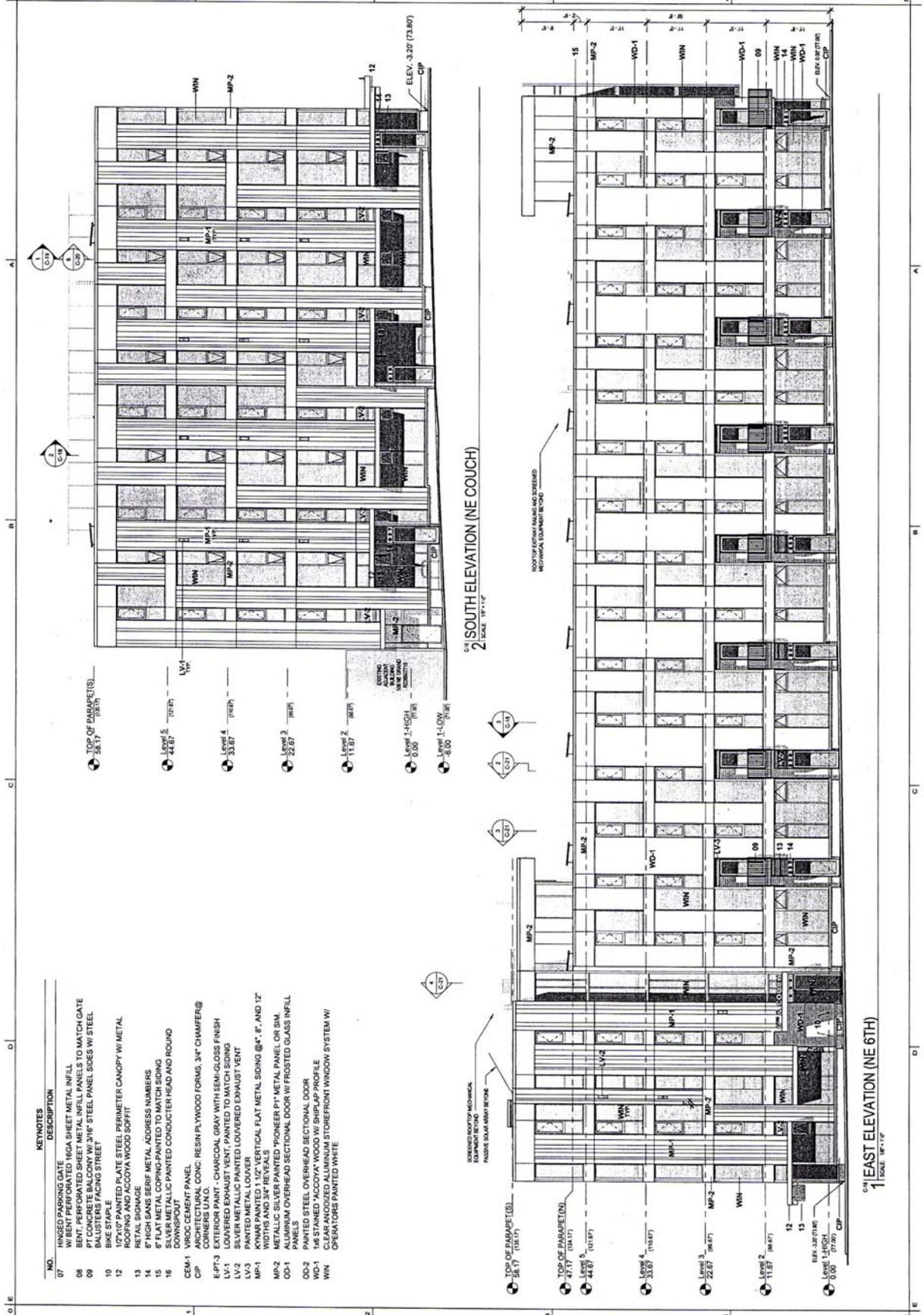
ISSUE DATES:
 DESIGN REVIEW 03/27/11
 DESIGN REVIEW 03/29/11
 DESIGN REVIEW 04/11/11
 DESIGN REVIEW 04/20/11

JOB NO.:
 DRAWN BY:
 CHECKED BY:
 STATUS:

ELEVATIONS
 S & E

EXHIBIT
 C-16

ORIGINAL SHEET SIZE 24" x 36"

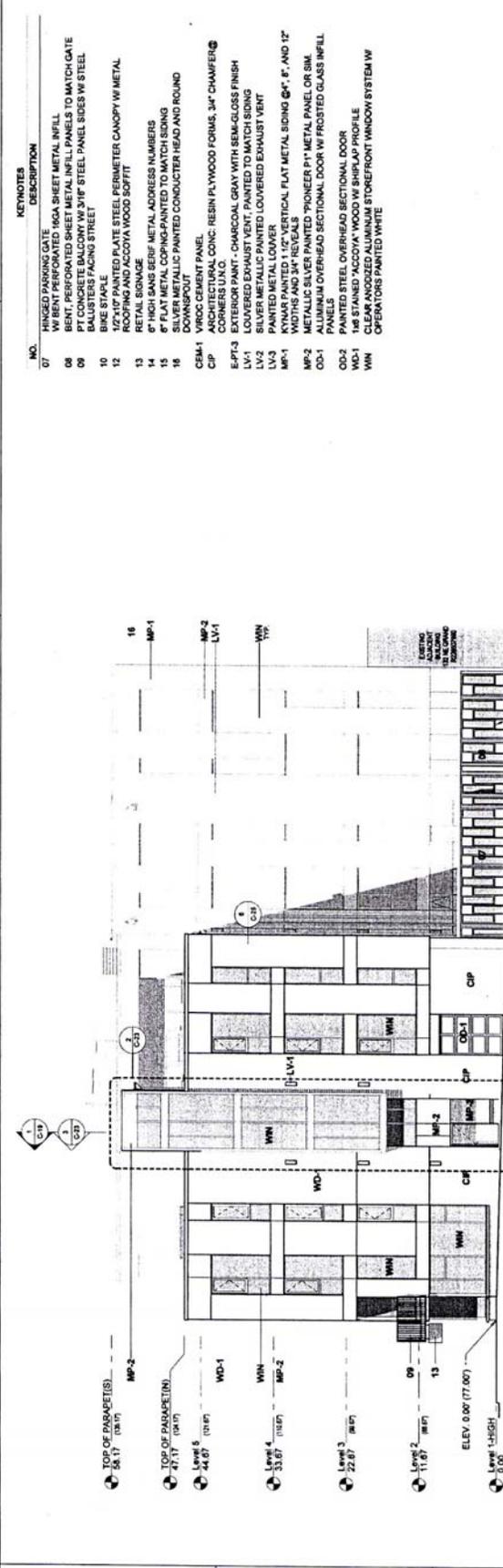


KEYNOTES

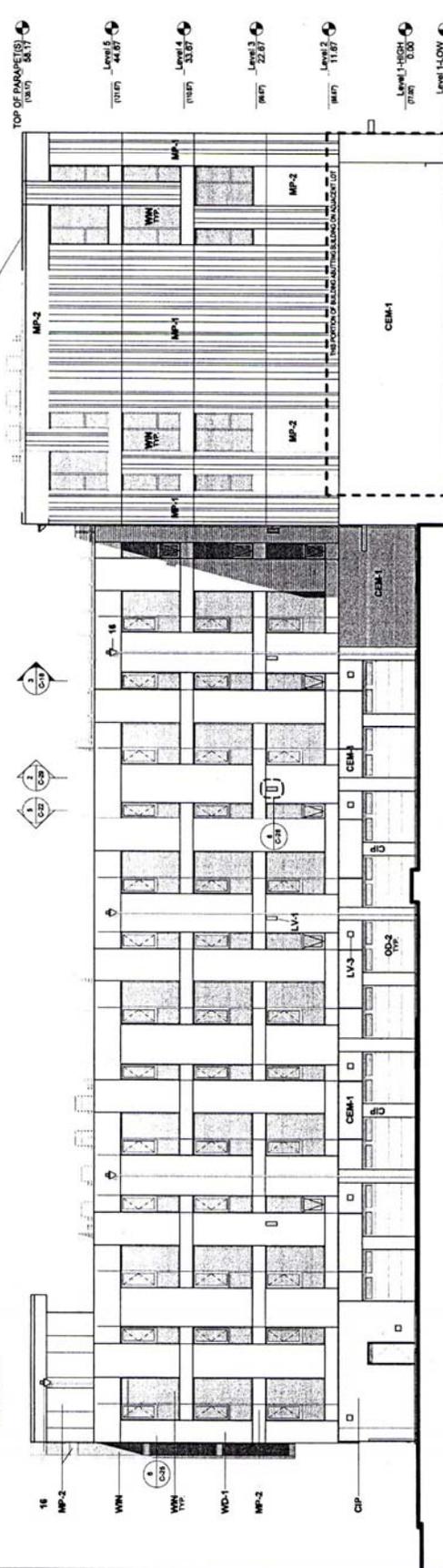
| NO. | DESCRIPTION |
|--------|---|
| 07 | HINGED PARKING GATE |
| 08 | MEGA SHEET METAL INELL |
| 09 | PERFORATED SHEET METAL INELL PANELS TO MATCH GATE |
| 10 | PT CONCRETE BALCONY W/ 3/4" STEEL PANEL SIDES W/ STEEL BALUSTERS FACING STREET |
| 11 | BIKE STAPLE |
| 12 | ROOFING AND ACCOYA WOOD SOFFIT |
| 13 | RETAL SIGNAGE |
| 14 | 8" HIGH SANS SERIF METAL ADDRESS NUMBERS |
| 15 | 8" FLAT METAL COPPING-PAINTED TO MATCH SIDING |
| 16 | SOLAR METALIC PAINTED CONDUCTOR HEAD AND ROUND VERTICAL FIN |
| CEM-1 | VIRO CEMENT PANEL |
| CP | ARCHITECTURAL CONC. RESIN PLYWOOD FORMS, 3/4" CHAMFER@ CORNERS UNLD. |
| E-PT-3 | EXTERIOR PAINT - CHARCOAL GRAY WITH SEMI-GLOSS FINISH |
| LV-1 | 1 1/2" VERTICAL FLAT METAL SIDING @ 4", 8", AND 12" SPACING |
| LV-2 | PAINTED METAL LOUVER |
| LV-3 | SILVER METALIC PAINTED COVERED EXHAUST VENT |
| MP-1 | KYMAR PAINTED 1 1/2" VERTICAL FLAT METAL SIDING @ 4", 8", AND 12" WIDTHS AND 3/4" REVEALS |
| MP-2 | METALLIC SILVER PAINTED THINNER P11 METAL PANEL OR SIM PANELS |
| OD-1 | OVERHEAD SECTIONAL DOOR W/ FROSTED GLASS INELL |
| OD-2 | PAINTED STEEL OVERHEAD SECTIONAL DOOR |
| WD-1 | 1/8" STAINED "ACCOYA" WOOD W/ SHIPLAP PROFILE |
| WIN | CLEAR ANODIZED ALUMINUM STOREFRONT WINDOW SYSTEM W/ OPERATORS PAINTED WHITE |

51 SOUTH ELEVATION (NE COUCH)
 SCALE: 1/8" = 1'-0"

51 EAST ELEVATION (NE 6TH)
 SCALE: 1/8" = 1'-0"



01 NORTH ELEVATION (NE DAVIS)
SCALE: 1/8"=1'-0"



02 WEST ELEVATION (PARKING & PROPERTY LINE)
SCALE: 1/8"=1'-0"

| NO. | KEYNOTES | DESCRIPTION |
|--------|---|---|
| 07 | HINGED PARKING GATE | WI BENT PERFORATED 16GA SHEET METAL INFILL |
| 08 | CONCRETE BALCONY | CONCRETE BALCONY W/ 3/4" STEEL PANEL SIEVES W/ 6" STEEL |
| 09 | BALLUSTERS FACING STREET | CONCRETE BALCONY W/ 3/4" STEEL PANEL SIEVES W/ 6" STEEL |
| 10 | BIKE STAPLE | 1/2"x1/2" PAINTED PLATE STEEL PERIMETER CANOPY W/ METAL |
| 11 | WOOD SPOKE | 1/2" DIA. ACCOVA WOOD SOFFIT |
| 12 | WOOD SPOKE | 1/2" DIA. ACCOVA WOOD SOFFIT |
| 13 | WOOD SPOKE | 1/2" DIA. ACCOVA WOOD SOFFIT |
| 14 | WOOD SPOKE | 1/2" DIA. ACCOVA WOOD SOFFIT |
| 15 | WOOD SPOKE | 1/2" DIA. ACCOVA WOOD SOFFIT |
| 16 | WOOD SPOKE | 1/2" DIA. ACCOVA WOOD SOFFIT |
| CEM-1 | CONCRETE | CONCRETE |
| CIP | CONCRETE | CONCRETE |
| E-PT-3 | EXTERIOR PAINT - CHARCOAL GRAY WITH SEMI-GLOSS FINISH | |
| LV-1 | LOUVERED EXHAUST VENT - PAINTED TO MATCH SIDING | |
| LV-2 | SILVER METALLIC PAINTED LOUVERED EXHAUST VENT | |
| LV-3 | PAINTED METAL LOUVERED EXHAUST VENT | |
| MP-1 | METALLIC SILVER PAINTED FLAT METAL SIDING 6"x 8" AND 12" WADHS AND 3/4" REVEALS | |
| MP-2 | METALLIC SILVER PAINTED PIONEER P-11 METAL PANEL OR SIM. | |
| OD-1 | ALUMINUM OVERHEAD SECTIONAL DOOR W/ FROSTED GLASS INFILL | |
| OD-2 | PAINTED STEEL OVERHEAD SECTIONAL DOOR | |
| WD-1 | 1/4" STAINED ACCOVA WOOD W/ SHIP LAP PROFILE | |
| WN | CLEAR ANODIZED ALUMINUM STOREFRONT WINDOW SYSTEM W/ OPERATORS PAINTED WHITE | |