

# City of Portland, Oregon Bureau of Development Services

#### **Land Use Services**

FROM CONCEPT TO CONSTRUCTION

Carmen Rubio, Commissioner Rebecca Esau, Director Phone: (503) 823-7310 TTY: (503) 823-6868 www.portland.gov/bds

## FINAL FINDINGS AND DECISION BY THE LANDMARKS COMMISSION RENDERED ON March 13, 2023

CASE FILE NUMBER: LU 22-204531 HRM AD

PC # 22-128290 1819 NW Everett

BUREAU OF DEVELOPMENT SERVICES STAFF: Hannah Bryant 503-865-6520 /

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#### **GENERAL INFORMATION**

**Applicant:** Andrew Becker | Hartshorne Plunkard Architecture

315 W. Walton St Chicago, IL 60610

abecker@hparchitecture.com

**Developer:** Tanya Toby | Gaia Ventures I, LLC

5821 SE Powell Blvd Portland, OR 97026

**Owner:** Northwest Neighborhood Cultural Center

PO Box 96116 Portland, OR 97296

Site Address: 1819 NW EVERETT ST

Legal Description: BLOCK 172 LOT 1-4, COUCHS ADD; BLOCK 172 LOT 6&7, COUCHS

ADD

**Tax Account No.:** R180215530, R180215610

**State ID No.:** 1N1E33DB 02800, 1N1E33DB 02900

Quarter Section: 3026

**Neighborhood:** Northwest District, contact Greg Theisen at

planningchair@northwestdistrictassociation.org

**Business District:** Northwest Portland, contact at nobhillportland@gmail.com. **District Coalition:** Neighbors West/Northwest, contact at admin@nwnw.org

**Plan District:** Northwest

**Other Designations:** Historic Landmark – First Church of Christ Scientist

Zoning: CM2(MU-U)d – Commercial Mixed-Use 2 with a Design Overlay

**Case Type:** HRM – Historic Resource Review w/ Modification Review

**Procedure:** Type III, with a public hearing before the Landmarks Commission. The decision of the Landmarks Commission can be appealed to City Council.

#### Proposal:

Type III **Historic Resource Review** to convert the former Landmark church into a club with restaurants, spa and bar, and construction of a new, detached, six-story hotel on a vacant parcel within the Landmark boundary. One loading space and no parking is proposed in the

hotel. Four **Modifications** are requested: to increase building height; reduce the dimensions of the loading space; increase the distance between short-term bike parking and main entrances; and allow loading to be entered/exited in a rearward motion.

Historic Resource Review is required for non-exempt exterior alterations and non-exempt development in a Historic District.

#### Relevant Approval Criteria:

In order to be approved, this proposal must comply with the criteria of Title 33, Portland's Zoning Code. The relevant criteria are:

- Community Design Guidelines
- 33.846.070 Modifications That Better Meet Design Guidelines
- Alphabet Historic District Addendum to the Community Design Guidelines

#### ANALYSIS

**Site and Vicinity:** The site is a 30,000 square foot L-shaped parcel located in the Alphabet Historic District, the Northwest Plan District and the Northwest District Pedestrian District. The southern portion of the site is developed with the existing First Church of Christ Scientist Historic Landmark. This individually listed Landmark building was built in 1909 to be the first permanent home of the earliest Christian Scientist congregation in Portland. The northern portion of the site is approximately ten thousand square feet. It is currently developed as a surface parking area. There is no prior development history for this parcel in city records.

The site fronts NW Flanders to the north, NW 18<sup>th</sup> Avenue to the east, NW Everett Street to the south and NW 19<sup>th</sup> Avenue to the west. At this location, NW Flanders is classified as a Local Service Traffic Street, a Local Service Transit Street, a Major City Bikeway, a Major City Walkway, a Local Service Freight Street and a Minimum Emergency Response Street. NW 18<sup>th</sup> Avenue is classified as a Local Service Traffic Street, a Transit Access Street, a Major City Bikeway, a City Walkway, a Local Service Freight Street, and a Secondary Emergency Response Street. NW Everett Street is classified as a Local Service Traffic Street, a Transit Access Street, a City Bikeway, a Major City Walkway, a Local Service Freight Street, and a Major Emergency Response Street. NW 19<sup>th</sup> Avenue is classified as a Local Service Traffic Street, a Transit Access Street, a Major City Bikeway, a City Walkway, a Local Service Freight Street, and a Secondary Emergency Response Street.

The <u>Alphabet Historic District</u> is an area of Portland significant for its concentration of intact late 19<sup>th</sup> and early 20<sup>th</sup> Century, mostly middle class, housing stock and small-scale commercial buildings. Of special note are the many mid-sized apartment and institutional buildings. Many of these are in the various Period Revival styles, e.g., Tudor, Spanish Colonial, Byzantine, Jacobean, etc. and this is especially the case in the immediate vicinity of the proposed new development. The area is characterized by a grid of narrower, more tree-lined, east-west residential streets, named alphabetically after prominent Portlanders of the day, which are crossed by generally more robust north-south avenues. Two of these, NW 21<sup>st</sup> Avenue and NW 23<sup>rd</sup> Avenue are low-scale business corridors featuring a mix of purpose-built commercial structures and converted houses.

The Northwest Plan District implements the Northwest District Plan, providing for an urban level of mixed-use development including commercial, office, housing, and employment. Objectives of the plan district include strengthening the area's role as a commercial and residential center. The regulations of this chapter: promote housing and mixed-use development; address the area's parking scarcity while discouraging auto-oriented developments; enhance the pedestrian experience; encourage a mixed-use environment, with transit supportive levels of development and a concentration of commercial uses, along main

streets and the streetcar alignment; and minimize conflicts between the mixed-uses of the plan district and the industrial uses of the adjacent Guild's Lake Industrial Sanctuary.

**Zoning:** The Commercial/Mixed Use 2 (CM2) zone is a medium-scale zone intended for sites in a variety of centers, along corridors, and in other mixed-use areas that have frequent transit service. The zone allows a wide range and mix of commercial and residential uses, as well as employment uses that have limited off-site impacts. Buildings in this zone will generally be up to four stories tall unless height and floor area bonuses are used, or plan district provisions specify other height limits. Development is intended to be pedestrian-oriented, provide a strong relationship between buildings and sidewalks, and complement the scale of surrounding residentially zoned areas.

The <u>historic resource overlay zone</u> protects historic resources that have been identified as significant to the history of the city and region. The regulations implement Portland's Comprehensive Plan policies that address historic preservation. These policies recognize the role historic resources have in promoting education and enjoyment for those living in and visiting the region. The regulations foster awareness, memory, and pride among the region's current and future residents in their city and its diverse architecture, culture, and history. Historic preservation recognizes social and cultural history, retains significant architecture, promotes economic and environmental health, and stewards important resources for the use, education, and enjoyment of future generations.

The <u>Design overlay zone</u> ensures that Portland is both a city designed for people and a city in harmony with nature. The Design overlay zone supports the city's evolution within current and emerging centers of civic life. The overlay promotes design excellence in the built environment through the application of additional design standards and design guidelines that:

- Build on context by enhancing the distinctive physical, natural, historic and cultural qualities of the location while accommodating growth and change;
- Contribute to a public realm that encourages social interaction and fosters inclusivity in people's daily experience; and
- Promotes quality and long-term resilience in the face of changing demographics, climate and economy.

**Land Use History:** City records indicate that prior land use reviews include:

• <u>LU 12-170628 HDZ</u> – Historic Design Review approval for the removal of a chimney at the Landmark Church, includes conditions of approval that must be maintained regarding the cataloging and storage of the historic chimney materials.

**Agency Review:** A "Notice of proposal in Your Neighborhood" was mailed February 21, 2023. The following Bureaus have responded with no issue or concerns:

- Fire Bureau (E.1)
- Bureau of Environmental Services (E.2)
- Portland Bureau of Transportation (E.3)
- Urban Forestry (E.4)
- Life Safety (E.5)
- Site Development (E.6)
- Water Bureau

**Neighborhood Review:** A Notice of Proposal in Your Neighborhood was mailed on February 21, 2023. Three written response has been received from either the Neighborhood Association or notified property owners in response to the proposal.

- <u>Shannon and David Lea</u>, February 24, 2023, neighbors support the proposal, including the height. Some concerns about on-street parking impacts.
- <u>Dennis Harper</u>, February 25, 2023, neighbor has some concern about the proposed fence and lack of landscaping details. Supports the rooftop canopy design proposed for the penthouse of the new hotel.
- <u>Steve Pinger (Northwest District Association)</u>, February 23, 2023. The NWDA strongly supports the project concept, including the renovation and repurposing of the existing landmark building, and the proposed new hotel building on the adjacent parcel.
- <u>Charisse Tooze</u>, March 6, 2023, neighbor has concerns about the impact of the new development on on-street parking availability.

**Staff Response**: Staff appreciates these neighbors for taking the time to share their thoughtful and nuanced comments. While potential on-street parking impacts are not approval criteria for new development, the concerns about quality and durability of proposed materials *are* subject to review. Staff shared the concerns, and requested additional information related to building materials and detailing in order to determine whether the proposal meets the approval criteria related to quality of construction detailing and materials.

#### PROCEDURAL HISTORY

This application was submitted on November 16, 2022. The applicant deemed it complete on January 23, 2023. A hearing was scheduled for March 13, 2023 (49 days after deeming the application complete). The applicant submitted a full extension. The land use application will expire on January 23, 2024. Prior to the first hearing, a staff report published on March 3, 2023 did not recommend approval due to a lack of necessary building and material details. The applicant submitted additional materials, and a revised staff report recommending approval was published on March 10, 2023. The proposal was unanimously approved by the Historic Landmarks Commission at its first hearing on March 13, 2023.

#### ZONING CODE APPROVAL CRITERIA

#### Chapter 33.846.060 - Historic Resource Review

#### **Purpose of Historic Resource Review**

Historic Resource Review ensures the conservation and enhancement of the special characteristics of historic resources.

#### Historic Resource Review Approval Criteria

Requests for Historic Resource Review will be approved if the review body finds the applicant has shown that all of the approval criteria have been met.

**Findings:** The site is a designated Historic Landmark outside the Central City Plan District and not within in a Historic or Conservation District, and the proposal is for non-exempt treatments. Therefore, the proposal requires Historic Resource Review approval. The approval criteria are those listed in *33.846.060 G – Other Historic Approval Criteria*.

Staff has considered all of the approval criteria and addressed only those applicable to this proposal.

#### <u>Historic Alphabet District - Community Design Guidelines Addendum</u>

- **1. Historic Changes.** Most properties change over time; those changes that have acquired historic significance will be preserved.
- 2. **Differentiate New from Old.** New additions, exterior alterations, or related new construction will retain historic materials that characterize a property to the extent practicable. Replacement materials should be reasonable facsimiles of the historic materials they replace. The design of new construction will be compatible with the historic qualities of the district as identified in the Historic Context Statement.

**Findings for 1 and 2:** The two-part proposal includes the restoration of the existing First Church of Christ Scientist Historic Landmark building, and the construction of a new freestanding hotel on an existing surface parking lot within the Landmark boundary. While the new hotel is designed to complement the Landmark, it is clearly differentiated from the Landmark in architectural style, material, fenestration pattern and massing.

The existing three-story, masonry Landmark building is notable for its character-defining domed roof form, arched leaded windows with opalescent glass and massive midblock portico. The proposed renovations are primarily limited to the interior, however a few exterior alterations are proposed. These include the repair and restoration of existing original windows on the three street-facing facades; repair or partial replacement of the existing tin roof; new frameless glass guardrail around the perimeter of the level three roof; new stucco rainscreen cladding at the level three mass; two new fabric awnings over basement level doors at the east façade; removal of an original chimney and an elevator overrun on the north side of the roof and construction of new stair overruns at the location of the original chimneys; replacement of all windows on the north (non-street facing) façade on levels one through three; a new basement level service entry on the north façade, and modification of the primary arched windows on the east and west level three facades to incorporate a person door within the existing opening.

While there are numerous elements proposed to be altered, the scope of the proposed changes is modest in proportion to the scale of the existing building, its many years of deferred maintenance, and the complexity of adaptively reusing a building designed for such a different use. The alterations are carefully located to minimize visibility from the street level, while maintaining the character-defining features on three street-facing facades, and facilitating spill-out activation spaces that will add vibrancy to the public realm.

Therefore, these guidelines are met.

**3. Hierarchy of Compatibility.** Exterior alterations and additions will be designed to be compatible primarily with the original resource, secondarily with adjacent properties, and finally, if located within a historic or conservation district, with the rest of the District. Where practical, compatibility will be pursued on all three levels. New development will seek to incorporate design themes characteristic of similar buildings in the Historic Alphabet District.

**Findings for 3:** The proposal includes the removal of one remaining chimney on the north side, as well as a non-original elevator overrun also located on the north side of the roof. The chimney is one of the original pair that flanked the central dome and reinforced the building's symmetry. One of the paired chimneys was removed years ago, and the remaining unreinforced masonry chimney no longer serves the building functionally and detracts from the symmetrical architecture. The removal of the

chimney and elevator overrun will facilitate the addition of two symmetrical stair overruns at the location of the original chimneys. The overruns are tucked in toward the center of the roof edge, furthest from the east and west street facades. They are proposed to be clad in a stucco cladding that will serve as a discreet background for the original cut stone cladding without drawing attention to itself.

As noted above, the bulk of the proposed alterations are targeted to the areas that are not visible from the public realm. The interior program and floor plans have been designed to ensure retention of the original architectural elements on the street-facing facades. The alterations and additions do not diminish the Landmark's iconic architecture, nor do they detract from the district.

Therefore, this guideline is met.

#### **Community Design Guidelines**

**P1. Plan Area Character.** Enhance the sense of place and identity by incorporating site and building design features that respond to the area's desired characteristics and traditions. **P2. Historic and Conservation Districts.** Enhance the identity of historic and conservation districts by incorporating site and building design features that reinforce the area's historic significance. Near historic and conservation districts, use such features to reinforce and complement the historic areas.

**Findings for P1 and P2:** The Alphabet Historic District is notable for its diversity of architectural styles and building types. However, some architectural themes are present throughout the district, including cornice treatments; a consistent fenestration pattern and visual transition between floor levels – particularly between the ground level and upper stories.

The proposed new hotel enhances the district's identity by incorporating numerous building features that respond to the traditional Alphabet District context and reinforce its significance. These include the active, pedestrian-oriented ground level, defined by large operable windows and granite pilasters, that is strongly differentiated from the upper levels through a change in cladding material and fabric awnings.

Above the ground level, the windows repeat the regular fenestration pattern typical throughout the district. Ornate metal balcony railings add rich detailing in front of French doors at the level two balconies and at all windows at the building's corner. Additional stucco detailing at the cornice is typical of many historic buildings throughout the district. Above the cornice, a conservatory-inspired penthouse is set back from the roof edges. While the penthouse screens the building's mechanical equipment, it also contains a food service area to serve the adjacent roof deck. This activation of all available outdoor area is consistent with the highly walkable, pedestrian-oriented context, and will provide an asset to the neighborhood with rare view of the rooftops of numerous Historic Landmarks, as well as incredible vistas of the surrounding mountains and hills.

Overall, the hotel strikes the appropriate balance for new development within a Landmark boundary. It is an elegant stand-alone addition to the neighborhood while also being a graceful neighbor to the iconic adjacent Landmark.

Therefore, these guidelines are met.

**E1. The Pedestrian Network.** Create an efficient, pleasant, and safe network of sidewalks and paths for pedestrians that link destination points and nearby residential areas while

visually and physically buffering pedestrians from vehicle areas.

- **E2. Stopping Places.** New large-scale projects should provide comfortable places along pedestrian circulation routes where people may stop, visit, meet, and rest.
- **E3.** The Sidewalk Level of Buildings. Create a sense of enclosure and visual interest to buildings along sidewalks and pedestrian areas by incorporating small scale building features, creating effective gathering places, and differentiating street level facades.
- **E4.** Corners that Build Active Intersections. Create intersections that are active, unified, and have a clear identity through careful scaling detail and location of buildings, outdoor areas and entrances.
- **E5.** Light, Wind, and Rain. Enhance the comfort of pedestrians by locating and designing buildings and outdoor areas to control the adverse effects of sun, shadow, glare, reflection, wind, and rain.
- **D2. Main Entrances.** Make the main entrances to houses and buildings prominent, interesting, pedestrian-accessible, and transit-oriented.

**Findings for E1, E2, E3, E4, E5 and D2:** The proposed hotel is designed to facilitate an indoor-outdoor experience for its guests and to maximize activation of the pedestrian realm. The hotel introduces an active ground level, with operable storefront windows and recessed bays between pilasters to facilitate café seating along both street frontages (NW 19<sup>th</sup> Ave. and NW Flanders Street). Fixed fabric awnings provide year-round weather protection for pedestrians, while reinforcing the building's European-inspired style. The primary entrance is located at the curved corner, where it is most visible to guests arriving via one-way streets and it activates the corner. Wrapped by a large canopy, the tall entry doors accentuate the building's unique curved corner. The design utilizes operable windows, French doors, balconies and a roof deck on upper levels to ensure that the entire building serves to activate the pedestrian realm.

Therefore, these guidelines are met.

- **D1. Outdoor Areas.** When sites are not fully built on, place buildings to create sizable, usable outdoor areas. Design these areas to be accessible, pleasant, and safe. Connect outdoor areas to the circulation system used by pedestrians;
- **D3.** Landscape Features. Enhance site and building design through appropriate placement, scale, and variety of landscape features.

**Findings for D1 & D3:** The new hotel building is built to the property lines on both street frontages, however it is set back fifteen feet from the Landmark to the south as well as ten feet from the shared property line with the existing multifamily apartment building to the east. A service path and code-required landscape buffer along the east side facilitate access between the loading space in the new building and the new basement level service door into the north façade of the existing Landmark. At the south side, a sunken courtyard between the basement levels of the new hotel and the existing Landmark serves to screen mechanical equipment, reducing the requirements for rooftop mechanical and facilitating a more active rooftop on the hotel. On the west side of this setback, the grade aligns with the sidewalk and the ADA access ramp to the Landmark building doubles as access to short-term bike parking.

The existing Landmark has deep, landscaped setbacks between the building and all street lot lines. On the east and west sides, the building is set back more than twenty feet from the sidewalks. New, occupiable outdoor spill out spaces are proposed on the east and west side of the Landmark. On the east side, a stepped courtyard is proposed between the building and the sidewalk, to serve as outdoor seating for a new basement-level café. On the west side, an outdoor garden with seating is proposed along the sidewalk edge. The enlarged site plan (C.2) indicates that the outdoor seating for both areas is proposed to be enclosed by a six-foot fence at the sidewalk edge, although this

fence is not shown in the building elevations. Staff has concerns that the fence will create a fortress-like effect at the sidewalk edge, and that its presence may offset the public benefits of activating these outdoor spaces. Staff suggests that setting the fence back three feet from the sidewalk could allow a landscape buffer along the sidewalk edge that would better maintain the original landscape design, softening the fence and allowing it to visually recede from view.

The proposal maximizes the use of the site on behalf of both buildings, while utilizing grade and the buildings to screen necessary service and mechanical needs. The required bike parking is located on three street frontages, ensuring that hotel, club and retail guests are all able to easily locate convenient bike racks.

Therefore, these guidelines are met with an added nominal landscape buffer between the public sidewalk edge and the fences.

**D5. Crime Prevention.** Use site design and building orientation to reduce the likelihood of crime through the design and placement of windows, entries, active ground level uses, and outdoor areas.

**Findings for D5:** The proposal for both buildings is exemplary in its focus on activating the ground level, including exterior spaces. As described above, the new hotel fronts both street lots lines, with its undeveloped outdoor space located between the buildings, where it can be easily secured and illuminated. Both street frontages are lined with operable windows, and the ground level has been designed to facilitate café seating along the sidewalks. Ground level lighting embedded in each pilaster at the new hotel ensures the spaces are well lit throughout the day.

Spill out seating in the east and west setbacks of the Landmark building incentivize more people to spend time outside, adjacent to the sidewalk, where they can provide 'eyes on the street' and enhance pedestrian perceptions of safety. The proposed fences are intended to ensure that the outdoor spaces may be secured after hours and do not feel like dark, unsafe spaces for pedestrians.

Therefore, this guideline is met.

- **D6. Architectural Integrity.** Respect the original character of buildings when making modifications that affect the exterior. Make additions compatible in scale, color, details, material proportion, and character with the existing building.
- **D7. Blending into the Neighborhood.** Reduce the impact of new development on established neighborhoods by incorporating elements of nearby, quality buildings such as building details, massing, proportions, and materials.

**Findings for D6 & D7:** The two-part proposal includes the restoration of the existing First Church of Christ Scientist Historic Landmark building, and the construction of a new freestanding hotel on an existing surface parking lot within the Landmark boundary. While the new hotel is designed to complement the Landmark, it is clearly differentiated from the Landmark in architectural style, material, fenestration pattern and massing.

#### Existing Landmark

The existing three-story, masonry Landmark building is notable for its character-defining domed roof form, arched leaded windows with opalescent glass and massive midblock portico. The proposed exterior alterations include the repair and restoration of existing original windows on the three street-facing facades; repair or partial replacement of the existing tin roof; new frameless glass guardrail around the perimeter

of the level three roof; new stucco rainscreen cladding at the level three mass; two new fabric awnings over basement level doors at the east façade; removal of an original chimney and an elevator overrun on the north side of the roof and construction of new stair overruns at the location of the original chimneys; replacement of all windows on the north (non-street facing) façade on levels one through three; a new basement level service entry on the north façade, and modification of the primary arched windows on the east and west level three facades to incorporate a person door within the existing opening.

While there are numerous elements proposed to be altered on the existing Landmark, the scope of the proposed changes is modest in proportion to the scale of the existing building, its many years of deferred maintenance, and the complexity of adaptively reusing a building designed for such a different use. The alterations are carefully located to minimize visibility from the street level, while maintaining the character-defining features on three street-facing facades, and facilitating spill out activation spaces that will add vibrancy to the public realm.

#### New Hotel

The new hotel incorporates numerous elements of nearby, quality commercial and mixed-use buildings. These include the material palette that comprises of operable storefront windows and French doors overlooking the street; stucco detailing and material changes that differentiate the top, middle and bottom of the building; custom artistic balustrades, and fabric awnings providing pedestrian weather protection at every storefront bay. The proposed hotel design includes fine-grain aesthetic detailing, particularly at the bottom two levels, that are reminiscent of many of the nearby contributing resources.

The new building's massing is proposed to be built to the property line on both of its street facing frontages, while it is set back from both the contributing resource to its east and the Landmark building to the south. This differential setback respects the existing context while also responding to the neighborhood's desired character. The active ground level program at the sidewalk frontage serves to enliven the sidewalk environment, providing views of interior activity, new lighting, visual interest and opportunity for human interaction to the pedestrian realm.

The proposed hotel massing, architectural style, material palette and ground floor programming are an excellent model of respectful infill development in this historic district.

Therefore, these guidelines are met.

**D8. Interest, Quality, and Composition.** All parts of a building should be interesting to view, of long-lasting quality, and designed to form a cohesive composition.

**Findings for D8:** The applicant has provided numerous building details and cutsheets to reflect that the proposal is well-detailed, and utilizes materials that will both reflect the high-quality historic architecture of the surrounding context while ensuring a durable, long-lasting contribution to the city.

The new hotel is proposed to be primarily a triple-coat, field applied stucco rainscreen, with integrated stucco detailing. While pre-fabricated stucco panels are not always durable in our climate, high-quality, field applied stucco has been utilized in Portland for over a century, and has held up well through the temperature and weather extremes that are typical in our climate. The thick walls and applied stucco mouldings around the hotel windows foster the deeply recessed window punch typical of historic buildings

and consistent with buildings of this style. A wall section demonstrates that the punch from the face of the exterior wall to the face of the window sash will be approximately six-inches deep, and will appear deeper due to the additional depth created by the stucco window mouldings. At level two, where bays are framed with additional stucco detailing, the transom windows and operable French doors are recessed one-foot two-inches from the face of the exterior surrounds. The deep recess of the French doors, creates an elegant condition that draws attention to the rich detailing at this level, including custom aluminum balusters at each French door bay, designed to relate to the unique muntin pattern of the Landmark's leaded windows.

At the ground level, the applicant proposes to recess the storefront bays between pilasters by approximately two feet. A granite stone sill beneath the storefront bays matches the granite stone base of each pilaster, and serves to protect the storefront bays from moisture from the sidewalk. Beneath the storefront windows, the applicant proposes painted wood paneling. While wood is not always recommended at the ground level, in this situation it is proposed in a location and design that are consistent with many nearby commercial storefronts that have demonstrated durability for more than a century. To ensure resilience, the applicant has proposed red oak panels, with a paint finish. The stone sill below, the two-foot-deep recessed condition between pilasters, and the fixed overhead awnings at each storefront bay serve to minimize the wood's weather exposure as well as its exposure to pedestrian impacts. The limited deployment of the wood elements and their accessibility to the sidewalk ensure ease of maintenance access, should repair or repainting be necessary.

The hotel's primary street-facing corner, at the intersection of NW 19<sup>th</sup> Avenue and NW Flanders, is accentuated with curved stucco and baluster detailing and the main entrance located at the chamfered corner. The entrance is highlighted with a unique, custom aluminum and glass canopy that provides deep coverage while also reinforcing the curved detailing of upper levels at the pedestrian realm.

The details provided demonstrate careful consideration of the materials proposed and that they are applied in a manner that ensures their durability and facilitates ease of access for maintenance and repair. The deep punches and rich detailing are consistent with the historic masonry aesthetic and with the surrounding historic context. The Commission highlighted a single concern, that the proposed transom above the storefront windows did not carry over the entry doors. This is atypical in the District, where transom window bars typically continue above all the windows and doors within the bay. Therefore, the Commission added a condition of approval that at each ground floor storefront bay, the transom bar shall extend across the entire opening width, with a consistent set of transom windows above all storefront windows and entry doors. With this condition, the proposed design strikes the appropriate balance between being an attractive stand-alone contribution to the Alphabet Historic District while also being a deferential neighbor to the existing adjacent Landmark.

Therefore, with the condition of approval that the ground level transom bar shall extend across the entire opening width, with a consistent set of transom windows above all storefront windows and entry doors, this guideline is met.

#### 33.846.060 G - Other Historic Approval Criteria

**1. Historic character.** The historic character of the landmark or contributing resource will be retained and preserved. Removal of historic materials or alteration of features and spaces that contribute to the historic significance of the landmark or contributing resource will be avoided.

- **2. Record of its time.** The landmark or contributing resource will remain a physical record of its time, place, and use. Changes that create a false sense of historic development, such as adding conjectural features or architectural elements from other buildings will be avoided.
- **3. Historic changes.** Most resources change over time. Those changes that have acquired historic significance will be preserved.
- **4. Historic features.** Generally, deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement, the new feature will match the historic feature in design, color, texture, and other visual qualities and, where practical, in materials. Replacement of missing features must be substantiated by documentary, physical, or pictorial evidence.
- **5. Historic materials.** Historic materials will be protected. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials will not be used.
- **7. Differentiate new from old.** New additions, exterior alterations, or related new construction will not destroy historic materials that characterize a landmark or contributing resource. New work may be differentiated from the old if the differentiation does not diminish the character, features, materials, form, or integrity of the landmark or contributing resource and, if in a Historic District, the district as a whole.
- **8. Architectural compatibility.** New additions, exterior alterations, or new construction will be compatible with the massing, size, scale, and architectural features of the landmark or contributing resource and, if in a district, the district as a whole. When retrofitting to improve accessibility for persons with disabilities or accommodate seismic improvements, design solutions will not compromise the architectural integrity of the landmark or contributing resource.
- **9. Preserve the form and integrity of historic resources.** New additions, exterior alterations, or new construction will be undertaken in such a manner that if removed in the future, the essential form and integrity of the landmark or contributing resource and, if in a district, the district as a whole would be unimpaired.
- **10. Hierarchy of compatibility.** New additions, exterior alterations, or new construction will be designed to be compatible primarily with the landmark or contributing resource and, if located within a district, secondarily with contributing resources within 200 feet and, finally, with the rest of the district. Where practical, compatibility in districts will be pursued on all three levels.
  - **Findings for 1, 2, 3, 4, 5, 7, 8, 9, and 10:** The existing First Church of Christ Scientist, an individually-listed Historic Landmark is being retained and preserved. Its character-defining leaded windows and domed roof are being retained and restored, and it will continue to serve as a record of its time. The proposed exterior alterations are minor elements intended to allow the adaptive reuse of a challenging structure, and are mostly limited to the north façade, which is the building's only non-street facing façade. Extensive restoration and repair work is proposed to the existing windows, the cut stone cladding and the roofing.

Due to building code requirements, activation of the flat rooftops to create terraces with outdoor seating required the introduction of multiple new egress doors. The applicant proposes modest new stair and elevator overruns tucked toward the center of the rear façade, furthest from the street-facing facades. These additions are proposed to be clad in stucco, to complement the color of the cut stone cladding but to differentiate these minor service elements from the original architecture.

Additionally, a secondary egress is needed from both the east and west side of the roof in order to facilitate the rooftop conversion to publicly accessible outdoor space. At staff's direction, the applicant explored multiple locations for this additional door, including replacing south-facing windows with doors, or modifying the tallest windows on level three of the east and west facades. With the goal of limiting the introduction of new regulating lines caused by the door frame, the applicant is proposing to insert a

custom ten-foot door into the center of both of these windows. At that height, the door will infill the entire height of the existing window opening, eliminating the need for an infill panel above the door. The original window glazing will be modified and retained on either side of the door to maintain the character-defining leaded window pattern. To ensure that this modification may be reversed in the future, the Commission added a condition of approval that the existing center window bay on the east and west level three windows shall be carefully removed and retained on site in preparation for installation of new egress doors.

At the Design Advice Request, a Commissioner suggested the applicant explore opportunities to utilize the site's slope to introduce a new ADA ramp to the front door. The applicant provided explorative diagrams of various ADA access schemes. However, the investigation ultimately revealed that due to an eight-inch step at the main entrance door, and the location of existing historic columns, a ramp at this location does not meet the requirements for ADA turning radius and ramp width, and meeting the requirements would require fundamentally changing the design of the original entrance. Therefore, the applicant is proposing accessible entrances off both the east and the west facades. An accessible path through the new east courtyard provides access from NW 18th Avenue to the proposed basement level coffee shop at this frontage. At the west entrance, located at the north end of the west façade, the ramped entry will bring users into the main level, and doubles as a primary entrance for guests arriving via the hotel or those accessing the new proposed spill-out garden seating on the west side of the hotel. While it would be more equitable to ensure all guests are able to arrive via the same entrance, the design, location and programming of the west ADA entrance demonstrate that it will be a primary entrance for users of all abilities and that guests arriving via these doors will not be navigating back of house conditions.

Therefore, with the Condition of Approval that the center window bay on the level three east and west windows shall be carefully removed and retained on site in preparation for the installation of new egress doors, these criteria are met.

#### 33.846.070 Modifications Considered During Historic Resource Review

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the historic resource review process. These modifications are done as part of historic resource review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through historic resource review may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

- A. **Better meets historic resource review approval criteria.** The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and
- B. Purpose of the standard.
  - 1. The resulting development will meet the purpose of the standard being modified; or
  - 2. The preservation of the character of the historic resource is more important than meeting the purpose of the standard for which a modification has been requested.

Modification #1: Building Height, PZC 33.130.210.B.1 / Table 130-2 / Table 130-3 - increase the allowed building height by ten feet, from the allowed 60-feet to 70-feet above basepoint.

Purpose Statement: The height limits are intended to control the overall scale of buildings. The height limits in the CM2 and CE zones allow for a greater building height at a scale that can accommodate the growth intended for centers and corridors, while relating to the low- to mid-rise scale of neighborhood residential areas. Exceptions to height limit standards accommodate minor projections that do not significantly increase the visual scale of buildings; provide flexibility in the height of parapets and railings to facilitate rooftop outdoor spaces and equipment screening; and accommodate ground-floor spaces with high ceilings to encourage ground-floor commercial uses, mechanical parking, and other uses that benefit from high ceilings.

Standard: 33.130.210.B.1 - The base height standards for all structures, except detached accessory structures, are stated in Table 130-2. Table 130-2 states that the base height in the CM2 zone is 45 feet, and bonus height is in Table 130-3. Table 130-3 states that the maximum height in the CM2 zone with bonus is 55 feet.

Note: the applicant has combined numerous code-allowed height exceptions with allowed bonus heights to achieve an allowable height of 60-feet. These include a 45-foot base height, plus a five-foot high ceilings height exception, plus a ten-foot height bonus for providing an affordable commercial space, for a total of sixty-feet of allowable height. The ten-foot Modification request is in addition to that sixty-feet.

**A.** Better meets historic resource review approval criteria. The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and

**Findings for A:** The modification is to increase the height of the building to allow for occupiable space at the rooftop penthouse level (in addition to the mechanical, stair and elevator overruns that are already allowed by code). The code allows for height exceptions for mechanical and stair/elevator overruns, provided they fall within specific height limits and are set back a minimum of fifteen feet from roof edges on all street facing facades. The proposed occupiable space is set back more than fifteen feet from the street facing roof edges, and meets the ten-foot height limitation allowed for rooftop mechanical, except for where the stair and elevator overrun are located. These elements meet the sixteen-foot height exception in the code. Only the occupiable floor area at the rooftop level necessitates this modification request.

The modification better meets the guidelines D1 – Outdoor Areas and D3 – Landscape Features. The benefits of allowing the penthouse-level interior space are that it facilitates the active use of a rooftop, creating an additional publicly accessible commercial space with a rooftop restaurant, and opportunity for outdoor activity in a vibrant and densifying neighborhood. As the city grows, it is increasingly important to maximize the use of outdoor spaces. Private outdoor space is a privilege many do not have access to, and rooftops are one of the underutilized opportunities for introducing new places for the public to enjoy Portland's mild climate and phenomenal views.

**B.** Purpose of the standard. The resulting development will meet the purpose of the standard being modified.

**Findings for B:** The proposal meets the purpose of the standard because the proposed interior penthouse level space meets the height limits and setbacks required for allowed rooftop mechanical and overruns and does not dramatically alter the building's perceived height or scale, particularly within its immediate context. Sight line diagrams indicate that the mass is set back sufficiently to not be seen from the adjacent rights of way, and is only slightly visible from a block away, where the higher grade allows pedestrians to see the

roofline of the new mass. It will have little impact on the building's perceived scale. Further, immediately across NW 19th Avenue, an existing non-conforming apartment building is over 129-feet tall, with additional mechanical elements extending above that height.

The proposal better meets the approval criteria and the purpose statement for the standard being modified.

Therefore, this Modification merits approval.

Modification #2: Loading Space Size, PZC 33.266.310.D.a – decrease the standard A loading space size from the required 35-feet long, 10-feet wide and 13-foot clearance to 25-feet long, 10-feet wide and 13-foot clearance.

Modification #3: Forward Motion, PZC 33.266.310.F.1 – allow loading space to be accessed and exited in a rearward motion.

*Purpose Statement*: A minimum number of loading spaces are required to ensure adequate areas for loading for larger uses and developments. These regulations ensure that the appearance of loading areas will be consistent with that of parking areas. The regulations ensure that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

*Standard:* 33.266.310.D.a: Standard A: the loading space must be at least 35 feet long, 10 feet wide, and have a clearance of 13 feet.

Standard 33.266.310.F.1: Outside the Central City plan district. Outside the Central City plan district, loading facilities generally must be designed so that vehicles enter and exit the site in a forward motion. Standard B loading spaces that are accessed from a Local Service Traffic Street are exempt from this requirement

**A.** Better meets historic resource review approval criteria. The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and

**Findings for Modification #2.A:** The modification to reduce the length of the proposed loading space better meets approval criteria E1 – The Pedestrian Network and E3 – The Sidewalk Level of Buildings. The modification request is based on loading data collected by the applicant and reviewed by the Portland Bureau of Transportation engineer. The collected data demonstrated that the vehicles utilized to service this size and type of program do not require the 35-foot length of a Standard A space, but are too large to be served by a Standard B space. Therefore, the modified dimensions are proposed to ensure that the space is adequately sized to serve the vehicles that will be accessing the space but that it is not unnecessarily large. The reduced size allows the floor area to be better utilized for other uses accessory to the retail sales and service program. Maximizing the active uses at the ground floor benefits the sidewalk environment and pedestrian realm, facilitating more opportunity for visual interest at the sidewalk level of the building.

**Findings for Modification #3.A:** The modification to reduce the length of the proposed loading space better meets approval criteria E1 – The Pedestrian Network and E3 – The Sidewalk Level of Buildings. In order to meet the code standard requiring a loading space may be entered and exited in a forward motion, the proposal would need to have a one-way driveway wrapping around the building. Neither PBOT nor BDS supported this proposal, as it would have required a large percentage of the site area to be allocated to surface vehicle area, and PBOT determined that having a truck exit in a rearward motion onto NW

Flanders Street will create fewer traffic conflicts than having vehicles exit onto NW 19<sup>th</sup> Avenue near the complex intersection of NW 19<sup>th</sup> Avenue and NW Everett streets.

Minimizing the amount of on-site vehicle area, with a design that internalizes the loading space into the building mass upholds the approval criteria dedicated to fostering an active and vibrant pedestrian realm.

**B. Purpose of the standard.** The resulting development will meet the purpose of the standard being modified.

**Findings for Modifications #2 and #3 B:** The proposal meets the purpose of the standard, since the requested loading space dimension reflects the size of trucks servicing the site, and is derived from data collected specific to this use and size of program. The appropriately sized loading space ensures that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

Allowing vehicles accessing the loading space to enter or exit in a rearward motion has been deemed the safest option, and will result in fewer conflicts with other vehicles, bicyclists and pedestrians than if the standard were met with a one-way driveway around the site.

The proposal better meets the approval criteria and meets the purpose statement of the standard proposed to be modified.

Therefore, these Modifications merit approval.

Modification #4: Short Term Bike Parking Locations, PZC 33.266.210.E.1.a.(3)—increase the maximum distance that short-term bike parking may be located from a main entrance, from 50 feet to 65 feet.

Purpose 33.266.210: These standards ensure that required bicycle parking is designed so people of all ages and abilities can access the bicycle parking and securely lock their bicycle without undue inconvenience. Bicycle parking is in areas that are reasonably safeguarded from theft and accidental damage. The standards allow for a variety of bicycle types, including but not limited to standard bicycles, tricycles, hand cycles, tandems, electric motor assisted cycles and cargo bicycles. Long-term bicycle parking is in secure, weather protected facilities and is intended for building and site occupants, and others who need bicycle parking for several hours or longer. Short-term bicycle parking is located in publicly accessible, highly visible locations that serve the main entrance of a building. Short-term bicycle parking is visible to pedestrians and bicyclists on the street and is intended for building and site visitors.

**Standard 33.266.210.E.1.a** - For a building with more than one main entrance, the bicycle parking must be along all façades with a main entrance, and within 50 feet of at least one main entrance on each façade that has a main entrance, as measured along the most direct pedestrian access route.

**A.** Better meets historic resource review approval criteria. The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and

**Findings for A:** The proposal to increase the maximum distance allowed between the required short-term bike parking and the main entrances on both buildings better meets historic resource review approval criteria E1 – The Pedestrian Network. Because the new

hotel is built to the property line on both street frontages and does not have any on-site space within fifty feet of the main entrances where short term bike parking could meet the locational standards, the applicant had the option to buy into the short-term bike parking fund. This would have eliminated the requirement to provide any short-term bike parking on site, however there is no guarantee that PBOT will be able to use the associated fees to locate bike parking in the adjacent right-of-way. The applicant desires to provide convenient, on-site bike parking accessible to each of their retail sales and service uses, despite not all the short-term bike parking being located within the maximum 50-foot distance from a main entrance.

This solution better meets the approval criteria, as the resulting proposal cumulatively provides all the required short-term bike parking spaces but separates them into three distinct locations, located off three separate street frontages. The site has four street frontages, and while locating all the bike parking in one location may have met the code standard, it would not have resulted in convenient, findable bike parking facilities for all guests to the site. The dispersed bike parking proposal is more efficient, pleasant and safe, as it locates bike parking in convenient areas that are adjacent to windows, sidewalks and other active spaces where many people can see into the bike parking areas.

**B.** Purpose of the standard. The resulting development will meet the purpose of the standard being modified;

**Findings for B:** The proposed short-term bike parking proposal is designed so people of all ages and abilities can access the bike parking. It is conveniently located adjacent to three of the site's four frontages. The locations are highly visible, close to primary entrances to many of the retail sales and service uses in both buildings.

The proposal better meets the approval criteria and meets the purpose statement of the standard proposed to be modified.

Therefore, this Modification merits approval.

#### **CONCLUSIONS**

The purpose of the Historic Resource Review process is to ensure that additions, new construction, and exterior alterations to historic resources do not compromise their ability to convey historic significance. This proposal respectfully converts an existing Landmark church building into a complicated program of multiple new retail sales and service uses. It will adaptively reuse the existing building to house numerous active uses, while prioritizing minimal exterior alterations. The new hotel is designed to complement both the adjacent Landmark and the surrounding historic district context. It models an excellent ground level and maximizes opportunities for activation adjacent to the streets. However, the proposal lacks necessary details to determine quality and resilience guidelines. Therefore, it does not yet warrant approval.

#### LANDMARKS COMMISSION DECISION

It is the decision of the Landmarks Commission to approve of the restoration of an existing Historic Landmark to a private club with rooftop dining and retail sales and service uses, and the construction of a new six-story hotel within the Landmark boundary, and approval of the following Modification requests:

- 1. Increase the allowed building height by ten feet, from the allowed 60-feet to 70-feet above basepoint (PZC 33.130.210.B.1 / Table 130-2 / Table 130-3);
- 2. Decrease the standard A loading space size from the required 35-feet long, 10-feet wide and 13-foot clearance to 25-feet long, 10-feet wide and 13-foot clearance (PZC 33.266.310.D.a);
- 3. Allow loading space to be accessed and exited in a rearward motion (PZC 33.266.310.F.1); and
- 4. Increase the maximum distance that short-term bike parking may be located from a main entrance, from 50 feet to 65 feet (PZC 33.266.210.E.1.a.(3)).

Approvals per Exhibits C.1-C-74, signed, stamped, and dated March 24, 2023, subject to the following conditions:

- A. As part of the building permit application submittal, the following development-related conditions (B through C) must be noted on each of the 4 required site plans or included as a sheet in the numbered set of plans. The sheet on which this information appears must be labeled "ZONING COMPLIANCE PAGE Case File LU 22-204531 HRM AD". All requirements must be graphically represented on the site plan, landscape, or other required plan and must be labeled "REQUIRED."
- B. At the time of building permit submittal, a signed Certificate of Compliance form (https://www.portlandoregon.gov/bds/article/623658) must be submitted to ensure the permit plans comply with the Design/Historic Resource Review decision and approved exhibits.
- C. At the east and west third floor of the Landmark, the level three existing center window bays shall be carefully removed and retained on site in preparation for installation of new egress doors.
- D. At each ground floor storefront bay, the transom bar shall extend across the entire opening width, with a consistent set of transom windows above all storefront windows and entry doors.

E. No field changes allowed.							
By:							
And	drew Smith, Landmarks Commission Chair						

Application Filed: November 16, 2022 Decision Filed: March 14, 2023 Decision Filed: March 14, 2023 Decision Mailed: March 28, 2023

**About this Decision.** This land use decision is **not a permit** for development. Permits may be required prior to any work. Contact the Development Services Center at 503-823-7310 for information about permits.

**Procedural Information.** The application for this land use review was submitted on November 16, 2022, and was determined to be complete on January 23, 2023.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the

application is complete at the time of submittal, or complete within 180 days. Therefore, this application was reviewed against the Zoning Code in effect on November 16, 2022.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant waived the 120-day review period, as stated with Exhibit A.2. The **120 days expire on:** January 23, 2024

**Some of the information contained in this report was provided by the applicant.** As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. This report is the final decision of the Landmarks Commission with input from other City and public agencies.

**Conditions of Approval.** This approval may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term "applicant" includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

**Appeal of this decision.** This decision is final unless appealed to City Council, who will hold a public hearing. Appeals must be filed by 4:30 pm on March 27, 2023. The appeal application form can be accessed at <a href="https://www.portlandoregon.gov/bds/45477">https://www.portlandoregon.gov/bds/45477</a>. The completed appeal application form must be e-mailed to <a href="mailto:BDSLUSTeamTech@portlandoregon.gov">BDSLUSTeamTech@portlandoregon.gov</a> and to the planner listed on the first page of this decision. If you do not have access to e-mail, please telephone the planner listed on the front page of this notice about submitting the appeal application.

If you are interested in viewing information in the file, please contact the planner listed on the front of this decision. The planner can provide some information over the phone. Please note only digital copies of material in the file are available for viewing. Additional information about the City of Portland, city bureaus, and a digital copy of the Portland Zoning Code is available on the internet at <a href="https://www.portlandoregon.gov/citycode/28197">https://www.portlandoregon.gov/citycode/28197</a>.

If this decision is appealed, a hearing will be scheduled and you will be notified of the date and time of the hearing. The decision of City Council is final; any further appeal is to the Oregon Land Use Board of Appeals (LUBA).

Upon submission of their application, the applicant for this land use review chose to waive the 120-day time frame in which the City must render a decision. This additional time allows for any appeal of this proposal to be held as an evidentiary hearing, one in which new evidence can be submitted to City Council.

**Who can appeal:** You may appeal the decision only if you have written a letter which was received before the close of the record at the hearing or if you testified at the hearing, or if you are the property owner or applicant. Appeals must be filed within 14 days of the decision. **An appeal fee of \$5,789.00 will be charged.** 

Neighborhood associations may qualify for a waiver of the appeal fee. Additional information on how to file and the deadline for filing an appeal will be included with the decision. Assistance in filing the appeal and information on fee waivers are available from the Bureau of Development Services website: <a href="https://www.portlandoregon.gov/bds/article/411635">https://www.portlandoregon.gov/bds/article/411635</a>. Fee waivers for neighborhood associations require a vote of the authorized body of your association. Please see appeal form for additional information.

#### Recording the final decision.

If this Land Use Review is approved the final decision will be recorded with the Multnomah County Recorder.

• *Unless appealed*, the final decision will be recorded after **March 29, 2023** by the Bureau of Development Services.

The applicant, builder, or a representative does not need to record the final decision with the Multnomah County Recorder.

For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

**Expiration of this approval.** An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Zone Change and Comprehensive Plan Map Amendment approvals do not expire.

**Applying for your permits.** A building permit, occupancy permit, or development permit must be obtained before carrying out this project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed here.
- All applicable development standards, unless specifically exempted as part of this land use review.
- All requirements of the building code.
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

Hannah Bryant March 20, 20238

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

#### **EXHIBITS** – NOT ATTACHED UNLESS INICATED

- A. Applicant's Submittals
  - 1. Original Submittal
  - 2. Full Extension, November 28, 2022
  - 3. Rooftop Egress Door Options, January 13, 2023
  - 4. Request from Applicant to deem complete, January 19, 2023

- 5. Response to Incomplete, January 23, 2023
- 6. Final Submittal, dated February 21, 2023
- 7. Revised Final Submittal, dated March 8, 2023
- B. Zoning Map (attached)
- C. Plan & Drawings
  - 1. Not used
  - 2. Site Plan (Landmark) (attached)
  - 3. Basement Floor Plan (Landmark)
  - 4. First Floor Plan (Landmark)
  - 5. Second Floor Plan (Landmark)
  - 6. Third Floor Plan (Landmark)
  - 7. Roof Plan (Landmark)
  - 8. Black and White South Elevation (Landmark)
  - 9. Black and White West Elevation (Landmark)
  - 10. Black and White East Elevation (Landmark)
  - 11. Black and White North Elevation (Landmark)
  - 12. Color South Elevation (Landmark)
  - 13. Color West Elevation (Landmark)
  - 14. Color East Elevation (Landmark)
  - 15. Color North Elevation (Landmark)
  - 16. Demolition South Elevations (Landmark)
  - 17. Demolition West Elevations (Landmark)
  - 18. Demolition East Elevations (Landmark)
  - 19. Demolition North Elevations (Landmark)
  - 20. Existing Conditions (Landmark)
  - 21. Details new door at Landmark level 3 (Landmark)
  - 22. Rooftop addition Landmark level 3 (Landmark)
  - 23. Window Awning Details
  - 24. Exterior Materials
  - 25. Window Repair and Replacement Scope (Landmark)
  - 26. Not used
  - 27. Site Plan (New Hotel)
  - 28. Floor Plans (New Hotel)
  - 29. Floor Plans (New Hotel)
  - 30. Black and White West Elevations (New Hotel)
  - 31. Black and White North Elevation (New Hotel)
  - 32. Black and White South Elevation (New Hotel)
  - 33. Black and White East Elevation (New Hotel)
  - 34. Color West Elevations (New Hotel)
  - 35. Color North Elevation (New Hotel)
  - 36. Color South Elevation (New Hotel)
  - 37. Color East Elevation (New Hotel)
  - 38. Details Storefront (New Hotel)
  - 39. Details Awning and Baluster (New Hotel)
  - 40. Details Wall Section
  - 41. Details Penthouse Wall Section and Awning
  - 42. Details Main Entrance Canopy
  - 43. Affordable Commercial Space
  - 44. Exterior Materials
  - 45. FAR Summary
  - 46. Short Term Bike Parking
  - 47. Long Term Bike Parking
  - 48. Ground Floor Windows (New Hotel)
  - 49. Height Diagram (New Hotel)
  - 50. Window and Door Cutsheets

- 51. Oasis Multifold Door
- 52. Oasis Multifold Door
- 53. French Door
- 54. French Door
- 55. Garage Door Cutsheet
- 56. Stucco Cutsheet
- 57. Mechanical Screen & Exterior Lighting
- 58. Glass Railing Cutsheet
- 59. Rooftop Mechanical Cutsheet
- 60. Rooftop Mechanical Cutsheet
- 61. Landscape Tree Plan
- 62. Landscape Site Plan
- 63. Landscape Roof Plan
- 64. Landscape Planting Plan
- 65. Landscape Planting Plan Roof Decks
- 66. Landscape Planting Sections
- 67. Existing Civil Plan (New Hotel)
- 68. Existing Civil Plan (Landmark)
- 69. Civil Plan (Hotel)
- 70. Civil Plan (Landmark)
- 71. Utility Plan (Hotel)
- 72. Utility Plan (Landmark)
- 73. Stormwater Plan (Hotel)
- 74. Stormwater Plan (Landmark)
- D. Notification information:
  - 1. Request for response
  - 2. Posting letter sent to applicant
  - 3. Notice to be posted
  - 4. Applicant's statement certifying posting
  - 5. Mailed notice
  - 6. Mailing list
- E. Agency Responses:
  - 1. Bureau of Environmental Services
  - 2. Bureau of Transportation Engineering and Development Review
  - 3. Water Bureau
  - 4. Fire Bureau
  - 5. Bureau of Parks, Forestry Division
  - 6. Site Development Review Section of BDS
- F. Letters
  - 1. Shannon and David Lea, February 24, 2023, neighbors support the proposal, including the height. Some concerns about on-street parking impacts.
  - 2. Dennis Harper, February 25, 2023, neighbor has some concern about the proposed fence and lack of landscaping details. Supports the rooftop canopy design proposed for the penthouse of the new hotel.
  - 3. Steve Pinger (NWDA), February 23, 2023. The NWDA strongly supports the project concept, including the renovation and repurposing of the existing landmark building, and the proposed new hotel building on the adjacent parcel.
  - 4. Charisse Tooze, March 6, 2023, neighbor has concerns about the impacts of the new development on on-street parking availability.
- G. Other
  - 1. Original LUR Application
  - 2. Incomplete Letter, December 7, 2022
- H. Hearing
  - 1. Staff Report #1, dated March 3, 2023
  - 2. Staff Report # 2, dated March 10, 2023

- 3. Staff Presentation
- 4. Staff Memo to Commission
- 5. Applicant Presentation

cc: Applicants and Representatives
Neighborhood Associations
Those who testified, orally or in writing
City Auditor's Office

## 204531\_22\_LU\_3FFDEC

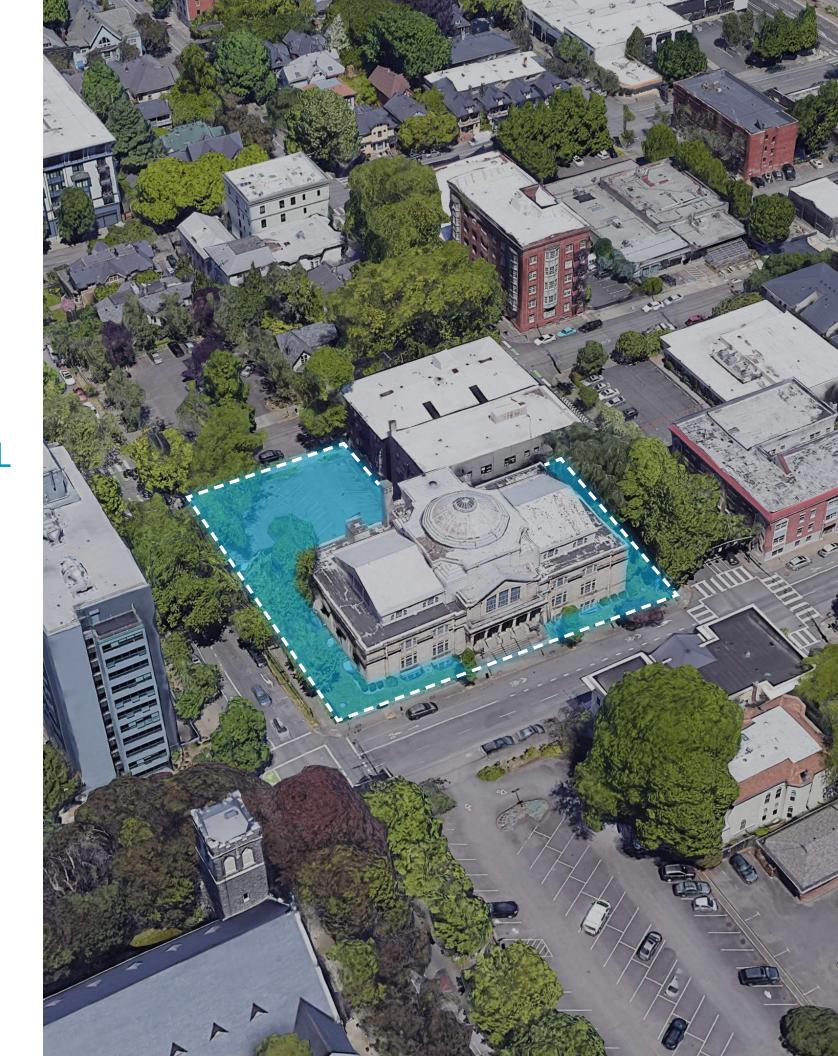
	А	В	С	D	E	F
1	ENDORSEMENT	INFO1	INFO2	NAME	ADDRESS/IO ADDRESS	CITYSTATEZIP/ADDRESSEE
2	RETURN SERVICE REQUESTED	RESPONDENT	1N1E33DB 60006	TOOZE CHARISSE M	410 NW 18TH AVE #104	PORTLAND OR 97209
3	RETURN SERVICE REQUESTED	RESPONDENT	1N1E33DB 90004	HARPER DENNIS M	221 NW 18TH AVE	PORTLAND OR 97209-2111
4	RETURN SERVICE REQUESTED	RESPONDENT	1N1E33DB 60015	LEA DAVID & LEA SHANNON	10330 HYLA AVE NE	BAINBRIDGE ISLAND WA 98110
5	RETURN SERVICE REQUESTED	OWNER	1N1E33DB 2800	NW NEIGHBORHOOD CULTURAL CENTER	PO BOX 96116	PORTLAND OR 97296
6	RETURN SERVICE REQUESTED	APPLICANT	HARTSHORNE PLUNKARD ARCHITECTURE	BECKER ANDREW	315 W WALTON ST	CHICAGO IL 60610
7	RETURN SERVICE REQUESTED	OWNERS AGENT	GAIA VENTURES I LLC	TOBY TANYA	5821 SE POWELL BLVD	PORTLAND OR 97206
8	RETURN SERVICE REQUESTED		NEIGHBORS WEST-NORTHWEST	GARRETT DARLENE URBAN	2257 NW RALEIGH ST	PORTLAND OR 97210
9	RETURN SERVICE REQUESTED		LAND USE CONTACT	NORTHWEST PORTLAND BA	25 NW 23RD AVE #6-PMB 217	PORTLAND OR 97210
10	RETURN SERVICE REQUESTED		NORTHWEST DISTRICT ASSOCIATION	THEISEN GREG AND PINGER STEVE	2257 NW RALEIGH ST	PORTLAND OR 97210
11	RETURN SERVICE REQUESTED		LAND USE CONTACT	PEARL DISTRICT BA	PO BOX 6767	PORTLAND OR 97228
12	RETURN SERVICE REQUESTED		PEARL DISTRICT NA	FARHOODI REZA & DYSERT DAVID	2257 NW RALEIGH STREET	PORTLAND OR 97210
13	RETURN SERVICE REQUESTED		PORTLAND SCHOOL DISTRICT	LAND USE NOTICE CONTACT	501 N DIXON	PORTLAND OR 97227
14	RETURN SERVICE REQUESTED	LAND USE CONTACT	STADIUM DISTRICT BUSINESS GROUP	C/O VENTURE PORTLAND	1125 SE MADISON ST #112	PORTLAND OR 97214
15	RETURN SERVICE REQUESTED		LAND USE CONTACT	CENTRAL CITY CONCERN	232 NW 6TH AVE	PORTLAND OR 97209
16	RETURN SERVICE REQUESTED		LAND USE CONTACT	GOOSE HOLLOW BUSINESS ASSOCIATION	PO BOX 915	PORTLAND OR 97207
17	RETURN SERVICE REQUESTED		LAND USE CONTACT	GOOSE HOLLOW FOOTHILLS LEAGUE	2257 NW RALEIGH STREET	PORTLAND OR 97210
18	RETURN SERVICE REQUESTED			JUDY PETERS	6916 NE 40TH ST	VANCOUVER WA 98661
	RETURN SERVICE REQUESTED		LAND USE CONTACT	STATE HISTORIC PRESERVATION OFFICE	725 SUMMER NE #C	SALEM OR 97301
	RETURN SERVICE REQUESTED		PORTLAND METRO REGIONAL SOLUTIONS	C/O DLCD REGIONAL REPRESENTATIVE	1600 SW FOURTH AVE #109	PORTLAND OR 97201
	RETURN SERVICE REQUESTED		LAND USE CONTACT	PORT OF PORTLAND PLANNING	PO BOX 3529	PORTLAND OR 97208
	RETURN SERVICE REQUESTED		LAND USE CONTACT	TRANSIT DEVELOPMENT	1800 SW FIRST AVE #300	PORTLAND OR 97201
	RETURN SERVICE REQUESTED			KARLA MOORE-LOVE (CITY HALL)	1221 SW 4TH AVE #130	PORTLAND OR 97204
24	RETURN SERVICE REQUESTED		RISK & LAND DEPARTMENT	NW NATURAL	250 SW TAYLOR ST	PORTLAND OR 97204-3038
	RETURN SERVICE REQUESTED		LAND USE CONTACT	PACIFIC POWER & LIGHT	7544 NE 33RD DR	PORTLAND OR 97211
26				LAND USE CONTACT	PROSPER PORTLAND	129/PROSPER
27					HEARINGS CLERK	299/3100
28					DAWN KRANTZ	B299/R5000

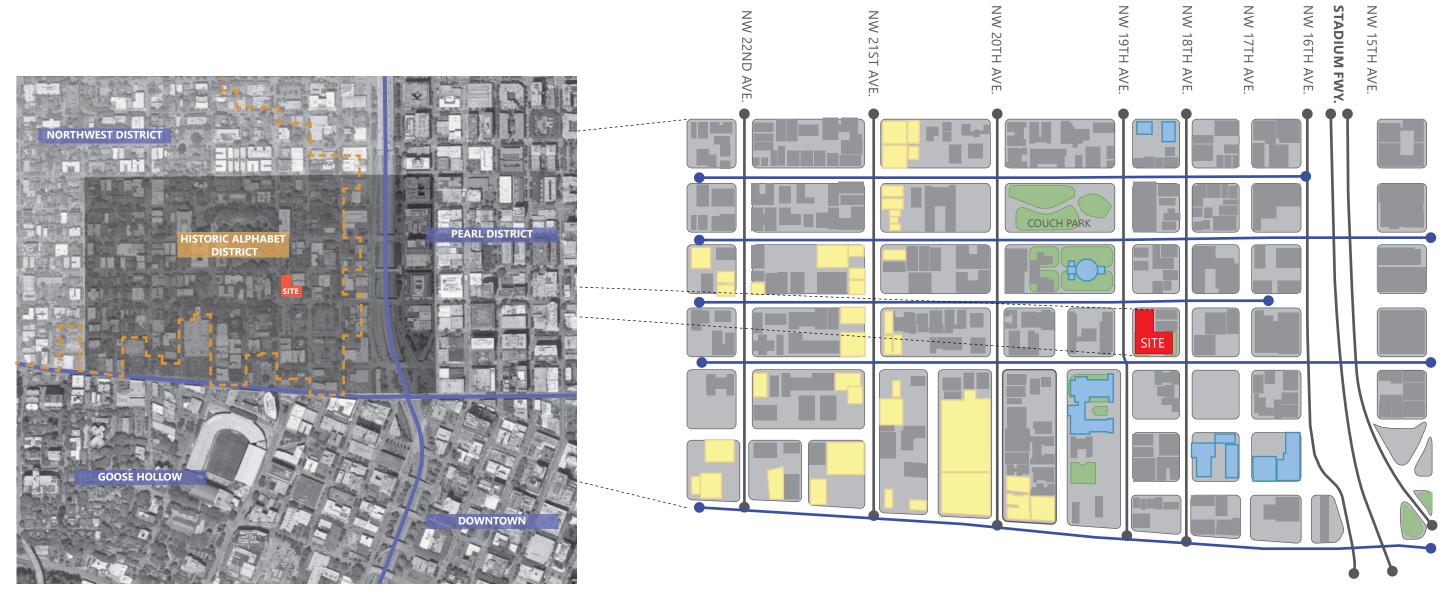
## TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

1819 NW EVERETT STREET 1880 NW FLANDERS STREET

Appendix - Supporting Information

March 13th, 2023





DISTRICT CONNECTION LANDMARK DIAGRAM

> RETAIL RELIGIOUS STRUCTURE GREENSCAPE

AREA CONTEXT











### **URBAN CONTEXT**









### **BUILDING TYPES**



BETH ISRAEL SYNAGOGUE - 1972 NW FI



239 NW 20TH



239 NW 20TH



TRINITY EPISCOPAL CATHEDRAL - 147 NW 19TH



THE BILTMORE - 2014 NW GLISAN ST.

## **DETAILS**



ROUNDED BAY



METAL DETAILING & RAILINGS















## **MATERIALS**



2030 NW FLANDERS



2068 NW FLANDERS



1730 NW COUCH



2105 NW FLANDERS





2120 NW FLANDERS

STUCCO FACADES

**CONTEXTUAL ANALYSIS** 



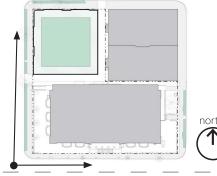








3D PERSPECTIVE FROM THE INTERSECTION OF NW EVERETT ST. AND NW 19TH AVE.



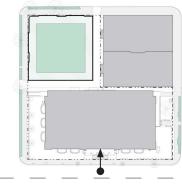
## 3D PERSPECTIVES (1819 NW EVERETT ST.)













3D PERSPECTIVE FROM NW EVERETT ST. LOOKING NORTH

## 3D PERSPECTIVES (1819 NW EVERETT ST.)











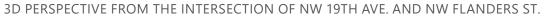
3D PERSPECTIVE FROM THE INTERSECTION OF NW 19TH AVE. AND NW FLANDERS ST.

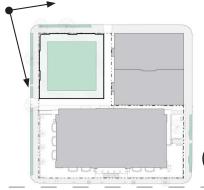
## 3D PERSPECTIVES (1880 NW FLANDERS ST.)











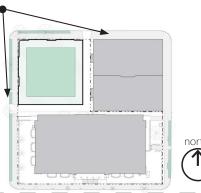








3D PERSPECTIVE - AERIAL VIEW OF PENTHOUSE LOOKING SOUTHEAST





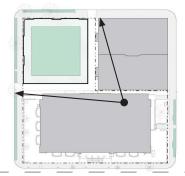








3D PERSPECTIVE - AERIAL VIEW OF PENTHOUSE LOOKING NORTHWEST



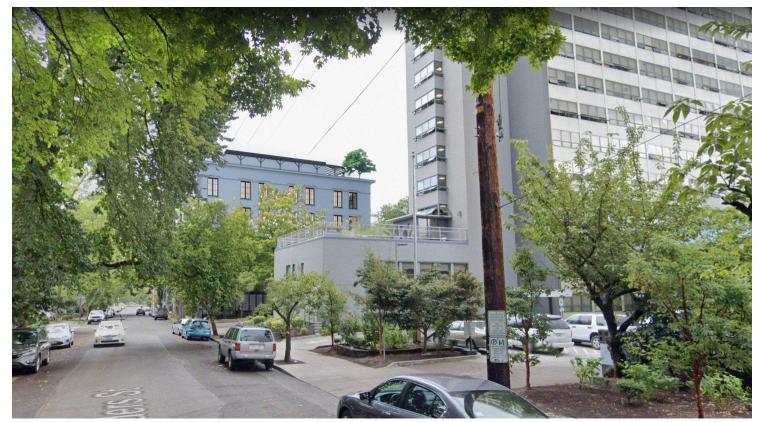


3D PERSPECTIVES (1880 NW FLANDERS ST.)









STREET PERSPECTIVE 1 - LOOKING EAST ON FLANDERS ST.



STREET PERSPECTIVE 3 - LOOKING NORTH ON 19TH AVE.

## 3D PERSPECTIVES (1880 NW FLANDERS ST.)

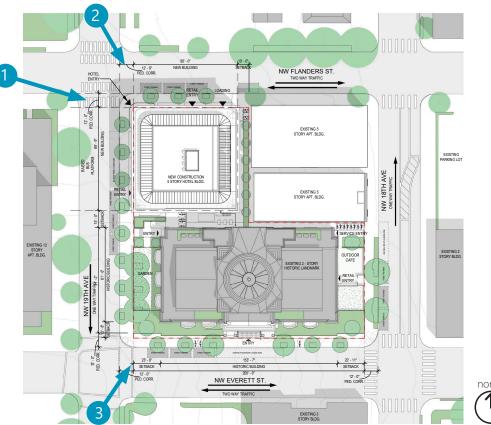








STREET PERSPECTIVE 2 - LOOKING SOUTH ON 19TH AVE.













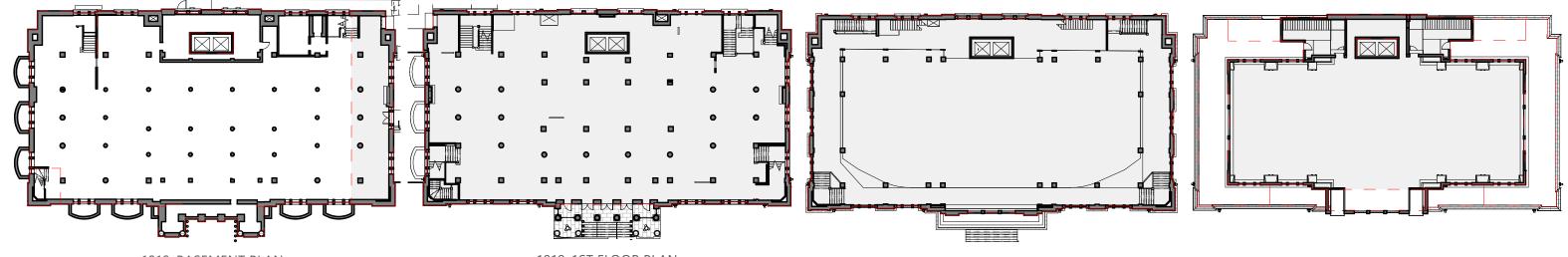


PENTHOUSE CONCEPT IMAGES (1880 NW FLANDERS ST.)









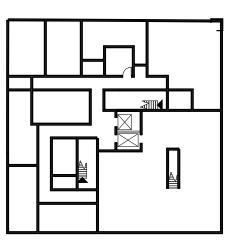
1819: BASEMENT PLAN 1,116SF

\*AREA TO WEST OF LINE, +4'-0" BELOW GRADE

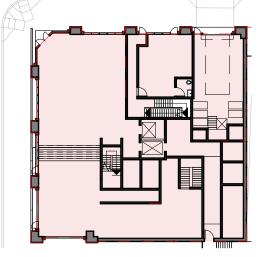


1819: 2ND FLOOR PLAN 10,548SF

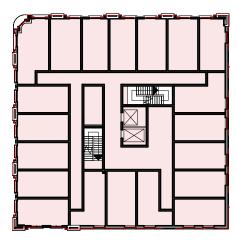
1819: 3RD FLOOR PLAN 6,520SF



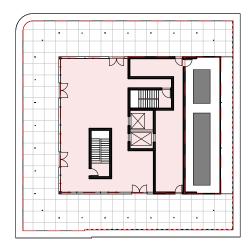




1880: 1ST FLOOR PLAN 7,580 SF



1880: 2ND - 5TH FLOOR PLAN 7,780 SF



1880: ROOF PLAN 2,700 SF

#### **AREA SUMMARIES**

1819 NW FLANDERS (HISTORIC)							
FAR SUMMARY: APPLICABLE AREA							
BASEMENT	1,401 SF						
1ST FLOOR	12,132 SF						
2ND FLOOR	12,205 SF						
3RD FLOOR	7,692 SF						
TOTAL	33,431 SF						

1880 NW FLANDERS (HOTEL)					
FAR SUMMARY: APPLICABLE AREA					
BASEMENT	N/A				
1ST FLOOR	7,580 SF				
2ND FLOOR	7,780 SF				
3RD FLOOR	7,780 SF				
4TH FLOOR	7,780 SF				
5TH FLOOR	7,780 SF				
ROOF	2,700 SF				
TOTAL	41,400 SF				

FAR G	RAND	TOTALS	5
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29,997 SF (SITE AREA) X 2.5 = 74,992.5 SF (ALLOWABLE FAR)

TOTAL SF: (33,431 + 41,400) = 74,831 SF (TOTAL PROPOSED FAR)

74,831 SF < 74,992 SF

FAR SUMMARY

| SCALE: 1" = 40'-0"











### NEW BUILDING 12' - 0" NW FLANDERS ST. HOTEL TWO WAY TRAFFIC **ENTRY** EXISTING 5 STORY APT. BLDG. NW 18TH AVE NEW CONSTRUCTION EXISTING 5 STORY APT. BLDG. **\$7\$7\$7\$7** ENTRY ) SERVICE ENTR OOR EXISTING 2 - STORY HISTORIC LANDMARK RETAIL N NW 19TH AVE ONE WAY TRAFFIG. . 2" BIKE PARKING BIKE PARKING 153' - 7" 23' - 6" 22' - 11" 12'-L 9ED. C SETBACK HISTORIC BUILDING SETBACK \_12' - 0" 200' - 0" 12' - 0' PED. CORR. NW EVERETT ST. PED. CORR TWO WAY TRAFFIC

SITE PLAN - BIKE PARKING: LOCATIONS & DISTANCE TO ENTRIES

#### **1819 NW EVERETT STREET**

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

#### **RESTAURANT & BAR: 4,649 SF**

#### SHORT TERM

- REQUIREMENTS: 2, OR 1 PER 2,300 SF
- 2 SPACES

#### **RETAIL SALES AND SERVICES: 30,658 SF SHORT TERM**

- REQUIREMENTS: 2, OR 1 PER 3,800SF
- 8 SPACES

#### SHORT TERM

**1880 NW FLANDERS STREET** 

**RESTAURANT & BAR: 7,034** 

- REQUIREMENTS: 2, OR 1 PER 2,300 SF
- 4 SPACES

#### **RETAIL SALES AND SERVICES: 4,815 SF SHORT TERM**

- REQUIREMENTS: 2, OR 1 PER 3,800SF
- 2 SPACES

#### TEMPORARY LODGING: 80 RENTABLE ROOMS **SHORT TERM**

- REQUIREMENTS: 2, OR 1 PER 20 RENTABLE ROOMS

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

- 4 SPACES

#### 1880 NW FLANDERS ST TOTAL:

10 SPACES REQUIRED

**TOTAL SPACES PROVIDED:** 

10 SPACES REQUIRED

1819 NW EVERETT STREET TOTAL:

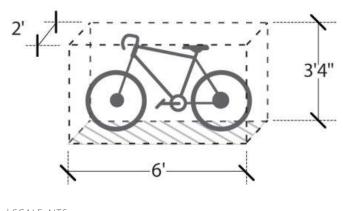
SHORT TERM: 20 SPACES REQUIRED, 20 SPACES PROVIDED

#### SHORT TERM BICYCLE PARKING: RACK DESIGN **LANDSCAPE FORMS (BOLA)**

#### 33.266.200 BICYCLE PARKING **DEVELOPMENT STANDARDS**







| SCALE: NTS

SHORT TERM BIKE PARKING



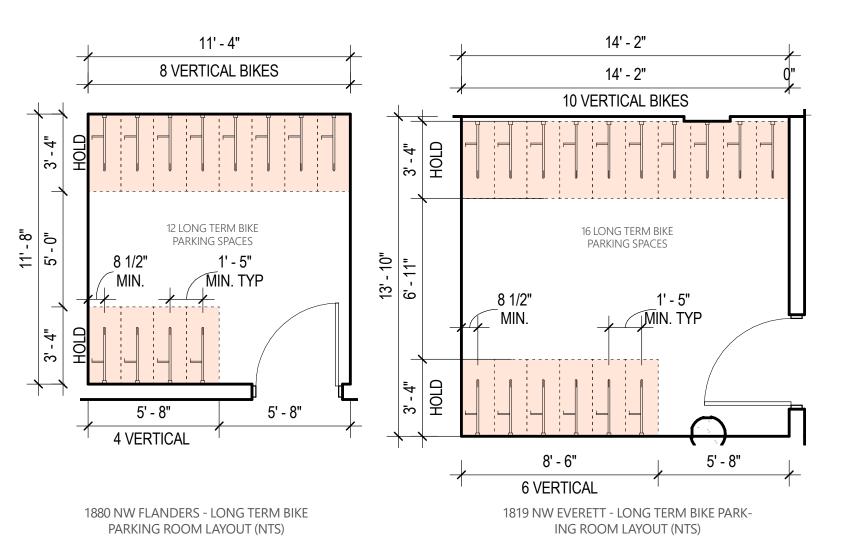
| SCALE: 1" = 40'-0"











# 

1819 NW EVERETT - BASEMENT FLOOR PLAN (NTS) -

**BIKE ROOM ACCESS** 

#### **1819 NW EVERETT STREET**

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

#### **RESTAURANT & BAR: 4,649 SF**

#### LONG TERM

- REQUIREMENTS: 2, OR 1 PER 1,000SF
- 5 SPACES

#### **RETAIL SALES AND SERVICES: 30,658 SF**

#### **LONG TERM**

- REQUIREMENTS: 2, OR 1 PER 2,700SF
- 11 SPACES

#### **LONG TERM** - REQUIREMENTS: 2, OR 1 PER 1,000SF

**1880 NW FLANDERS STREET** 

**RESTAURANT & BAR: 7,034** 

#### - 8 SPACES

#### **RETAIL SALES AND SERVICES: 4,815 SF**

#### **LONG TERM**

- REQUIREMENTS: 2, OR 1 PER 2,700SF
- 2 SPACES

#### TEMPORARY LODGING: 80 RENTABLE ROOMS **LONG TERM**

- REQUIREMENTS: 2, OR 1 PER 40 RENTABLE ROOMS

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

- 2 SPACES

#### 1880 NW FLANDERS ST TOTAL:

12 SPACES REQUIRED

#### 1819 NW EVERETT ST TOTAL:

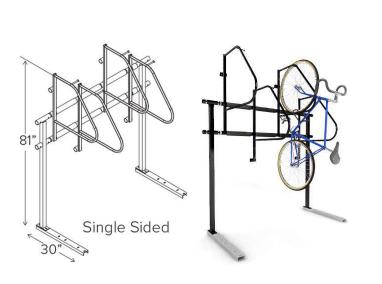
16 SPACES REQUIRED

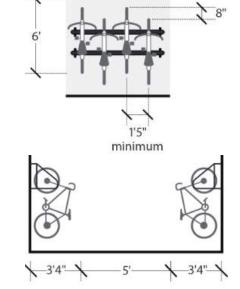
#### **TOTAL SPACES PROVIDED:**

LONG TERM: 28 SPACES REQUIRED, 28 SPACES PROVIDED

#### LONG TERM BICYCLE PARKING: DERO ULTRA **SPACE SAVER SQUARED**

#### 33.266.200 BICYCLE PARKING **DEVELOPMENT STANDARDS**





| SCALE: NTS

#### LONG TERM BIKE PARKING





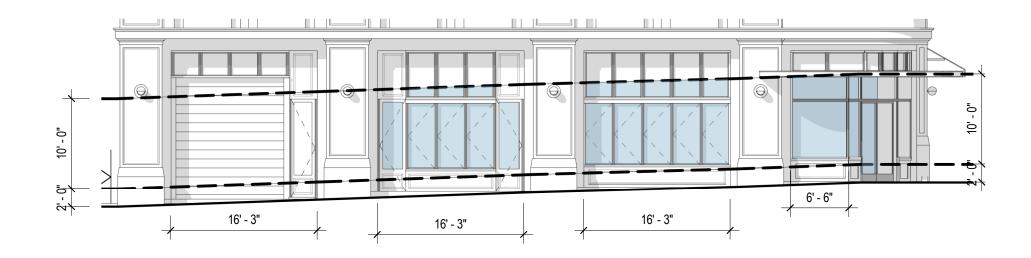


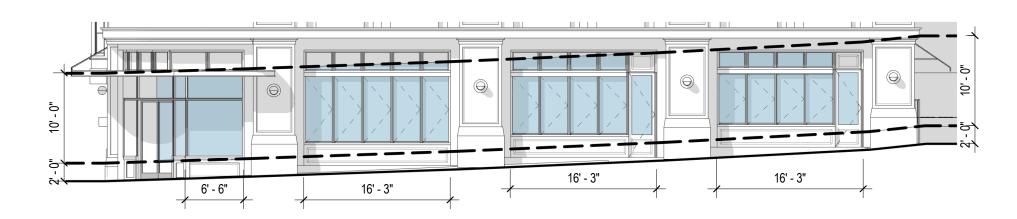
1880 NW FLANDERS - BASEMENT FLOOR PLAN (NTS) -

**BIKE ROOM ACCESS** 

1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

A.6 LU 22-204531 HRM





**GROUND FLOOR WINDOWS** 

STOREFRONT AREA = 361 SF

TITLE 33

40%

PER ZONING CODE SECTION 33.130.230 A MINIMUM OF 40% GLAZING ON THE GROUND FLOOR IS REQUIRED PER

<u>GROUND FLOOR GLAZING - NORTH ELEVATION</u>

LEVEL 1 AREA IN MEASURED ZONE = 857 SF

GROUND FLOOR GLAZING - WEST ELEVATION

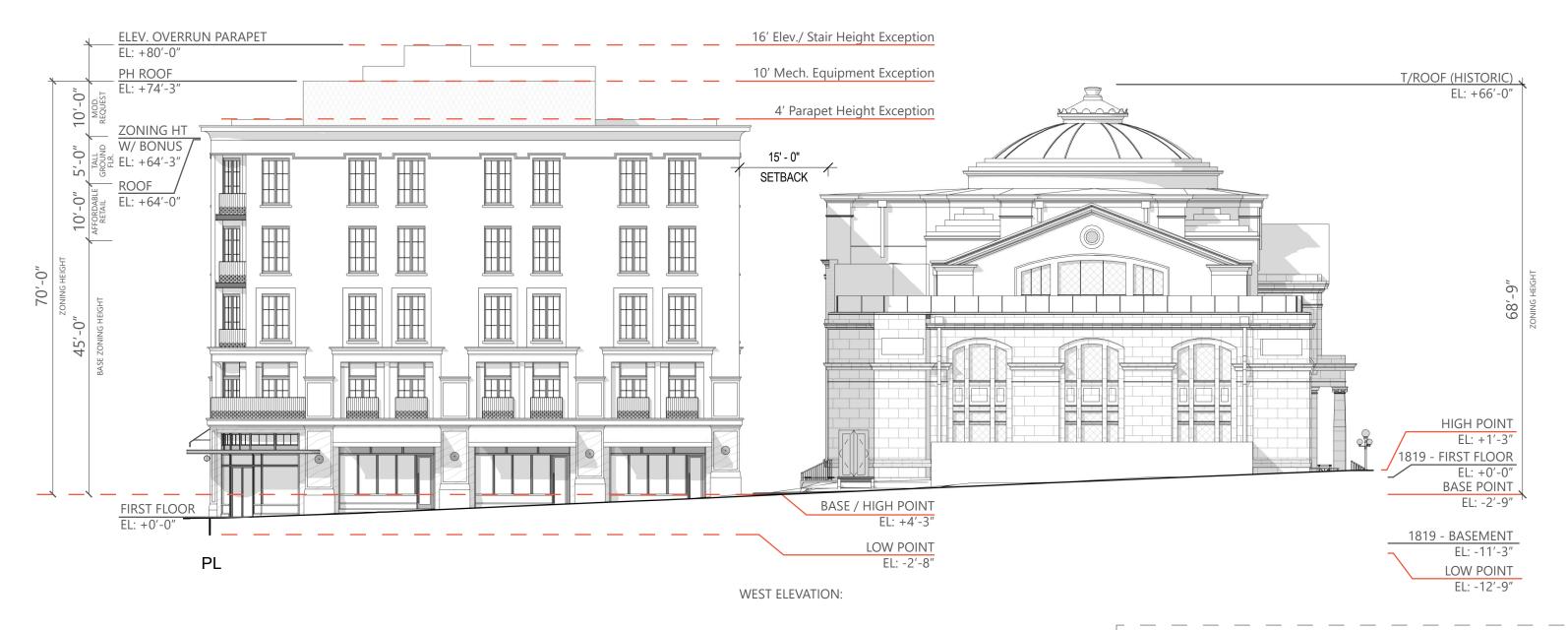
LEVEL 1 AREA IN MEASURED ZONE = 872 SF STOREFRONT AREA = 369 SF 42%

## **ELEVATIONS - GROUND FLOOR GLAZING (HOTEL)**

| SCALE: 3/32" = 1'-0"







#### **ZONING HEIGHT DIAGRAM**

| SCALE: 1/16" = 1'-0"





AREA FOR CITY STAMP

## Steel Look Aluminum Windows

Fixed T225



#### The true steel alternative option

Arcadia Custom Steel Look aluminum products are very similar to traditional steel systems and more than simply "steel look" products. They were derived from actual steel products that we have been manufacturing since 1947.

Thermally broken and artistically designed, the products in our premium Metal Window and Door Series retain many of the features and aesthetics of the classic original steel products.

What makes them really look like steel is our narrow 2 ¼" frame depth, the lock box synonymous with traditional steel doors, the beveled glass stop and ¾" SDL beveled muntin that simulate the look of original "putty" glazed windows. Also, we have the unique ability to mull "gang" different units together to create a Steel Look window wall.

Created with modern design needs in mind, our Steel Look aluminum products can be combined with our Thermal Steel products, allowing for a truly transformative degree of design flexibility.



#### **Features**

Size Up to 5' wide, 10' high

Thermally Broken Frames designed for improved energy performance, NFRC Certified

High Performance Glass 1" Dual-glazed, Low-E glass

Spacer High performance superspacer, Matte

black Finish; Stainless steel spacer

available

Frame Depth 2 1/4"

Hardware Single arm roto-crank with butt hinges

and multipoint casement lock; push out cam locks with 4 bar friction hinges; optional clerestory hardware available

Crank-out or push-out operation options;

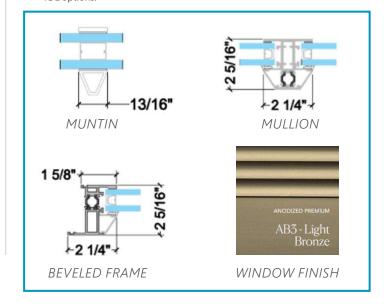
Configuration available as single, double, triple, or multiple operating sashes; matching

transoms available

FIXED WINDOW

#### Mu

Use muntins to create interest or fit particular architectural designs. Arcadia Custom products can be ordered with custom grid patterns in either SDL or TDL options.





# Thermal Aluminum Multi-Slide Door

4000T

#### Inviting natural light in, with expanded views

Available in an unlimited number of door panels, the Multi-Slide Door is the best way to create an indoor/outdoor space. The sleek, unobtrusive design allows you to completely open up parts of your home to outdoor living spaces in a seamless fashion. This system is available in both pocketing and corner configurations

#### **Features**

ze Individual door panel sizes up to 6'

wide,12' tall, with unlimited overall unit length based on number of door panels

Thermally Broken Door panels and frames for improved

energy performance

High Performance Glass 1" dual-glazed, Low-E glass, see Technical

Specifications for NFRC Chart

Frame Depth 2" per door; i.e., three door panels will

equal a 6" total track width

Hardware Flush pulls with stainless steel mortise

lock, 3" diameter roller available and recommended when door panels exceed

50 sq. ft.

Track Marine-grade, non-corrosive stainless

steel track insert for optimum EZ-gliding

AREA FOR CITY STAMP

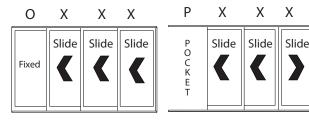
performance





#### Muntins

Use muntins to create interest or fit particular architectural designs. Our ULT4000T can be ordered with custom grid patterns in SDL options.



MULTI-SLIDE DOOR

## PRODUCT DATA: WINDOWS & STOREFRONT (1880 NW FLANDERS ST.)

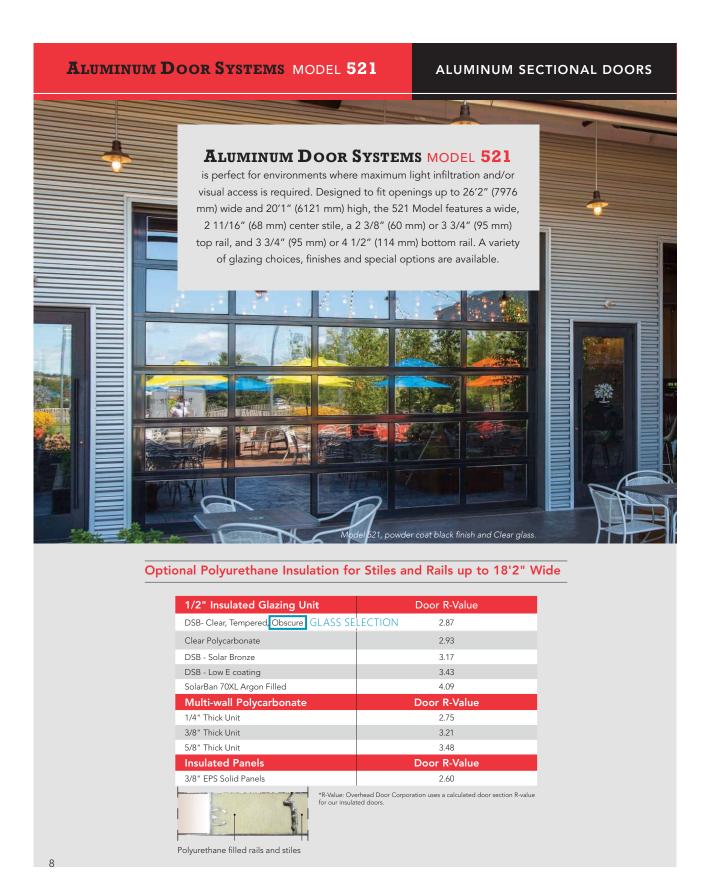
| SCALE: NTS

Operation/









## PRODUCT DATA: OVERHEAD ALUMINUM DOOR (1880 NW FLANDERS ST.)

| SCALE: NTS







#### MODEL **521**



#### Standard Features at a Glance **Options**

Section Thickness 1 ¾" (45 mm)  Maximum Standard Height 20'1" (6121 mm)  Maximum Standard Width 26'2" (7976 mm)	Glazing Options <sup>†</sup> : 1/8" (3 mm) DSB; 1/8" (3 mm) or 1/4" (6 mm) acrylic; 1/8" (3 mm) or 1/4" (6 mm) tempered; 1/8" (3 mm) or 1/4" (6 mm) clear polycarbonate;
Maximum Standard Height 20'1" (6121 mm)	1/8" (3 mm) or 1/4" (6 mm) acrylic; 1/8" (3 mm) or 1/4" (6 mm) tempered; 1/8" (3 mm) or 1/4" (6 mm) clear polycarbonate;
3 , , ,	1/8" (3 mm) or 1/4" (6 mm) clear polycarbonate;
Material Extruded 6061-T6 aluminum	1/4" (6mm) and 3/8" twin-wall polycarbonate, 5/8" triple-
Standard Finish 204R-1 clear anodized (painted white at no charge)	wall polycarbonate; 1/4" (6 mm) 3/8" (10 mm) and 5/8" (16 mm) twin-wall
Center Stile Width 2 11/16" (68 mm)	polycarbonate, triple-wall polycarbonate 1/4" (6 mm) wire glass; 1/2" (12 mm) insulated glass
End Stile Width 3 5/16" (85 mm)	Electric operator or chain hoist
Top Rail Width 2 3/8" (60 mm) or 3 3/4" (95 mm)	Bottom sensing edge
Top Intermediate Rail Width 2 1/8" (54 mm)	3" track
Bottom Intermediate Rail Width 1 19/32" (40 mm)	Bracket mounting (not available on full vertical door tracks)
Bottom Rail Width 3 ¾" (95 mm) or 4 ½" (114 mm)	Higher-cycle springs in 25k, 50k, 75k, 100k cycles
Weatherseals Bottom, flexible PVC	Exhaust ports
Standard Springs 10,000 cycle	Four-section pass door
Track 2" (51 mm)	Wind load and impact rated door available
Mounting Angle	Posi-tension drums
Operation Manual pull rope	Bronze anodization
Hinges and Fixtures Galvanized steel	Powder coat finish
Lock Galvanized, interior-mounted single unit	Pass door
Warranty 1-Year Limited; 3-Year Limited on powder coat finish	<sup>†</sup> Contact your local Overhead Door <sup>™</sup> Distributor for special glazing requirements. Verify 1/4" (6 mm) glass applications with factory.

#### **Structure Options**



Clear (standard)







Black

**Powder Coat Finishes** Select from approximately 200 RAL powder coat color options to best match your home.

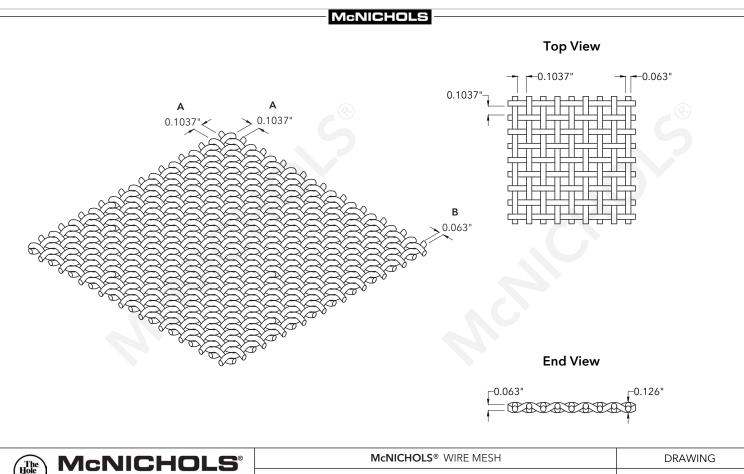


Cherry with Flame Dark Walnut Actual door colors may vary from brochure photos due to fluctuations in the printing process. Always request a color sample from your Overhead Door™ Distributor for accurate color matching.

Panel Layo	out	Section Stack		
Door Width	Number of Panels	Door Height	Number of Sections	
to 9'2" (to 2794 mm)	2 or 3 (standard)	to 8'6" (2591 mm)	4	
9'3" to 12'2" (2819 mm to 3708 mm)	3	8'7" to 10'1" (2616 mm to 3073 mm)	5	
12'3" to 16'2" (3734 mm to 4953 mm)	4	10'2" to 12'1" (3099 mm to 3683 mm)	6	
16'3" to 18'2" (4978 mm to 5537 mm)	4 or 5 (standard)	12'2" to 14'1" (3708 mm to 4293 mm)	7	
18'3" to 19'2" (5562 mm to 5842 mm)	5	14'2" to 16'1" (4318 mm to 4902 mm)	8	
19'3" to 20'11" (5867 mm to 6375 mm)	6**	16'2" to 18'1" (4928 mm to 5512 mm)	9	
21'0" to 23'11" (6401 mm to 7290 mm)	8**	18'2" to 20'1" (5537 mm to 6121 mm)	10	
24'0" to 26'2" (7315 mm to 7976 mm)	10**	**Special construction. Consult your local Overhead <sup>™</sup> Door Distributor for additional information.		

overheaddoor.com

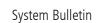




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Drawing is for visual and conceptual purposes, in ominal manufacturing data subject to star McNICHOLS CO. does not provide engineering information provided is for evaluation by techniany use thereof to be at their independent discresshall have no responsibility or liability for results from improper evaluation or use. McNICHOLS C	ndard production tolerances. services of any kind. Technical cally skilled persons only, with etion and risk. McNICHOLS CO. obtained or damages resulting O. makes no representation or
warranty of any kind, express or implied, at law including with respect to merchantability, fitne purpose, or design. All other representation disclaimed. This document and other related McNICHOLS CO. Terms and Conditions.	ess for any particular use or ns or warranties are hereby

	IVICI	DRAWING		
	MESH TYPE CONSTRUCTION TYPE	Square Woven	NOT TO SCALE	
n	PRIMARY MATERIAL WEAVE or TRIM TYPE	Stainless Steel Woven - Plain Weave	Item Number 380663	
al h	PERCENT OPEN AREA	39%	Revision Date 03.23.2021	
). g	MESH SIZE  A OPENING SIZE	6 x 6 0.1037" x 0.1037"	Page Number 1 of 1	
r g r y	<b>B</b> WIRE DIAMETER/WIRE GAUGE	0.063" Thick (16 Gauge)	© 2021 <b>McNICHOLS CO.</b> All Rights Reserved.	

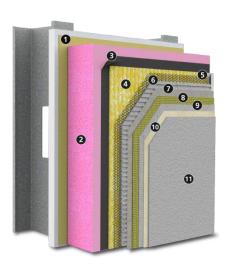
MECHANICAL SCREEN WALL





#### StoPowerwall® ci

Portland cement stucco with continuous air and water-resistive barrier, continuous insulation, advanced cavity wall design, and high performance finish



Substrate: Glass mat gypsum sheathing in compliance with ASTM C 1177 (or building code compliant wood-based sheathing - Type V construction only)

	,,
1)	StoGuard <sup>®</sup> Air and Water-Resistive Barrier
2)	Owens Corning or Dupont Type IV XPS Insulation Board
3)	Code compliant paper or felt Water-Resistive Barrier
4)	Sto DrainScreen® drainage mat
5)	Code compliant miniumum 2.5 lb/yd² (1.4 kg/m²) self- furred galvanized steel diamond mesh metal lath
6), 7)	ASTM C 926 compliant stucco scratch and brown coat (as manufactured or listed by Sto Corp.)
8), 9)	Sto Crack Defense (optional): Sto base coat with Sto Mesh embedded
10)	Sto primer (optional)
11)	<ul> <li>Choose among:</li> <li>Sto Textured Finishes</li> <li>Sto Custom Cast Finish: Wood or Brick</li> <li>Sto Signature Series or Sto Speciatly Finishes</li> </ul>

STUCCO WALL

#### System Description

StoPowerwall ci is an energy efficient stucco wall assembly with a continuous air and water-resistive barrier and continuous insulation. It combines the strength and durability of traditional stucco with an advanced cavity wall design and Sto high performance finishes.

StoPowerwall ci can be used in residential or commercial wall construction where energy efficiency, superior aesthetics, and air and moisture control are essential in the climate extremes of

Features	Benefits	
Integrally colored factory blended textured finish	Consistent color and aesthetics increase curb appeal	
Continuous exterior insulation	Energy efficient, reduced heating and cooling costs	
Impact and puncture resistant	Withstands abuse, reduced maintenance	
Continuous air and moisture barrier	Protects against mold and moisture problems	
Fully tested, building code compliant		

Compilant	
Properties	
Weight (excluding sheathing / studs)	< 12 psf (56.6 kg/m²)
Assembly Thickness (from exterior stud face)	Nominal $3^{-5}/8$ " (92 mm) with 2" (51 mm) XPS
R-value (insulation only)	5 – 10 ft²•h•°F / Btu (0.88 – 1.76 m²•K / W)
Wind Load Resistance	Capable of achieving: +65, -48 psf (+3.11 to -2.29 kPa)
Compliance	<ul><li>IBC, IRC, and IECC</li><li>ASHRAE 90.1-2019</li></ul>
Construction Types, Fire Resistance	NFPA 285 tested for use on noncombustible construction     ASTM E119 1-hour rated assembly

Up to 12-year Limited Warranty available on Sto products, depending on options selected

#### Maintenance

Requires periodic cleaning to maintain appearance, repair of cracks and impact damage if they occur, recoating to enhance appearance of weathered finish. Sealants and other façade components must be maintained to prevent water infiltration.

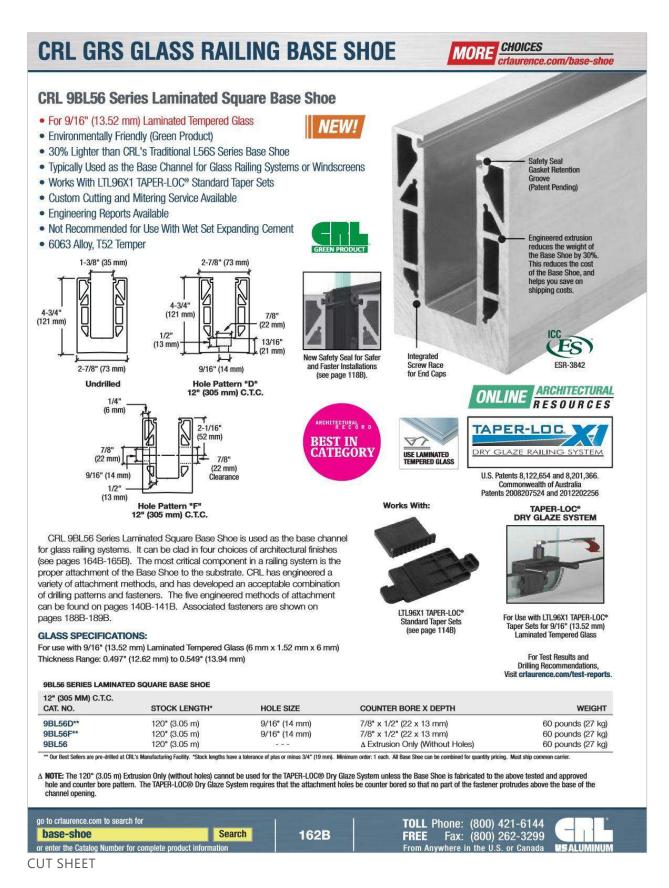
| SCALE: NTS





PRODUCT DATA: MECHANICAL SCREEN, STUCCO

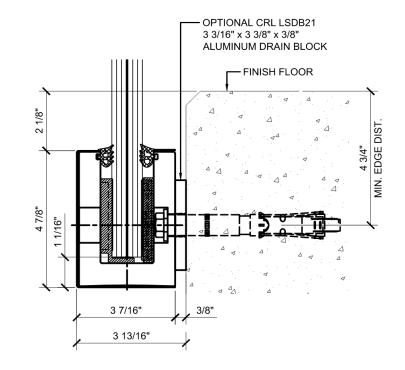






PRODUCT IMAGE - BASE DETAIL

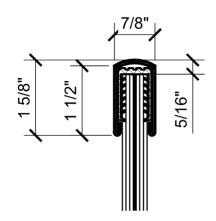
**ELEVATION DETAIL** 



# FASCIA MOUNT DETAIL CONCRETE SUBSTRATE

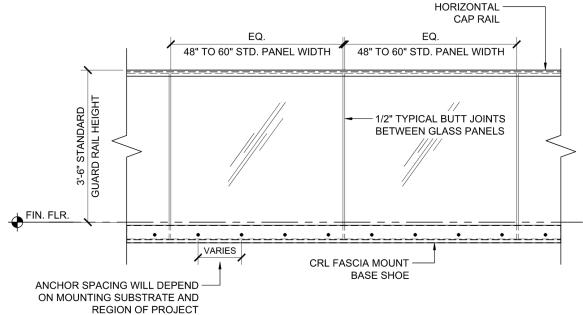
ANCHOR SPACING WILL DEPEND ON MOUNTING SUBSTRATE AND REGION OF PROJECT 12" O.C. SPACING IS STANDARD

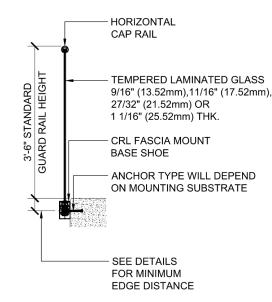
SECTION DETAIL AT BASE



GRCR5 - 7/8" x 1 1/2" FOR 9/16" (13.52mm) LAMINATED TEMPERED GLASS

TOP CAP DETAIL





SECTION DETAIL

#### PRODUCT DATA: GLASS GUARDRAIL (1819 NW EVERETT ST.)

I SCALE: NTS







**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

AREA FOR CITY STA

A.6 LU 22-204531 HRM

ob Name				Location	•	rator, single zone split system
Purchaser				Engineer		
					Approval	Construction
Submitted to				Reference _	Approval	Construction
Jnit Designation	on			Schedule #		
		Specifications				
	Nominal Capacity	Cooling (Btu/h)	24,			
	Norminal Capacity	Heating (Btu/h)	27,I 4,405 -			SAMSUNG
Performance	Capacity Range	Cooling (Btu/h) Heating (Btu/h)	5,700 -			<u>.</u>
	SEER / EER			/ 9.6		OND AVELLA ON
	HSPF Condensate (pints/hour)		7.8 6.34		AQN24VFUAGM	
	Voltage (ø/V/Hz)	,,,,,	1 / 208-:			
	Working Voltage F		176 - 254 (max. 3%	deviation from each)		
ower	Rated Current Cooling (A) (Low/Std./Max.) Heating (A)		2.6 / 11. 2.3 / 13.	9 / 12.5		SAMSUNG
	Max. Breaker (A)	nealing (A)	2.3713.			
	Min. Circuit Ampa	city (A)	12	.0	1668	
	WXHXD	Indoor Unit	41 15/16 X 1			
imensions	(inches)	Outdoor Unit Indoor Unit	34 5/8 X 31 9 2		1	
	Weight (lbs.)	Outdoor Unit	11	8	792	Source and
	Condensate Conn	ection	11/16			· ·
	Indoor & Outdoor	Type FPI	Aluminum Fin			*
eat Exchanger	Unit	Pipe Diameter	1/4		A	QX24VFUAGM
· ·	Indoor Unit		2 row /			
	Outdoor Unit		2 row /			
ound Pressure Level	Indoor Unit (dB) Outdoor Unit (dB)	(L/H) High	30 /		Low ambient control built in	n
	Outdoor Offit (dB)	T T	Standard: 1		Outdoor unit shall provide	208/230V power to indoor unit via 14 AW
perating	Outdoor	Cooling	0 ≤ T ≤ 115 with wi		3 interconnect power cable	
emperatures (°F)		Heating	5 ≤ T		• Electro-static, washable, H	D (high density) main filter as standard
. ,	Indoor	Cooling Heating	61 ≤ T	≤ 90 80		, ,
	İ	High side (flare)	1/-		Construction Indoor unit chassis shall be	UL94 V0 with a galvanized steel mounting
ipe Connections	Indoor & Outdoor	Low side (flare)	5/-		bracket	
pe conficultions	Maximum / Minime Maximum Vertical	um Line Set Length (ft.)	98 /		The outdoor unit shall be ga	Ivanized steel with a baked on powder co
	Type	Separation (it.)	R4 <sup>2</sup>	-	finish for durability	
	Control Method		Electronic Exp		Heat Exchanger	
efrigerant	Factory Charge (o	z.)	58		The heat exchanger shall be mechanically bonded fin to coppe	
	Charged for Additional Refrige	rant		25 ft. /ft. over 25 ft. Refrigerant System		
	Manufacturer		Sam			rmetically sealed, inverter controlled, Twi
	Туре		DC, Inverter D		BLDC Rotary	
ompressor	RLA (A)	Cooling (lawfold (high)	8. 15 / 7		Refrigerant flow shall be cor	ntrolled by EEV (electronic expansion val
	Operating Frequency (Hz)	Cooling (low/std./high) Heating (low/std./high)	15 / 76		at outdoor unit	
	Туре		BLDC motor with	cross-flow fan (1)	Indoor Fan	
vaporator Fan	Air Volume (max.		53	30		ngle, antibacterial cross-flow type
	Consumption Operating Current	(A)	2 0.:		Three fan speed settings an	d auto setting
	Motor		BLDC motor w	-		a add oothing
ondenser Fan	Output	W	12	24	Controls	type signal
	FLA	Amps	0.8		Control signal shall be DDC	
	Condensate pump	)	ASP-MO-UNIV			hall be 16 AWG X 2 shielded wire between
	L.,	Standard	AQN-WRS (inclu WH00 controller)	des sub-PCB and MWR-	outdoor and indoor units	
	Wired Controller	Premium	AQN-WRP (inclu	des sub-PCB and MWR-	Unit shall be operated via w	ireless controller (included)
ccessories	Wall bracket (for o		WE10 controller  CKN-250	with scheduling)	Optional wired control availa	able
		ed and flared, interconnect	25' - ILS2509		., 24 35.145. 474110	
	cables included		50' - ILS5009		Convenience	
	Wind Baffle	Front Back	☐ WBMF-24/36 ☐ WBMB-24		<ul><li>Auto restart</li><li>Turbo mode (during cooling)</li></ul>	a operation only)
	Certifications	<u></u>	ETL & ETLc		Auto changeover	g oporation only)
	Certinoations	PCB fuses, indoor unit terr	ETL & ETLc minal block thermal fuse, current transformer, over-		• 24 hour timer	
afety	Devices	voltage protection, crant	case heating, temperature	e limit protection logic,	Good'sleep mode     Quiet mode	
	1	co	mpressor overload sensing	3	• Dry mode	
arranty	1	5 Years compressor, 3 Yea	ar Parts, 120 Day limited	labor		

CU PRODUCT DATA



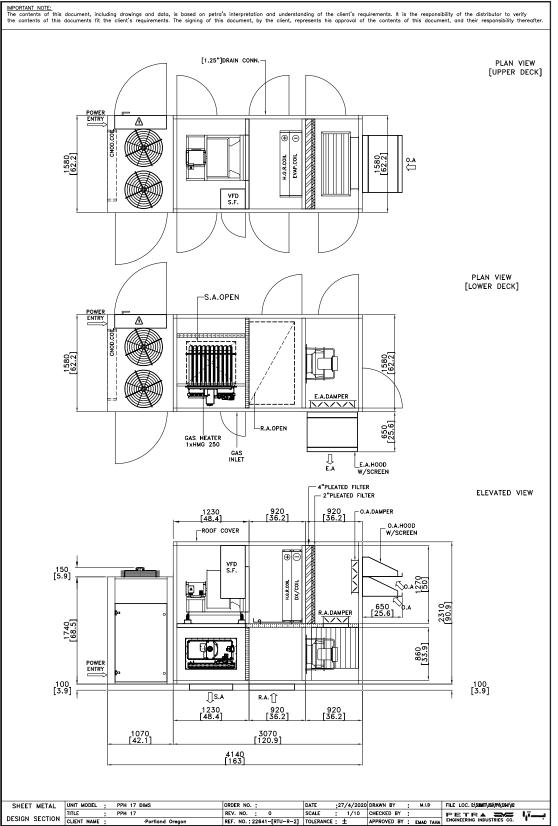
\* NOTE: PRELIMINARY PRODUCT DATA: DESIGN AND LAYOUT PENDING FINAL ENGINEERING
\* EQUIPMENT TO COMPLY WITH TITLE 18 NOISE CONTROL

REQUIREMENTS

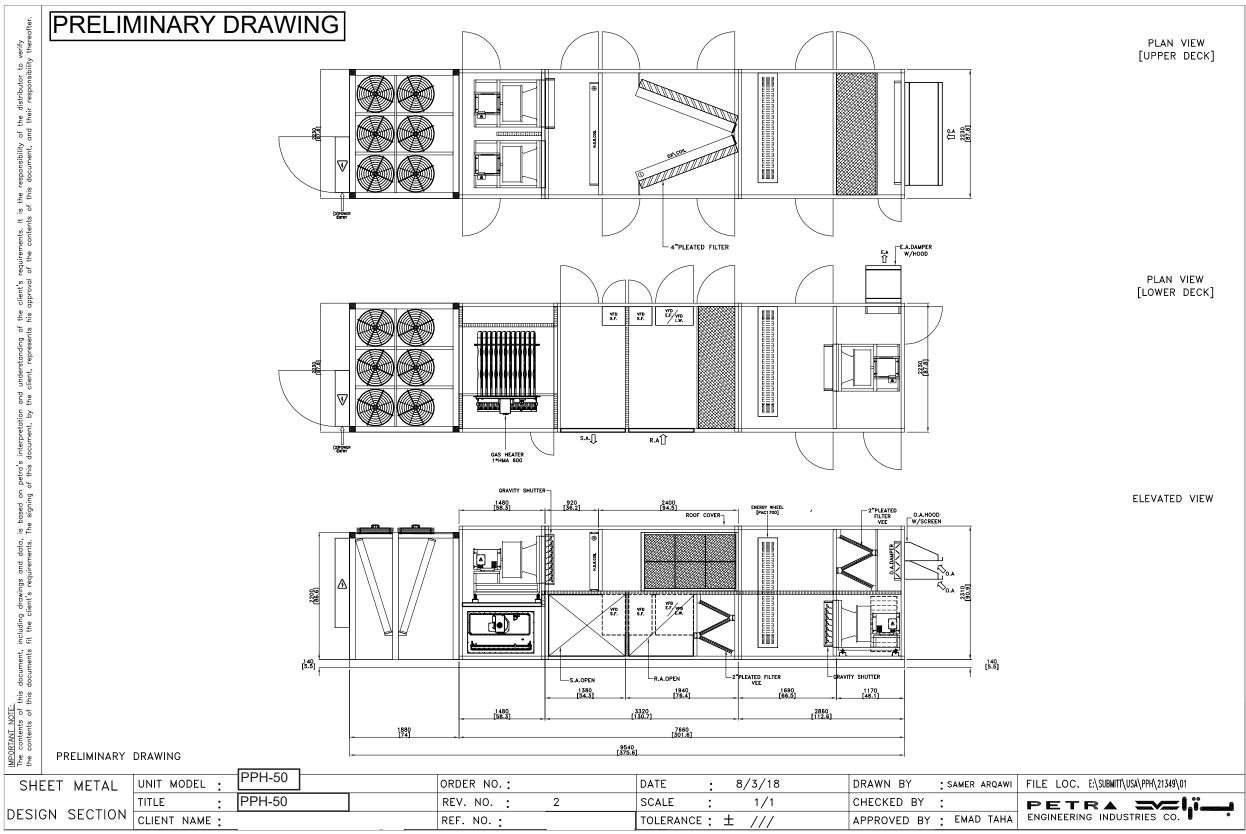
| SCALE: NTS



1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR



RTU / RTU-1 PRODUCT DATA



DOAS-1 PRODUCT DATA

PRODUCT DATA: MECHANICAL EQUIPMENT \*

\* NOTE: PRELIMINARY PRODUCT DATA: DESIGN AND LAYOUT
PENDING FINAL ENGINEERING
\* EQUIPMENT TO COMPLY WITH TITLE 18 NOISE CONTROL
REQUIREMENTS

APP.23

| SCALE: NTS





#### Tree Table Common / Botanical Name Size Location NW FLANDERS ST American Elm / Ulmus americana 36" 2 American Elm / Ulmus americana 30" 3 American Elm / Ulmus americana 32" 4 American Elm / Ulmus americana 40" Ginkgo biloba / Ginkgo 6" 5 Ginkgo biloba / Ginkgo 6" 6 Ginkgo biloba / Ginkgo 8 Frontier Elm / Ulmus 'Frontier' Frontier Elm / Ulmus 'Frontier 9 Newport Plum / Prunus cer. 'Newport' 25" 10 11 Linden / Tilia spp. Linden / Tilia spp. 12 30" Birch / Betula spp. 13 26" NW 19th AVE 0 Tree Protection Fencing, typ NW EVERETT ST LANDSCAPE: TREE PLAN



| SCALE: AS SHOWN



1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

Cond. Status

Poor

Poor

Street Tree Good To be preserved

Poor To be removed

To be removed

To be removed

To be removed

Good To be preserved

To be removed

To be removed

Street Tree

Street Tree

Street Tree

Street Tree

On Site

On Site

On Site

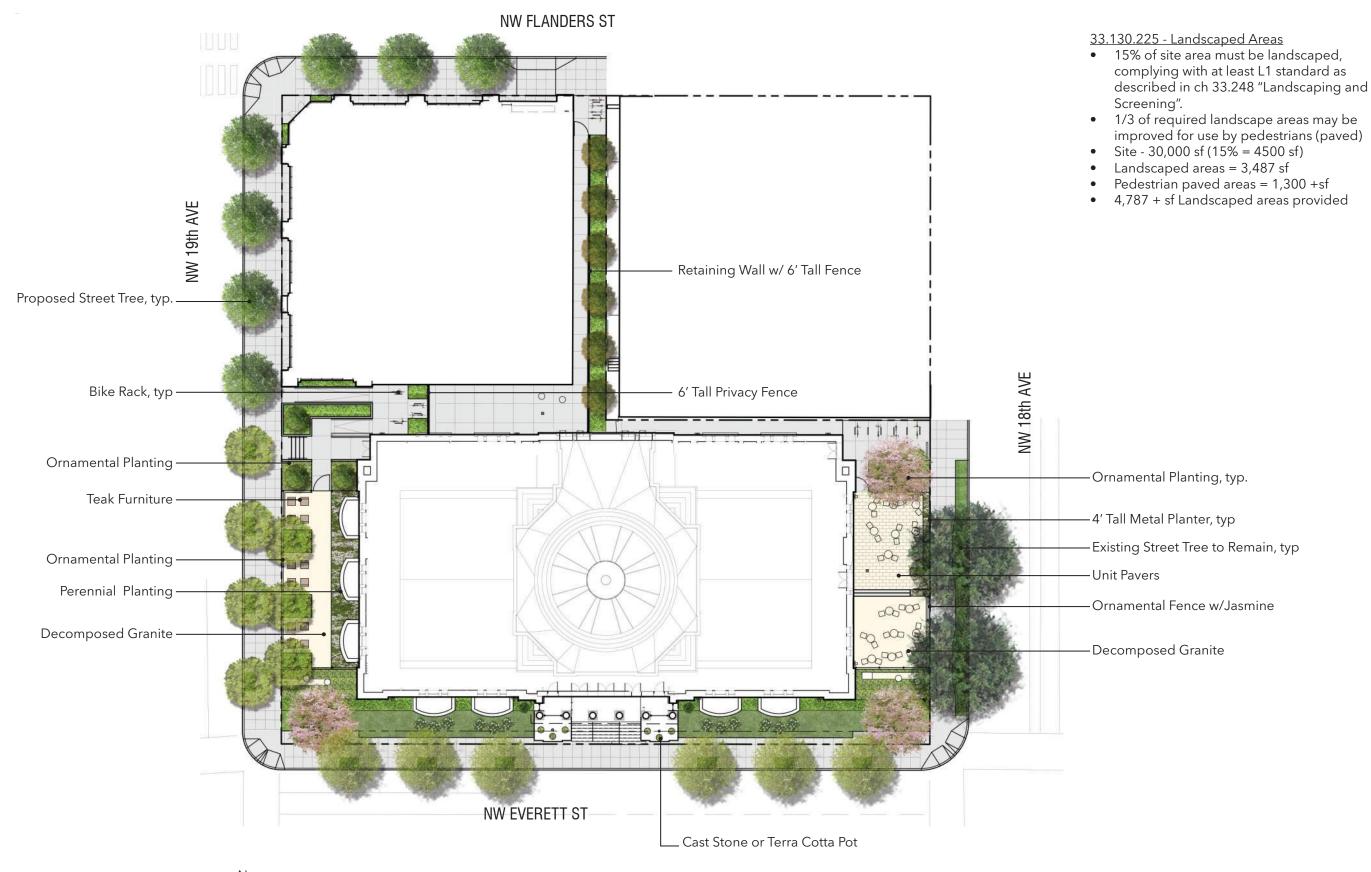
Street Tree

Street Tree

Street Tree

Street Tree

Onsite

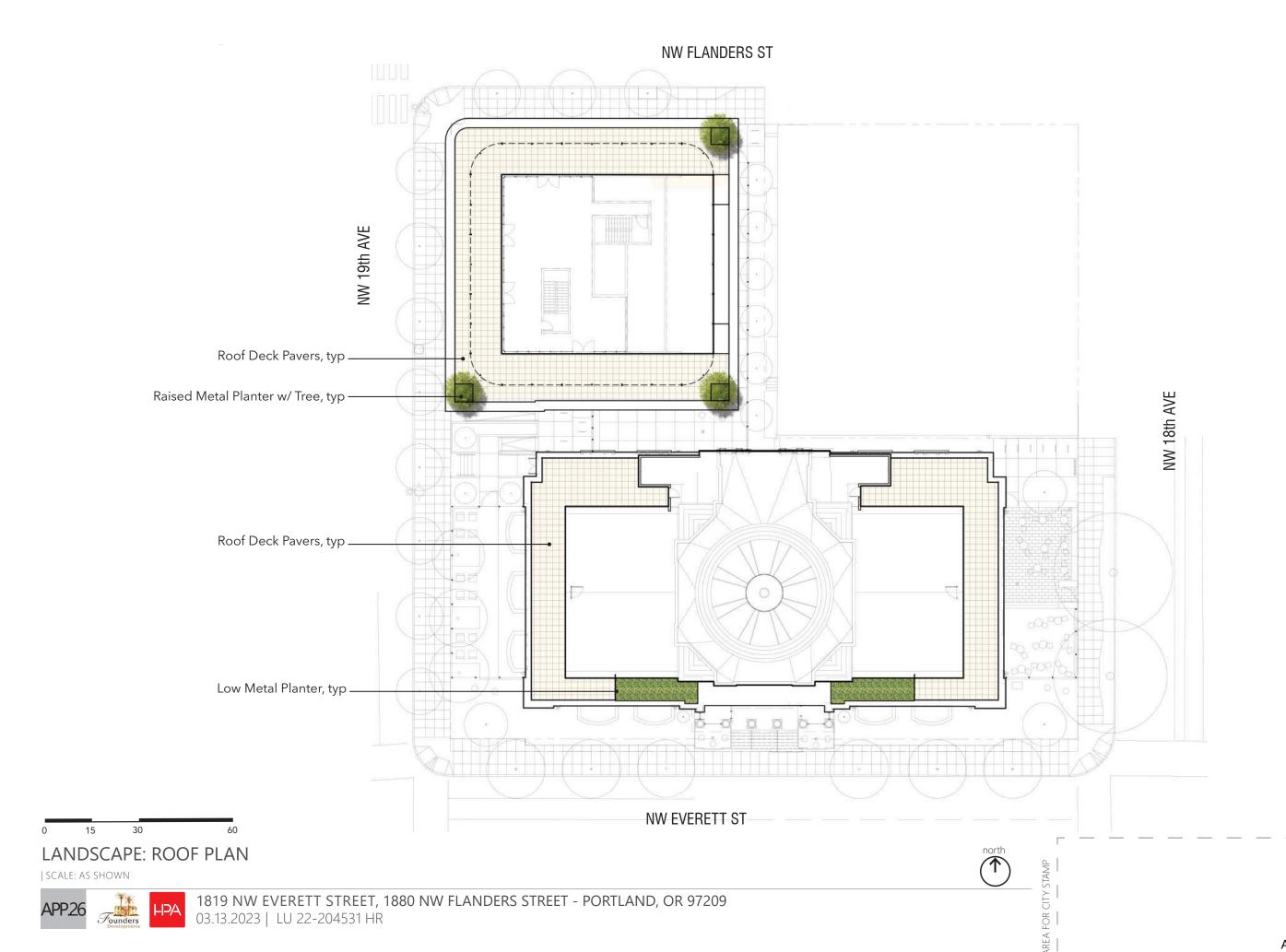




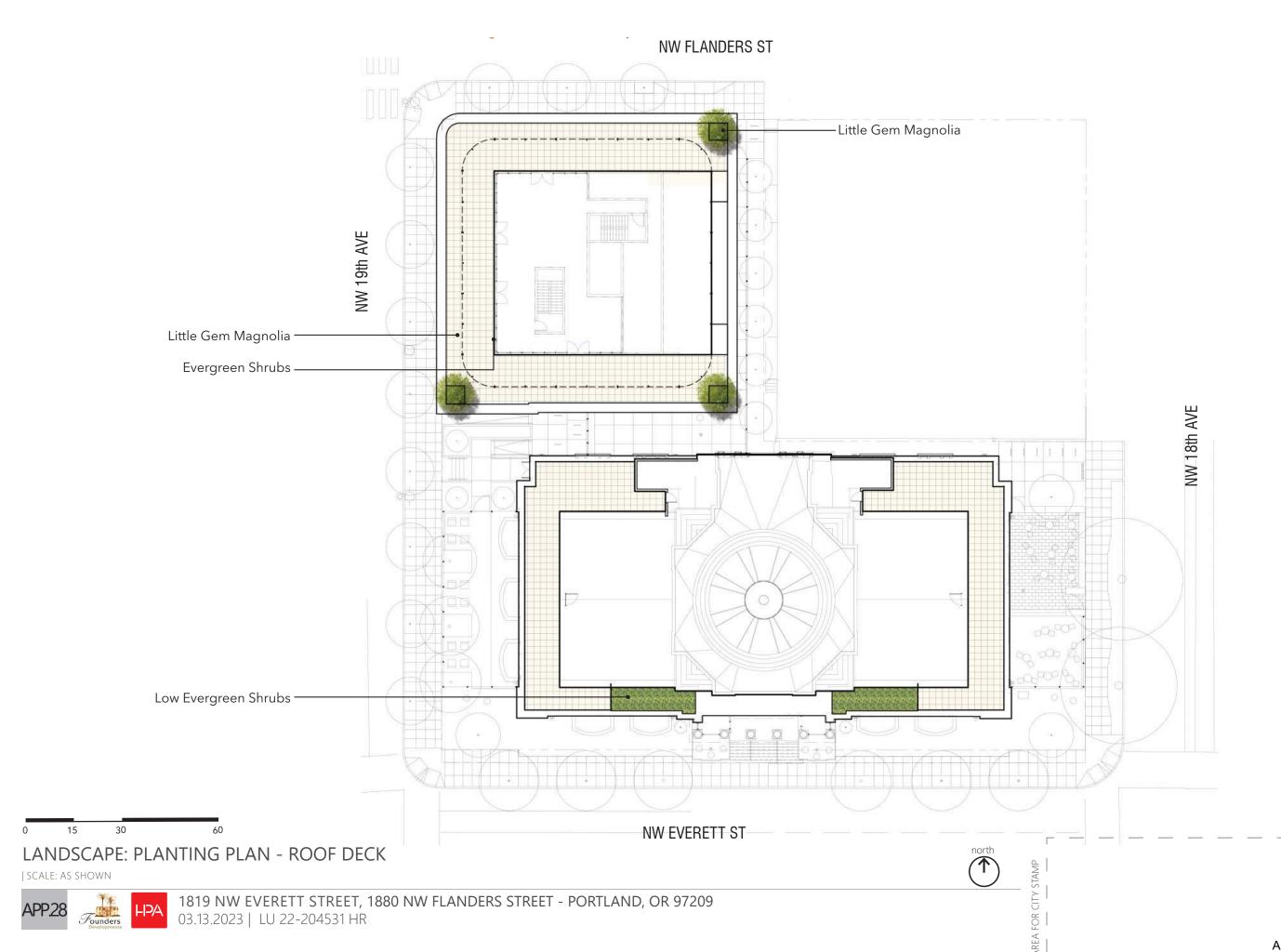












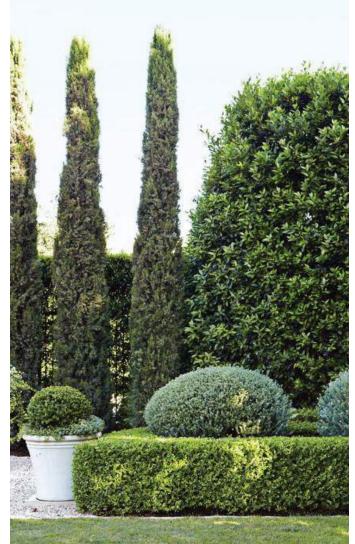


## LANDSCAPE: EVERETT STREET ELEVATION

| SCALE: NTS





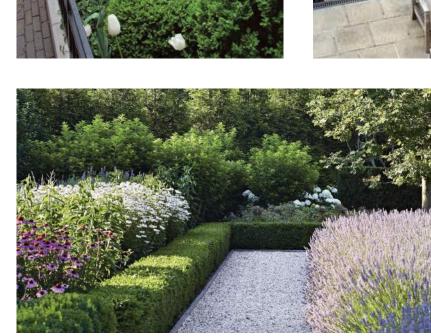


















LANDSCAPE: PRECEDENT IMAGES









Metal Picket Fence w/ Vine Planting (Vines not to exceed 50% coverage)



Raised Metal Planters w/ anti graffiti coating



Picket Guardrail



Bike Rack



At-grade and Roof Deck Pavers

### LANDSCAPE: SITE FURNISHINGS











Golden Colonade Gingko



Columnar Red Maple



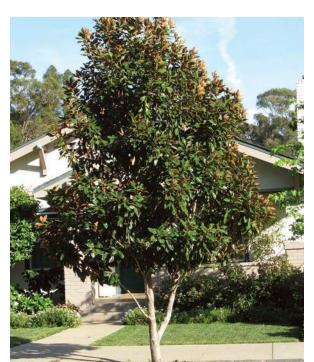
Columnar Red Oak



Italian Cypress



Yoshino Cherry



Evergreen Magnolia



Little Gem Magnolia

LANDSCAPE: PLANT PALETTE - TREES











Kinnikinnik

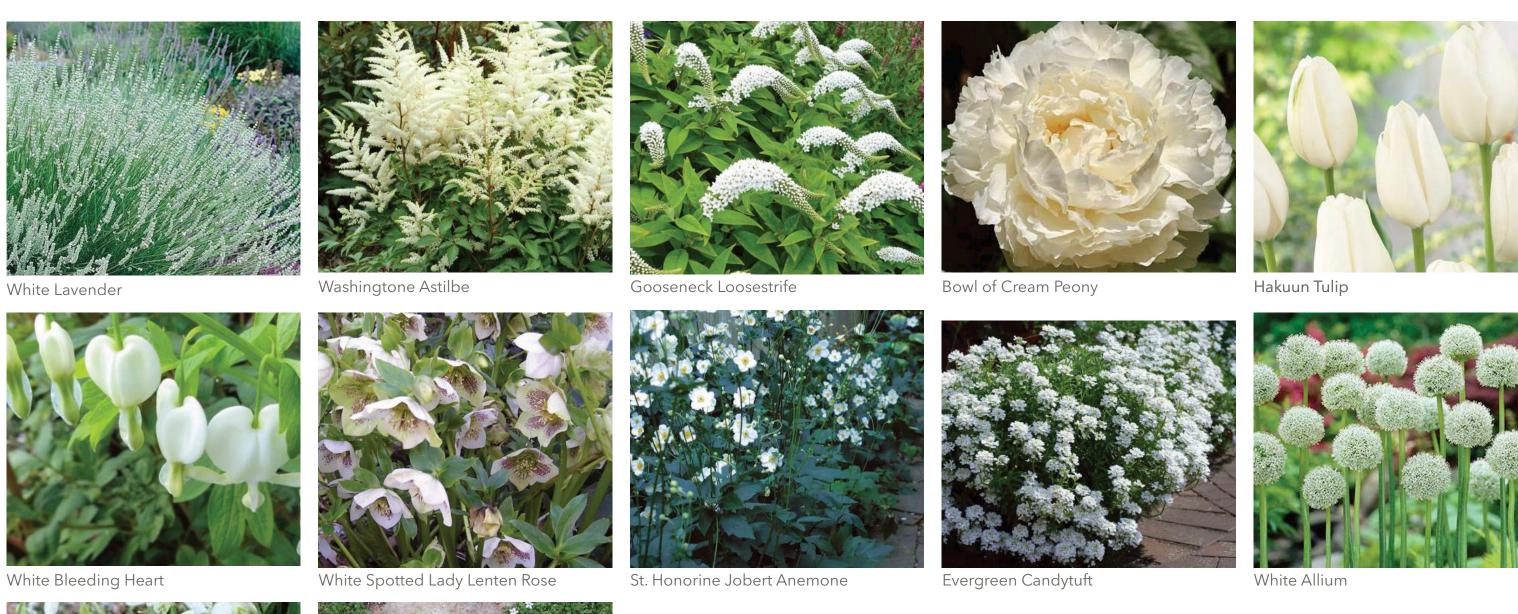
LANDSCAPE: PLANT PALETTE - SHRUBS & GROUNDCOVER





Japanese Spurge







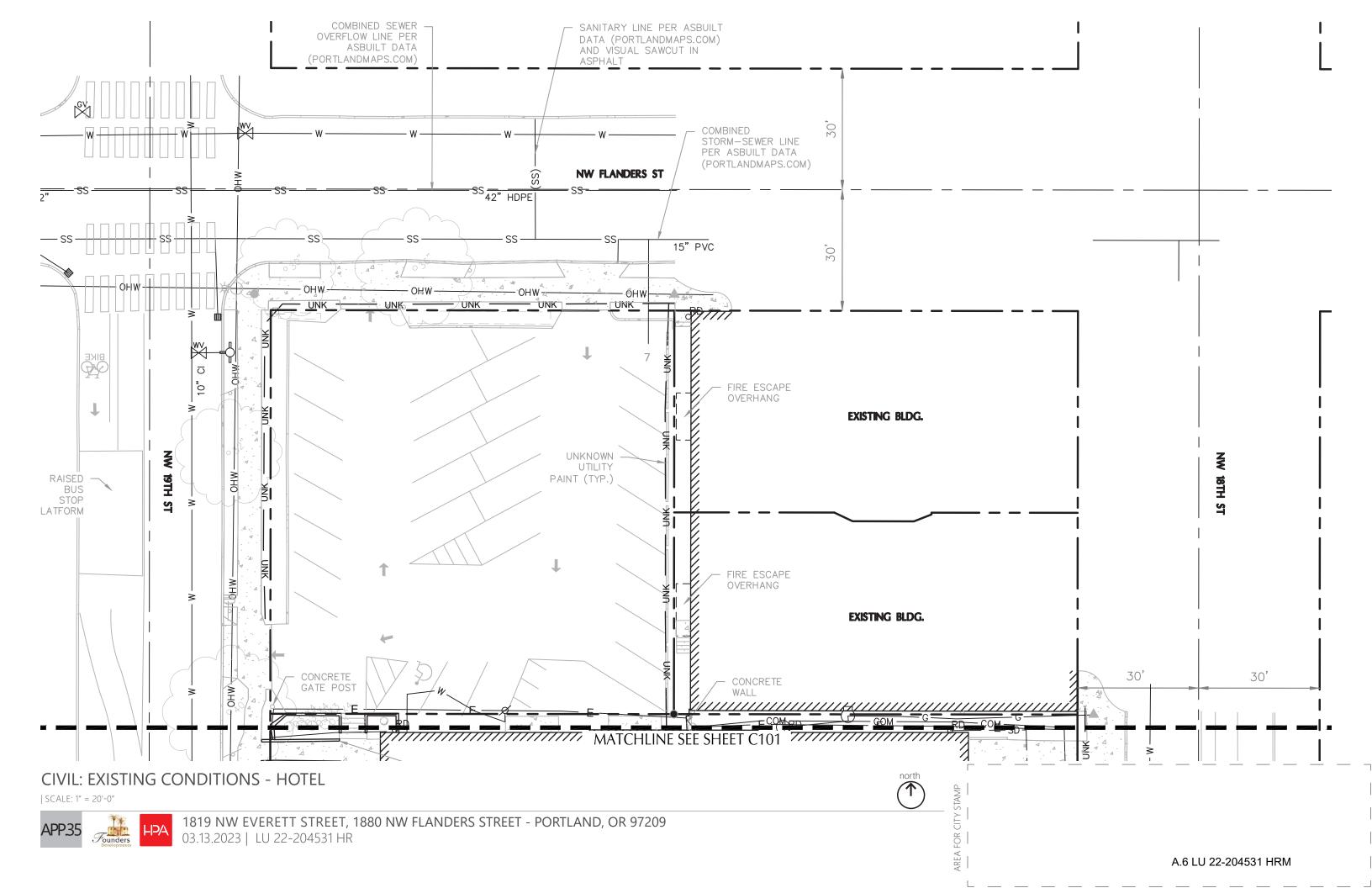


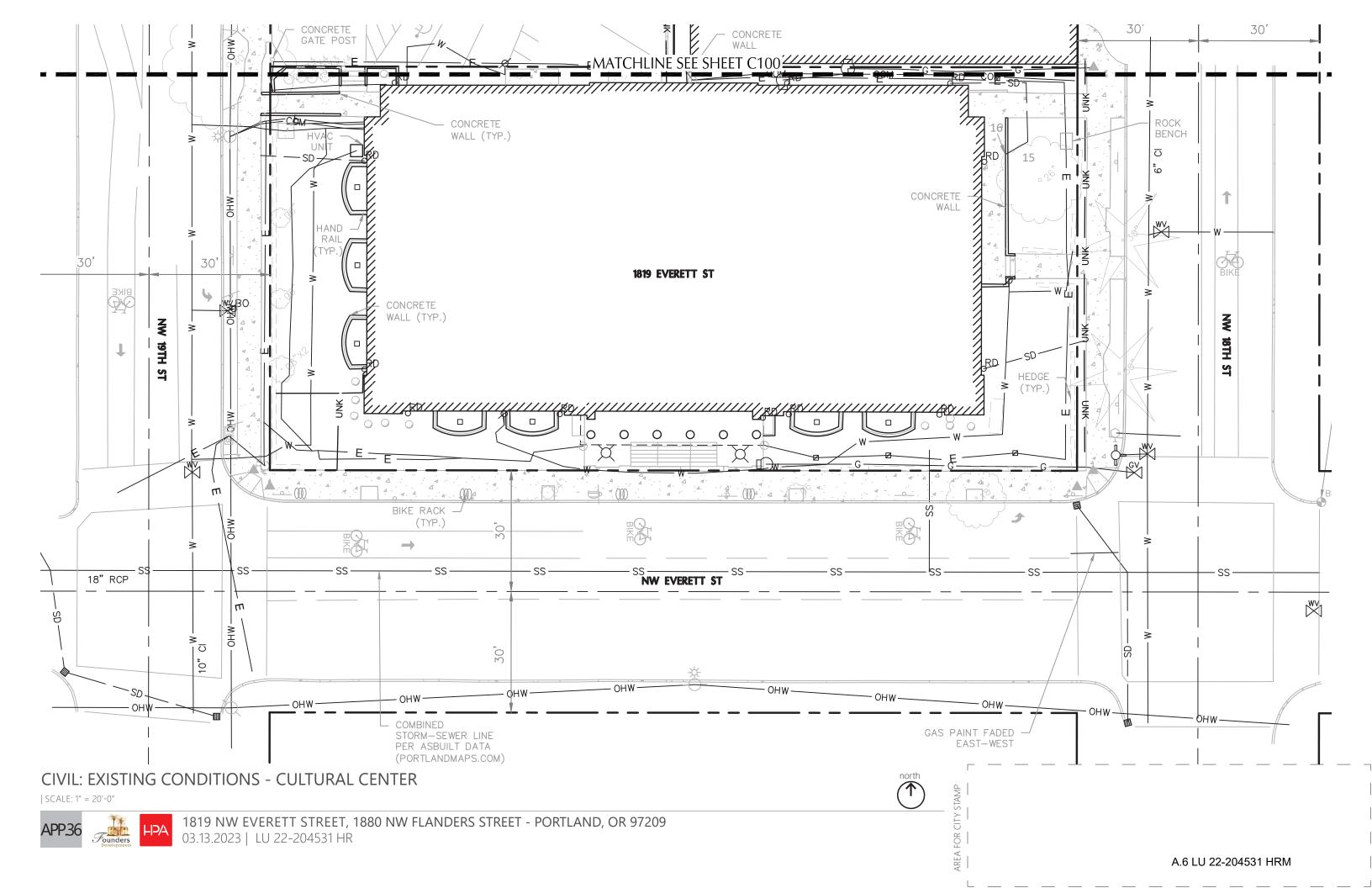
Snowdrops Blue Star Creeper

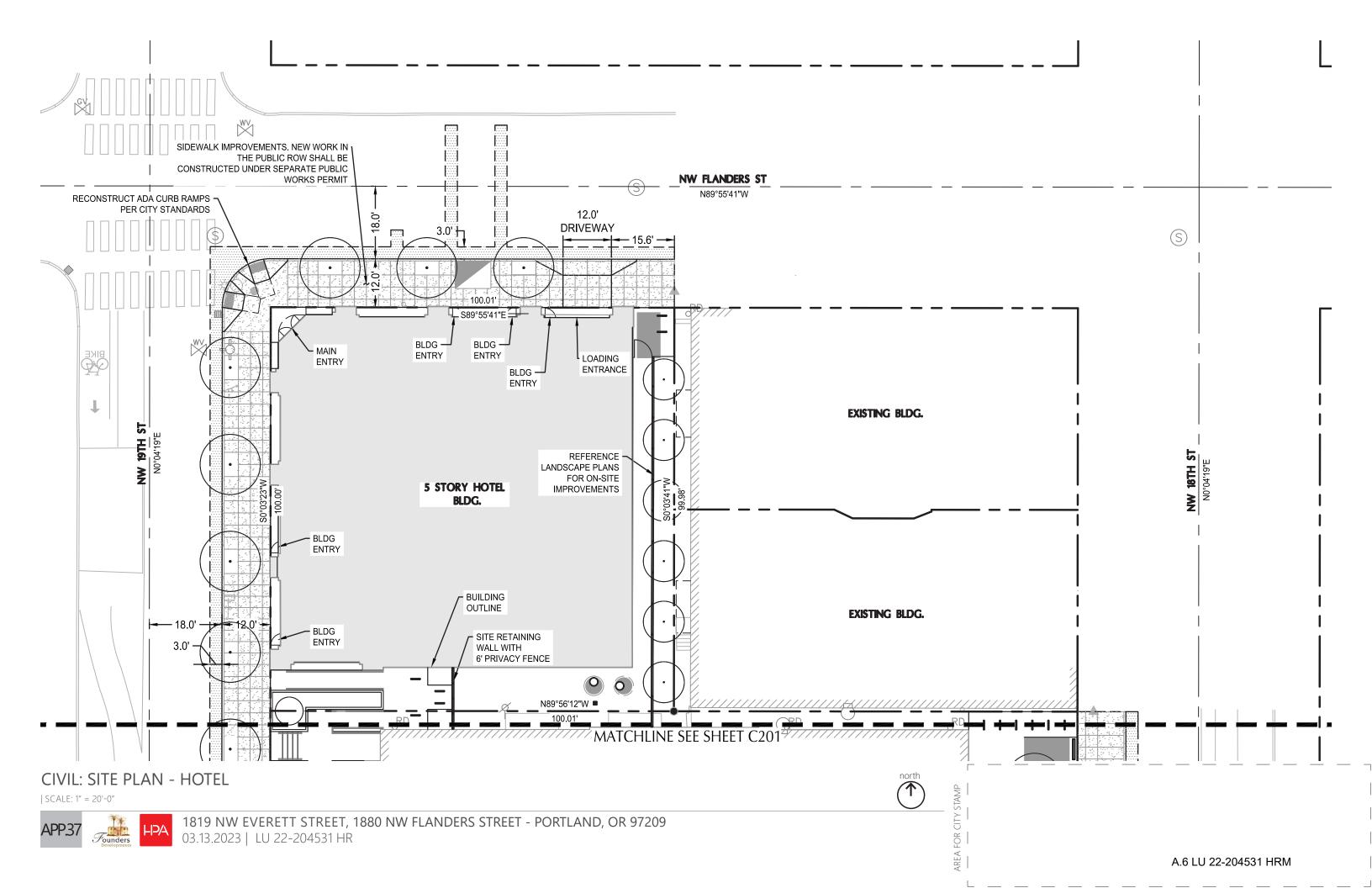
LANDSCAPE: PLANT PALETTE - PERENNIALS

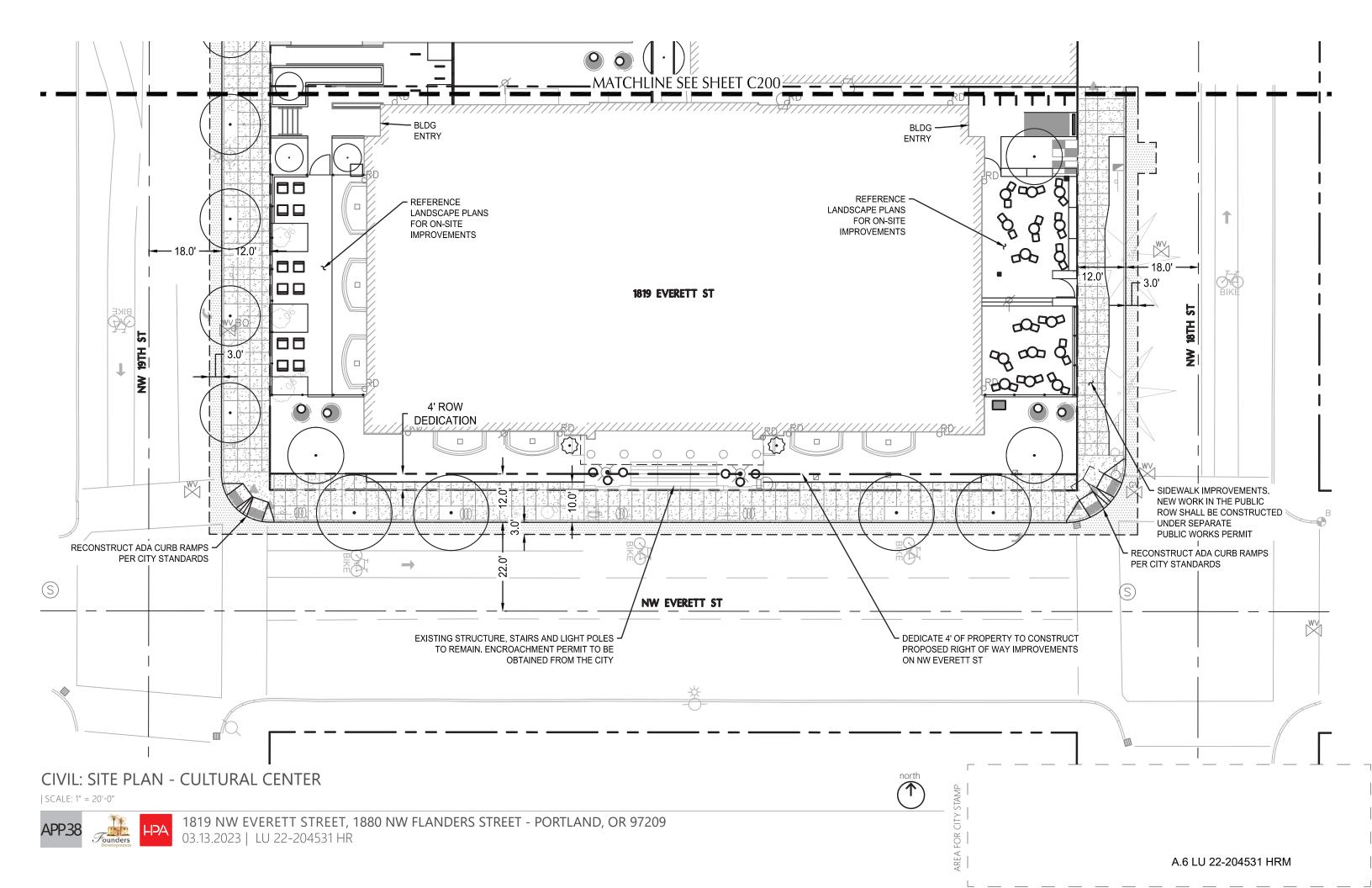


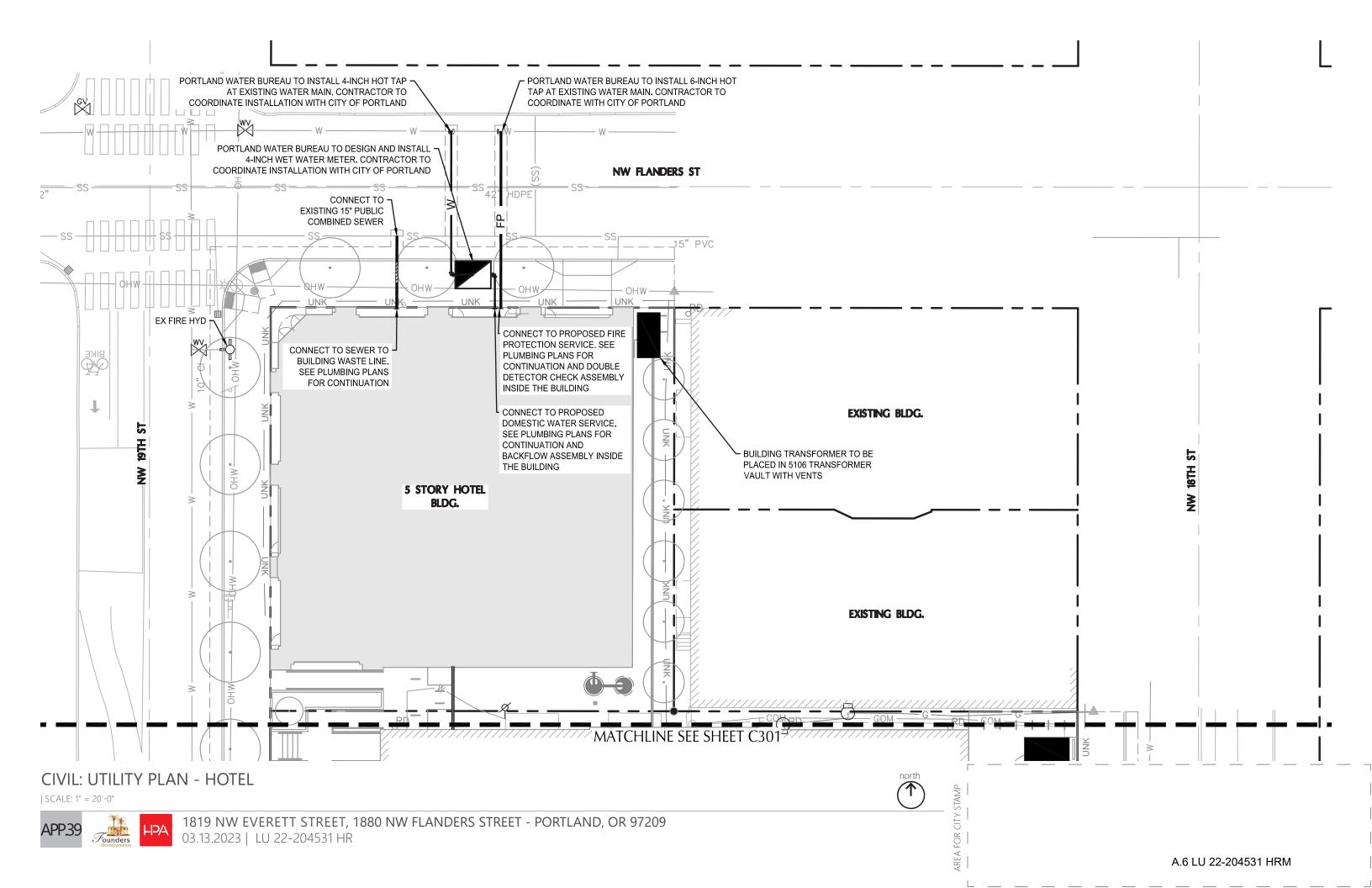


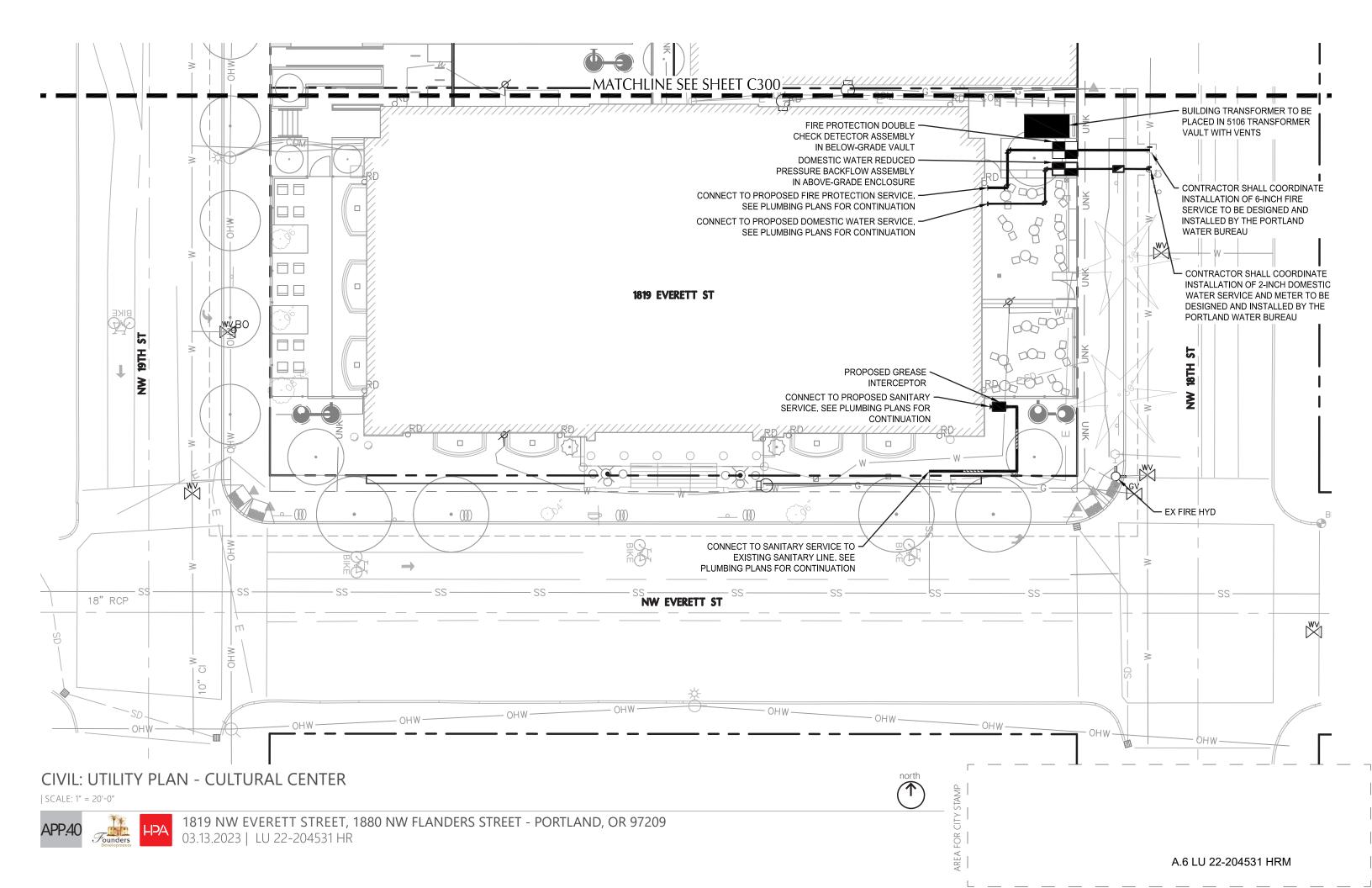


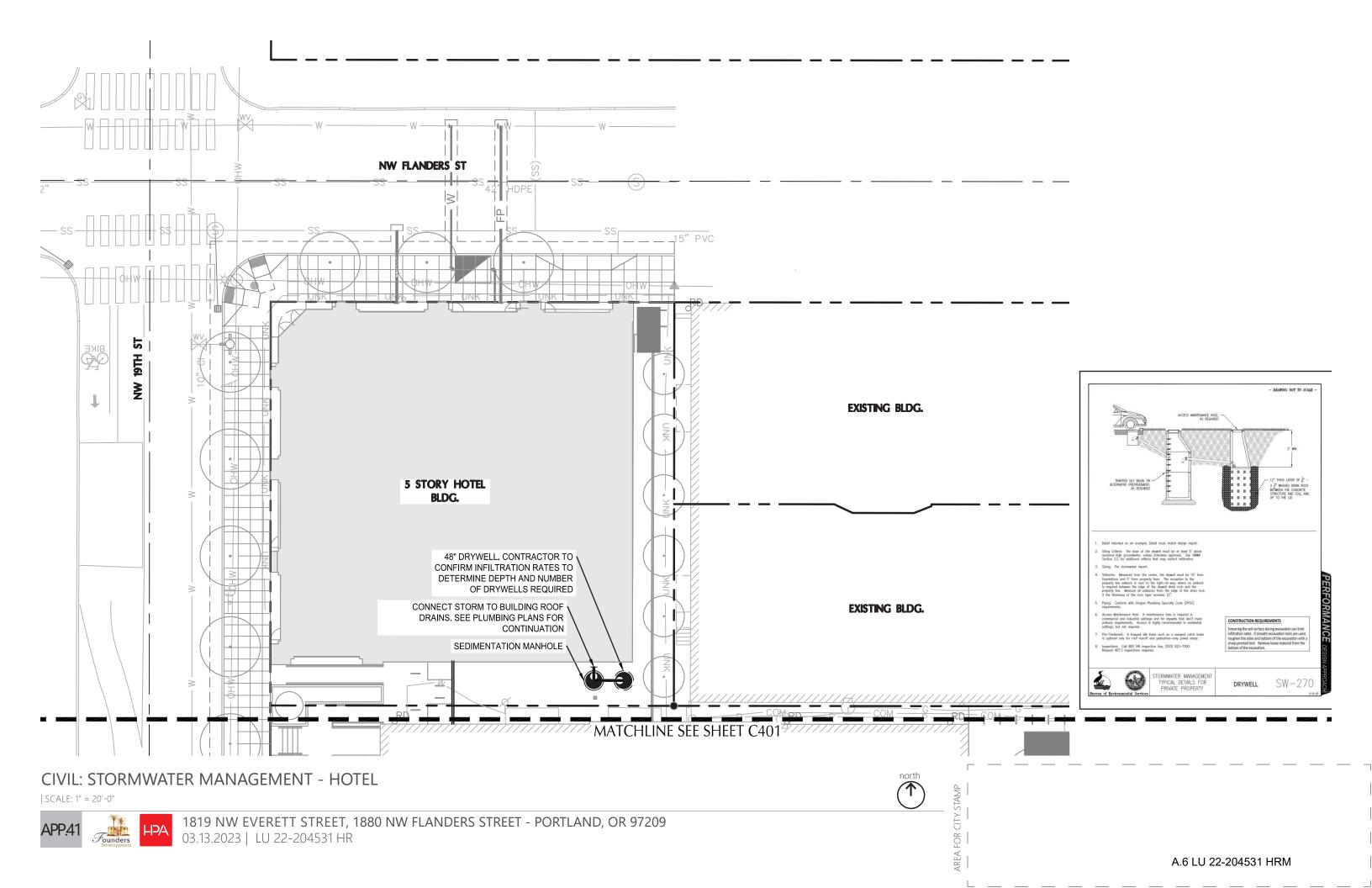


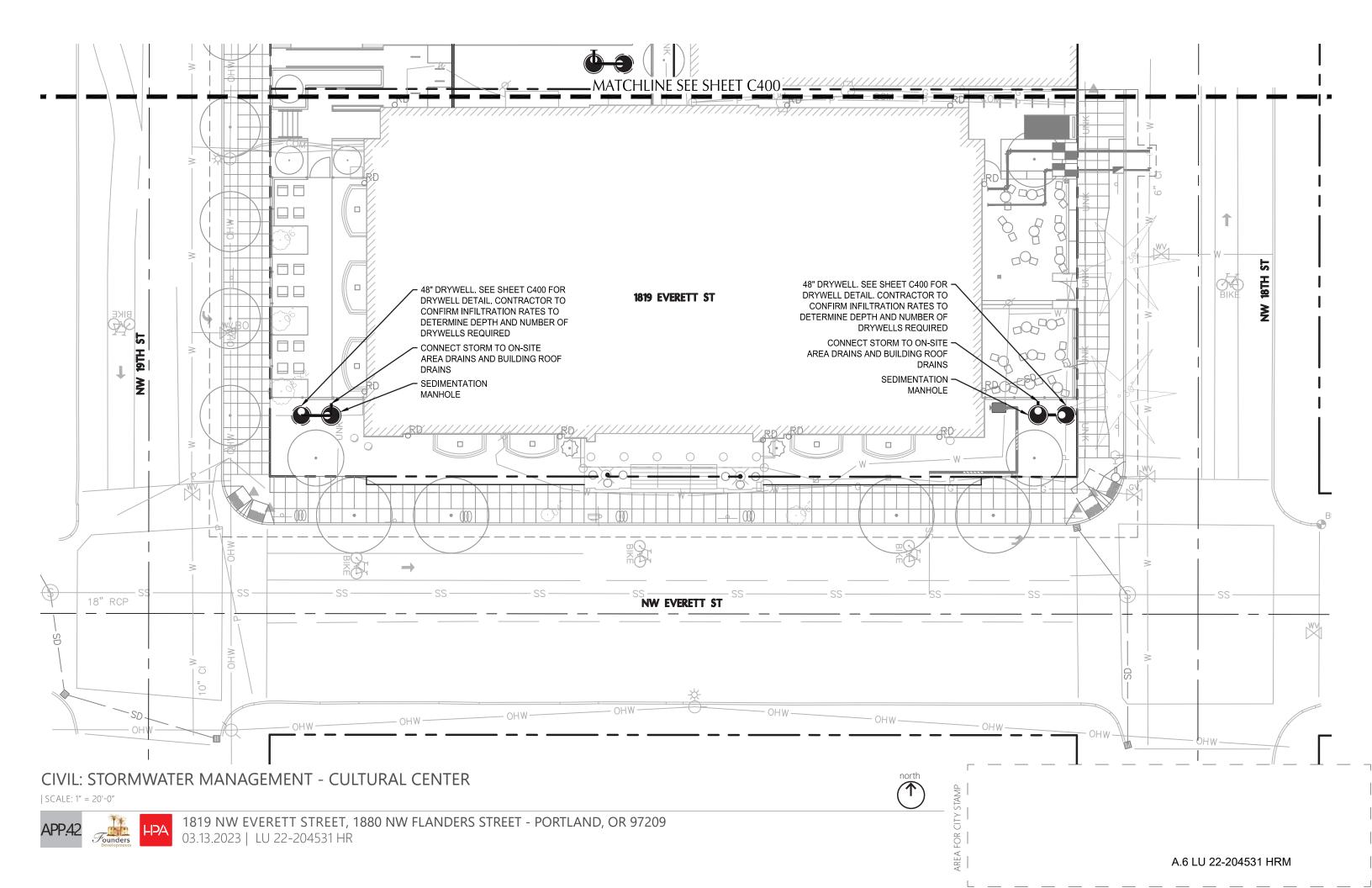












# TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

1819 NW EVERETT STREET
1880 NW FLANDERS STREET

Appendix - Supporting Information

March 13th, 2023

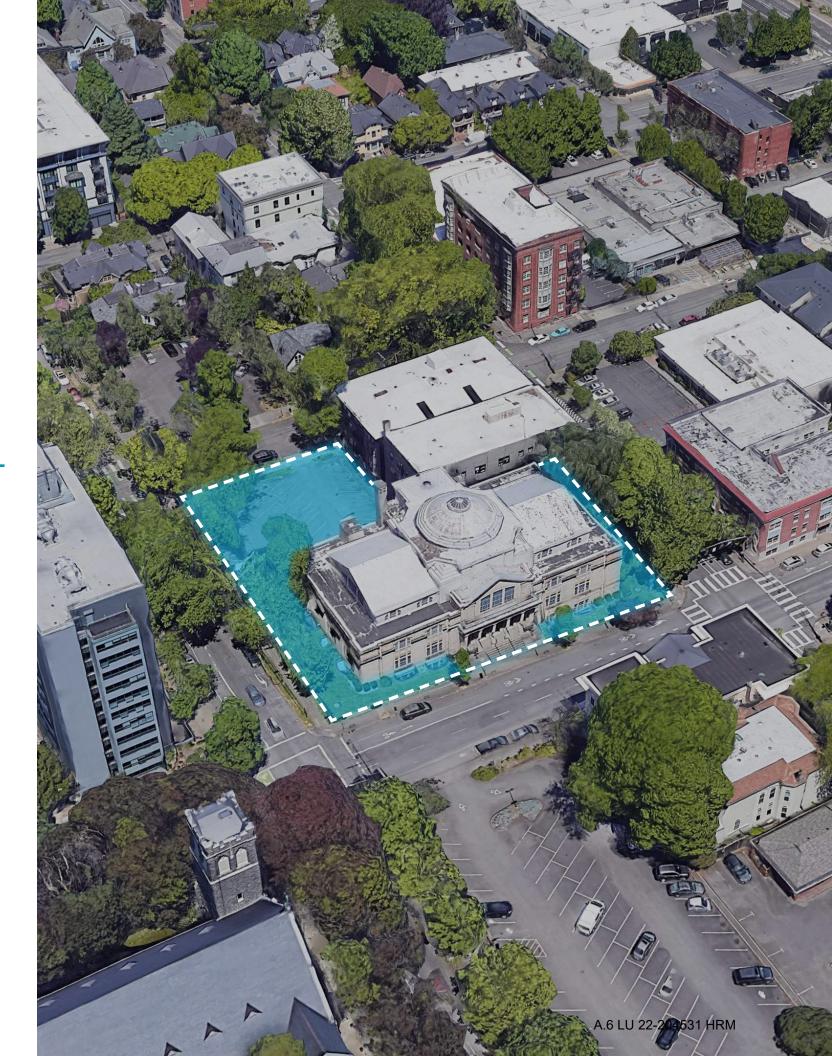
# TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

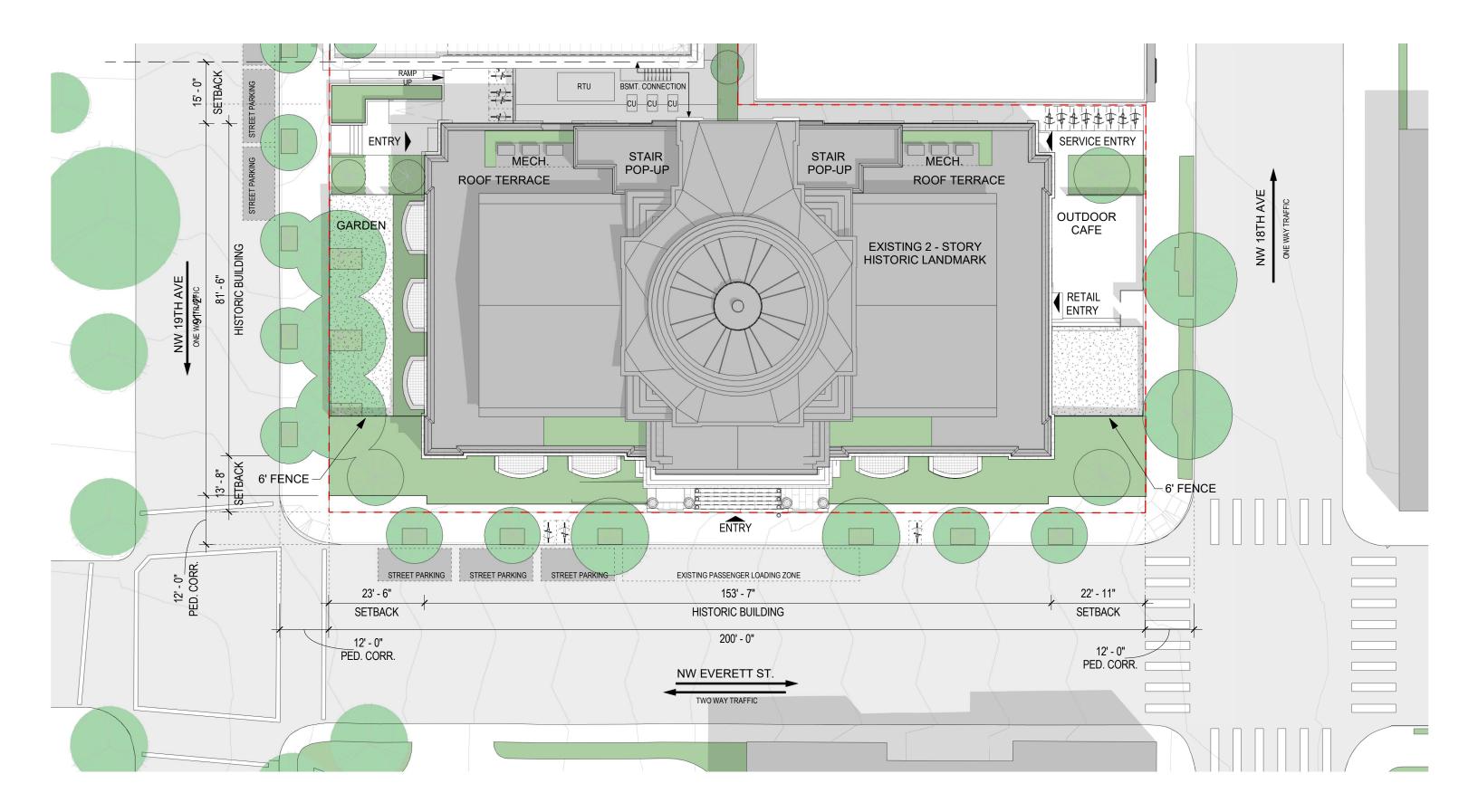
# 1819 NW EVERETT STREET

Historic Construction - Drawings

March 13th, 2023







## ENLARGED SITE PLAN (1819 NW EVERETT ST.)

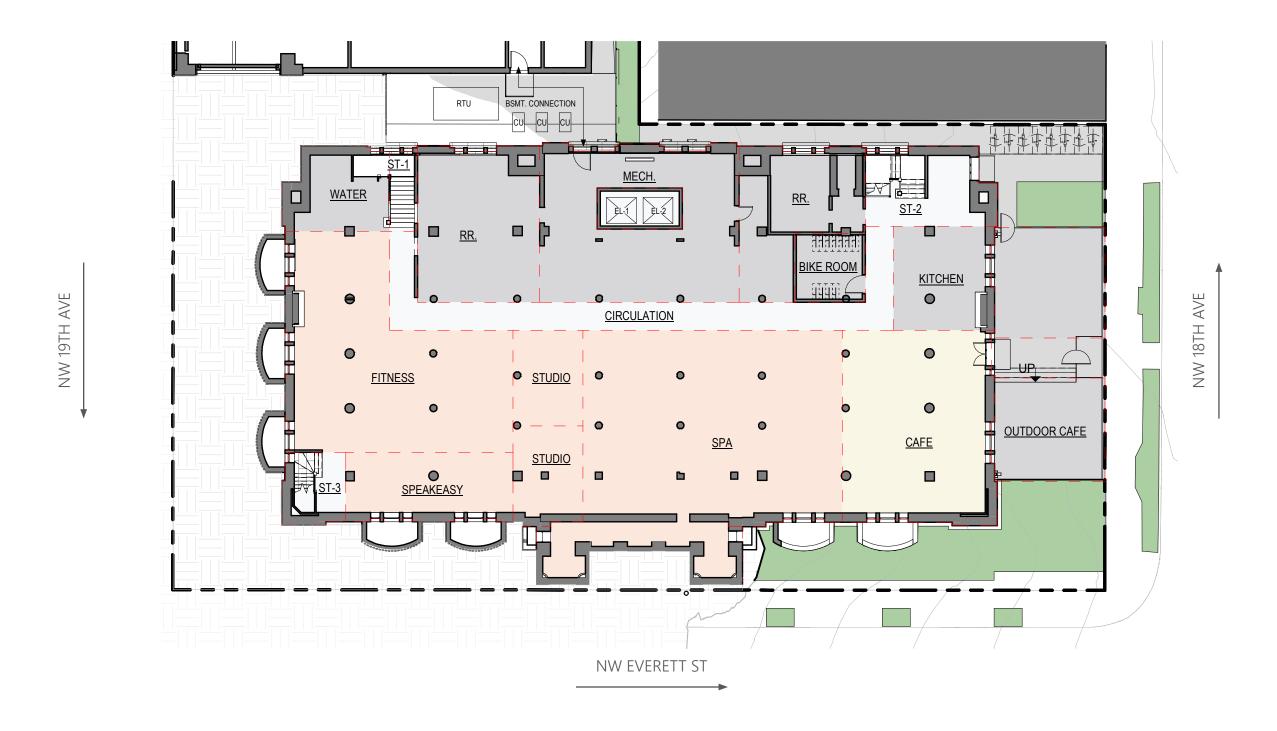
| SCALE: 1" = 64'-0"













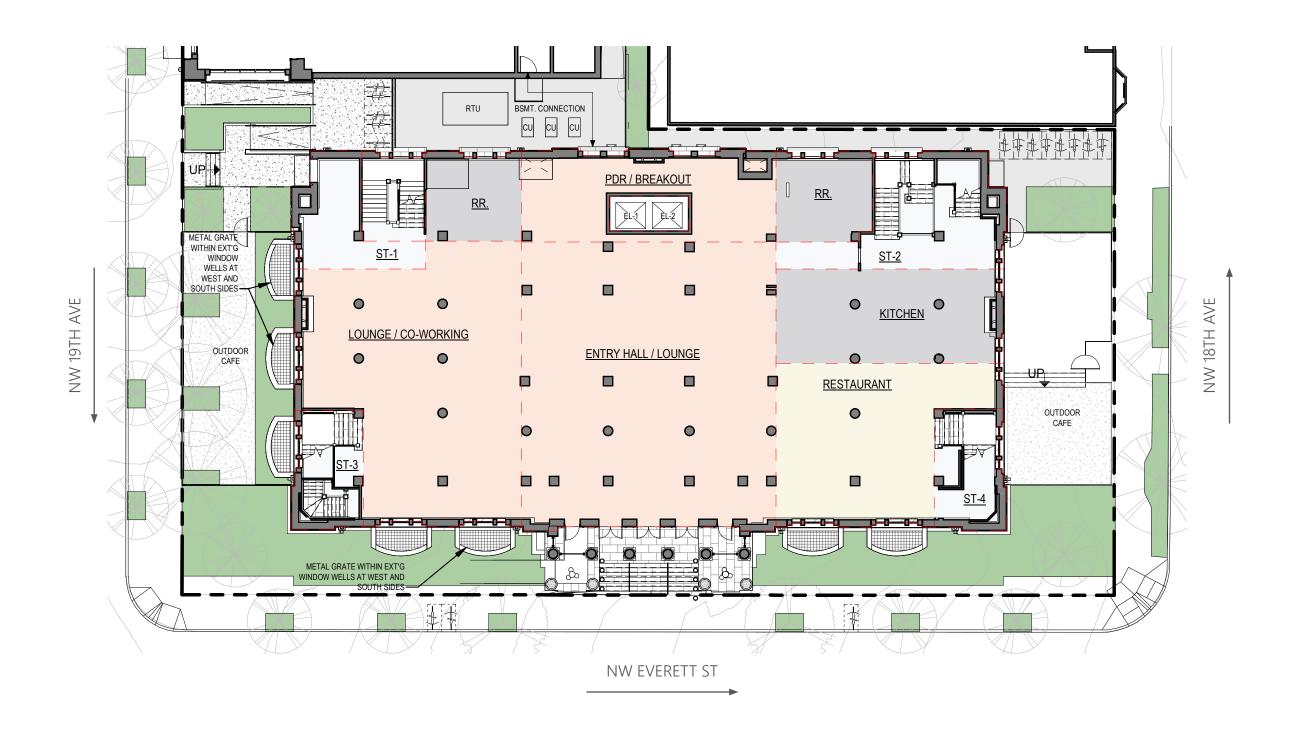
| SCALE: 1" = 20-0"











FIRST FLOOR PLAN (1819 NW EVERETT ST.)

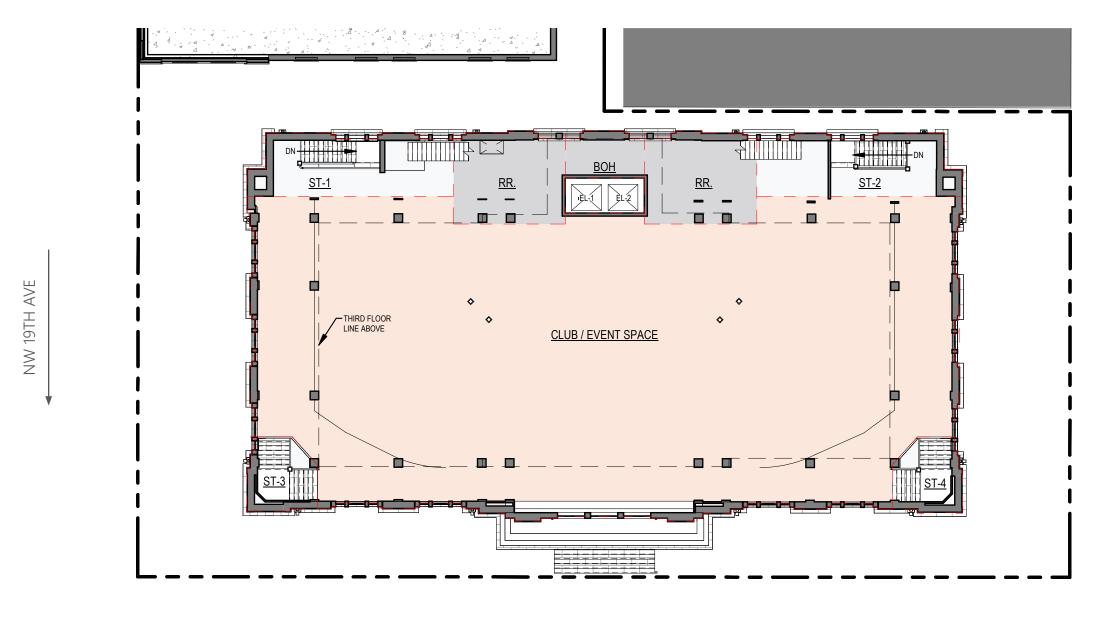
| SCALE: 1" = 20-0"











NW EVERETT ST

SECOND FLOOR PLAN (1819 NW EVERETT ST.)

| SCALE: 1" = 20-0"







NW 18TH AVE

NW EVERETT ST

## THIRD FLOOR PLAN (1819 NW EVERETT ST.)

NW 19TH AVE

| SCALE: 1" = 20-0"









NW 18TH AVE

NW 18TH AVE

NW EVERETT ST

# ROOF PLAN (1819 NW EVERETT ST.)

| SCALE: 1" = 20-0"

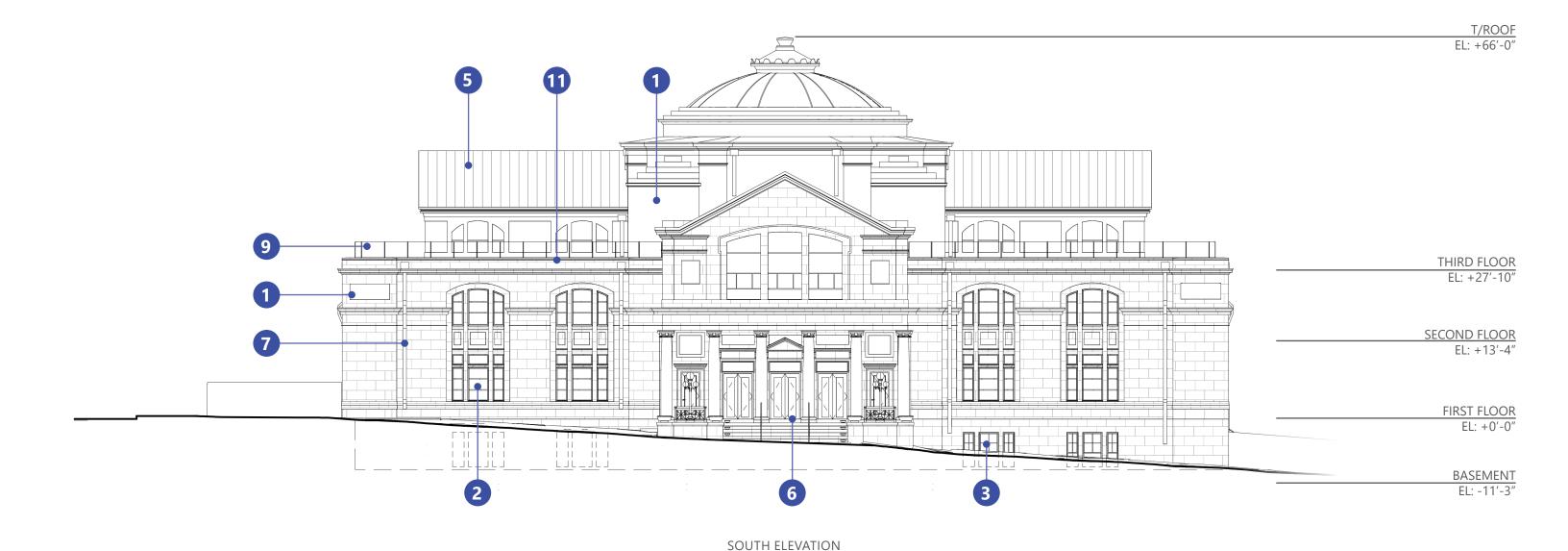


- EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
- 2 EXISTING STEEL WINDOW WITH LEADED GLASS (SEE WINDOW SCOPE PAGE FOR DTLS)
  - EXISTING WOOD WINDOW FOR REPAIR AND PAINT
- 4 EXISTING METAL CLADDING TO REMAIN; REPAIR AND PAINT TO MATCH EXISTING
- 5 EXISTING COATED TIN ROOFING TO BE REPAIRED AND/OR REPLACED IN KIND

NEW METAL COPING CAP FOR PAINT TO MATCH WALL

- 6 EXISTING WOOD DOOR FOR REPAIR AND STAIN
- NEW METAL DOOR FOR PAINT TO MATCH WALL

- EXISTING SCUPPER AND DOWNSPOUT TO REMAIN FOR PAINT
- NEW FABRIC AWNING IN EXISTING LOCATION
- 9 NEW FRAMELESS GLASS GAURDRAIL, CLEAR LOW-IRON GLASS
- 10 NEW STUCCO RAINSCREEN



BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"







A.6 LU 22-204531 HRM

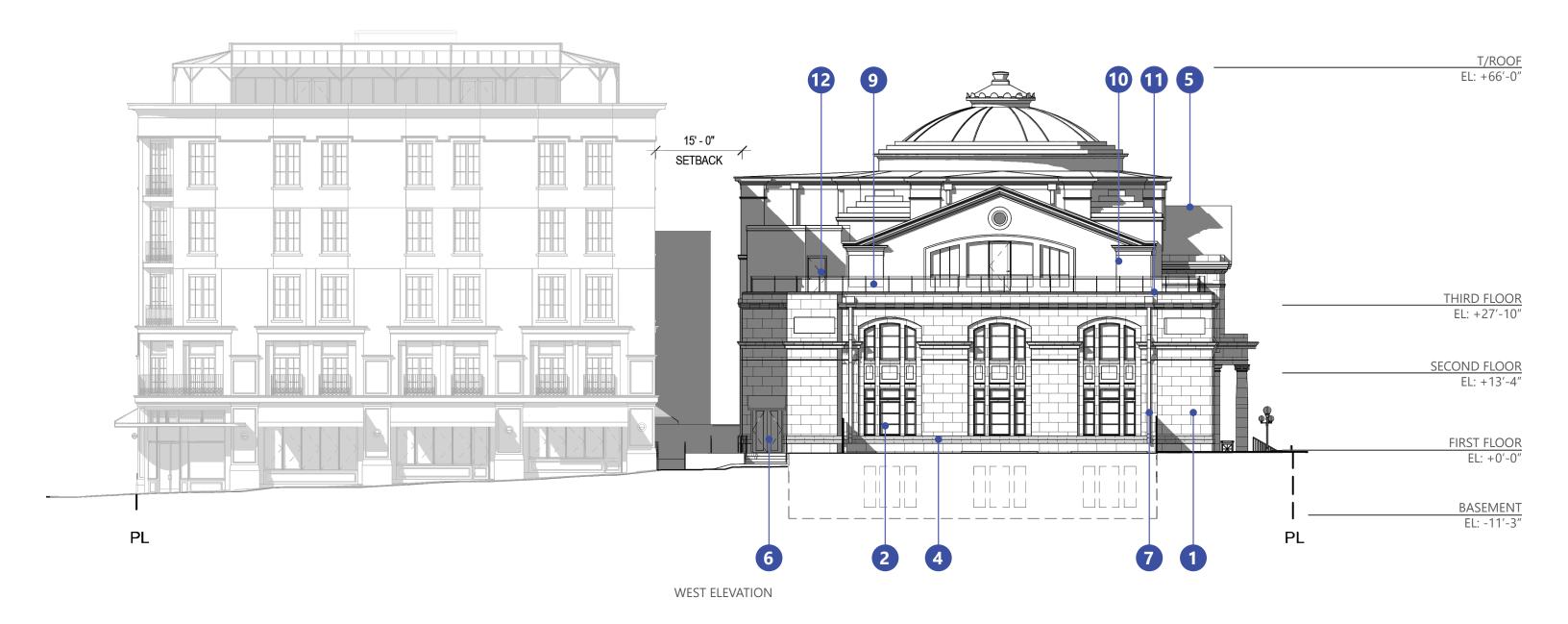
AREA FOR CITY STAMP

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### BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





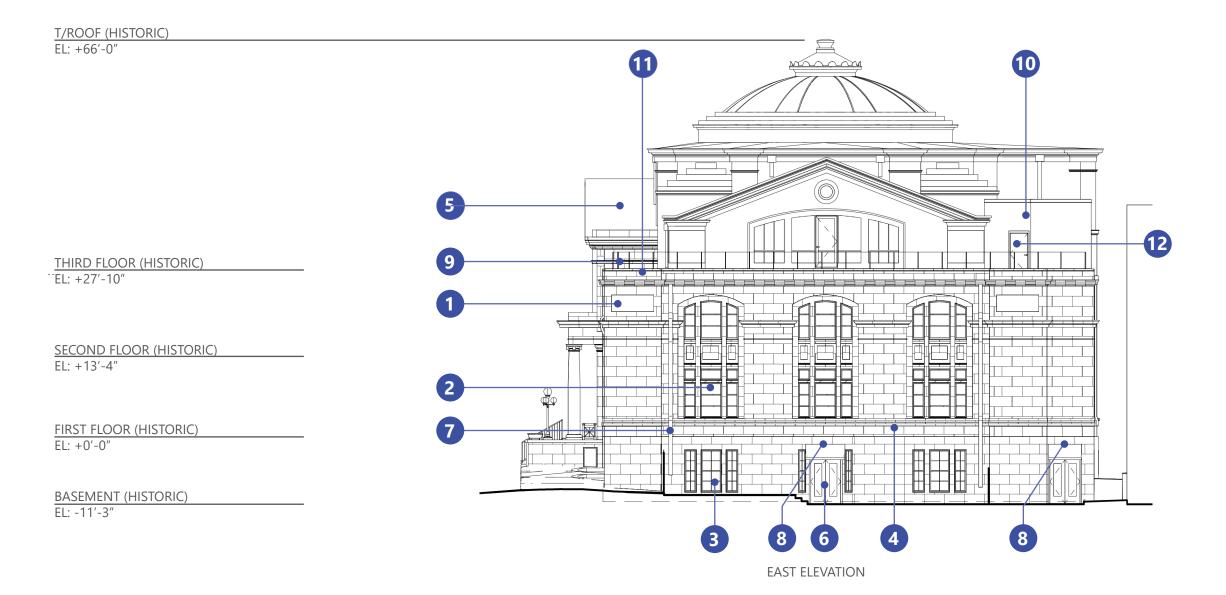
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

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### BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)

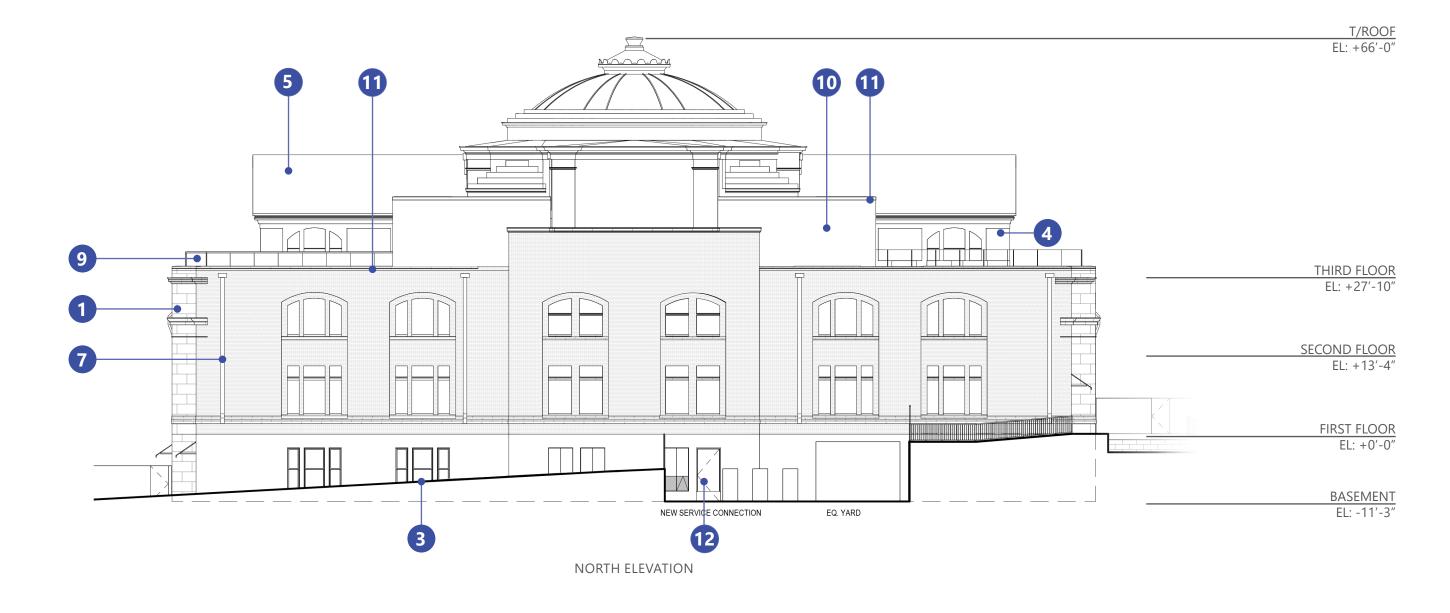




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  - NEW METAL COPING CAP FOR PAINT TO MATCH WALL

AREA FOR CITY STAMP



BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)



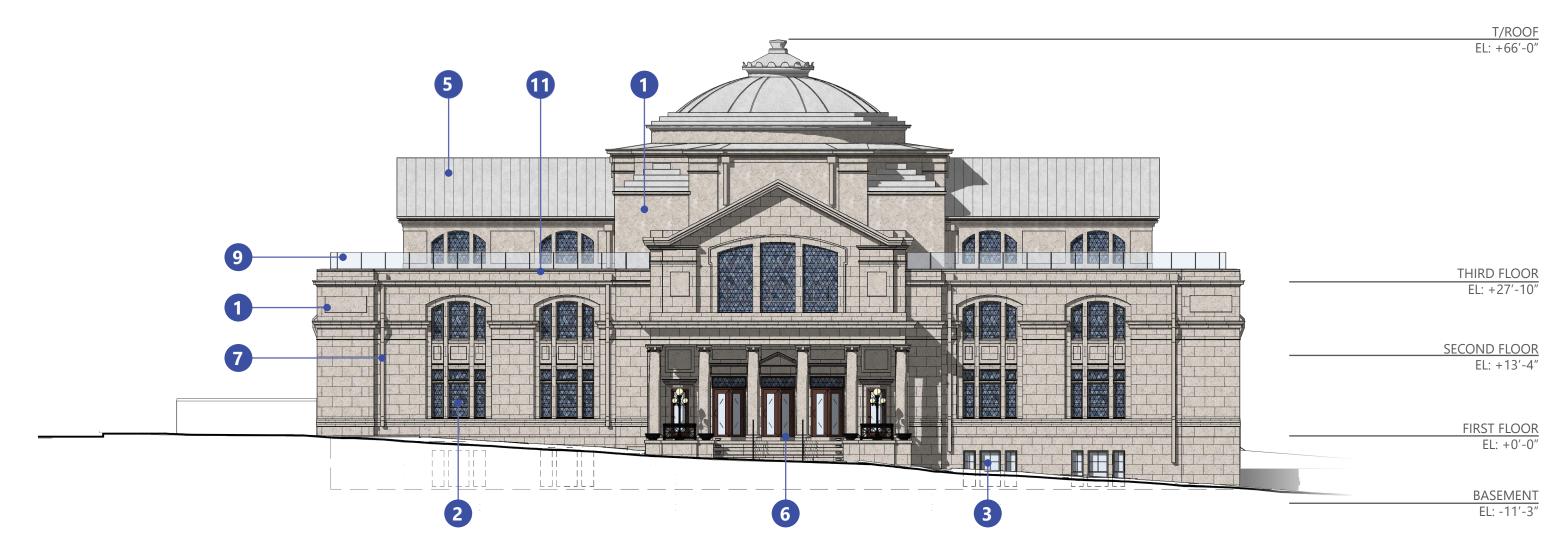


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SOUTH ELEVATION

COLOR ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





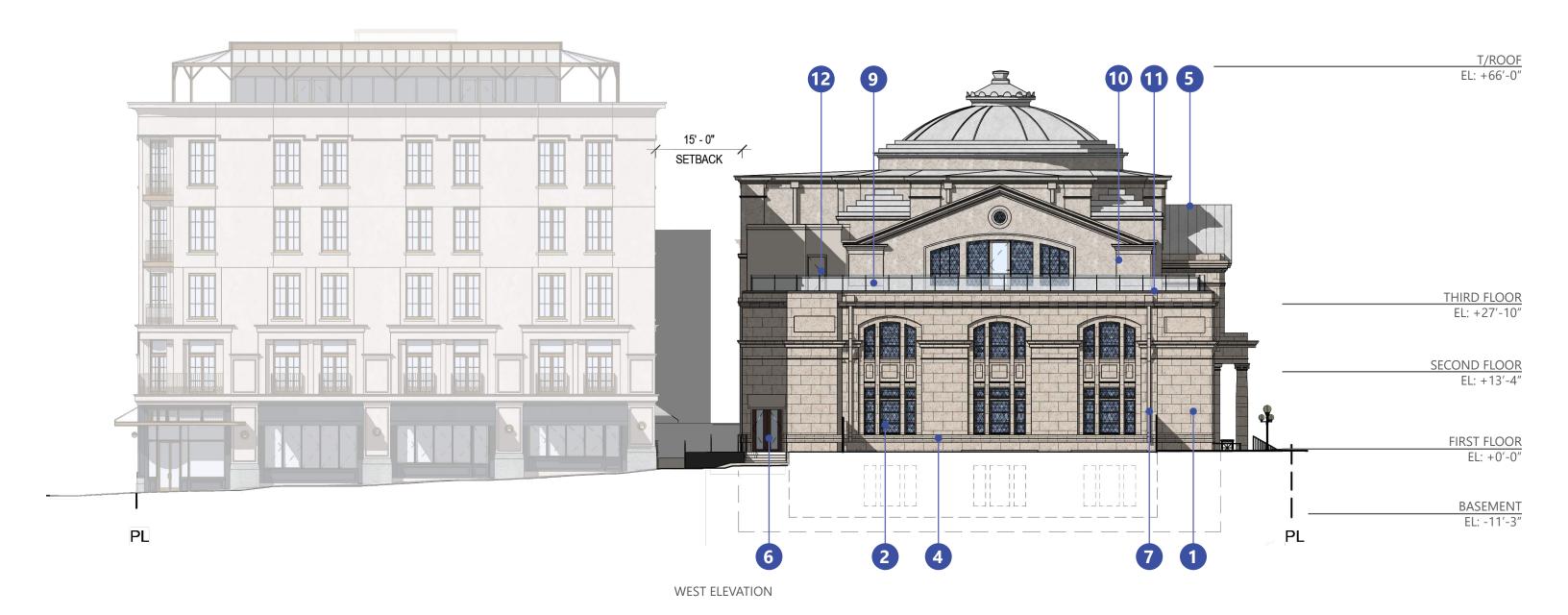
AREA FOR CITY STAMP

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- LESS GLASS GAURDRAIL, 10 NEW STUCCO RAINSCREEN



COLOR ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"







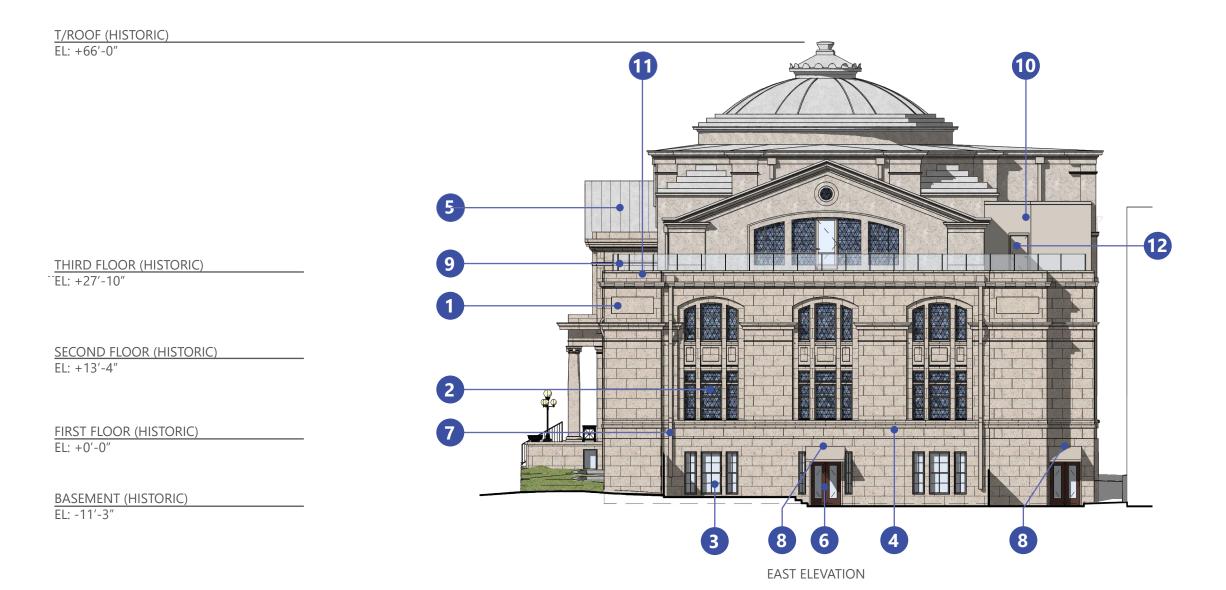
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

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- 8 NEW FABRIC AWNING IN EXISTING LOCATION
- NEW FRAMELESS GLASS GAURDRAIL, 9 CLEAR LOW-IRON GLASS
- 10 NEW STUCCO RAINSCREEN



COLOR ELEVATIONS (1819 NW EVERETT ST.)

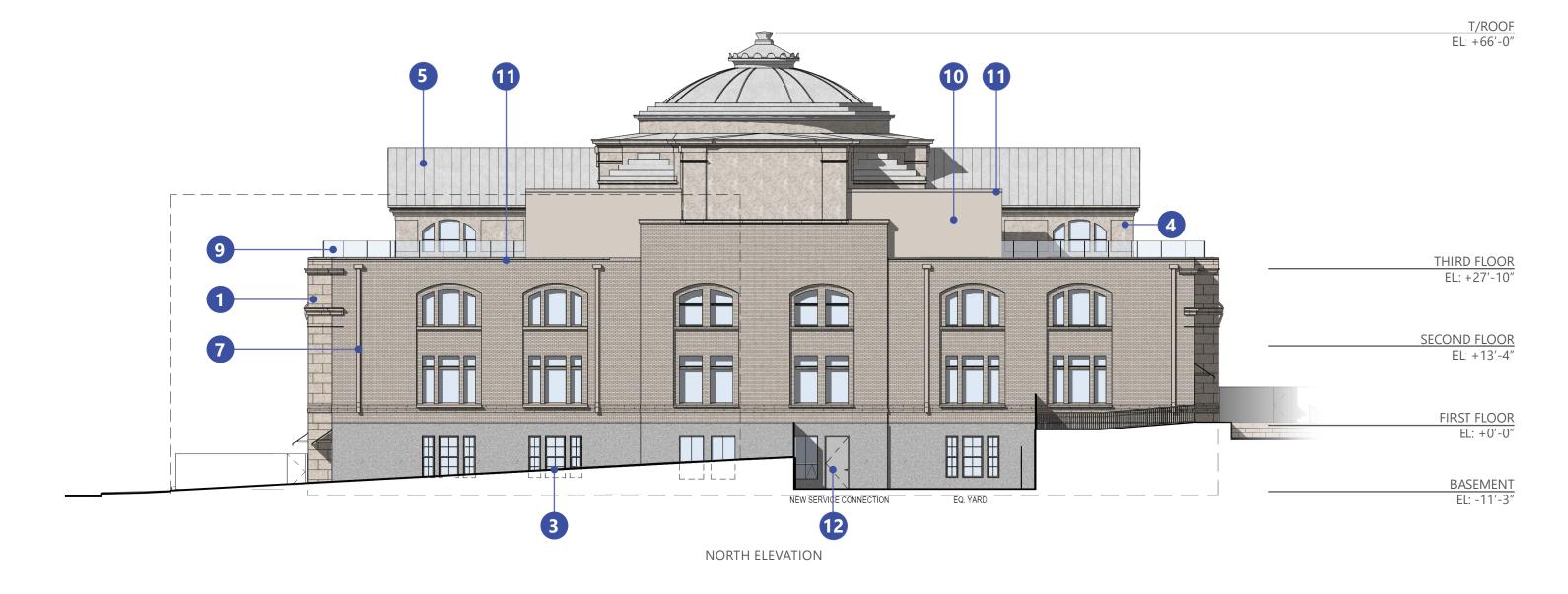






- 1 EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
- IN, 2 EXISTING STEEL WINDOW WITH LEADED GLASS (SEE WINDOW SCOPE PAGE FOR DTLS)
- ASS **3** EXISTING WOOD WINDOW FOR REPAIR AND PAINT
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- EXISTING WOOD DOOR FOR REPAIR AND STAIN
- NEW METAL COPING CAP FOR PAINT 12 NEW METAL DOOR FOR PAINT TO MATCH WALL

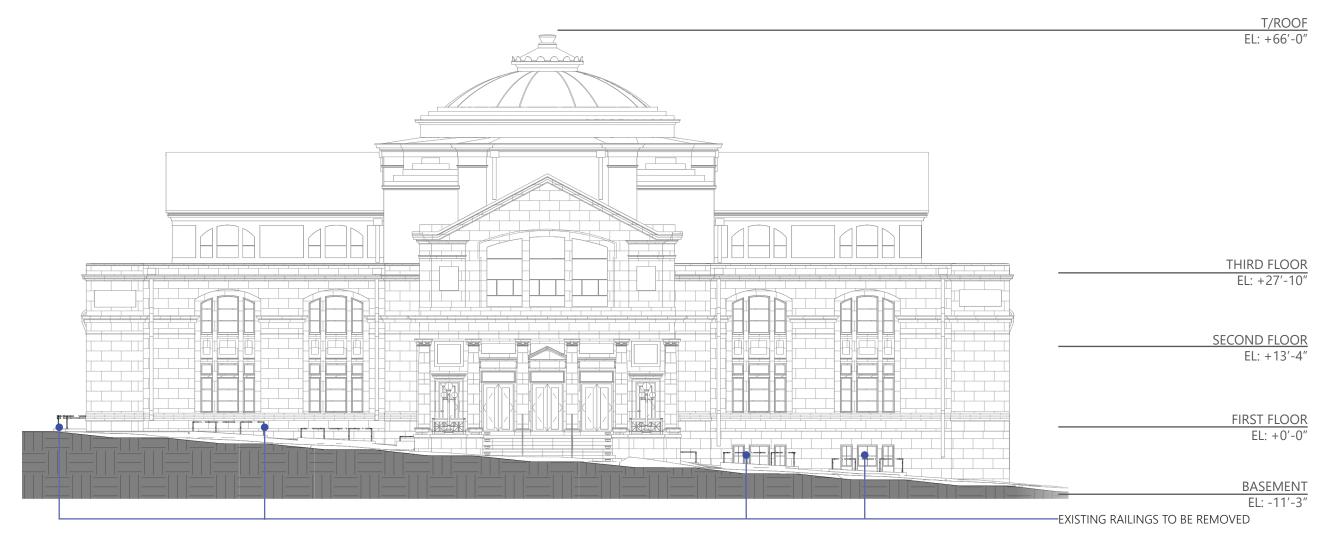
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COLOR ELEVATIONS (1819 NW EVERETT ST.)







SOUTH ELEVATION

# DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)

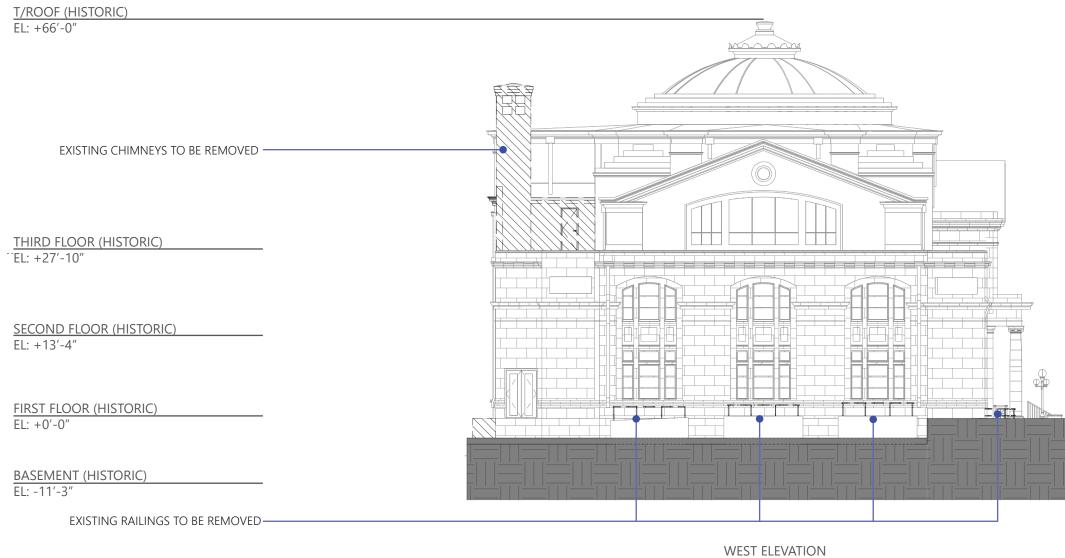
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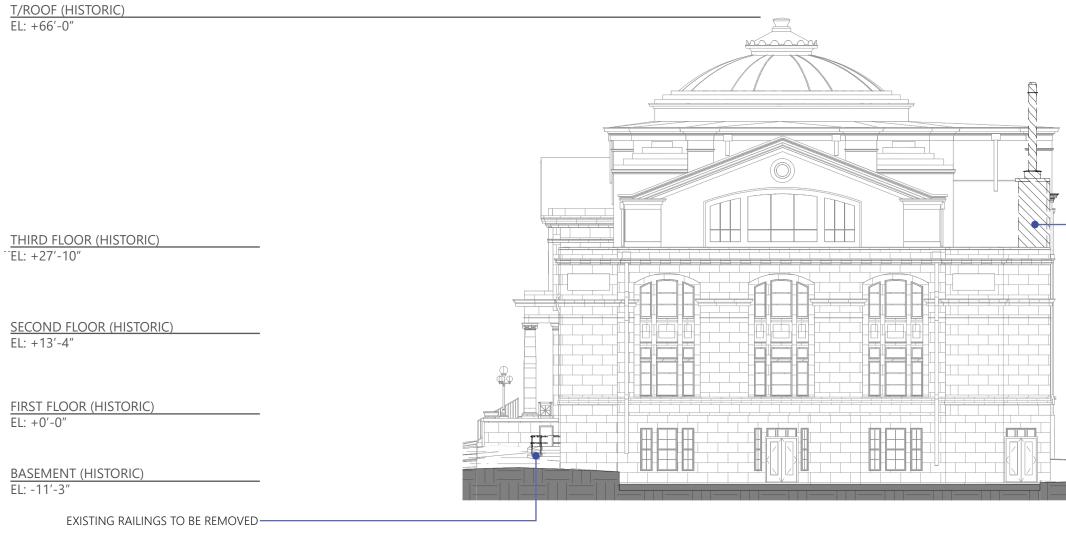
AREA FOR CITY STAMP



# DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)







EXISTING CHIMNEYS TO BE REMOVED

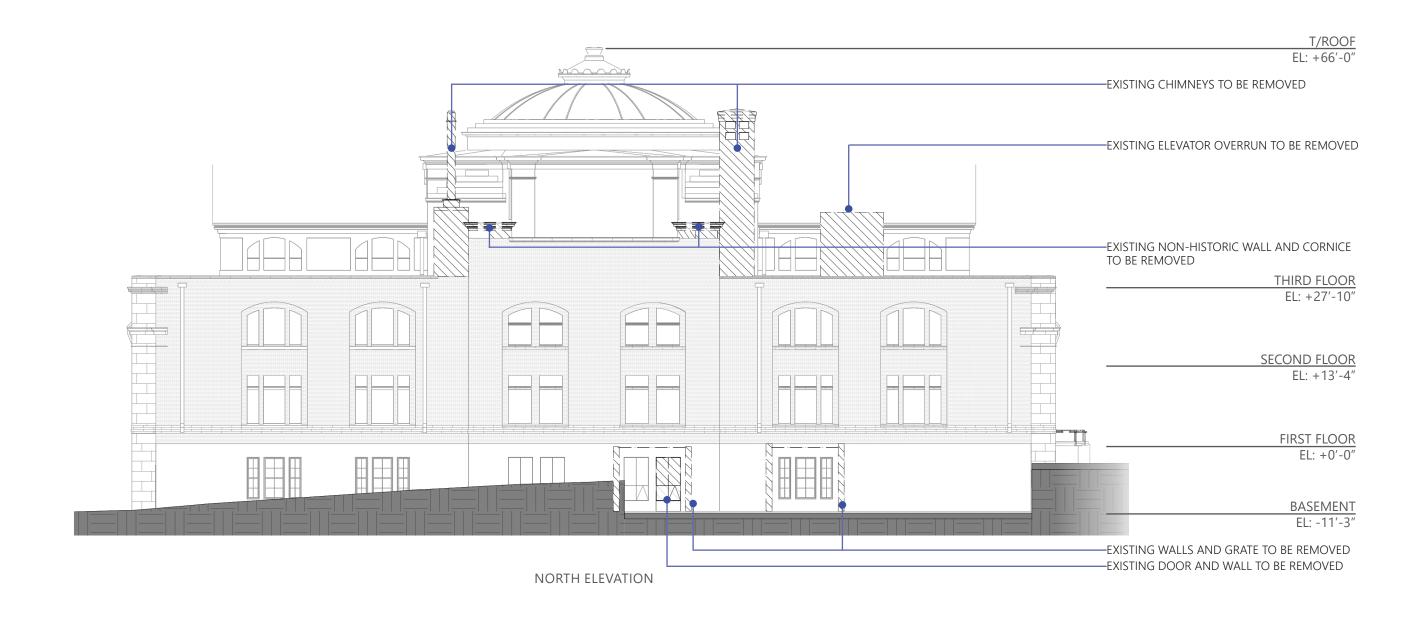
EAST ELEVATION

# DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)

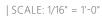








# DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)









**EXISTING NORTHEAST** CHIMNEY - PROPOSED TO BE REMOVED AND REPLACED WITH STAIR ADDITION



**EXISTING NORTHWEST** REMOVED AND REPLACED WITH STAIR ADDITION

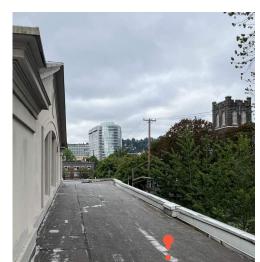


EXISTING, NON-HISTORIC, EXISTING NORTHWEST

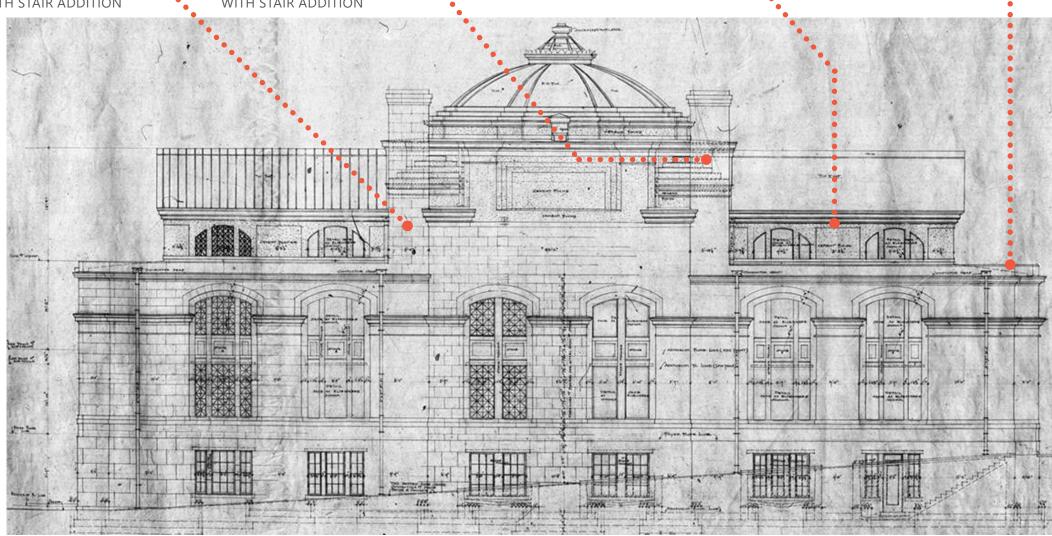
• EXISTING, NON-HISTORIC,

CHIMNEY - PROPOSED TO BE

• ELEVATOR OVERRUN TO BE DEMOLISHED



12" HIGH EXISTING PARAPET CONDITION



ORIGINAL NORTH ELEVATION DRAWING

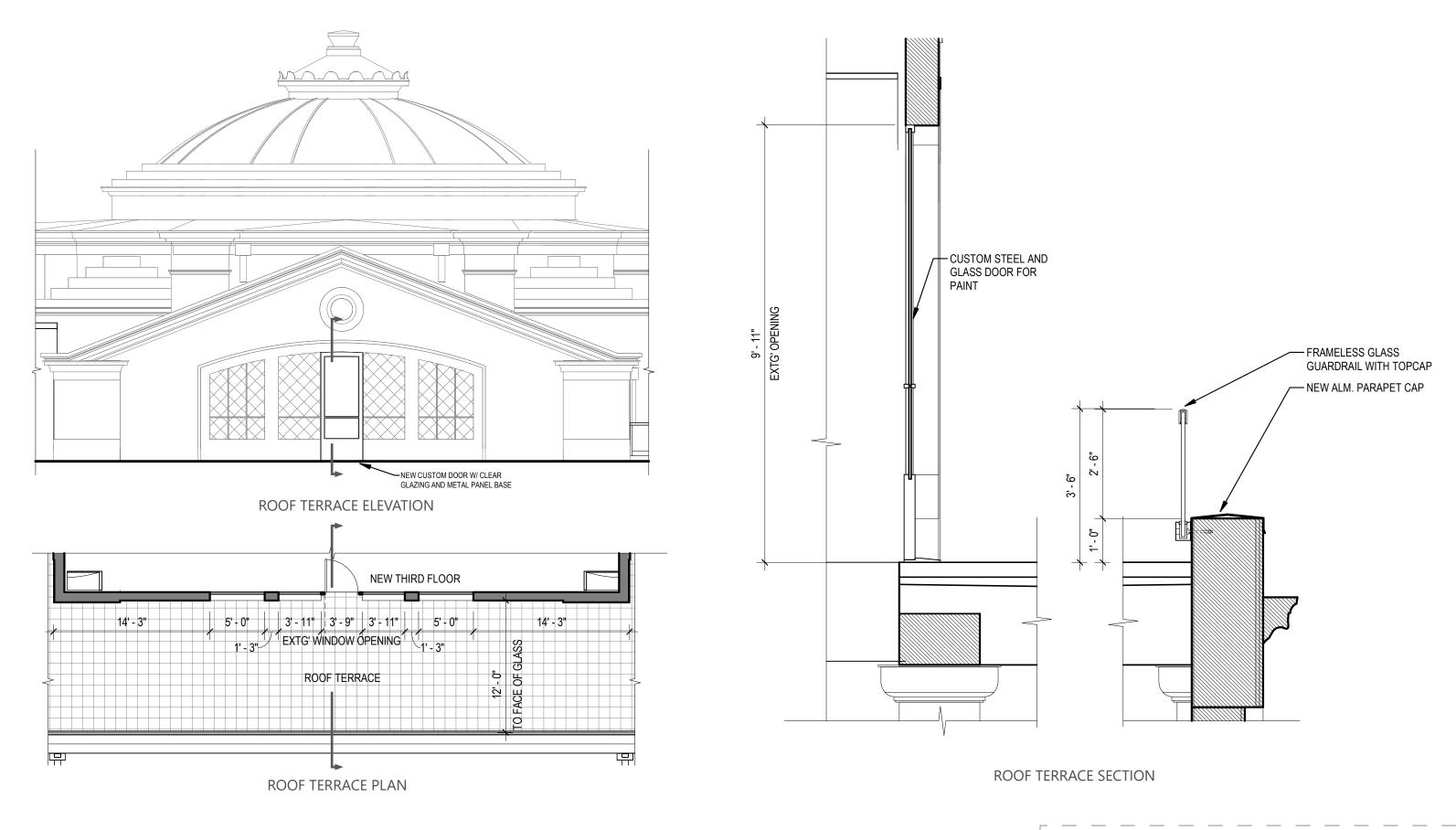
### EXISTING CONDITIONS (1819 NW EVERETT ST.)

| SCALE: NTS









# DETAILS (1819 NW EVERETT ST.)

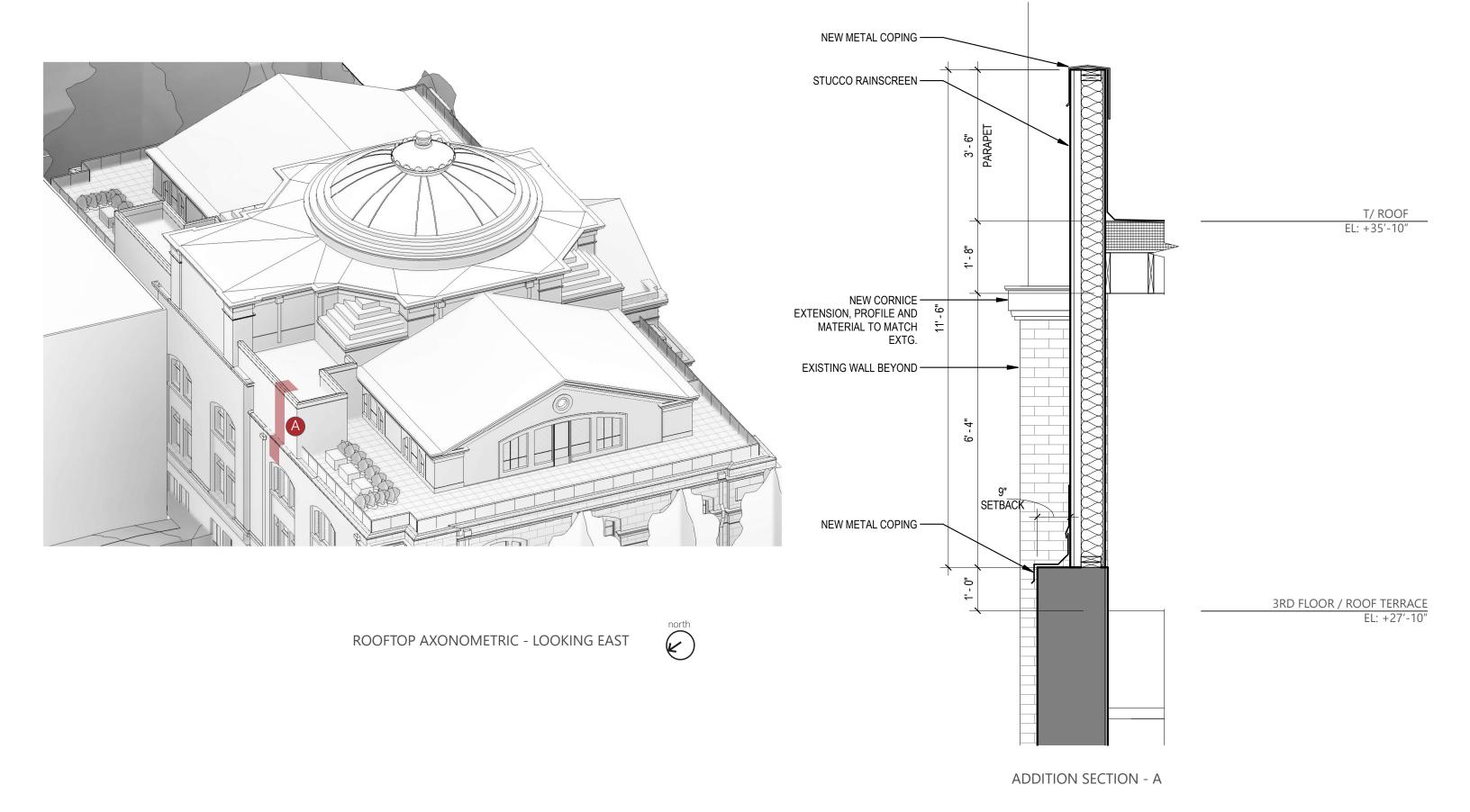
| SCALE: 1/8" = 1'-0" & 1/2" = 1'-0"







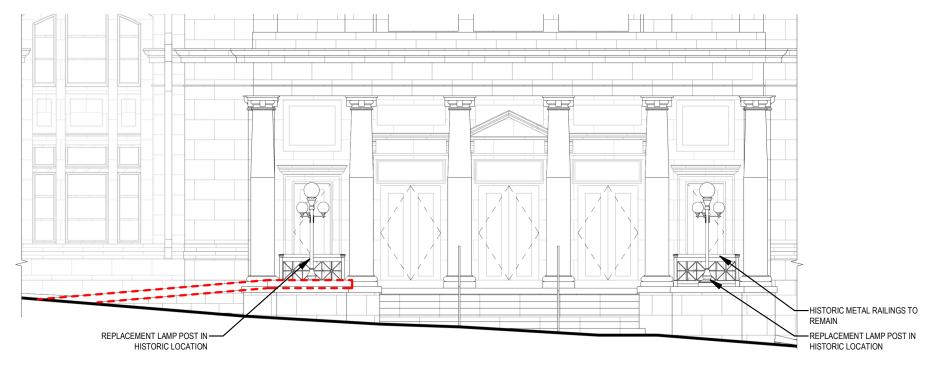




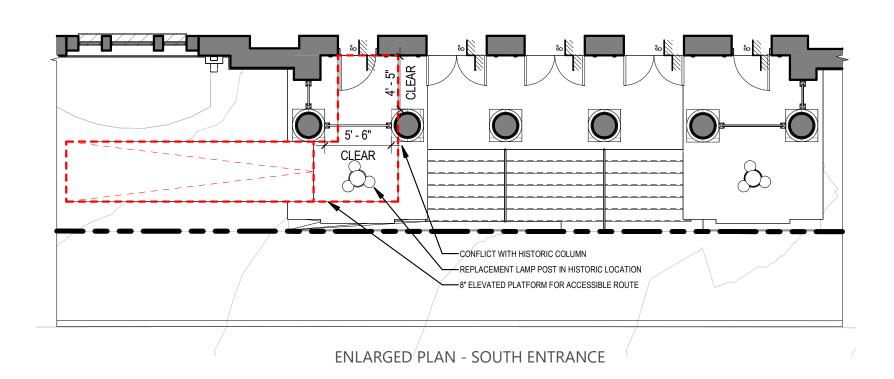
# ROOFTOP ADDITION (1819 NW EVERETT ST.)







MAIN ENTRANCE ELEVATION





| SCALE: 1/8" = 1'-0"





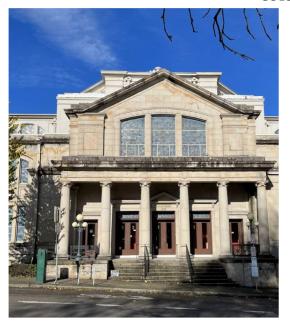




8" STEP TO ENTRY DOORS



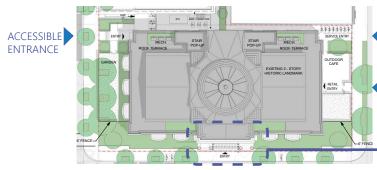
CLEAR BETWEEN EXTERIOR WALL AND COLUMN BASE



**EXISTING ENTRANCE CONDITIONS** 



HISTORIC ENTRANCE CONDITIONS

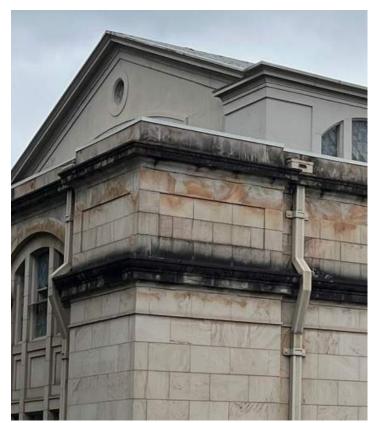


ACCESSIBLE ENTRANCE

ACCESSIBLE ENTRANCE

SEE ENLARGED SOUTH ENTRANCE PLAN





**EXISTING FACADE CONDITION** 



NORTH WINDOW BAY/FACADE MATERIAL



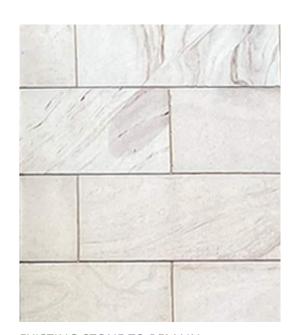
EXISTING METAL SHEET CLADDING OVER HISTORIC CEMENTITIOUS FINISH



WINDOW CONDITION EXAMPLE



ENLARGED WINDOW DETAIL



EXISTING STONE TO REMAIN





STAIR TOWER ADDITIONS - STUCCO AND METAL COPING

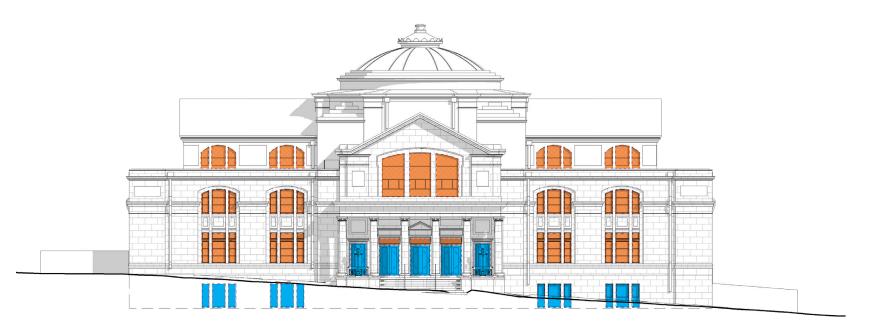
# EXTERIOR MATERIALS (1819 NW EVERETT ST.)

| SCALE: NTS

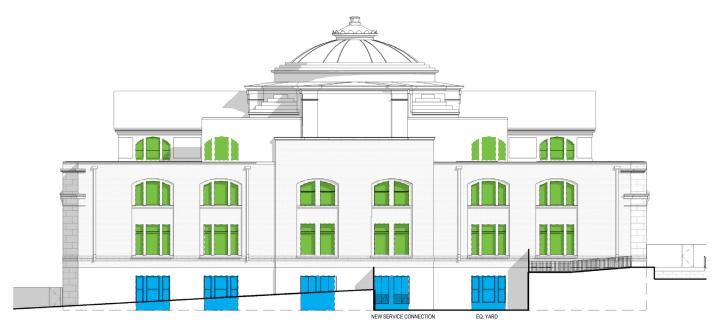








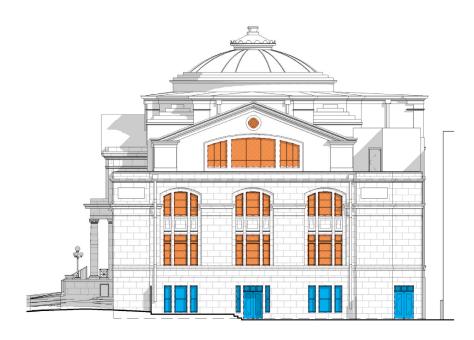
### SOUTH ELEVATION



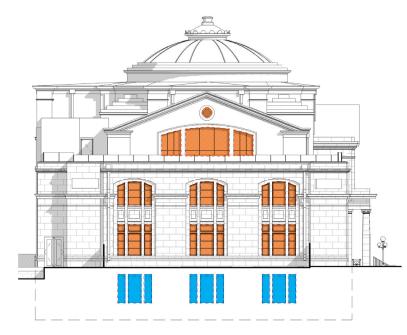
NORTH ELEVATION







#### EAST ELEVATION



WEST ELEVATION

EXISTING GLASS TO BE REPLACED WITH SEMI-TRANSPARENT PRIVACY GLASS WITHOUT GEOMETRIC PATTERN. FRAME AND MULLIONS TO BE REPLACED WITH NEW IN HISTORIC CONFIGURATION

### WINDOW REPAIR AND REPLACEMENT SCOPE (1819 NW EVERETT ST.)

| SCALE: NTS







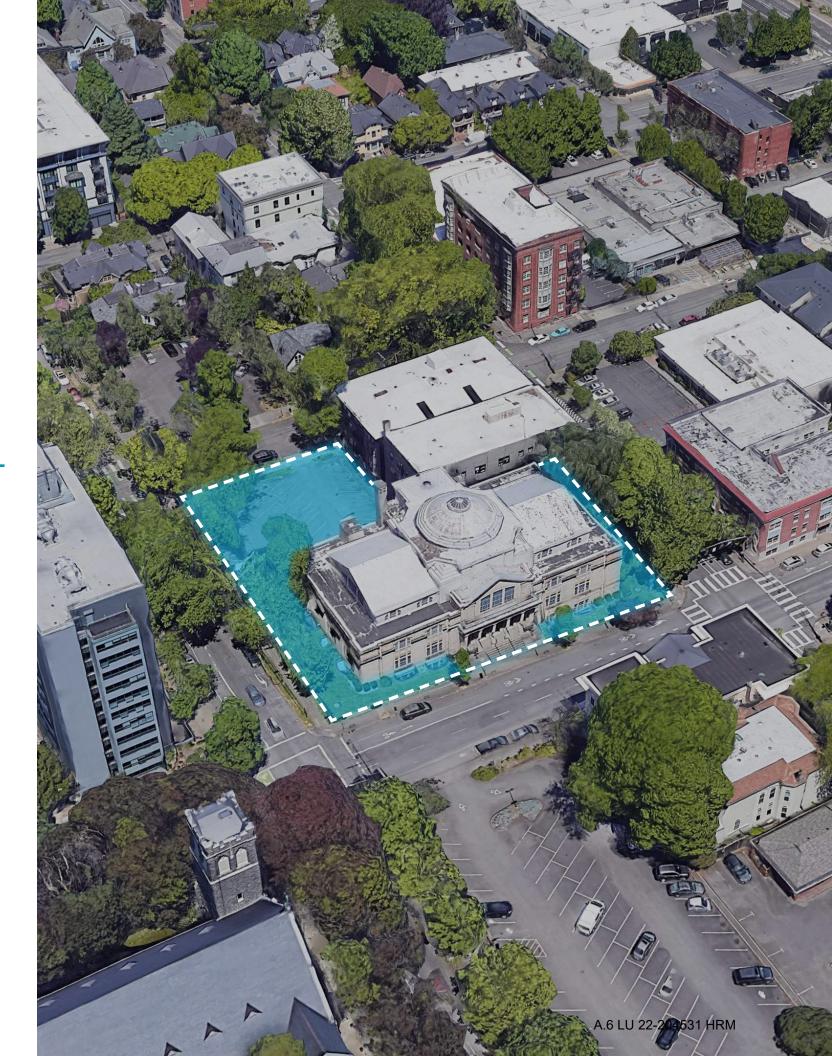
# TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

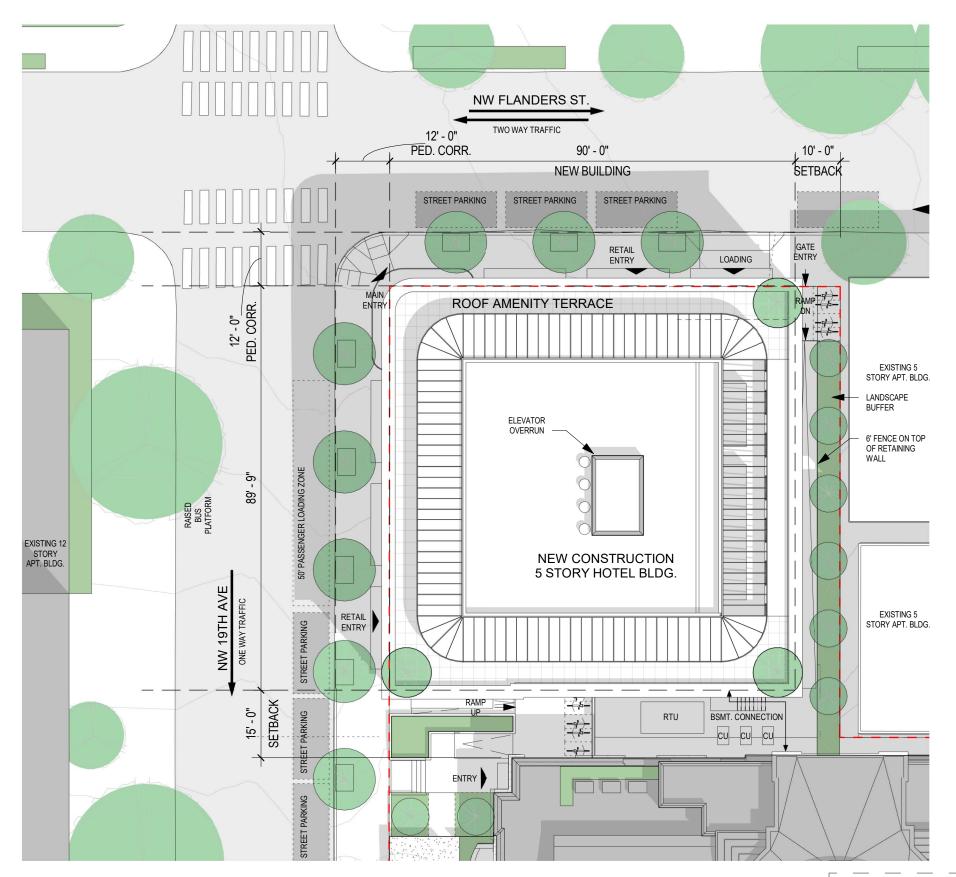
1880 NW FLANDERS STREET

New Construction - Drawings

March 13th, 2023







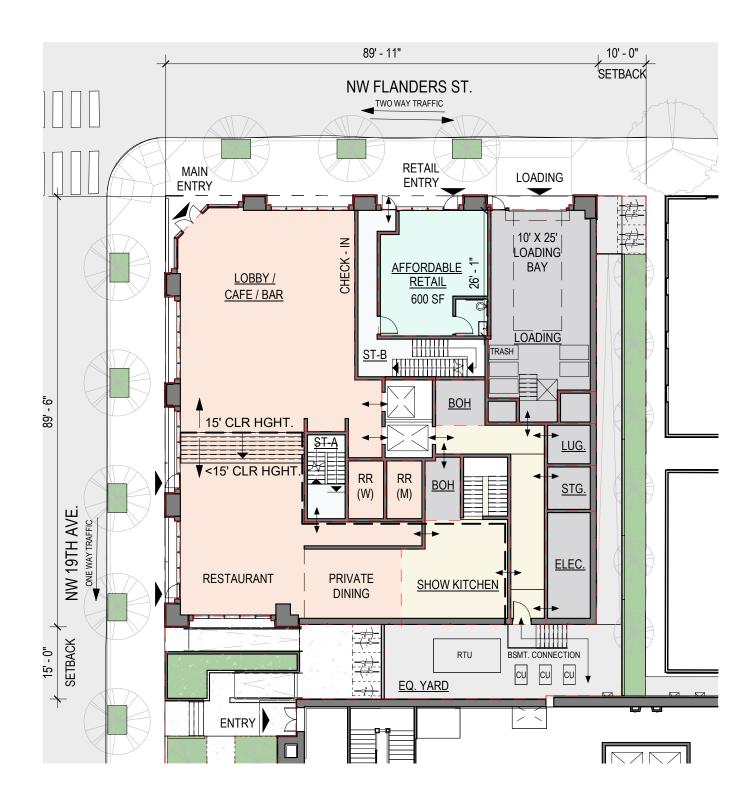


| SCALE: 1" = 64'-0"









07 05 12'-0" x 22'-5" 03 04 06 13'-10" x 22'-5" ELEC./HK¬\ GUESTROOMS— ST-B 08 20 19 22'-0" x 11'-1" 9 22'-0" x 11'-1" ST-A 10 18 11 17 15 14 12'-0" x 22'-0" 13 12 16 15' - 0" SETBACK

89' - 9"

TYPICAL FLOOR PLAN

### FLOOR PLANS (1880 NW FLANDERS ST.)

| SCALE: 1" = 20'-0"





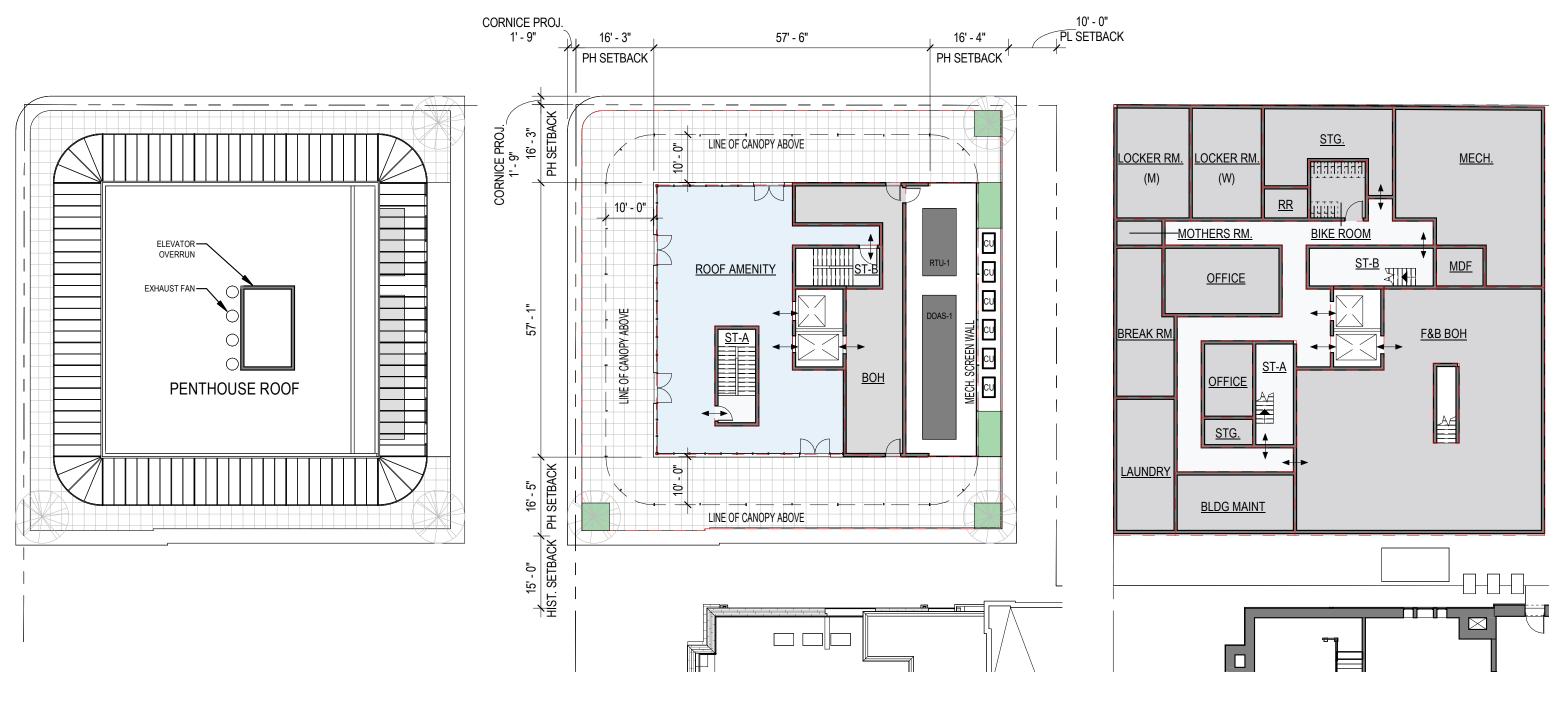
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

FIRST FLOOR PLAN



A.6 LU 22-204531 HRM

10' - 0" SETBACK



PENTHOUSE ROOF PLAN **ROOF PLAN BASEMENT PLAN** 

# FLOOR PLANS (1880 NW FLANDERS ST.)

| SCALE: 1" = 20'-0"











2 ALM. WINDOW, BRONZE **FINISH COLOR** 















10 ALM. COPING CAP FOR PAINT TO MATCH WALL

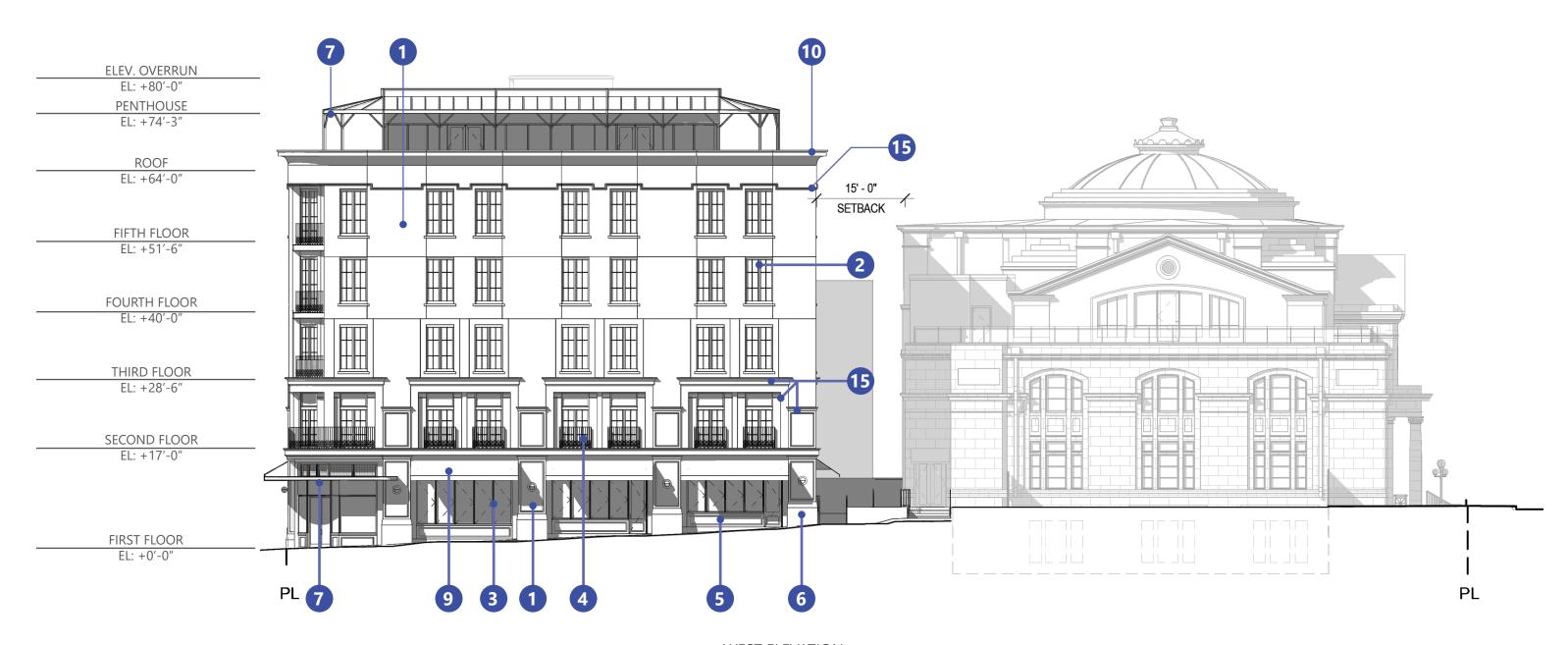
MTL DOOR FOR PAINT TO MATCH WALL

OVERHEAD DOOR W/ TRANSLUCENT GLAZING

MATCH STUCCO

METAL PANEL MECH. SCREEN
WALL, MATTE PAINT FINISH TO 14 PAINT , COLOR TO MATCH STUCCO

DECORATIVE STUCCO 16 ALM. STOREFRONT TO MOULDING TRIM



**WEST ELEVATION** 

### BLACK AND WHITE ELEVATIONS (1880 NW FLANDERS ST.)







- STUCCO RAINSCREEN
- ALM. COPING CAP FOR PAINT TO MATCH WALL
  - MTL DOOR FOR PAINT TO MATCH WALL
- ALM. OPERABLE STOREFRONT, ALM. WINDOW, BRONZE BRONZE FINISH COLOR, W/ **FINISH COLOR DECORATIVE GLASS TRANSOM** 
  - OVERHEAD DOOR W/ TRANSLUCENT GLAZING
- CUSTOM ALM. GUARDRAIL, **BRONZE FINISH COLOR**
- WOOD PANELING FOR PAINT
  - STUCCO
- 6 GRANITE STONE VENEER
  - FINISH COLOR, W/ **GLASS ROOF**

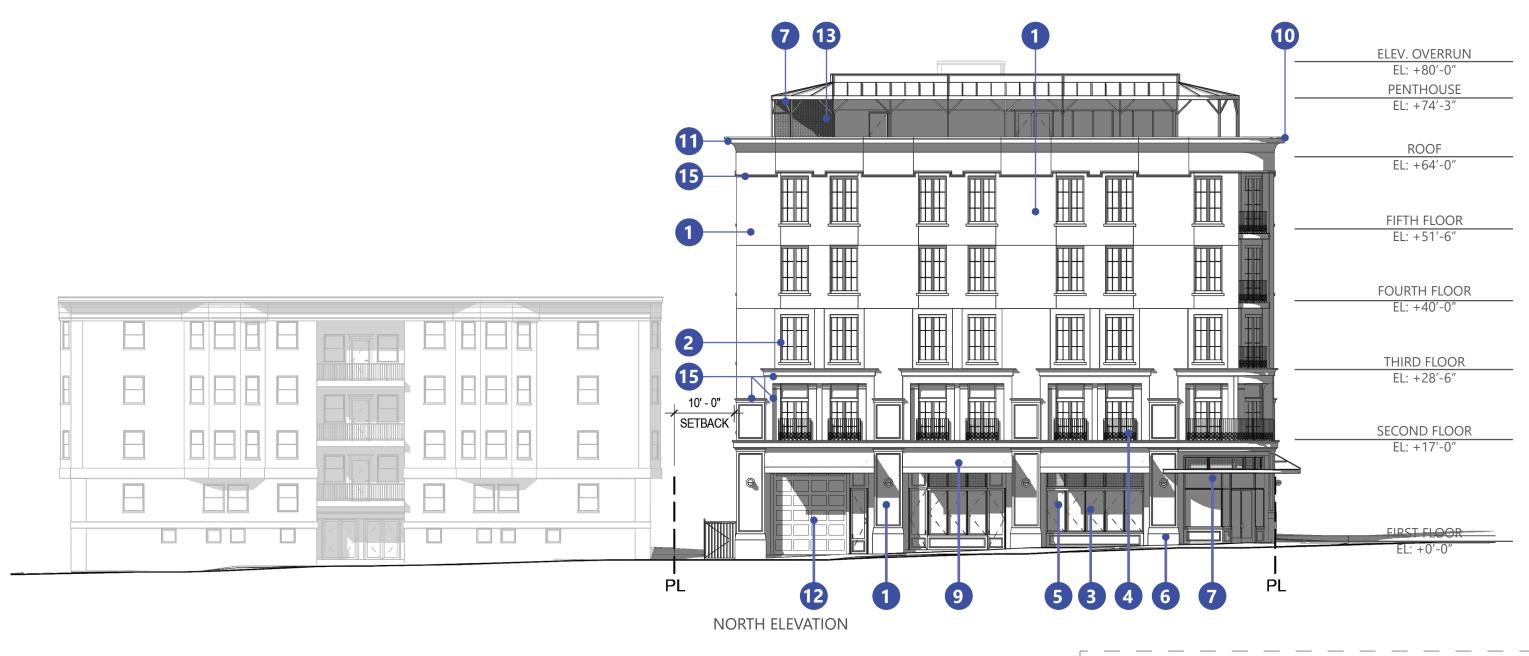




METAL PANEL MECH. SCREEN
WALL, MATTE PAINT FINISH TO 14 PAINT , COLOR TO MATCH MATCH STUCCO

DECORATIVE STUCCO 16 ALM. STOREFRONT TO MOULDING TRIM

AREA FOR CITY STAMP



BLACK AND WHITE ELEVATIONS (1880 NW FLANDERS ST.)







- STUCCO RAINSCREEN
- FINISH COLOR MTL DOOR FOR PAINT ALM. COPING CAP FOR PAINT TO MATCH WALL

TO MATCH WALL

- 2 ALM. WINDOW, BRONZE ALM. OPERABLE STOREFRONT, BRONZE FINISH COLOR, W/ **DECORATIVE GLASS TRANSOM** 
  - OVERHEAD DOOR W/ TRANSLUCENT GLAZING
- CUSTOM ALM. GUARDRAIL, **BRONZE FINISH COLOR**

MATCH STUCCO

- WOOD PANELING FOR PAINT
- METAL PANEL MECH. SCREEN
  WALL, MATTE PAINT FINISH TO 14
  PAINT , COLOR TO MATCH STUCCO
- GRANITE STONE VENEER
- DECORATIVE STUCCO 16 ALM. STOREFRONT TO MOULDING TRIM

AREA FOR CITY STAMP

ALM. CANOPY, BRONZE 8 ALM. CLADDING, FINISH COLOR, W/ **GLASS ROOF** 





ELEV. OVERRUN	
EL: +80'-0"	
PENTHOUSE	
EL: +74'-3"	
ROOF	
EL: +64'-0"	
FIFTH FLOOR	

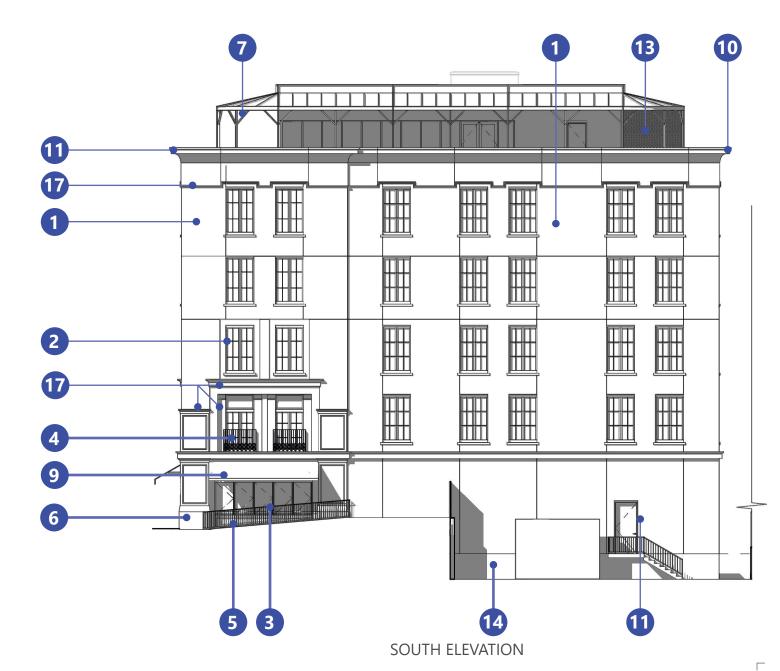
FOURTH FLOOR EL: +40'-0"

EL: +51'-6"

THIRD FLOOR EL: +28'-6"

SECOND FLOOR EL: +17'-0"

> FIRST FLOOR EL: +0'-0"



# BLACK AND WHITE ELEVATIONS (1880 NW FLANDERS ST.)







STUCCO RAINSCREEN

ALM. COPING CAP FOR PAINT TO MATCH WALL

- 2 ALM. WINDOW, BRONZE FINISH COLOR
- MTL DOOR FOR PAINT TO MATCH WALL
- BRONZE FINISH COLOR, W/ **DECORATIVE GLASS TRANSOM** 
  - OVERHEAD DOOR W/ TRANSLUCENT GLAZING

ALM. OPERABLE STOREFRONT,

CUSTOM ALM. GUARDRAIL, **BRONZE FINISH COLOR** 

MATCH STUCCO

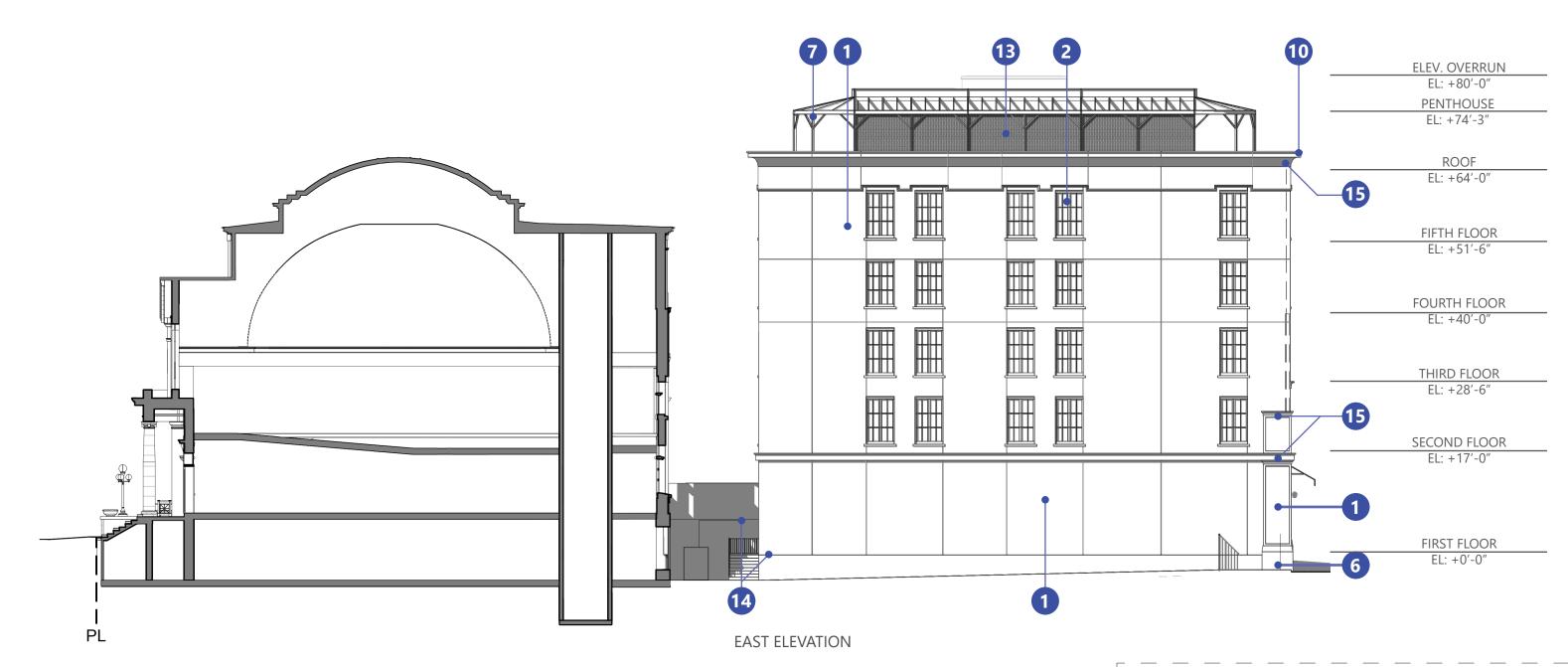
- WOOD PANELING FOR PAINT
- METAL PANEL MECH. SCREEN CONCRETE W/ ELASTOMERIC PAINT, COLOR TO MATCH STUCCO

  MATCH STUCCO

  CONCRETE W/ ELASTOMERIC PAINT, COLOR TO MATCH STUCCO
- GRANITE STONE VENEER
- DECORATIVE STUCCO 16 ALM. STOREFRONT TO MOULDING TRIM
- ALM. CANOPY, BRONZE 8 FINISH COLOR, W/ **GLASS ROOF**







BLACK AND WHITE ELEVATIONS (1880 NW FLANDERS ST.)

| SCALE: 1/16" = 1'-0"





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

A.6 LU 22-204531 HRM

STUCCO RAINSCREEN

2 ALM. WINDOW, BRONZE **FINISH COLOR** 

ALM. OPERABLE STOREFRONT, BRONZE FINISH COLOR, W/ **DECORATIVE GLASS TRANSOM** 



WOOD PANELING FOR PAINT

GRANITE STONE VENEER

ALM. CANOPY, BRONZE 8 **GLASS ROOF** 

ALM. CLADDING, ALM. CLADDING,
BRONZE FINISH COLOR

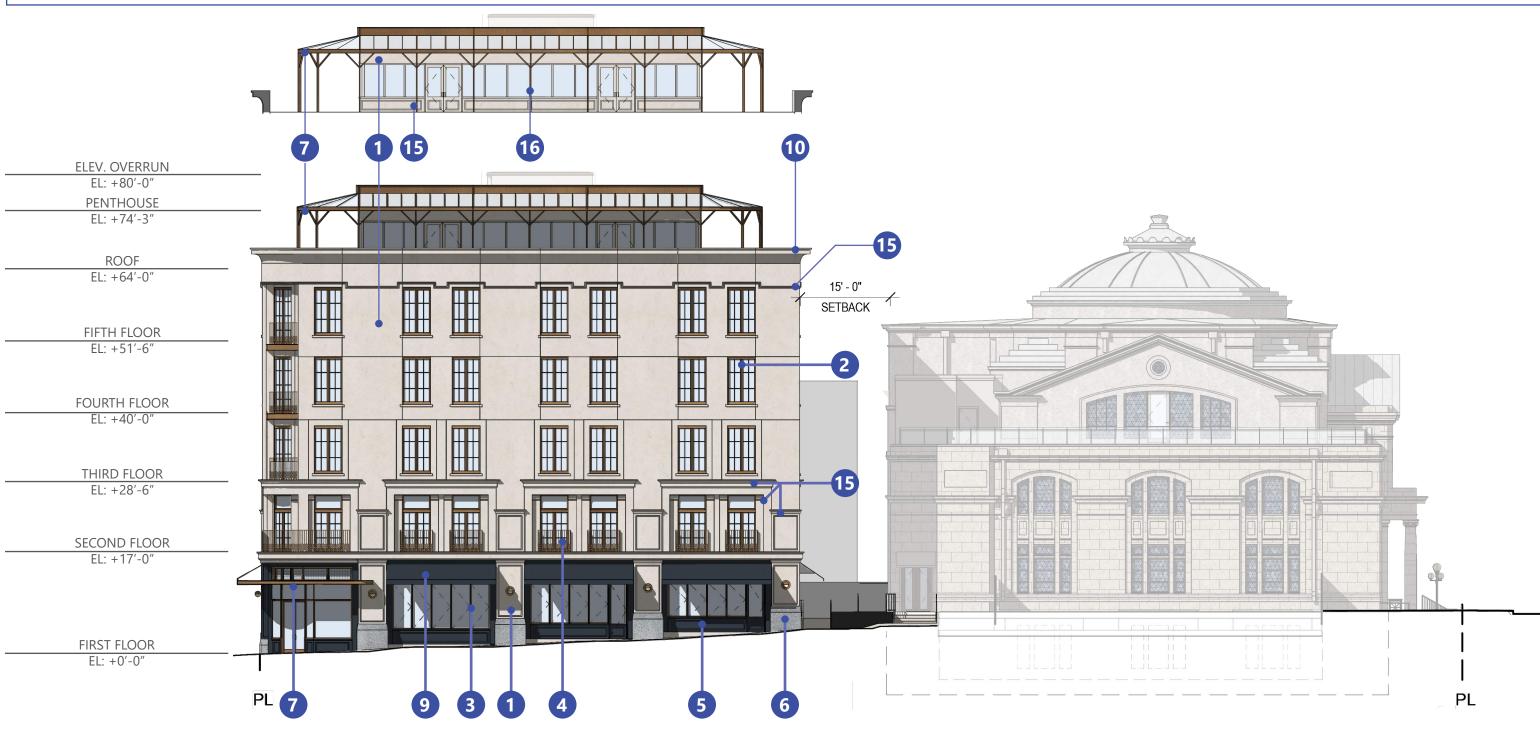
9
FABRIC
AWNING

10 ALM. COPING CAP FOR PAINT TO MATCH WALL

MTL DOOR FOR PAINT TO MATCH WALL

OVERHEAD DOOR W/ TRANSLUCENT GLAZING METAL PANEL MECH. SCREEN CONCRETE W/ ELASTOMERIC PAINT, COLOR TO MATCH STUCCO MATCH STUCCO

DECORATIVE STUCCO 16 ALM. STOREFRONT TO MOULDING TRIM



**WEST ELEVATION** 

COLOR ELEVATIONS (1880 NW FLANDERS ST.)

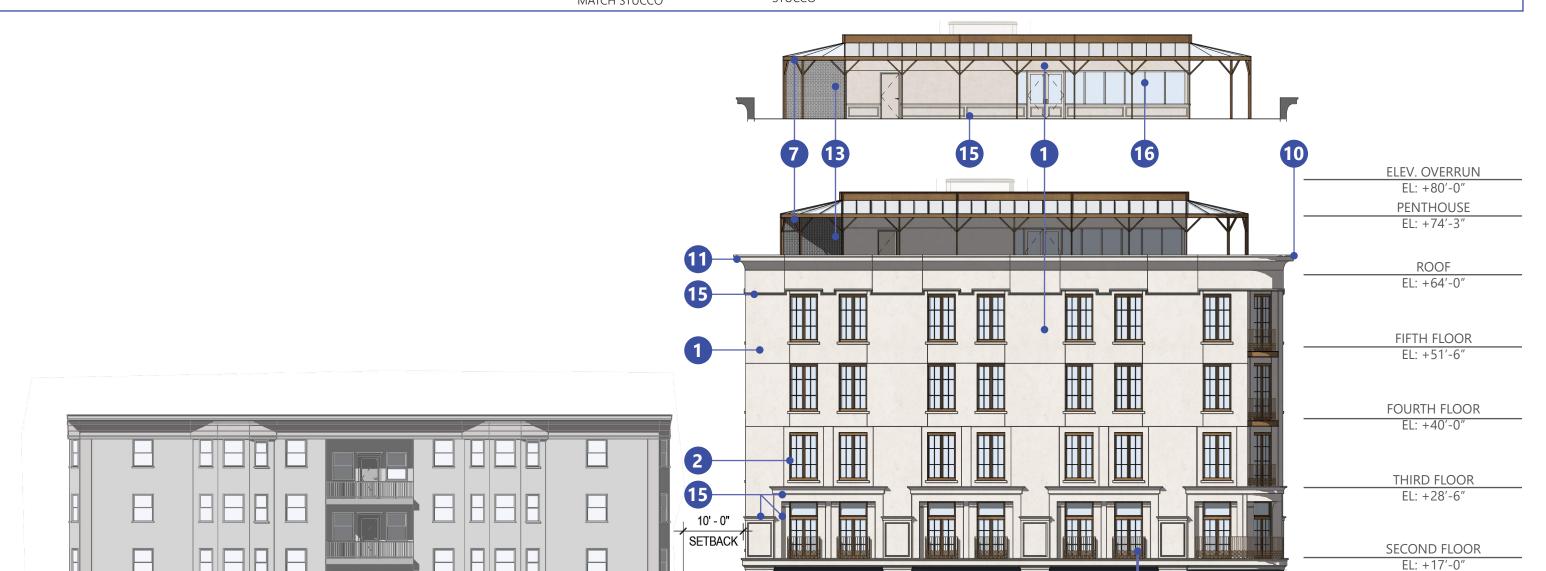




- STUCCO RAINSCREEN
- 2 ALM. WINDOW, BRONZE FINISH COLOR
- MTL DOOR FOR PAINT 10 ALM. COPING CAP FOR PAINT TO MATCH WALL TO MATCH WALL
- ALM. OPERABLE STOREFRONT, BRONZE FINISH COLOR, W/ **DECORATIVE GLASS TRANSOM**
- OVERHEAD DOOR W/ TRANSLUCENT GLAZING
- CUSTOM ALM. GUARDRAIL, **BRONZE FINISH COLOR**
- METAL PANEL MECH. SCREEN
  WALL, MATTE PAINT FINISH TO 14 PAINT , COLOR TO MATCH STUCCO MATCH STUCCO
- GRANITE STONE 5 WOOD PANELING FOR PAINT VENEER
  - DECORATIVE STUCCO 16 ALM. STOREFRONT TO MOULDING TRIM
- ALM. CANOPY, BRONZE FINISH COLOR, W/ **GLASS ROOF** 
  - ALM. CLADDING,
    BRONZE FINISH COLOR

    9
    FABRIC
    AWNING ALM. CLADDING,





0

PL

**NORTH ELEVATION** 

COLOR ELEVATIONS (1880 NW FLANDERS ST.)

| SCALE: 1/16" = 1'-0"









5 3 4

9

6

EL: +0'-0

PL











OVERHEAD DOOR W/ TRANSLUCENT GLAZING





WOOD PANELING FOR PAINT 6 GRANITE STONE VENEER

DECORATIVE STUCCO 16 ALM. STOREFRONT TO MOULDING TRIM

ALM. CANOPY, BRONZE 8 **GLASS ROOF** 

ALM. CLADDING,
BRONZE FINISH COLOR

9
FABRIC
AWNING ALM. CLADDING,



ELEV. OVERRUN EL: +80'-0" **PENTHOUSE** EL: +74'-3"

**ROOF** EL: +64'-0"

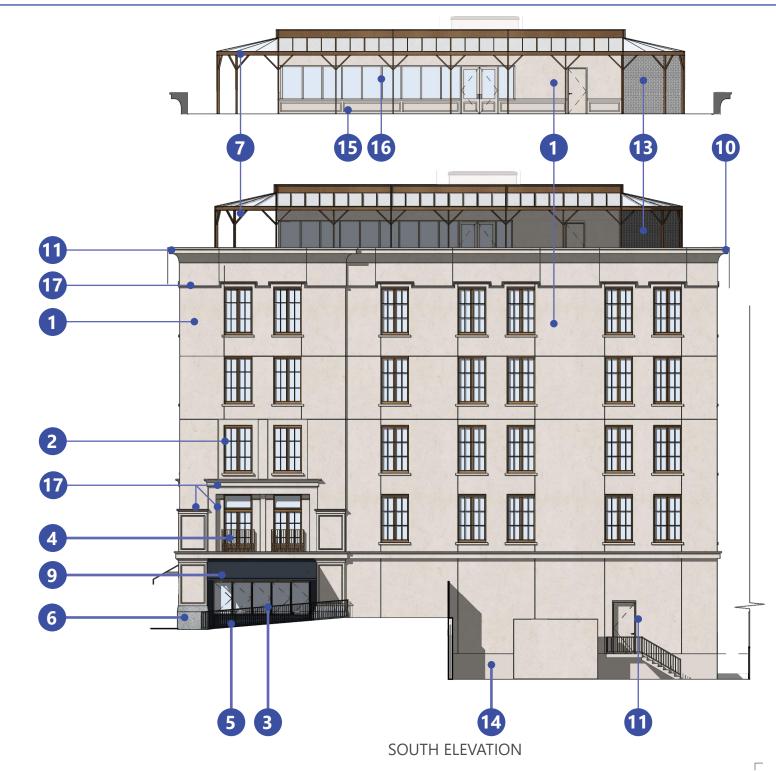
> FIFTH FLOOR EL: +51'-6"

FOURTH FLOOR EL: +40'-0"

THIRD FLOOR EL: +28'-6"

SECOND FLOOR EL: +17'-0"

FIRST FLOOR EL: +0'-0"



# COLOR ELEVATIONS (1880 NW FLANDERS ST.)



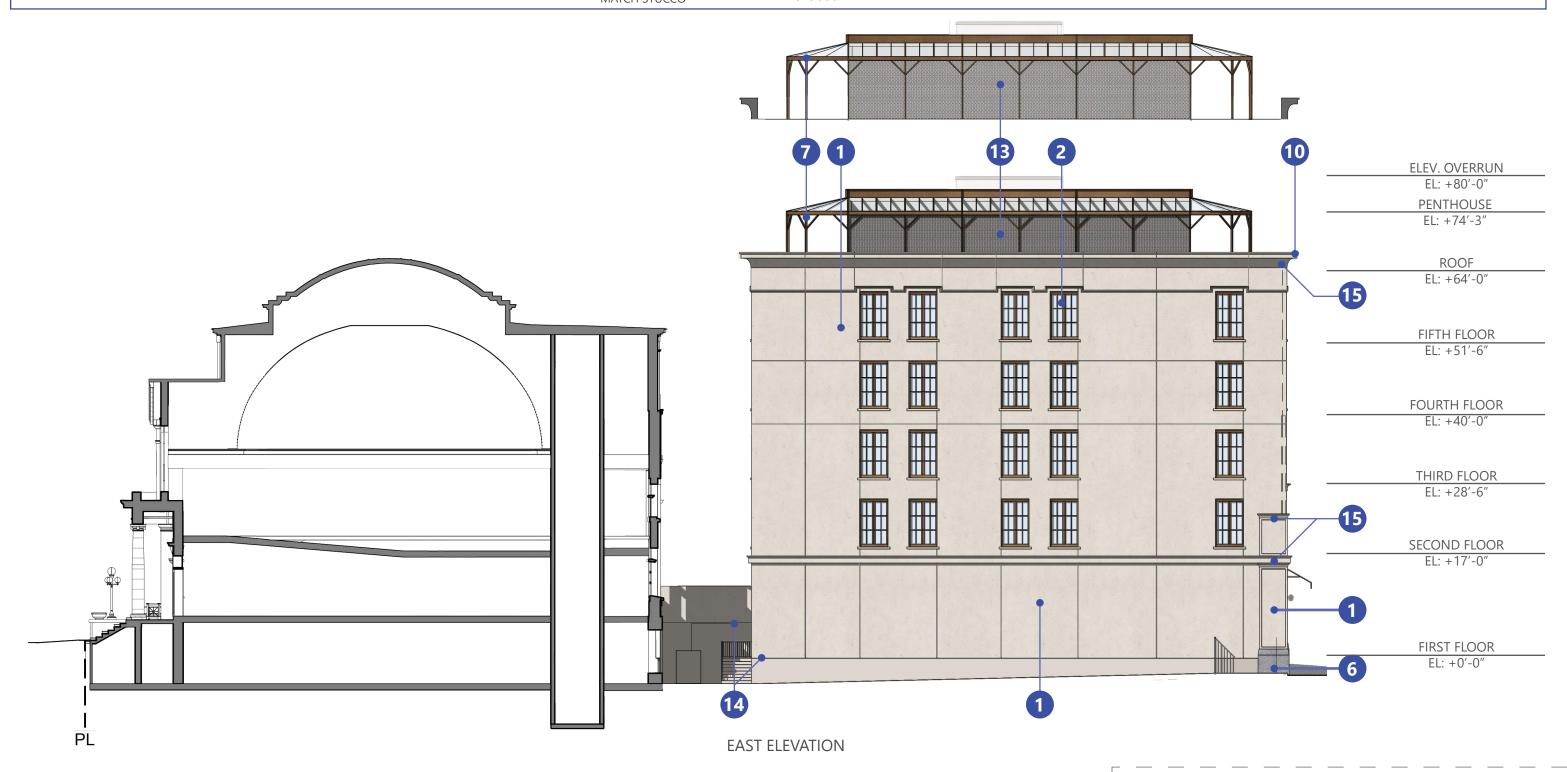


- STUCCO RAINSCREEN
- ALM. WINDOW, BRONZE FINISH COLOR
- 10 ALM. COPING CAP FOR PAINT TO MATCH WALL
- MTL DOOR FOR PAINT TO MATCH WALL
- ALM. OPERABLE STOREFRONT, BRONZE FINISH COLOR, W/ **DECORATIVE GLASS TRANSOM**
- OVERHEAD DOOR W/ TRANSLUCENT GLAZING
- CUSTOM ALM. GUARDRAIL, **BRONZE FINISH COLOR**
- METAL PANEL MECH. SCREEN CONCRETE W/ ELASTOMERIC PAINT, COLOR TO MATCH STUCCO MATCH STUCCO
- WOOD PANELING FOR PAINT
- 6 GRANITE STONE VENEER
- ALM. CANOPY, BRONZE 8 FINISH COLOR, W/ **GLASS ROOF**





DECORATIVE STUCCO 16 ALM. STOREFRONT TO MOULDING TRIM

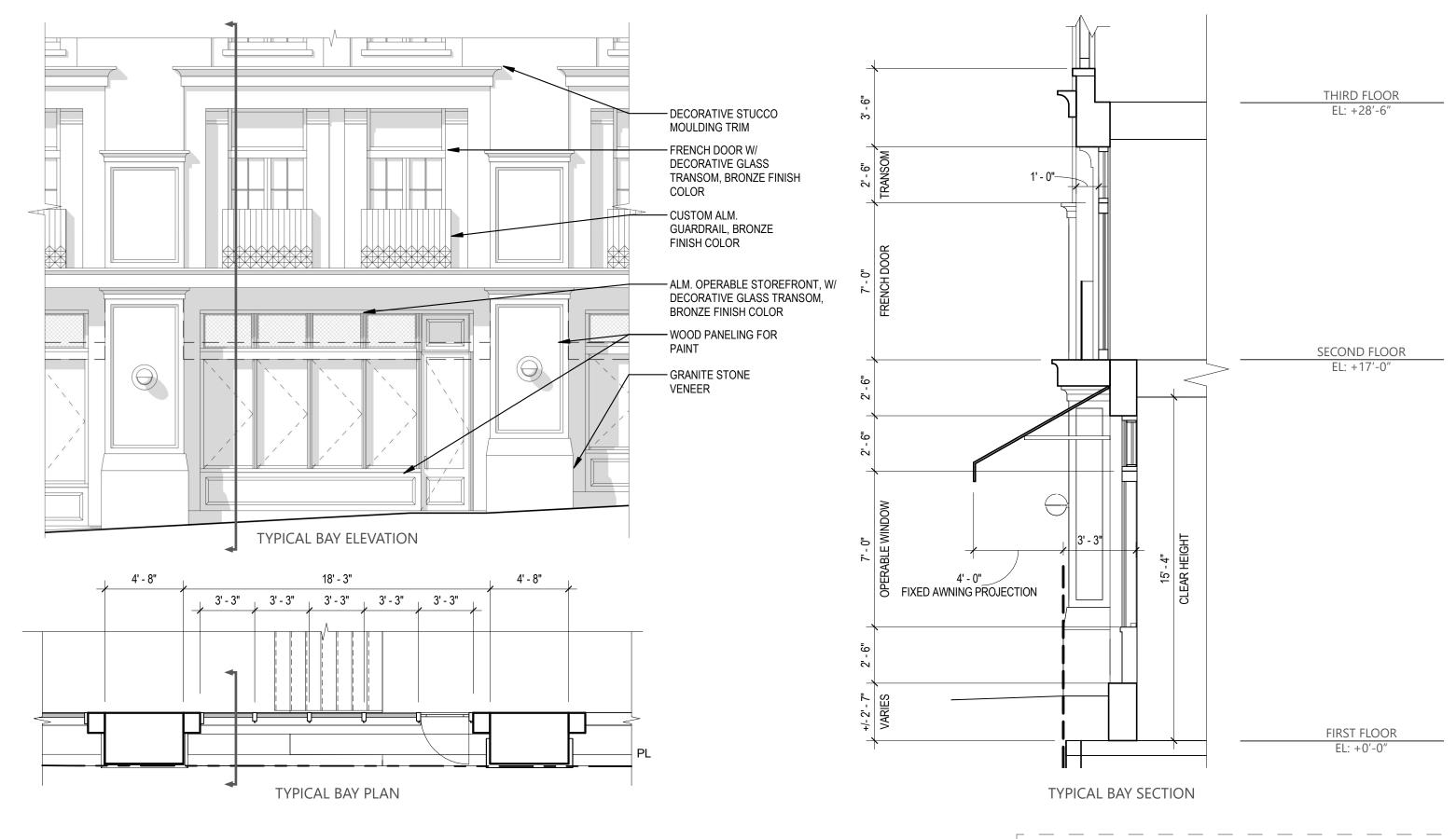


COLOR ELEVATIONS (1880 NW FLANDERS ST.)









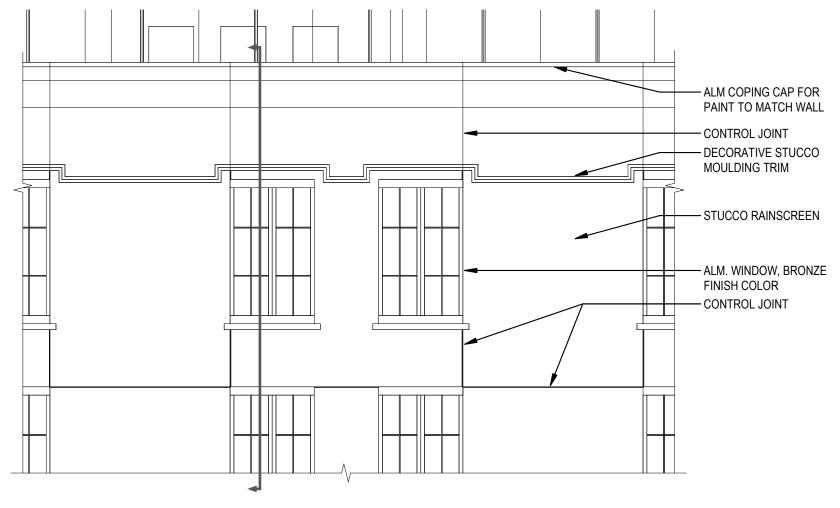
DETAILS (1880 NW FLANDERS ST.)

| SCALE: 3/16" = 1'-0" & 1/4" = 1'-0"

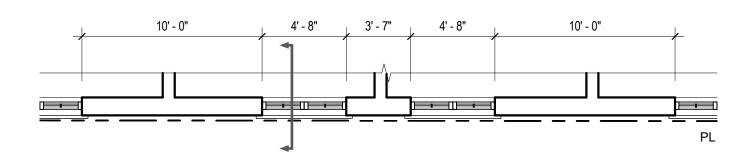








TYPICAL WINDOW AND ROOF ELEVATION



TYPICAL WINDOW PLAN

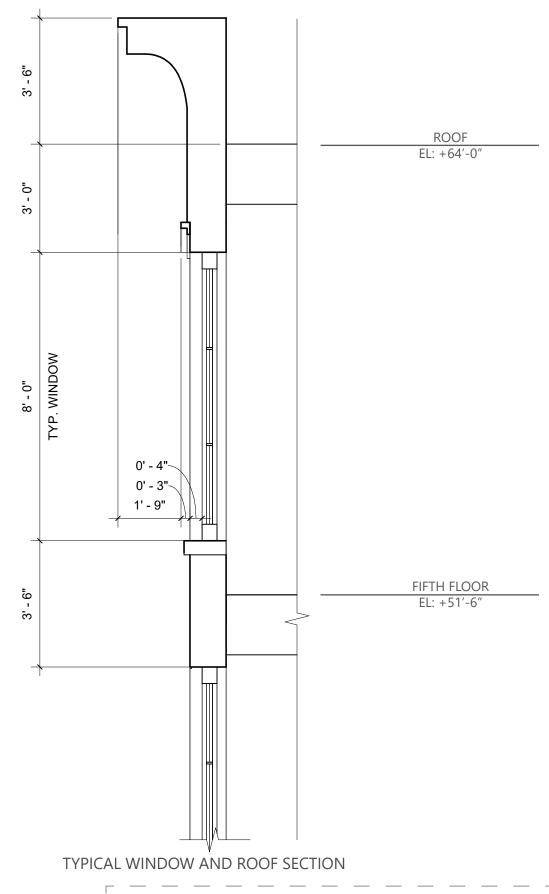
# DETAILS (1880 NW FLANDERS ST.)

| SCALE: 3/16" = 1'-0" & 3/8" = 1'-0"

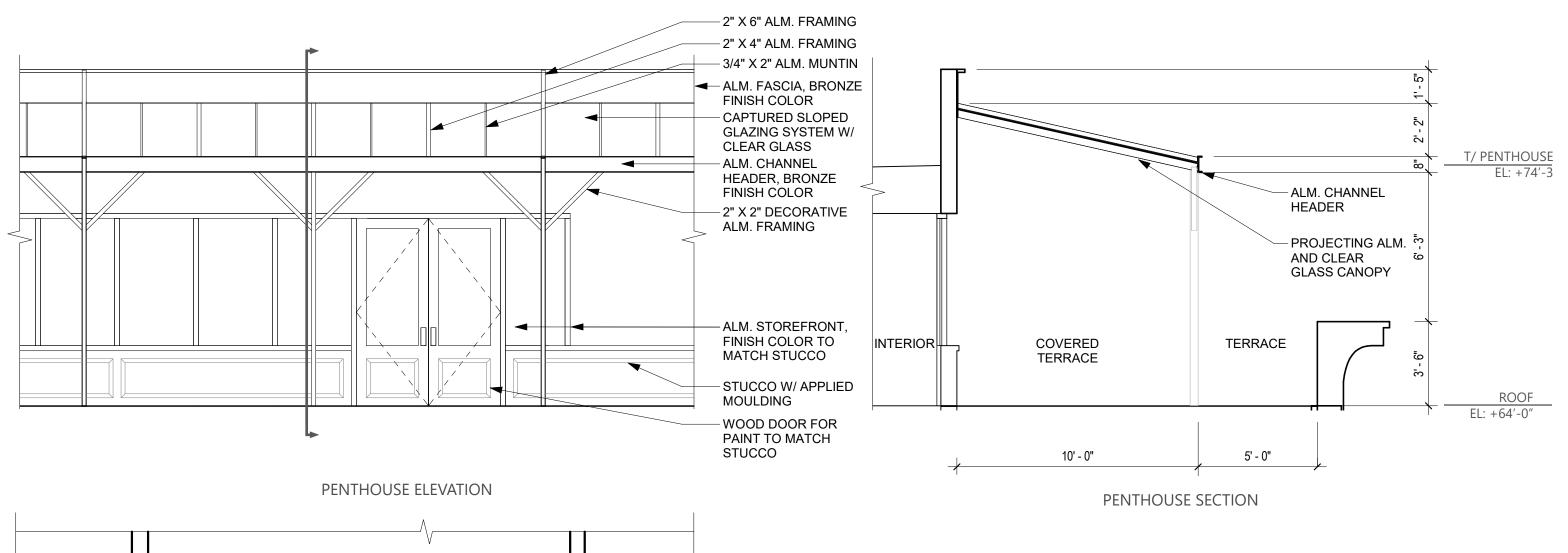


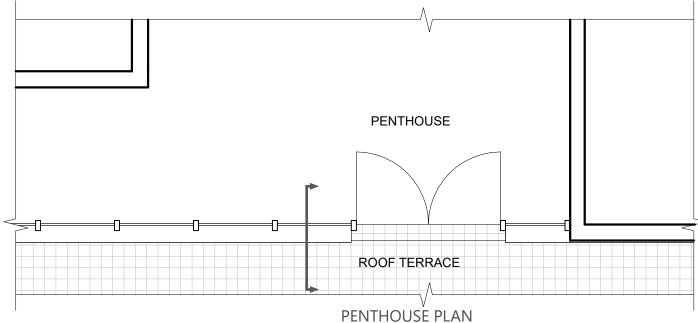


**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR









DETAILS (1880 NW FLANDERS ST.)

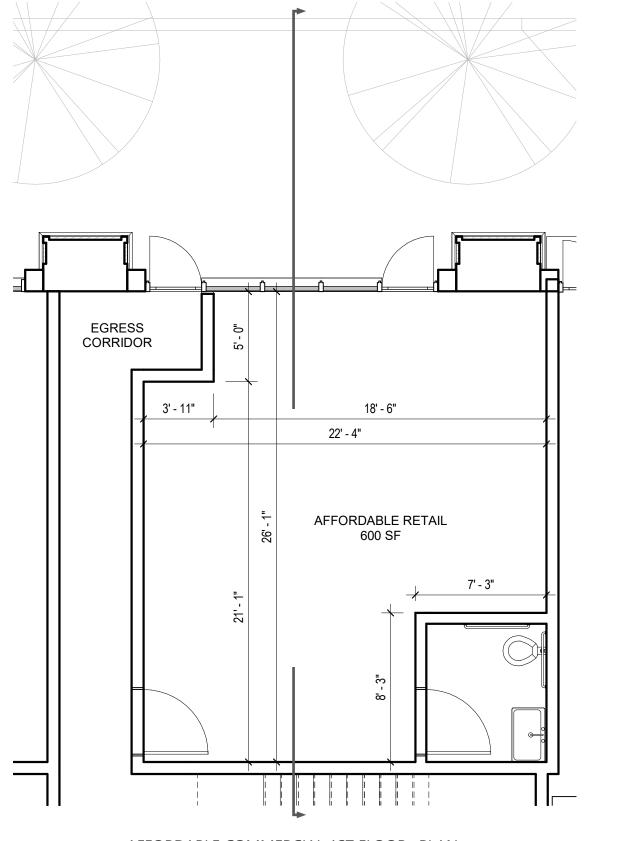


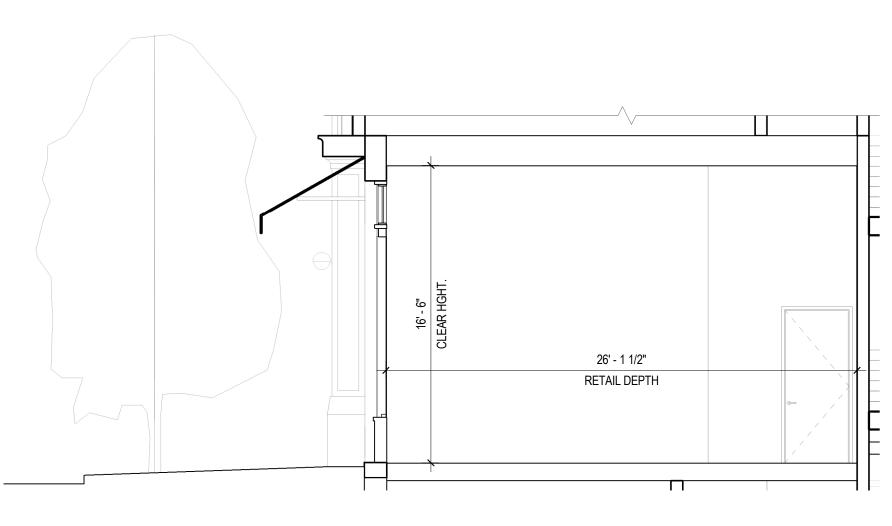












AFFORDABLE COMMERCIAL 1ST FLOOR PLAN

### AFFORDABLE COMMERCIAL SECTION

# AFFORDABLE COMMERCIAL SPACE

| SCALE: 3/16" = 1'-0"









A.6 LU 22-204531 HRM



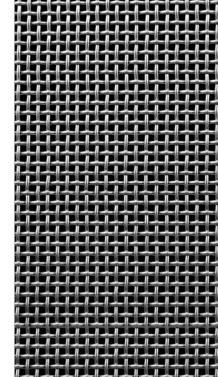




WOOD STOREFRONT SURROUND



CUSTOM RAILING EXAMPLE-LIGHT BRONZE FINISH



MESH SCREEN



EXISTING HISTORIC STONE FACADE



STUCCO RAINSCREEN, SMOOTH FINISH



DECORATIVE GLASS



CHARCOAL GRANITE STONE VENER -FLAMED FINISH



METAL - LIGHT BRONZE

# FACADE MATERIALS (1880 NW FLANDERS ST.)

| SCALE: NTS







# TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

1819 NW EVERETT STREET
1880 NW FLANDERS STREET

Historic & New Construction Drawings

March 13th, 2023

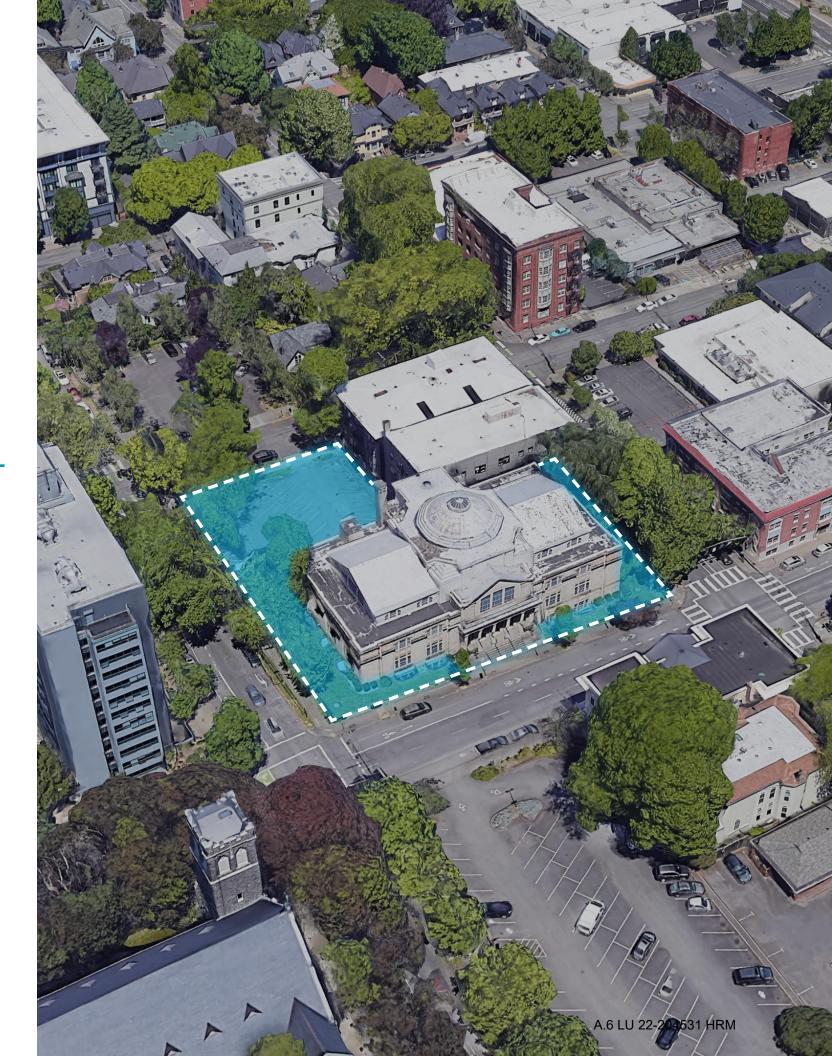
# TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

# 1819 NW EVERETT STREET 1880 NW FLANDERS STREET

Project Information

March 13th, 2023





## PI.1 - PI.11 General

PI.3	Project Information
PI.4	Zoning Summary
PI.5	Design Guideline Summary
PI.6	DAR Responses
PI.7	Modification Requests & Adjustment
PI.8	Zoning Map
PI.9	Existing Site Survey & Vicinity Photos
PI.10	Enlarged Site Plan

## C.1 - C.25 Historic

C.2	Enlarged Site Plan
C.3-7	Floor Plans
C.8-19	Elevations
C.20	Existing Conditions
C.21-23	Details
C.24	Exterior Materials
C.25	Window Scope

## C.26 - C.43 New Construction

C.27	Enlarged Site Plan
C.28-29	Floor Plans
C.30-37	Elevations
C.38-C.41	Details
C.42	Facade Materials

## APP.1 - APP.42 Appendix

APP.2	Area Context
APP.3	Urban Context
APP.4	Contextual Analysis
APP.5 - 11	3D Perspectives
APP.12	Penthouse Concept Images
APP.13	FAR Summary
APP.14 - 15	Bike Parking Requirements
APP.16	Elevations - Ground Floor Glazing
APP.17	Zoning Height Diagram
APP.18 - 23	Product Data
APP.24 - 34	Landscape Drawings
APP.35 - 42	Civil Drawings

## SHEET INDEX



## PROJECT TEAM

## Owner:

Gaia Ventures I, LLC

5821 SE Powell Blvd.

Portland, Orgeon 97206

## **Architect:**

Hartshorne Plunkard Architecture LLC

315 West Walton Street

Chicago, IL 60610

## **Structural & Civil Engineer:**

KPFF

111 SW 5th Ave. #2600

Portland, OR 97204

## **Landscape Architect:**

Lango Hansen

1100 NW Glisan St #3A

Portland, OR 97209

## WRITTEN DESCRIPTIONS

## **Site Description**

## Site

The site totals 29,997 square feet with the southern parcel consisting of an existing building listed on the National Register of Historic Buildings, First Church of Christ Scientist, and a surface parking lot in the northern parcel. Two distinct buildings are planned, one adaptive reuse of the historic landmark building and another new construction hotel on the adjacent parking lot. The site requires reconstruction of the adjacent public right-of-way, new utility services, and exterior landscaping for both parcels.

## Zoning

The site is zoned as Commercial Mixed-Use and allows for the proposed primary uses as-of-right. Due to it's historic landmark status a Type III Historic Resource Review approval is required for the project,. Additionally, the new hotel project is seeking approvals for two zoning height bonuses and a zoning adjustment for additional building height.

## **Development Summary**

## 1819 NW Everett St. - Adaptive Reuse

Founders Developments is honored to take on the restoration and adaptive re-use of Solon Spencer Beman's historic 1909 First Church of Christ, Scientist, most recently the Northwest Neighborhood Cultural Center. The project goal is to maintain the property's National Landmark status and through preservation, ensure longevity of the building for the public to enjoy for generations to come.

Founders Developments intends to convert the historic property into an exceptional social club, while preserving and restoring the architectural features of the exterior facade's intricate Beaux-Arts design. The adaptive reuse of the historic landmark building includes event spaces, food and beverage, wellness, spa, social amenities and other and back-of-house uses.

The existing building currently consists of two stories with a full basement partially below grade. Rennovations include the addition of a new third floor, minor rooftop additions, seismic upgrade, façade and window restoration according to National Park Service historic standards, landscaping and new mechanical systems throughout.

## 1880 NW Flanders St. - New Hotel

The new construction hotel is five stories plus a rooftop penthouse. The ground floor includes food and beverage use, approximately 80 hotel rooms on floors 2-5, roof terrace amenity, and a full basement with amenity and back-of-house uses. The basement is anticipated to be cast-in-place concrete and wood framed construction above grade.

## **METRICS**

## **South Parcel (Historic):**

## **Address**

- 1819 NW Everett Street Portland, OR 97209

#### Site Area

- 29,997 SF - R140882 & R140879 (PARCELS 1-4, 6, 7)

## **Gross Building Area**

- 44,042 SF

## **Primary Use**

- Retail Sales and Service (A-2/3)

## **North Parcel (New Construction):**

## **Address**

-1880 NW Flanders Portland, OR 97209

## **Site Area**

- 29,997 SF - R140882 & R140879 (PARCELS 1-4, 6, 7)

## **Gross Building Area**

- 49,800 SF

## **Primary Use**

- Hotel (R-1) & Retail Sales and Service (A-2/3)









	REQUIREMENT	ZONING CODE	SHEET REFERENCE	1819 NW Everett (Historic)	1880 NW Flanders (New)
Existing Zone	CM2d (MU-U) - Commercial D - Design Overlay Mixed Use - Urban Center AB - Alphabet Historic District NP - Northwest Plan District	33.420 - MAP 420-1	PI.8	No Change	No Change
FAR	2.5:1 Base with Bonus to 4:1; Existing Site = 29,997sf Allowable FAR	33.130.205 / Table 130-2	APP.13	Historic FAR = 33,431 sf New Construction FAR = 41,400 sf Total FAR = 74,831 sf < 74,992 sf Allov	vable FAR 2.46
Zoning Height	45' max with bonus up to 60' max height Bonus height +10' affordable commercial space +5' first floor height	33.130.205 / Table 130-2	PI7, APP:17	Existing no change, T/ROOF = 68'-9" (Relative to Historic Building Base Point)	T/MAIN ROOF = 64'-0" T/PENTHOUSE ROOF = 74'-3" See Modification Summary page
Zoning Height Exceptions	4' parapet & railings *setback 15' at street facade  16' elevator equipment / 10' mech & stair* ** ** 10% max roof coverage	33.130.210.C	APP.17	N/A	Exception elements comply
Primary Uses	Retail Sales, & Service	33.130.100/ Table 130-1	C.3-7, C.28-29	Retail Sales and Service	Retail Sales and Service
Building Setbacks	0' min. @ street lot line 10' max. @ street facing facades	33.130.215	PI.10	Complies, no change to existing historic building	
Max Building Coverage	100% of site area (Inner Pattern Area)	33.130.220/ Table 130-2	PI.10	Existing, No Change	Complies
Parking	No minimum parking requirement	33.562.280	PI.10	Complies	Complies
Loading Access	1 standard A (35' x 10' x 13' clear) or 2 standard B (18' x 9' x 10' clear)		C.28	Existing no change to historic bldg	(1) Modified Loading Zone @ 10'WX25'LX11'H Refer to loading study and modification summary page
Bicycle Parking	Restaurant and Bar 2 or 1 per 2,300 sf net / 2 or 1 per 1,000 sf net Temporary Lodging 2 or 1 per 20 rent. rms. / 2 or 1 per 40 rent. rms.	33.266.210 / Table 266-6	APP:14-15	Complies	Complies
Windows (Ground Floor) Windows (Above Ground Floor)	40% coverage of wall area (2'-10') 15% of facade area to be glazed		APP.16	Complies	Complies
Min. Landscaped Area	15% of site area required 29,997sf site area x 15% = 5,000 sf	33.130.225 table 130-2	APP.24-30	Complies	Complies
Landscape Buffer Abutting an RF - RM4 or RMP Zoned Lot	10ft @ L3	33.130.215.B	APP.24-30	Existing, no change	Complies

## **ZONING SUMMARY**







## TYPE III HISTORIC RESOURCE REVIEW

## APPROVAL CRITERIA: COMMUNITY DESIGN GUIDELINES WITH HISTORIC ALPHABET DISTRICT ADDENDUM

#### **COMMUNITY DESIGN GUIDELINES**

## P1: Community Plan Area Character

The projects are designed to enhance the sense of place and identity of the existing community by incorporating design features and massing which are contextual to the historic Alphabet District. A pedestrian focused design of the new hotel ground level includes large storefront windows, awnings, outdoor space for

dining and other activities to enhance the public realm. To integrate with the residential context the new construction hotel building is sited to provide landscape setbacks from the apartment building directly to the east. Additional facade scaling elements are included to articulate the facade at the pedestrian level by the inclusion of decorative mouldings, detailed windows and Juliette balconies. The integration of roof terraces on both buildings are included to take advantage of views to downtown, river and surrounding context. Refer to floor plans, elevations, and enlarged detail sheets for application.

#### **P2: Historic and Conservation Districts**

The Historic identity of the existing building is to be retained through extensive rehabilitation efforts of the character defining windows and facade. The new construction hotel is designed **D4: Parking Areas and Garages** to seamlessly integrate with the wide variety building types present in the district. Contextual Not applicable. features are integral to the design, such as the massing, decorative railings, and projecting cornice. Refer to elevations and contextual analysis sheets for application.

## P3: Gateways

Not applicable.

#### **E1: Pedestrian Networks**

The pedestrian right-of-way is proposed to be upgraded to meet current PBOT standards along all street frontages. A new accessible entrance is proposed for the historic landmark building. New exterior lighting will help define a pleasant pedestrian experience. Refer to site plans, civil and landscape drawings for application.

#### **E2: Stopping Places**

Places for pedestrian stopping points are integrated into both projects through outdoor dining and lounge areas, cafe, and planter seating wall. Refer to landscape drawings for application.

#### E3: The Sidewalk Level of Buildings

The architectural design of the new construction hotel building enhances the visual interest of the pedestrian level through large operable storefront windows leading into active use interiors. The ground and second level include differentiating details such as panelized wood storefronts, granite base pilasters, lighting, entrance canopies, and Juliette balconies. Refer to elevations, enlarged details, and renderings for application.

#### **E4: Corners that Build Active Intersections**

The corner of the new construction hotel design includes the main entrance characterized by a decorative metal canopy and rounded corner Juliette balconies above. These features provide a strong visual presence at the intersection. Refer to renderings for application.

#### E5: Light, Wind, and Rain

The new construction hotel design includes fabric awnings at all storefront bays and a glass covered metal canopy at the main entrance to provide shelter to pedestrians. A matte finish stucco and painted building base reduces glare. Refer to elevations, enlarged details, and renderings for application.

#### **D1: Outdoor Areas**

The new construction and existing historic building include sizable usable outdoors areas including outdoor dining, cafe, and lounger areas. Refer to landscape plans for application.

#### **D2: Main Entrances**

The corner of the new construction hotel design includes the main entrance characterized by a decorative metal canopy and rounded corner Juliette balconies above. These features provide a strong visual presence at the intersection. Refer to renderings for application.

#### **D3: Landscape Features**

Existing street trees are preserved wherever possible. Due to conditions of street trees and at the recommendation of Urban Forestry there are several trees noted for replacement. New landscape design is proposed around both buildings to enhance the site design. Refer to landscape drawings for application.

#### **D5: Crime Prevention**

The projects are designed to include active uses and exterior lighting at the ground floors. Refer to floor plans and renderings for application.

#### **D6: Architectural Integrity**

New additions to the existing historic structure are designed to be compatible with the existing building. Refer to elevations and renderings for application.

#### D7: Blending into the Neighborhood

The new construction hotel is designed to seamlessly integrate with the wide variety building types present in the district. Contextual features are integral to the design, such as the massing, decorative railings, and projecting cornice. Refer to elevations and contextual analysis sheets for application.

#### **D8: Interest, Quality and Composition**

The new construction hotel is designed with high quality and durable materials. Visual interest is provided in the material qualities themselves, such as granite, and with decorative features, such as simulated divided lite windows, mouldings/brackets, and metal work. Refer to elevations, enlarged details, and renderings for application.

#### **ALHABET HISTORIC DISTRICT**

## **#1: Historic changes**

Non-original elements which have acquired historic significance will be

#### #2: Differentiate new from old

#### 1819 NW Everett:

The rooftop additions will be clearly differentiated from the historic building by insetting the massing of the additions from

the existing primary facades. The additions is located to avoid the destruction of the historic materials on the primary facades. Replacement materials will be reasonable facsimile of the historic materials they replace.

#### 1880 NW Flanders:

The new construction hotel is situated on the site to clearly distinguish the building as a separate, but complimentary structure. A setback of 15' is provided from the historic building to retain the massing character of the historic building.

#### #3: Hierarchy of compatibility

#### 1819 NW Everett:

The rooftop additions will be compatible primarily with the historic resource, secondly with adjacent properties and finally with the historic district. The rooftop additions are designed to reflect the simplified aesthetic of the upper story of the historic building.

#### 1880 NW Flanders:

The timeless European architectural style of the new construction building is utilized to draw compatibility with the adjacent historic building and neighborhood. The facade arrangement includes a traditional base, middle, top hierarchy and delicate metal detailing which denotes its modern insertion into the historic context while carrying forward decorative elements found throughout the neighborhood.

**DESIGN GUIDELINE SUMMARY** 







## **DAR RESPONSES**

## **1880 NW FLANDERS STREET**

## 1) Overall building height

- a) See modification summary for height modification requested for the penthouse level.
- b) Height Exceptions
  - i. Parapets: All parapets proposed are below the 4'-0" height exception limit.
    - Refer to zoning height exhibit, sheet APP.17
  - ii. Elevator Overrun and Stairs: Elevator overrun and stairs are within the 16'-0" height exception limit.
  - Refer to zoning height exhibit (APP.17). Exception measured from the bonus height of 60'-0" above base point.

    Mashapisal Faulisment (Screening: The agricument and angles we have been revised to somely with the 10' 0" ha
  - iii. Mechanical Equipment/Screening: The equipment and enclosure has been revised to comply with the 10'-0" height exception limit. Refer to zoning height exhibit, sheet APP.17.
- 2) Loading
  - a) See modification summary for request for reduced loading zone size requirements and maneuvering procedures.
- 3) Entry
  - See renderings and elevations sheets (APP.7-8 and C.30-31) for enlarged corner entry architecture and canopy design.
- 4) Materials
  - a) See materials sheet (C.42) for proposed materials on the new construction project in relationship to the historic building.
- 5) Fenestration
  - a) See building elevations sheets (C.34-C.37) which outline a uniform fenestration approach.

## **1819 NW EVERETT STREET**

- 1) Windows
  - a) Submittal has been revised to incorporate repair of existing street facing windows. Refer to sheet C.25 for details of window repair scope.
- 2) Chimney and existing elevator overrun
  - a) Submittal includes confirmation of proposed removal of chimneys. Refer to sheet C.20 for details.
- 3) Parapets
  - a) Due to the height of the existing parapet at the upper level of the historic building new frameless glass guardrails are proposed to minimize visual impact to the façade. Refer to sheet C.7 for guardrail extents and APP.5 and APP.18 for details of the proposed guardrail system.
- 4) Outdoor seating
  - a) Outdoor seating is proposed at both east and west side of the historic landmark building, refer to APP.25-App.26 for details. There is no proposed modification of openings in the historic façade. New fabric awnings in replacement of existing fabric awnings are proposed over exterior door locations, refer to sheets C.9 and C.10
- 5) Sidewalk
  - a) See sheet APP.36-37 for public right-of-way dedication conditions per coordination with PBOT.
- 6) All items requested have been included with the resubmittal documents with the exception of the following:
  - Accessible upgrades at southern entry of historic landmark building the existing conditions at the southern entry prevent the ability to modify the conditions to be in compliance with accessible requirements without severely altering the historic elements. There is an existing 8" tall step directly below the main entry doors and not enough space between the door and the portico columns to create a compliant landing (3'-6" available, 5'-0" required). Relocation of one portico column and removal of adjacent window wells would be required to integrate accessible requirements. The submittal includes accessible upgrades to the east and west entry points, refer to sheet C.02

## DAR RESPONSES







## **MODIFICATIONS**

## Overall height of new construction hotel

- Base zoning height plus bonuses: 60'-0" above basepoint Modification request: +10'-0" above the allowable 60'-0"
- Description: A modification request is submitted to allow for the inclusion of a penthouse level to provide rooftop access for occupant use and appreciation of predominate views of downtown and surrounding area. A modification is requested to include an additional 10′-0″ of allowable zoning height to the penthouse roof level. The minimum 2′-6″ code required parapet would be included above the requested 10′-0″ but would fall below the allowable 4′-0″ height exception for parapets. Due to the setbacks of the penthouse from the main building footprint the proposed modification does not impact the street facing facades or perceived height at the ground level. The benefit of providing usable rooftop area and the lack of visual impact illustrates the proposed modification meets the intent of the approval criteria.

## Loading zone size

- Code requirement: 1 Standard A (35'Lx10'Wx13'H) or 2 Standard B (18'Lx9'Wx10'H)
- Modification request: 1 Standard A space with modified dimensions (25'Lx10'Wx13'H)

  Description: Per the loading demand study and coordination with PBOT the modified dimensions provide adequate space for the size of trucks the loading zone is anticipated to serve based on building use.

## Short-Term Bike Parking Locations

- Code requirement: Bicycle racks to be located along all facades and within 50' of a main entry.
- Modification request: Increase allowable distance to 200' from main entries and allow locations as proposed on
- Description: Several conditions informed the location of the proposed bicycle racks including: multiple entries on the historic and new construction buildings, PBOT right-of-way encroachments, accessible site ramp for historic structure, and the BDS requested outdoor dining area at SW corner of the new construction hotel. Due to these conditions the submitted locations allow for adequate access to bike parking around the site without unduly impacting the landscaping and historic façade along NW Everett Street. The proposed modification would provide an equivalent multi-modal access to the property as intended by the code.

## **ADJUSTMENTS**

## Loading zone maneuvering

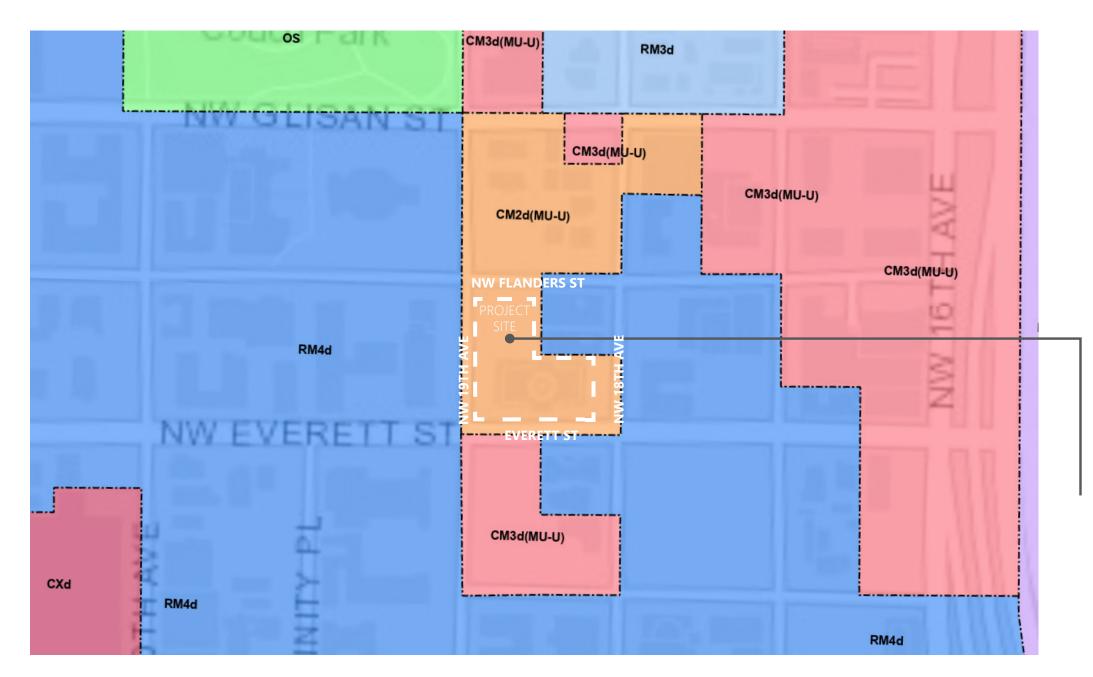
- Code Requirement: Forward motion only
- Adjustment request: Reward motion
- Description: Due to the site constraints a path allowing for full forward motion through the site is not supported by PBOT or BDS and would negatively impact the ability to include the required ground floor program to service the building and provide ground level active uses.

## **MODIFICATION REQUESTS & ADJUSTMENT**









## **BASE ZONE:**

- CM2d (MU-U) COMMERCIAL MIXED USE - URBAN CENTER

## **OVERLAY ZONES:**

- d (DESIGN)
- -AB (ALPHABET HISTORIC DISTRICT)
- -NP (NORTHWEST PLAN DISTRICT)

**ZONING MAP** 

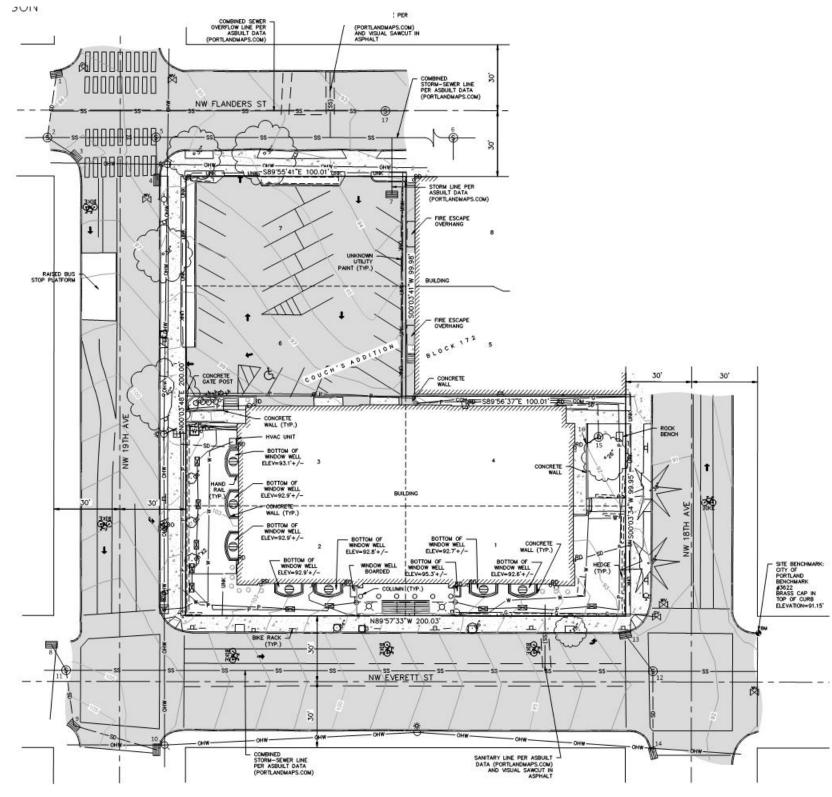












SITE SURVEY

## **EXISTING SITE SURVEY & VICINITY PHOTOS**

| SCALE: NTS









VIEW SOUTH EAST AT NW FLANDERS & NW 19TH AVE

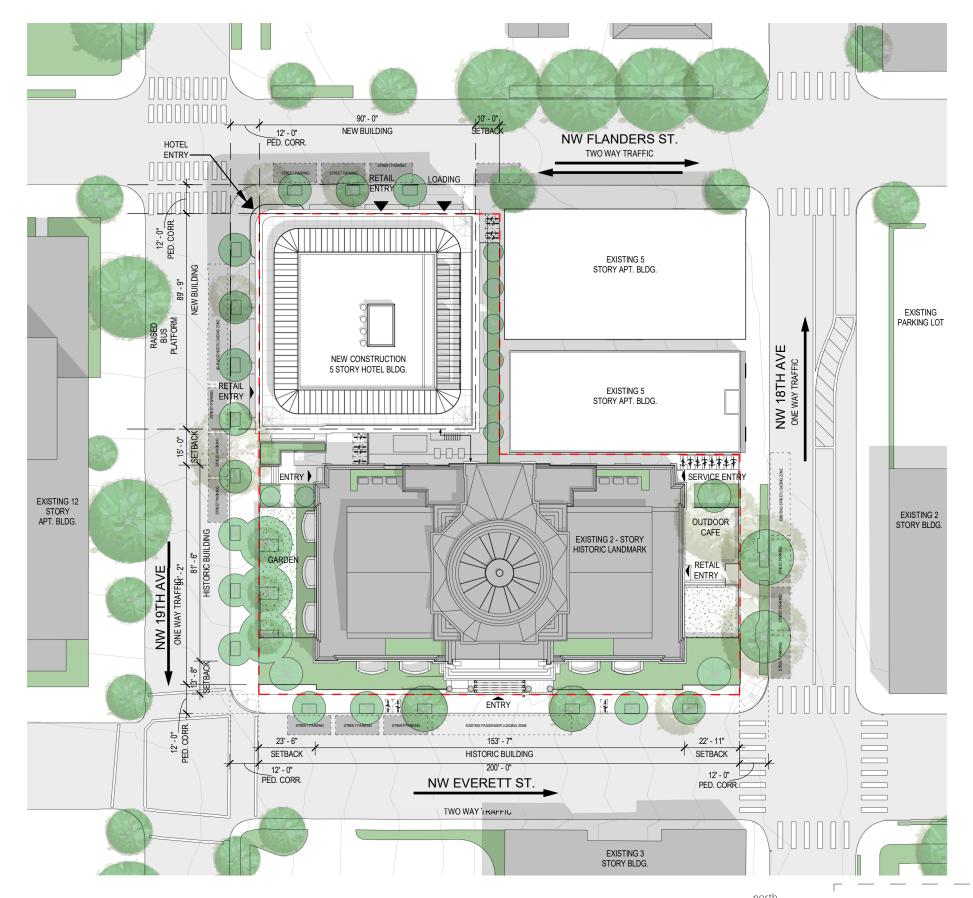


VIEW NORTH EAST AT NW EVERETT & NW 19TH AVE



VIEW NORTH WEST AT NW EVERETT ST & 18TH AVE

area for city stami --- --- ---



## **ENLARGED SITE PLAN**

| SCALE: 1" = 64'-0"







# TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

1819 NW EVERETT STREET
1880 NW FLANDERS STREET

Project Information

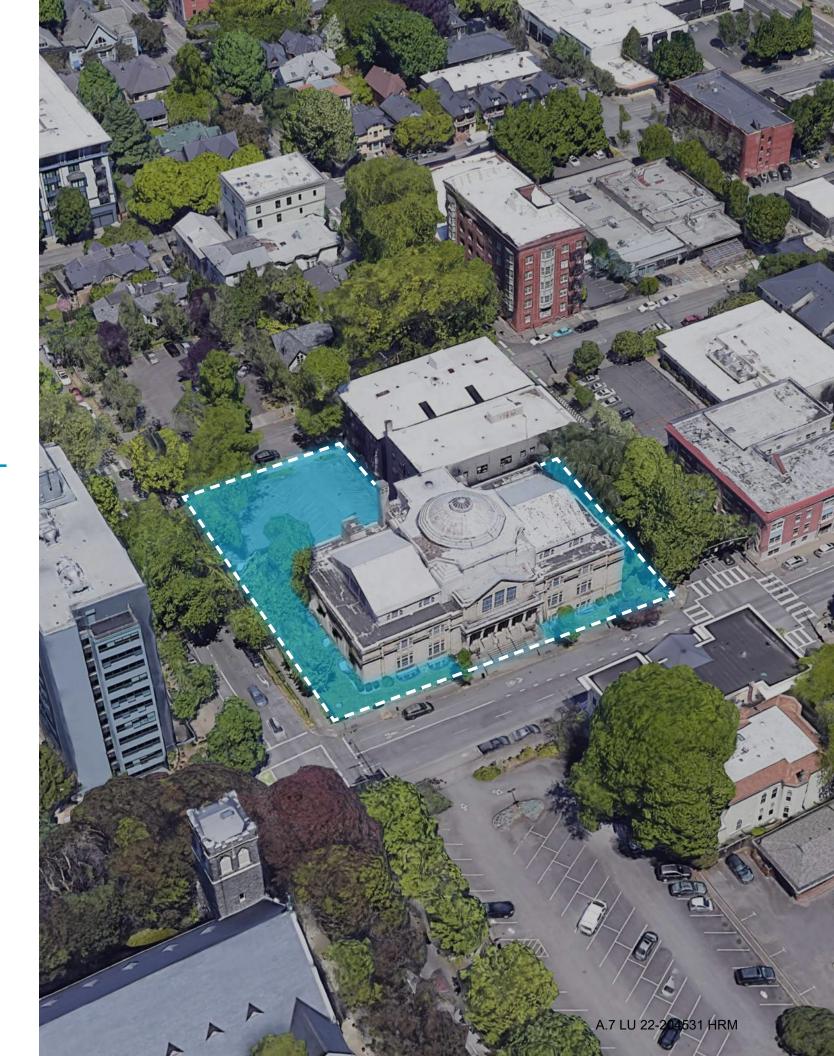
March 13th, 2023

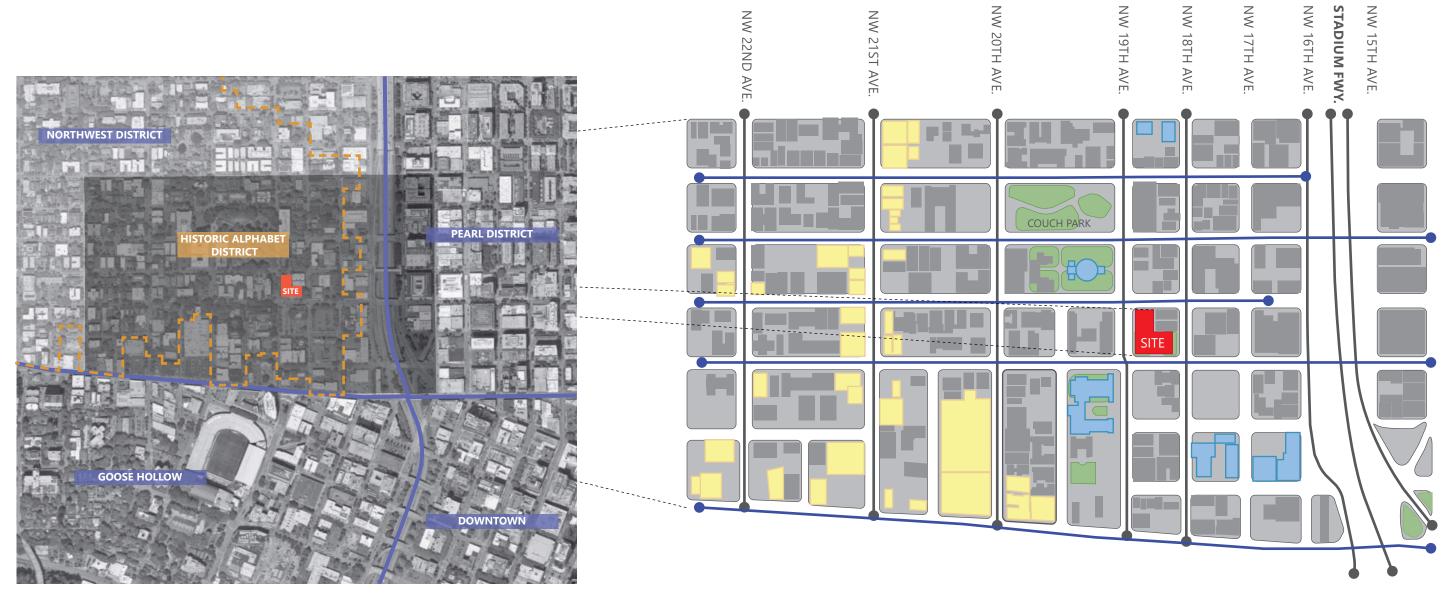
# TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

# 1819 NW EVERETT STREET 1880 NW FLANDERS STREET

Appendix - Supporting Information

March 13th, 2023





DISTRICT CONNECTION LANDMARK DIAGRAM

> RETAIL RELIGIOUS STRUCTURE GREENSCAPE

AREA CONTEXT











## **URBAN CONTEXT**









## **BUILDING TYPES**



BETH ISRAEL SYNAGOGUE - 1972 NW FI



239 NW 20TH



239 NW 20TH



TRINITY EPISCOPAL CATHEDRAL - 147 NW 19TH



THE BILTMORE - 2014 NW GLISAN ST.

## **DETAILS**



ROUNDED BAY



METAL DETAILING & RAILINGS















## **MATERIALS**



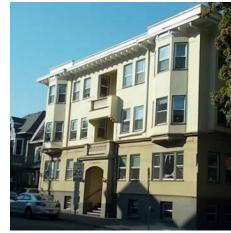
2030 NW FLANDERS



2068 NW FLANDERS



1730 NW COUCH



2105 NW FLANDERS





2120 NW FLANDERS

STUCCO FACADES

## **CONTEXTUAL ANALYSIS**



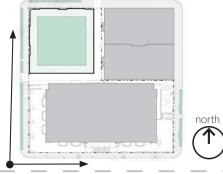








3D PERSPECTIVE FROM THE INTERSECTION OF NW EVERETT ST. AND NW 19TH AVE.

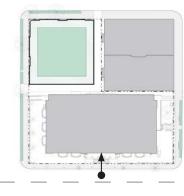


## 3D PERSPECTIVES (1819 NW EVERETT ST.)











3D PERSPECTIVE FROM NW EVERETT ST. LOOKING NORTH

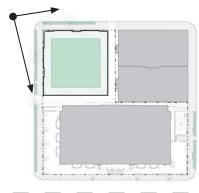
## 3D PERSPECTIVES (1819 NW EVERETT ST.)











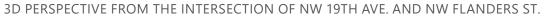


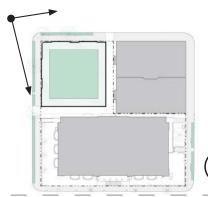
3D PERSPECTIVE FROM THE INTERSECTION OF NW 19TH AVE. AND NW FLANDERS ST.









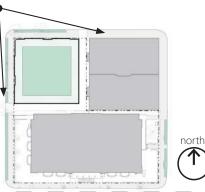








3D PERSPECTIVE - AERIAL VIEW OF PENTHOUSE LOOKING SOUTHEAST



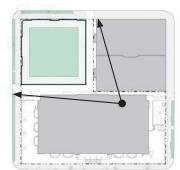








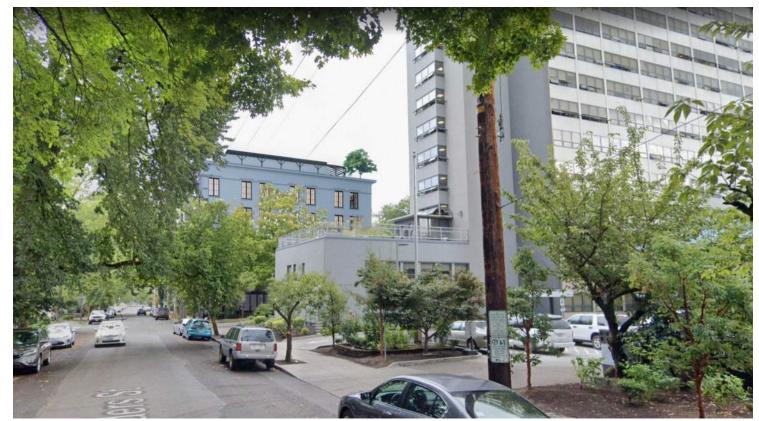
3D PERSPECTIVE - AERIAL VIEW OF PENTHOUSE LOOKING NORTHWEST



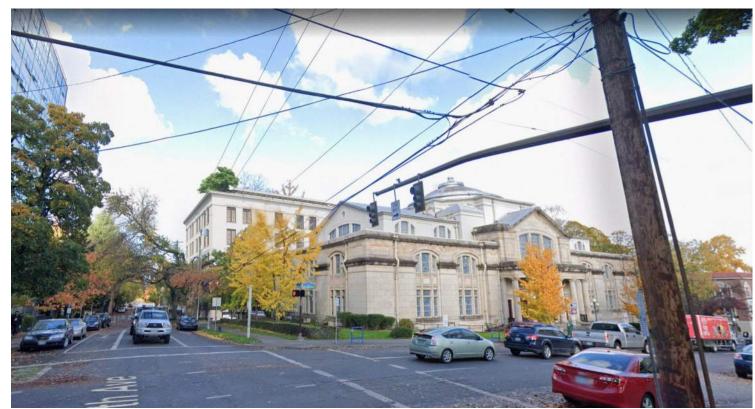








STREET PERSPECTIVE 1 - LOOKING EAST ON FLANDERS ST.



STREET PERSPECTIVE 3 - LOOKING NORTH ON 19TH AVE.

| SCALE: NTS

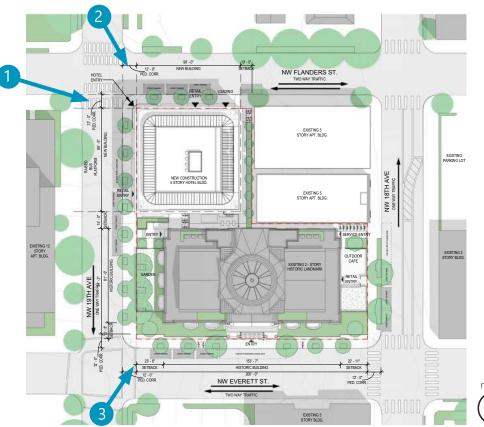




**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR



STREET PERSPECTIVE 2 - LOOKING SOUTH ON 19TH AVE.











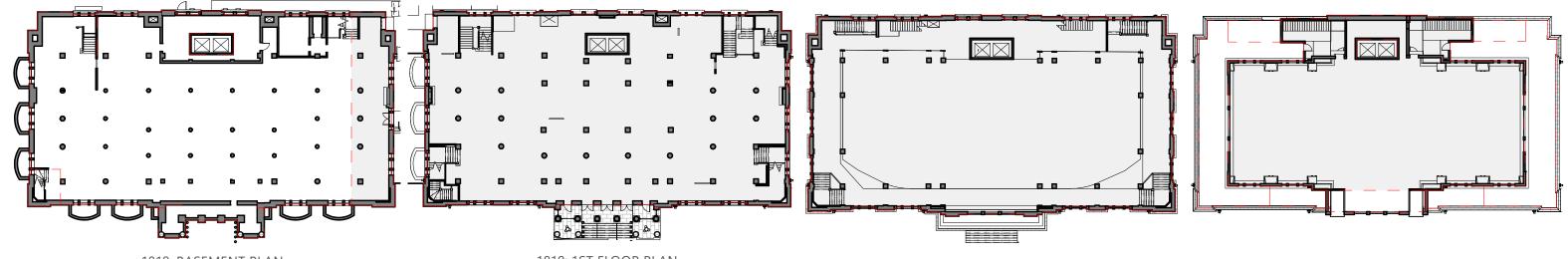




PENTHOUSE CONCEPT IMAGES (1880 NW FLANDERS ST.)







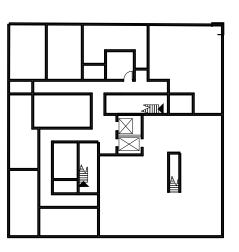
1819: BASEMENT PLAN 1,116SF

\*AREA TO WEST OF LINE, +4'-0" BELOW GRADE

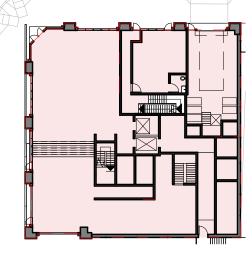


1819: 2ND FLOOR PLAN 10,548SF

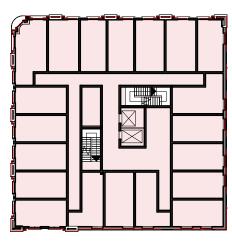
1819: 3RD FLOOR PLAN 6,520SF



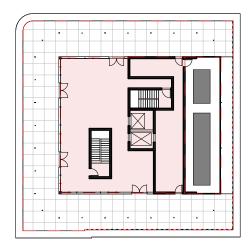




1880: 1ST FLOOR PLAN 7,580 SF



1880: 2ND - 5TH FLOOR PLAN 7,780 SF



1880: ROOF PLAN 2,700 SF

## **AREA SUMMARIES**

1819 NW FLANDERS (HISTORIC)			
FAR SUMMARY: APPLICABLE AREA			
BASEMENT	1,401 SF		
1ST FLOOR	12,132 SF		
2ND FLOOR	12,205 SF		
3RD FLOOR	7,692 SF		
TOTAL	33,431 SF		

03.13.2023 | LU 22-204531 HR

1880 NW FLANDERS (HOTEL)		
FAR SUMMARY: APPLICABLE AREA		
BASEMENT	N/A	
1ST FLOOR	7,580 SF	
2ND FLOOR	7,780 SF	
3RD FLOOR	7,780 SF	
4TH FLOOR	7,780 SF	
5TH FLOOR	7,780 SF	
ROOF	2,700 SF	
TOTAL	41,400 SF	

## **FAR GRAND TOTALS**

29,997 SF (SITE AREA) X 2.5 = 74,992.5 SF (ALLOWABLE FAR)

TOTAL SF: (33,431 + 41,400) = 74,831 SF (TOTAL PROPOSED FAR)

74,831 SF < 74,992 SF

FAR SUMMARY

| SCALE: 1" = 40'-0"











## NEW BUILDING 12' - 0" NW FLANDERS ST. HOTEL TWO WAY TRAFFIC **ENTRY** EXISTING 5 STORY APT. BLDG. NW 18TH AVE NEW CONSTRUCTION EXISTING 5 STORY APT. BLDG. ENTRY ) SERVICE ENTR OOR EXISTING 2 - STORY HISTORIC LANDMARK RETAIL N NW 19TH AVE ONE WAY TRAFFIGH - 2" BIKE PARKING BIKE PARKING 153' - 7" 23' - 6" 22' - 11" 12'-6 PED. C SETBACK HISTORIC BUILDING SETBACK \_12' - 0" 200' - 0" 12' - 0' PED. CORR. NW EVERETT ST. PED. CORR TWO WAY TRAFFIC

SITE PLAN - BIKE PARKING: LOCATIONS & DISTANCE TO ENTRIES | SCALE: 1" = 40'-0"

#### **1819 NW EVERETT STREET**

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

## **RESTAURANT & BAR: 4,649 SF**

#### SHORT TERM

- REQUIREMENTS: 2, OR 1 PER 2,300 SF
- 2 SPACES

# **RETAIL SALES AND SERVICES: 30,658 SF** SHORT TERM

- REQUIREMENTS: 2, OR 1 PER 3,800SF
- 8 SPACES

## **RESTAURANT & BAR: 7,034**

**1880 NW FLANDERS STREET** 

## SHORT TERM

- REQUIREMENTS: 2, OR 1 PER 2,300 SF
- 4 SPACES

# **RETAIL SALES AND SERVICES: 4,815 SF**SHORT TERM

- REQUIREMENTS: 2, OR 1 PER 3,800SF
- 2 SPACES

# **TEMPORARY LODGING:** 80 RENTABLE ROOMS SHORT TERM

- REQUIREMENTS: 2, OR 1 PER 20 RENTABLE ROOMS

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

- 4 SPACES

## 1880 NW FLANDERS ST TOTAL:

10 SPACES REQUIRED

## 1819 NW EVERETT STREET TOTAL:

10 SPACES REQUIRED

## **TOTAL SPACES PROVIDED:**

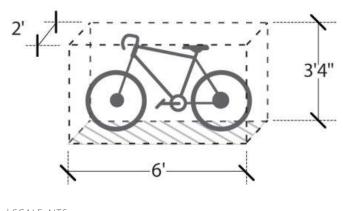
SHORT TERM: 20 SPACES REQUIRED, 20 SPACES PROVIDED

# SHORT TERM BICYCLE PARKING: RACK DESIGN LANDSCAPE FORMS (BOLA)

# 33.266.200 BICYCLE PARKING DEVELOPMENT STANDARDS







| SCALE: NTS

SHORT TERM BIKE PARKING

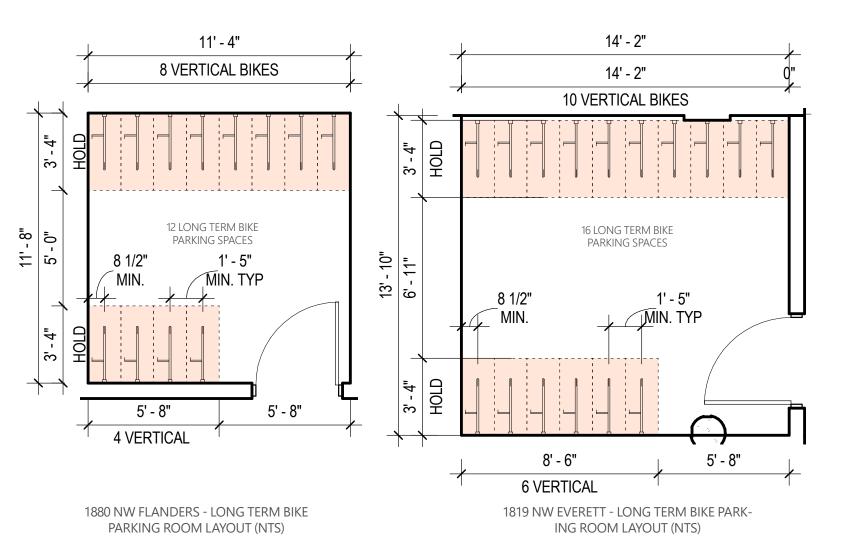


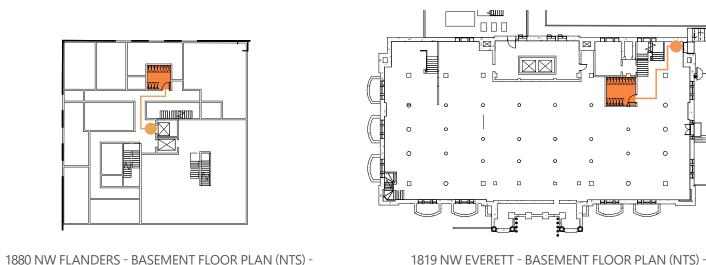












#### **1819 NW EVERETT STREET**

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

#### **RESTAURANT & BAR: 4,649 SF**

#### LONG TERM

- REQUIREMENTS: 2, OR 1 PER 1,000SF
- 5 SPACES

## **RETAIL SALES AND SERVICES: 30,658 SF**

#### **LONG TERM**

- REQUIREMENTS: 2, OR 1 PER 2,700SF
- 11 SPACES

## 1880 NW FLANDERS STREET

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

#### **RESTAURANT & BAR: 7,034**

#### LONG TERM

- REQUIREMENTS: 2, OR 1 PER 1,000SF
- 8 SPACES

## **RETAIL SALES AND SERVICES: 4,815 SF**

#### **LONG TERM**

- REQUIREMENTS: 2, OR 1 PER 2,700SF
- 2 SPACES

# TEMPORARY LODGING: 80 RENTABLE ROOMS LONG TERM

- REQUIREMENTS: 2, OR 1 PER 40 RENTABLE ROOMS
- 2 SPACES

## 1880 NW FLANDERS ST TOTAL:

12 SPACES REQUIRED

## 1819 NW EVERETT ST TOTAL:

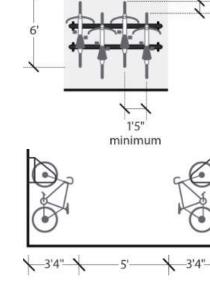
16 SPACES REQUIRED

#### **TOTAL SPACES PROVIDED:**

LONG TERM: 28 SPACES REQUIRED, 28 SPACES PROVIDED

# LONG TERM BICYCLE PARKING: DERO ULTRA SPACE SAVER SQUARED

# 33.266.200 BICYCLE PARKING DEVELOPMENT STANDARDS





# Single Sided

## LONG TERM BIKE PARKING

**BIKE ROOM ACCESS** 

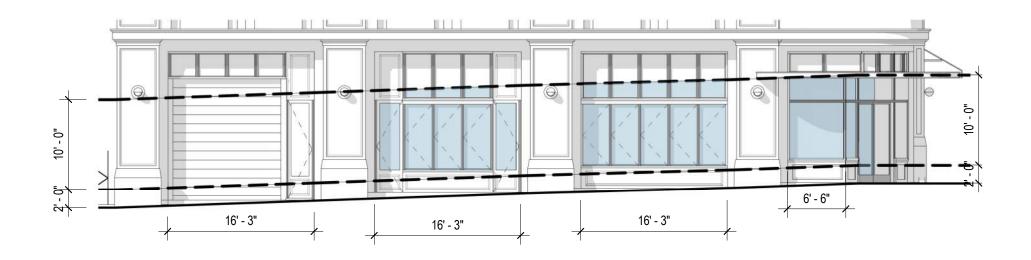


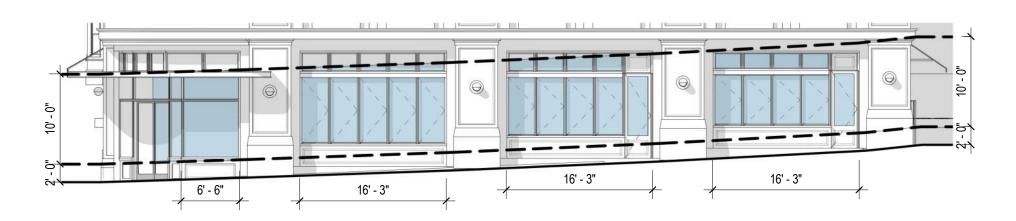




**BIKE ROOM ACCESS** 







## **GROUND FLOOR WINDOWS**

PER ZONING CODE SECTION 33.130.230 A MINIMUM OF 40% GLAZING ON THE GROUND FLOOR IS REQUIRED PER TITLE 33

## <u>GROUND FLOOR GLAZING - NORTH ELEVATION</u>

LEVEL 1 AREA IN MEASURED ZONE = 857 SF STOREFRONT AREA = 361 SF 40%

<u>GROUND FLOOR GLAZING - WEST ELEVATION</u> LEVEL 1 AREA IN MEASURED ZONE = 872 SF

STOREFRONT AREA = 369 SF

42%

## **ELEVATIONS - GROUND FLOOR GLAZING (HOTEL)**

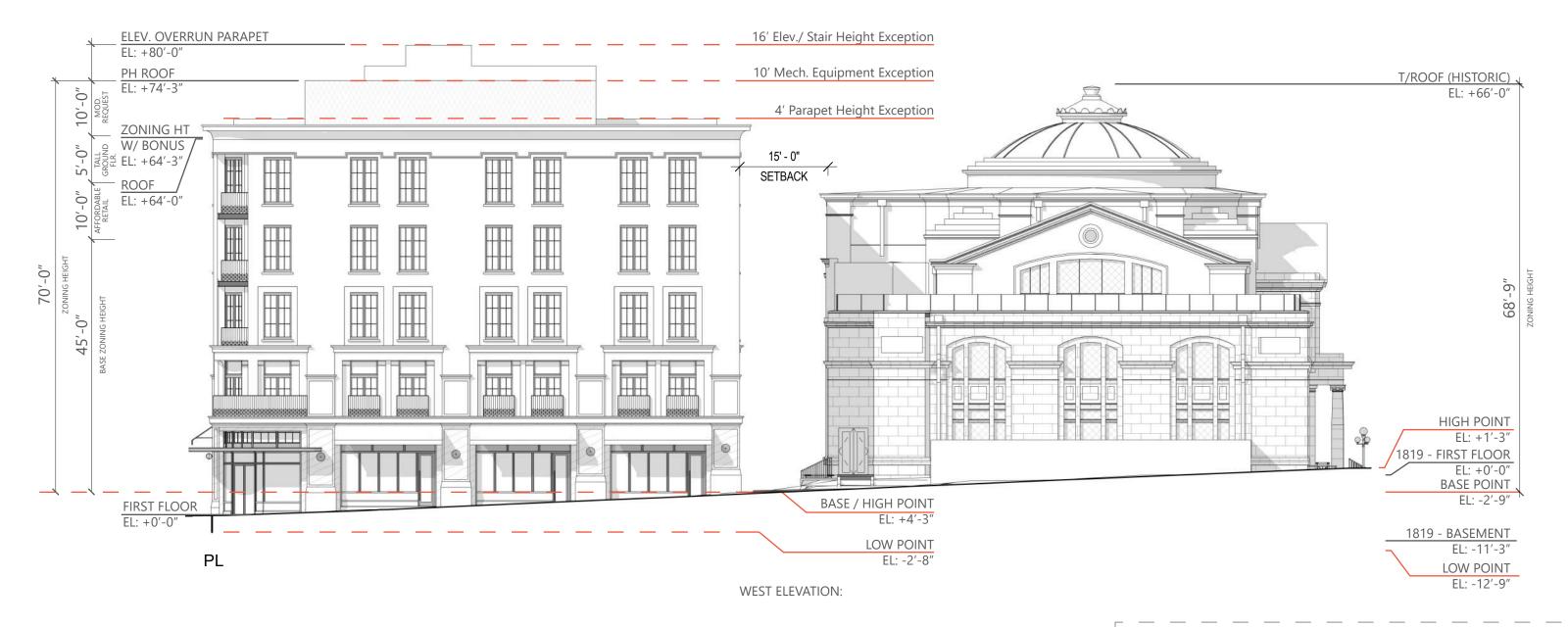
| SCALE: 3/32" = 1'-0"







1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR



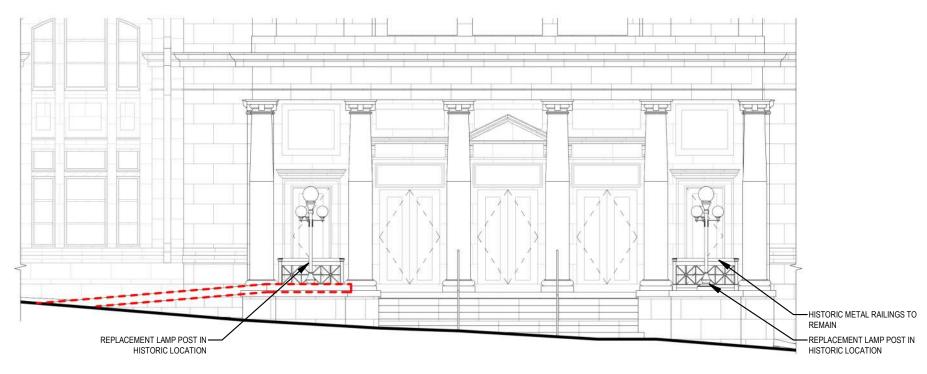
## **ZONING HEIGHT DIAGRAM**

| SCALE: 1/16" = 1'-0"

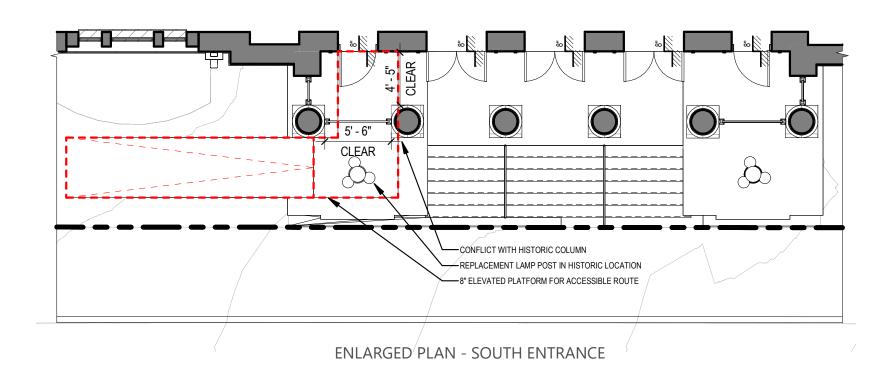




AREA FOR CITY STAMP



MAIN ENTRANCE ELEVATION





| SCALE: 1/8" = 1'-0"





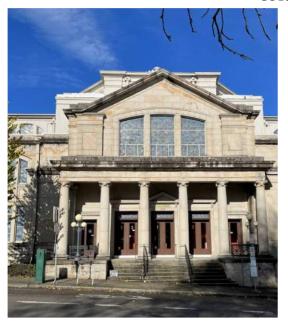




8" STEP TO ENTRY DOORS



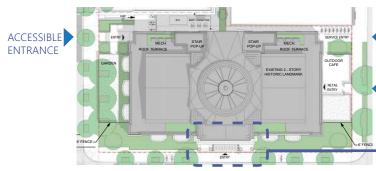
CLEAR BETWEEN EXTERIOR WALL AND COLUMN BASE



**EXISTING ENTRANCE CONDITIONS** 



HISTORIC ENTRANCE CONDITIONS



ACCESSIBLE ENTRANCE

ACCESSIBLE ENTRANCE

SEE ENLARGED SOUTH ENTRANCE PLAN

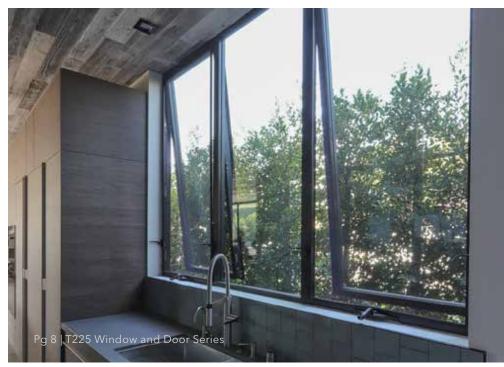




## T225 Windows

Our T225 **Modern Look** or **Steel Look** windows are offered as fixed, casements, awnings and hopper configurations. Windows can be mulled to create long runs of fixed glazing which transition effortlessly into our T225 Doors.

Frame Depth: 2 ¼"
High Performance Glass:
1" dual-glazed, Low-E glass





PRODUCT DATA: FIXED WINDOWS (1880 NW FLANDERS ST.)





Fixed

Long equal leg frame shown



Pa 10 | T225 Window and Door Series

PRODUCT DATA: FIXED WINDOWS (1880 NW FLANDERS ST.)







# Oasis 10000 Multi-Fold Doors

Thermally broken, high-performance, and energy efficient, the Oasis 10000 Multi-Fold Door is a top-hung system for large openings. Door panels can be folded away to reveal an uninterrupted view when open, yet remain in the same plane when closed. Removing the need for fixed panels or structural posts allows for maximum sunlight and fresh air to enter the room. Designed and engineered as an outswing for maximum weather performance.





PRODUCT DATA: OPERABLE STOREFRONT (1880 NW FLANDERS ST.)







## Oasis 10000 Specifications

Individual door panels up to 39" wide,10' tall, not to exceed 220 lbs. Overall door system length a maximum of 52' wide,10' tall.

Thermally Broken
Door panels and frames for improved energy performance.

**High Performance Glass** 1" dual-glazed, Low-E glass, see Technical Specifications for NFRC Chart

## Frame Depth

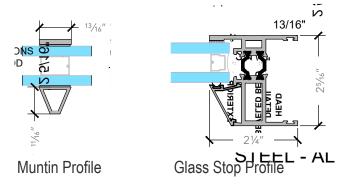
6 1/2"

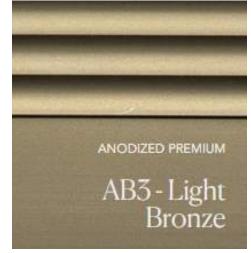
#### Hardware

Stainless steel precision bearing top rollers, intermediate pivot hinges.

## **Twin-Point Lock**

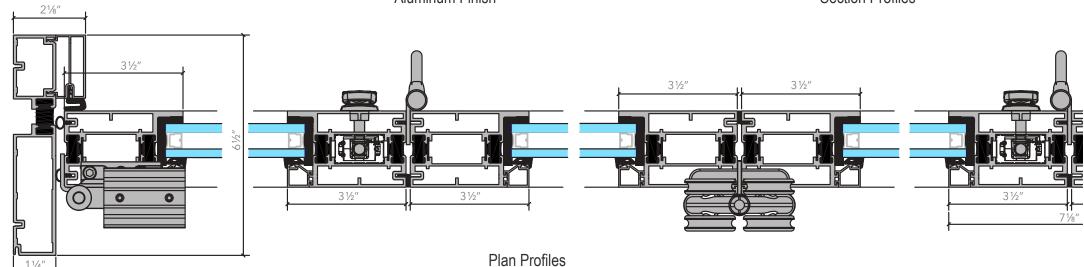
Streamline twin-point locks keep doors secure when closed and provide for easy, one-handed operation; sturdy 2 1/4" thick door profile.





Aluminum Finish

Section Profiles



Pg 44 | 10000 Multi-Fold Doors

PRODUCT DATA: OPERABLE STOREFRONT (1880 NW FLANDERS ST.)







# T225 Swing Doors

Size Single door panel sizes up to 3'6" wide, 9' tall or 3' wide, 10' tall.

#### Hardware

3-point latch lock, throws shoot bolts top and bottom when the lever is raised, locks with key from the exterior and thumb turn interior.

## **Weather Tightness**

Double weather stripped for maximum air and water resistance.

## Configurations

Inswing or outswing, single or pairs, with sidelite and transom options.



T225 Swing Door Configuration Options



SINGLE



PAIR



SINGLE MULLED SIDELIGHTS



PAIR MULLED SIDELIGHTS &TRANSOM





## PRODUCT DATA: FRENCH DOORS (1880 NW FLANDERS ST.)







# Inswing Pair 35/8" 35/8" 21/4" 77/16" INSWING SINGLE MULLED SIDELIG Plan Profiles Muntin Profile 13/16" ANODIZED PREMIUM AB3 - Light Bronze SIEEL - AL

Aluminum Finish

Pg 16 | T225 Window and Door Series

Glass Stop Profile

## PRODUCT DATA: FRENCH DOORS (1880 NW FLANDERS ST.)

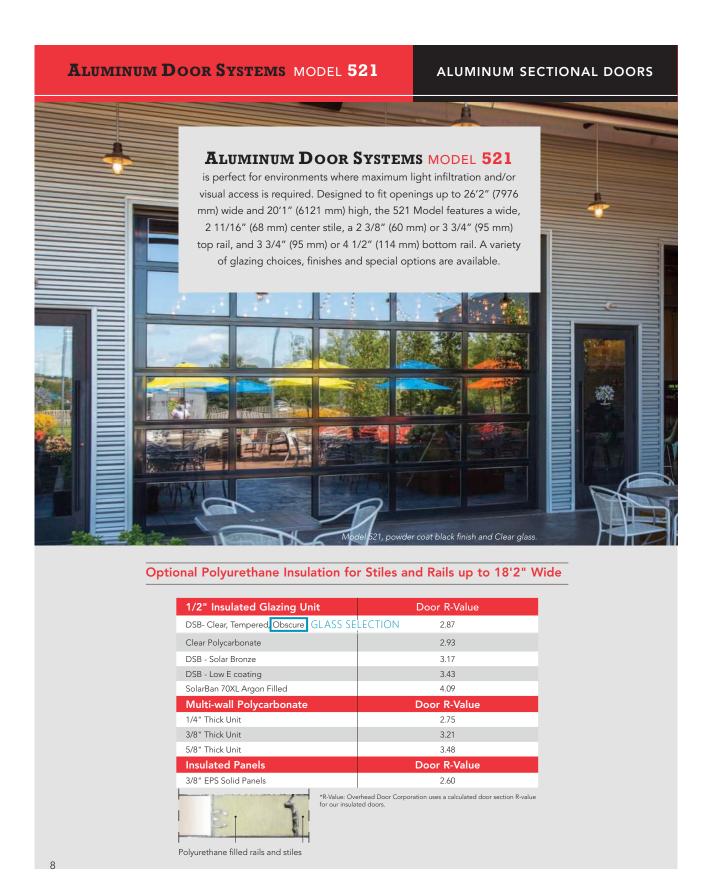
| SCALE: NTS







Section Profiles



## PRODUCT DATA: OVERHEAD ALUMINUM DOOR (1880 NW FLANDERS ST.)

| SCALE: NTS







### MODEL **521**



#### Standard Features at a Glance **Options**

Glazing Options <sup>†</sup> : 1/8" (3 mm) DSB; 1/8" (3 mm) or 1/4" (6 mm) acrylic; 1/8" (3 mm) or 1/4" (6 mm) tempered;
1/8" (3 mm) or 1/4" (6 mm) acrylic; 1/8" (3 mm) or 1/4" (6 mm) tempered;
1/8" (3 mm) or 1/4" (6 mm) clear polycarbonate;
1/4" (6mm) and 3/8" twin-wall polycarbonate, 5/8" triple-
wall polycarbonate; 1/4" (6 mm) 3/8" (10 mm) and 5/8" (16 mm) twin-wall
polycarbonate, triple-wall polycarbonate 1/4" (6 mm) wire glass; 1/2" (12 mm) insulated glass
Electric operator or chain hoist
Bottom sensing edge
3" track
Bracket mounting (not available on full vertical door tracks)
Higher-cycle springs in 25k, 50k, 75k, 100k cycles
Exhaust ports
Four-section pass door
Wind load and impact rated door available
Posi-tension drums
Bronze anodization
Powder coat finish
Pass door
<sup>†</sup> Contact your local Overhead Door™ Distributor for special glazing requirements. Verify 1/4″ (6 mm) glass applications with factory.

#### **Structure Options**



Clear (standard)







Black



**Powder Coat Finishes** Select from approximately 200 RAL powder coat color options to best match your home.

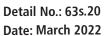
Cherry with Flame Dark Walnut Actual door colors may vary from brochure photos due to fluctuations in the printing process. Always request a color sample from your Overhead Door™ Distributor for accurate color matching.

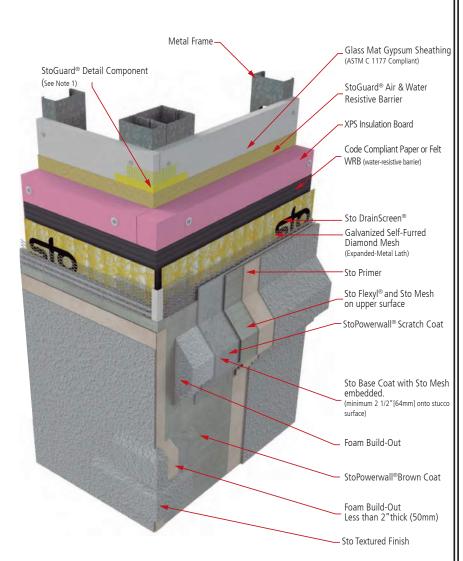
or Height Number of Sections m) 4		
m) 4		
516 mm to 3073 mm) 5		
3099 mm to 3683 mm) 6		
3708 mm to 4293 mm) 7		
4318 mm to 4902 mm) 8		
(4928 mm to 5512 mm) 9		
(5537 mm to 6121 mm) 10		
 **Special construction. Consult your local Overhead™ Door Distributor for additional information.		
43 (49 (5)		

overheaddoor.com



### Sto Powerwall®ci **Foam Buildouts**





#### Notes:

- 1. Refer to StoGuard Product Use Chart (Table 1.1) for StoGuard detail components; joint treatment, rough opening protection, backing for masonry anchors, or transitions to dissimilar materials, joints and seams in construction.
- 2. Refer to General Notes for specific information and design guidance on wall assembly components.
- 3. XPS Insulation options, in compliance with ASTM C 578:
- Owens Corning Type IV XPS rigid insulation board.
- DuPont Type IV XPS rigid insulation board.
- 4. Glass mat gypsum sheathing in compliance with ASTM C1177, exterior grade and Exposure 1 wood based sheathing, or cementitious sheathing in compliance with ASTM C1325.
- 5. Minimum 2.5 lb/yd2 (1.4 kg/m2) self-furred galvanized diamond mesh metal lath.
- 6. Stucco scratch and brown coat material in compliance with ASTM C926 and manufactured or listed by Sto Corp.
- 7. Components not identified as Sto are furnished by other manufacturers and are not necessarily installed by trades who install the Sto products. Refer to project specific contract documents.

www.stocorp.com



ATTENTION

Sto products are intended for use by qualified professional contractors, not consumers, as a component of a larger construction assembly as specified by a qualified design professional, general contractor or builder. They should be installed in accordance with those specifications and Sto's instructions. Sto Corp. disclaims all, and assumes no, liability for on-site inspections, for its products applied improperly, or by unqualified persons or entities, or as part of an improperly designed or constructed building, for the nonperformance of adjacent building components or assemblies, of for on activities beyond Sto's control. Improper use of Sto products or use as part of an improperly designed or constructed larger assembly or building may result in serious damage to Sto products, and to the structure of the building or its components

STO CORP. DISCLAIMS ALL WARRANTIES EXPRESSED OR IMPLIED EXCEPT FOR EXPLICIT LIMITED WRITTEN WARRANTIES ISSUED TO AND ACCEPTED BY BUILDING OWNERS IN ACCORDANCE WITH STO'S WARRANTY PROGRAMS WHICH ARE SUBJECT TO CHANGE FROM TIME TO TIME. For the fullest, most current information on proper application, clean-up,

FOAM DETAIL

### PRODUCT DATA: STUCCO

I SCALE: NTS







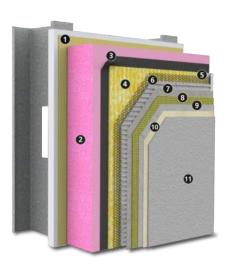
Property of Sto Corp. All Rights Reserved



System Bulletin Building with conscience.

## StoPowerwall® ci

Portland cement stucco with continuous air and water-resistive barrier, continuous insulation, advanced cavity wall design, and high performance finish



Substrate: Glass mat gypsum sheathing in compliance with ASTM C 1177 (or building code compliant wood-based sheathing - Type V construction only)

<ol> <li>StoGuard® Air and Water-Resistive Barrier</li> <li>Owens Corning or Dupont Type IV XPS Insulation Board</li> <li>Code compliant paper or felt Water-Resistive Barrier</li> <li>Sto DrainScreen® drainage mat</li> <li>Code compliant miniumum 2.5 lb/yd² (1.4 kg/m²) self-furred galvanized steel diamond mesh metal lath</li> <li>ASTM C 926 compliant stucco scratch and brown coat (as manufactured or listed by Sto Corp.)</li> <li>Sto Crack Defense (optional): Sto base coat with Sto Mesh embedded</li> <li>Sto primer (optional)</li> <li>Choose among:         <ul> <li>Sto Textured Finishes</li> <li>Sto Custom Cast Finish: Wood or Brick</li> <li>Sto Signature Series or Sto Speciatly Finishes</li> </ul> </li> </ol>		
3) Code compliant paper or felt Water-Resistive Barrier 4) Sto DrainScreen® drainage mat 5) Code compliant miniumum 2.5 lb/yd² (1.4 kg/m²) self-furred galvanized steel diamond mesh metal lath 6), 7) ASTM C 926 compliant stucco scratch and brown coat (as manufactured or listed by Sto Corp.) 8), 9) Sto Crack Defense (optional): Sto base coat with Sto Mesh embedded 10) Sto primer (optional) 11) Choose among:  • Sto Textured Finishes  • Sto Custom Cast Finish: Wood or Brick	1)	StoGuard® Air and Water-Resistive Barrier
<ul> <li>4) Sto DrainScreen® drainage mat</li> <li>5) Code compliant miniumum 2.5 lb/yd² (1.4 kg/m²) self-furred galvanized steel diamond mesh metal lath</li> <li>6), 7) ASTM C 926 compliant stucco scratch and brown coat (as manufactured or listed by Sto Corp.)</li> <li>8), 9) Sto Crack Defense (optional): Sto base coat with Sto Mesh embedded</li> <li>10) Sto primer (optional)</li> <li>11) Choose among: <ul> <li>Sto Textured Finishes</li> <li>Sto Custom Cast Finish: Wood or Brick</li> </ul> </li> </ul>	2)	Owens Corning or Dupont Type IV XPS Insulation Board
<ul> <li>Code compliant miniumum 2.5 lb/yd² (1.4 kg/m²) self-furred galvanized steel diamond mesh metal lath</li> <li>ASTM C 926 compliant stucco scratch and brown coat (as manufactured or listed by Sto Corp.)</li> <li>Sto Crack Defense (optional): Sto base coat with Sto Mesh embedded</li> <li>Sto primer (optional)</li> <li>Choose among: <ul> <li>Sto Textured Finishes</li> <li>Sto Custom Cast Finish: Wood or Brick</li> </ul> </li> </ul>	3)	Code compliant paper or felt Water-Resistive Barrier
furred galvanized steel diamond mesh metal lath  6), 7) ASTM C 926 compliant stucco scratch and brown coat (as manufactured or listed by Sto Corp.)  8), 9) Sto Crack Defense (optional): Sto base coat with Sto Mesh embedded  10) Sto primer (optional)  11) Choose among:  Sto Textured Finishes  Sto Custom Cast Finish: Wood or Brick	4)	Sto DrainScreen® drainage mat
(as manufactured or listed by Sto Corp.)  8), 9) Sto Crack Defense (optional): Sto base coat with Sto Mesh embedded  10) Sto primer (optional)  11) Choose among:  Sto Textured Finishes Sto Custom Cast Finish: Wood or Brick	5)	
Mesh embedded  10) Sto primer (optional)  11) Choose among:  • Sto Textured Finishes  • Sto Custom Cast Finish: Wood or Brick	6), 7)	·
11) Choose among:	8), 9)	
<ul><li>Sto Textured Finishes</li><li>Sto Custom Cast Finish: Wood or Brick</li></ul>	10)	Sto primer (optional)
	11)	<ul><li>Sto Textured Finishes</li><li>Sto Custom Cast Finish: Wood or Brick</li></ul>

#### STUCCO WALL

#### **System Description**

StoPowerwall ci is an energy efficient stucco wall assembly with a continuous air and water-resistive barrier and continuous insulation. It combines the strength and durability of traditional stucco with an advanced cavity wall design and Sto high performance finishes.

#### Uses

StoPowerwall ci can be used in residential or commercial wall construction where energy efficiency, superior aesthetics, and air and moisture control are essential in the climate extremes of

NOTH AMERICA					
Features	Benefits  Consistent color and aesthetics increase curb appeal				
Integrally colored factory blended textured finish					
Continuous exterior insulation	Energy efficient, reduced heating and cooling costs				
Impact and puncture resistant	resistant Withstands abuse, reduced maintenance				
Continuous air and moisture barrier	Protects against mold and moisture problems				
Fully tested, building code compliant					
Properties					
Weight (excluding sheathing / studs)	< 12 psf (56.6 kg/m²)				
Assembly Thickness (from	Nominal 3-5/8" (92 mm) with 2"				

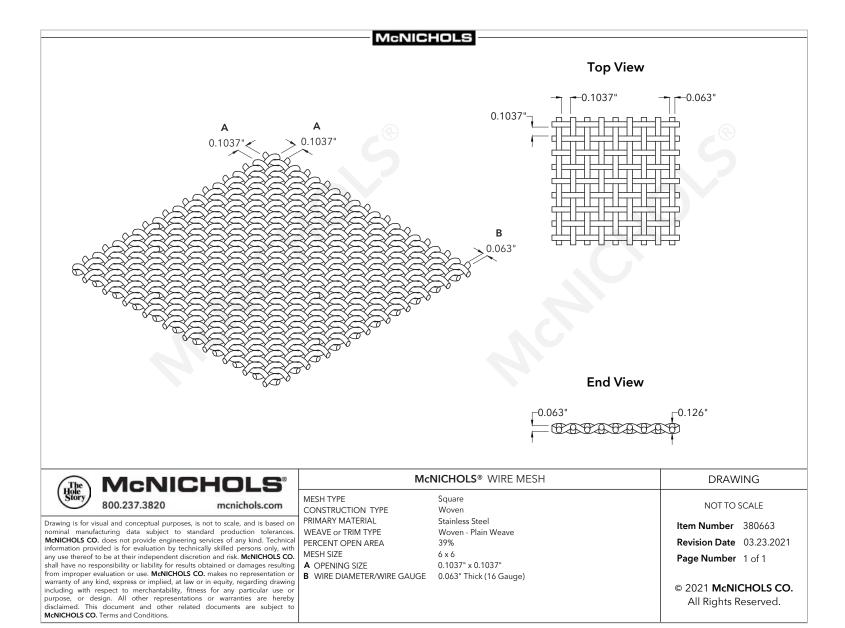
Properties	
Weight (excluding sheathing / studs)	< 12 psf (56.6 kg/m²)
Assembly Thickness (from exterior stud face)	Nominal 3-5/8" (92 mm) with 2" (51 mm) XPS
R-value (insulation only)	5 – 10 ft²•h•°F / Btu (0.88 – 1.76 m²•K / W)
Wind Load Resistance	Capable of achieving: +65, -48 psf (+3.11 to -2.29 kPa)
Compliance	IBC, IRC, and IECC     ASHRAE 90.1-2019
Construction Types, Fire Resistance	NFPA 285 tested for use on noncombustible construction     ASTM E119 1-hour rated assembly
Warranty	

Up to 12-year Limited Warranty available on Sto products, depending on options selected

#### Maintenance

Requires periodic cleaning to maintain appearance, repair of cracks and impact damage if they occur, recoating to enhance appearance of weathered finish. Sealants and other façade components must be maintained to prevent water infiltration.

AREA FOR CITY STAMP



or **B** WIRE DIAMETER/WIRE GAUGE 0.063" Thick (16 Gauge)

MECHANICAL SCREEN WALL

### PRODUCT DATA: MECHANICAL SCREEN & EXTERIOR LIGHT FIXTURE

| SCALE: NTS





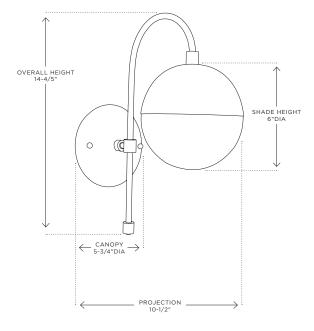


## CEDAR & MOSS

NORTHWEST MODERN GLOBE SCONCE

AVAILABLE SIZES:

A0054 - 6"DIA (shown in Brushed Satin)





#### DETAILS

© 2021 McNICHOLS CO. All Rights Reserved.

UL RATING: Damp NUMBER OF SOCKETS: 1 MAX WATTAGE: 40W

AVAILABLE SOCKET TYPES: Medium Base SHADE SHOWN: B0017 - 6" White Globe (included)

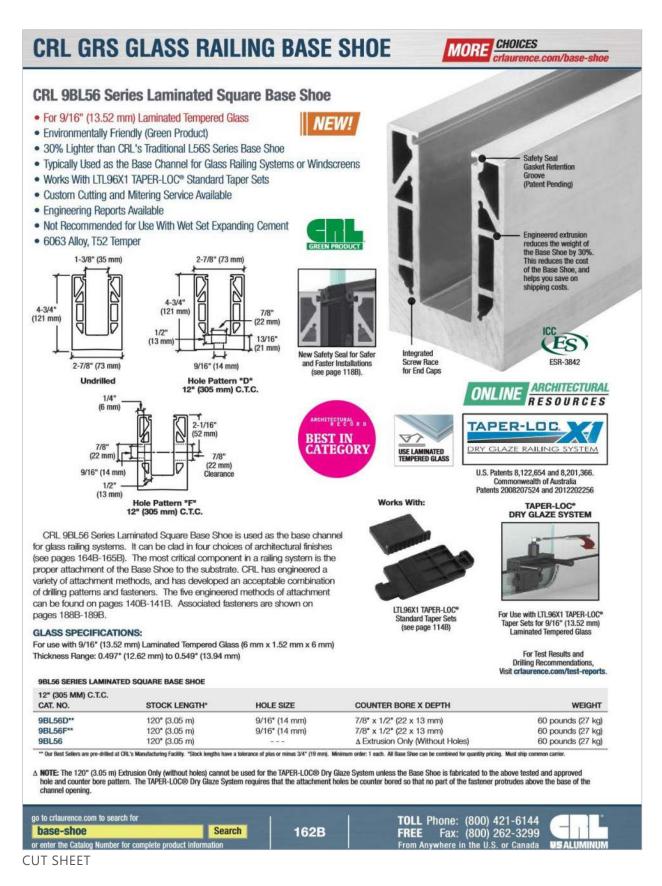
#### FIXTURE FINISH

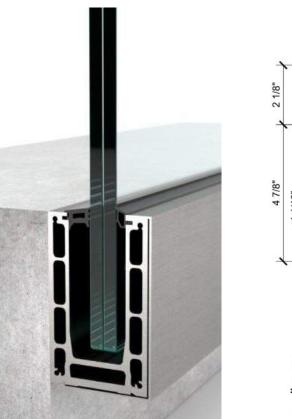


EXTERIOR LIGHT FIXTURE

REJUVENATION.COM

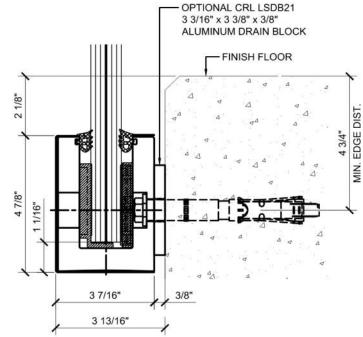
A.7 LU 22-204531 HRM





PRODUCT IMAGE - BASE DETAIL

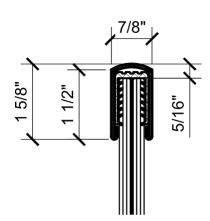
**ELEVATION DETAIL** 



# FASCIA MOUNT DETAIL CONCRETE SUBSTRATE

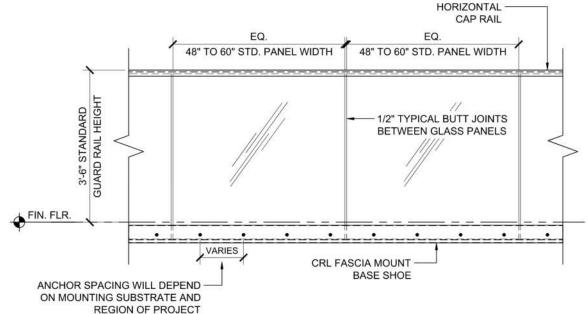
ANCHOR SPACING WILL DEPEND ON MOUNTING SUBSTRATE AND REGION OF PROJECT 12" O.C. SPACING IS STANDARD

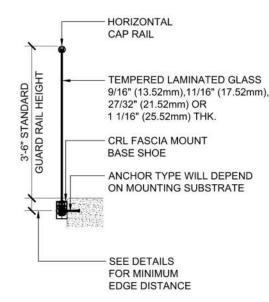
SECTION DETAIL AT BASE



GRCR5 - 7/8" x 1 1/2" FOR 9/16" (13.52mm) LAMINATED TEMPERED GLASS

TOP CAP DETAIL





SECTION DETAIL

## PRODUCT DATA: GLASS GUARDRAIL (1819 NW EVERETT ST.)

I SCALE: NTS







**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

AREA FOR CITY STA

lob Name				Location	,	
Purchaser				Engineer		
				Reference	Approval	Construction
Submitted to					Approvai	Construction
Jnit Designation	on			Schedule #		
		Specifications			6	
	Nominal Capacity	Cooling (Btu/h)	24,			
		Heating (Btu/h) Cooling (Btu/h)	27,ı 4,405 -			MARKET
Performance	Capacity Range	Heating (Btu/h)	5,700 -			
	SEER / EER			/ 9.6		AQN24VFUAGM
	HSPF Condensate (pints	/hour)	7.		•	AQN24VFUAGIVI
	Voltage (ø/V/Hz)	<u> </u>	1 / 208-		-	
	Working Voltage F		176 - 254 (max. 3%			
ower	Rated Current (Low/Std./Max.)	Cooling (A) Heating (A)	2.6 / 11. 2.3 / 13.	9 / 12.5		SAMEUNO
	Max. Breaker (A)		2	0		
	Min. Circuit Ampa		12		1	
	W X H X D (inches)	Indoor Unit Outdoor Unit	41 15/16 X 1 34 5/8 X 31 9		I Waster	
imensions	Weight (lbs.)	Indoor Unit	34 3/8 X 31 9		3	
		Outdoor Unit	11/16		11	Short Name
	Condensate Conn		11/16 Aluminum Fin		*	1
	Indoor & Outdoor	Type FPI	Aluminum Fin			A OVO AVELLA OM
eat Exchanger	Unit	Pipe Diameter	1/4	nch		AQX24VFUAGM
	Indoor Unit Outdoor Unit		2 row / 2 row /			
	Indoor Unit (dB)	(L/H)	30 /		Low ambient control built	in
ound Pressure Level	Outdoor Unit (dB)	High	5			
	1	Cooling	Standard: 1		<ul> <li>Outdoor unit shall provide</li> <li>3 interconnect power cable</li> </ul>	e 208/230V power to indoor unit via 14 AW
perating	Outdoor		0 ≤ T ≤ 115 with wi		3 interconnect power cable	•
emperatures (°F)		Heating Cooling	5 ≤ T 61 ≤ T		• Electro-static, washable,	HD (high density) main filter as standard
	Indoor	Heating		80	Construction	
	Indoor & Outdoor	High side (flare)	1/-			e UL94 V0 with a galvanized steel mounting
Pipe Connections		Low side (flare) Im Line Set Length (ft.)	5/ 98 /		bracket	
	Maximum Vertical		5			alvanized steel with a baked on powder co
	Туре		R41		finish for durability	
Refrigerant	Control Method Factory Charge (o	7 \	Electronic Exp 58		Heat Exchanger	
terrigerant	Charged for	2.)	25		The heat exchanger shall be	be mechanically bonded fin to copper tube
	Additional Refriger	ant	0.16 oz./ft.	over 25 ft.	Refrigerant System	
	Manufacturer		Sam		The compressor shall be h BLDC Rotary	ermetically sealed, inverter controlled, Twir
ompressor	Type RLA (A)		DC, Inverter E		-	
	Operating	Cooling (low/std./high)	15 / 7	6 / 88	Refrigerant flow shall be co at outdoor unit	ontrolled by EEV (electronic expansion valv
	Frequency (Hz)	Heating (low/std./high)	15 / 76		at outdoor unit	
	Type Air Volume (max.	CFM)	BLDC motor with		Indoor Fan The indoor fan shall be a s	ingle, antibacterial cross-flow type
vaporator Fan	Consumption	W	2		The indoor fall shall be a s	iligie, altibacterial cross-flow type
	Operating Current	(A)	0.		Three fan speed settings a	nd auto setting
ondenser Fan	Motor	W/	BLDC motor w		Controls	
ondenser Fall	Output FLA	W Amps	0.9		Control signal shall be DD0	C type signal
	Condensate pump	•	ASP-MO-UNIV		Interconnect control wiring	shall be 16 AWG X 2 shielded wire between
		Standard	AQN-WRS (inclu	des sub-PCB and MWR-	outdoor and indoor units	
	Wired Controller		AON-WRP (inclu	des sub-PCB and MWR-	Unit shall be operated via v	wireless controller (included)
		Premium	WE10 controller			, ,
ccessories	Wall bracket (for o		CKN-250 25' - ILS2509		Optional wired control avai	iable
	Line sets - insulated and flared, interconnect cables included		50' - ILS5009		Convenience	
	Wind Baffle	Front			<ul> <li>Auto restart</li> </ul>	
	1	Back	WBMB-24		<ul> <li>Turbo mode (during cooli</li> <li>Auto changeover</li> </ul>	ng operation only)
	Certifications	PCR fuege indoor unit to	ETL & ETLc	urrent transformer over	<ul> <li>24 hour timer</li> </ul>	
Safety	PCB fuses, indoor unit terr Devices voltage protection, cranl		ninai biock thermai fuse, c case heating, temperature		Good'sleep mode     Quiet mode	
	<u> </u>		mpressor overload sensing		Dry mode	
/arranty		Years compressor, 3 Years	ar Parts, 120 Day limited	labor	•	
	-					
_						

CU PRODUCT DATA



\* NOTE: PRELIMINARY PRODUCT DATA: DESIGN AND LAYOUT PENDING FINAL ENGINEERING
\* EQUIPMENT TO COMPLY WITH TITLE 18 NOISE CONTROL

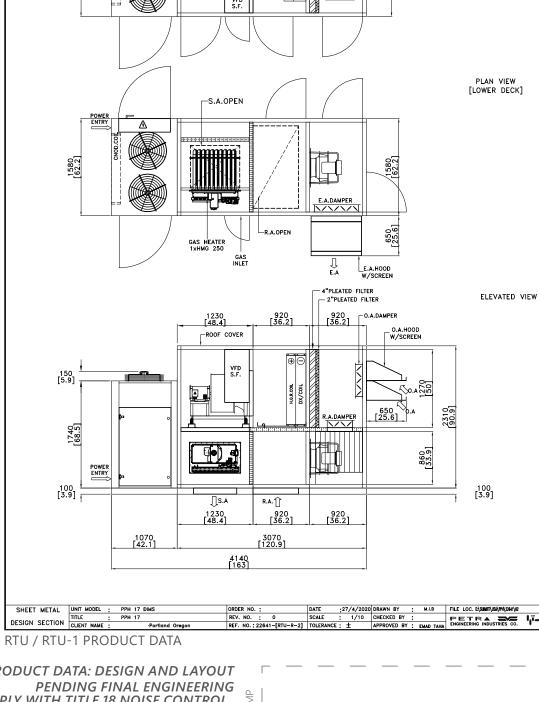
REQUIREMENTS



| SCALE: NTS



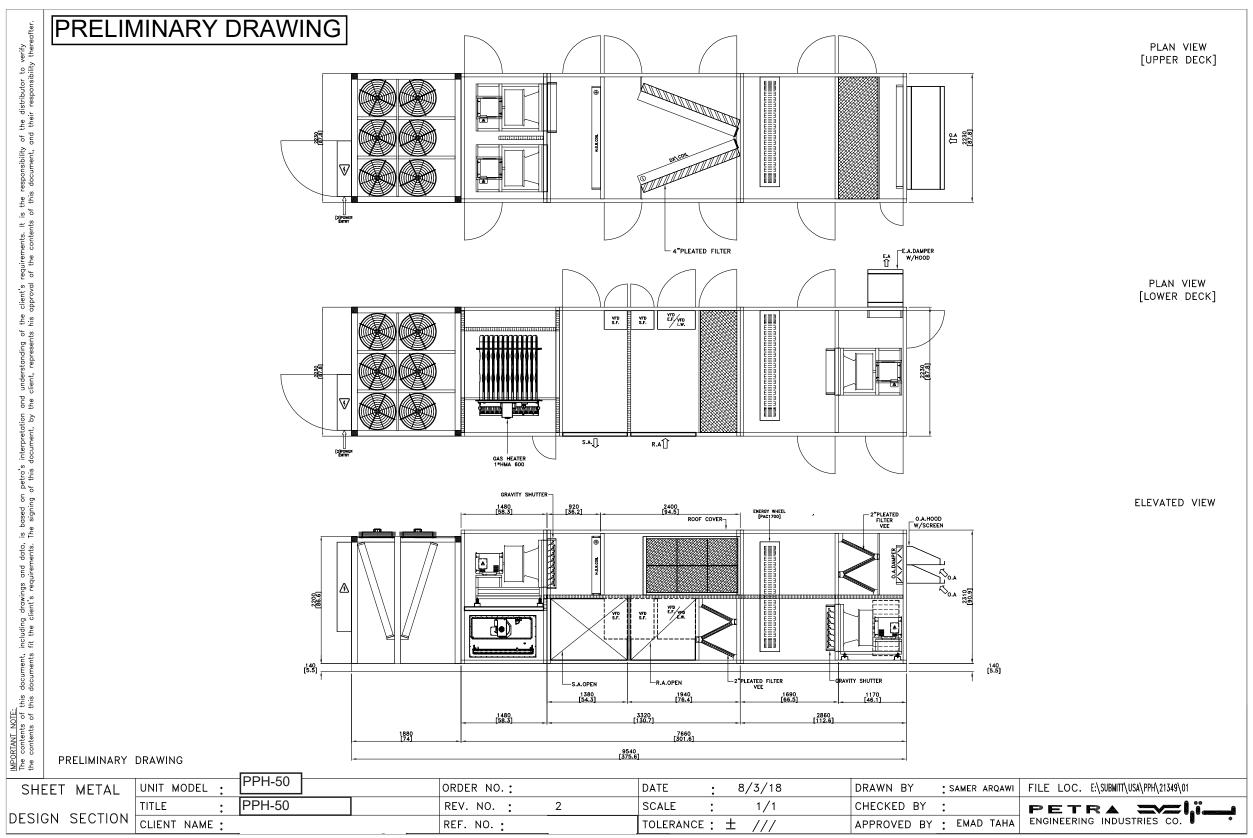




MPORTANT NOTE:
The contents of this document, including drawings and data, is based on petro's interpretation and understanding of the client's requirements. It is the responsibility of the distributor to verify the contents of this document in the client's requirements. The signing of this document by the client, represents his approval of the contents of this document, and their responsibility there

[1.25"]DRAIN CONN.

PLAN VIEW [UPPER DECK]



DOAS-1 PRODUCT DATA

PRODUCT DATA: MECHANICAL EQUIPMENT \*

\* NOTE: PRELIMINARY PRODUCT DATA: DESIGN AND LAYOUT PENDING FINAL ENGINEERING
\* EQUIPMENT TO COMPLY WITH TITLE 18 NOISE CONTROL REQUIREMENTS

| SCALE: NTS







## Tree Table Common / Botanical Name Size Location NW FLANDERS ST American Elm / Ulmus americana 36" 2 American Elm / Ulmus americana 30" 3 American Elm / Ulmus americana 32" 4 American Elm / Ulmus americana 40" Ginkgo biloba / Ginkgo 6" 5 Ginkgo biloba / Ginkgo 6" 6 Ginkgo biloba / Ginkgo 8 Frontier Elm / Ulmus 'Frontier' Frontier Elm / Ulmus 'Frontier 9 Newport Plum / Prunus cer. 'Newport' 25" 10 11 Linden / Tilia spp. Linden / Tilia spp. 12 30" Birch / Betula spp. 13 26" NW 19th AVE 0 Tree Protection Fencing, typ NW EVERETT ST LANDSCAPE: TREE PLAN





| SCALE: AS SHOWN



Cond. Status

Poor

Poor

Street Tree Good To be preserved

Poor To be removed

To be removed

To be removed

To be removed

Good To be preserved

To be removed

To be removed

Street Tree

Street Tree

Street Tree

Street Tree

On Site

On Site

On Site

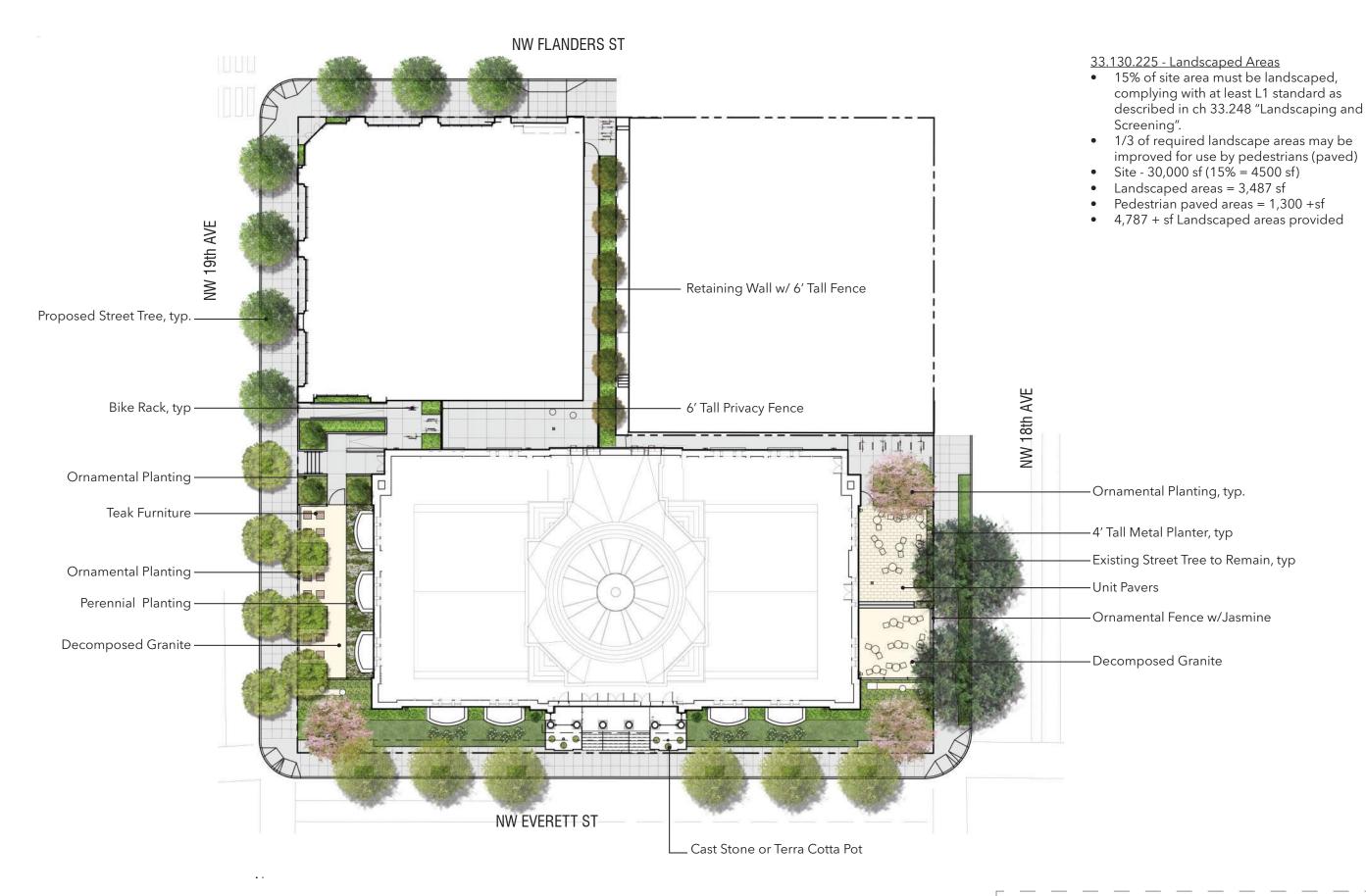
Street Tree

Street Tree

Street Tree

Street Tree

Onsite





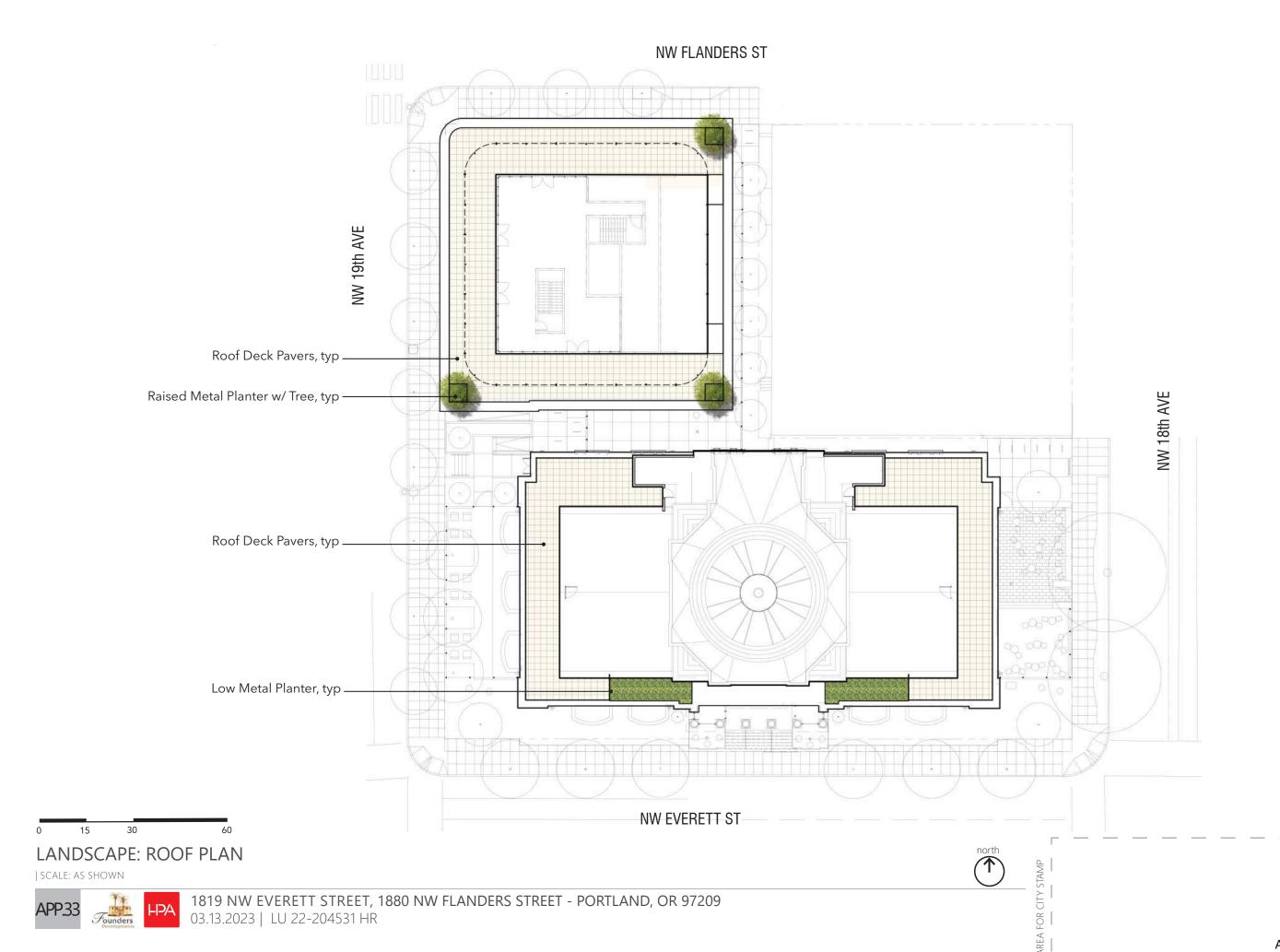
| SCALE: AS SHOWN



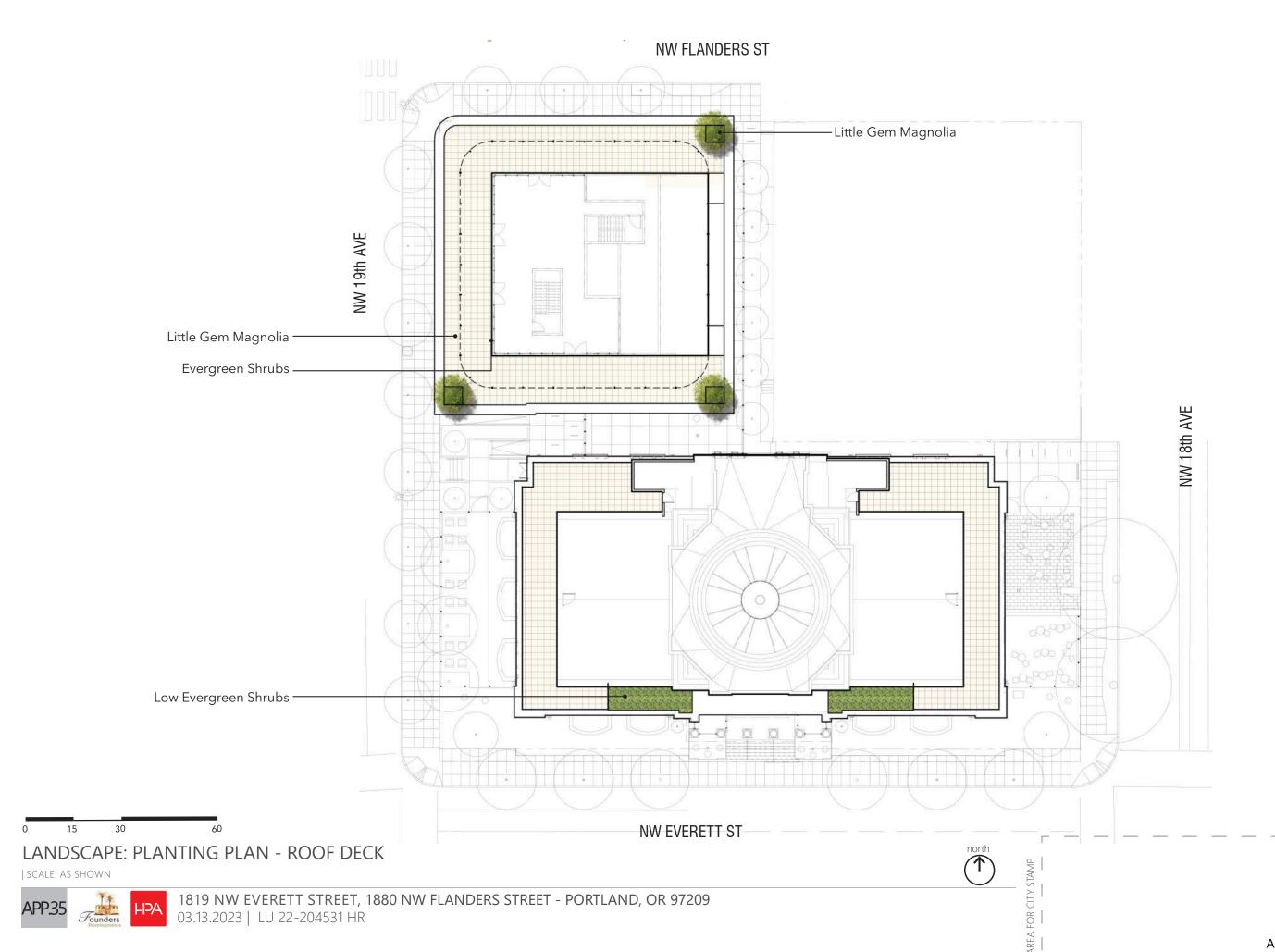














LANDSCAPE: EVERETT STREET ELEVATION

| SCALE: NTS

























LANDSCAPE: PRECEDENT IMAGES









Metal Picket Fence w/ Vine Planting (Vines not to exceed 50% coverage)



Raised Metal Planters w/ anti graffiti coating



Picket Guardrail



Bike Rack



At-grade and Roof Deck Pavers

## LANDSCAPE: SITE FURNISHINGS











Golden Colonade Gingko



Columnar Red Maple



Columnar Red Oak



Italian Cypress



Yoshino Cherry



Evergreen Magnolia



Little Gem Magnolia

LANDSCAPE: PLANT PALETTE - TREES











Japanese Spurge Kinnikinnik

LANDSCAPE: PLANT PALETTE - SHRUBS & GROUNDCOVER







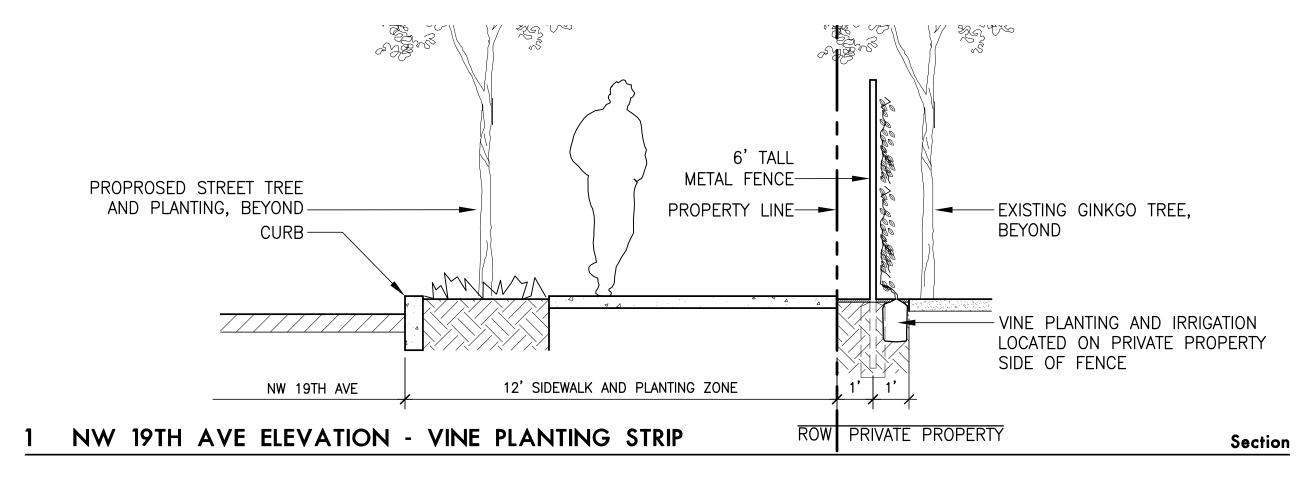


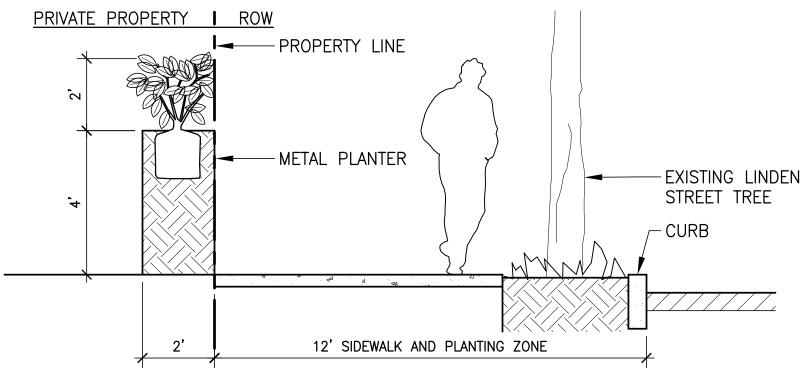
Snowdrops Blue Star Creeper

LANDSCAPE: PLANT PALETTE - PERENNIALS









## 2 NW 18TH AVE ELEVATION - RAISED METAL PLANTER

Section

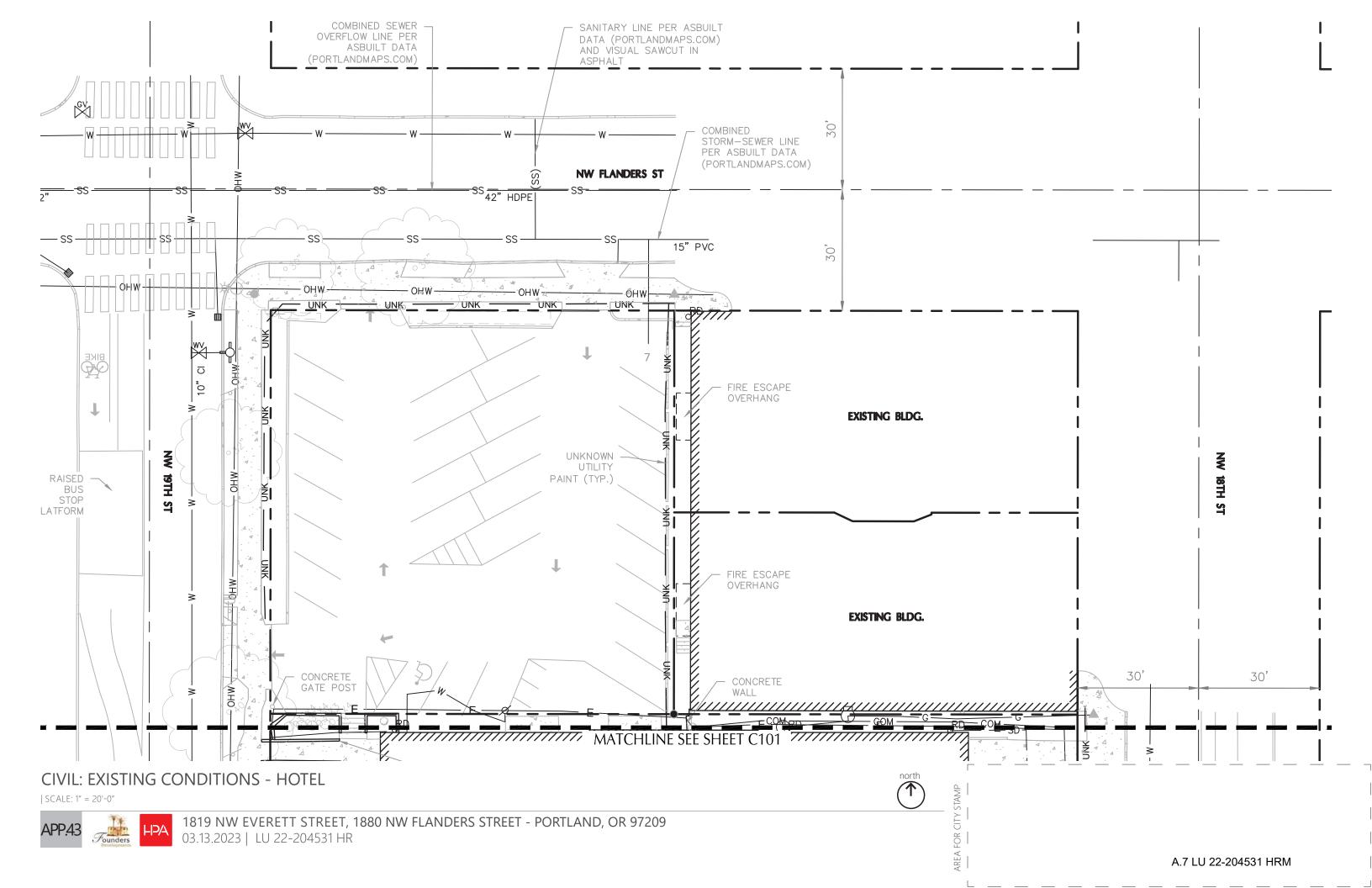
LANDSCAPE: PLANTING ELEVATIONS

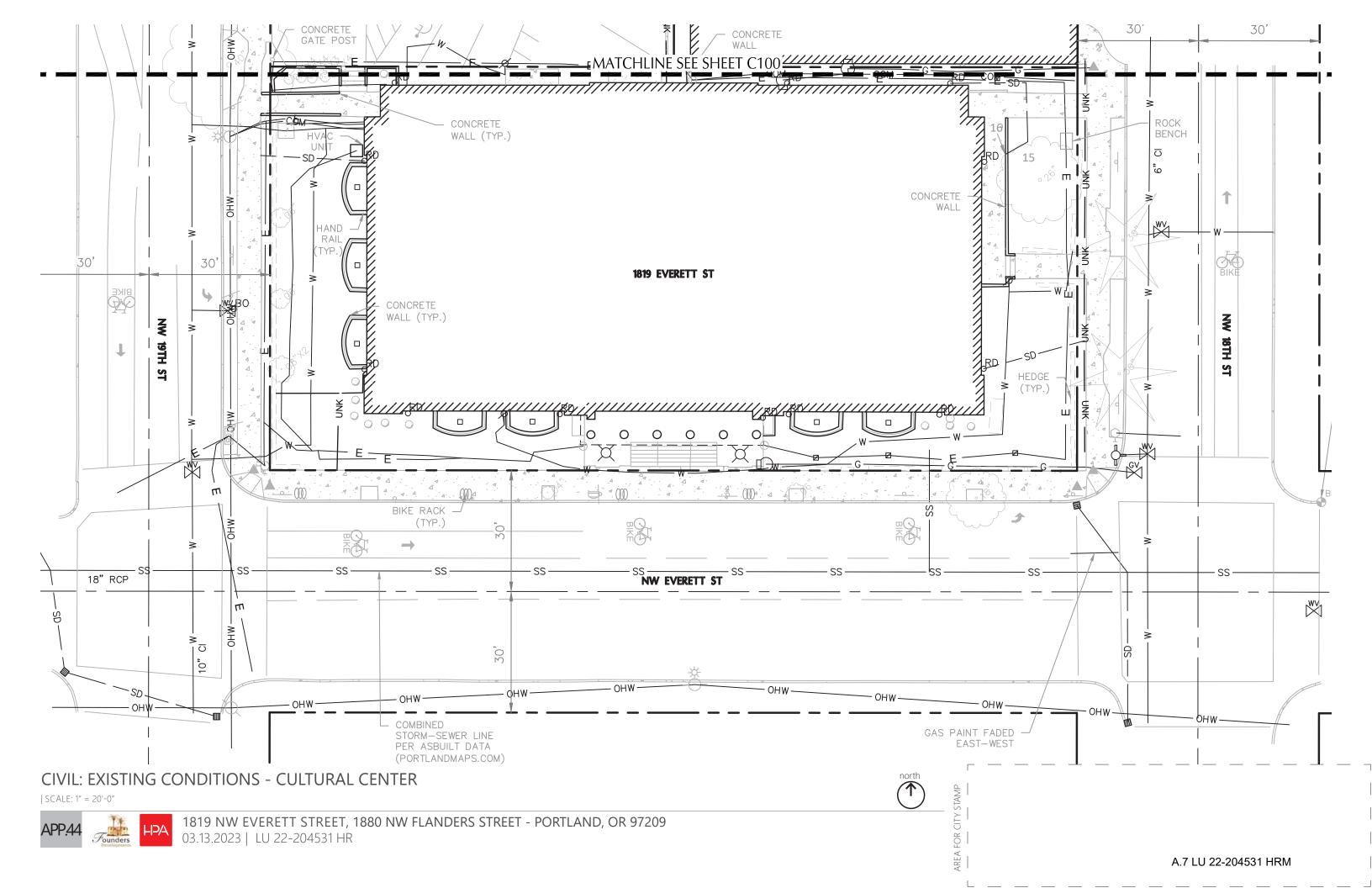


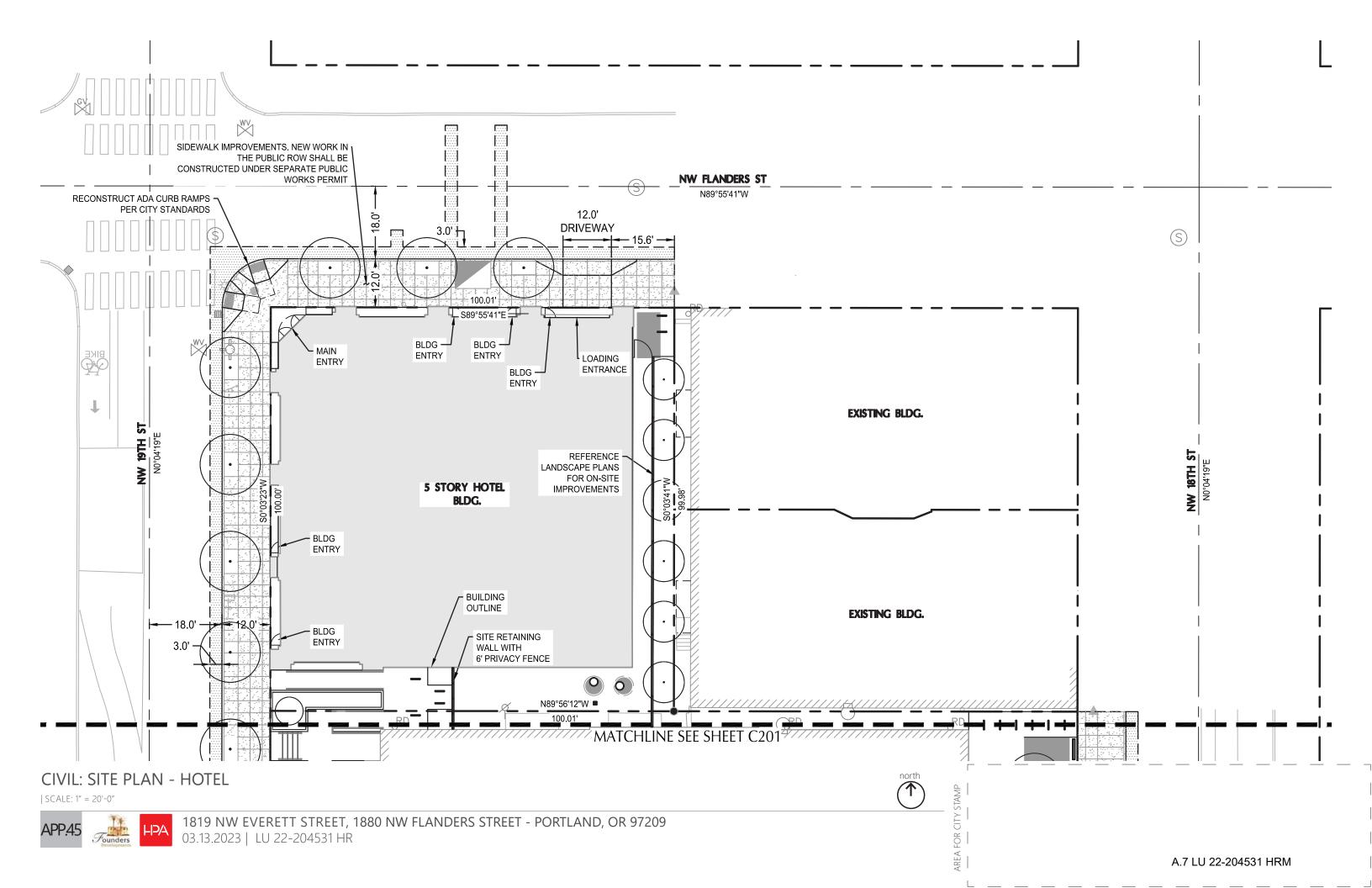
| SCALE: 3/8" = 1'-0"

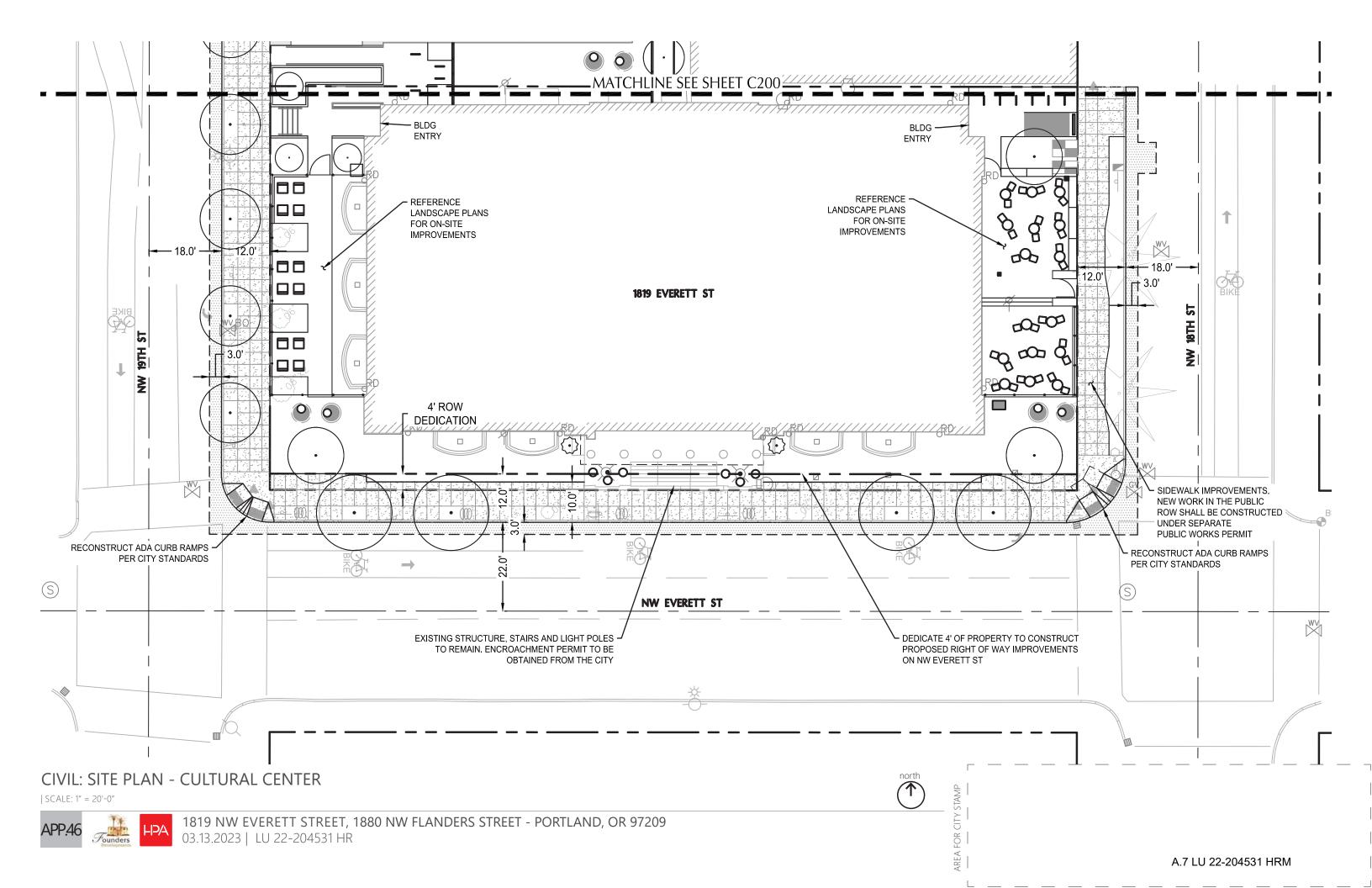


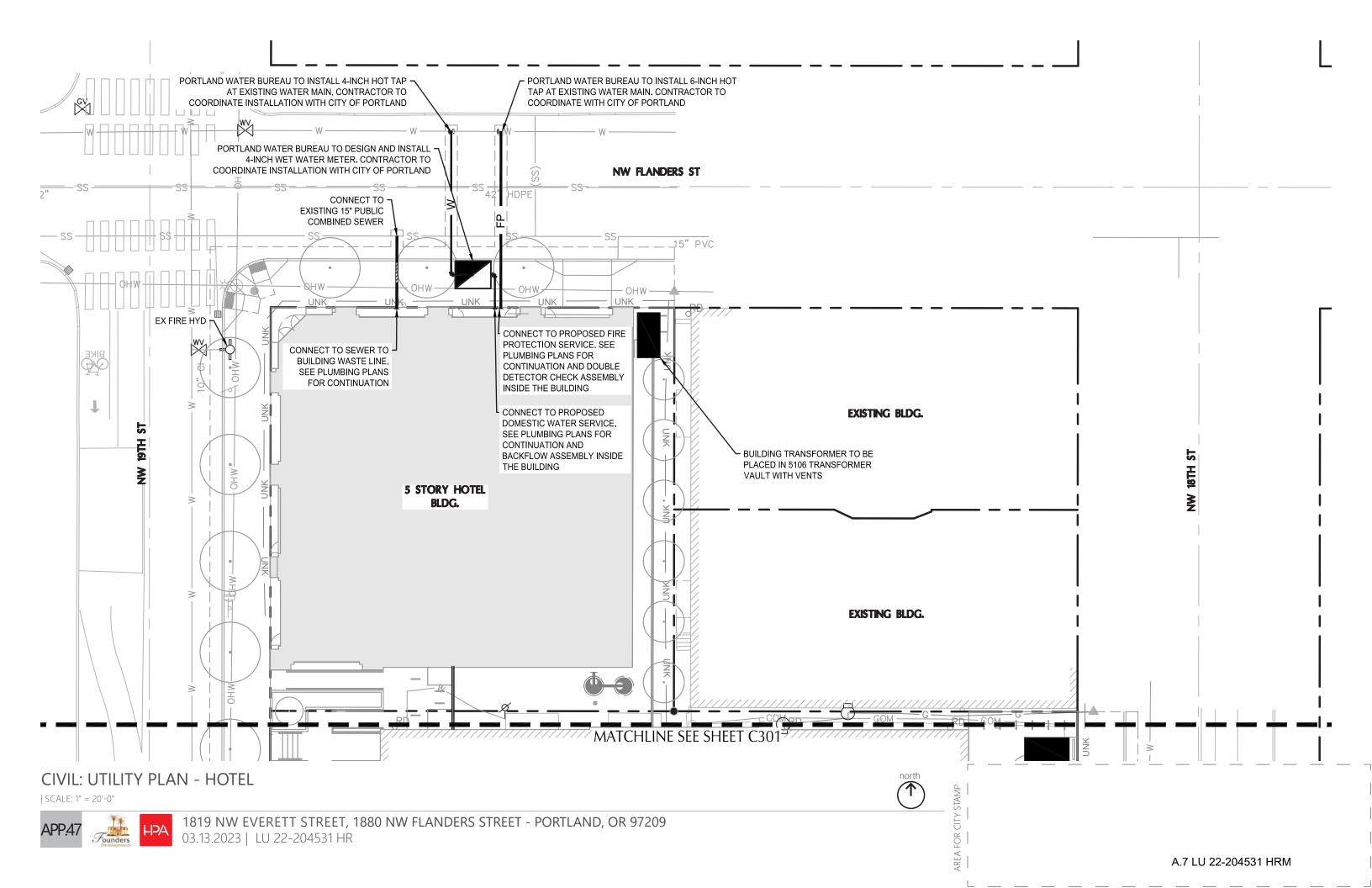
HP/A

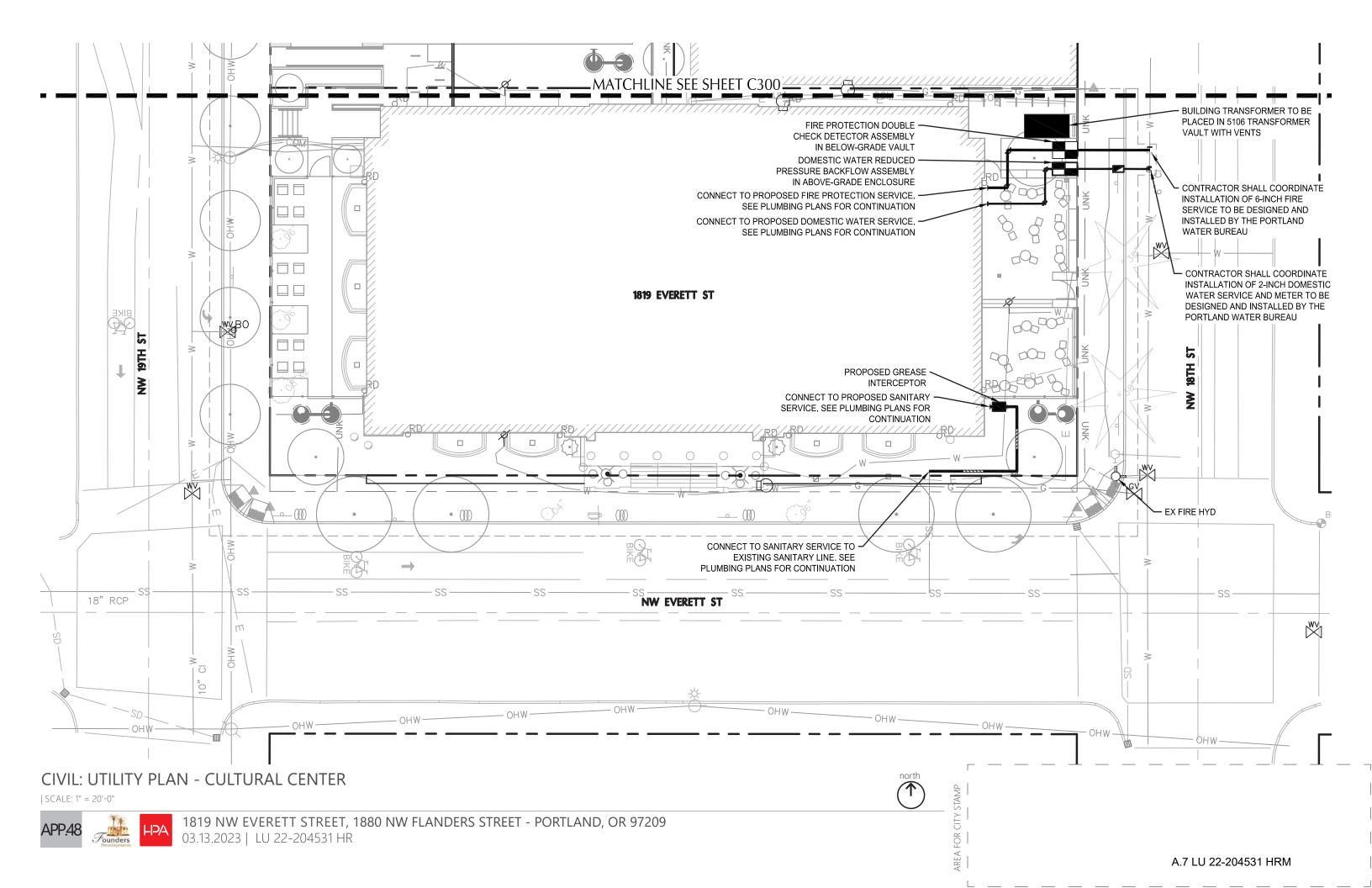


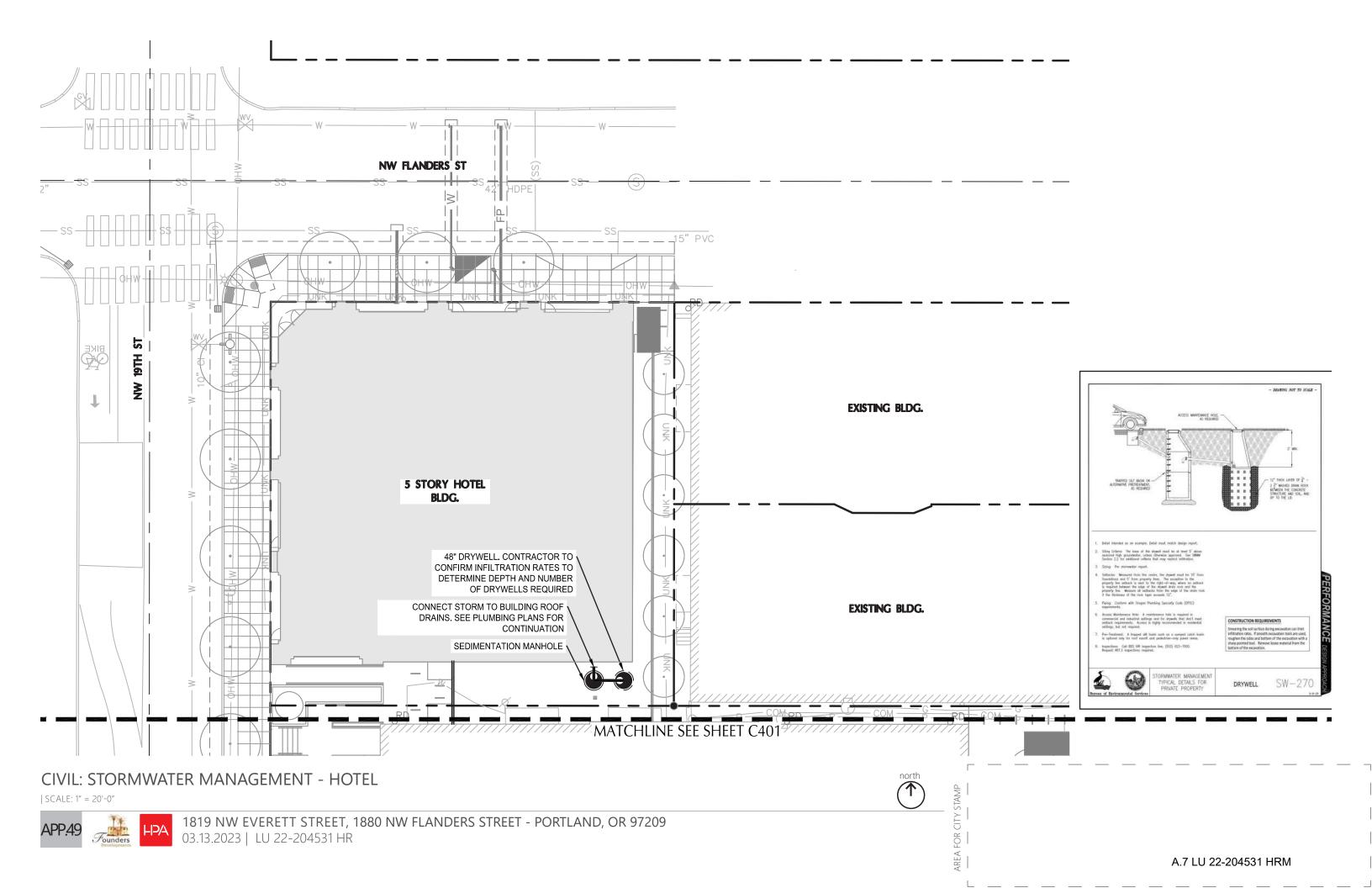


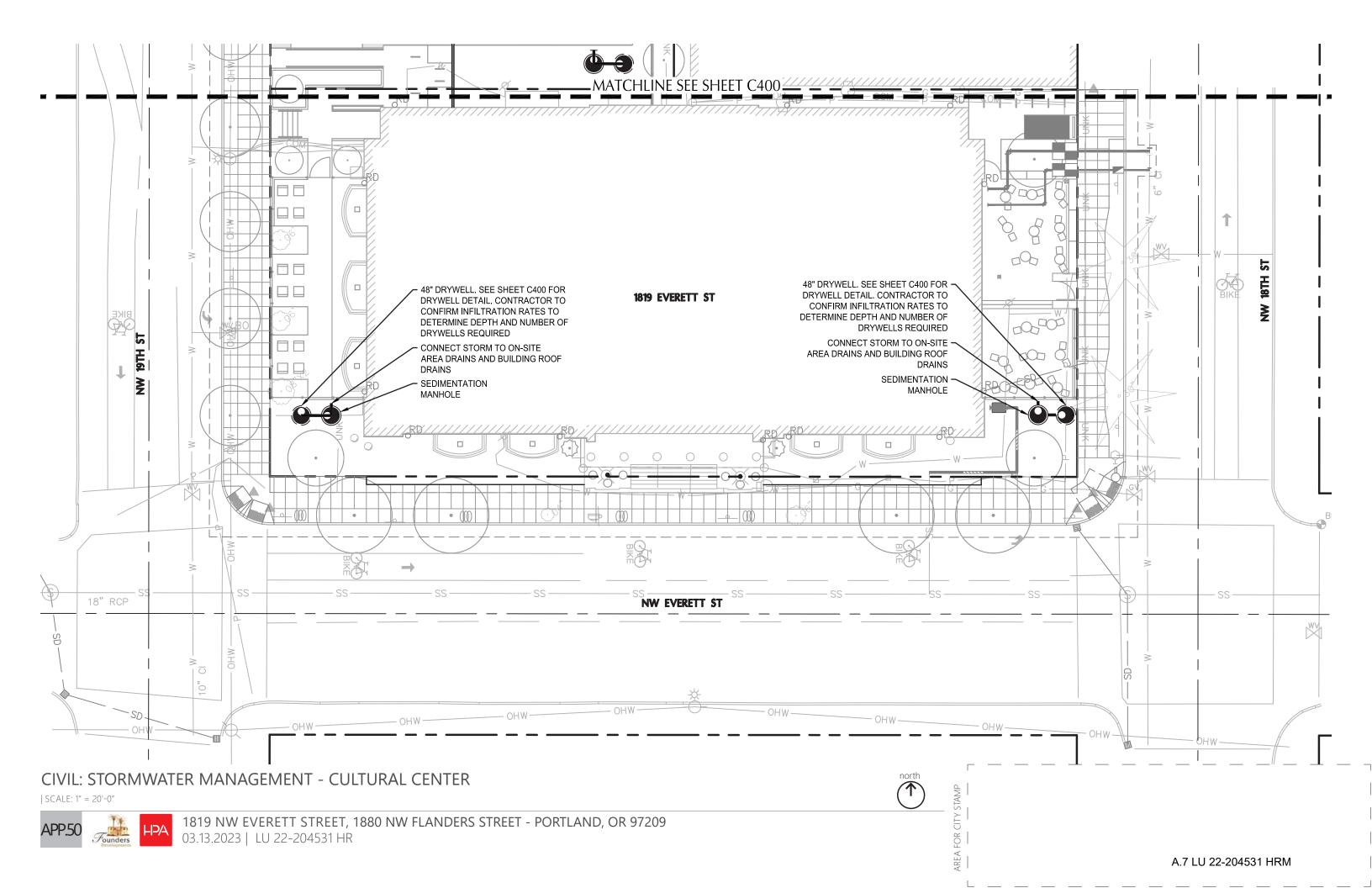












# TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

1819 NW EVERETT STREET
1880 NW FLANDERS STREET

Appendix - Supporting Information

March 13th, 2023

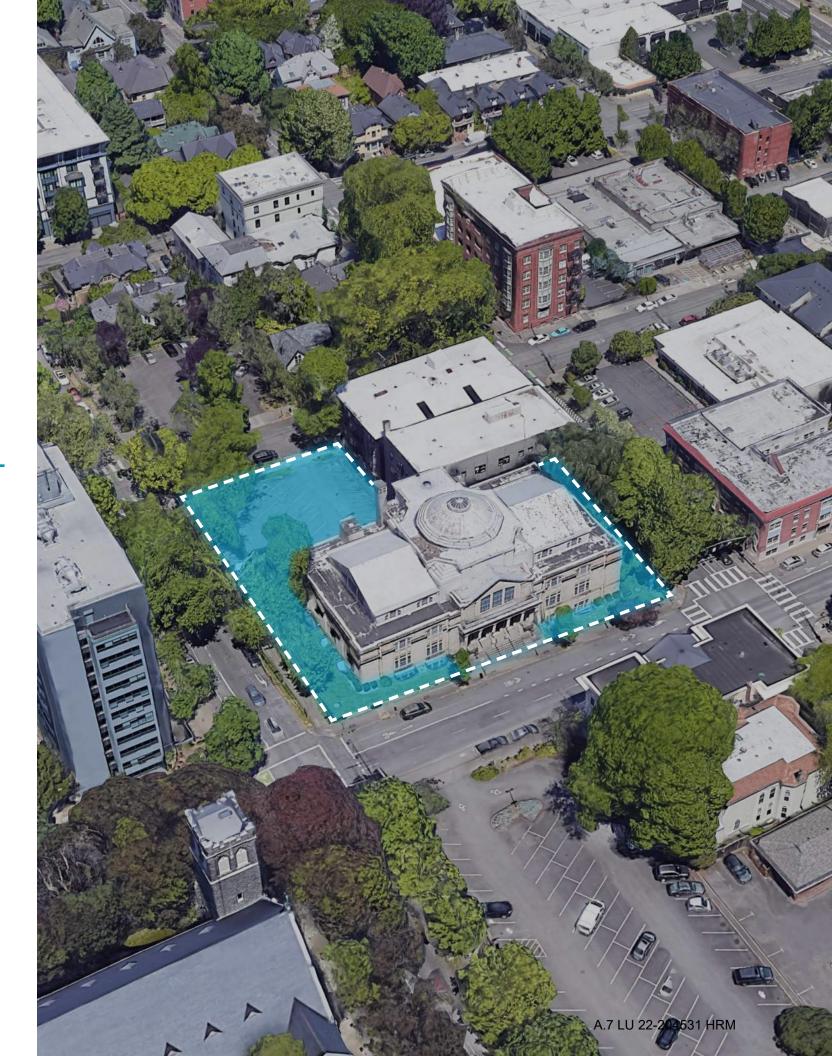
## TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

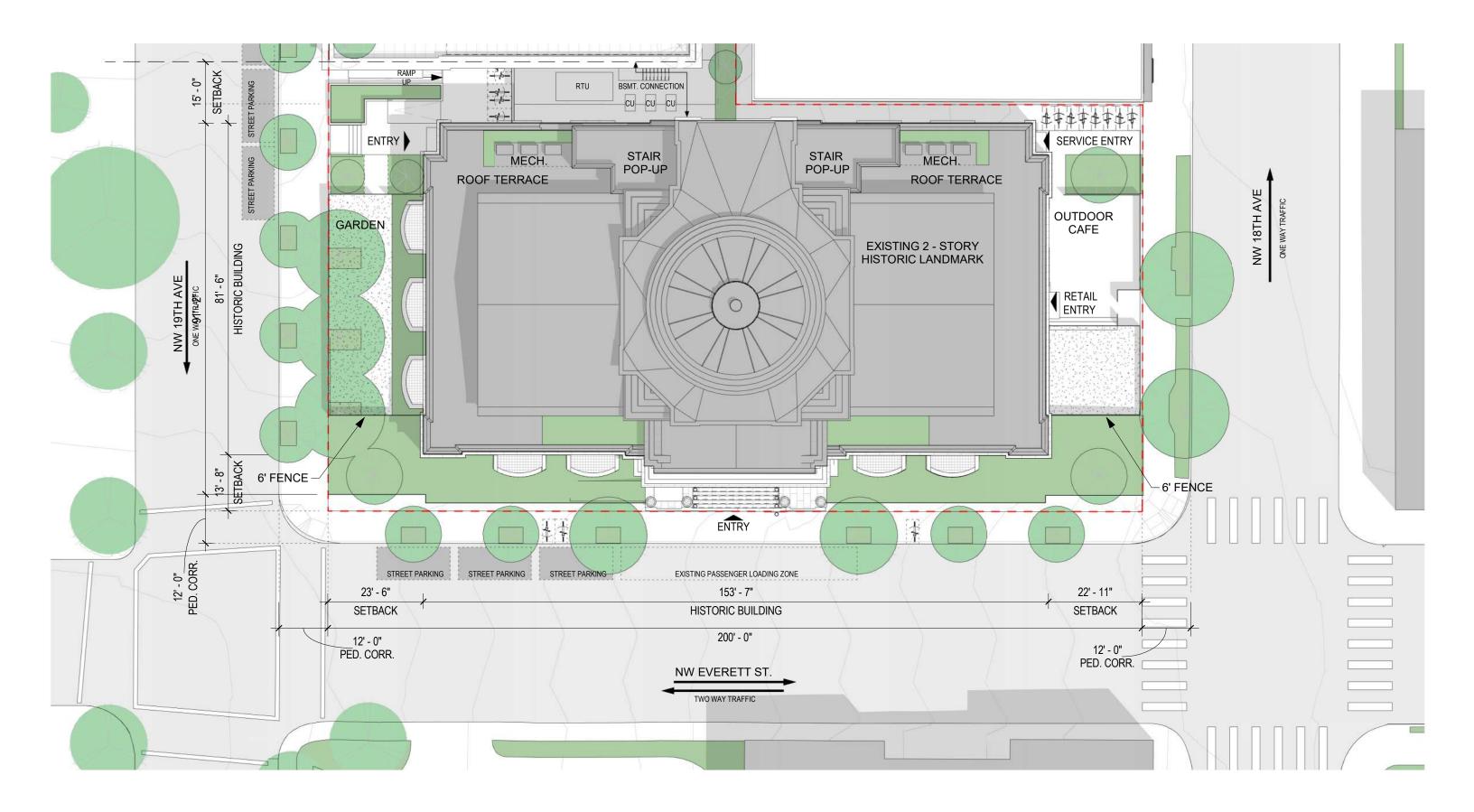
# 1819 NW EVERETT STREET

Historic Construction - Drawings

March 13th, 2023







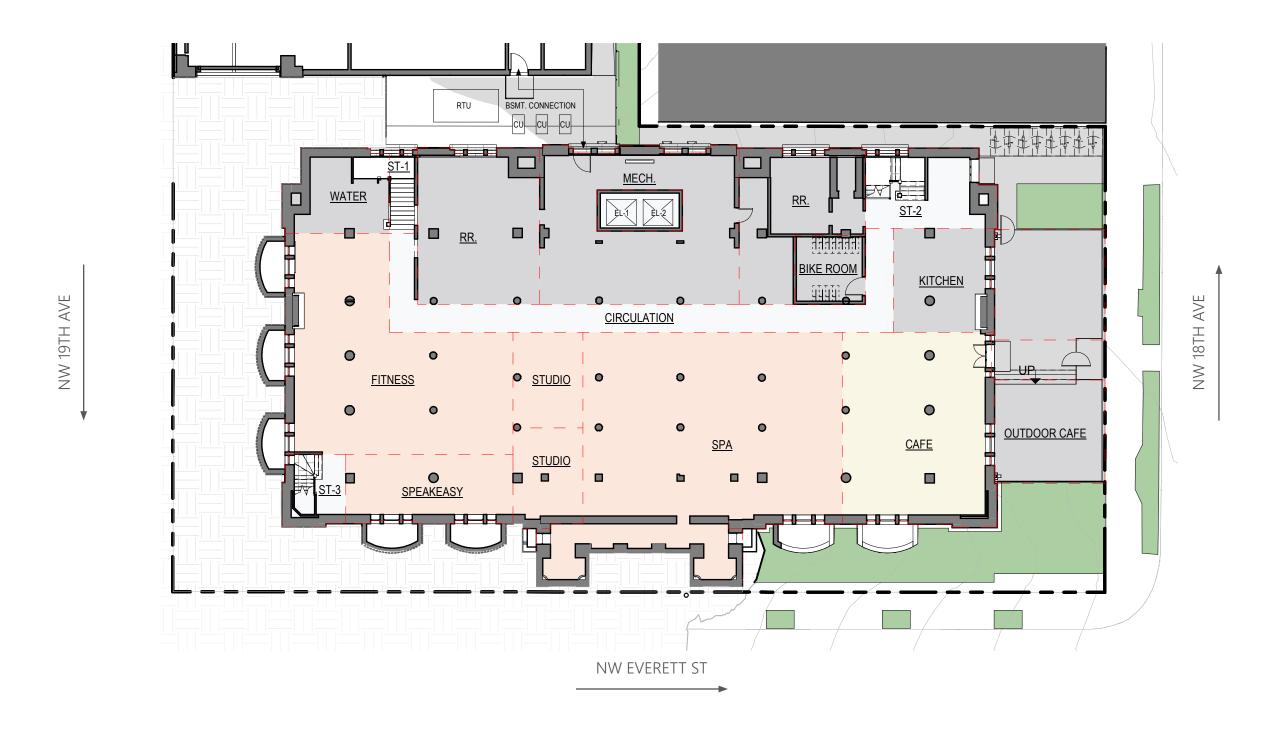
## ENLARGED SITE PLAN (1819 NW EVERETT ST.)

| SCALE: 1" = 64'-0"











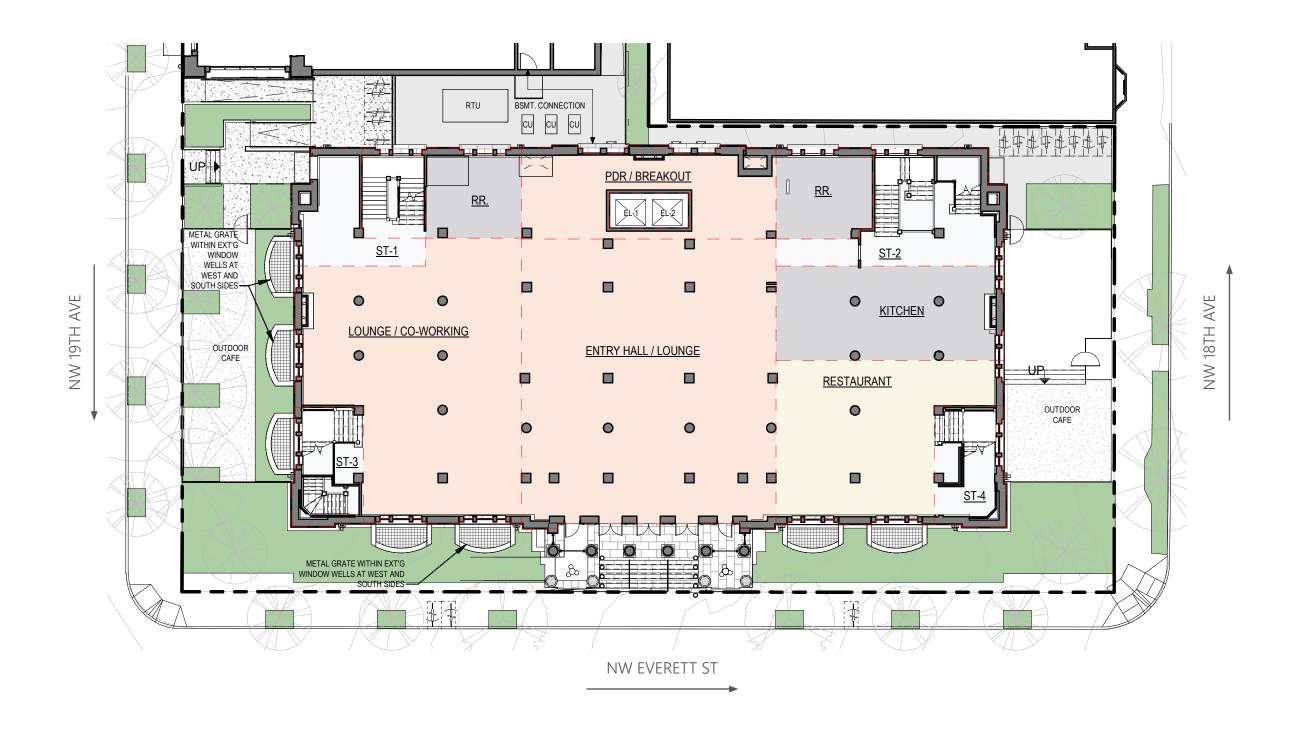
| SCALE: 1" = 20-0"











FIRST FLOOR PLAN (1819 NW EVERETT ST.)

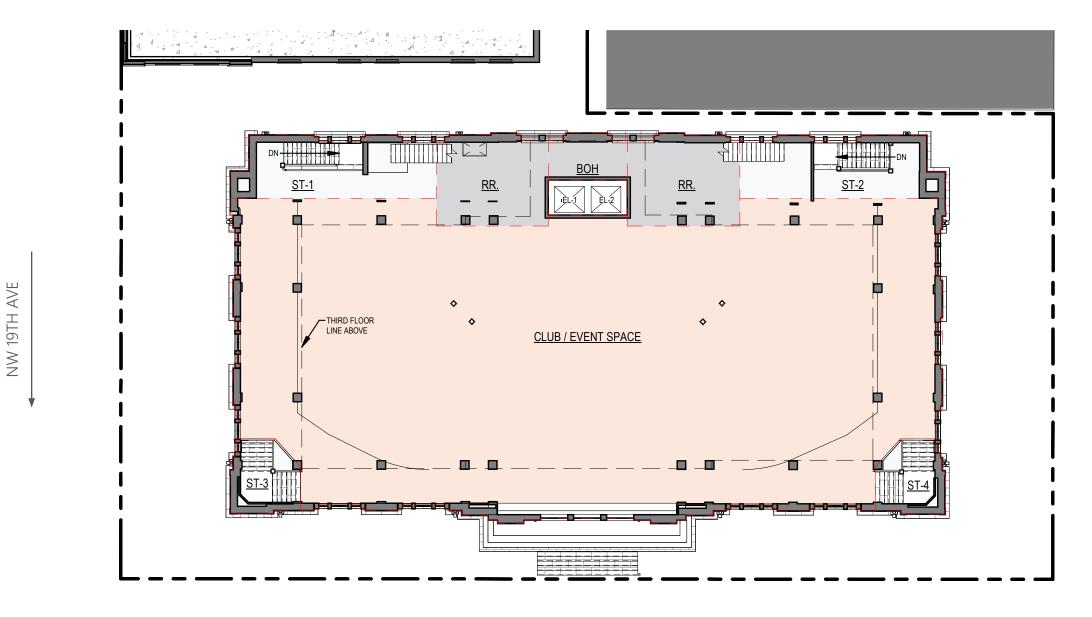
| SCALE: 1" = 20-0"











NW EVERETT ST

SECOND FLOOR PLAN (1819 NW EVERETT ST.)

**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

| SCALE: 1" = 20-0"









NW 18TH AVE

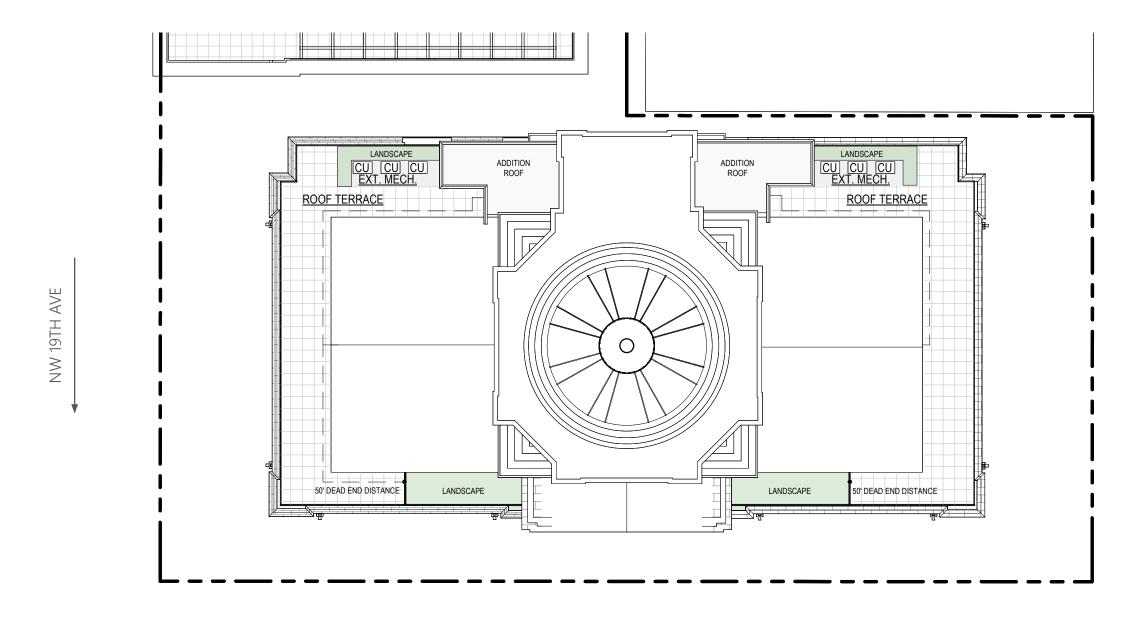
NW 18TH AVE

NW EVERETT ST

## THIRD FLOOR PLAN (1819 NW EVERETT ST.)

NW 19TH AVE

| SCALE: 1" = 20-0"



NW 18TH AVE

NW EVERETT ST

ROOF PLAN (1819 NW EVERETT ST.)

| SCALE: 1" = 20-0"





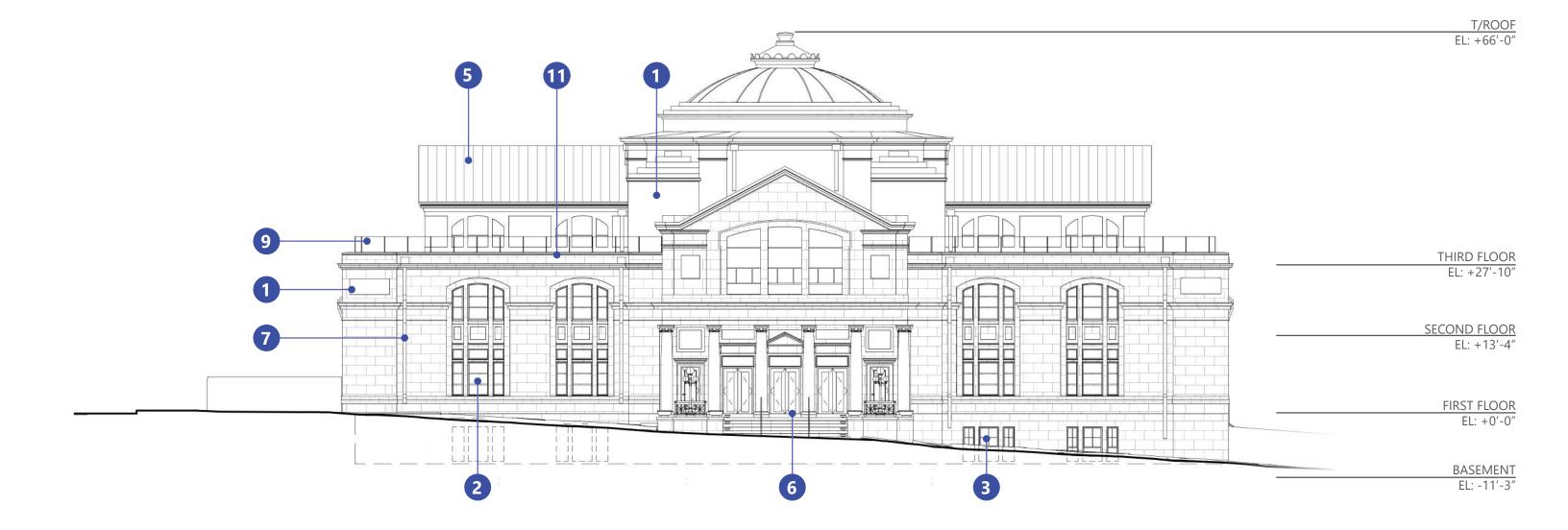




#### **MATERIAL KEY**

- EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
- 2 EXISTING STEEL WINDOW WITH LEADED GLASS (SEE WINDOW SCOPE PAGE FOR DTLS)
  - 3 EXISTING WOOD WINDOW FOR REPAIR AND PAINT
- 4 EXISTING METAL CLADDING TO REMAIN; REPAIR AND PAINT TO MATCH EXISTING
- 5 EXISTING COATED TIN ROOFING TO BE REPAIRED AND/OR REPLACED IN KIND
- E 6 EXISTING WOOD DOOR FOR REPAIR AND STAIN
- NEW METAL COPING CAP FOR PAINT TO MATCH WALL
- NEW METAL DOOR FOR PAINT TO MATCH WALL

- 7 EXISTING SCUPPER AND DOWNSPOUT TO REMAIN FOR PAINT
- R NEW FABRIC AWNING IN EXISTING LOCATION
- 9 NEW FRAMELESS GLASS GAURDRAIL, CLEAR LOW-IRON GLASS
- L, 10 NEW STUCCO RAINSCREEN



SOUTH ELEVATION

BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"



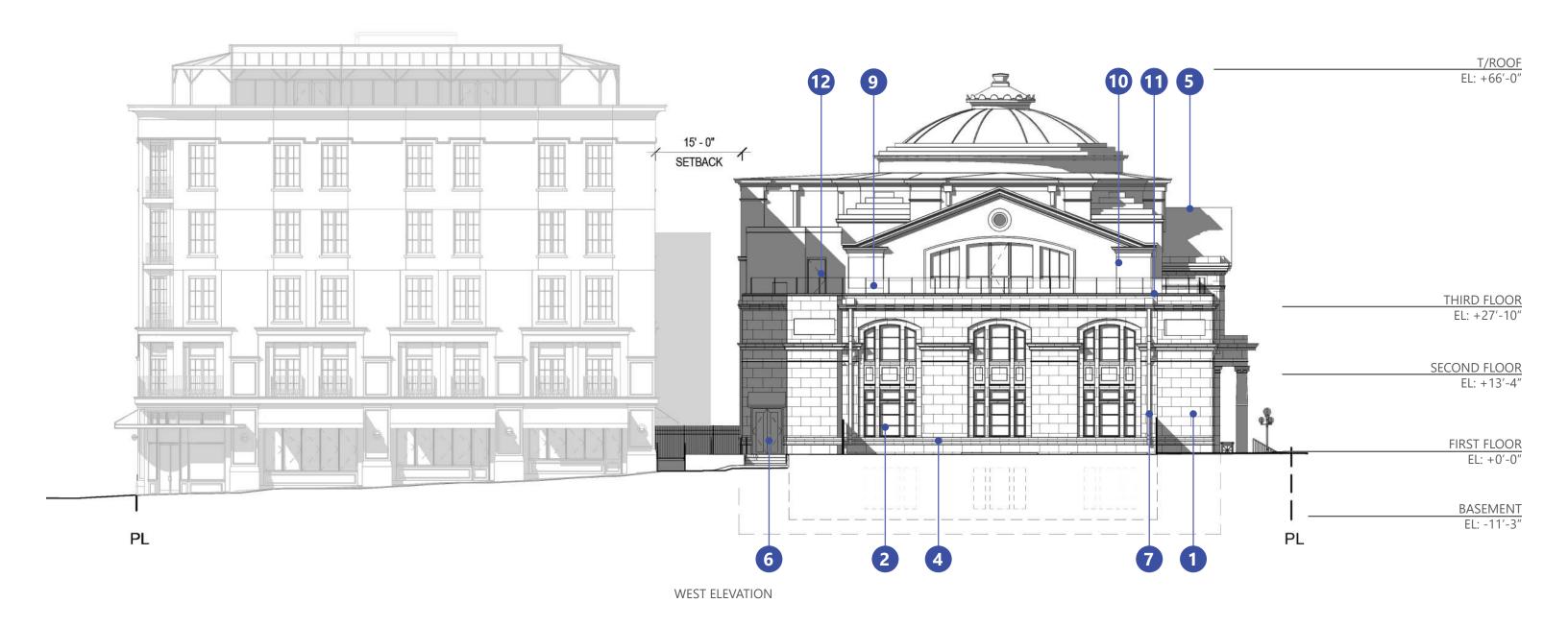


AREA FOR CITY STAMP

#### MATERIAL KEY

- EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
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- E 6 EXISTING WOOD DOOR FOR REPAIR AND STAIN
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BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"







AREA FOR CITY STAMP

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NEW METAL COPING CAP FOR PAINT TO MATCH WALL

- BE 6 EXISTING WOOD DOOR FOR REPAIR AND STAIN
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**EAST ELEVATION** 

THIRD FLOOR (HISTORIC)

EL: +27'-10'

SECOND FLOOR (HISTORIC)

EL: +13'-4"

FIRST FLOOR (HISTORIC)

EL: +0'-0'

BASEMENT (HISTORIC)

EL: +10'-0'

BASEMENT (HISTORIC)

EL: -11'-3'

3 8 6 4 8

#### BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)

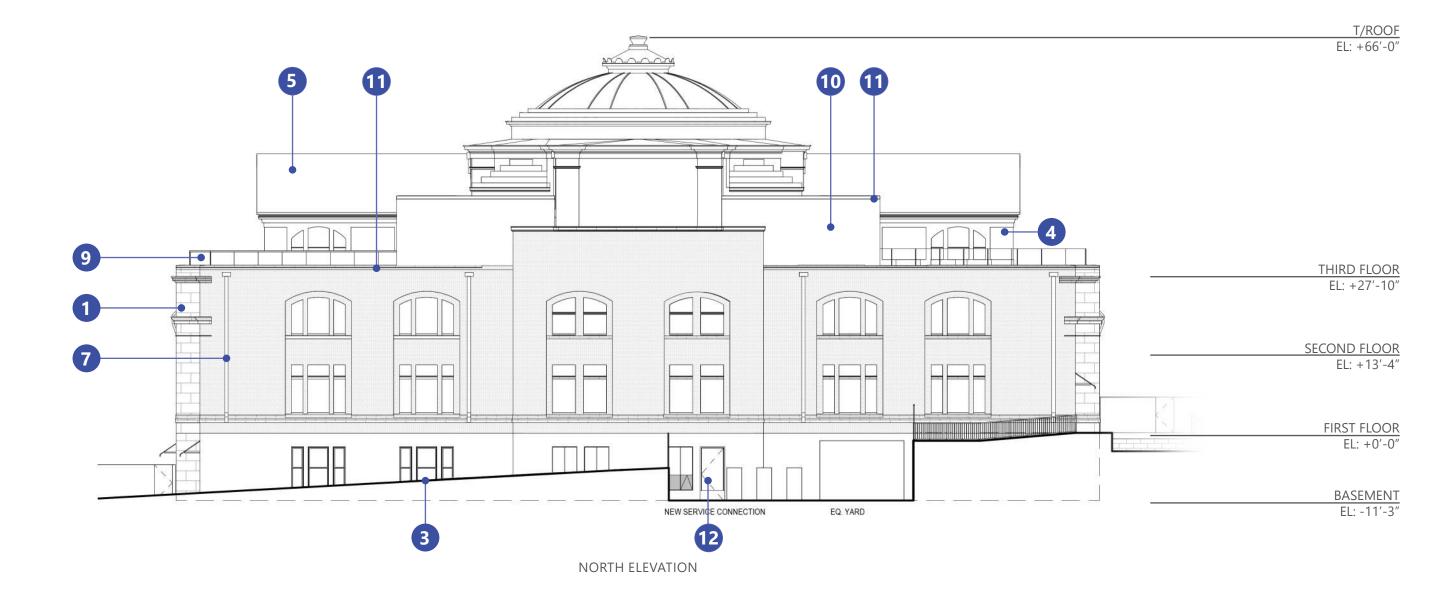




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  - NEW METAL COPING CAP FOR PAINT TO MATCH WALL

AREA FOR CITY STAMP



BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)

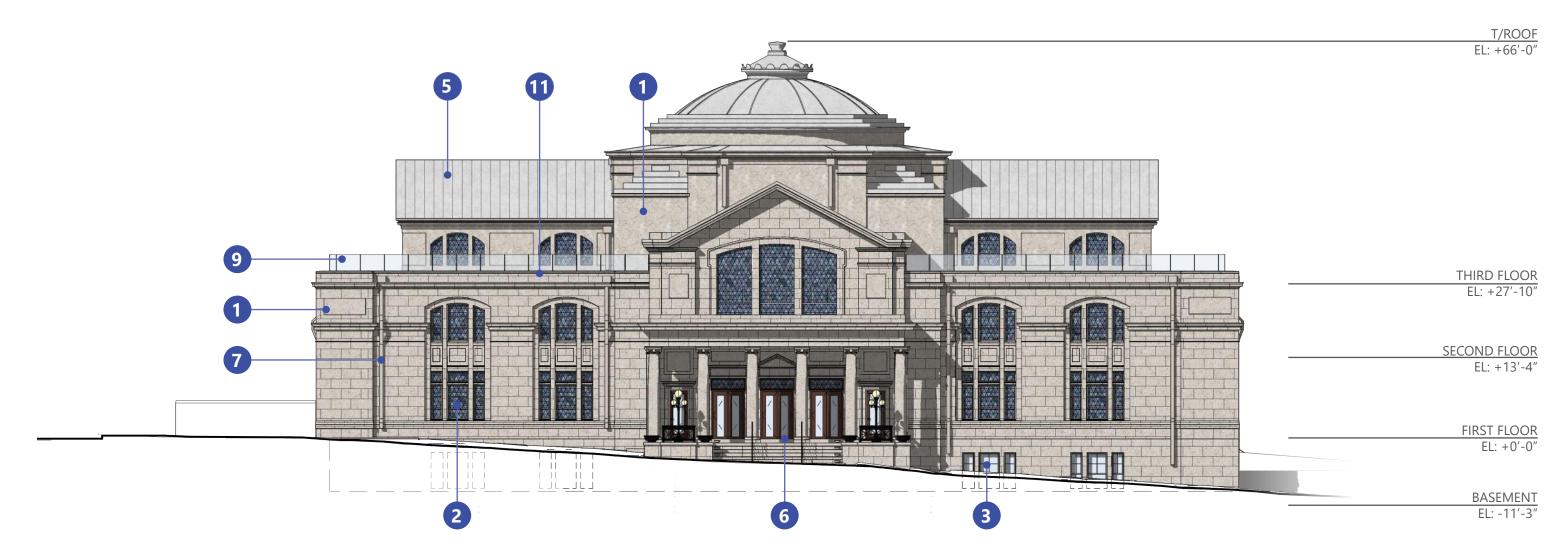






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- 10 NEW STUCCO RAINSCREEN NEW METAL COPING CAP FOR PAINT TO MATCH WALL



SOUTH ELEVATION

COLOR ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

AREA FOR CITY STAMP

- EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
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- AURDRAIL, 10 NEW STUCCO RAINSCREEN

T/ROOF 10 11 5 EL: +66'-0" 15' - 0" SETBACK THIRD FLOOR EL: +27'-10" SECOND FLOOR EL: +13'-4" FIRST FLOOR EL: +0'-0" **BASEMENT** EL: -11'-3" PLPL 2 6 **WEST ELEVATION** 

COLOR ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





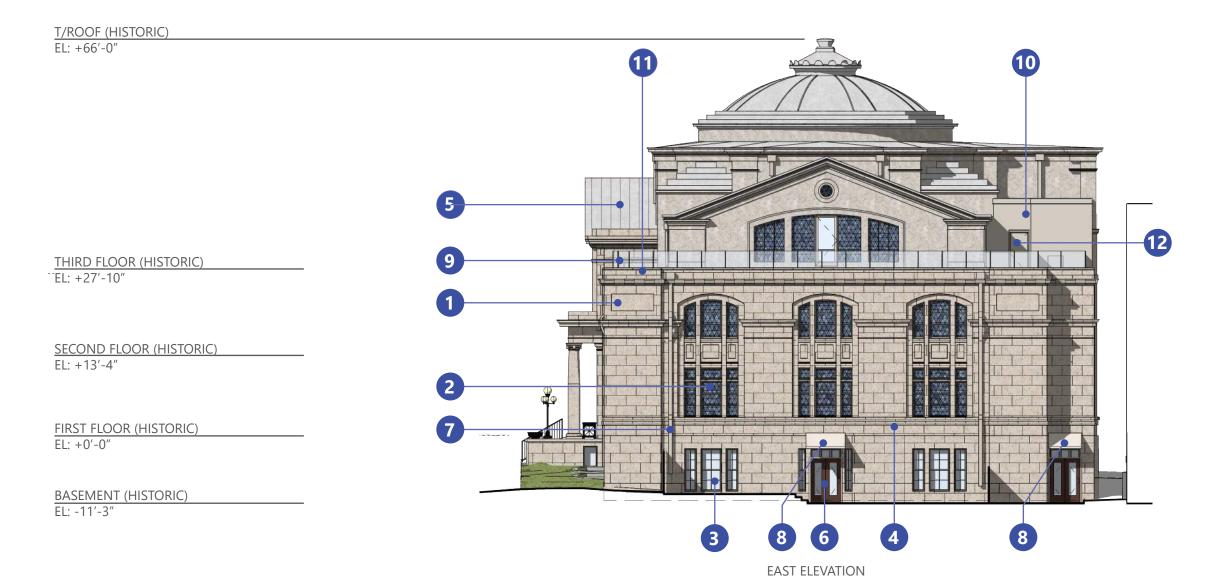
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

- 1 EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
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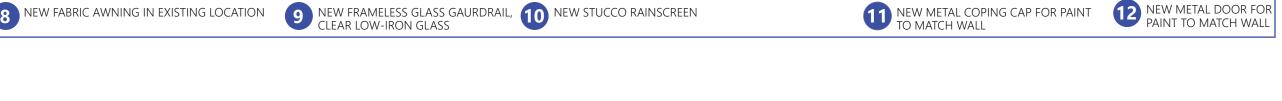
COLOR ELEVATIONS (1819 NW EVERETT ST.)

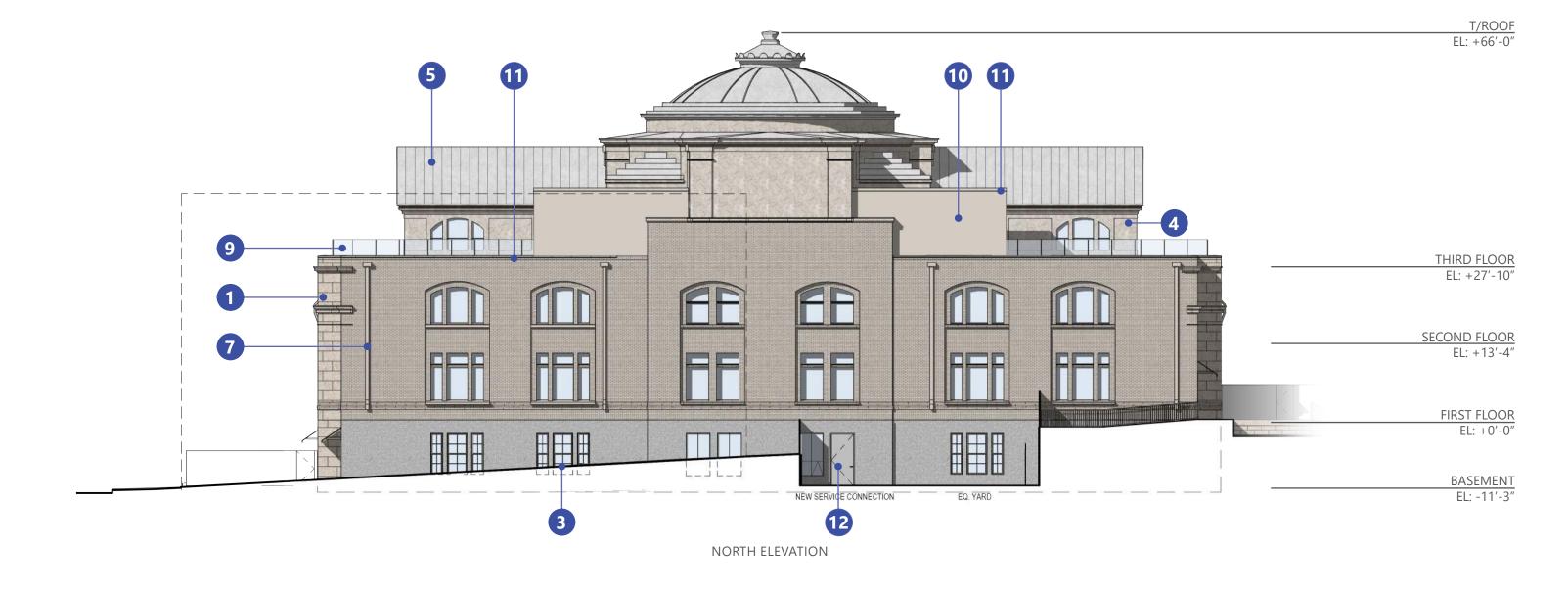




- EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
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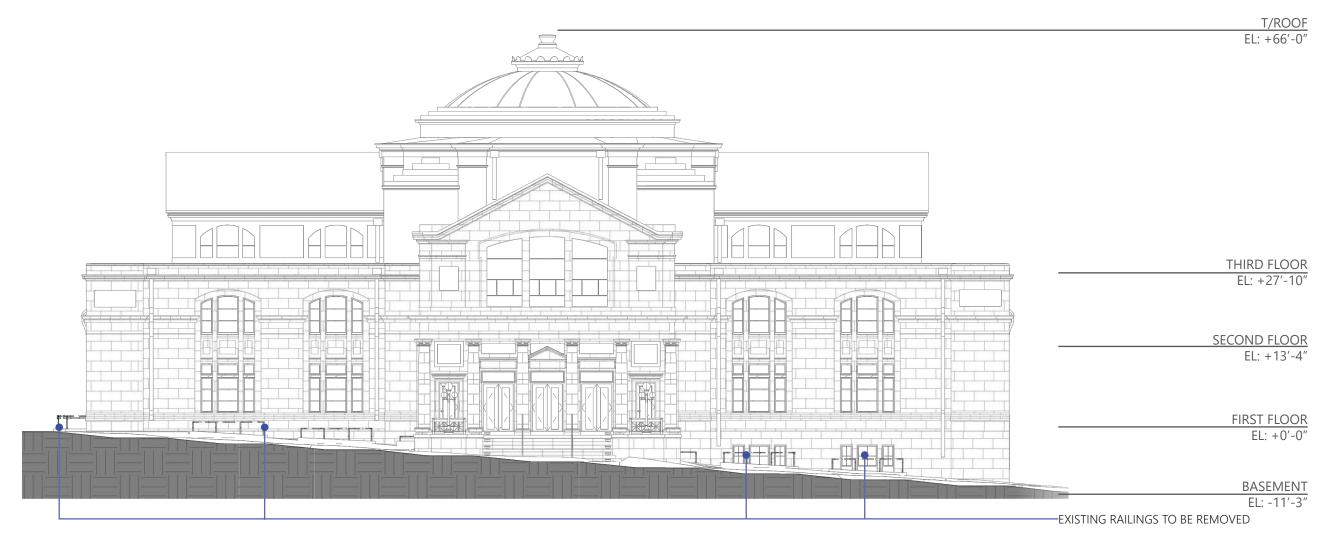
COLOR ELEVATIONS (1819 NW EVERETT ST.)











SOUTH ELEVATION

## DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"



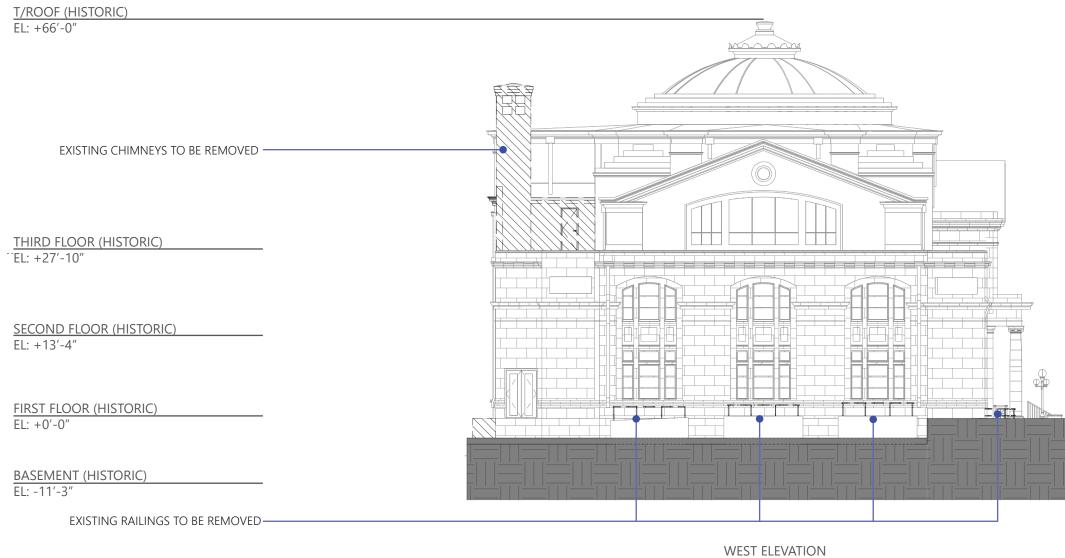




**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

AREA FOR CITY STAMP

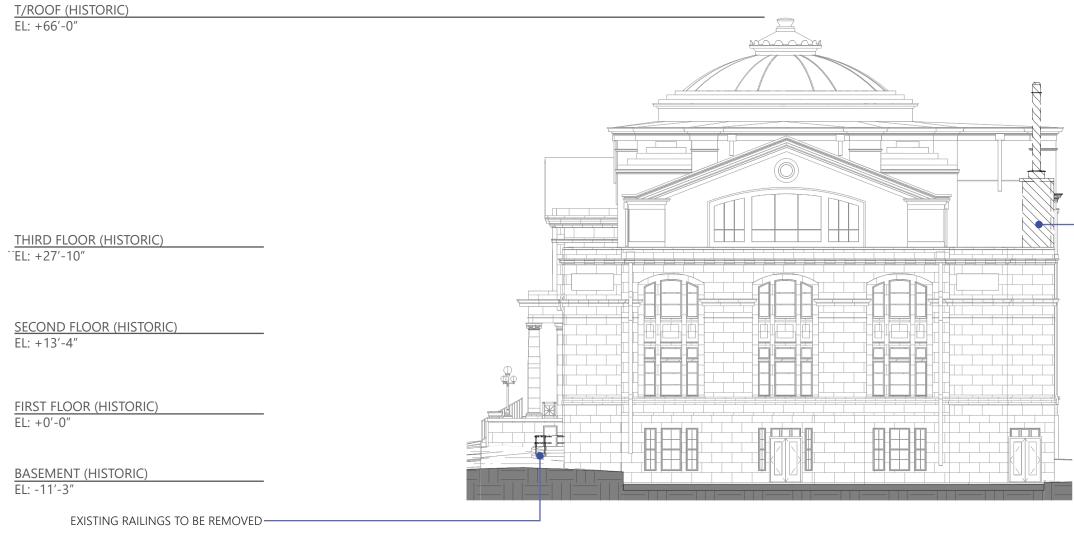
A.7 LU 22-204531 HRM



## DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)







EXISTING CHIMNEYS TO BE REMOVED

EAST ELEVATION

## DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)

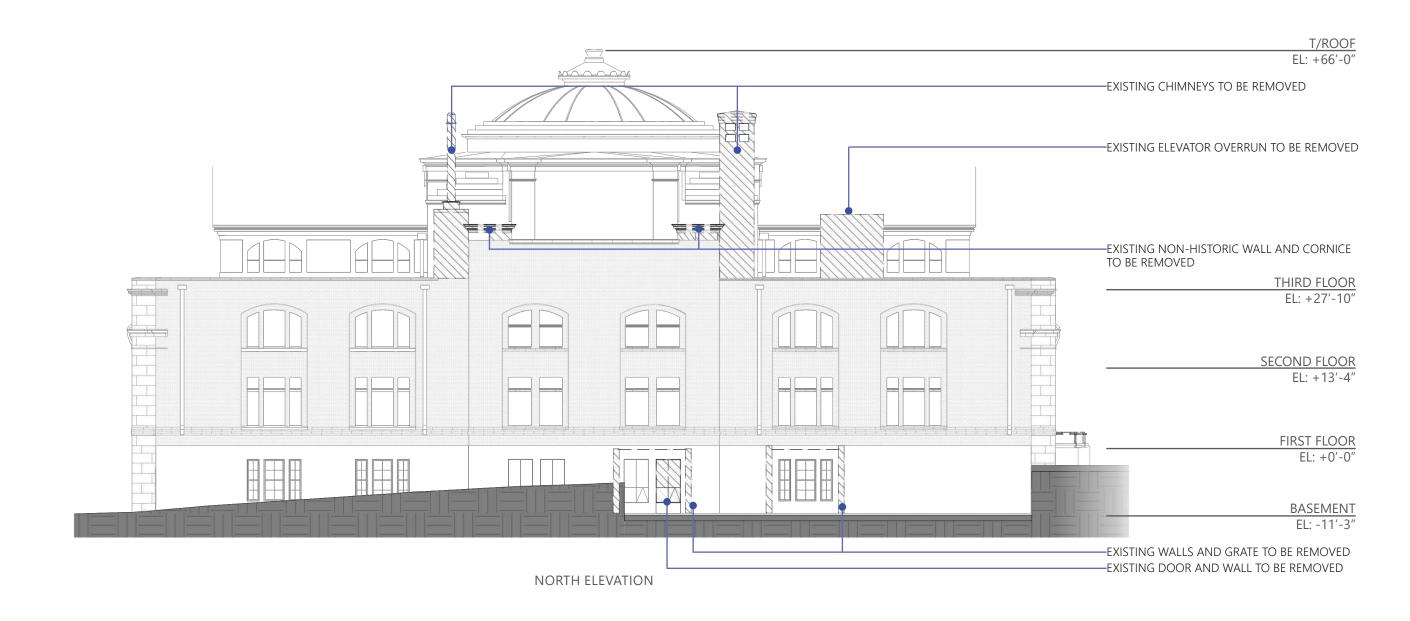
| SCALE: 1/16" = 1'-0"







1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR



## DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR



A.7 LU 22-204531 HRM



**EXISTING NORTHEAST** CHIMNEY - PROPOSED TO BE REMOVED AND REPLACED WITH STAIR ADDITION



**EXISTING NORTHWEST** REMOVED AND REPLACED WITH STAIR ADDITION

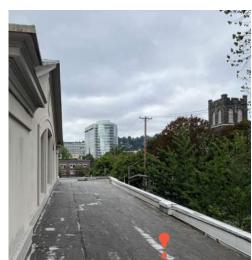


EXISTING, NON-HISTORIC, EXISTING NORTHWEST

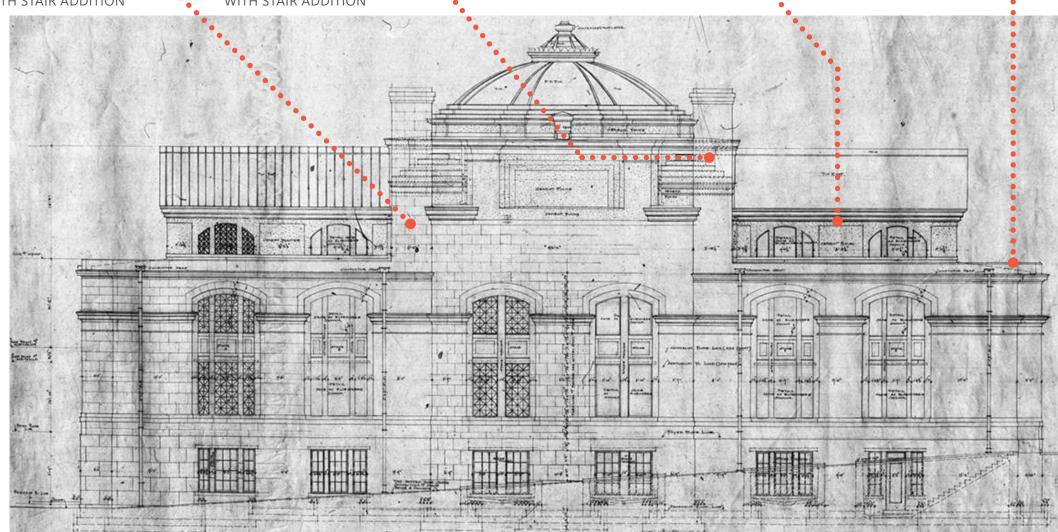
• EXISTING, NON-HISTORIC,

CHIMNEY - PROPOSED TO BE

• ELEVATOR OVERRUN TO BE DEMOLISHED



12" HIGH EXISTING PARAPET CONDITION

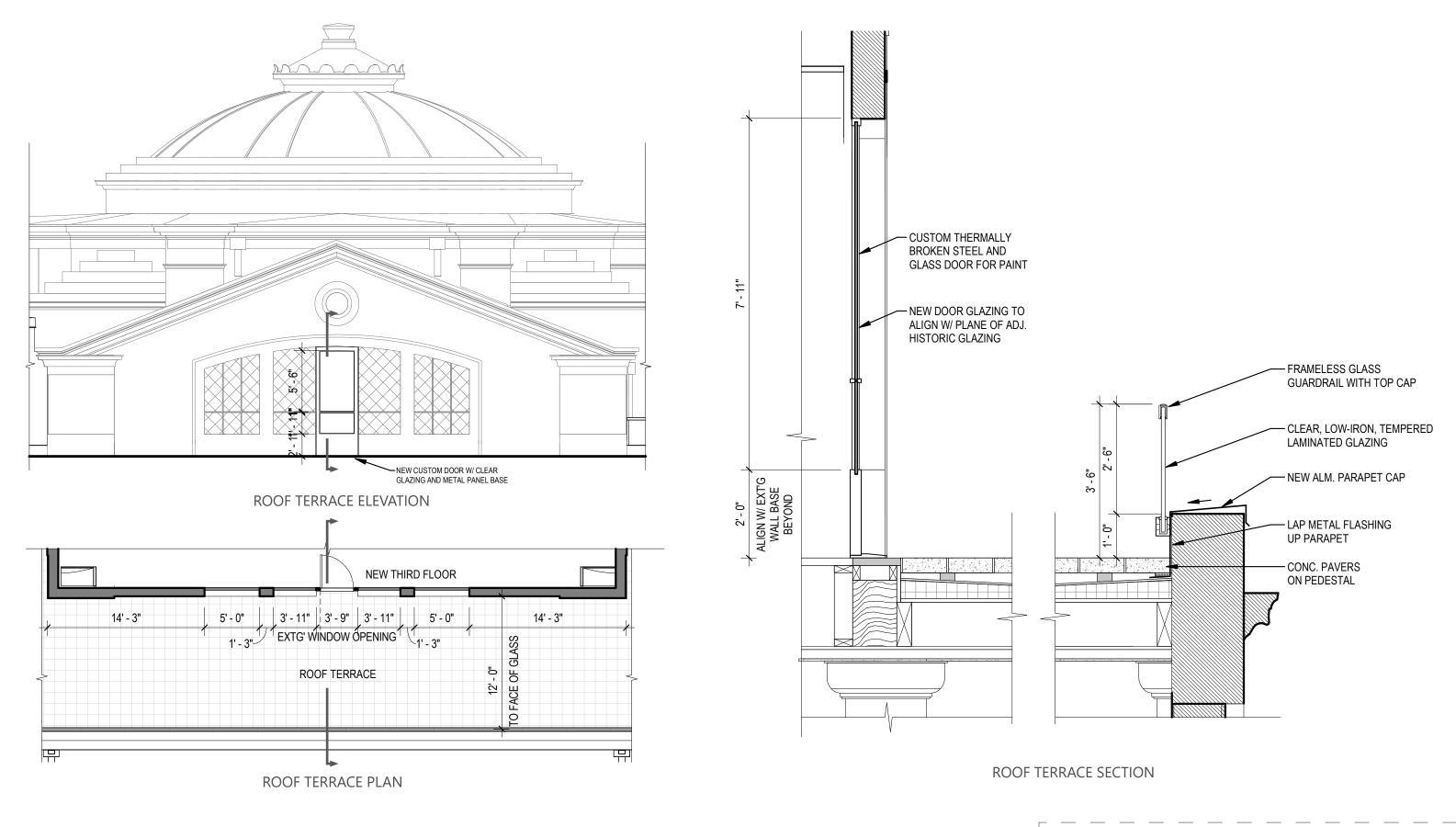


ORIGINAL NORTH ELEVATION DRAWING

## EXISTING CONDITIONS (1819 NW EVERETT ST.)







## DETAILS (1819 NW EVERETT ST.)

| SCALE: 1/8" = 1'-0" & 1/2" = 1'-0"

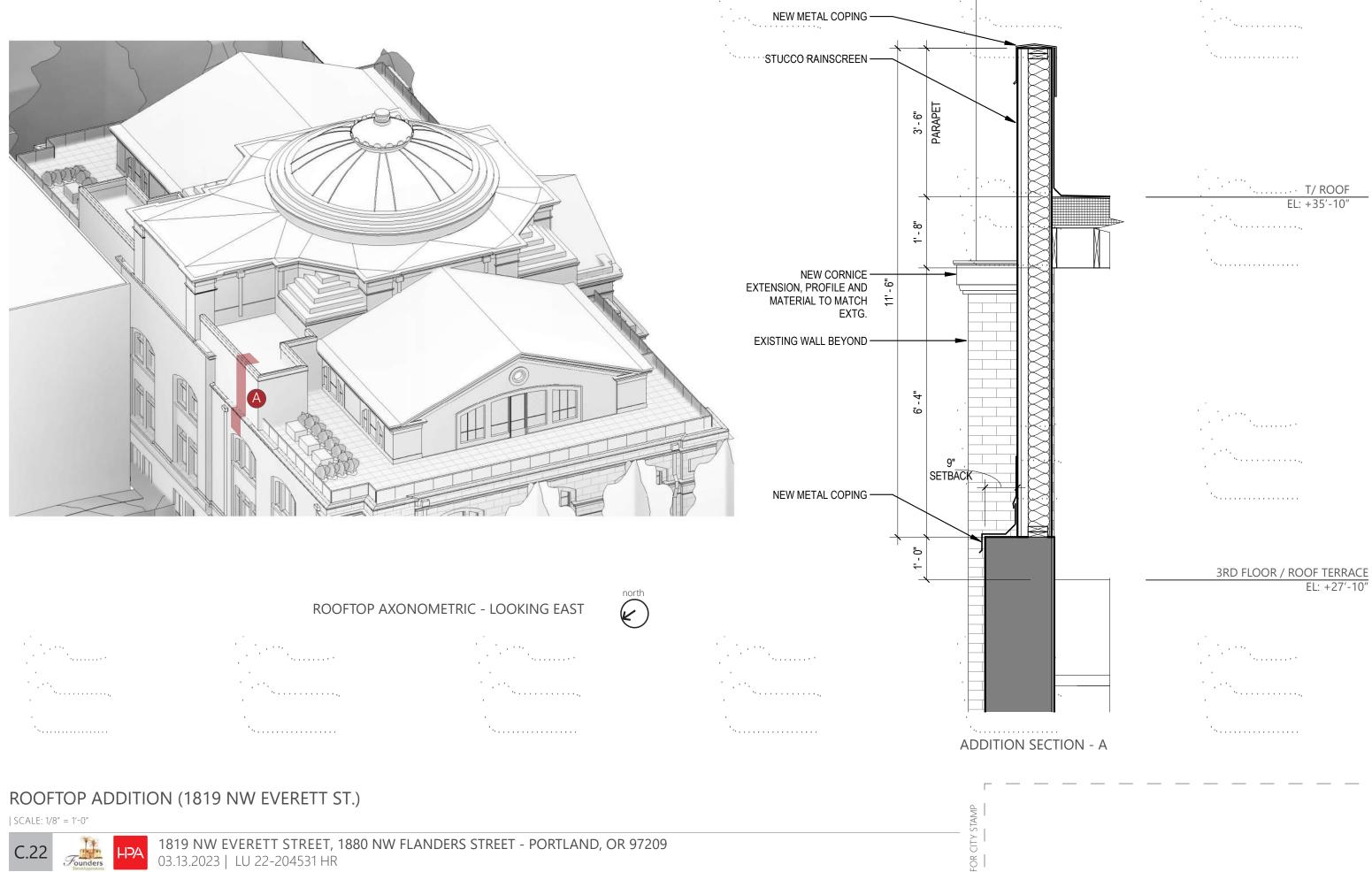


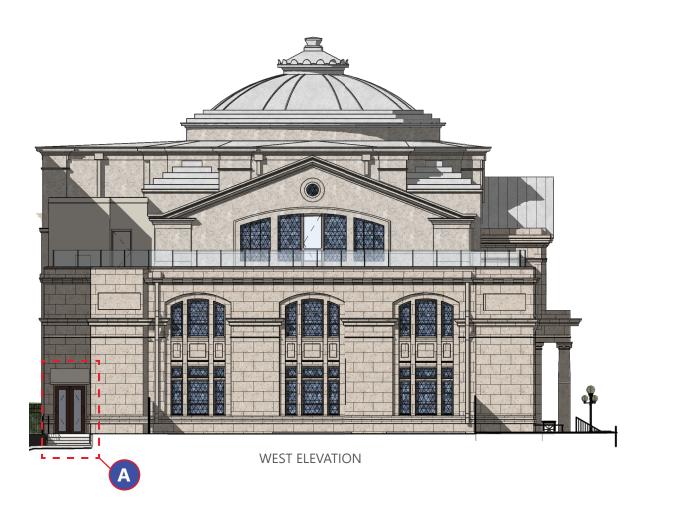


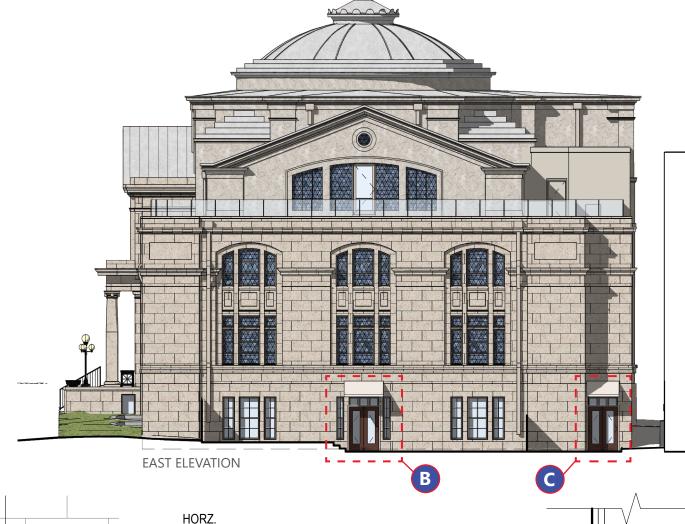
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

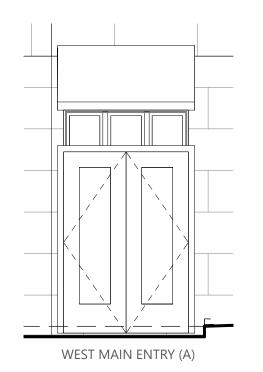


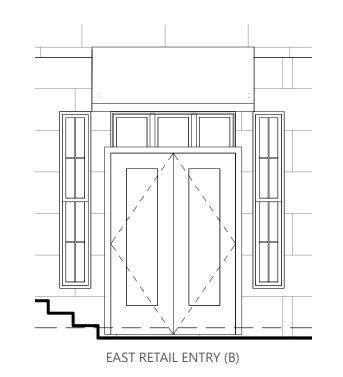
A.7 LU 22-204531 HRM

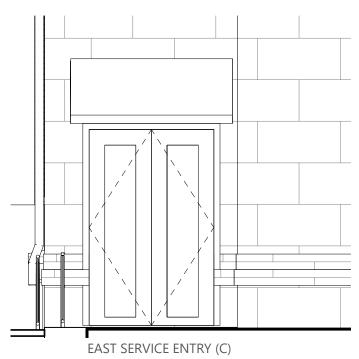


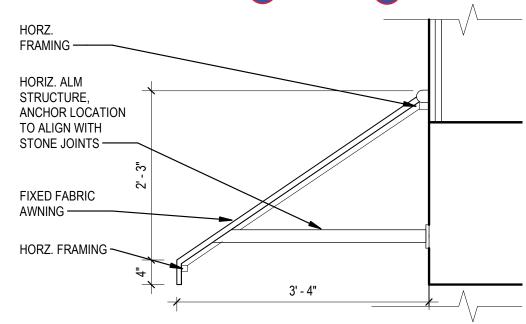










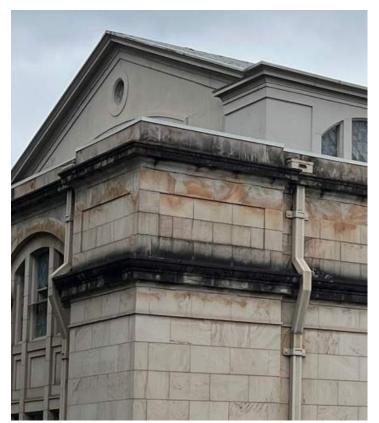


## WINDOW AWNING DETAILS (1819 NW EVERETT ST.)









EXISTING FACADE CONDITION



NORTH WINDOW BAY/FACADE MATERIAL



EXISTING METAL SHEET CLADDING OVER HISTORIC CEMENTITIOUS FINISH



WINDOW CONDITION EXAMPLE



ENLARGED WINDOW DETAIL



EXISTING STONE TO REMAIN



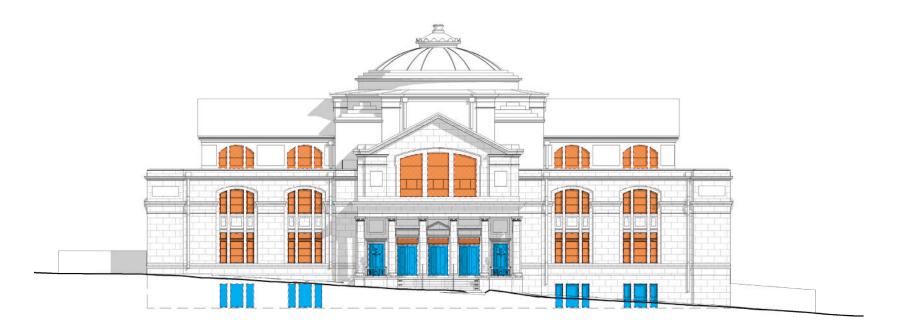


STAIR TOWER ADDITIONS - STUCCO AND METAL COPING

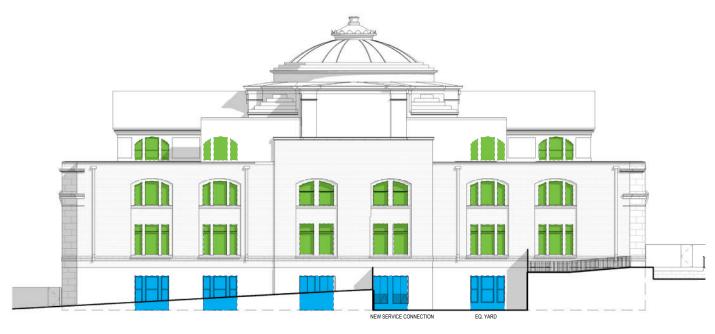
## EXTERIOR MATERIALS (1819 NW EVERETT ST.)







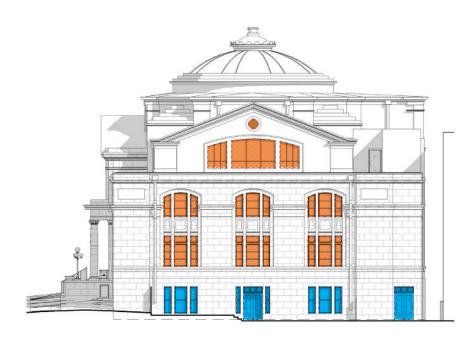
#### SOUTH ELEVATION



NORTH ELEVATION







#### EAST ELEVATION



WEST ELEVATION

EXISTING GLASS TO BE REPLACED WITH SEMI-TRANSPARENT PRIVACY GLASS WITHOUT GEOMETRIC PATTERN. FRAME AND MULLIONS TO BE REPLACED WITH NEW IN HISTORIC CONFIGURATION

### WINDOW REPAIR AND REPLACEMENT SCOPE (1819 NW EVERETT ST.)







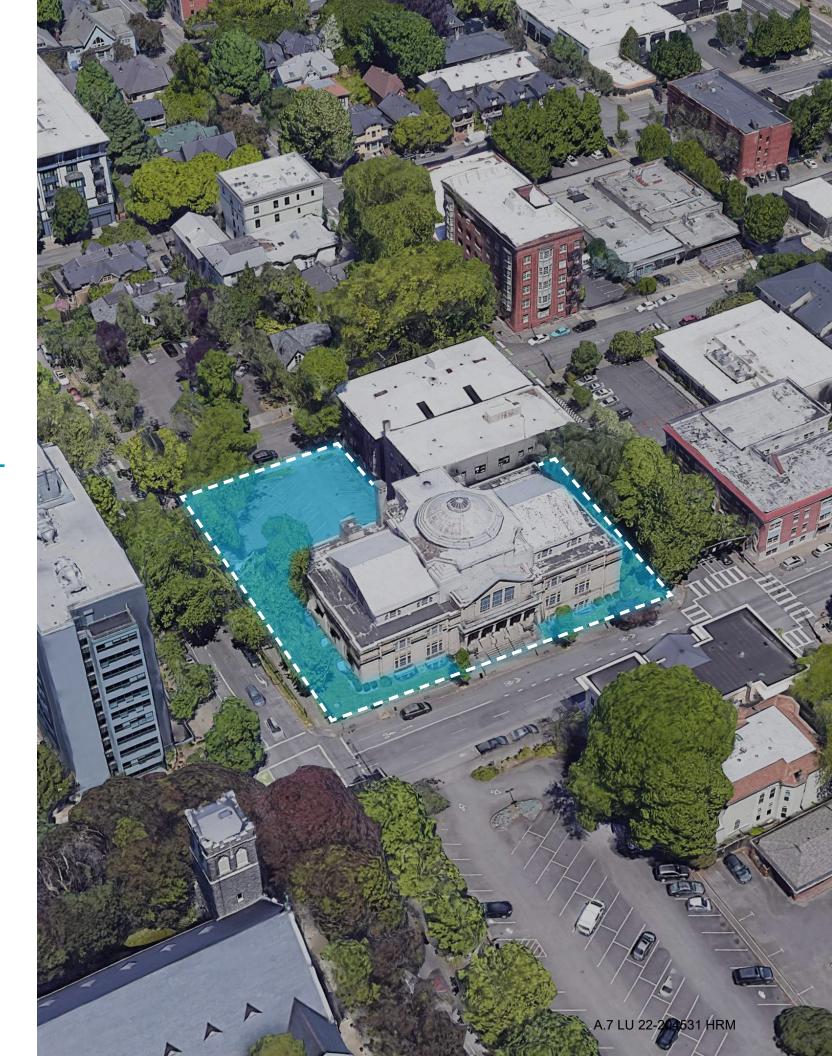
## TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

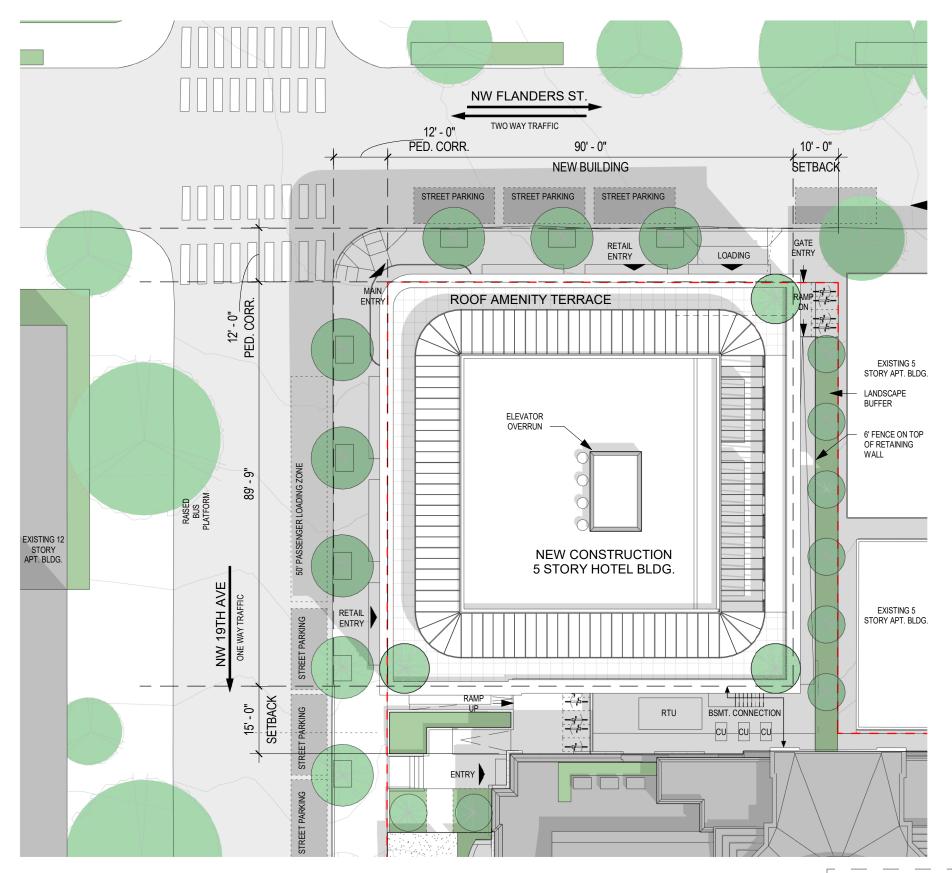
1880 NW FLANDERS STREET

New Construction - Drawings

March 13th, 2023









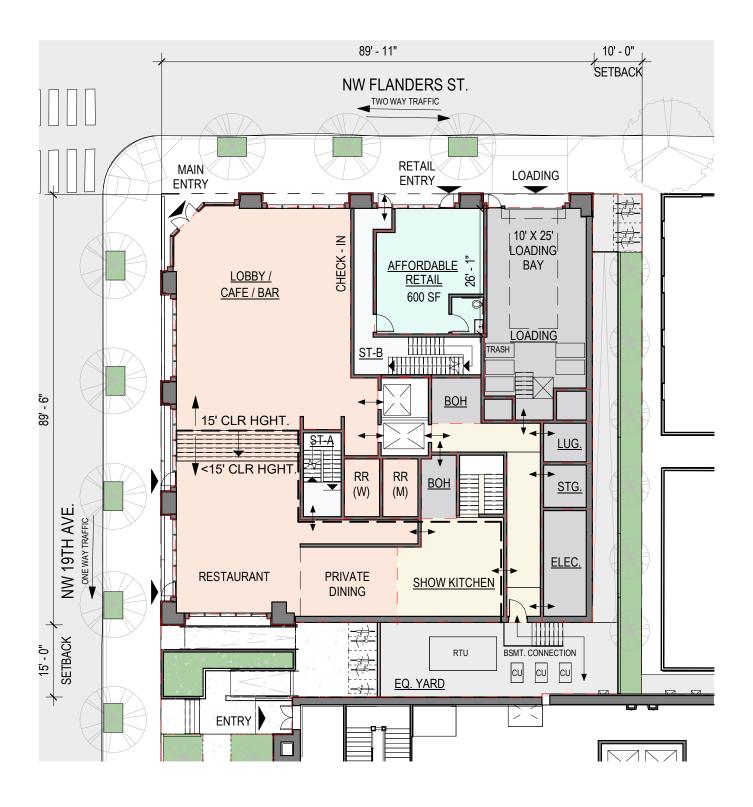
| SCALE: 1" = 64'-0"











FIRST FLOOR PLAN

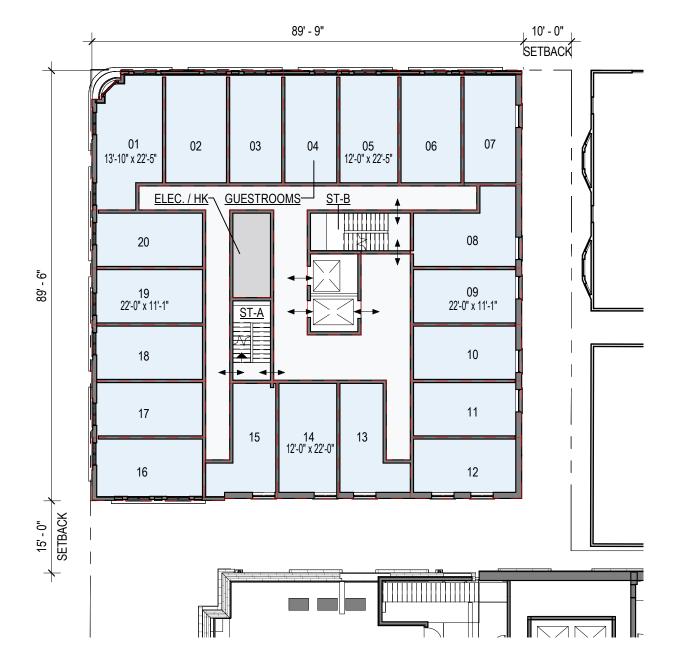
## FLOOR PLANS (1880 NW FLANDERS ST.)

| SCALE: 1" = 20'-0"



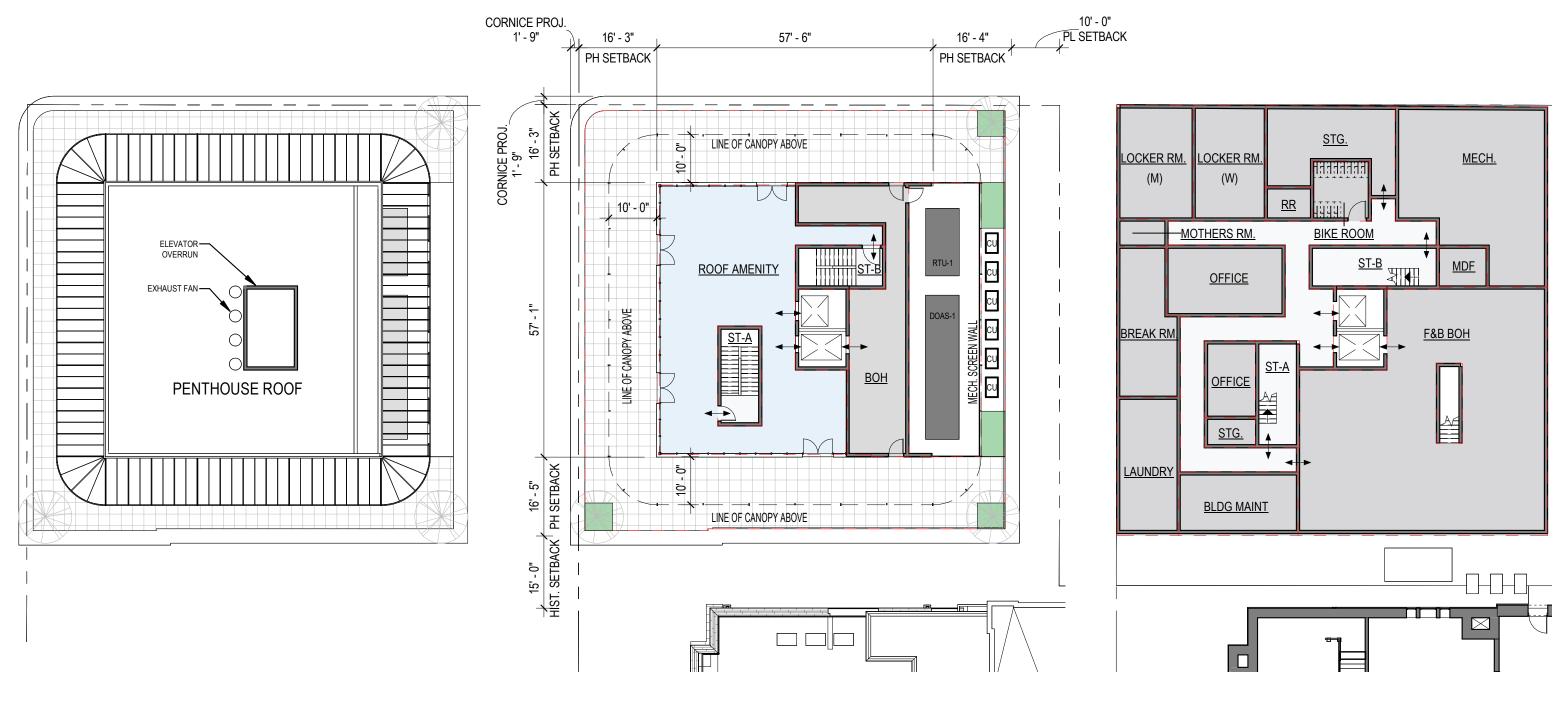






TYPICAL FLOOR PLAN





PENTHOUSE ROOF PLAN ROOF PLAN BASEMENT PLAN

## FLOOR PLANS (1880 NW FLANDERS ST.)

| SCALE: 1" = 20'-0"



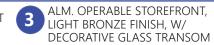








ALM. WINDOW, LIGHT BRONZE FINISH









ALM. CANOPY, LIGHT BRONZE FINISH, W/ CLEAR GLASS ROOF





10 ALM. COPING CAP, PAINT 11 ALM. DOOR, PAINT TO TO MATCH STUCCO MATCH WALL

ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS

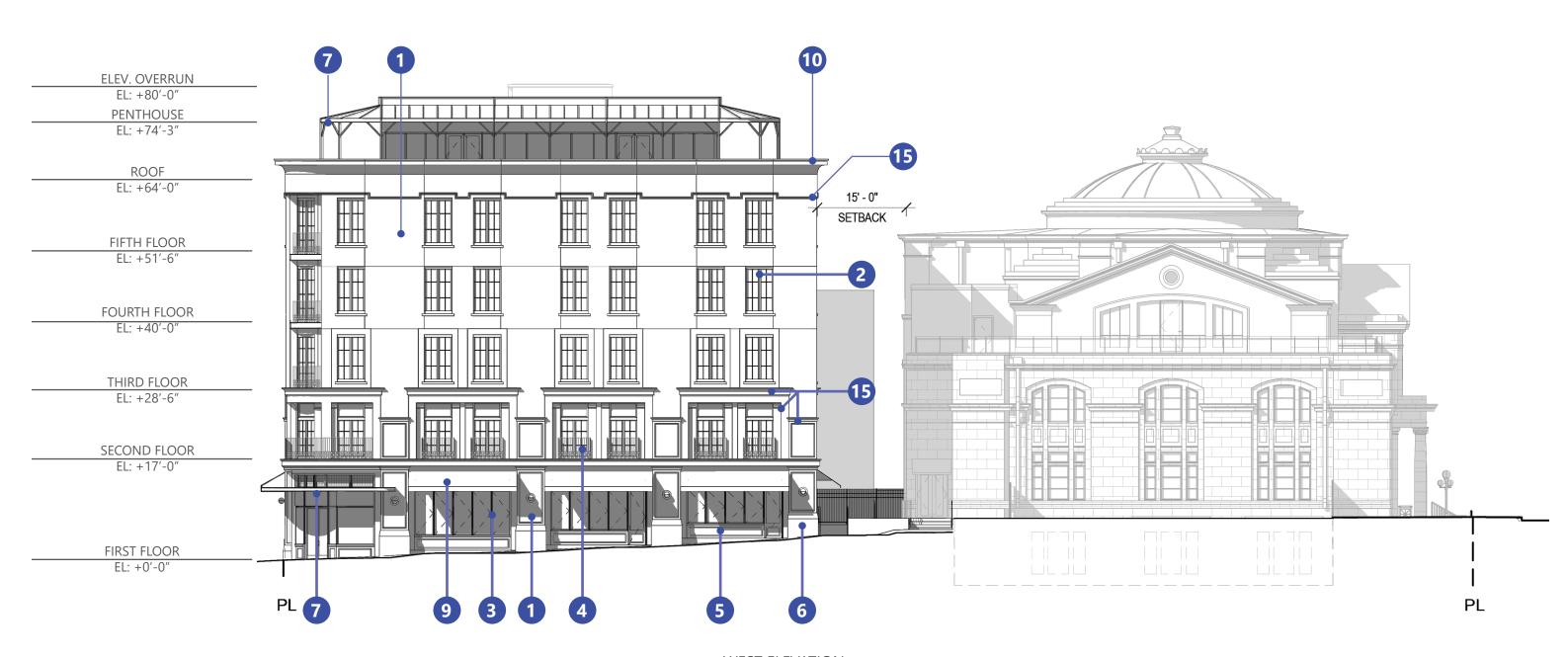
DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH

CONCRETE W/ ELASTOMERIC 14 PAINT , COLOR TO MATCH STUCCO

DECORATIVE STUCCO MOULDING TRIM

ALM. STOREFRONT, 16 PAINT TO MATCH STUCCO

AREA FOR CITY STAMP



WEST ELEVATION

## BLACK AND WHITE ELEVATIONS (1880 NW FLANDERS ST.)





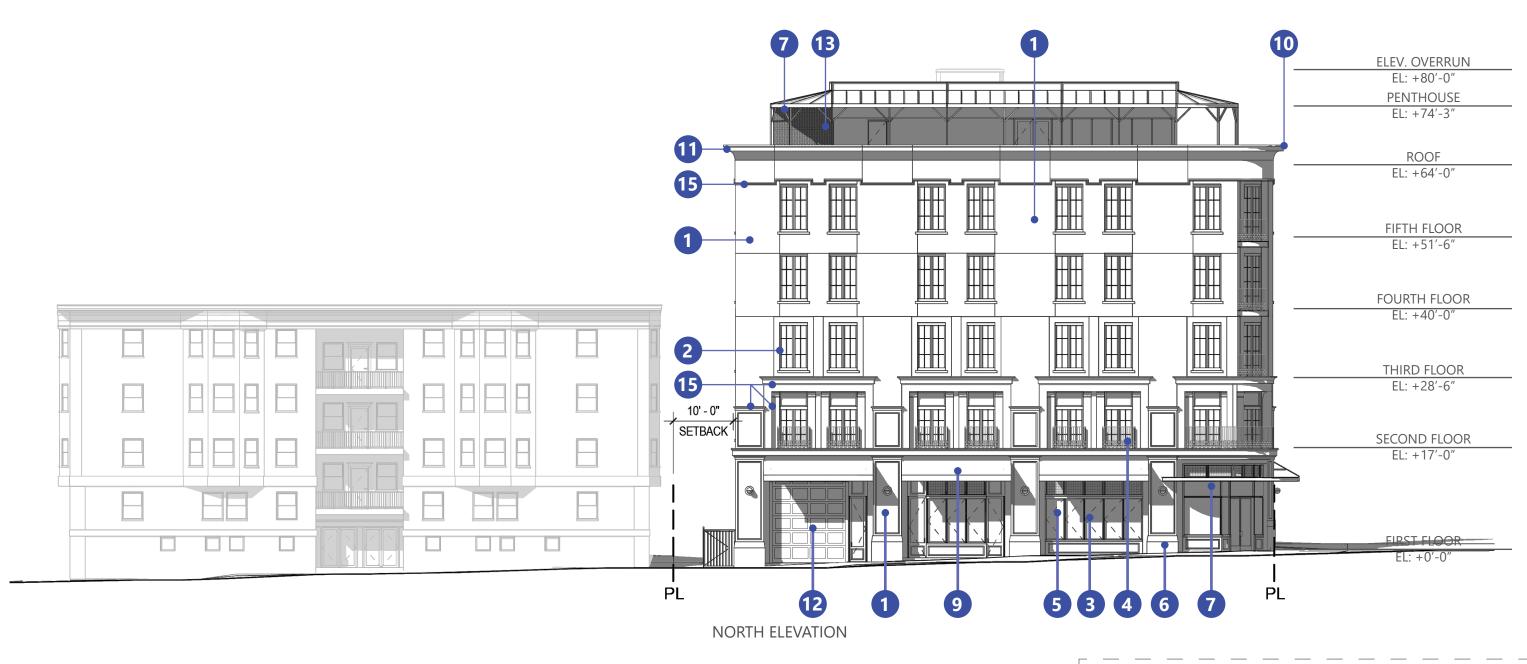
- 3-COAT STUCCO RAINSCREEN
- ALM. WINDOW, LIGHT BRONZE FINISH
- ALM. OPERABLE STOREFRONT, LIGHT BRONZE FINISH, W/ **DECORATIVE GLASS TRANSOM**
- CUSTOM ALM. GUARDRAIL, LIGHT BRONZE FINISH
- SOLID RED OAK WOOD PANELING FOR PAINT
- 3CM GRANITE STONE VENEER
- ALM. CANOPY, LIGHT BRONZE FINISH, W/ CLEAR GLASS ROOF





- 10 ALM. COPING CAP, PAINT TO MATCH STUCCO
  - ALM. DOOR, PAINT TO MATCH WALL
- ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS
  - DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH
- CONCRETE W/ ELASTOMERIC 14 PAINT , COLOR TO MATCH STUCCO
- DECORATIVE STUCCO MOULDING TRIM
- ALM. STOREFRONT, 16 PAINT TO MATCH STUCCO

AREA FOR CITY STAMP



BLACK AND WHITE ELEVATIONS (1880 NW FLANDERS ST.)







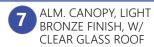












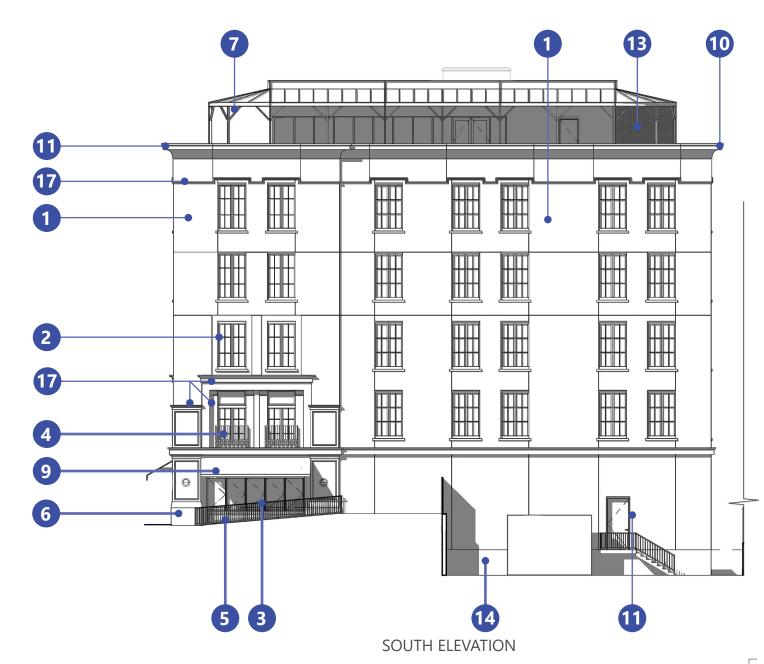




- 10 ALM. COPING CAP, PAINT 11 ALM. DOOR, PAINT TO TO MATCH STUCCO MATCH WALL
- ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS
  - DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH
- CONCRETE W/ ELASTOMERIC 14 PAINT , COLOR TO MATCH STUCCO
- DECORATIVE STUCCO MOULDING TRIM
- ALM. STOREFRONT, 16 PAINT TO MATCH STUCCO

ELEV. OVERRUN	
EL: +80'-0"	
PENTHOUSE	
EL: +74′-3″	
ROOF	
EL: +64'-0"	
FIFTH FLOOR	
EL: +51′-6″	
FOURTH FLOOR	
EL: +40'-0"	
THIRD FLOOR	
EL: +28'-6"	
SECOND FLOOR	
EL: +17'-0"	





BLACK AND WHITE ELEVATIONS (1880 NW FLANDERS ST.)

| SCALE: 1/16" = 1'-0"



AREA FOR CITY STAMP



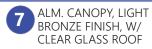












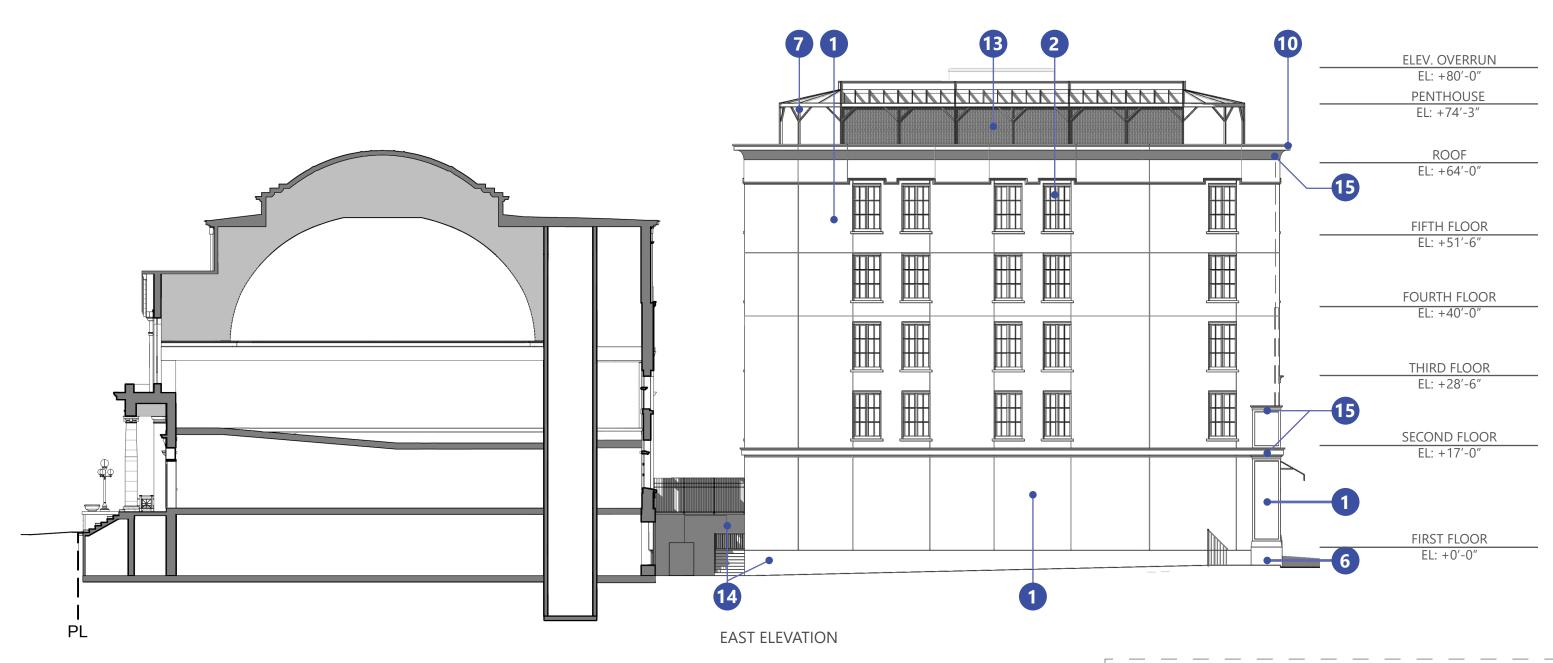




- 10 ALM. COPING CAP, PAINT 11 ALM. DOOR, PAINT TO TO MATCH STUCCO MATCH WALL
- ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS
- DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH
- CONCRETE W/ ELASTOMERIC 14 PAINT , COLOR TO MATCH STUCCO
- DECORATIVE STUCCO MOULDING TRIM

AREA FOR CITY STAMP

16 ALM. STOREFRONT, PAINT TO MATCH STUCCO



BLACK AND WHITE ELEVATIONS (1880 NW FLANDERS ST.)

| SCALE: 1/16" = 1'-0"





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR



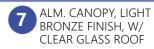
ALM. WINDOW, LIGHT BRONZE FINISH















10 ALM. COPING CAP, PAINT 11 ALM. DOOR, PAINT TO TO MATCH STUCCO MATCH WALL

ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS

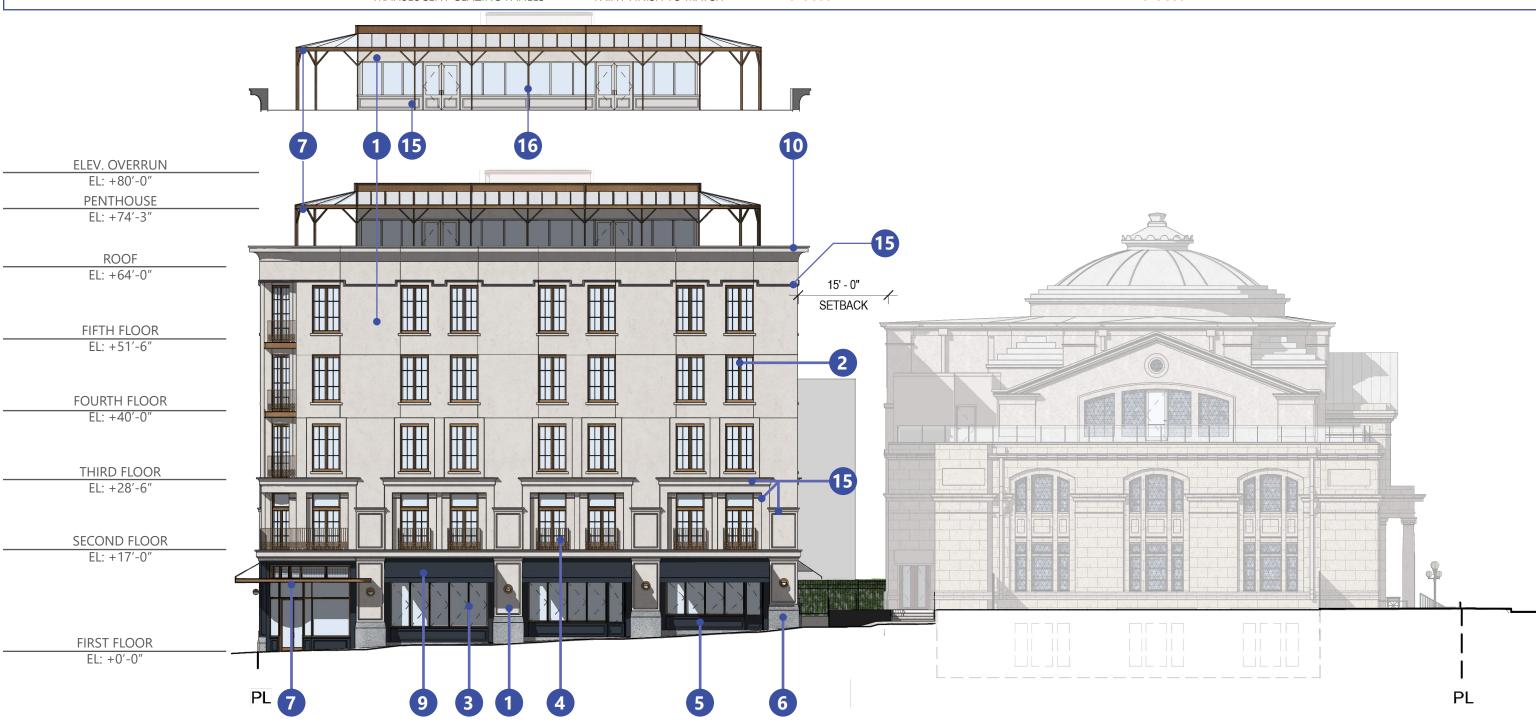
DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH

CONCRETE W/ ELASTOMERIC 14 PAINT , COLOR TO MATCH STUCCO

DECORATIVE STUCCO MOULDING TRIM

16 ALM. STOREFRONT, PAINT TO MATCH STUCCO

area for city stamp



WEST ELEVATION

## COLOR ELEVATIONS (1880 NW FLANDERS ST.)







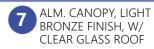
2 ALM. WINDOW, LIGHT BRONZE FINISH















10 ALM. COPING CAP, PAINT TO MATCH STUCCO

ALM. DOOR, PAINT TO MATCH WALL

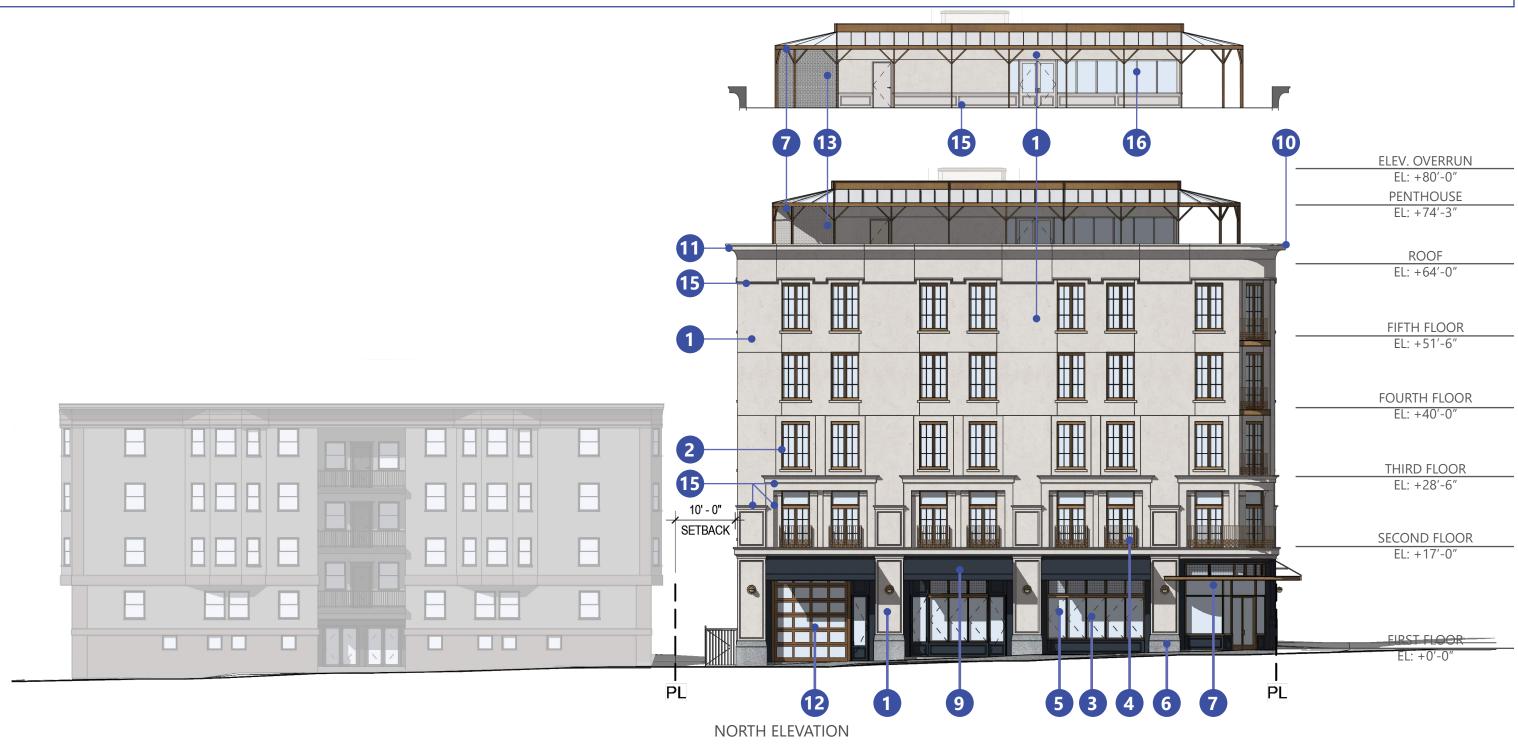
ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS

DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH

CONCRETE W/ ELASTOMERIC PAINT , COLOR TO MATCH STUCCO

15 DECORATIVE STUCCO MOULDING TRIM

ALM. STOREFRONT, 16 PAINT TO MATCH STUCCO



COLOR ELEVATIONS (1880 NW FLANDERS ST.)

| SCALE: 1/16" = 1'-0"







1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

area for city stamp



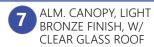












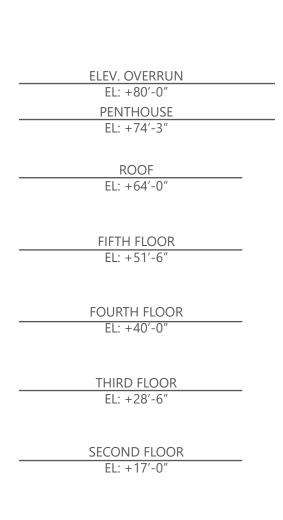


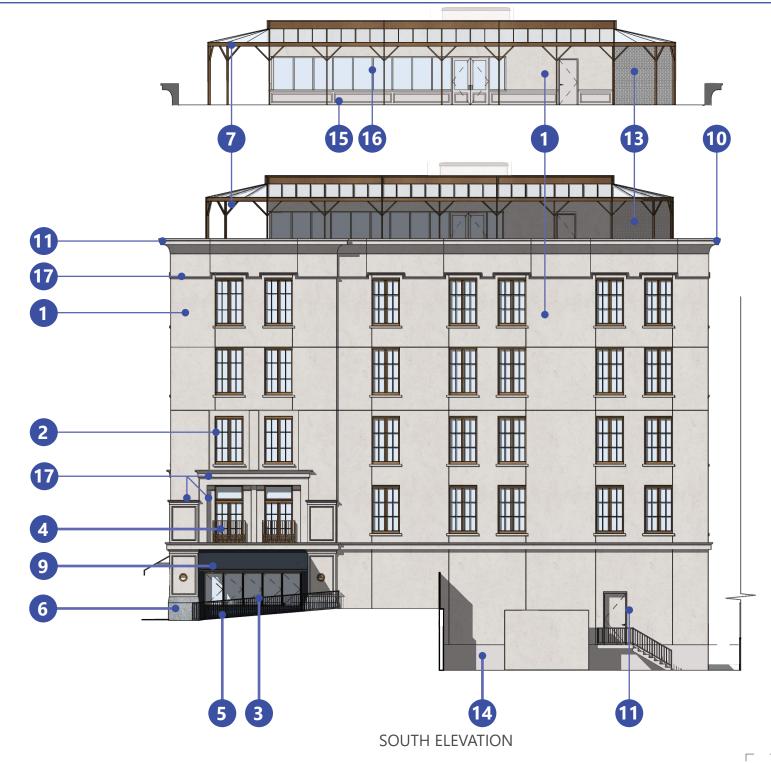


- 10 ALM. COPING CAP, PAINT 11 ALM. DOOR, PAINT TO TO MATCH STUCCO MATCH WALL
- ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS
  - DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH
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- DECORATIVE STUCCO MOULDING TRIM

AREA FOR CITY STAMP

ALM. STOREFRONT, 16 PAINT TO MATCH STUCCO





COLOR ELEVATIONS (1880 NW FLANDERS ST.)

| SCALE: 1/16" = 1'-0"



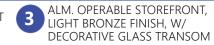


FIRST FLOOR EL: +0'-0"





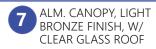
ALM. WINDOW, LIGHT BRONZE FINISH















10 ALM. COPING CAP, PAINT 11 ALM. DOOR, PAINT TO TO MATCH STUCCO MATCH WALL

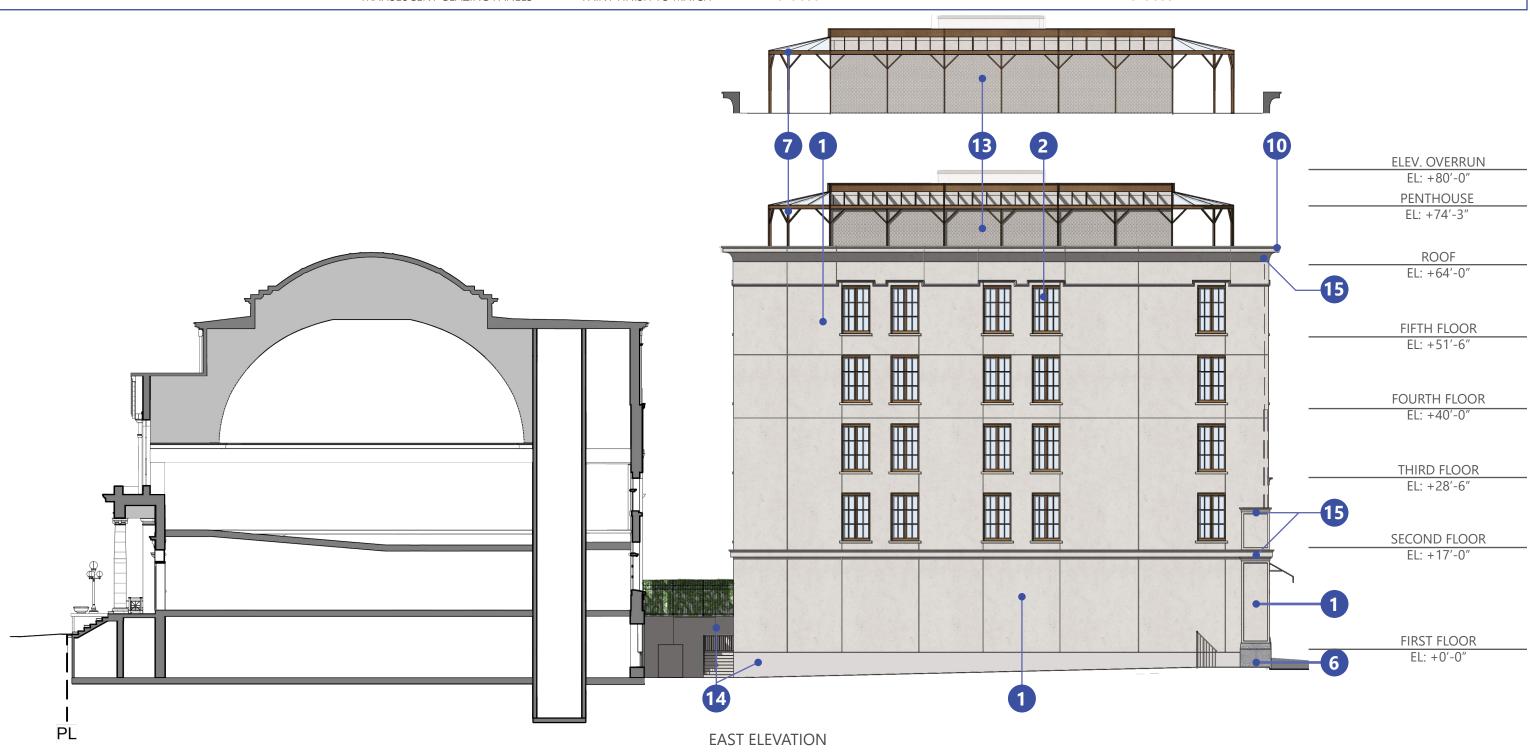
ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS

DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH

CONCRETE W/ ELASTOMERIC PAINT , COLOR TO MATCH STUCCO

DECORATIVE STUCCO MOULDING TRIM

ALM. STOREFRONT, 16 PAINT TO MATCH STUCCO

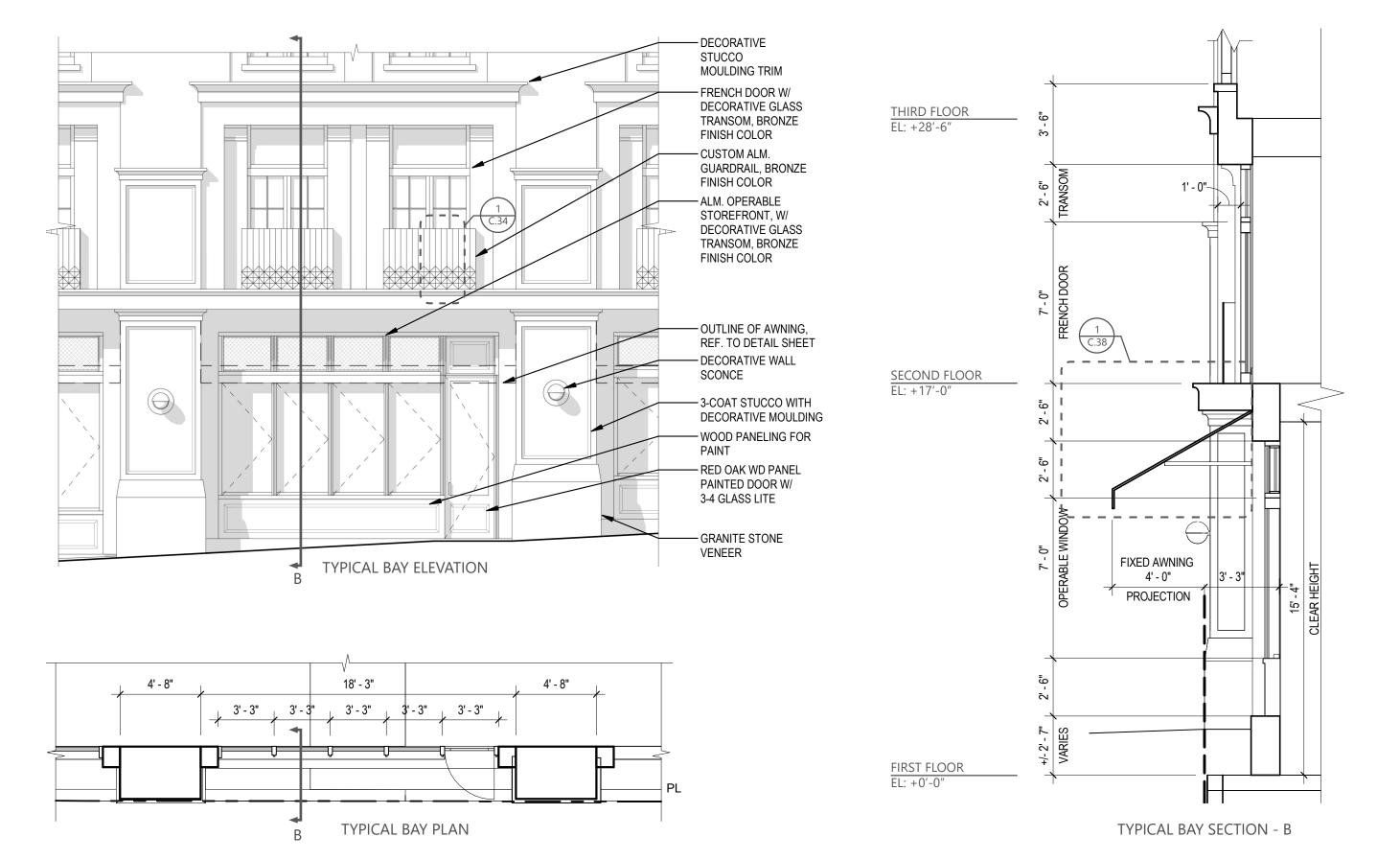


COLOR ELEVATIONS (1880 NW FLANDERS ST.)









#### DETAILS (1880 NW FLANDERS ST.)

| SCALE: 3/16" = 1'-0" & 1/4" = 1'-0"







north



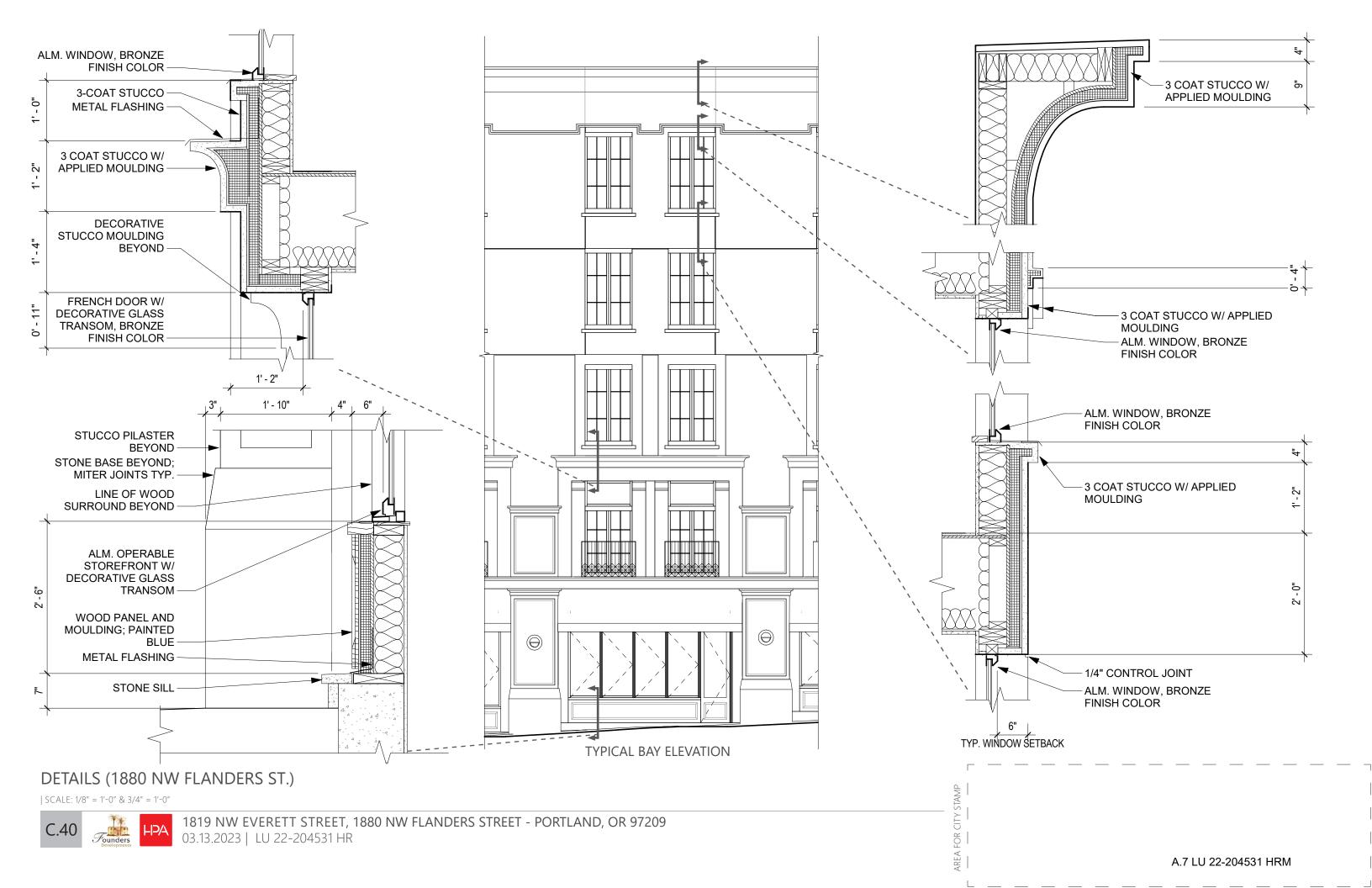
TYPICAL AWNING ELEVATION FIXED AWNING SECTION

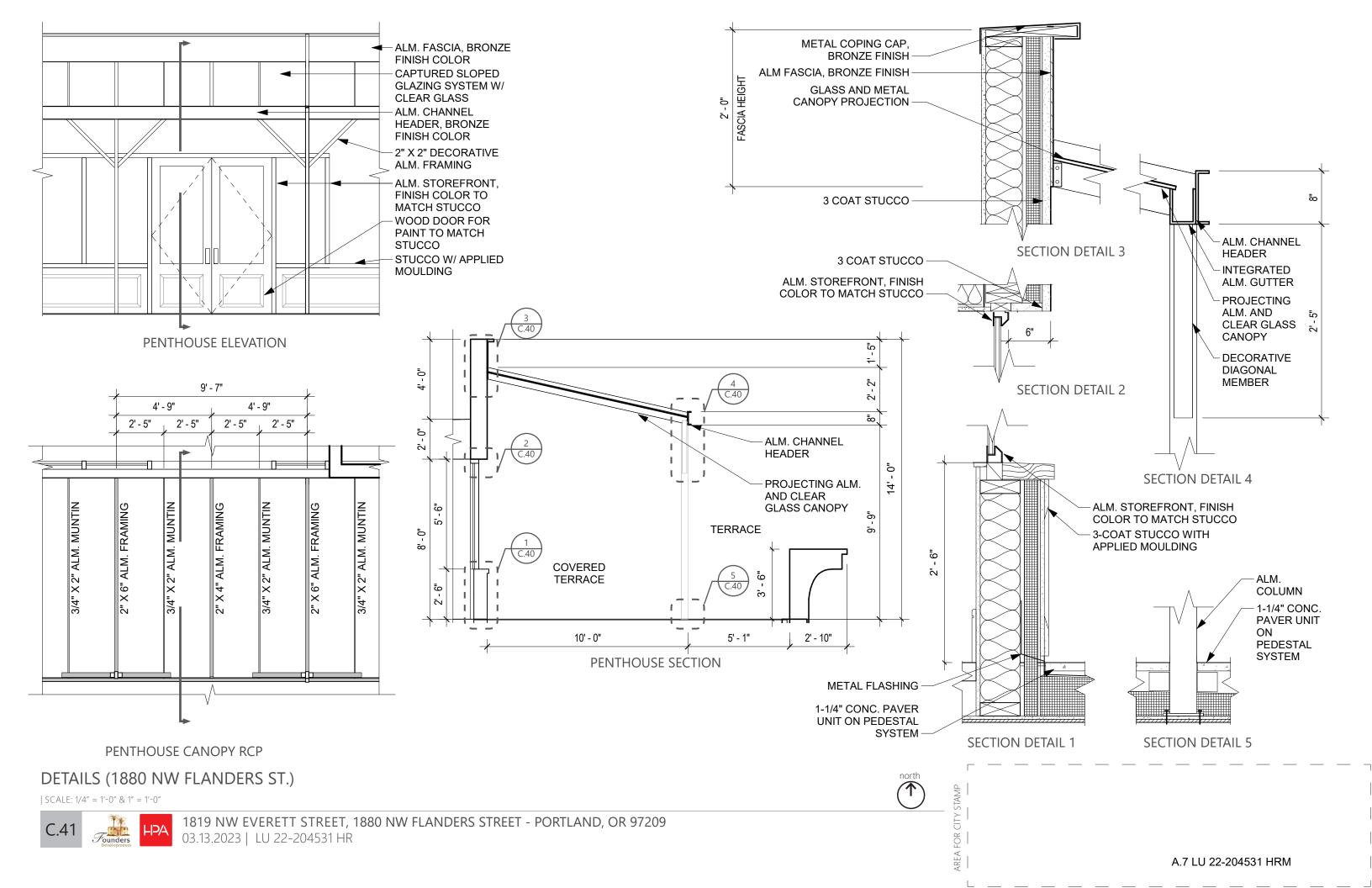
### DETAILS (1880 NW FLANDERS ST.)

| SCALE: 3/8" = 1'-0" & 3/4" = 1'-0"

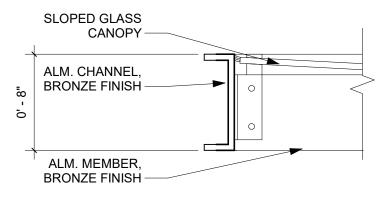




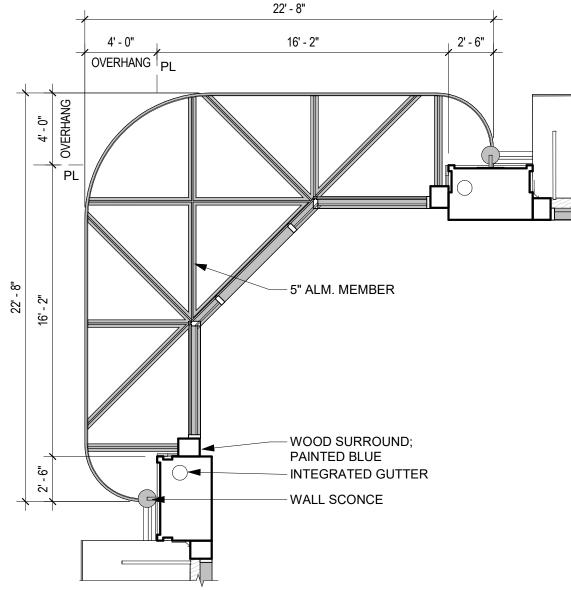




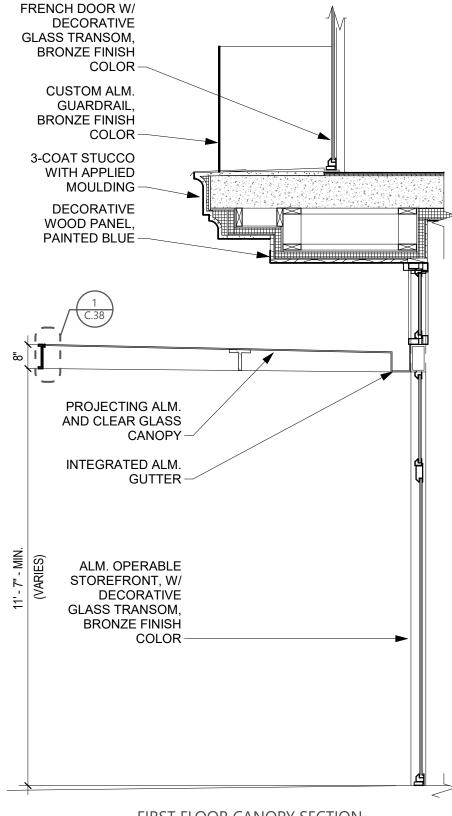




**CANOPY DETAIL SECTION 1** 



FIRST FLOOR CANOPY RCP



FIRST FLOOR CANOPY SECTION

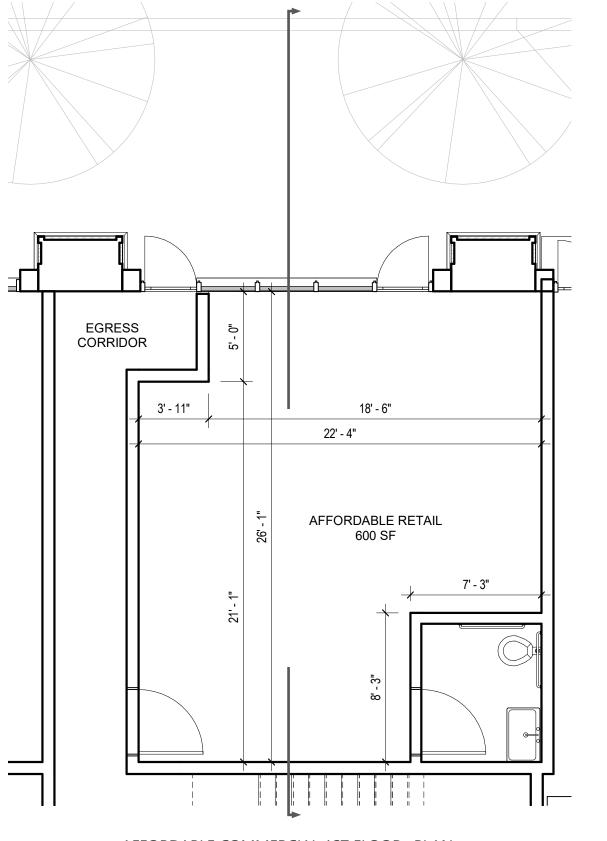
DETAILS (1880 NW FLANDERS ST.)

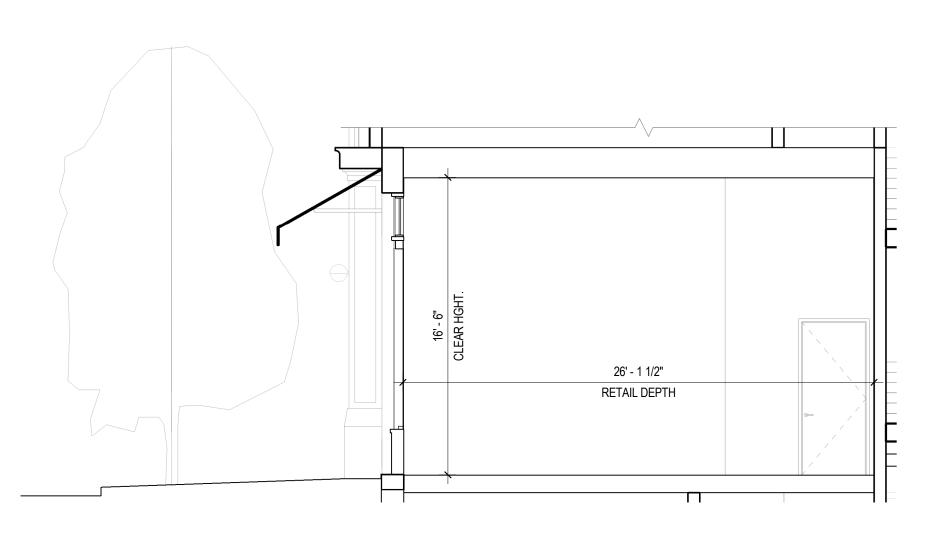
| SCALE: 3/16" = 1'-0", 3/8" = 1'-0", & 1-1/2" = 1'-0"











AFFORDABLE COMMERCIAL 1ST FLOOR PLAN

#### AFFORDABLE COMMERCIAL SECTION

## AFFORDABLE COMMERCIAL SPACE

| SCALE: 3/16" = 1'-0"





**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR



A.7 LU 22-204531 HRM



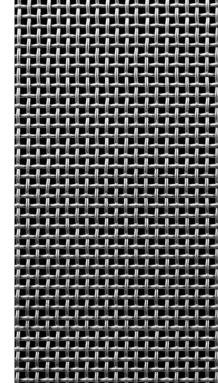




WOOD STOREFRONT SURROUND



CUSTOM RAILING EXAMPLE-LIGHT BRONZE FINISH



MESH SCREEN



EXISTING HISTORIC STONE FACADE



STUCCO RAINSCREEN, SMOOTH FINISH



DECORATIVE GLASS



CHARCOAL GRANITE STONE VENER -FLAMED FINISH



METAL - LIGHT BRONZE

## FACADE MATERIALS (1880 NW FLANDERS ST.)



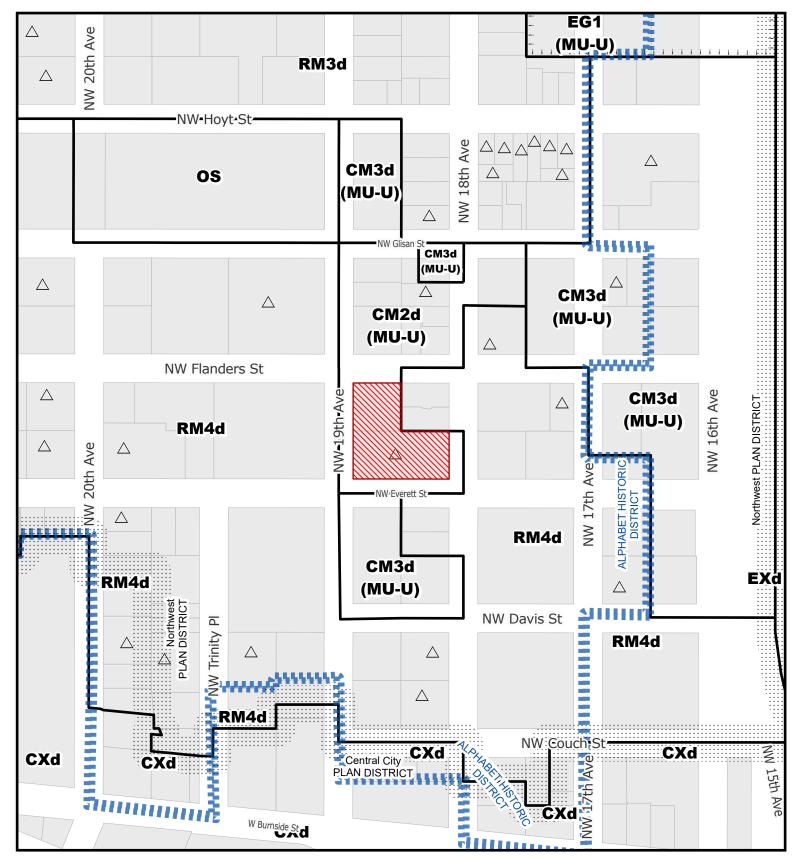


# TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

# 1819 NW EVERETT STREET 1880 NW FLANDERS STREET

Historic & New Construction Drawings

March 13th, 2023



For Zoning Code in Effect Post October 1, 2022



THIS SITE LIES WITHIN THE: NORTHWEST PLAN DISTRICT ALPHABET HISTORIC DISTRICT Site

Plan District

Historic District

A Historic Landmark

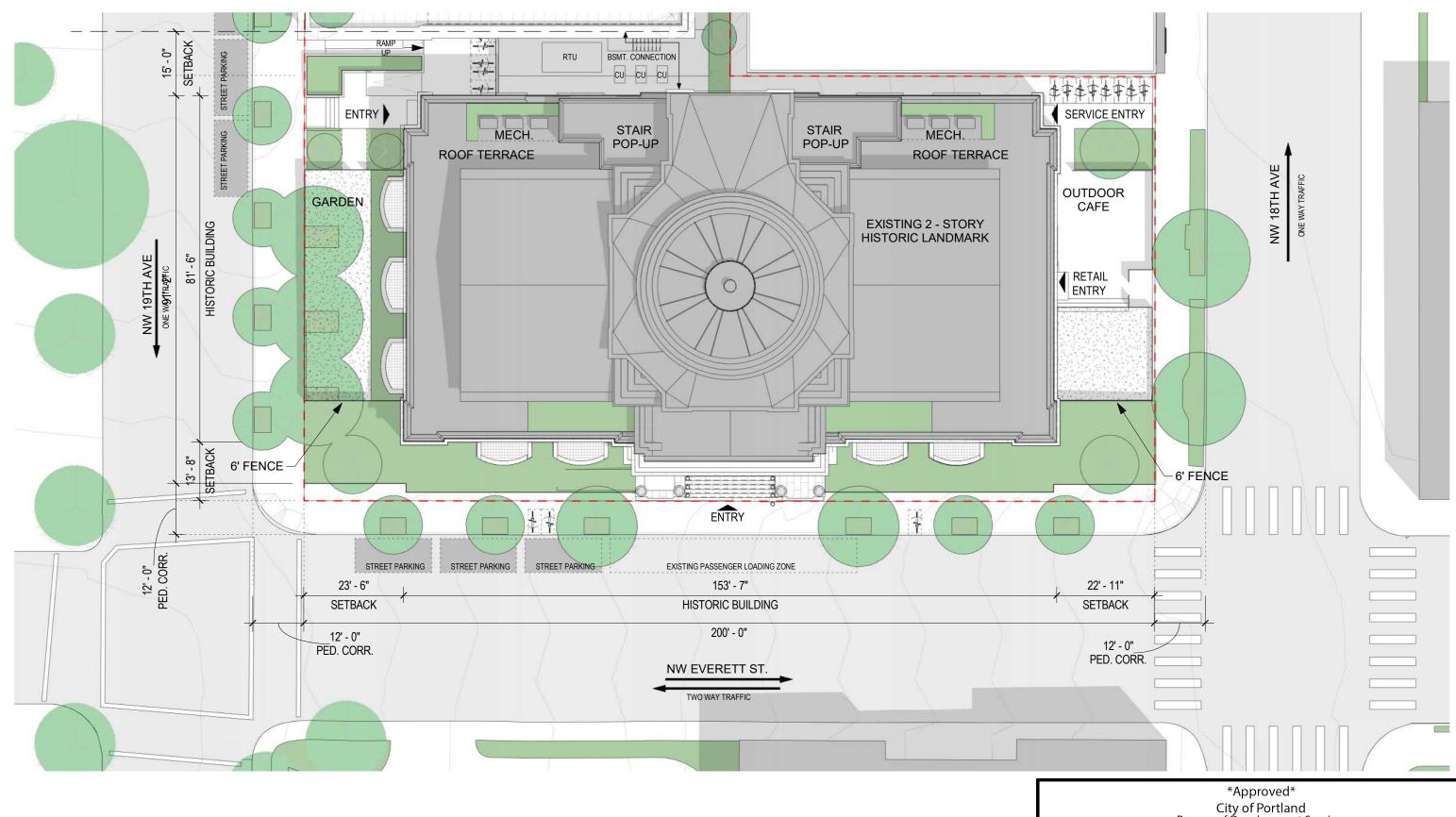
File No. LU 22 - 204531 HRM AD

1/4 Section 3028

Scale 1 inch = 200 feet

State ID 1N1E33DB 2800

Exhibit B Jan 23, 2023



### ENLARGED SITE PLAN (1819 NW EVERETT ST.)

| SCALE: 1" = 64'-0"





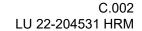
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

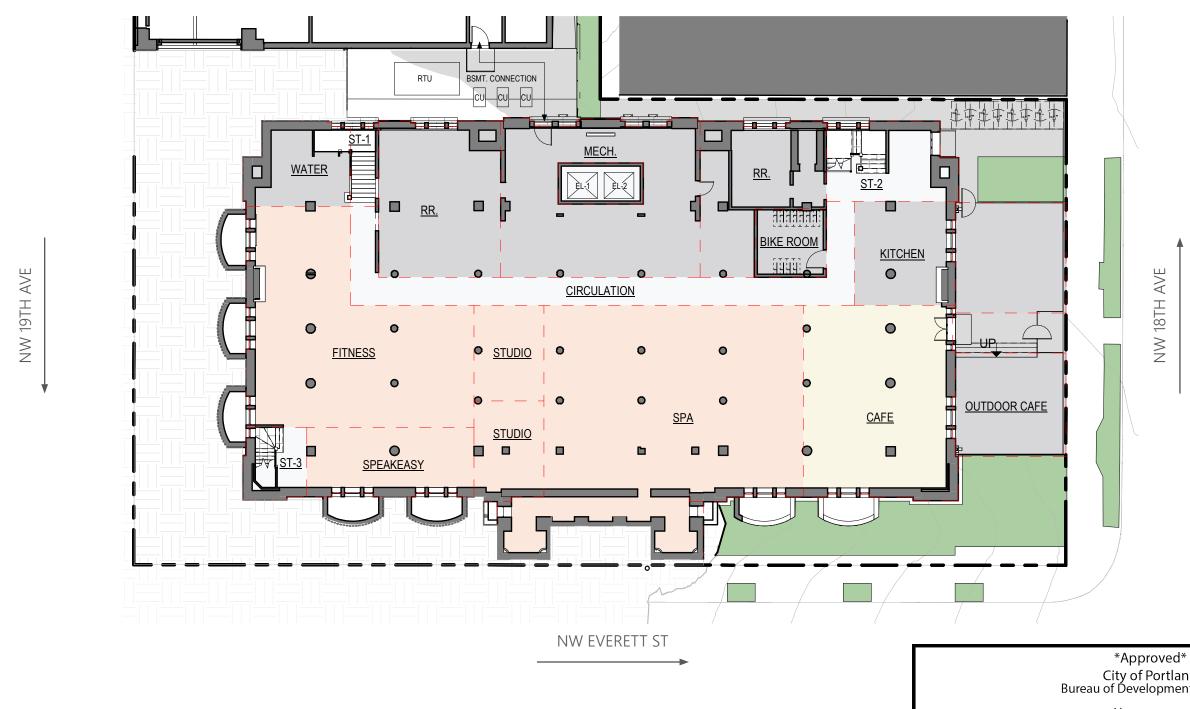


Date 03-24-2023

north

\*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.





BASEMENT FLOOR PLAN (1819 NW EVERETT ST.)

| SCALE: 1" = 20-0"





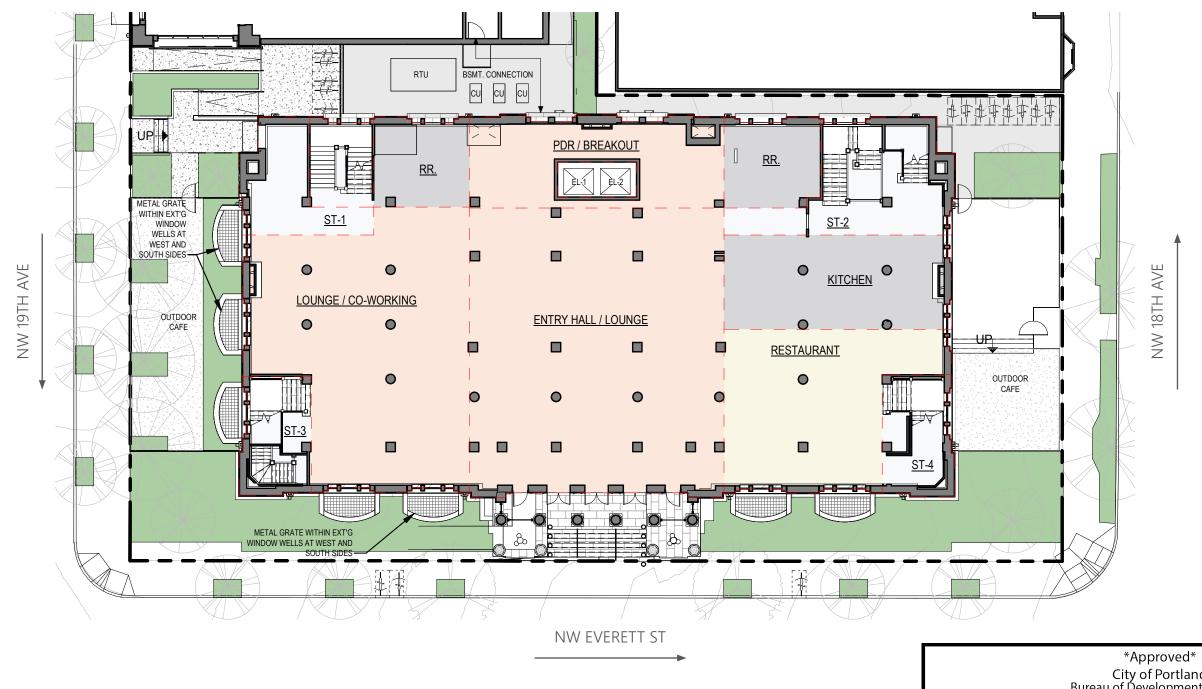
City of Portland Bureau of Development Services

Planner

03-24-2023

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> C.003 LU 22-204531 HRM



FIRST FLOOR PLAN (1819 NW EVERETT ST.)

| SCALE: 1" = 20-0"





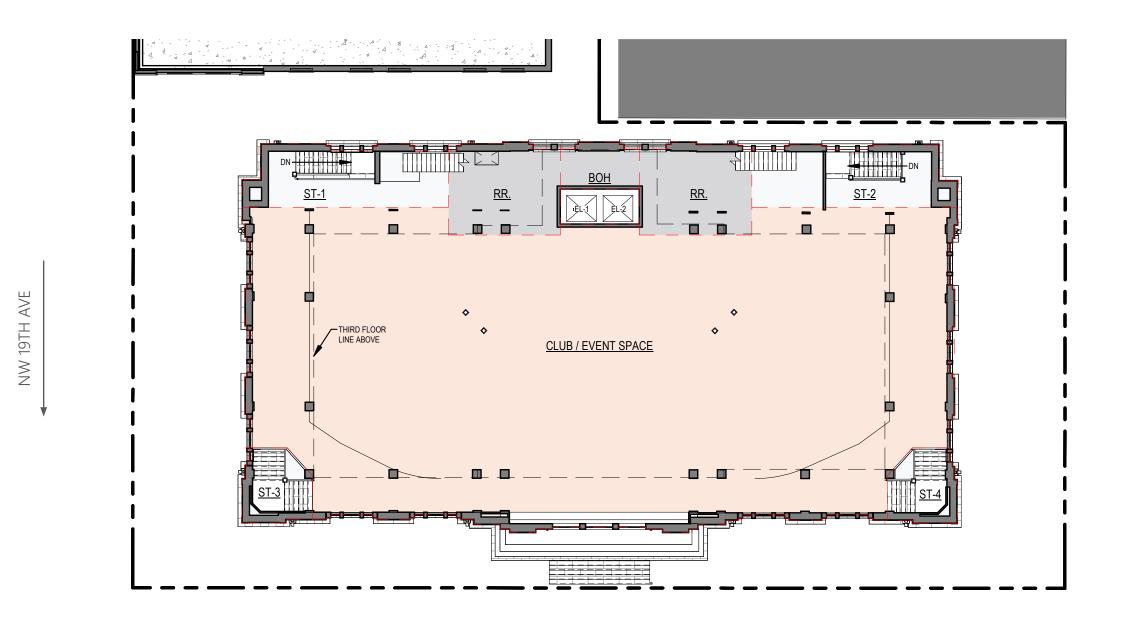


City of Portland Bureau of Development Services

Planner

03-24-2023

Date 03-24-2023
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NW EVERETT ST

\*Approved\* City of Portland Bureau of Development Services

NW 18TH AVE

Planner\_

03-24-2023

Date 03-24-2023
\* This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

SECOND FLOOR PLAN (1819 NW EVERETT ST.)

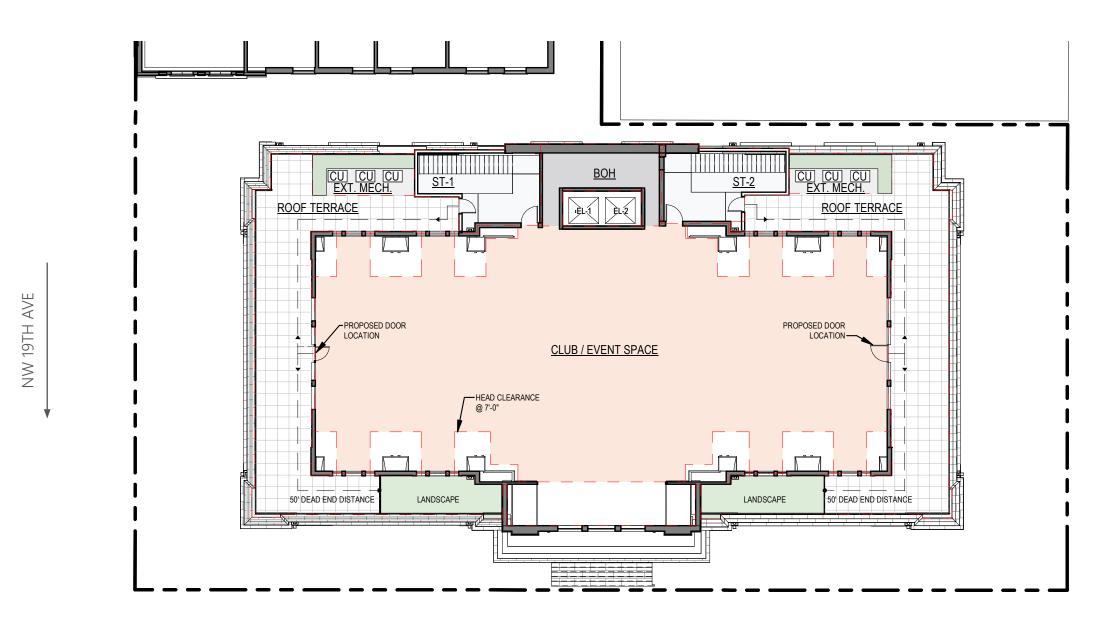
| SCALE: 1" = 20-0"











NW EVERETT ST

\*Approved\* City of Portland Bureau of Development Services

NW 18TH AVE

Planner

03-24-2023

Date <u>03-24-2023</u>
\*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

THIRD FLOOR PLAN (1819 NW EVERETT ST.)

| SCALE: 1" = 20-0"

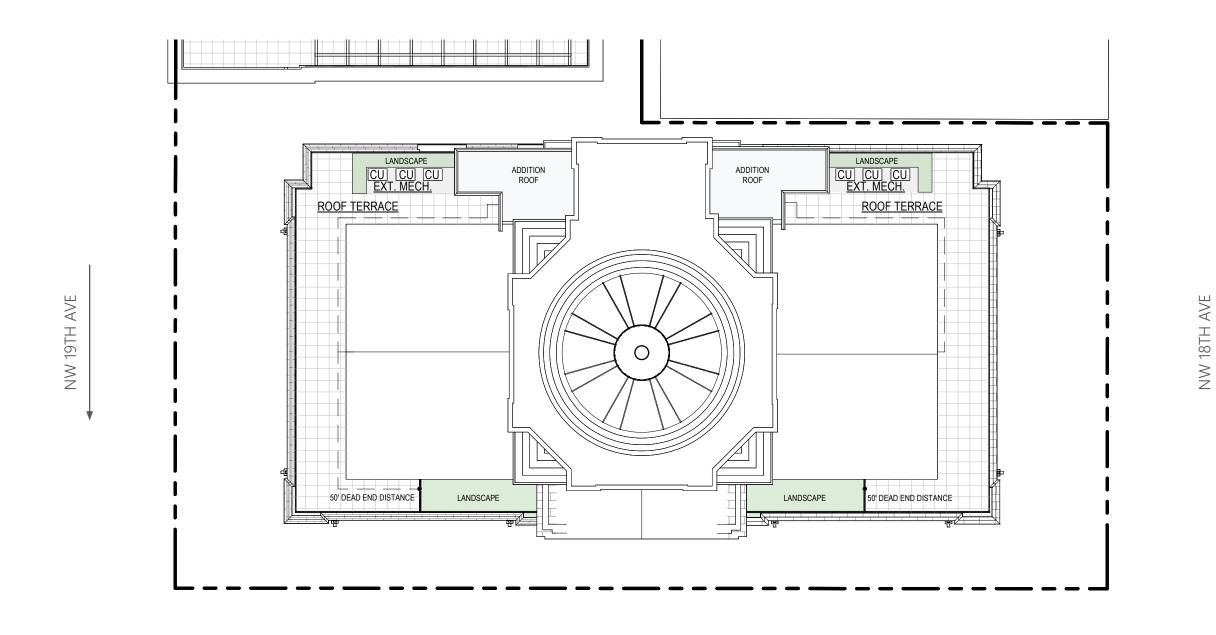








C.006 LU 22-204531 HRM



NW EVERETT ST

\*Approved\* City of Portland Bureau of Development Services

Planner\_

03-24-2023

Date \_\_\_\_\_\_03-24-2023
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ROOF PLAN (1819 NW EVERETT ST.)

| SCALE: 1" = 20-0"



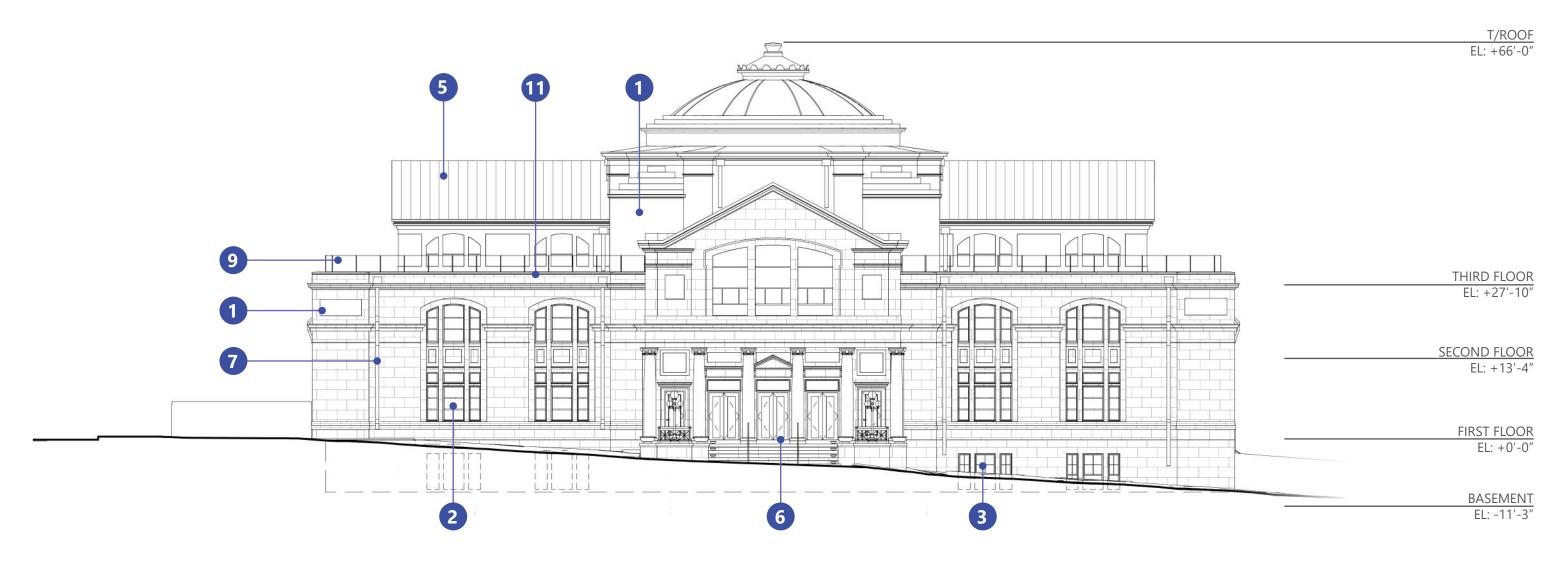






- EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
- 2 EXISTING STEEL WINDOW WITH LEADED GLASS (SEE WINDOW SCOPE PAGE FOR DTLS)
- 3 EXISTING WOOD WINDOW FOR REPAIR AND PAINT
- 4 EXISTING METAL CLADDING TO REMAIN; REPAIR AND PAINT TO MATCH EXISTING
- 5 EXISTING COATED TIN ROOFING TO BE REPAIRED AND/OR REPLACED IN KIND
- 6 EXISTING WOOD DOOR FOR REPAIR AND STAIN
- NEW METAL COPING CAP FOR PAINT TO MATCH WALL
- NEW METAL DOOR FOR PAINT TO MATCH WALL

- EXISTING SCUPPER AND DOWNSPOUT TO REMAIN FOR PAINT
- NEW FABRIC AWNING IN EXISTING LOCATION 9
  - NEW FRAMELESS GLASS GAURDRAIL, CLEAR LOW-IRON GLASS
- 10 NEW STUCCO RAINSCREEN



**SOUTH ELEVATION** 

### BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

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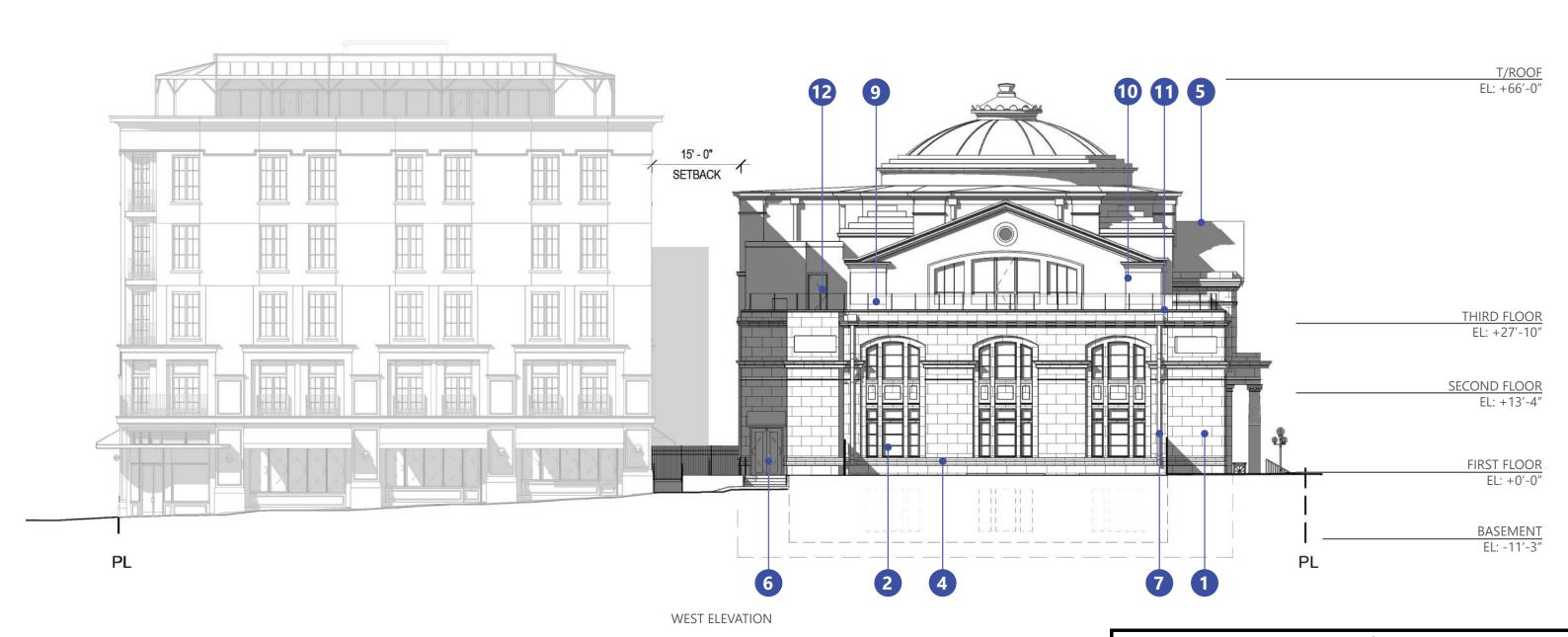
03-24-2023

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> C.008 LU 22-204531 HRM

- EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
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- 6 EXISTING WOOD DOOR FOR REPAIR AND STAIN
  - NEW METAL DOOR FOR PAINT TO MATCH WALL

- EXISTING SCUPPER AND DOWNSPOUT TO REMAIN FOR PAINT
- NEW FABRIC AWNING IN EXISTING LOCATION
- NEW FRAMELESS GLASS GAURDRAIL, 9 CLEAR LOW-IRON GLASS
- 10 NEW STUCCO RAINSCREEN
  - NEW METAL COPING CAP FOR PAINT TO MATCH WALL



### BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

\*Approved\* City of Portland Bureau of Development Services

Planner 03-24-2023

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> C.009 LU 22-204531 HRM

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NEW METAL COPING CAP FOR PAINT TO MATCH WALL

- 6 EXISTING WOOD DOOR FOR REPAIR AND STAIN
  - NEW METAL DOOR FOR PAINT TO MATCH WALL

- EXISTING SCUPPER AND DOWNSPOUT TO REMAIN FOR PAINT
- NEW FABRIC AWNING IN EXISTING LOCATION
- NEW FRAMELESS GLASS GAURDRAIL, 9 CLEAR LOW-IRON GLASS
- 10 NEW STUCCO RAINSCREEN

EL: +66'-0"

T/ROOF (HISTORIC)

THIRD FLOOR (HISTORIC)

EL: +27'-10"

SECOND FLOOR (HISTORIC)

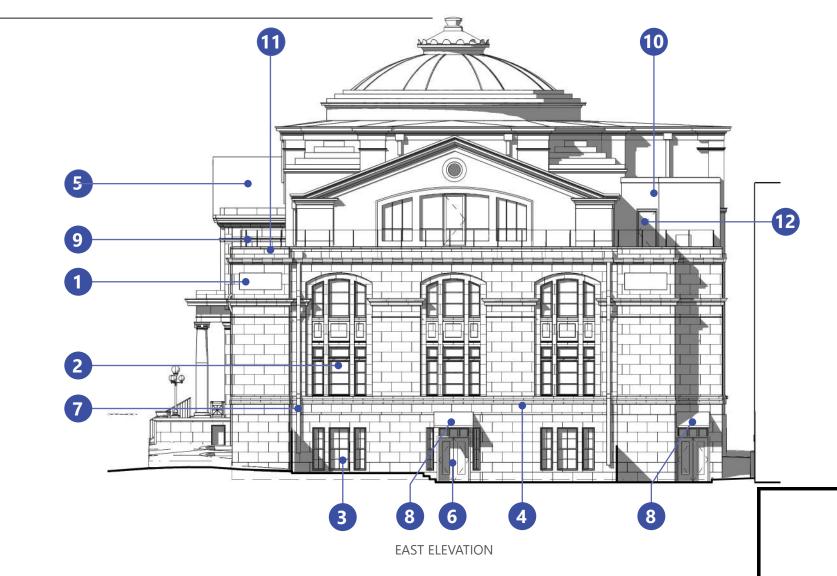
EL: +13'-4"

FIRST FLOOR (HISTORIC)

EL: +0'-0"

**BASEMENT (HISTORIC)** 

EL: -11'-3"



\*Approved\* City of Portland Bureau of Development Services

Planner

03-24-2023

Date \* This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

C.010 LU 22-204531 HRM

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- NEW METAL COPING CAP FOR PAINT TO MATCH WALL

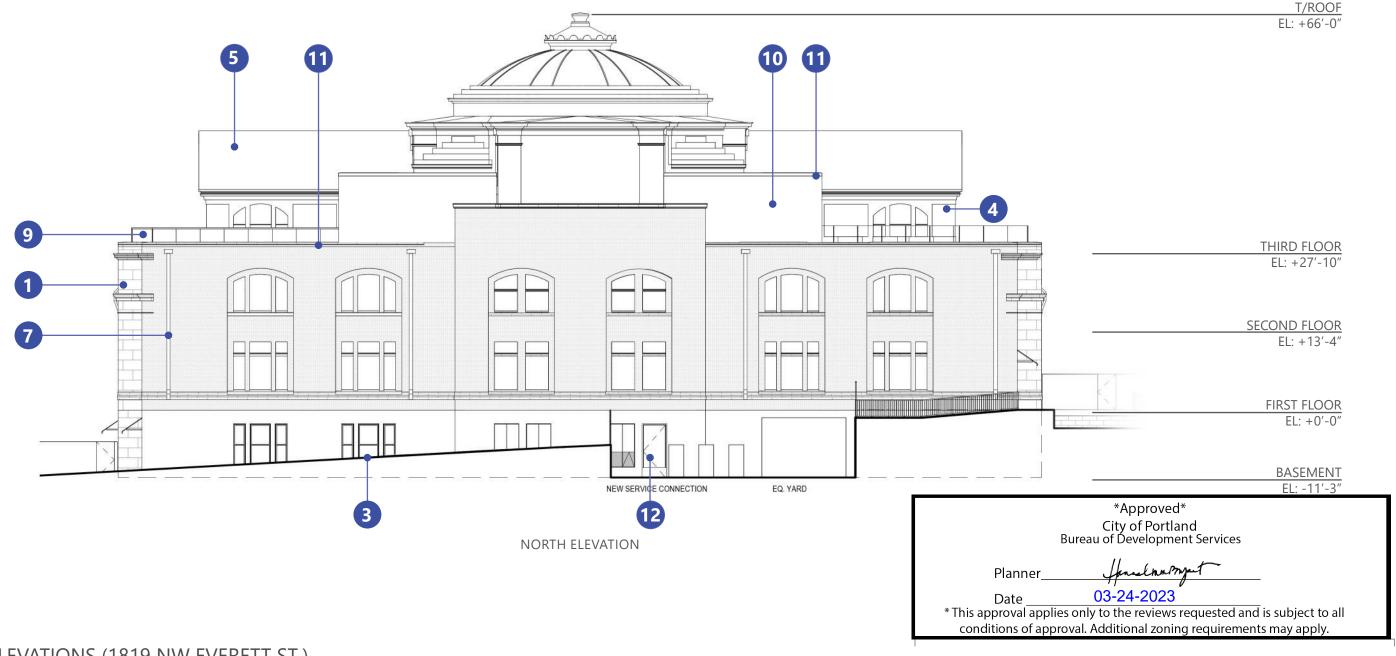




NEW FABRIC AWNING IN EXISTING LOCATION

NEW FRAMELESS GLASS GAURDRAIL, 9 CLEAR LOW-IRON GLASS

10 NEW STUCCO RAINSCREEN



BLACK AND WHITE ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

C.011 LU 22-204531 HRM

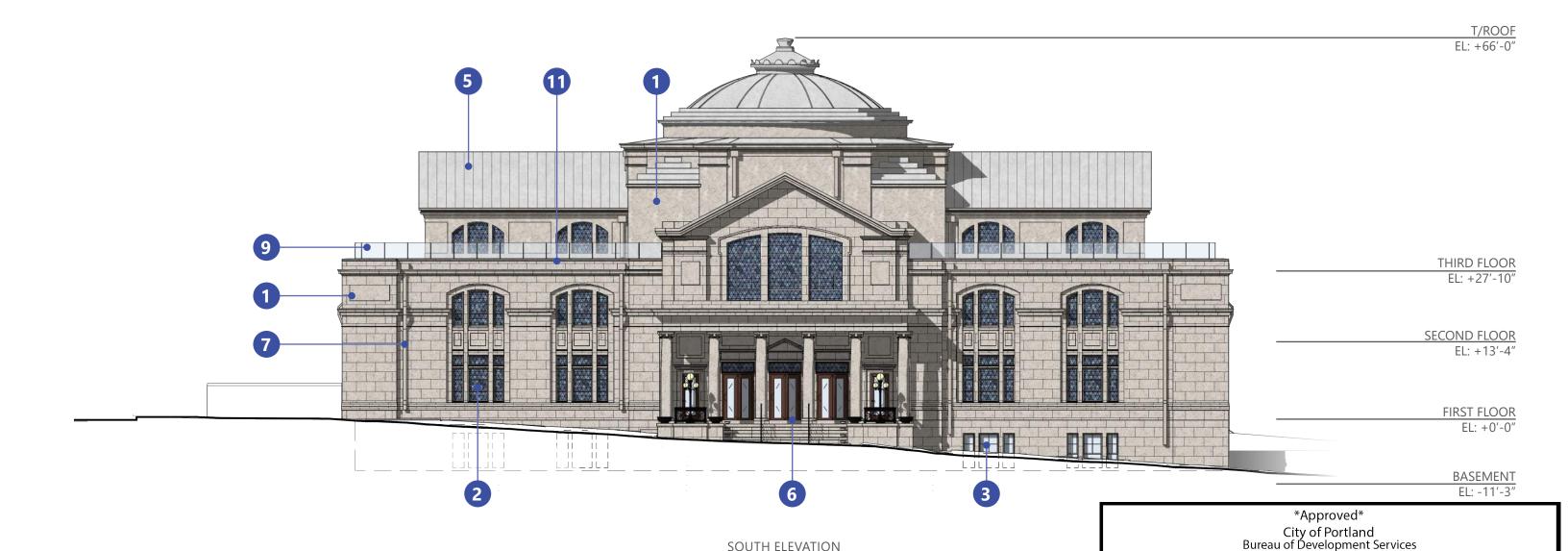
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- NEW METAL COPING CAP FOR PAINT TO MATCH WALL

Planner

03-24-2023 \*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

NEW METAL DOOR FOR PAINT TO MATCH WALL

- EXISTING SCUPPER AND DOWNSPOUT TO REMAIN FOR PAINT
- NEW FABRIC AWNING IN EXISTING LOCATION
- NEW FRAMELESS GLASS GAURDRAIL, 9 CLEAR LOW-IRON GLASS
- 10 NEW STUCCO RAINSCREEN



**SOUTH ELEVATION** 

COLOR ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

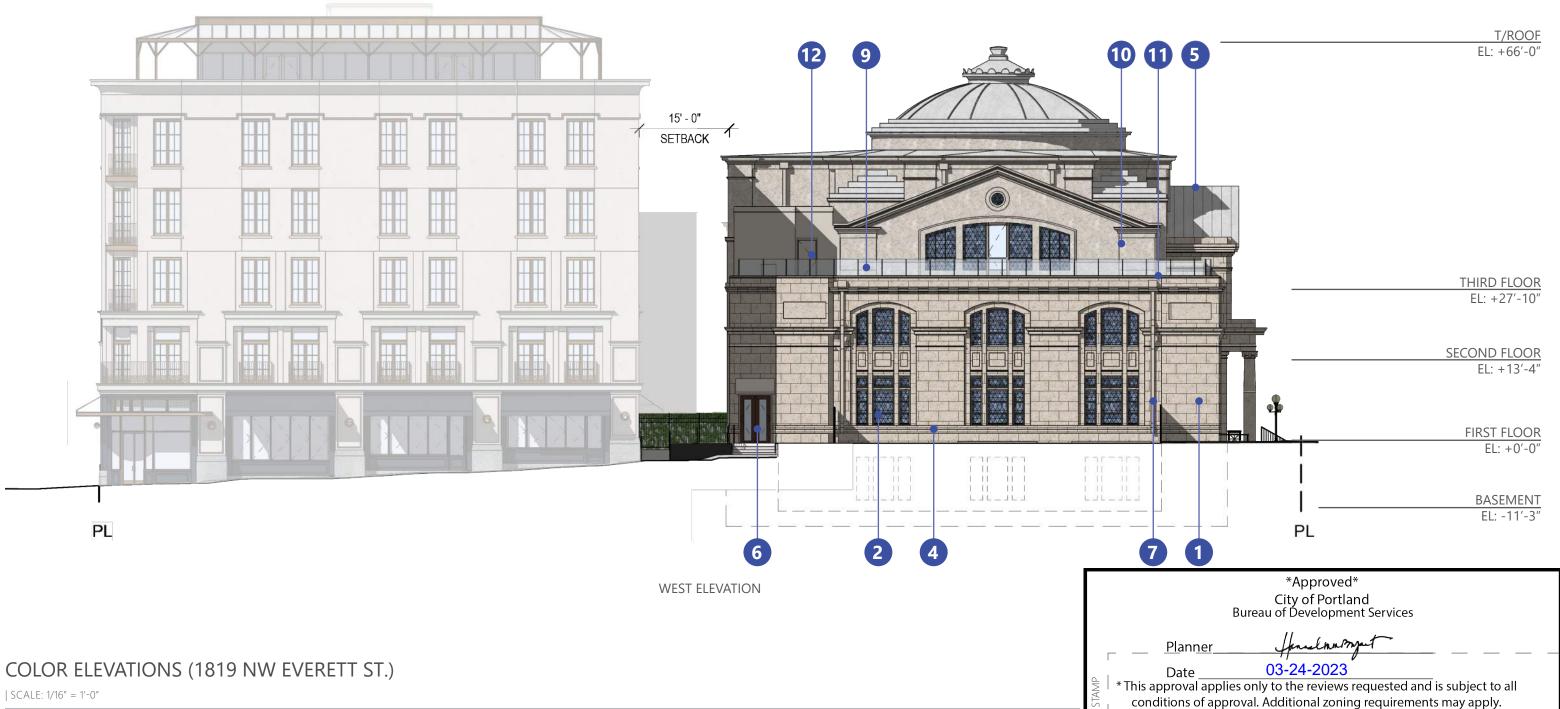
C.012 LU 22-204531 HRM

- EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
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1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

C.013 LU 22-204531 HRM

- 1 EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
- 2 EXISTING STEEL WINDOW WITH LEADED GLASS (SEE WINDOW SCOPE PAGE FOR DTLS)
- 3 EXISTING WOOD WINDOW FOR REPAIR AND PAINT
- 4 EXISTING METAL CLADDING TO REMAIN; REPAIR AND PAINT TO MATCH EXISTING
- 5 EXISTING COATED TIN ROOFING TO BE REPAIRED AND/OR REPLACED IN KIND
- E 6 EXISTING WOOD DOOR FOR REPAIR AND STAIN
- NEW METAL COPING CAP FOR PAINT 12 NEW METAL DOOR FOR PAINT TO MATCH WALL

- 7 EXISTING SCUPPER AND DOWNSPOUT TO REMAIN FOR PAINT
- 8 NEW FABRIC AWNING IN EXISTING LOCATION
- 9 NEW FRAMELESS GLASS GAURDRAIL, CLEAR LOW-IRON GLASS
- 10 NEW STUCCO RAINSCREEN

T/ROOF (HISTORIC) EL: +66'-0"

THIRD FLOOR (HISTORIC)

EL: +27'-10"

SECOND FLOOR (HISTORIC)

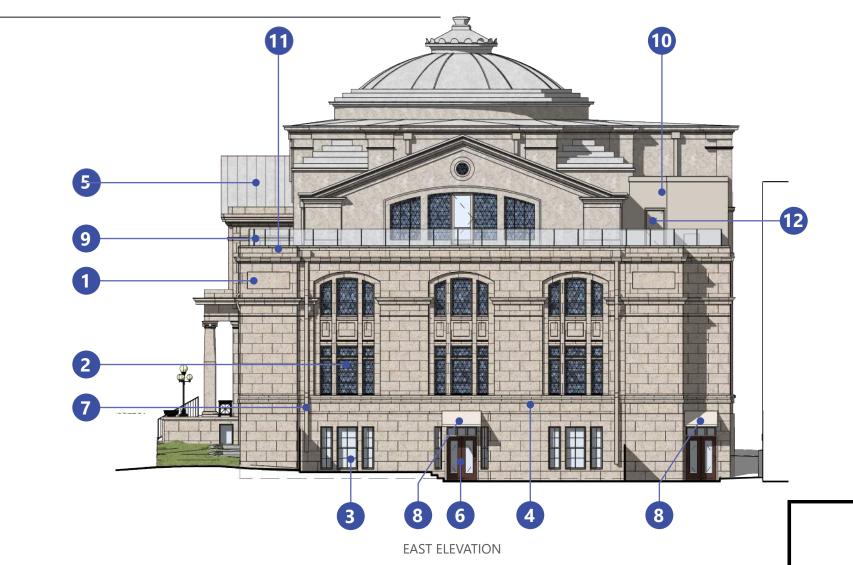
EL: +13'-4"

FIRST FLOOR (HISTORIC)

EL: +0'-0"

**BASEMENT (HISTORIC)** 

EL: -11'-3"



\*Approved\*
City of Portland
Bureau of Development Services

Planner\_\_

Date 03-24-2023

\*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

COLOR ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

C.014 LU 22-204531 HRM

- EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
- 2 EXISTING STEEL WINDOW WITH LEADED GLASS (SEE WINDOW SCOPE PAGE FOR DTLS)
- 3 EXISTING WOOD WINDOW FOR REPAIR AND PAINT
- 4 EXISTING METAL CLADDING TO REMAIN; REPAIR AND PAINT TO MATCH EXISTING
- 5 EXISTING COATED TIN ROOFING TO BE REPAIRED AND/OR REPLACED IN KIND
- 6 EXISTING WOOD DOOR FOR REPAIR AND STAIN
- NEW METAL COPING CAP FOR PAINT TO MATCH WALL
- NEW METAL DOOR FOR PAINT TO MATCH WALL

- EXISTING SCUPPER AND DOWNSPOUT TO REMAIN FOR PAINT
- NEW FABRIC AWNING IN EXISTING LOCATION
- NEW FRAMELESS GLASS GAURDRAIL, 9 CLEAR LOW-IRON GLASS
- 10 NEW STUCCO RAINSCREEN

T/ROOF EL: +66'-0" M 4 THIRD FLOOR EL: +27'-10" SECOND FLOOR EL: +13'-4" FIRST FLOOR EL: +0'-0" **BASEMENT** NEW SERVICE CONNECTION EL: -11'-3" EQ. YARD 3

NORTH ELEVATION

COLOR ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"



1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

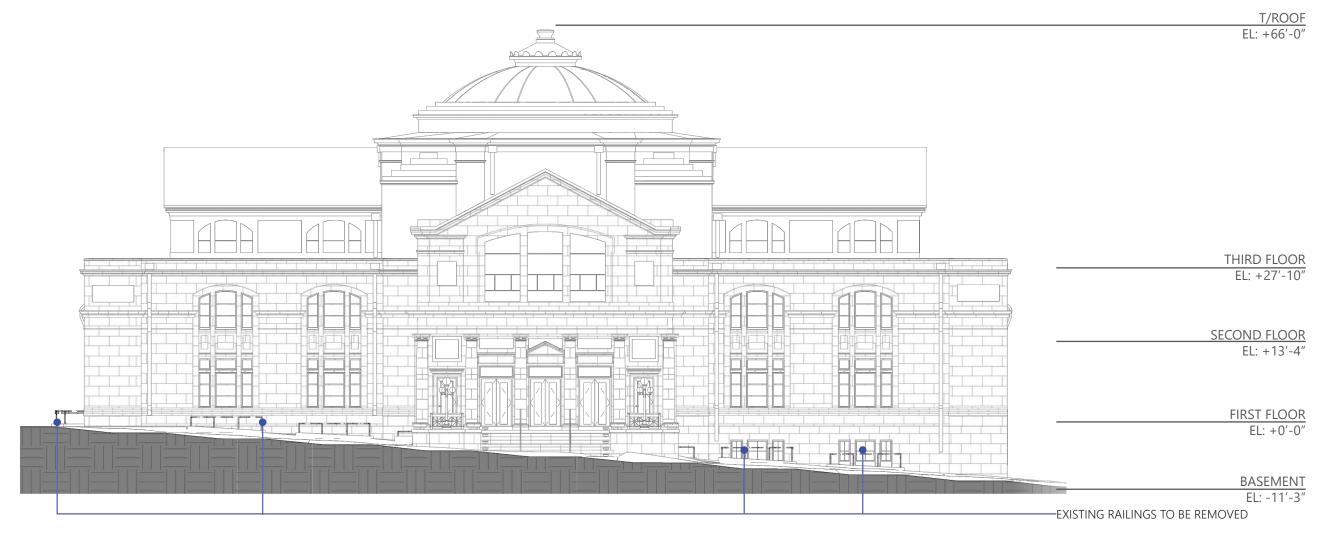
\*Approved\* City of Portland Bureau of Development Services

Planner

03-24-2023

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> C.015 LU 22-204531 HRM



SOUTH ELEVATION

## DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"

C.16



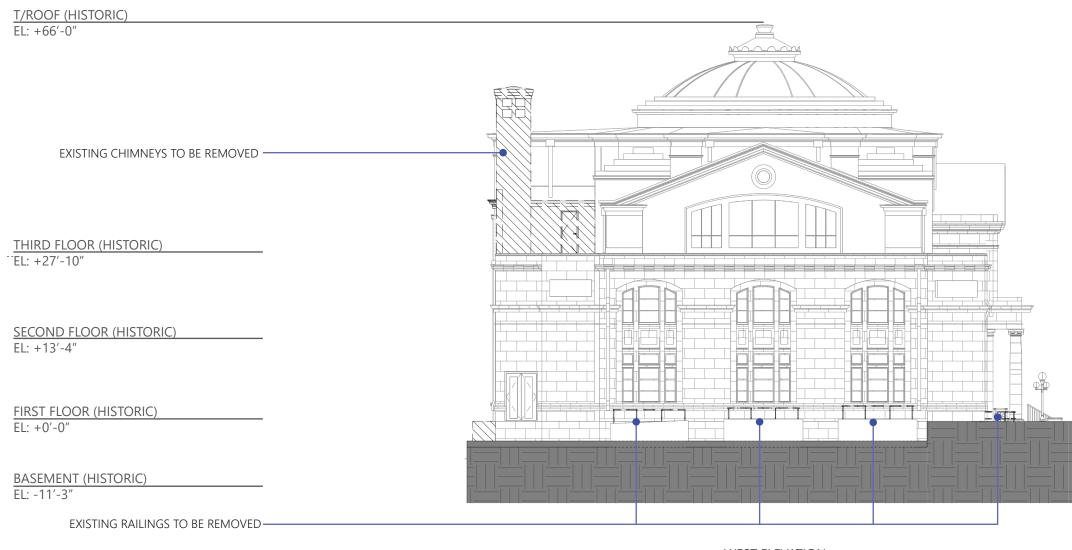
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

\*Approved\* City of Portland Bureau of Development Services

Planner January T

\*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

C.016 LU 22-204531 HRM



WEST ELEVATION

### DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"





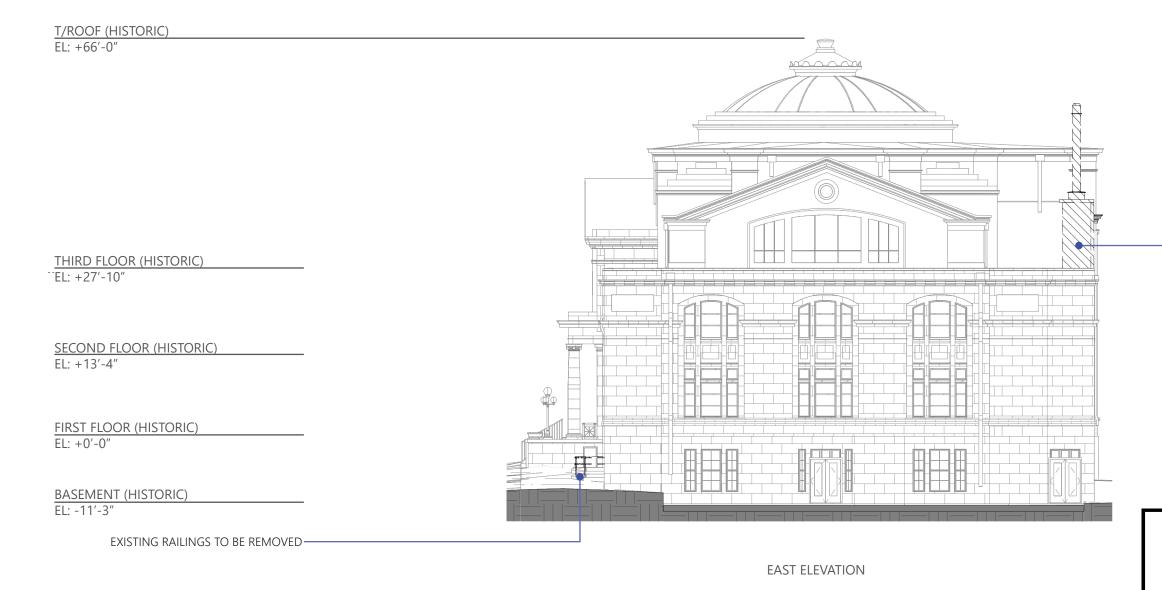
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

\*Approved\*
City of Portland
Bureau of Development Services

Date 03-24-2023

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C.017 LU 22-204531 HRM



EXISTING CHIMNEYS TO BE REMOVED

\*Approved\* City of Portland Bureau of Development Services

Planner\_

03-24-2023

Date 03-24-2023
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# DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)

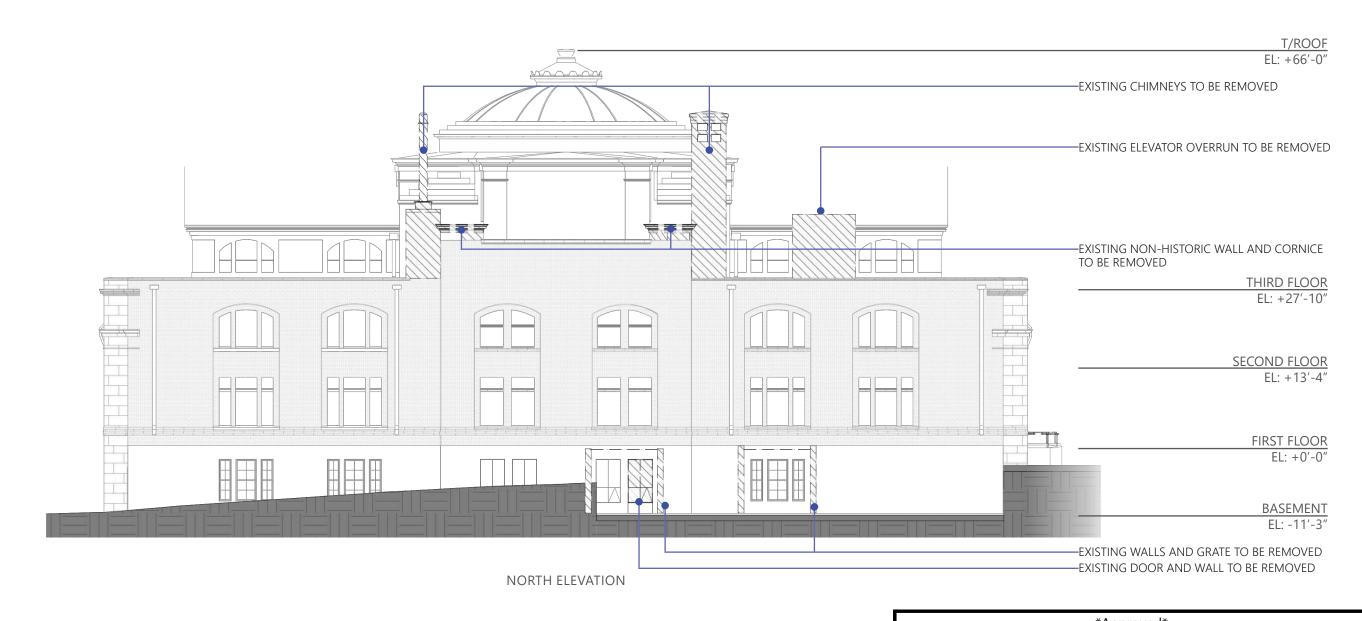
| SCALE: 1/16" = 1'-0"





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

C.018 LU 22-204531 HRM



# DEMOLITION ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"

C.19



**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

\*Approved\*
City of Portland
Bureau of Development Services

Planner function of the Date 03-24-2023

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C.019 LU 22-204531 HRM



**EXISTING NORTHEAST** CHIMNEY - PROPOSED TO BE REMOVED AND REPLACED WITH STAIR ADDITION



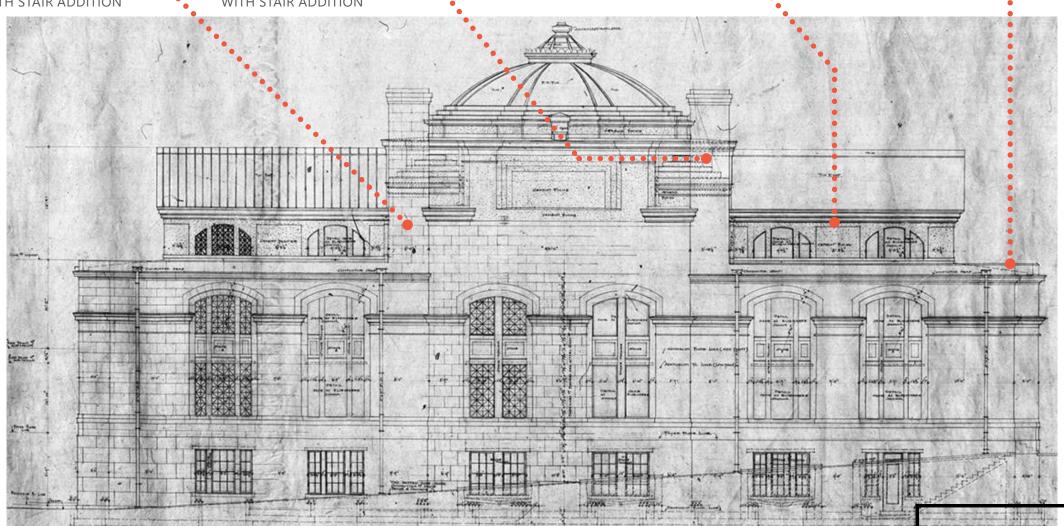
**EXISTING NORTHWEST** CHIMNEY - PROPOSED TO BE REMOVED AND REPLACED WITH STAIR ADDITION



EXISTING, NON-HISTORIC, • ELEVATOR OVERRUN TO BE DEMOLISHED



12" HIGH EXISTING PARAPET CONDITION



ORIGINAL NORTH ELEVATION DRAWING

EXISTING CONDITIONS (1819 NW EVERETT ST.)

| SCALE: NTS







1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

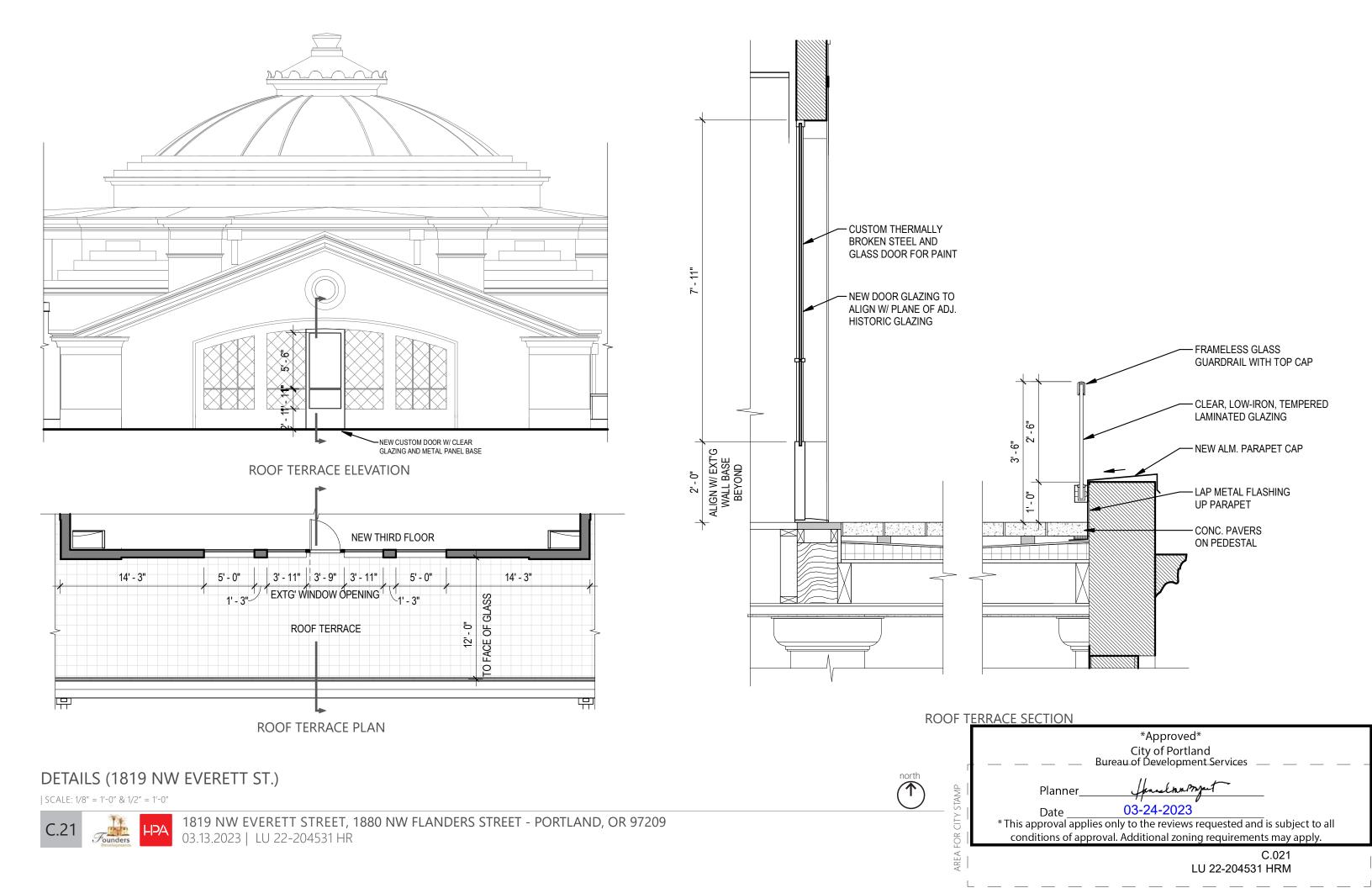
\*Approved\* City of Portland Bureau of Development Services

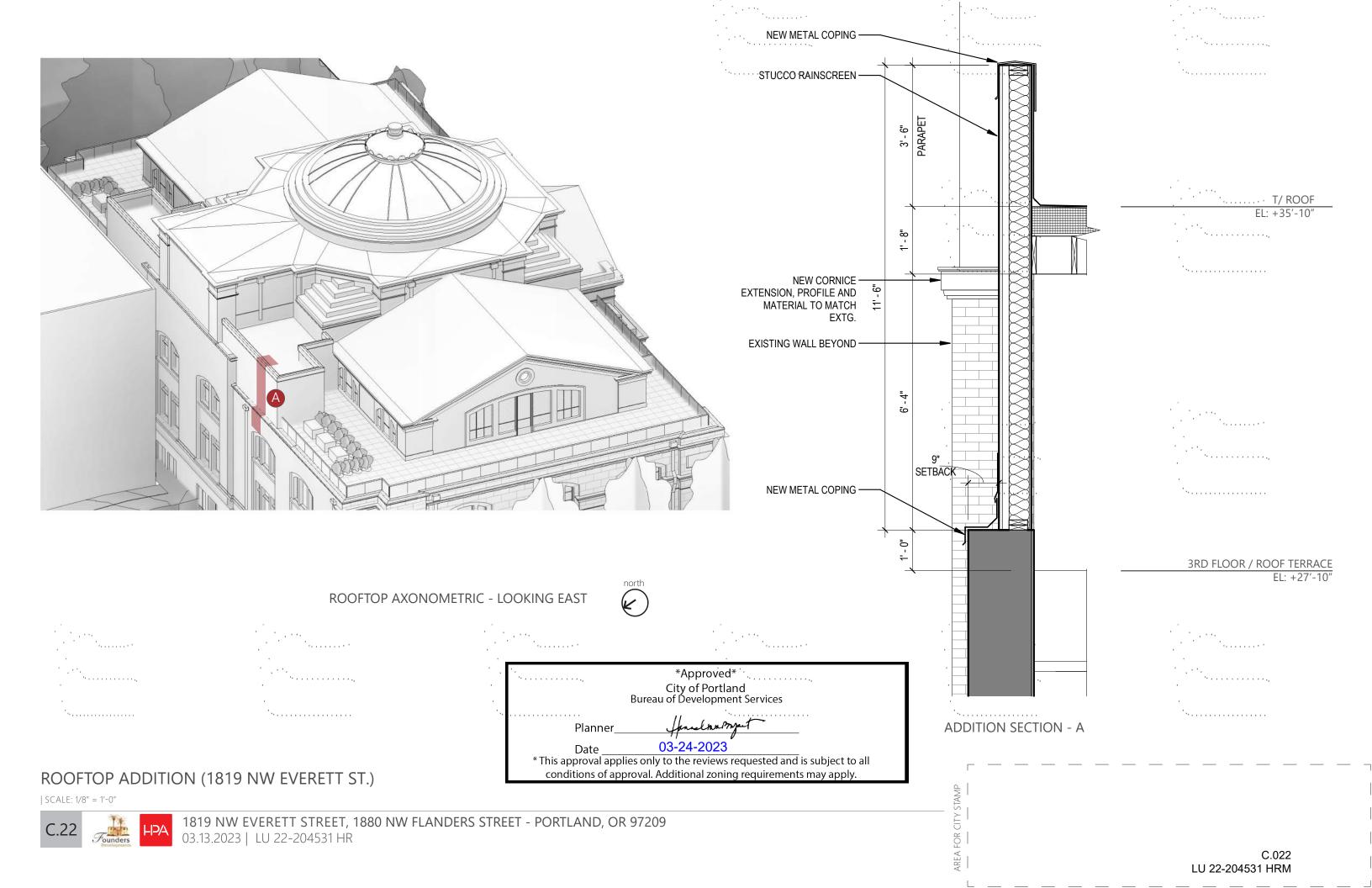
Handland myset

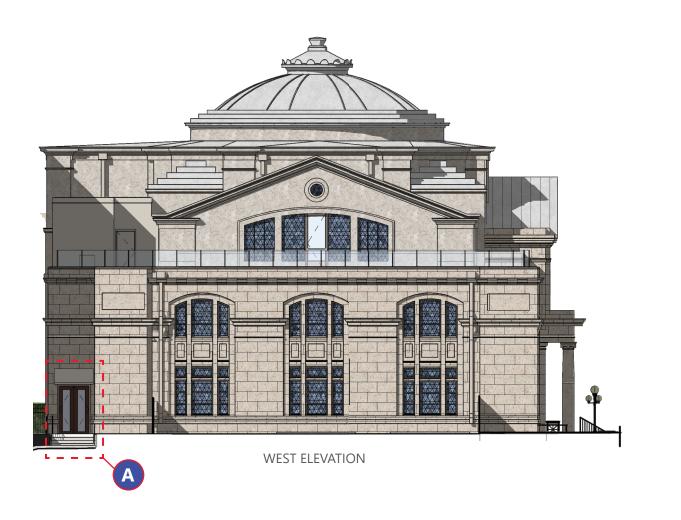
Planner

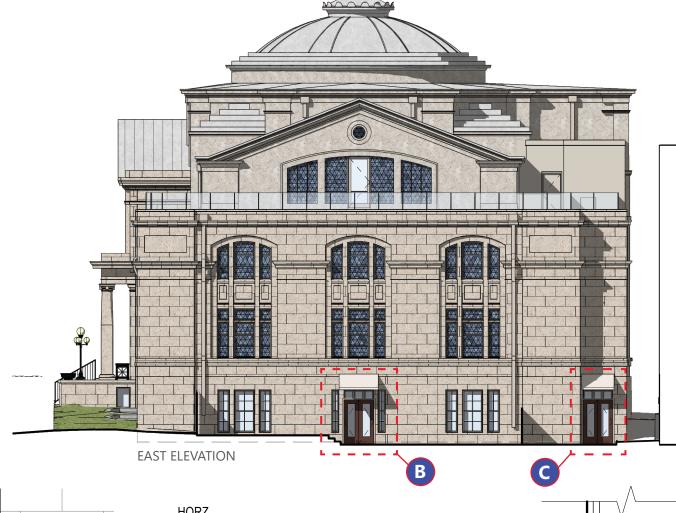
03-24-2023 \*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

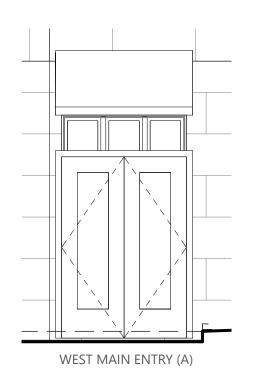
> C.020 LU 22-204531 HRM

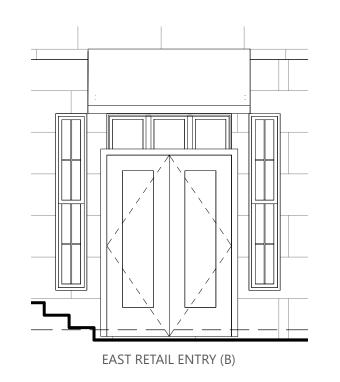


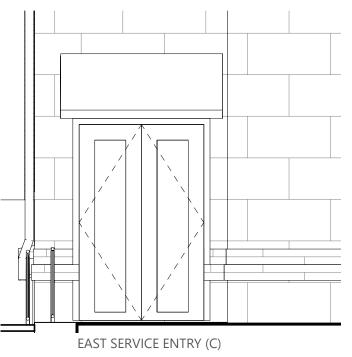


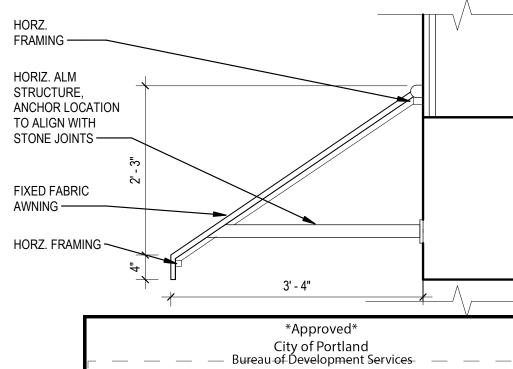












Planner\_

## WINDOW AWNING DETAILS (1819 NW EVERETT ST.)

| SCALE: NTS





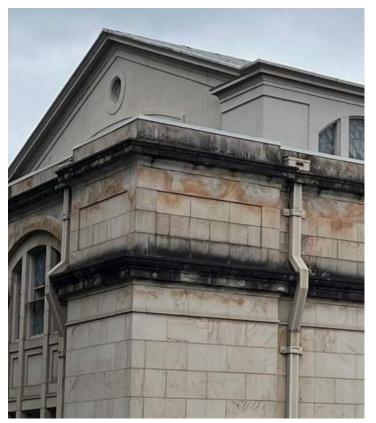


**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

Date 03-24-2023
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C.023

LU 22-204531 HRM







NORTH WINDOW BAY/FACADE MATERIAL



**EXISTING METAL SHEET** CLADDING OVER HISTORIC **CEMENTITIOUS FINISH** 



WINDOW CONDITION EXAMPLE



ENLARGED WINDOW DETAIL



**EXISTING STONE TO REMAIN** 





STAIR TOWER ADDITIONS - STUCCO AND METAL COPING

### EXTERIOR MATERIALS (1819 NW EVERETT ST.)

| SCALE: NTS







1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

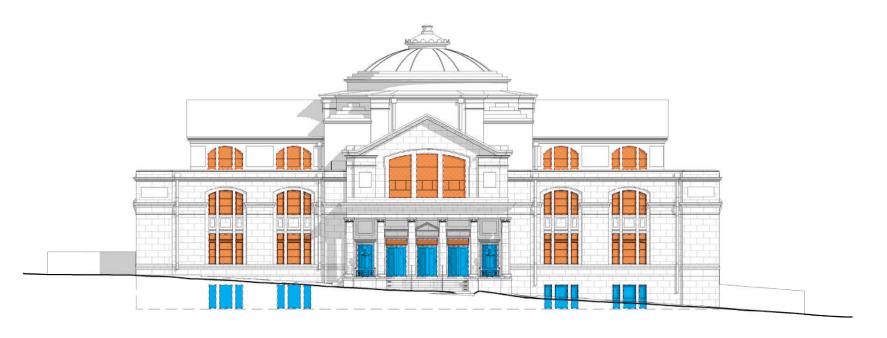
\*Approved\* City of Portland Bureau of Development-Services

Planner\_

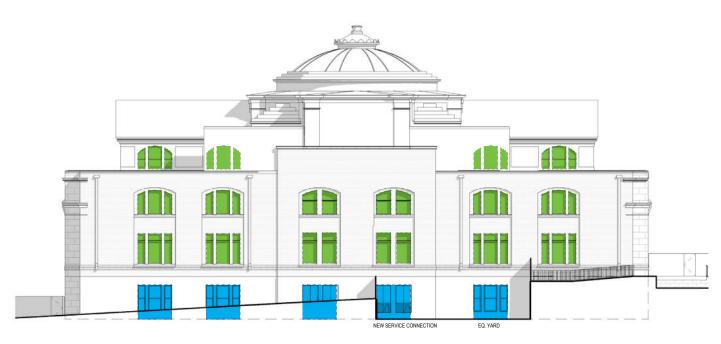
03-24-2023

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> C.024 LU 22-204531 HRM



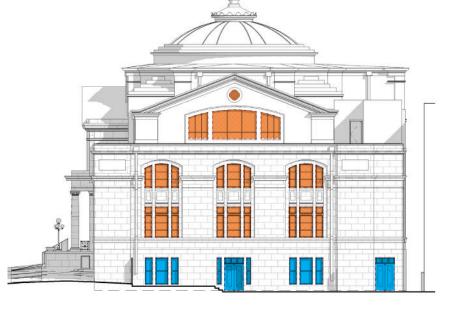
#### SOUTH ELEVATION



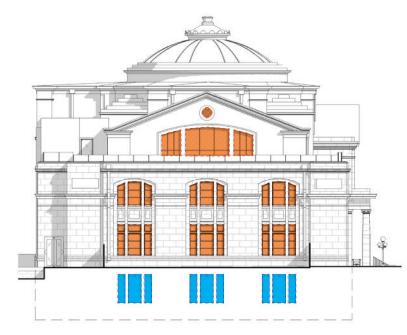
NORTH ELEVATION



EXISTING CLEAR GLASS AND WOOD FRAME TO BE REPAIRED AND/OR REPLACED IN KIND TO MATCH HISTORIC CONDITION



#### EAST ELEVATION



WEST ELEVATION

EXISTING GLASS TO BE REPLACED WITH SEMI-TRANSPARENT PRIVACY GLASS WITHOUT GEOMETRIC PATTERN. FRAME AND MULLIONS TO BE REPLACED WITH NEW IN HISTORIC CONFIGURATION

# WINDOW REPAIR AND REPLACEMENT SCOPE (1819 NW EVERETT ST.)

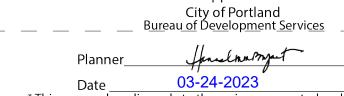
| SCALE: NTS







1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR



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\*Approved\*

C.025 LU 22-204531 HRM

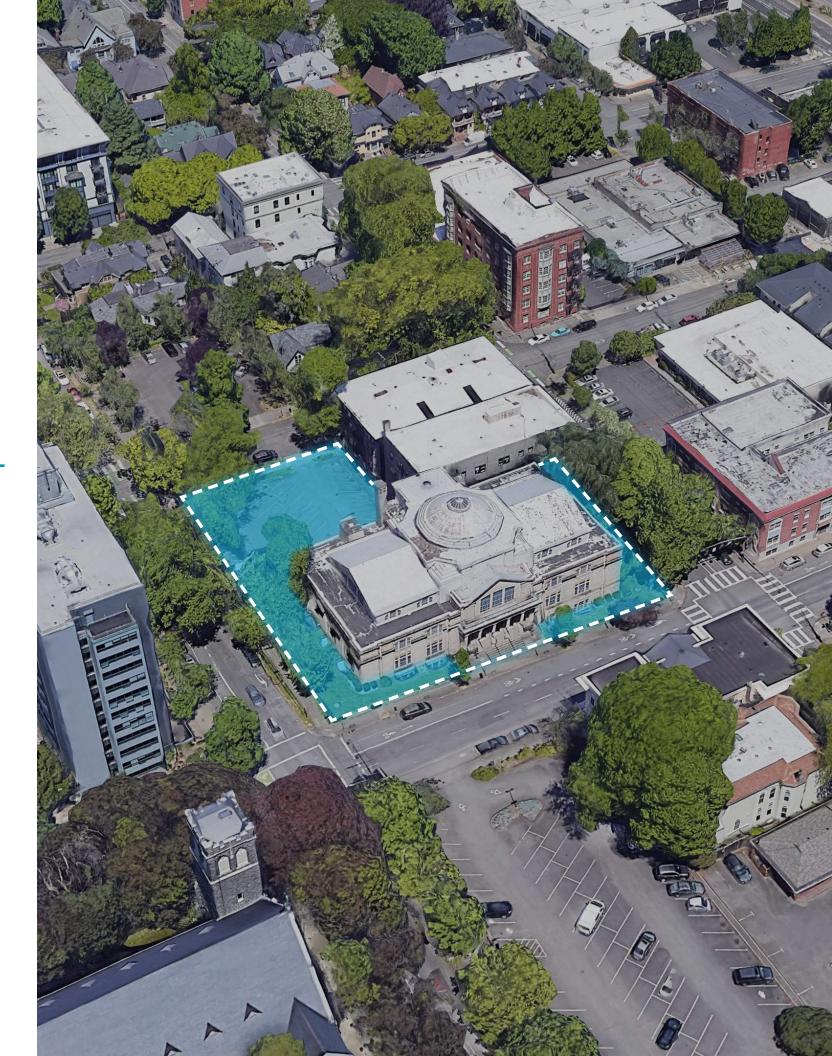
# TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

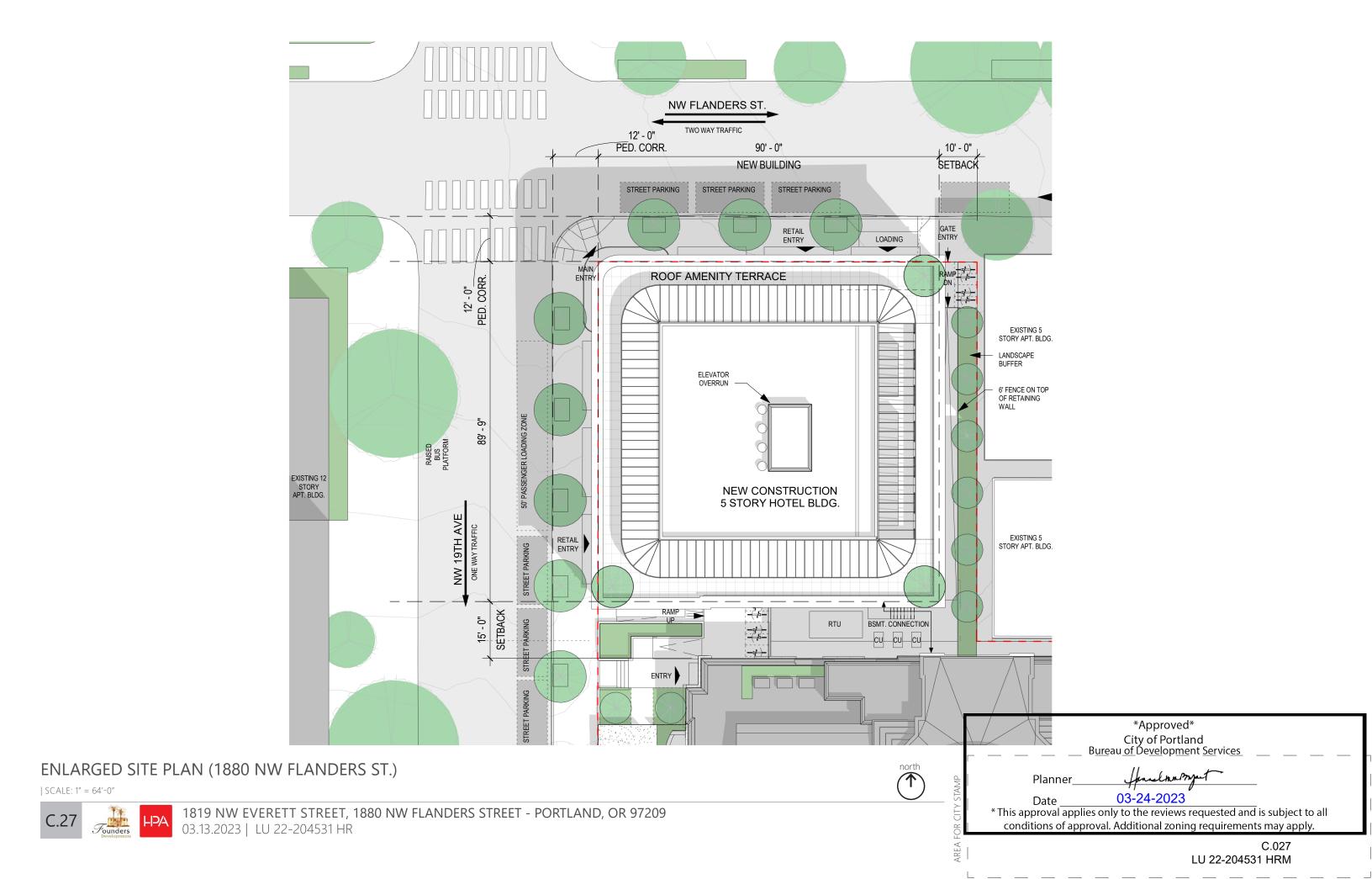
1880 NW FLANDERS STREET

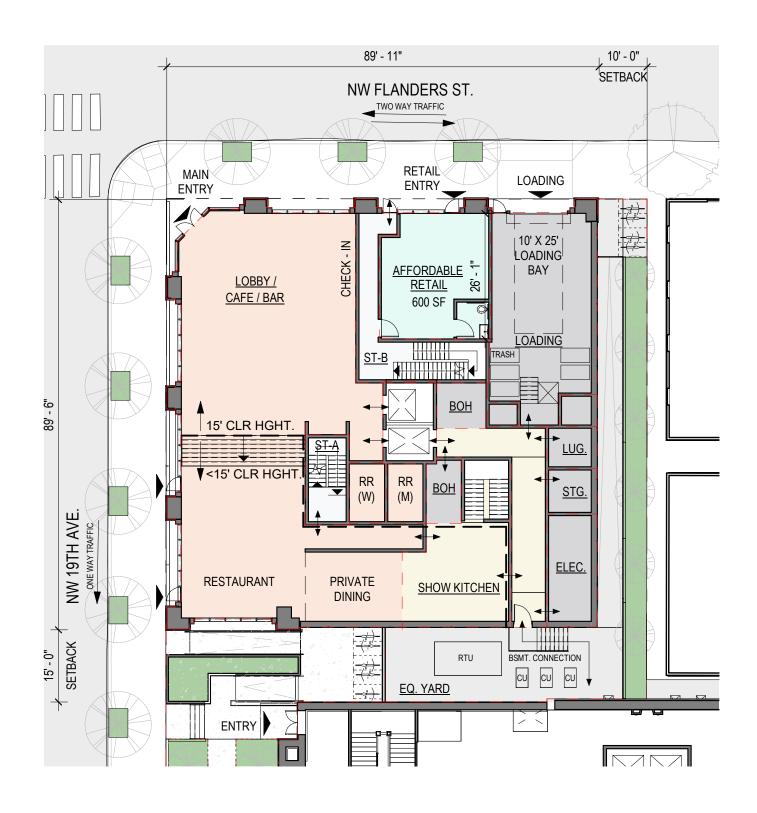
New Construction - Drawings

March 13th, 2023









FIRST FLOOR PLAN

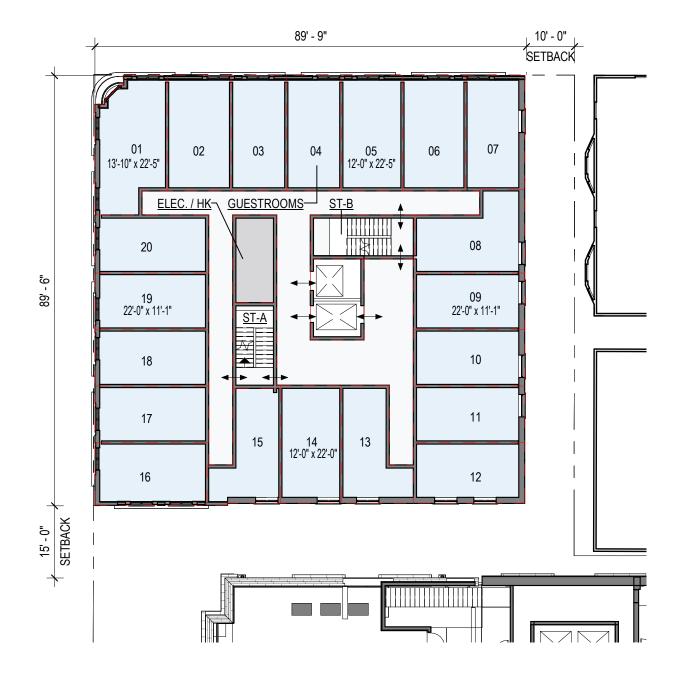
### FLOOR PLANS (1880 NW FLANDERS ST.)

| SCALE: 1" = 20'-0"





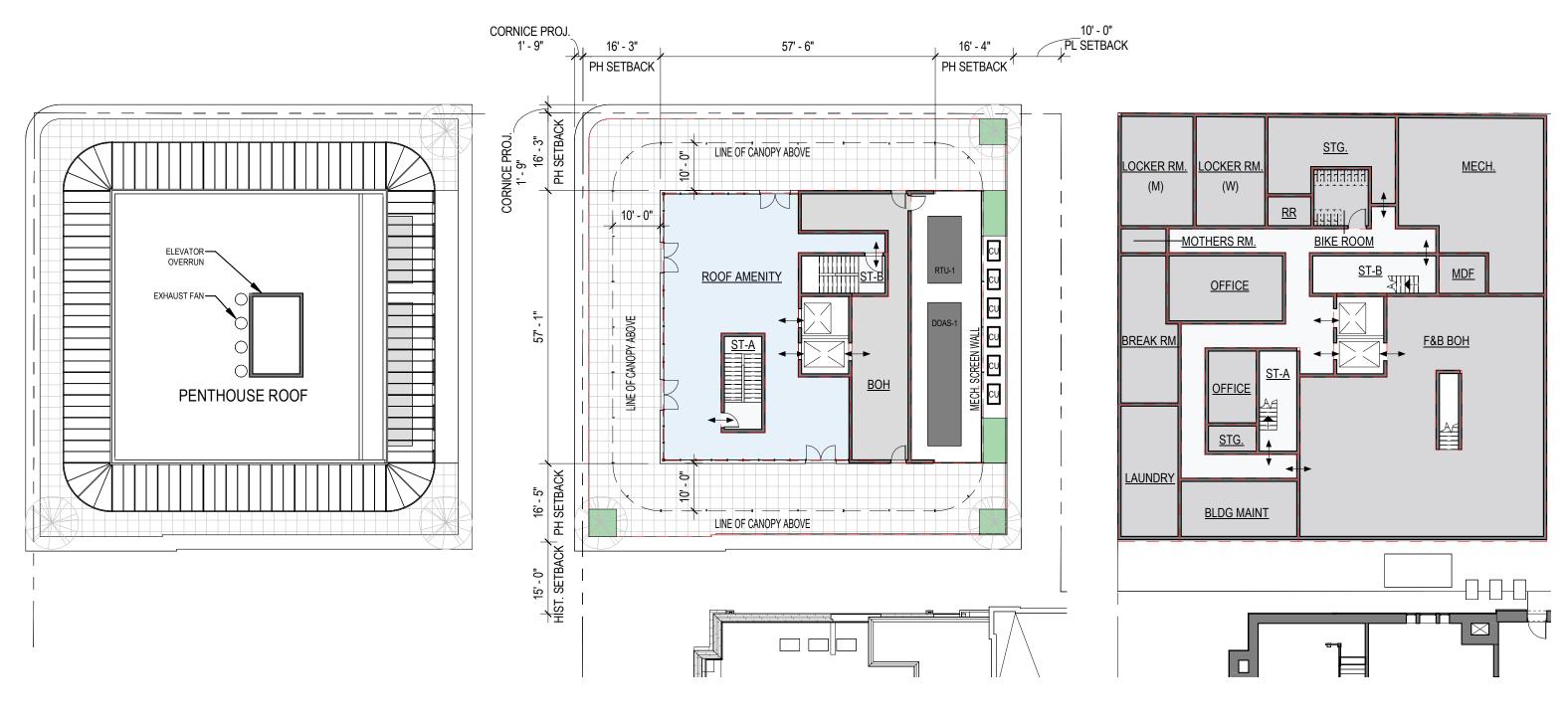
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR





north





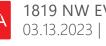
PENTHOUSE ROOF PLAN **ROOF PLAN BASEMENT PLAN** 

### FLOOR PLANS (1880 NW FLANDERS ST.)

| SCALE: 1" = 20'-0"







1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR



LU 22-204531 HRM

north

3-COAT STUCCO RAINSCREEN

2 ALM. WINDOW, LIGHT BRONZE FINISH

ALM. OPERABLE STOREFRONT, LIGHT BRONZE FINISH, W/ **DECORATIVE GLASS TRANSOM** 



5 SOLID RED OAK WOOD PANELING FOR PAINT

3CM GRANITE STONE VENEER ALM. CANOPY, LIGHT BRONZE FINISH, W/ CLEAR GLASS ROOF

ALM. CLADDING, LIGHT **BRONZE FINISH** 



10 ALM. COPING CAP, PAINT TO MATCH STUCCO

ALM. DOOR, PAINT TO MATCH WALL

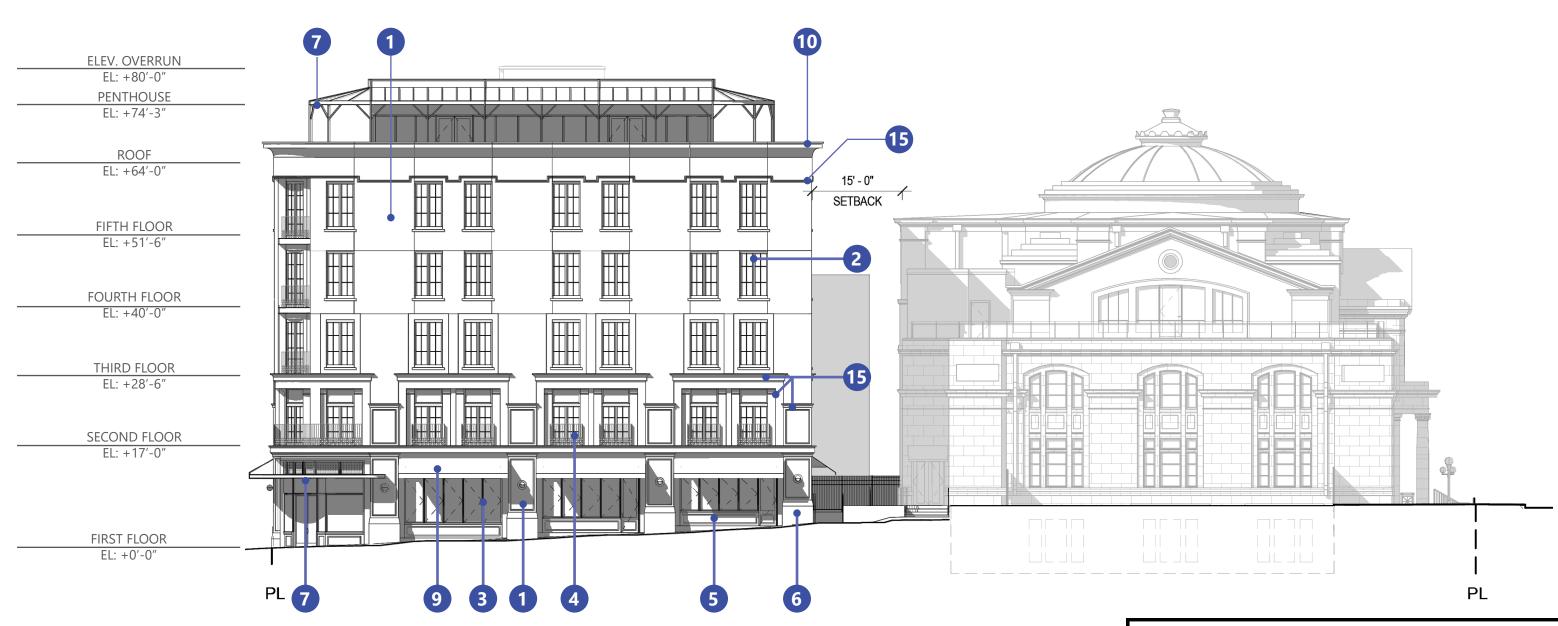
ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS

DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH

CONCRETE W/ ELASTOMERIC 14 PAINT , COLOR TO MATCH STUCCO

DECORATIVE STUCCO MOULDING TRIM

ALM. STOREFRONT, 16 PAINT TO MATCH STUCCO



WEST ELEVATION

BLACK AND WHITE ELEVATIONS (1880 NW FLANDERS ST.)

| SCALE: 1/16" = 1'-0"





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

\*Approved\* \_\_\_\_City of Portland Bureau of Development Services

Charles marsgart Planner 03-24-2023

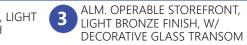
AREA FOR CITY STAMP

Date \* This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

LU 22-204531 HRM

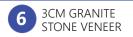


2 ALM. WINDOW, LIGHT BRONZE FINISH















C.031

LU 22-204531 HRM

10 ALM. COPING CAP, PAINT TO MATCH STUCCO

11 ALM. DOOR, PAINT TO MATCH WALL

ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS

DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH

CONCRETE W/ ELASTOMERIC PAINT , COLOR TO MATCH STUCCO

DECORATIVE STUCCO MOULDING TRIM

ALM. STOREFRONT,
PAINT TO MATCH
STUCCO

10 ELEV. OVERRUN EL: +80'-0" PENTHOUSE EL: +74'-3" **ROOF** EL: +64'-0" 13 FIFTH FLOOR EL: +51'-6' FOURTH FLOOR EL: +40'-0' 2 THIRD FLOOR 15 EL: +28'-6" 10' - 0" **SETBACK** SECOND FLOOR EL: +17'-0' PLPL5 3 4 6 9 \*Approved\* **NORTH ELEVATION** City of Portland Bureau of Development Services BLACK AND WHITE ELEVATIONS (1880 NW FLANDERS ST.) Janual marrogent **Planner** | SCALE: 1/16" = 1'-0" 03-24-2023 1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 \* This approval applies only to the reviews requested and is subject to all 03.13.2023 | LU 22-204531 HR conditions of approval. Additional zoning requirements may apply.

3-COAT STUCCO RAINSCREEN

ALM. WINDOW, LIGHT BRONZE FINISH

ALM. OPERABLE STOREFRONT, LIGHT BRONZE FINISH, W/ **DECORATIVE GLASS TRANSOM** 





3CM GRANITE STONE VENEER ALM. CANOPY, LIGHT BRONZE FINISH, W/ CLEAR GLASS ROOF ALM. STOREFRONT,

16 PAINT TO MATCH

STUCCO





10 ALM. COPING CAP, PAINT TO MATCH STUCCO

ALM. DOOR, PAINT TO MATCH WALL

ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS

DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH

CONCRETE W/ ELASTOMERIC 14 PAINT , COLOR TO MATCH STUCCO

DECORATIVE STUCCO MOULDING TRIM

ELEV. OVERRUN EL: +80'-0" **PENTHOUSE** EL: +74'-3"

> **ROOF** EL: +64'-0"

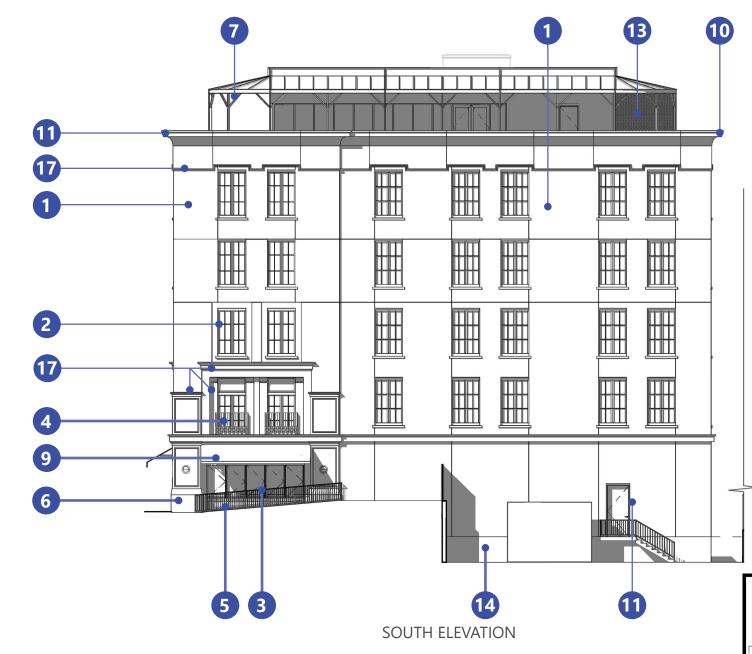
FIFTH FLOOR EL: +51'-6"

FOURTH FLOOR EL: +40'-0"

THIRD FLOOR EL: +28'-6"

SECOND FLOOR EL: +17'-0"

> FIRST FLOOR EL: +0'-0"



BLACK AND WHITE ELEVATIONS (1880 NW FLANDERS ST.)

| SCALE: 1/16" = 1'-0"



1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

\*Approved\* City of Portland Bureau of Development Services

Planner

03-24-2023 Date

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> C.032 LU 22-204531 HRM

1 3-COAT STUCCO RAINSCREEN

2 ALM. WINDOW, LIGHT BRONZE FINISH ALM. OPERABLE STOREFRONT, LIGHT BRONZE FINISH, W/ DECORATIVE GLASS TRANSOM

4 CUSTOM ALM. GUARDRAIL, LIGHT BRONZE FINISH 5 SOLID RED OAK WOOD PANELING FOR PAINT

6 3CM GRANITE STONE VENEER 7 ALM. CANOPY, LIGHT BRONZE FINISH, W/ CLEAR GLASS ROOF 8 ALM. CLADDING, LIGHT BRONZE FINISH



10 ALM. COPING CAP, PAINT TO MATCH STUCCO

ALM. DOOR, PAINT TO MATCH WALL

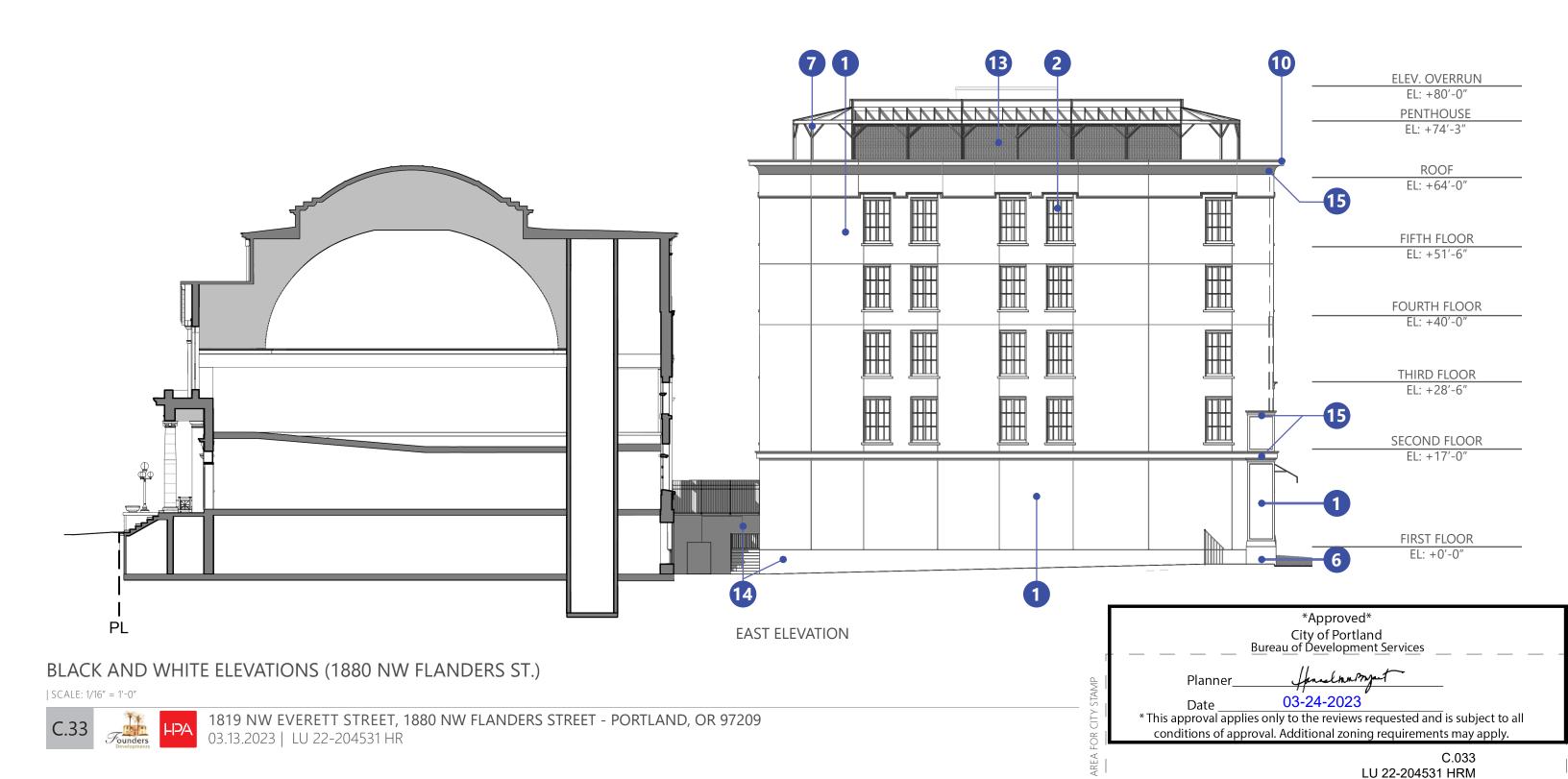
ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS

DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH

CONCRETE W/ ELASTOMERIC PAINT , COLOR TO MATCH STUCCO

DECORATIVE STUCCO MOULDING TRIM

ALM. STOREFRONT,
PAINT TO MATCH
STUCCO



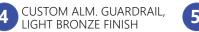
C.034

LU 22-204531 HRM



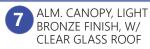
2 ALM. WINDOW, LIGHT BRONZE FINISH















10 ALM. COPING CAP, PAINT TO MATCH STUCCO

ALM. DOOR, PAINT TO MATCH WALL

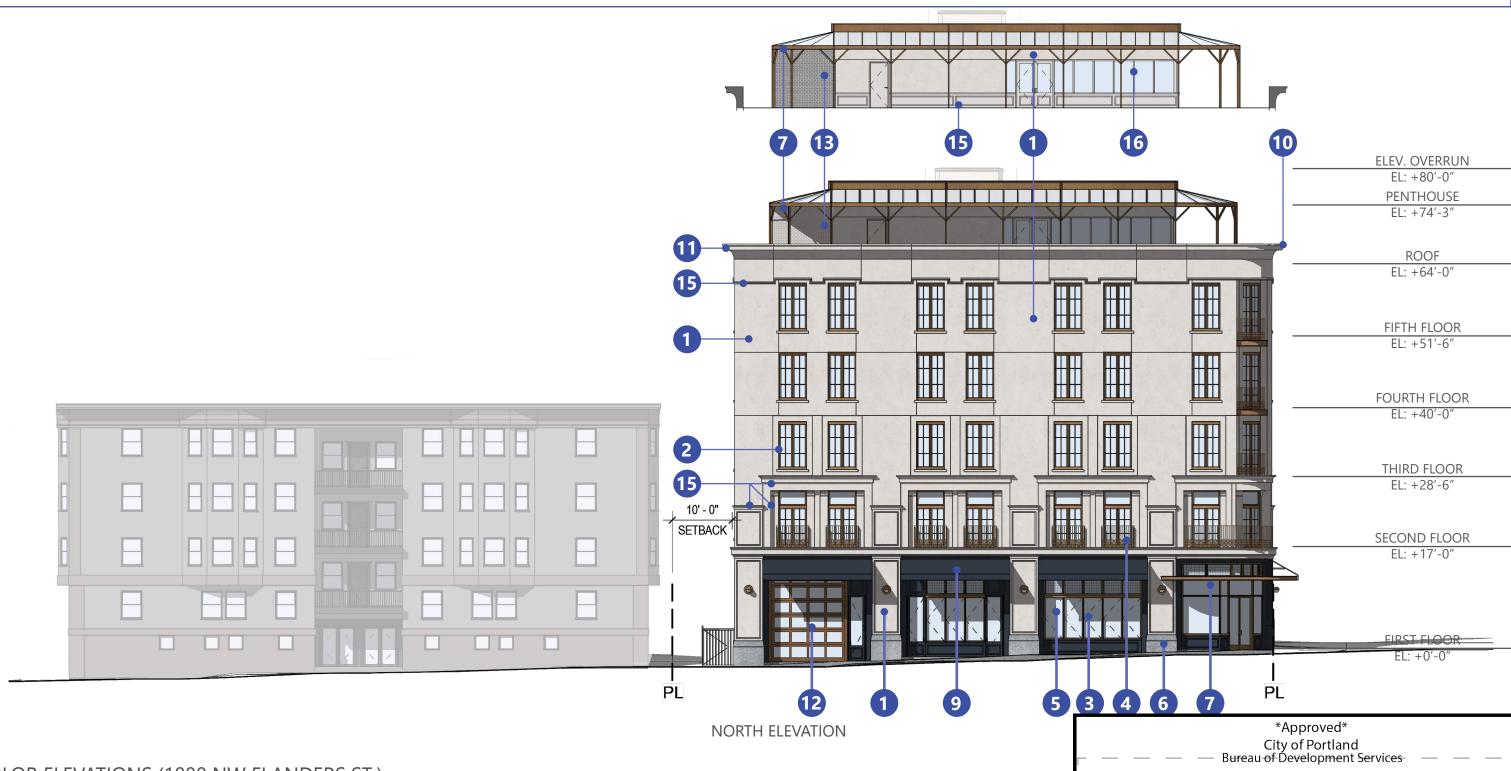
ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS

DECORATIVE ALM. PANEL
MECH. SCREEN WALL, MATTE
PAINT FINISH TO MATCH

CONCRETE W/ ELASTOMERIC PAINT , COLOR TO MATCH STUCCO

DECORATIVE STUCCO MOULDING TRIM

ALM. STOREFRONT,
PAINT TO MATCH
STUCCO



COLOR ELEVATIONS (1880 NW FLANDERS ST.)

| SCALE: 1/16" = 1'-0"





**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

Planner finelnurry 1

Date 03-24-2023

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C.035 LU 22-204531 HRM



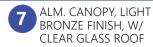
2 ALM. WINDOW, LIGHT BRONZE FINISH















10 ALM. COPING CAP, PAINT TO MATCH STUCCO

ALM. DOOR, PAINT TO MATCH WALL

ALM. OVERHEAD COILING DOOR WITH LIGHT BRONZE FINISH AND TRANSLUCENT GLAZING PANELS

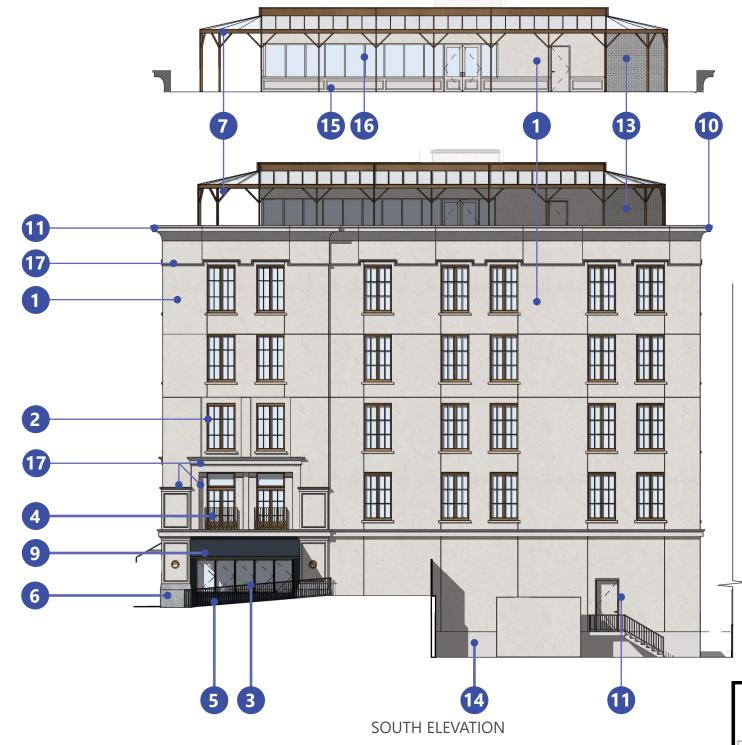
DECORATIVE ALM. PANEL
MECH. SCREEN WALL, MATTE
PAINT FINISH TO MATCH

CONCRETE W/ ELASTOMERIC PAINT , COLOR TO MATCH STUCCO

DECORATIVE STUCCO MOULDING TRIM

ALM. STOREFRONT,
PAINT TO MATCH
STUCCO

ELEV. OVERRUN EL: +80'-0" **PENTHOUSE** EL: +74'-3" **ROOF** EL: +64'-0" FIFTH FLOOR EL: +51'-6" FOURTH FLOOR EL: +40'-0" THIRD FLOOR EL: +28'-6" SECOND FLOOR EL: +17'-0" FIRST FLOOR EL: +0'-0"



COLOR ELEVATIONS (1880 NW FLANDERS ST.)

| SCALE: 1/16" = 1'-0"

C.36



**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

\*Approved\*
City of Portland
Bureau of Development Services

Charlen Propert

Planner\_\_

Date 03-24-2023

\* This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

C.036 LU 22-204531 HRM



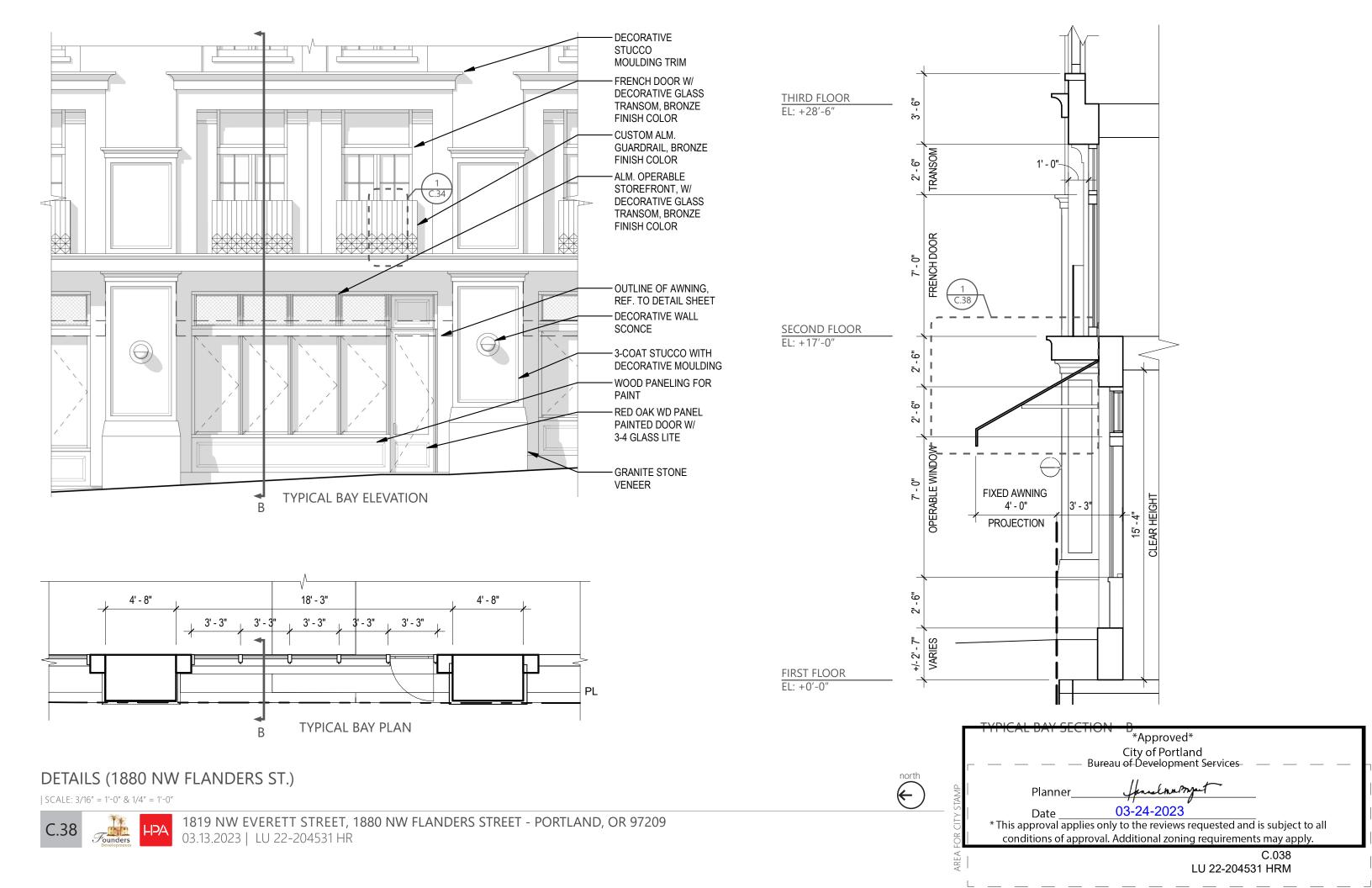


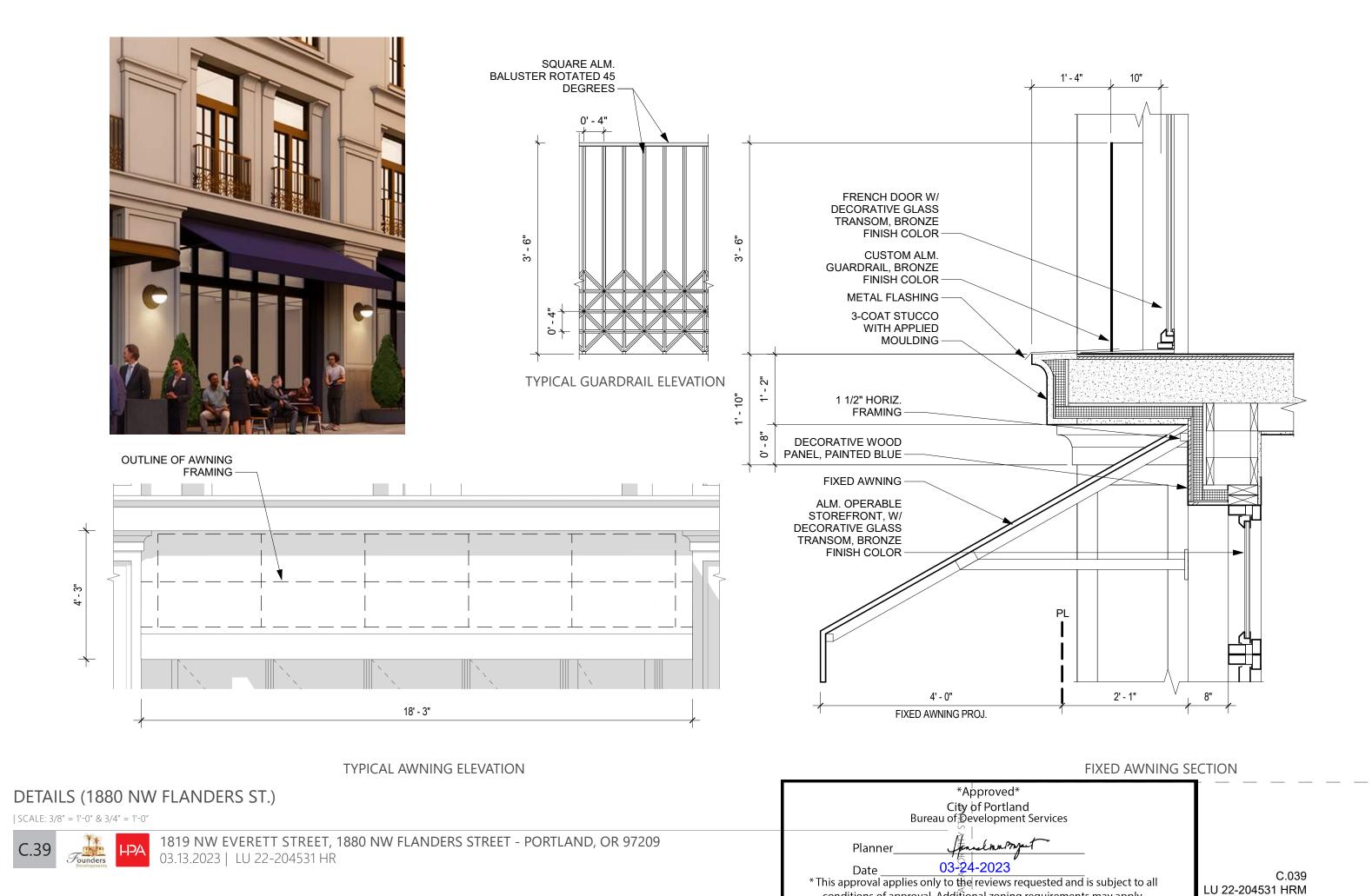


C.037 LU 22-204531 HRM

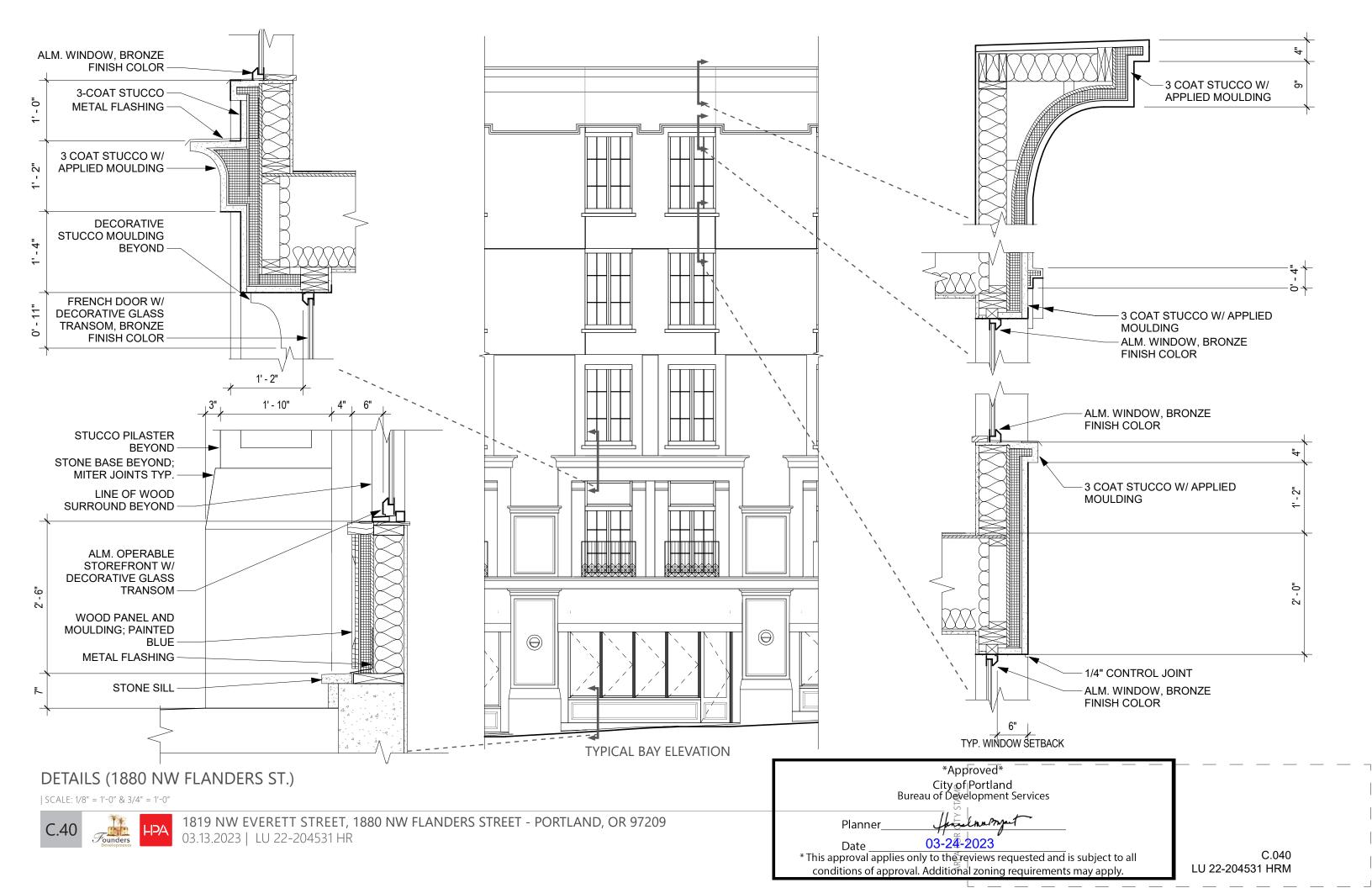
\* This approval applies only to the reviews requested and is subject to all

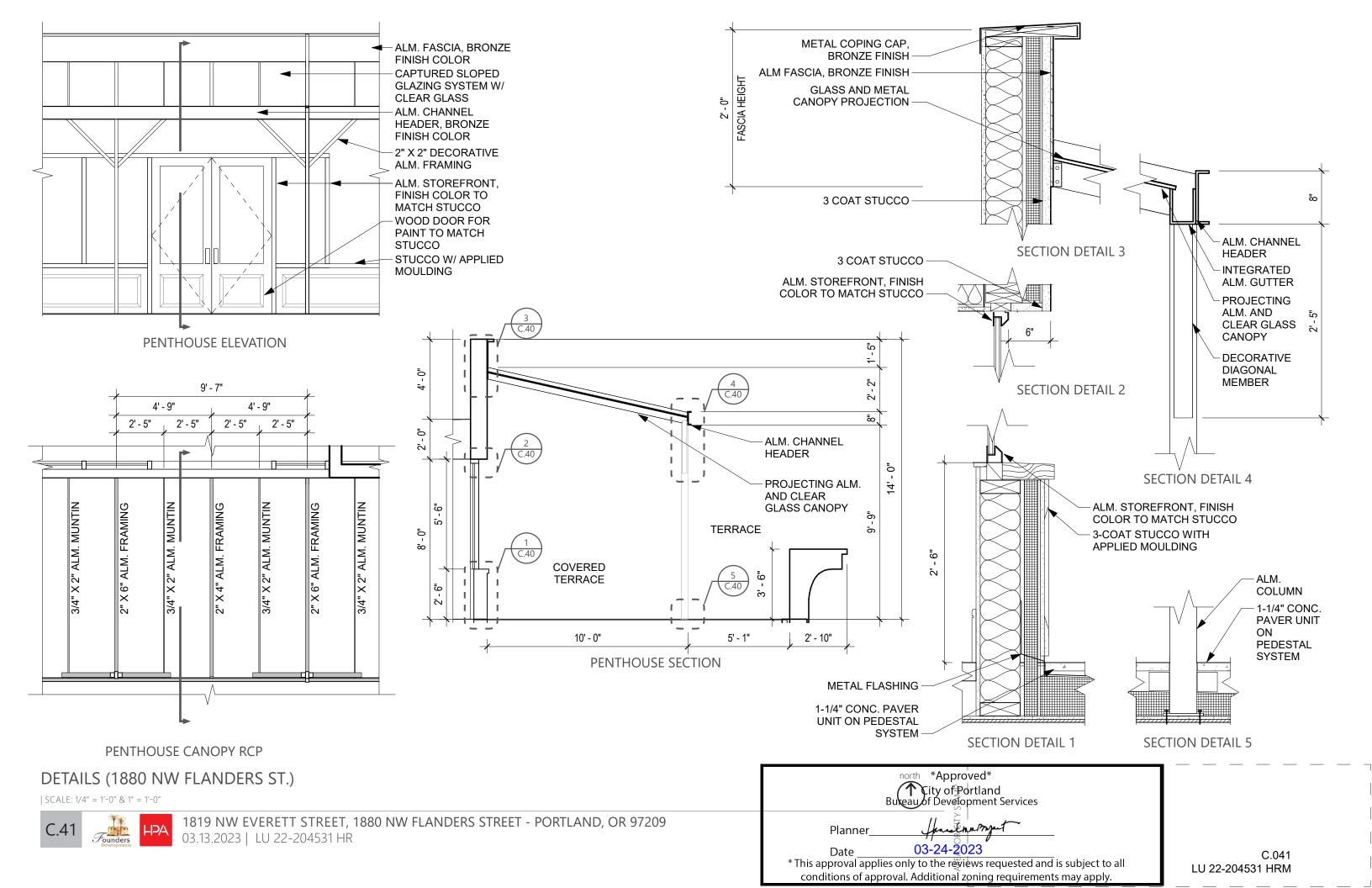
conditions of approval. Additional zoning requirements may apply.



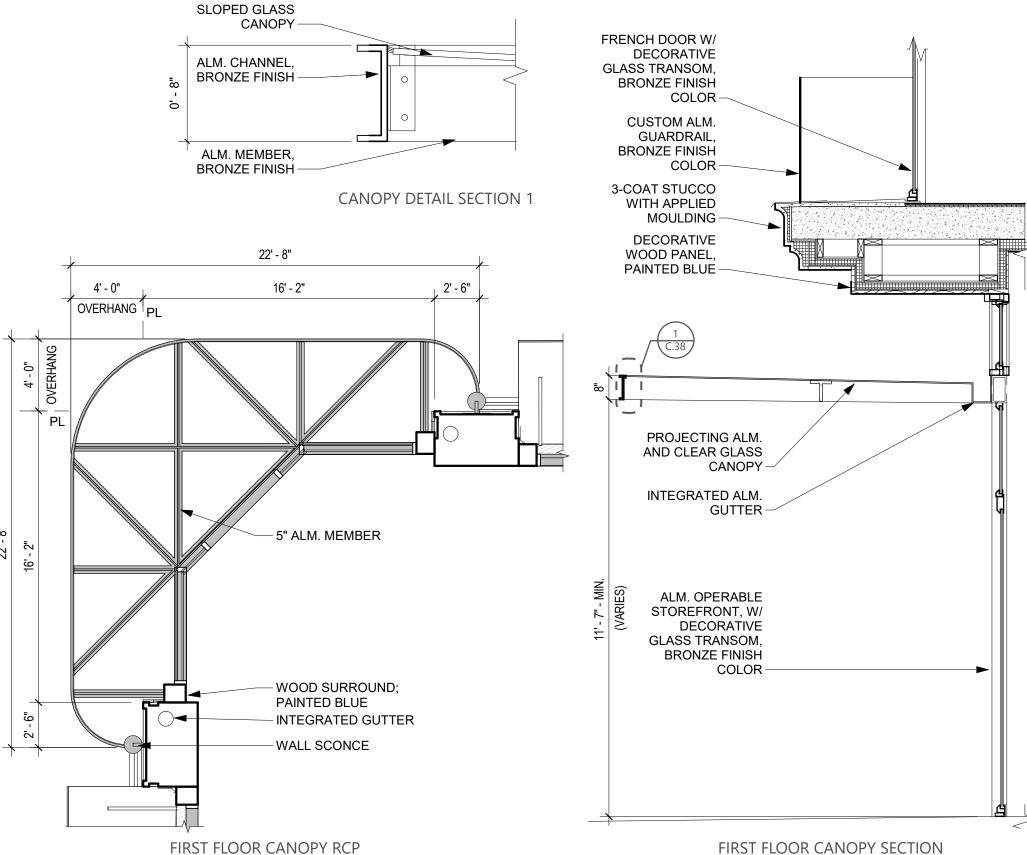


conditions of approval. Additional zoning requirements may apply.









## DETAILS (1880 NW FLANDERS ST.)

| SCALE: 3/16" = 1'-0", 3/8" = 1'-0", & 1-1/2" = 1'-0"

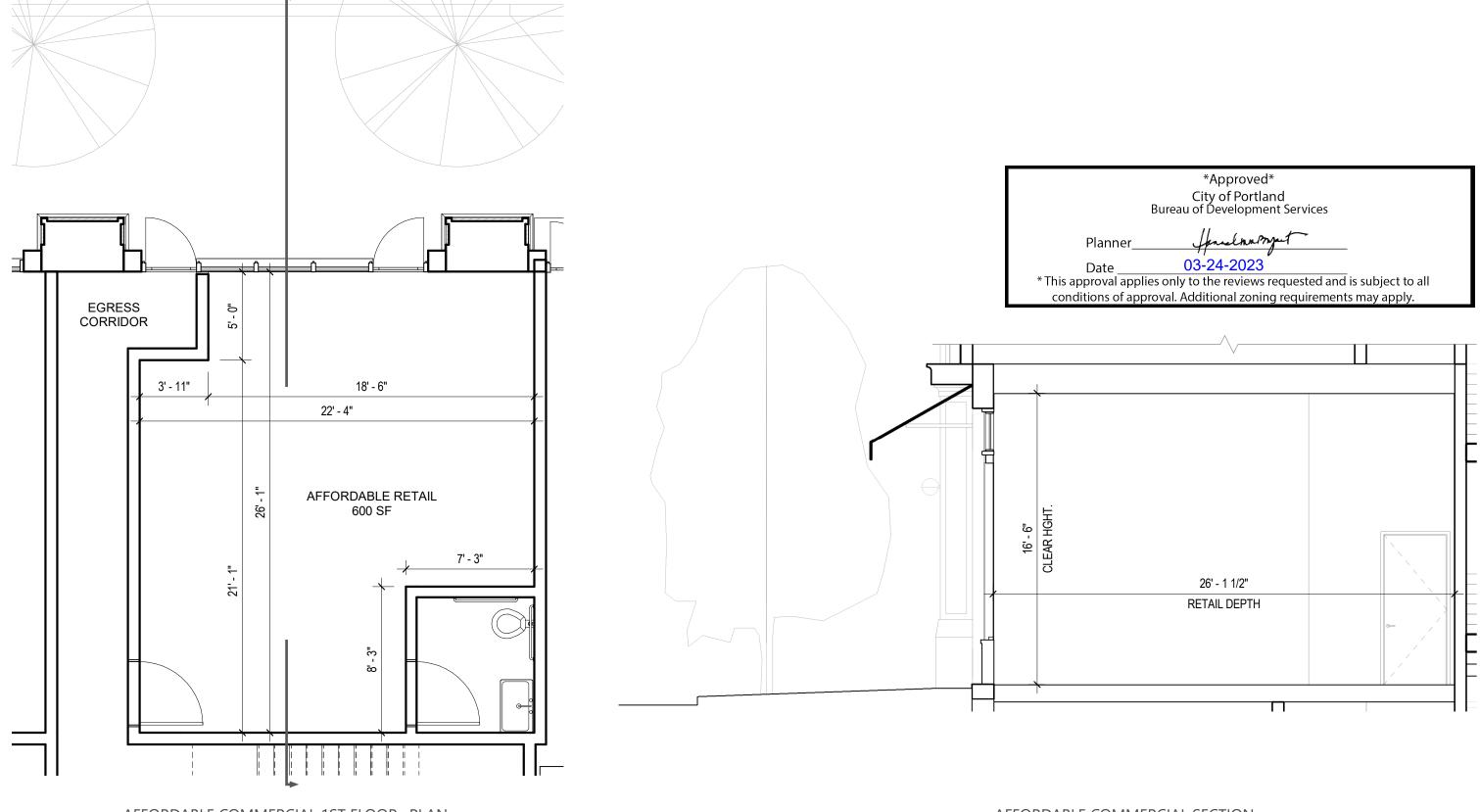




**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR



C.042 LU 22-204531 HRM



AFFORDABLE COMMERCIAL 1ST FLOOR PLAN

AFFORDABLE COMMERCIAL SECTION

## AFFORDABLE COMMERCIAL SPACE

| SCALE: 3/16" = 1'-0"











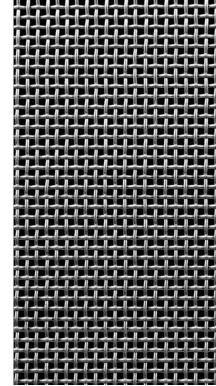




WOOD STOREFRONT SURROUND



CUSTOM RAILING EXAMPLE-LIGHT BRONZE FINISH



MESH SCREEN



EXISTING HISTORIC STONE FACADE



STUCCO RAINSCREEN, SMOOTH FINISH



DECORATIVE GLASS



CHARCOAL GRANITE STONE VENER -FLAMED FINISH



METAL - LIGHT BRONZE

# FACADE MATERIALS (1880 NW FLANDERS ST.)

| SCALE: NTS







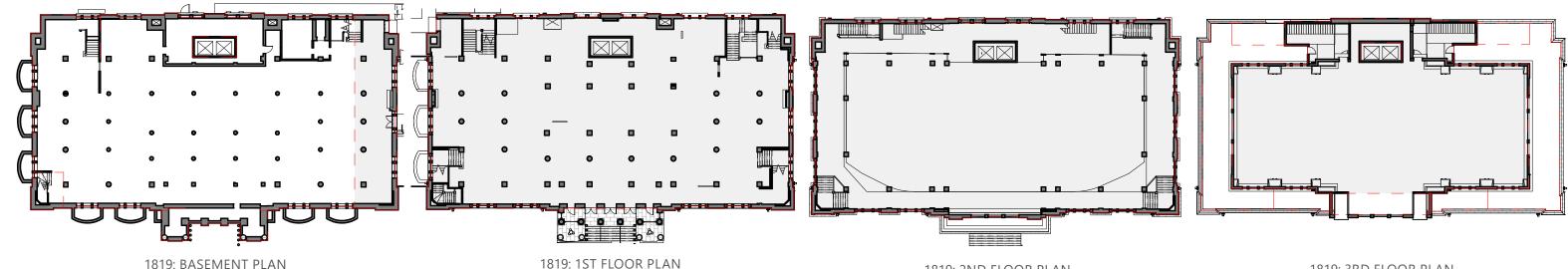
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

\*Approved\* City of Portland Bureau of Development Services

Planner January

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C.044 LU 22-204531 HRM



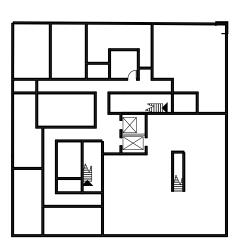
1819: BASEMENT PLAN 1,116SF

\*AREA TO WEST OF LINE, +4'-0" BELOW GRADE

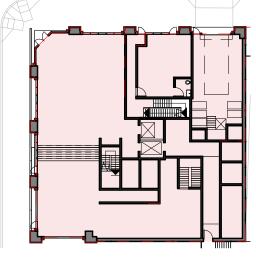


1819: 2ND FLOOR PLAN 10,548SF

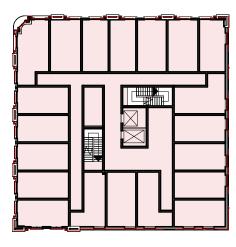
1819: 3RD FLOOR PLAN 6,520SF



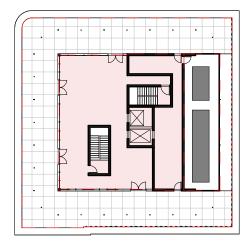




1880: 1ST FLOOR PLAN 7,580 SF



1880: 2ND - 5TH FLOOR PLAN 7,780 SF



1880: ROOF PLAN 2,700 SF

#### **AREA SUMMARIES**

1819 NW FLANDERS (HISTORIC)			
FAR SUMMARY: APPLICABLE AREA			
BASEMENT 1,401 SF			
1ST FLOOR	12,132 SF		
2ND FLOOR	12,205 SF		
3RD FLOOR	7,692 SF		
TOTAL	33,431 SF		
101AL 33,431 3F			

1880 NW FLANDERS (HOTEL)				
FAR SUMMARY: APPLICABLE AREA				
BASEMENT	N/A			
1ST FLOOR	7,580 SF			
2ND FLOOR	7,780 SF			
3RD FLOOR	7,780 SF			
4TH FLOOR	7,780 SF			
5TH FLOOR	7,780 SF			
ROOF	2,700 SF			
TOTAL	41,400 SF			

## **FAR GRAND TOTALS**

29,997 SF (SITE AREA) X 2.5 = 74,992.5 SF (ALLOWABLE FAR)

TOTAL SF: (33,431 + 41,400) = 74,831 SF (TOTAL PROPOSED FAR)

74,831 SF < 74,992 SF

# FAR SUMMARY

| SCALE: 1" = 40'-0"





**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR



C.45

# **NEW BUILDING** NW FLANDERS ST. HOTEL TWO WAY TRAFFIC ENTRY EXISTING 5 STORY APT. BLDG. NW 18TH AVE NEW CONSTRUCTION EXISTING 5 STORY APT. BLDG. **\$**\$\$\$\$\$\$ ENTRY ) SERVICE ENTR OOR EXISTING 2 - STORY HISTORIC LANDMARK RETAIL N NW 19TH AVE BIKE PARKING BIKE PARKING 153' - 7" 12'-6 PED. C 23' - 6" 22' - 11" SETBACK HISTORIC BUILDING SETBACK 200' - 0" 12' - 0" 12' - 0' PED. CORR. NW EVERETT ST. PED. CORR TWO WAY TRAFFIC

SITE PLAN - BIKE PARKING: LOCATIONS & DISTANCE TO ENTRIES | SCALE: 1" = 40'-0"

#### SHORT TERM BIKE PARKING





**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

#### **1819 NW EVERETT STREET**

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

#### **RESTAURANT & BAR: 4,649 SF**

#### SHORT TERM

- REQUIREMENTS: 2, OR 1 PER 2,300 SF
- 2 SPACES

# **RETAIL SALES AND SERVICES: 30,658 SF** SHORT TERM

- REQUIREMENTS: 2, OR 1 PER 3,800SF
- 8 SPACES

#### .

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

#### **RESTAURANT & BAR: 7,034**

**1880 NW FLANDERS STREET** 

#### **SHORT TERM**

- REQUIREMENTS: 2, OR 1 PER 2,300 SF
- 4 SPACES

# **RETAIL SALES AND SERVICES: 4,815 SF**SHORT TERM

- REQUIREMENTS: 2, OR 1 PER 3,800SF
- 2 SPACES

# TEMPORARY LODGING: 80 RENTABLE ROOMS SHORT TERM

- REQUIREMENTS: 2, OR 1 PER 20 RENTABLE ROOMS
- 4 SPACES

#### 1819 NW EVERETT STREET TOTAL:

10 SPACES REQUIRED

#### 1880 NW FLANDERS ST TOTAL:

10 SPACES REQUIRED

#### **TOTAL SPACES PROVIDED:**

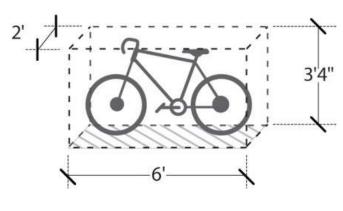
**SHORT TERM**: 20 SPACES REQUIRED, 20 SPACES PROVIDED

# SHORT TERM BICYCLE PARKING: RACK DESIGN LANDSCAPE FORMS (BOLA)

# 33.266.200 BICYCLE PARKING DEVELOPMENT STANDARDS



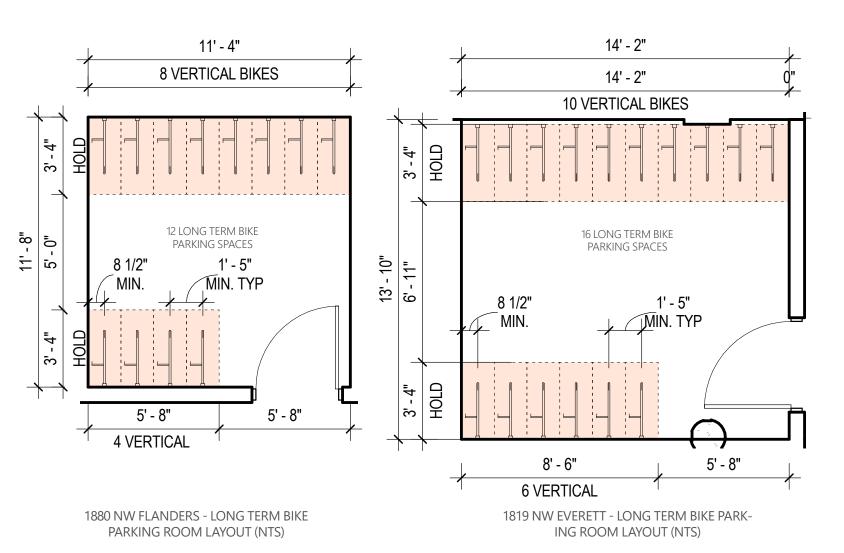


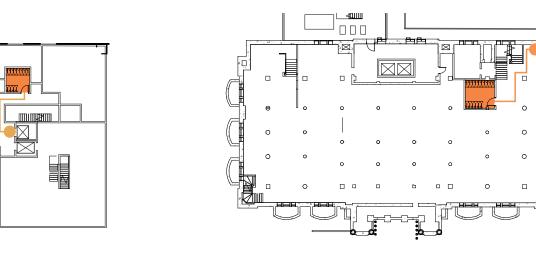


| SCALE: NTS



C.46





#### **1819 NW EVERETT STREET**

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

#### **RESTAURANT & BAR: 4,649 SF**

#### **LONG TERM**

- REQUIREMENTS: 2, OR 1 PER 1,000SF
- 5 SPACES

#### **RETAIL SALES AND SERVICES: 30,658 SF**

#### **LONG TERM**

- REQUIREMENTS: 2, OR 1 PER 2,700SF
- 11 SPACES

### **1880 NW FLANDERS STREET**

33.266.200 MINIMUM REQUIRED BICYCLE PARKING

#### **RESTAURANT & BAR: 7,034**

#### **LONG TERM**

- REQUIREMENTS: 2, OR 1 PER 1,000SF
- 8 SPACES

#### **RETAIL SALES AND SERVICES: 4,815 SF**

#### **LONG TERM**

- REQUIREMENTS: 2, OR 1 PER 2,700SF
- 2 SPACES

#### TEMPORARY LODGING: 80 RENTABLE ROOMS **LONG TERM**

- REQUIREMENTS: 2, OR 1 PER 40 RENTABLE ROOMS
- 2 SPACES

#### 1880 NW FLANDERS ST TOTAL:

12 SPACES REQUIRED

#### 1819 NW EVERETT ST TOTAL:

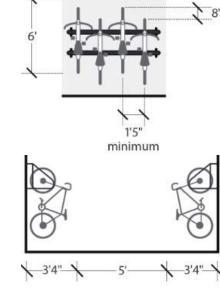
16 SPACES REQUIRED

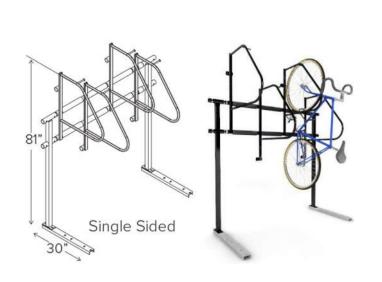
#### **TOTAL SPACES PROVIDED:**

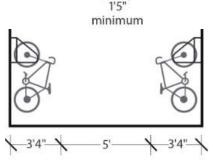
LONG TERM: 28 SPACES REQUIRED, 28 SPACES PROVIDED

#### LONG TERM BICYCLE PARKING: DERO ULTRA **SPACE SAVER SQUARED**

#### 33.266.200 BICYCLE PARKING **DEVELOPMENT STANDARDS**







| SCALE: NTS

## LONG TERM BIKE PARKING

1880 NW FLANDERS - BASEMENT FLOOR PLAN (NTS) -

**BIKE ROOM ACCESS** 





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

1819 NW EVERETT - BASEMENT FLOOR PLAN (NTS) -

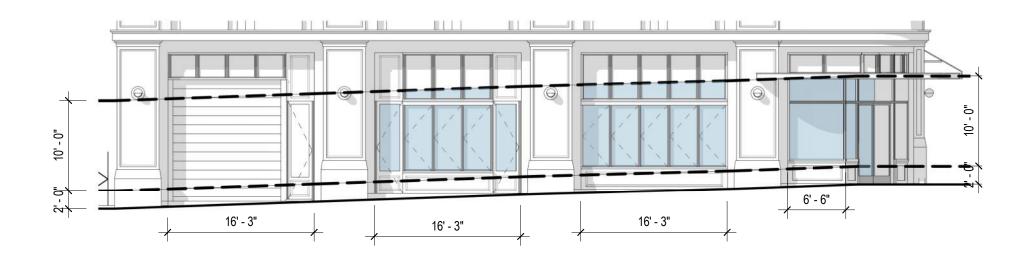
**BIKE ROOM ACCESS** 

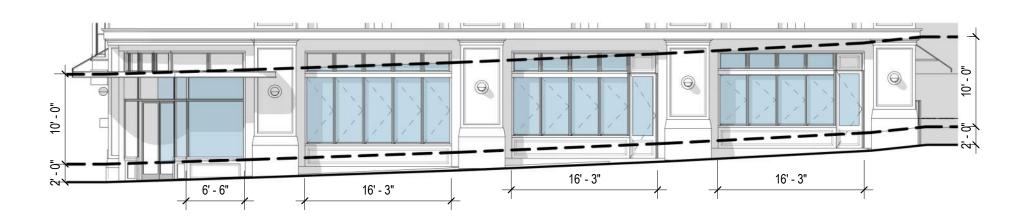


Planner 03-24-2023

Date \* This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

C.47





# **ELEVATIONS - GROUND FLOOR GLAZING (HOTEL)**

| SCALE: 3/32" = 1'-0"





**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

#### **GROUND FLOOR WINDOWS**

PER ZONING CODE SECTION 33.130.230 A MINIMUM OF 40% GLAZING ON THE GROUND FLOOR IS REQUIRED PER TITLE 33

#### <u>GROUND FLOOR GLAZING - NORTH ELEVATION</u>

LEVEL 1 AREA IN MEASURED ZONE = 857 SF STOREFRONT AREA = 361 SF 40%

GROUND FLOOR GLAZING - WEST ELEVATION

LEVEL 1 AREA IN MEASURED ZONE = 872 SF

STOREFRONT AREA = 369 SF

42%



C.48

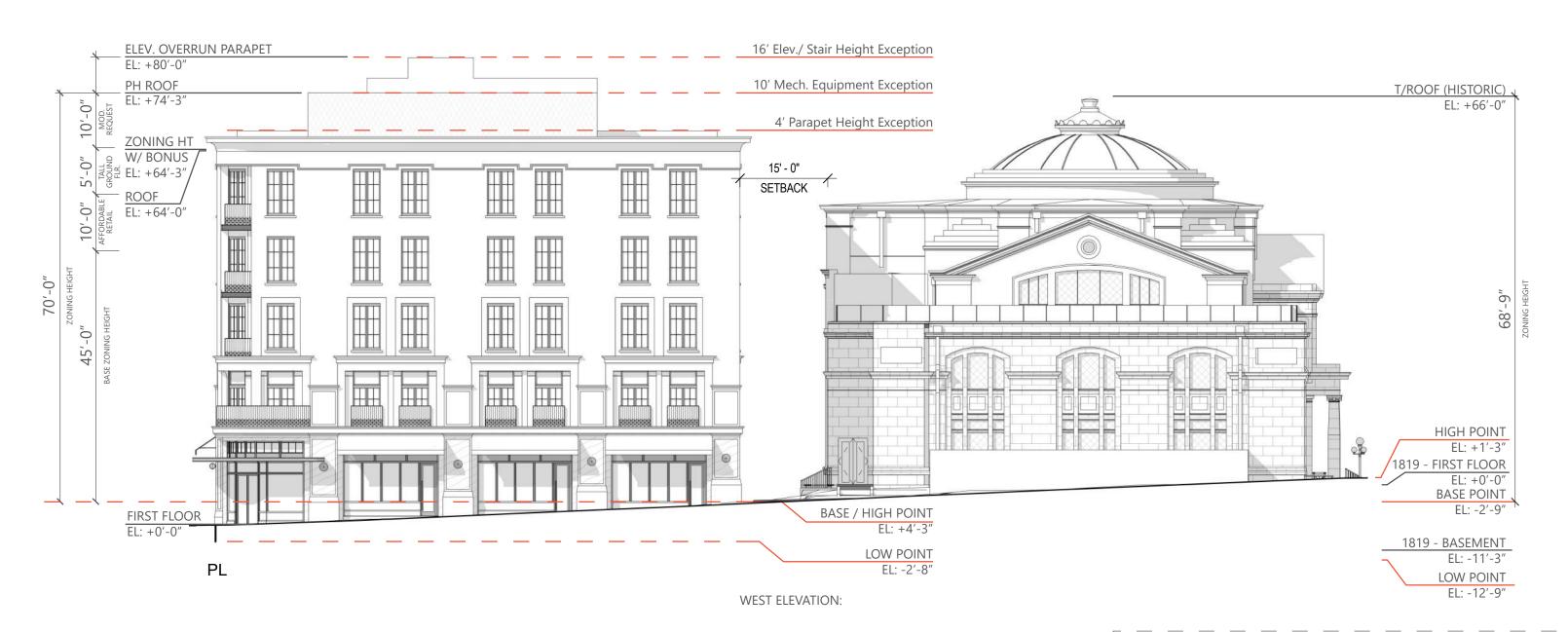
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Planner

AREA FOR CITY STAMP

03-24-2023

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## **ZONING HEIGHT DIAGRAM**

| SCALE: 1/16" = 1'-0"





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

C.49



# T225 Windows

Our T225 Modern Look or Steel Look windows are offered as fixed, casements, awnings and hopper configurations. Windows can be mulled to create long runs of fixed glazing which transition effortlessly into our T225 Doors.

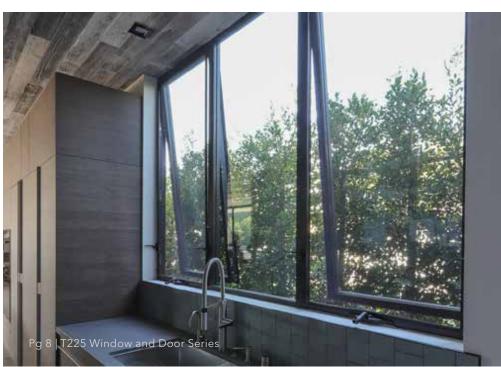
Frame Depth: 2 ¼"
High Performance Glass:
1" dual-glazed, Low-E glass

\*Approved\* City of Portland Bureau of Development Services

Planner\_

03-24-2023

Date 03-24-2023
\*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.





PRODUCT DATA: FIXED WINDOWS (1880 NW FLANDERS ST.)

| SCALE: NTS

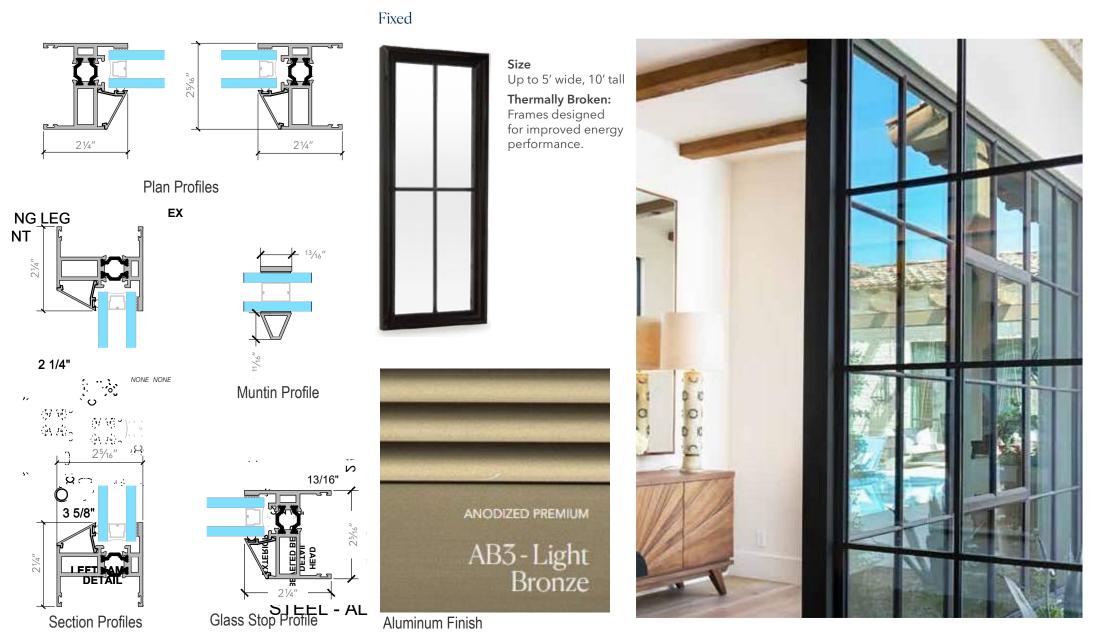




# T225 Window Details (Beveled or Square Glass Stop Options are Available)

#### Fixed

Long equal leg frame shown



Pa 10 | T225 Window and Door Serie

PRODUCT DATA: FIXED WINDOWS (1880 NW FLANDERS ST.)

| SCALE: NTS





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03-24-2023

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C.50

# \*Approved\* City of Portland Bureau of Development Services

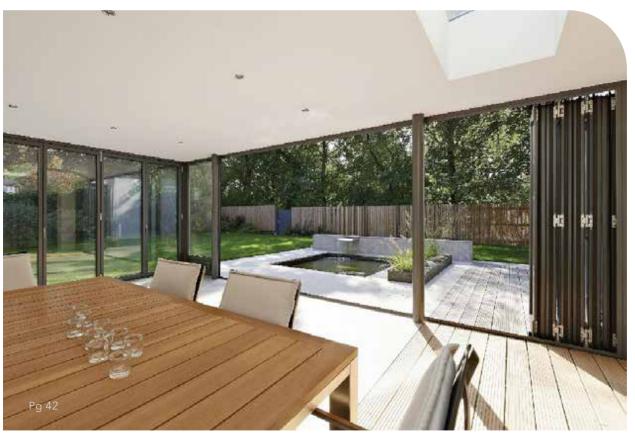
Planner\_

03-24-2023

conditions of approval. Additional zoning requirements may apply.

# Oasis 10000 Multi-Fold Doors

Thermally broken, high-performance, and energy efficient, the Oasis 10000 Multi-Fold Door is a top-hung system for large openings. Door panels can be folded away to reveal an uninterrupted view when open, yet remain in the same plane when closed. Removing the need for fixed panels or structural posts allows for maximum sunlight and fresh air to enter the room. Designed and engineered as an outswing for maximum weather performance.





PRODUCT DATA: OPERABLE STOREFRONT (1880 NW FLANDERS ST.)

| SCALE: NTS





# Oasis 10000 Specifications

Individual door panels up to 39" wide,10' tall, not to exceed 220 lbs. Overall door system length a maximum of 52' wide,10' tall.

#### Thermally Broken

Door panels and frames for improved energy performance.

#### **High Performance Glass**

1" dual-glazed, Low-E glass, see Technical Specifications for NFRC Chart

#### Frame Depth

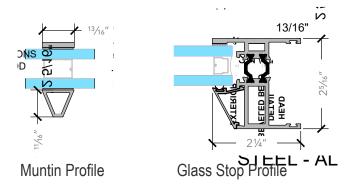
6 1/2"

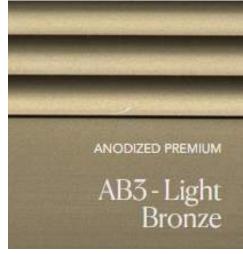
#### Hardware

Stainless steel precision bearing top rollers, intermediate pivot hinges.

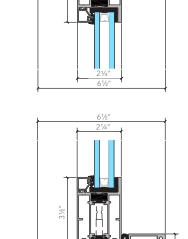
#### **Twin-Point Lock**

Streamline twin-point locks keep doors secure when closed and provide for easy, one-handed operation; sturdy 2 1/4" thick door profile.

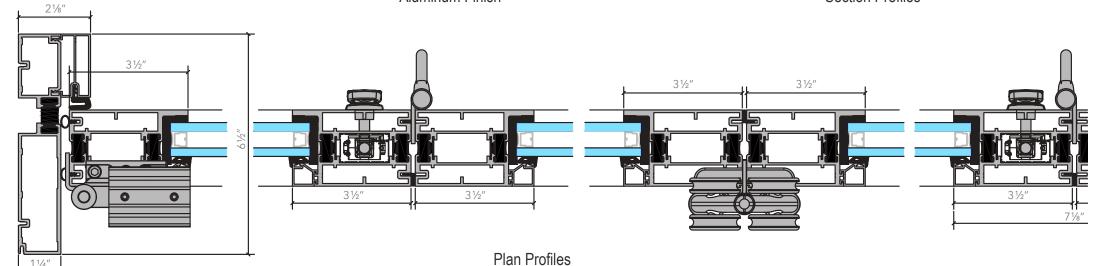




Aluminum Finish



Section Profiles



Pg 44 | 10000 Multi-Fold Doors

PRODUCT DATA: OPERABLE STOREFRONT (1880 NW FLANDERS ST.)

| SCALE: NTS







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Planner\_ 03-24-2023 Date

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C.52

# T225 Swing Doors

Size Single door panel sizes up to 3'6" wide, 9' tall or 3' wide, 10' tall.

#### Hardware

3-point latch lock, throws shoot bolts top and bottom when the lever is raised, locks with key from the exterior and thumb turn interior.

#### **Weather Tightness**

Double weather stripped for maximum air and water resistance.

#### Configurations

Inswing or outswing, single or pairs, with sidelite and transom options.



# T225 Swing Door Configuration Options



SINGLE



PAIR



SINGLE MULLED SIDELIGHTS



MULLED SIDELIGHTS &TRANSOM





# PRODUCT DATA: FRENCH DOORS (1880 NW FLANDERS ST.)

| SCALE: NTS







1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

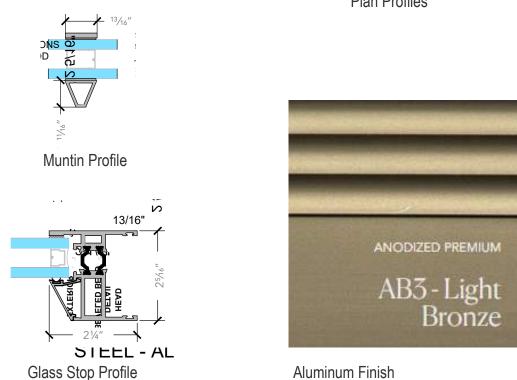
Spinelnurryant Planner

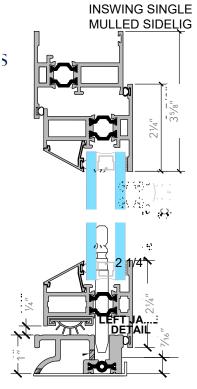
03-24-2023

\*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

C.53

# Inswing Pair 35/4" 21/4" Plan Profiles





Section Profiles

Pg 16 | T225 Window and Door Series

# PRODUCT DATA: FRENCH DOORS (1880 NW FLANDERS ST.)

| SCALE: NTS







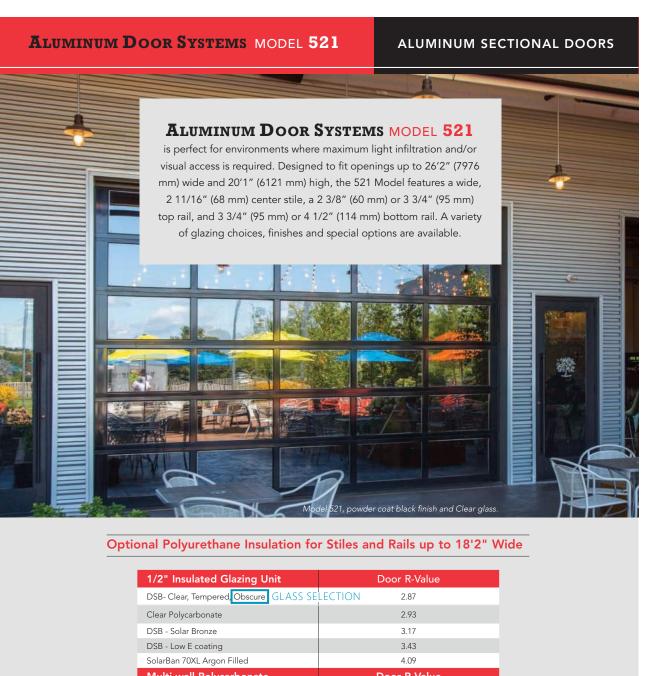
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

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City of Portland
Bureau of Development Services

Planner furthermet

Date 03-24-2023

C.54



DSB- Clear, Tempered, Obscure (	GLASS SE	LECTION 2.87	
Clear Polycarbonate		2.93	
DSB - Solar Bronze		3.17	
DSB - Low E coating		3.43	
SolarBan 70XL Argon Filled		4.09	
Multi-wall Polycarbonate		Door R-Value	
1/4" Thick Unit		2.75	
3/8" Thick Unit		3.21	
5/8" Thick Unit		3.48	
Insulated Panels		Door R-Value	
3/8" EPS Solid Panels		2.60	
Polyurethane filled rails and stiles	*R-Value: Ove for our insula	erhead Door Corporation uses a calculated door section R-value ted doors.	

# PRODUCT DATA: OVERHEAD ALUMINUM DOOR (1880 NW FLANDERS ST.)

| SCALE: NTS





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

#### MODEL **521**



#### Standard Features at a Glance **Options**

C .: TILL	4.2/11/45	
Section Thickness	1 ¾" (45 mm)	Glazing Options <sup>†</sup> : 1/8" (3 mm) DSB; 1/8" (3 mm) or 1/4"
Maximum Standard Height	20'1" (6121 mm)	(6 mm) acrylic; 1/8" (3 mm) or 1/4" (6 mm) tempered;
Maximum Standard Width	26'2" (7976 mm)	1/8" (3 mm) or 1/4" (6 mm) clear polycarbonate;
Material	Extruded 6061-T6 aluminum	1/4" (6mm) and 3/8" twin-wall polycarbonate, 5/8" triple- wall polycarbonate;
Standard Finish	204R-1 clear anodized (painted white at no charge)	1/4" (6 mm) 3/8" (10 mm) and 5/8" (16 mm) twin-wall
Center Stile Width	2 11/ <sub>16</sub> " (68 mm)	polycarbonate, triple-wall polycarbonate 1/4" (6 mm) wire glass; 1/2" (12 mm) insulated glass
End Stile Width	3 <sup>5</sup> / <sub>16</sub> " (85 mm)	Electric operator or chain hoist
Top Rail Width	2 3/8" (60 mm) or 3 3/4" (95 mm)	Bottom sensing edge
Top Intermediate Rail Width	2 1/8" (54 mm)	3" track
Bottom Intermediate Rail Width	1 <sup>19</sup> / <sub>32</sub> " (40 mm)	Bracket mounting (not available on full vertical door tracks)
Bottom Rail Width	3 ¾" (95 mm) or 4 ½" (114 mm)	Higher-cycle springs in 25k, 50k, 75k, 100k cycles
Weatherseals	Bottom, flexible PVC	Exhaust ports
Standard Springs	10,000 cycle	Four-section pass door
Track	2" (51 mm)	Wind load and impact rated door available
Mounting	Angle	Posi-tension drums
Operation	Manual pull rope	Bronze anodization
Hinges and Fixtures	Galvanized steel	Powder coat finish
Lock	Galvanized, interior-mounted single unit	Pass door
Warranty	1-Year Limited; 3-Year Limited on powder coat finish	†Contact your local Overhead Door™ Distributor for special glazing requirements.

#### **Structure Options**

Anodized Finishes

Clear (standard)







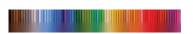


Dark Bronze

**Powder Coat Finishes** Select from approximately 200 RAL

Black

powder coat color options to best match your home.



Cherry with Flame Dark Walnut Actual door colors may vary from brochure photos due to fluctuations in the printing process. Always request a color sample from your Overhead Door™ Distributor for accurate color matching.

Panel Layout		Section Stack		
Door Width	Number of Panels	Door Height	Number of Sections	
to 9'2" (to 2794 mm)	2 or 3 (standard)	to 8'6" (2591 mm)	4	
9'3" to 12'2" (2819 mm to 3708 mm)	3	8'7" to 10'1" (2616 mm to 3073 mm)	5	
12'3" to 16'2" (3734 mm to 4953 mm)	4	10'2" to 12'1" (3099 mm to 3683 mm)	6	
16'3" to 18'2" (4978 mm to 5537 mm)	4 or 5 (standard)	12'2" to 14'1" (3708 mm to 4293 mm)	7	
18'3" to 19'2" (5562 mm to 5842 mm)	5	14'2" to 16'1" (4318 mm to 4902 mm)	8	
19'3" to 20'11" (5867 mm to 6375 mm)	6**	16'2" to 18'1" (4928 mm to 5512 mm)	9	
21'0" to 23'11" (6401 mm to 7290 mm)	8**	18'2" to 20'1" (5537 mm to 6121 mm)	10	
24'0" to 26'2" (7315 mm to 7976 mm)	10**	**Special construction. Consult your local Overhead™	Door Distributor for additional information.	

verheaddoor.com \*Approved\* City of Portland Bureau of Development Services

Planner 03-24-2023

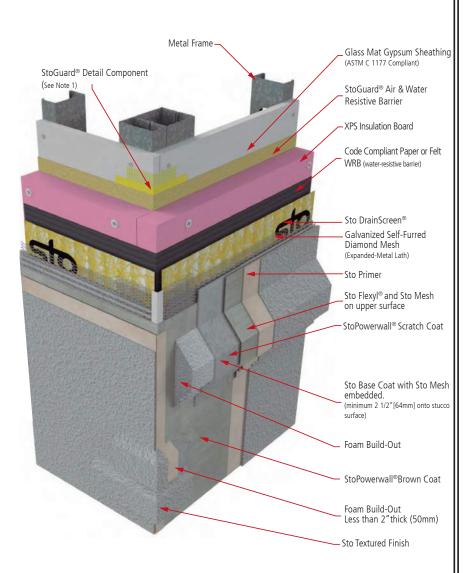
\* This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

C.55



#### Sto Powerwall®ci Foam Buildouts

Detail No.: 63s.20 Date: March 2022



#### Notes:

- 1. Refer to StoGuard Product Use Chart (Table 1.1) for StoGuard detail components; joint treatment, rough opening protection, backing for masonry anchors, or transitions to dissimilar materials, joints and seams in construction.
- 2. Refer to General Notes for specific information and design guidance on wall assembly components.
- 3. XPS Insulation options, in compliance with ASTM C 578:
- Owens Corning Type IV XPS rigid insulation board.
- DuPont Type IV XPS rigid insulation board.
- 4. Glass mat gypsum sheathing in compliance with ASTM C1177, exterior grade and Exposure 1 wood based sheathing, or cementitious sheathing in compliance with ASTM C1325.
- 5. Minimum 2.5 lb/yd² (1.4 kg/m² self-furred galvanized diamond mesh metal lath.
- 6. Stucco scratch and brown coat material in compliance with ASTM C926 and manufactured or listed by Sto Corp.
- 7. Components not identified as Sto are furnished by other manufacturers and are not necessarily installed by trades who install the Sto products. Refer to project specific contract documents.

www.stocorp.com



#### ATTENTION

Sto products are intended for use by qualified professional contractors, not consumers, as a component of a larger construction assembly as specified by a qualified design professional, general contractor or builder. They should be installed in accordance with those specifications and Sto's instructions. Sto Corp. (slacilians all, and assumes no, liability for on-site inspections, for its products applied improperly, or by unqualified persons or entities, or a part of an improperly designed or constructed building, for the construction activities beyond Sto's control. Improper use of Sto products or use as part of an improperly designed or constructed larger assembly or building may result in serious damage to Sto products or the structure of the building or its components.

STO CORP. DISCLAIMS ALL WARRANTIES EXPRESSED OR IMPLIED EXCEPT FOR EXPLICIT LIMITED WRITTEN WARRANTIES ISSUED TO AND ACCEPTED BY BUILDING OWNERS IN ACCORDANCE WITH STO'S WARRANTY PROGRAMS WHICH ARE SUBJECT TO CHANGE FROM TIME TO TIME. For the fullest, most current information on proper application, clean-up,

FOAM DETAIL

## PRODUCT DATA: STUCCO

| SCALE: NTS







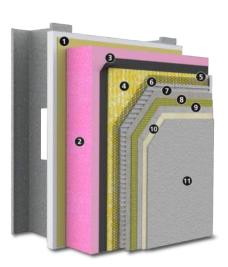
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System Bulletin

Sto Building with conscience.

StoPowerwall® ci

Portland cement stucco with continuous air and water-resistive barrier, continuous insulation, advanced cavity wall design, and high performance finish



Substrate: Glass mat gypsum sheathing in compliance with ASTM C 1177 (or building code compliant wood-based sheathing - Type V construction only)

1)	StoGuard® Air and Water-Resistive Barrier
2)	Owens Corning or Dupont Type IV XPS Insulation Board
3)	Code compliant paper or felt Water-Resistive Barrier
4)	Sto DrainScreen® drainage mat
5)	Code compliant miniumum 2.5 lb/yd² (1.4 kg/m²) self- furred galvanized steel diamond mesh metal lath
6), 7)	ASTM C 926 compliant stucco scratch and brown coat (as manufactured or listed by Sto Corp.)
8), 9)	Sto Crack Defense (optional): Sto base coat with Sto Mesh embedded
10)	Sto primer (optional)
11)	<ul> <li>Choose among:</li> <li>Sto Textured Finishes</li> <li>Sto Custom Cast Finish: Wood or Brick</li> <li>Sto Signature Series or Sto Speciatly Finishes</li> </ul>

Planner

#### **System Description**

StoPowerwall ci is an energy efficient stucco wall assembly with a continuous air and water-resistive barrier and continuous insulation. It combines the strength and durability of traditional stucco with an advanced cavity wall design and Sto high performance finishes.

#### Uses

StoPowerwall ci can be used in residential or commercial wall construction where energy efficiency, superior aesthetics, and air and moisture control are essential in the climate extremes of North America

Features		Benefits		
Integrally colored factory blended textured finish		Consistent color and aesthetics increase curb appeal		
Continuous exterior insulation	1	Energy efficient, reduced heating and cooling costs		
Impact and puncture resistant		Withstands abuse, reduced maintenance		
Continuous air and moisture barrier		Protects against mold and moisture problems		
Fully tested, building code compliant				
Properties				
Weight (excluding sheathing / studs)	< 12 psf (56.6 kg/m²)			
Assembly Thickness (from exterior stud face)	Nominal 3-5/8" (92 mm) with 2" (51 mm) XPS			
R-value (insulation only)	5 – 10 ft²•h•°F / Btu (0.88 – 1.76 m²•K / W)			
Wind Load Resistance	Capable of achieving:			
		+65, -48 psf		
	(+3.11 to -2.29 kPa)			
Compliance		<ul><li>IBC, IRC, and IECC</li><li>ASHRAE 90.1-2019</li></ul>		
Construction Types, Fire Resistance		<ul> <li>NFPA 285 tested for use on noncombustible construction</li> <li>ASTM E119 1-hour rated assembly</li> </ul>		

#### Varranty

Up to 12-year Limited Warranty available on Sto products, depending on options selected.

#### Maintenance

Requires periodic cleaning to maintain appearance, repair of cracks and impact damage if they occur, recoating to enhance appearance of weathered finish. Sealants and other façade components must be maintained to prevent water infiltration.

STUCCO WALL

\*Approved\*

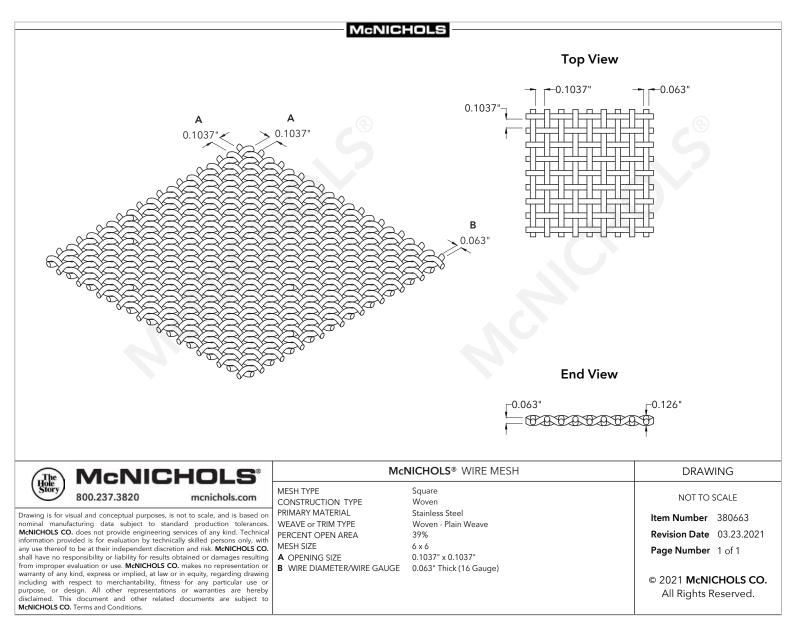
City of Portland

Bureau of Development Services

03-24-2023

\*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

C.56



MECHANICAL SCREEN WALL

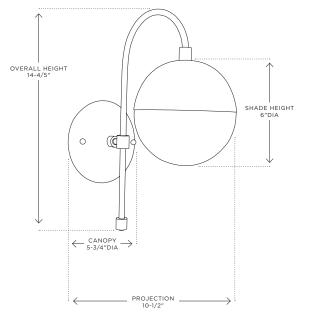


## CEDAR & MOSS

NORTHWEST MODERN GLOBE SCONCE

AVAILABLE SIZES:

A0054 - 6"DIA (shown in Brushed Satin)





#### **DETAILS**

UL RATING: Damp NUMBER OF SOCKETS: 1 MAX WATTAGE: 40W

AVAILABLE SOCKET TYPES: Medium Base SHADE SHOWN: B0017 - 6" White Globe (included)

#### FIXTURE FINISH



**EXTERIOR LIGHT FIXTURE** 

REJUVENATION.COM

PRODUCT DATA: MECHANICAL SCREEN & EXTERIOR LIGHT FIXTURE

| SCALE: NTS

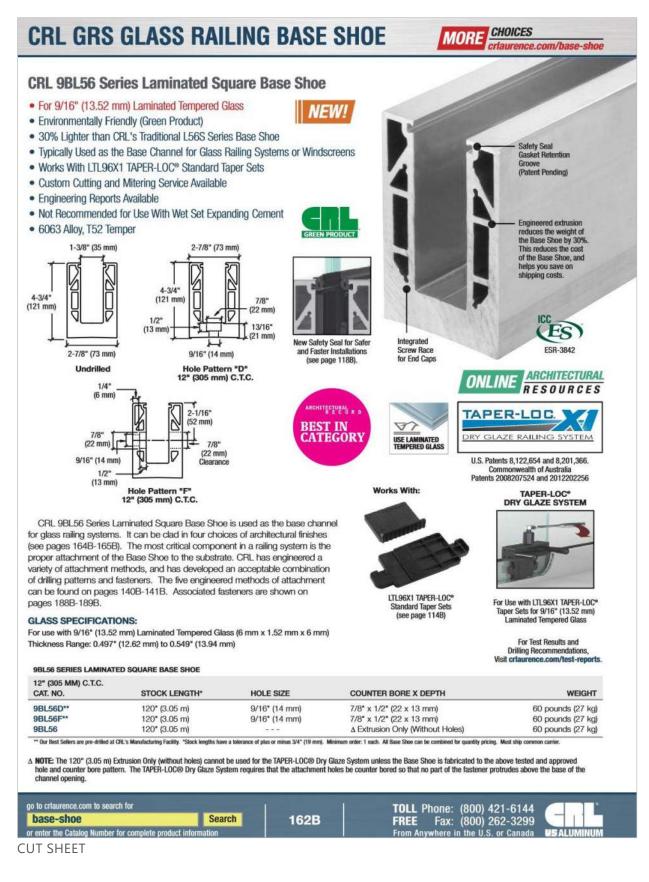






1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

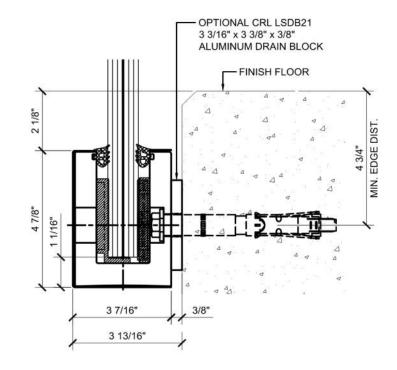
C.57





PRODUCT IMAGE - BASE DETAIL

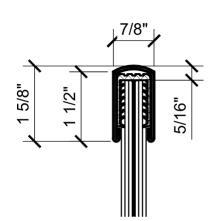
**ELEVATION DETAIL** 



FASCIA MOUNT DETAIL CONCRETE SUBSTRATE

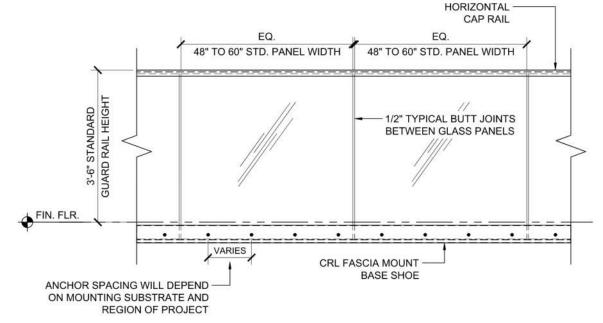
ANCHOR SPACING WILL DEPEND ON MOUNTING SUBSTRATE AND REGION OF PROJECT 12" O.C. SPACING IS STANDARD

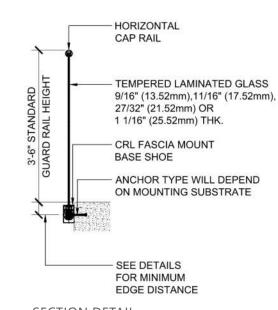
SECTION DETAIL AT BASE



GRCR5 - 7/8" x 1 1/2" FOR 9/16" (13.52mm) LAMINATED TEMPERED GLASS

TOP CAP DETAIL





SECTION DETAIL

## PRODUCT DATA: GLASS GUARDRAIL (1819 NW EVERETT ST.)

I SCALE: NTS





**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

\*Approved\*

City of Portland
Bureau of Development Services

Planner

Date

\*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

C.58

			SUBMITTAL AC			www.SamsungHVAC.co ator, single zone split system
ob Name			Lo	cation		
Purchaser			Er	ngineer		
Submitted to			Re	eference	Approval	Construction
Jnit Designation	on		Sc	hedule #		
					6	
	I	Specifications Cooling (Btu/h)	24,000			
Performance	Nominal Capacity	Heating (Btu/h)	27,000			Annual Control
	Capacity Range	Cooling (Btu/h) Heating (Btu/h)	4,405 - 27,00 5,700 - 34,0			1
	SEER / EER	riodalig (Blairi)	15.0 / 9.6	-		
	HSPF Condensate (pints	:/hour)	7.8 6.34		AQN24VFUAGM	
	Voltage (ø/V/Hz)	, nour	1 / 208-230 /	60		
	Working Voltage Range (VAC)		176 - 254 (max. 3% devia	tion from each)	AMMUNC	
ower	Rated Current Cooling (A) (Low/Std./Max.) Heating (A)		2.6 / 11.9 / 12 2.3 / 13.3 / 16			
	Max. Breaker (A)	rieating (A)	20	5.0		
	Min. Circuit Ampa	city (A)	12.0		1888	
	W X H X D (inches)	Indoor Unit Outdoor Unit	41 15/16 X 11 3/4 34 5/8 X 31 9/32 X			
imensions	(Inches) Weight (lbs.)	Indoor Unit	34 5/8 X 31 9/32 X 25	12 J/ 10	1	
	Condensate Conn	Outdoor Unit	118 11/16" OD		1	Short manual
	1	Type	Aluminum Fin - Cop		-	/
	Indoor & Outdoor	FPI	18	per rube	۸	QX24VFUAGM
eat Exchanger	Unit	Pipe Diameter	1/4 inch		AC	2XZ4VFUAGIVI
	Indoor Unit Outdoor Unit		2 row / 16 ste 2 row / 36 ste			
ound Pressure Level	Indoor Unit (dB)	(L/H)	30 / 41	• Low	ambient control built in	
ound Pressure Level	Outdoor Unit (dB)	High	55	- Out	door unit aball provide O	09/220\/ newer to indeer unit via 14 A\A\
	Outdoor	Cooling	Standard: 14 ≤ T	2 110 3 inte	coor unit snaii provide 2 rconnect power cable	08/230V power to indoor unit via 14 AW
perating	Outdoor	Heating	0 ≤ T ≤ 115 with wind bar 5 ≤ T ≤ 75		·	\( \text{\text{Link}} \\ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \
emperatures (°F)	Indoor	Cooling	61 ≤ T ≤ 90		ctro-static, washable, HL	) (high density) main filter as standard
	<u> </u>	Heating	T ≤ 80		struction	II 04 \/0 with a galvanized steel magnitin
	Indoor & Outdoor	High side (flare) Low side (flare)	5/8"	brack		JL94 V0 with a galvanized steel mounting
ipe Connections		um Line Set Length (ft.)	98 / 10	The	outdoor unit aball be eal	renized steel with a belied on newder of
	Maximum Vertical	Separation (ft.)	50 R410A		for durability	vanized steel with a baked on powder co
	Type Control Method		Electronic Expansion	on Valve	Heat Evelonmen	
efrigerant	Factory Charge (o	z.)	58.1	Heat Exchanger  The heat exchanger shall be mechanically bonded fin to co		mechanically bonded fin to copper tube
	Charged for Additional Refrige	rant	25 ft. 0.16 oz./ft. over	25 ft. Pofri	gerant System	
	Manufacturer		Samsung	The o	compressor shall be herr	metically sealed, inverter controlled, Twi
	Туре		DC, Inverter Driven	, Rotary BLD0	Rotary	
ompressor	RLA (A) Operating	Cooling (low/std./high)	8.3 15 / 76 / 88	Refri	gerant flow shall be cont	rolled by EEV (electronic expansion val
	Frequency (Hz)	Heating (low/std./high)	15 / 76 / 100		tdoor unit	
	Туре	OCAN)	BLDC motor with cross		or Fan	
vaporator Fan	Air Volume (max. Consumption	CFM) W	530 27	The i	ndoor tan shall be a sing	gle, antibacterial cross-flow type
	Operating Current		0.12	Three	e fan speed settings and	auto setting
andanaar E	Motor	Tray	BLDC motor with ax	ial fan (1) Cont	rols	
ondenser Fan	Output FLA	W Amps	124 0.52		ol signal shall be DDC t	ype signal
	Condensate pump		ASP-MO-UNIV 110-2	250 Interd	connect control wiring sh	nall be 16 AWG X 2 shielded wire between
		Standard	AQN-WRS (includes si		or and indoor units	
	Wired Controller	-	AON WPP (includes of	ub-PCB and MWR- Unit s	shall be operated via wire	eless controller (included)
		Premium	WE10 controller with se	cheduling)	•	, ,
ccessories	Wall bracket (for o	outdoor unit) ed and flared, interconnect	☐ CKN-250 ☐ 25' - ILS2509	Optio	nal wired control availab	DIE
	cables included	ed and hared, interconnect	50' - ILS5009	Conv	renience	
	Wind Baffle	Front Back	☐ WBMF-24/36 ☐ WBMB-24	• Auto	o restart	aparation only)
	Certifications	Daon	ETL & ETLc	• Auto	oo mode (during cooling o changeover	operation only)
-f-h.	Ceruncations	PCB fuses, indoor unit ten	minal block thermal fuse, current	• 24 h	nour timer	
Safety	Devices voltage protection, crankcase heating, temperature limit protection logic,		protection logic, • Qui	od'sleep mode et mode		
		<u> </u>	empressor overload sensing	• Dry	mode	
arranty		5 Years compressor, 3 Ye	ar Parts, 120 Day limited labor			
_						
			are based on: Indoor temperature			
				DE DD GODE WD O II	nperature: 47°F DB, 43°F W	ID.

CU PRODUCT DATA

# PRODUCT DATA: MECHANICAL EQUIPMENT \*

| SCALE: NTS





1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR

[1.25"]DRAIN CONN PLAN VIEW [UPPER DECK] PLAN VIEW [LOWER DECK] S.A.OPEN GAS HEATER 1xHMG 250 \_E.A.HOOD W/SCREEN 4"PLEATED FILTER
2"PLEATED FILTER ELEVATED VIEW 1230 [48.4] CO.A.DAMPER O.A.HOOD W/SCREEN FROOF COVER 860 [33.9] POWER ENTRY 100 [3.9] 100 [3.9] 1070 [42.1] 3070 [120.9] SHEET METAL UNIT MODEL : PPH 17 DIMS
TITLE : PPH 17 ENGINEANDI DAS CONTECT HIT

RTU / RTU-1 PRODUCT DATA

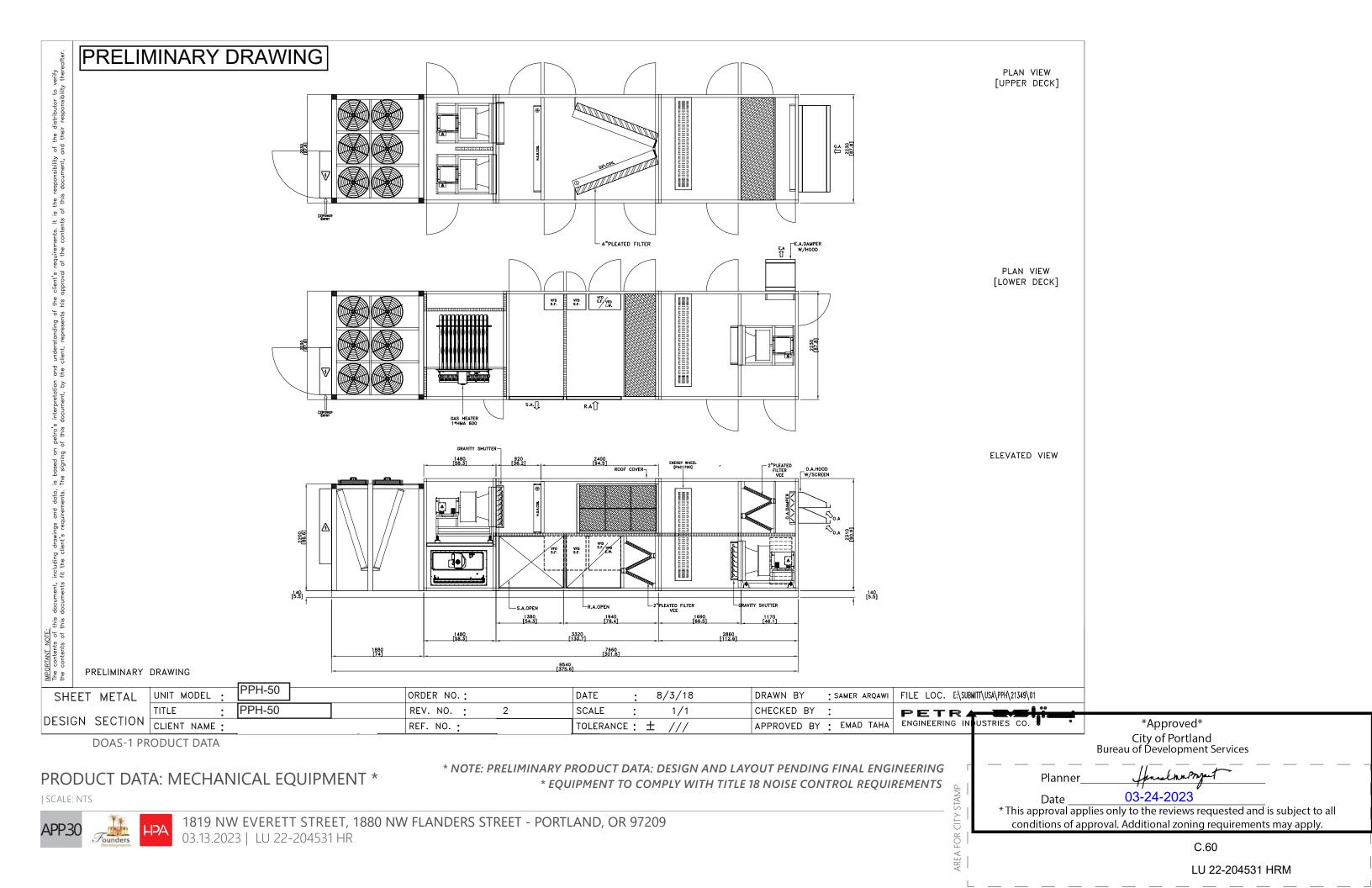
\* NOTE: PRELIMINARY PRODUCT DATA: DESIGN AND LAYOUT
PENDING FINAL ENGINEERING
\* EQUIPMENT TO COMPLY WITH TITLE 18 NOISE CONTROL

PLY WITH TITLE 18 NOISE CONTROL
REQUIREMENTS

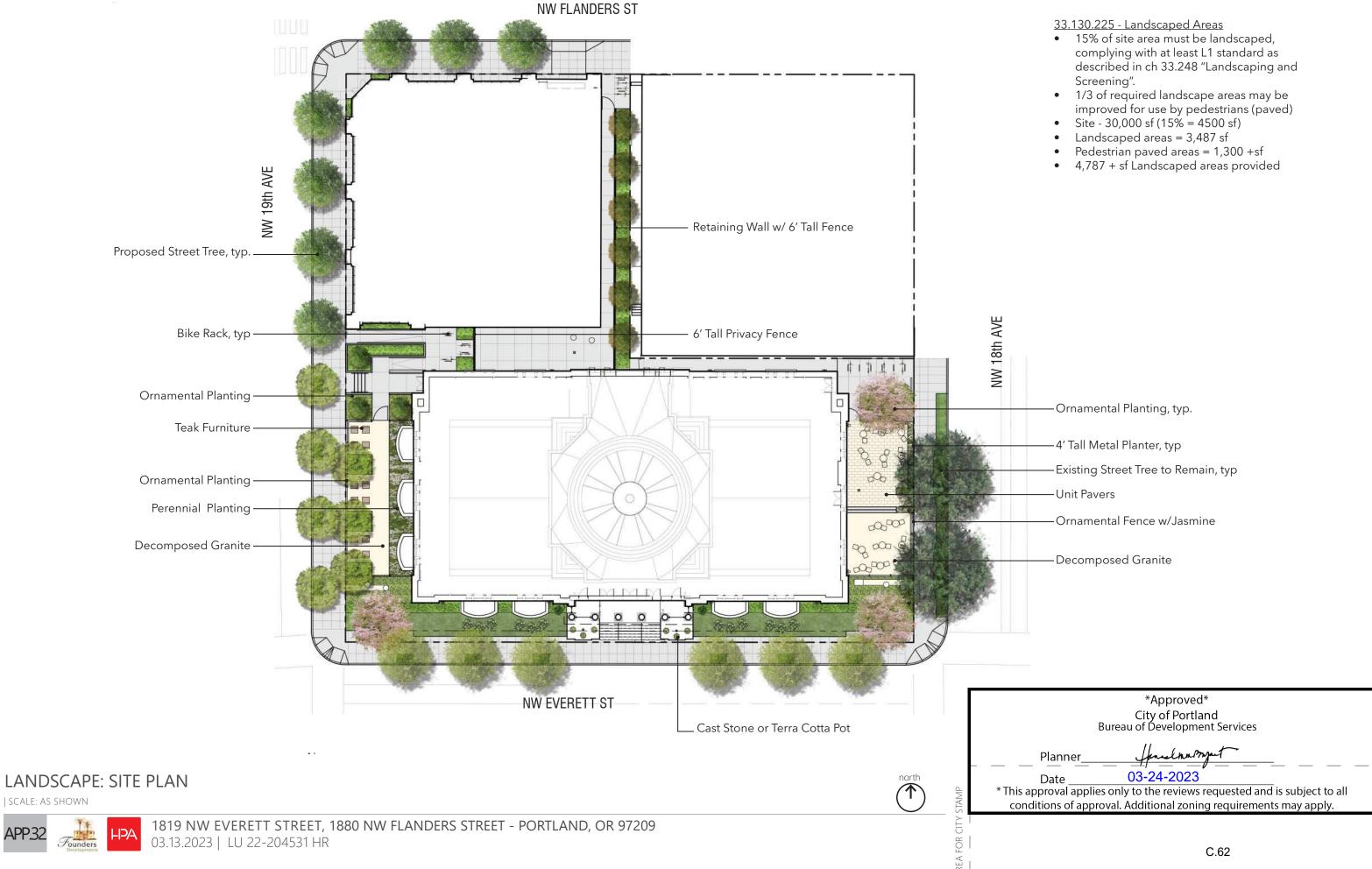
\*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply.

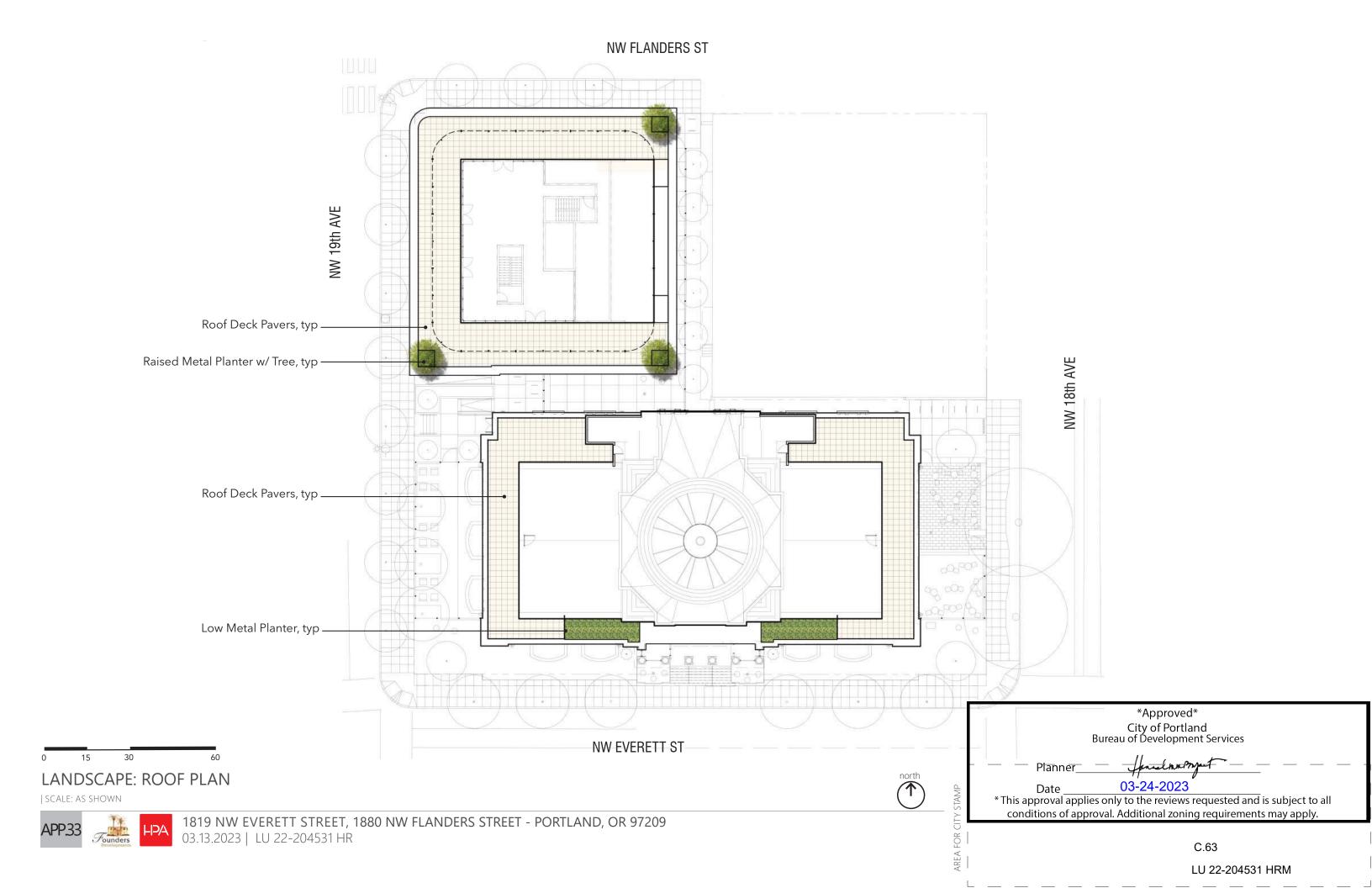
City of Portland Bureau of Development Services

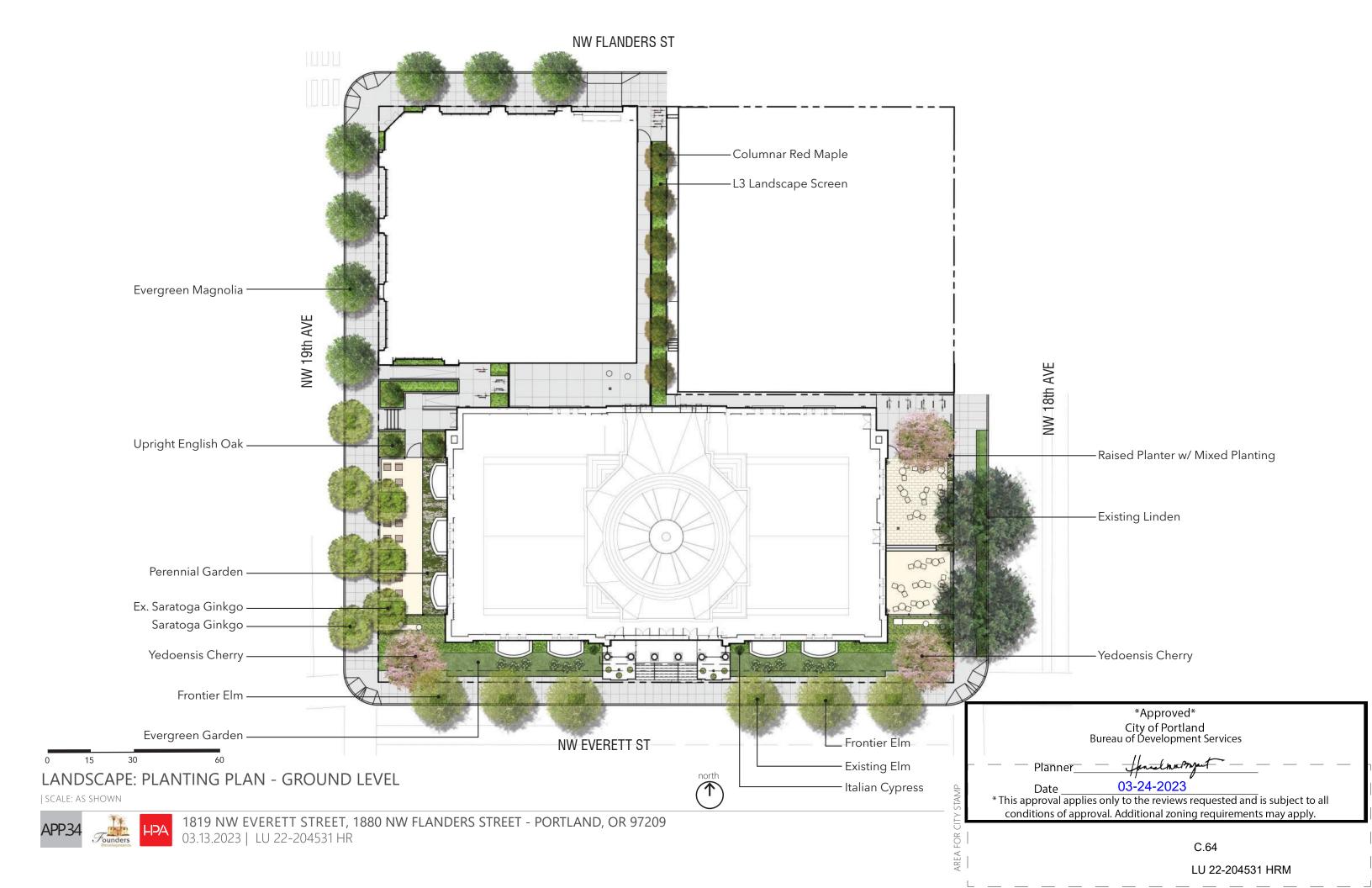
C.59

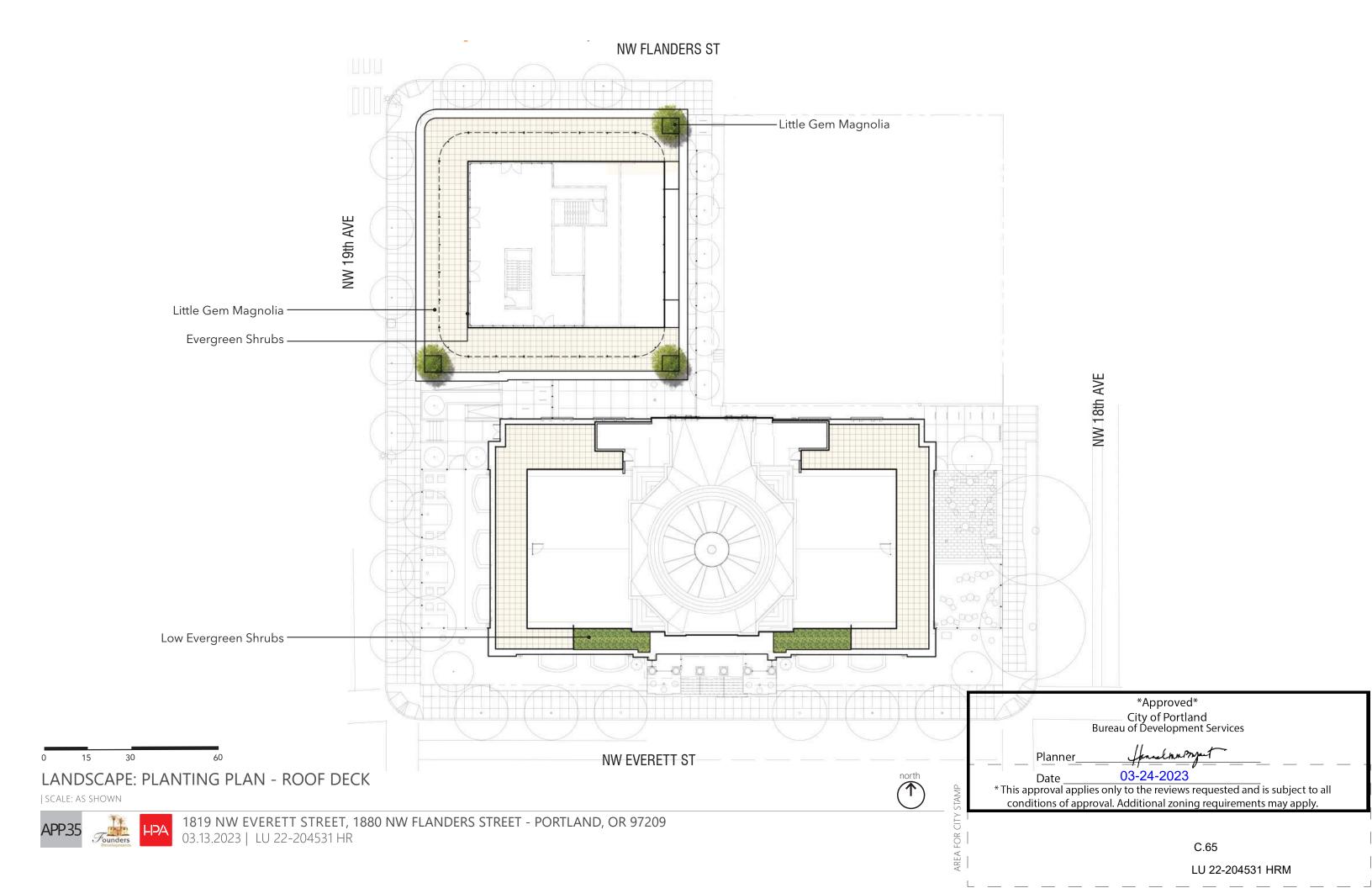


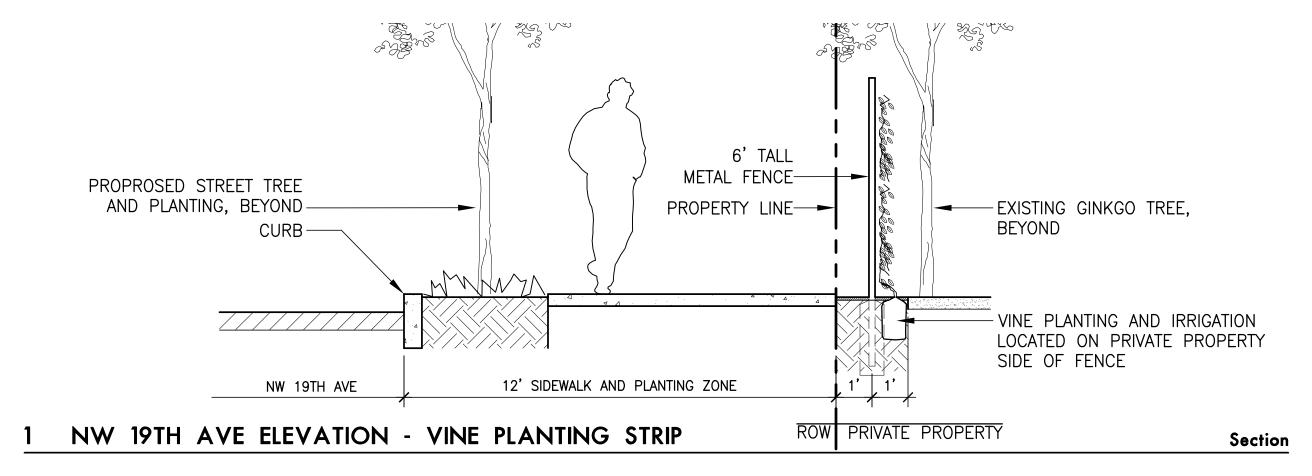
#### Tree Table Cond. Status Common / Botanical Name NW FLANDERS ST Size Location American Elm / Ulmus americana To be removed 36" Street Tree Poor American Elm / Ulmus americana 30" To be removed Street Tree 3 American Elm / Ulmus americana 32" Street Tree To be removed Poor 4 American Elm / Ulmus americana 40" Street Tree Poor To be removed Ginkgo biloba / Ginkgo 6" On Site Good To be preserved 5 Ginkgo biloba / Ginkgo 6" On Site Good To be preserved 6 Ginkgo biloba / Ginkgo On Site Good To be preserved 8 Frontier Elm / Ulmus 'Frontier' Good To be preserved Street Tree Frontier Elm / Ulmus 'Frontier Good To be preserved 9 Street Tree 10 Newport Plum / Prunus cer. 'Newport' 25" To be removed Street Tree 11 Linden / Tilia spp. Good To be preserved Street Tree Good To be preserved Linden / Tilia spp. 12 30" Street Tree 13 Birch / Betula spp. To be removed 26" Onsite 0 NW 19th AVE -Tree Protection Fencing, typ \*Approved\* City of Portland Bureau of Development Services NW EVERETT ST Planner\_ 03-24-2023 Date \*This approval applies only to the reviews requested and is subject to all conditions of approval. Additional zoning requirements may apply. LANDSCAPE: TREE PLAN | SCALE: AS SHOWN 1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209 03.13.2023 | LU 22-204531 HR C.61 LU 22-204531 HRM

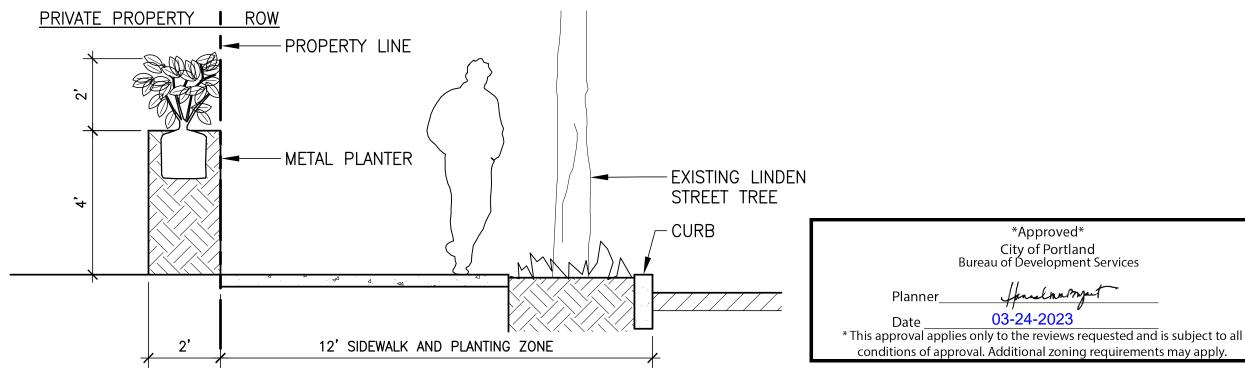












2 NW 18TH AVE ELEVATION - RAISED METAL PLANTER

LANDSCAPE: PLANTING ELEVATIONS

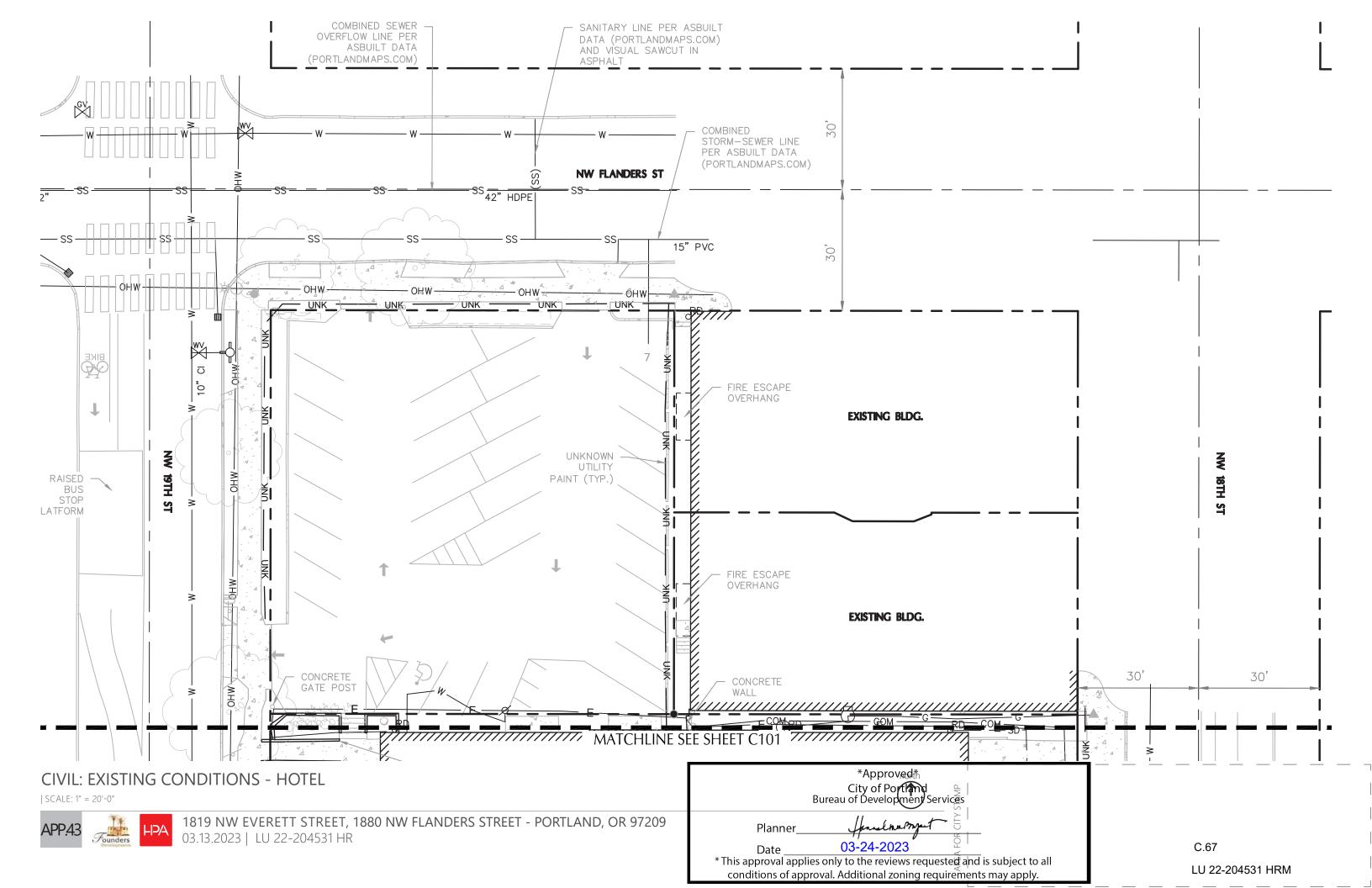


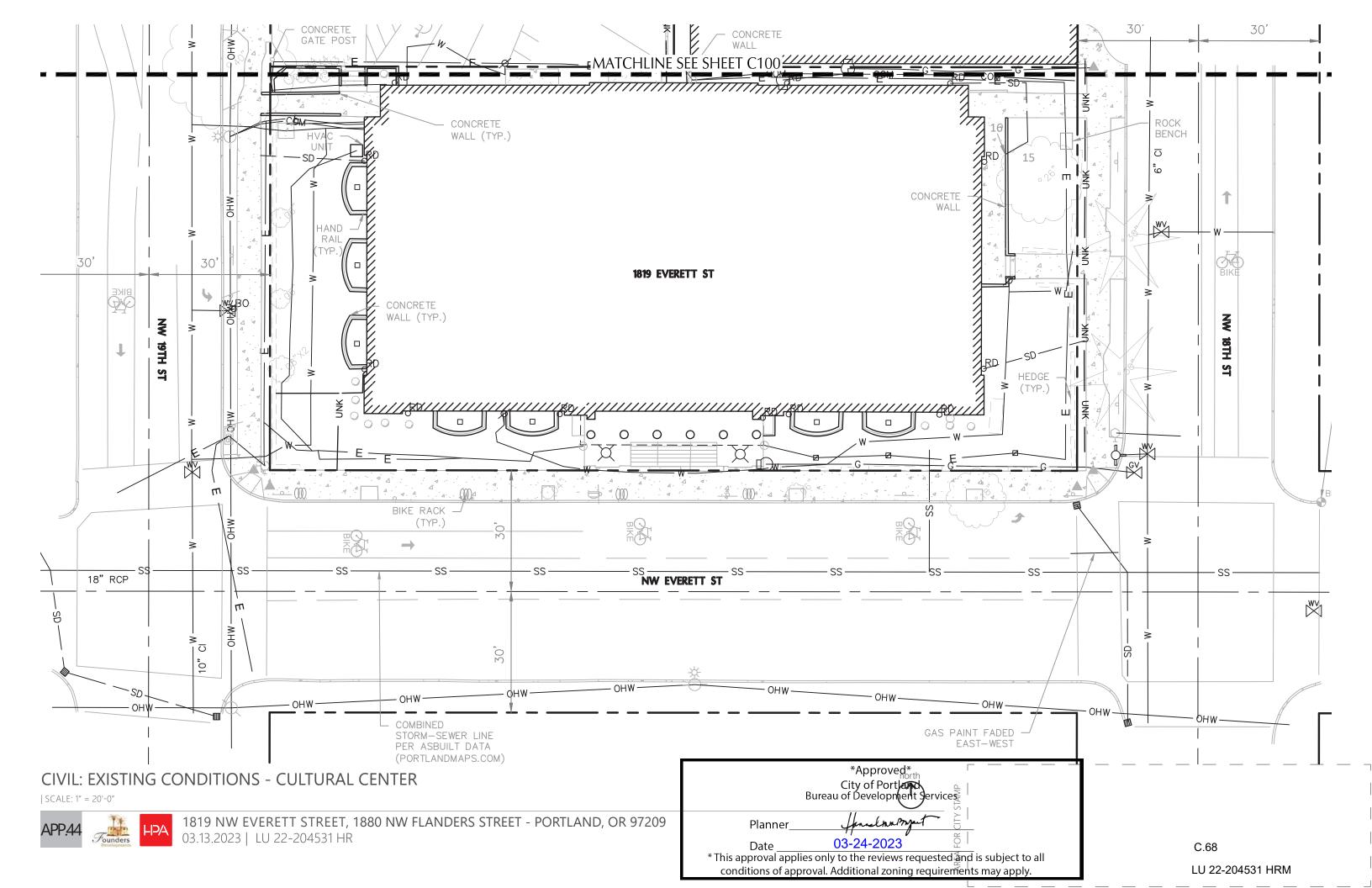
| SCALE: 3/8" = 1'-0"

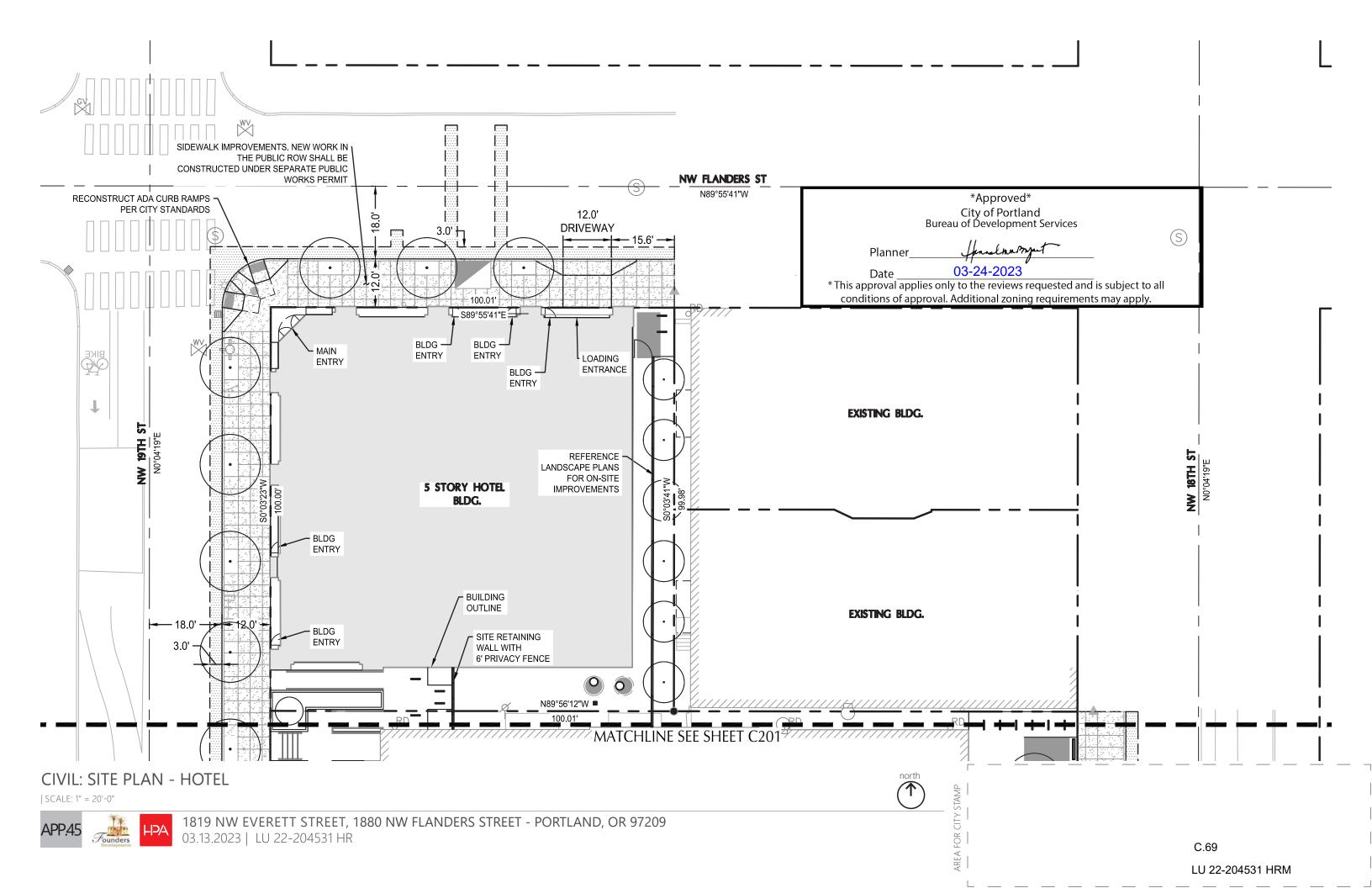


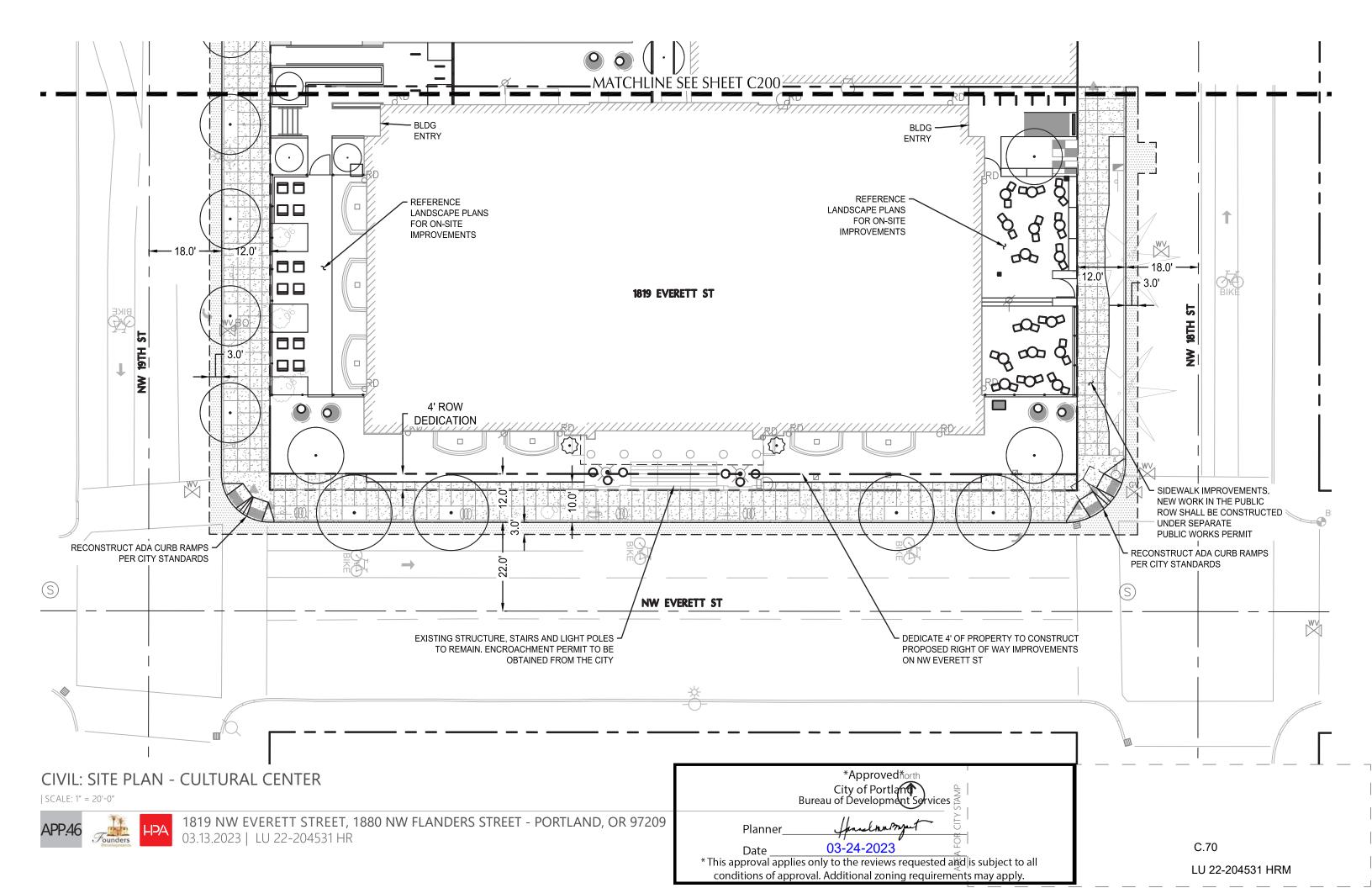
**1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209** 03.13.2023 | LU 22-204531 HR

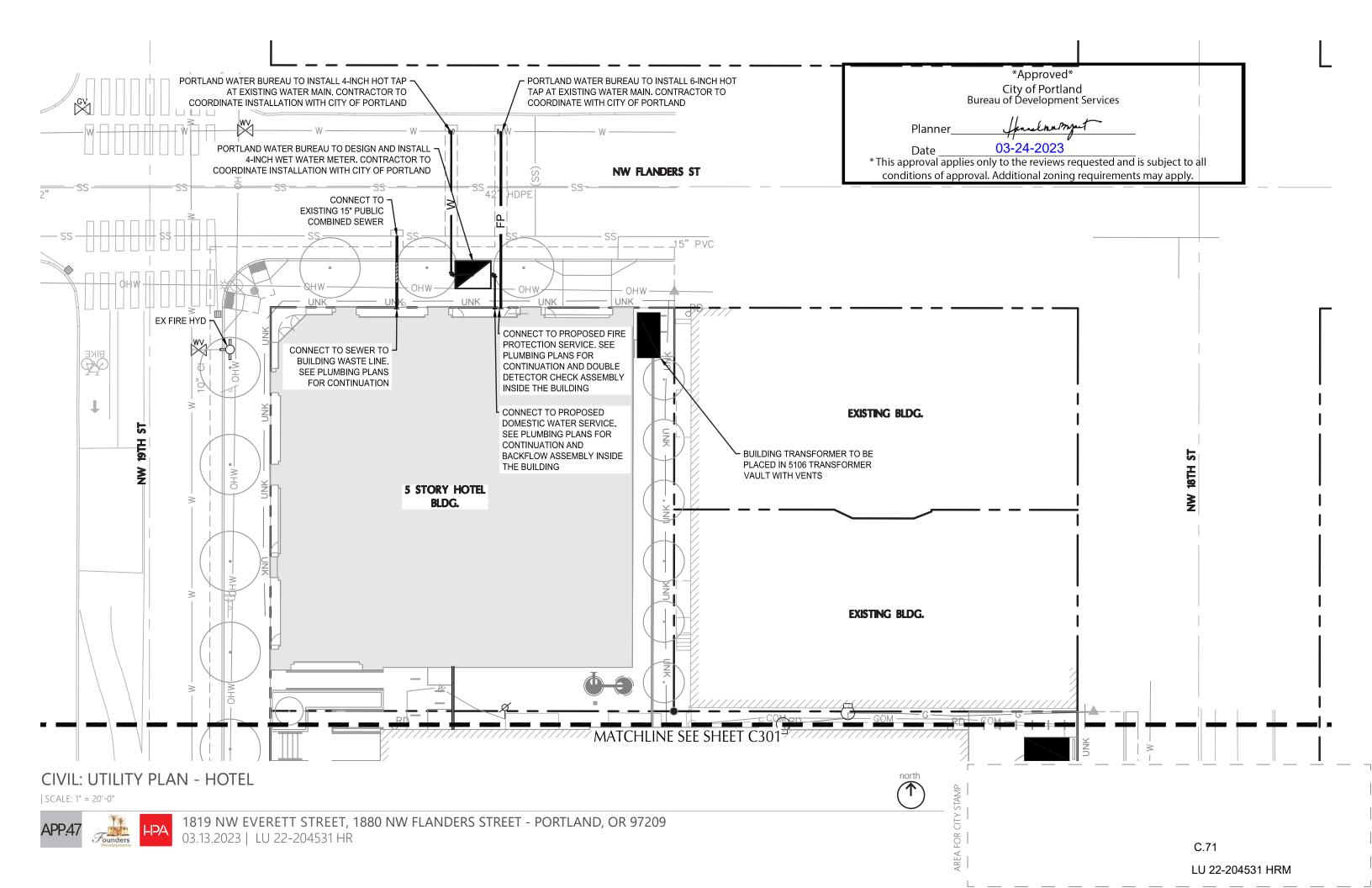
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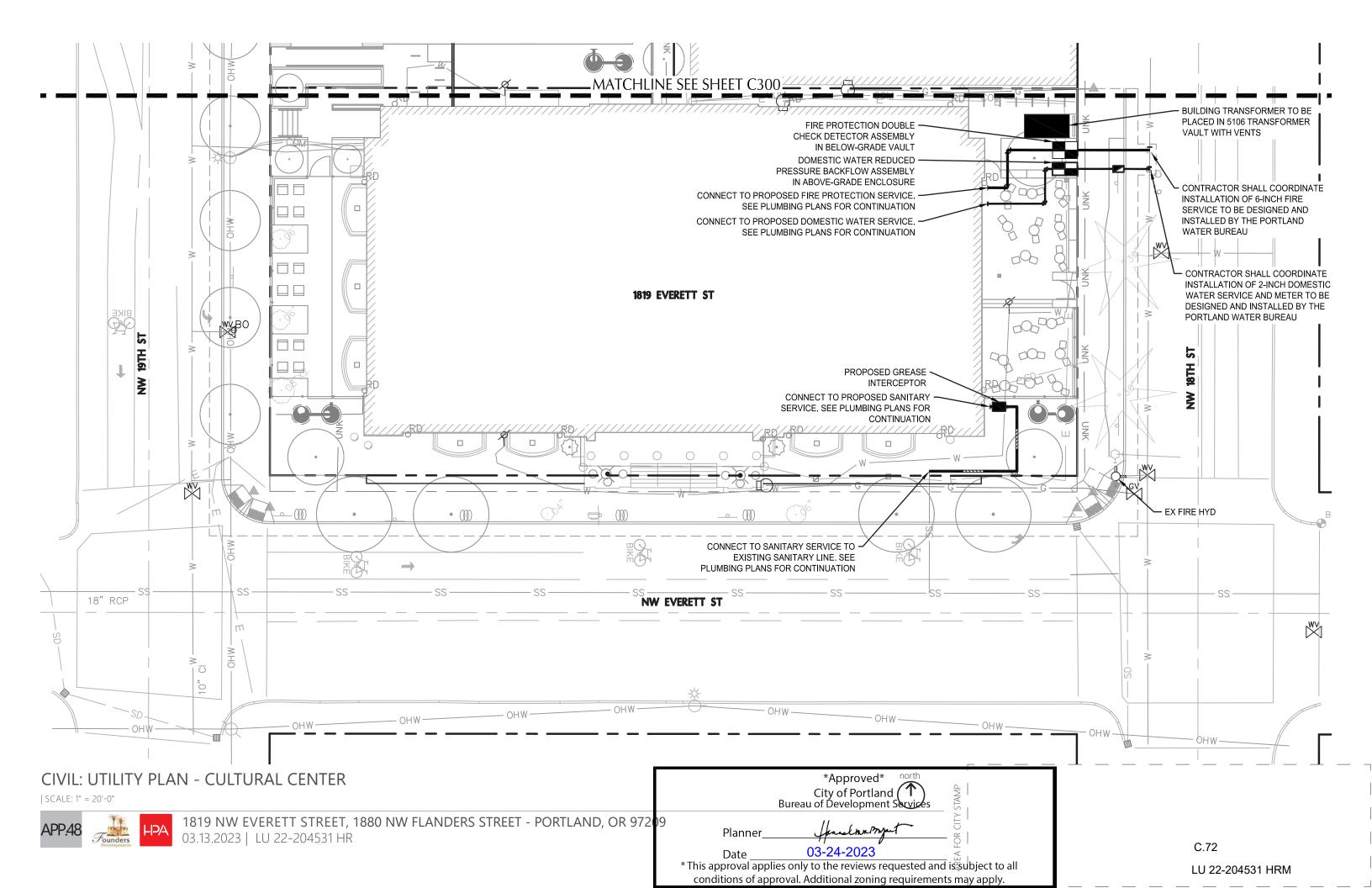


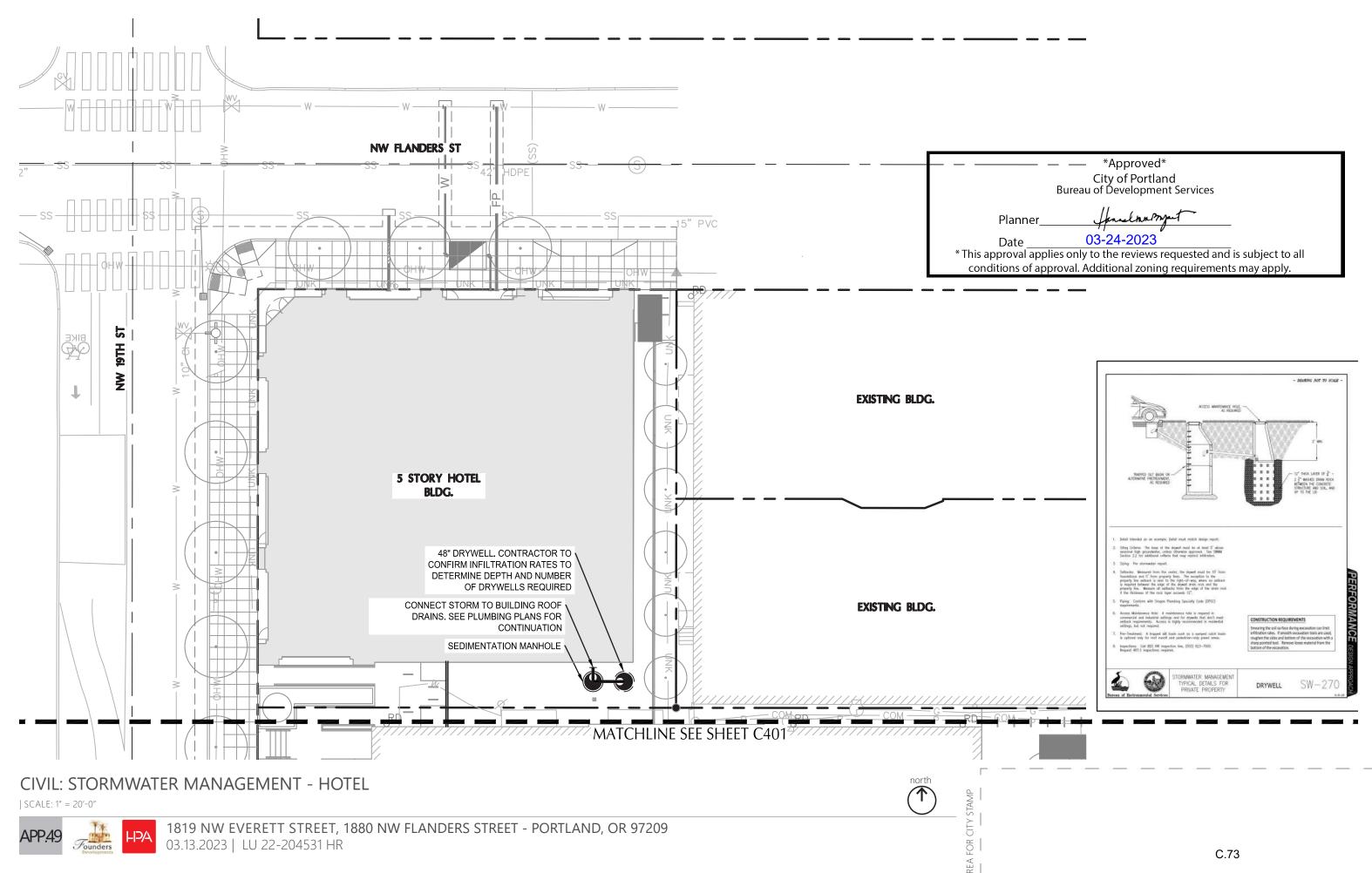




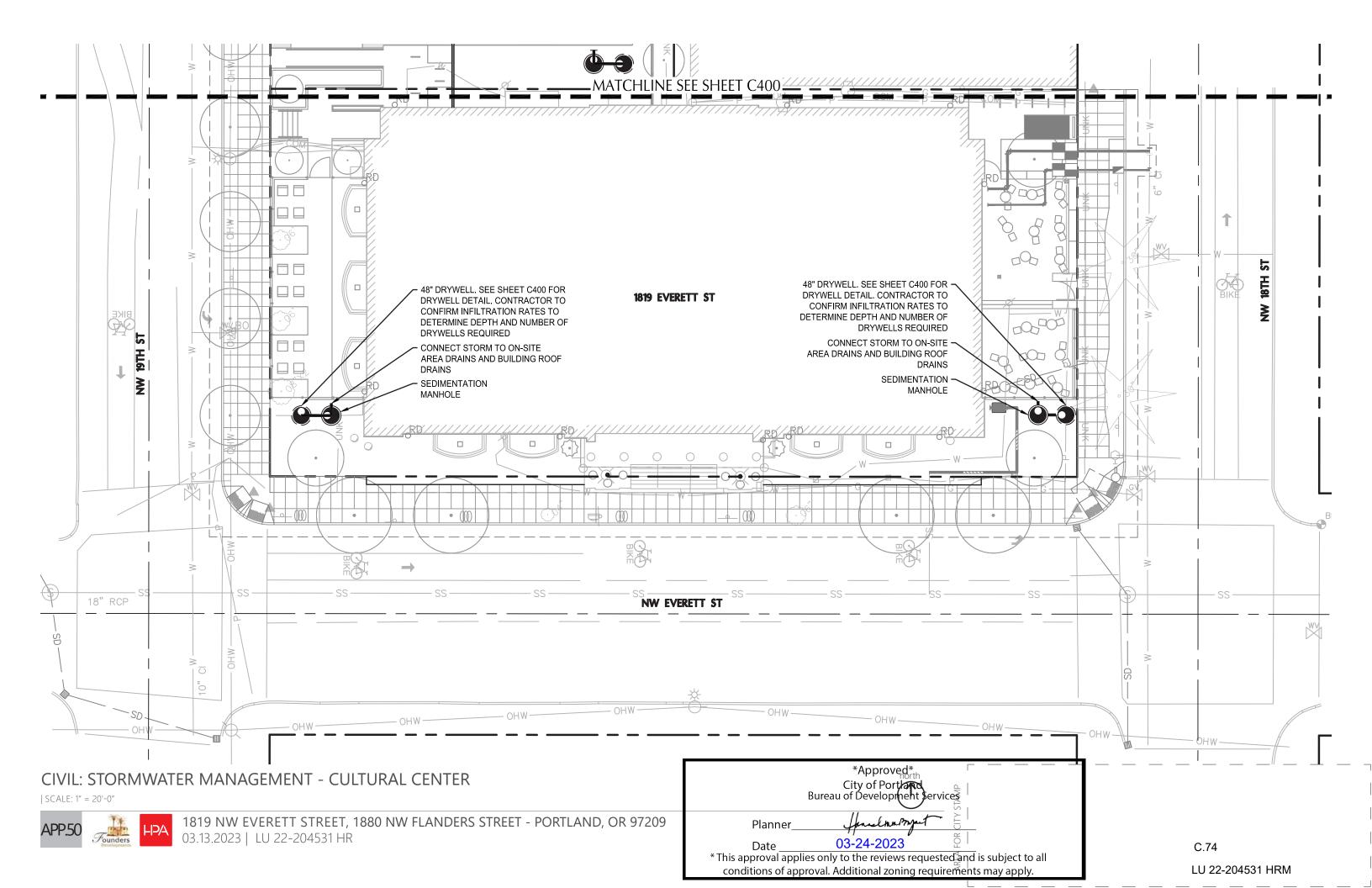








LU 22-204531 HRM





# City of Portland, Oregon Bureau of Development Services Land Use Services

FROM CONCEPT TO CONSTRUCTION

Carmen Rubio, Commissioner Rebecca Esau, Director Phone: (503) 823-7310 TTY: (503) 823-6868 www.portland.gov/bds

Date: January 24, 2022

From: Hannah Bryant, Land Use Services

503-865-6520 / Hannah.Bryant@portlandoregon.gov

#### **REQUEST FOR RESPONSE**

Case File: LU 22-204531 HRM AD

Pre App: PC # 22-128290

This notice is being sent to all service and technical review agencies for their input on the proposal described below. Neighborhood Associations also receive this advance notice via email. Your timely response, as indicated below, will help the assigned planner determine if applicable approval criteria can be met, or what conditions might be required.

- The approval criteria are listed below. Although we are interested in any comments you may have, please consider your response in terms of these criteria.
- All agencies are encouraged to use this as an opportunity to inform the applicant of any additional requirements that may be imposed by your agency during building permit phase especially those that would significantly affect the proposal.
- ➤ Please note in your response which requirements are specifically associated with the applicable land use review approval criteria, and which requirements you have the independent authority to impose at time of building permits.
- ➤ Neighborhood Associations are encouraged to submit comments by the deadline noted below. To comment, you may write to Hannah Bryant at 1900 SW Fourth Ave., Suite 4500, Portland, OR 97201. You can also e-mail your comments to me at my e-mail address identified above. After the staff report is published, please submit your comments to the Landmarks Commission at 1900 SW Fourth Ave., Suite 4500, Portland, OR 97201 and fax them to 503-823-5630.

The Bureau of Development Services recommendation will be published ten days before the scheduled hearing date. You will also receive a Notice of Public Hearing for this proposal, with hearing date and time confirmed, mailed twenty days prior to the hearing.

- Please send your response to BDS no later than: February 23, 2023 30 days after the date of this RFR (If I receive comments after this date, I may not have enough time to include them in the staff report).
- > We must publish our report by: March 3, 2023
- > A public hearing before the Landmarks Commission is tentatively scheduled for March 13, 2023; 1:30pm

**Applicant:** Andrew Becker | Hartshorne Plunkard Architecture

315 W. Walton St Chicago, IL 60610

abecker@hparchitecture.com

**Developer:** Tanya Toby | Gaia Ventures I, LLC

5821 SE Powell Blvd Portland, OR 97026

**Owner:** Northwest Neighborhood Cultural Center

PO Box 96116 Portland, OR 97296

Site Address: 1819 NW EVERETT ST

**Legal Description:** BLOCK 172 LOT 1-4, COUCHS ADD; BLOCK 172 LOT 6&7, COUCHS

ADD

**Tax Account No.:** R180215530, R180215610

**State ID No.:** 1N1E33DB 02800, 1N1E33DB 02900

Quarter Section: 3026

Neighborhood: Northwest District, contact Greg Theisen at

planningchair@northwestdistrictassociation.org

**Business District:** Northwest Portland, contact at nobhillportland@gmail.com. **District Coalition:** Neighbors West/Northwest, contact Darlene Urban Garrett at

darlene@nwnw.org

**Plan District:** Northwest

**Other Designations:** Historic Landmark – First Church of Christ Scientist

**Zoning:** CM2(MU-U)d – Commercial Mixed-Use 2 with a Design Overlay

**Case Type:** HRM AD – *Historic Resource with Modifications and Adjustment Review* **Procedure:** Type III, with a public hearing before the Landmarks Commission. The decision of the Landmarks Commission can be appealed to City Council.

#### Proposal:

The proposal includes two elements: First, the adaptive reuse of an existing three-story Historic Landmark to convert a former church into a private club with a hotel, restaurants, spa and bar. Second, the construction of a new, detached, six-story hotel with a ground floor restaurant and affordable commercial retail space on a vacant parcel within the Landmark boundary. One loading space is proposed in the new hotel. No parking is proposed.

#### Modification requests [PZC 33.846.070]:

- 1. Increase the building height from 60-feet to 69-feet 9-inches. (The 60-foot base building height is derived from a 45-foot base height plus 10-foot bonus for providing affordable commercial plus 5-foot for tall ground floors) (33.130.210.B.1, Table 130-2, Table 130-3);
- 2. Reduce the dimensions of a Standard A loading zone from 35-feet long by 10-feet wide by 13-feet high to 25-feet long by 9-feet wide by 10-feet high (33.266.310.D);
- 3. Increase the maximum distance short-term bike parking spaces may be from a main entrance from 50-feet to 160-feet (33.266.210.E.1.a); and

#### Adjustment request [PZC 33.805]:

- Reduce the quantity of short-term bike parking stalls from 19 to 17 (33.266.200.B, Table 266-6).
- Allow loading to be entered and exited in a rearward motion (33.266.310.F.1).

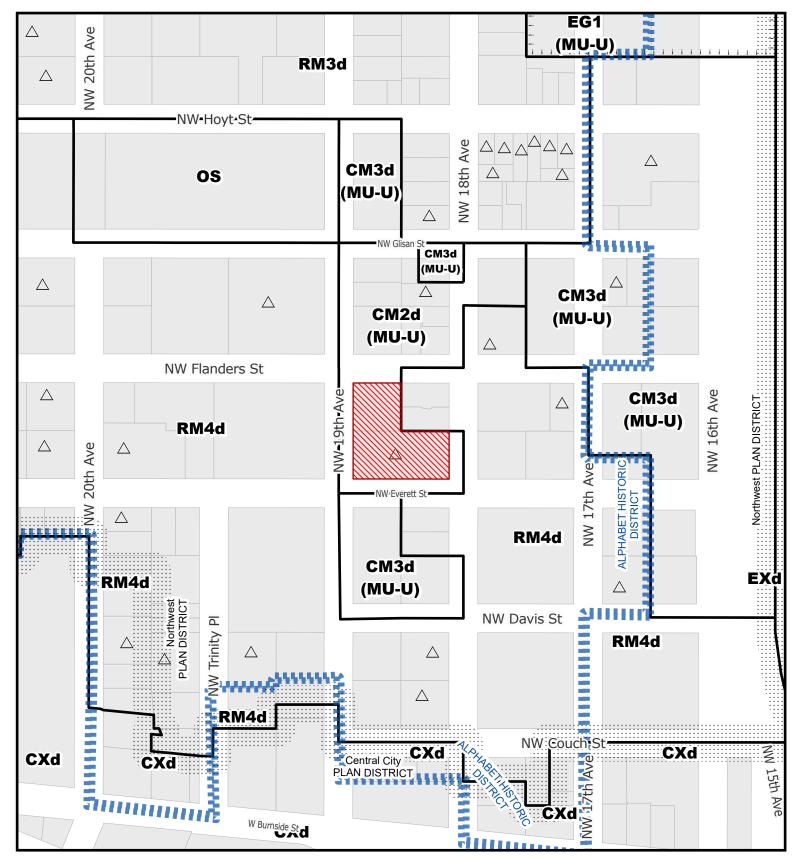
#### **Approval Criteria:**

In order to be approved, this proposal must comply with the approval criteria of Title 33, Portland Zoning Code. The applicable approval criteria are:

- Community Design Guidelines
- Alphabet Historic District Addendum to the Community Design Guidelines

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was filed, provided that the application is complete at the time of filing, or complete within 180 days. This application was filed on November 16, 2022 and determined to be complete on January 23, 2023.

Enclosures: Zoning Map, Site Plan

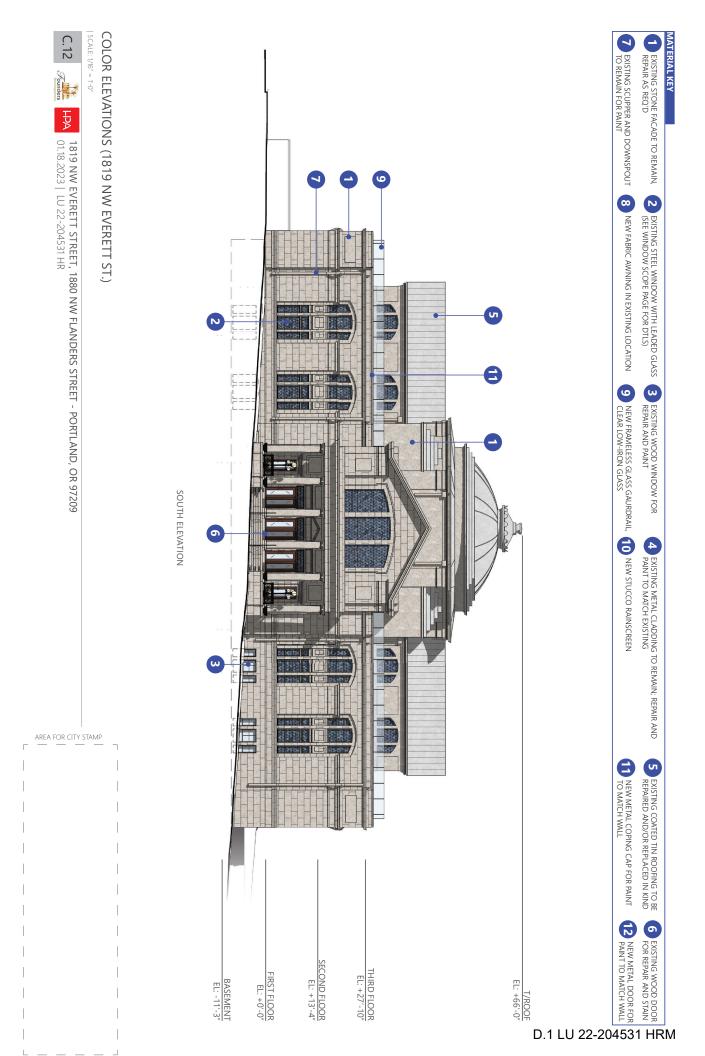


For Zoning Code in Effect Post October 1, 2022

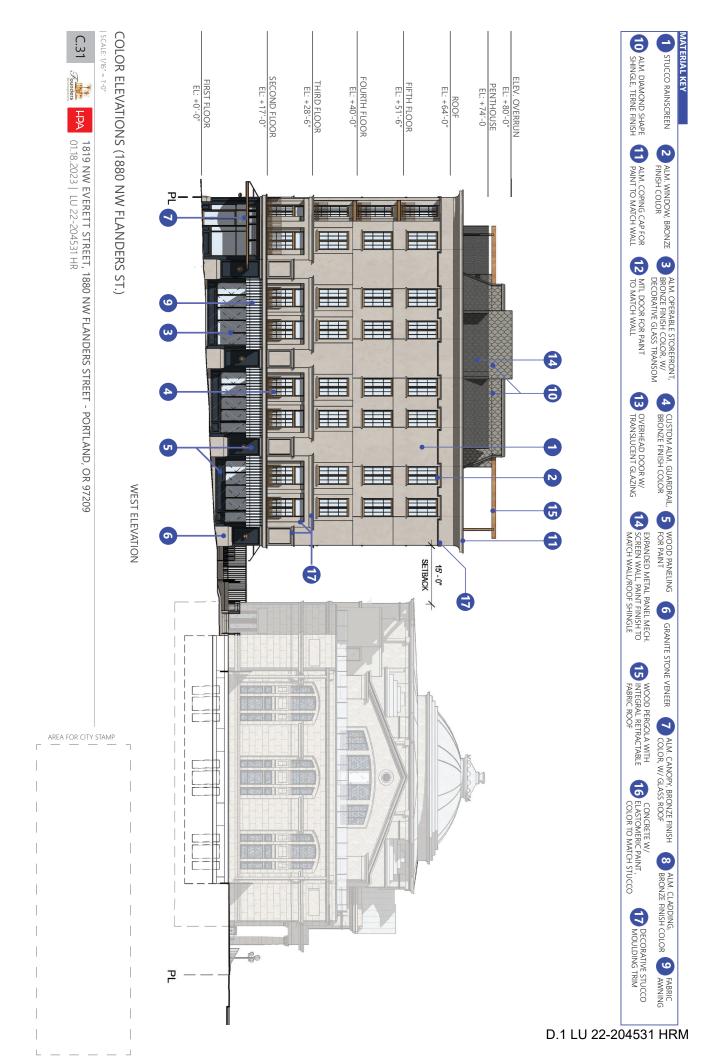


THIS SITE LIES WITHIN THE: NORTHWEST PLAN DISTRICT ALPHABET HISTORIC DISTRICT Site
Plan District
Historic District

Historic Landmark









# City of Portland, Oregon Bureau of Development Services Land Use Services

### FROM CONCEPT TO CONSTRUCTION

Carmen Rubio, Commissioner Rebecca Esau, Director Phone: (503) 823-7310 TTY: (503) 823-6868 www.portland.gov/bds

Date: February 8, 2023

To: Andrew Becker | Hartshorne Plunkard Architecture

315 W. Walton St Chicago, IL 60610 (312) 226-4488

From: Hannah Bryant

RE: LU 22-204531 HRM AD

Dear Andrew:

I have received your application for a Historic Resource Review w/Modifications AD at 1819 NW EVERETT ST. Your application was deemed complete on **January 23, 2023**. Your case number is given above; the hearing is scheduled for **March 13, 2023**. I am the planner handling your case, and can answer any questions you might have during the process.

The Zoning Code requires you to post notice on the site of your proposal 30 days before the hearing. The information below will help you do this. If you did not pick up poster boards from the Development Services Center when you filed your application, please contact me at least 24 hours in advance so the posting boards can be prepared for you to pick up. My phone number is (503) 865-6520. I am enclosing the notice that should be placed on the signs.

A. Your site has 100 feet of frontage on NW Flanders Street. You must post 1 signs along this street. There must be at least one sign every 600 feet.

Your site has 200 feet of frontage on NW 19th Avenue. You must post 1 signs along this street. There must be at least one sign every 600 feet.

Your site has 100 feet of frontage on NW 18th Avenue. You must post 1 signs along this street. There must be at least one sign every 600 feet.

Your site has 200 feet of frontage on NW Everett. You must post 1 signs along this street. There must be at least one sign every 600 feet.

- B. These signs must be placed within 10 feet of the street frontage line, and must be visible to pedestrians and motorists. You may <u>not</u> post in the public right-of-way.
- C. Because the hearing for your case is scheduled for **March 13, 2023**, you must post the notice by February 11, 2023, 30 days before the hearing.
- D. A certification statement is enclosed, which you must sign and return. The statement affirms that you posted the site. It also confirms your understanding that if you do not post the notice by the date above, your hearing will be automatically postponed. In addition, time limits on our processing of your case will be waived. You must return this statement to us by February 27, 2023, 14 days before the hearing.
- E. You should not remove the notice before the hearing, but it must be taken down within two weeks after the final decision is made on your request.

Encl:

Posting Notice Statement Certifying Posting

cc: Application Case File

315 W. Chicag	v Becker   Hartshorne Plunkard Arc Walton St o, IL 60610 26-4488	hitecture
DATE:		
TO:	Hannah Bryant Bureau of Development Services 1900 SW Fourth Ave., Suite 5000 Portland, Oregon 97201	
	APPLICANT'S STATEME	ENT CERTIFYING POSTING
	Case File l	LU 22-204531
unders		ny site as required by the Zoning Code. I or March 13, 2023, and that I was required to e hearing.
		the notices attached, were set up on e placed within 10 feet of the street frontage and motorists.
later the that if days be	nan February 27, 2023, 14 days befor I do not post the notices by 30 days	ed to the Bureau of Development Services no re the scheduled hearing. I also understand before the hearing, or return this form by 14 tomatically be postponed. I also understand for processing my case.
	ition, I understand that I may not read to remove them within two weeks	move the notices before the hearing, but am of the final decision on my request.
		Signature
		Print Name
		Address
		City/State/Zip Code

### **Type III Land Use Review**



### **1819 NW Everett Street**

WHEN	MONDAY, MARCH 13, 2023 @ 1:30 PM (hearing start time –see Historic Landmarks Commission agenda for estimated project start time)					
WHERE	ONLINE: Link to virtual hearing & info testify is available at					

**Questions? BDS Contact** 

Hannah Bryant, Planner / 503-865-6520 / Hannah.Bryant@PortlandOregon.gov Bureau of Development Services, 1900 SW 4th Ave, Suite 5000, Portland, OR 97201

Traducción e interpretación | Chuyển Ngữ hoặc Phiên Dịch | 翻译或传译 | Turjumida ama Fasiraadda | 翻訳または通訳 | ການແປພາສາ ຫຼື ການອະທິບາຍ Письменныйили устный перевод | Traducere sau Interpretare | 번역및통역 | الترجمة التحريرية أو الشفوية







503-823-7300 BDS@PortlandOregon.gov www.PortlandOregon.gov/bds/translated Relay Service: 711

Andrew Becker | Hartshorne Plunkard Architecture | 315 W. Walton St Chicago, IL 60610 (312) 226-4488

DATE: 02.13.23

TO: Hannah Bryant

Bureau of Development Services 1900 SW Fourth Ave., Suite 5000 Portland, Oregon 97201

#### APPLICANT'S STATEMENT CERTIFYING POSTING

#### Case File LU 22-204531

This certifies that I have posted notice on my site as required by the Zoning Code. I understand that the hearing is scheduled for March 13, 2023, and that I was required to post the property at least 30 days before the hearing.

The required number of poster boards, with the notices attached, were set up on <a href="Q2.10.23">Q2.10.23</a> (date). These were placed within 10 feet of the street frontage line so that they were visible to pedestrians and motorists.

I understand that this form must be returned to the Bureau of Development Services no later than February 27, 2023, 14 days before the scheduled hearing. I also understand that if I do not post the notices by 30 days before the hearing, or return this form by 14 days before the hearing, my hearing will automatically be postponed. I also understand this will result in a waiver of the time limits for processing my case.

In addition, I understand that I may not remove the notices before the hearing, but am required to remove them within two weeks of the final decision on my request.

Signature

Andrew Becker

315 W. Walton St.

Print Name

A 1 1

Address

Chicago, IL 60610

City/State/Zip Code





# City of Portland, Oregon Bureau of Development Services Land Use Services

FROM CONCEPT TO CONSTRUCTION

Carmen Rubio, Commissioner Rebecca Esau, Director Phone: (503) 823-7310 TTY: (503) 823-6868 www.portland.gov/bds

**Date:** February 21, 2023 **To:** Interested Person

**From:** Hannah Bryant, Land Use Services

503-865-6520 / Hannah.Bryant@portlandoregon.gov

## NOTICE OF A PUBLIC HEARING ON A PROPOSAL IN YOUR NEIGHBORHOOD

CASE FILE: LU 22-204531 HRM AD

PC # 22-128290

REVIEW BY: Landmarks Commission WHEN: March 13, 2023; 1:30pm

Remote Access: Historic Landmarks Commission Agenda

https://www.portlandoregon.gov/bds/HLCagenda

# This land use hearing will be limited to remote participation via **Zoom**. Please refer to the instructions included with this notice to observe and participate remotely.

Development has been proposed in your neighborhood requiring a land use review. The proposal, review process, and information on how to respond to this notice are described below. A copy of the site plan and zoning map are attached. I am the staff person handling the case. Please call me if you have questions regarding this proposal. Please contact the applicant if you have questions regarding any future development on the site.

**Applicant:** Andrew Becker | Hartshorne Plunkard Architecture

315 W. Walton St Chicago, IL 60610

abecker@hparchitecture.com

**Developer:** Tanya Toby | Gaia Ventures I, LLC

5821 SE Powell Blvd Portland, OR 97026

**Owner:** Northwest Neighborhood Cultural Center

PO Box 96116 Portland, OR 97296

Site Address: 1819 NW EVERETT ST

**Legal Description:** BLOCK 172 LOT 1-4, COUCHS ADD; BLOCK 172 LOT 6&7, COUCHS ADD

**Tax Account No.:** R180215530, R180215610

**State ID No.:** 1N1E33DB 02800, 1N1E33DB 02900

Quarter Section: 3026

**Neighborhood:** Northwest District, contact Greg Theisen at

planningchair@northwestdistrictassociation.org

**Business District:** Northwest Portland, contact at <a href="nobhillportland@gmail.com">nobhillportland@gmail.com</a> **District Coalition:** Neighbors West/Northwest, contact Darlene Urban Garrett at

darlene@nwnw.org

**Plan District:** Northwest

**Other Designations:** Historic Landmark – First Church of Christ Scientist

**Zoning:** CM2(MU-U)d – Commercial Mixed-Use 2 with a Design Overlay **Case Type:** HR – Historic Resource Review with an Adjustment Review

**Procedure:** Type III, with a public hearing before the Landmarks Commission. The

decision of the Landmarks Commission can be appealed to City Council.

#### Proposal:

Type III Historic Resource Review to convert the former Landmark church into a club with restaurants, spa and bar, and construction of a new, detached, six-story hotel on a vacant parcel within the Landmark boundary. One loading space and no parking is proposed in the hotel. Three Modifications and two Adjustments are requested: to increase building height; reduce the dimensions of the loading space; increase the distance between short-term bike parking and main entrances; reduce the quantity of short-term bike parking, and allow loading to be entered/exited in a rearward motion.

Historic Resource Review is required for non-exempt exterior alterations and non-exempt development in a Historic District.

#### Relevant Approval Criteria:

In order to be approved, this proposal must comply with the criteria of Title 33. The relevant criteria are:

- Community Design Guidelines
- 33.825.040 Modifications That Better Meet Design Guidelines
- Alphabet Historic District Addendum to the Community Design Guidelines
- 33.805 Adjustment Review Approval Criteria

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. This application was submitted on November 16, 2022 and determined to be complete on January 23, 2023.

#### **DECISION MAKING PROCESS**

The Bureau of Development Services will be making a recommendation on this proposal; our report and recommendation will be available 10 days before the hearing. The Staff report will be posted on the Bureau of Development Services website at

http://www.portlandoregon.gov/bds/35625 . Land use review notices are listed on the website by the District Coalition in which the site is located; the District Coalition for this site is identified at the beginning of this notice. If you are interested in viewing the file, please contact the planner listed on the front of this notice. The planner can provide information over the phone or via email. Only digital copies of the material in the file are available for viewing. Copies of all information in the file can be obtained for a fee equal to the cost of services. Additional information about the City of Portland, city bureaus, and a digital copy of the Portland Zoning Code is available on the City's website at <a href="http://www.portlandoregon.gov">http://www.portlandoregon.gov</a>.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be extended at the request of the applicant.

We are seeking your comments on this proposal. To comment, you may write or testify at the remote hearing. Please refer to the file number when seeking information or submitting testimony. In your comments, you must address the approval criteria as stated in the administrative report and decision which you previously received. Please note that all correspondence and testimony received will become part of the public record.

Written comments must be received by the close of the record and should include the case file number. Any new written testimony should be emailed to Hannah Bryant at Hannah.Bryant@portlandoregon.gov.

**Please note regarding USPS mail:** If you choose to mail written testimony via USPS, due to the Covid-19 Emergency, USPS mail is only received a couple times a week, and testimony must be received before the close of the record. Therefore, please mail testimony well in advance of the hearing date. Thank you for any information you can provide regarding this case. Note: If you have already written, it is not necessary to write again; your correspondence will be given to the Landmarks Commission.

If you plan to testify at the hearing, please refer to instructions included with this notice.

#### **APPEAL PROCESS**

You can appeal the decision of the Landmarks Commission to the City Council. If appealed, City Council will hold an evidentiary hearing. New evidence can be submitted to the City Council in the event of an appeal of an evidentiary hearing.

A fee is charged for appeals. Recognized neighborhood associations may qualify for an appeal fee waiver. The City Council's decision may be appealed to the Oregon Land Use Board of Appeals (LUBA).

Failure to raise an issue in person or in writing by the close of the record at or following the final evidentiary hearing on this case may preclude an appeal to LUBA on that issue. Also, if you do not provide enough detailed information to the Landmarks Commission, they may not be able to respond to the issue you are trying to raise. In such a situation an appeal to LUBA based on that issue may not be allowed.

#### **HEARING CANCELLATION**

This public hearing will be cancelled due the inclement weather or other similar emergency.

- Hearings Officer: This public hearing will be cancelled if the City of Portland is closed. Check local television or the City of Portland website (<a href="www.portlandoregon.gov">www.portlandoregon.gov</a>) for closures. Contact the Hearings Office at 503-823-7307, for immediate information regarding cancellations or rescheduling.
- Design Commission, Historic Landmarks Commission or Adjustment Committee: This public hearing will be cancelled if Portland Public Schools close due to inclement weather or other similar emergency. Check local television and radio reports for school closures. Please call the Bureau of Development Services at 503-823-7617, for information regarding cancellations or rescheduling.

The hearing will be rescheduled for the earliest possible date. A renotification notice will not be sent.

To attend the hearing, public transportation is available. Tri-Met buses stop near the BDS building at SW Fifth or Sixth Ave. at SW Hall St. and SW Harrison St. Call Tri-Met at 503-238-

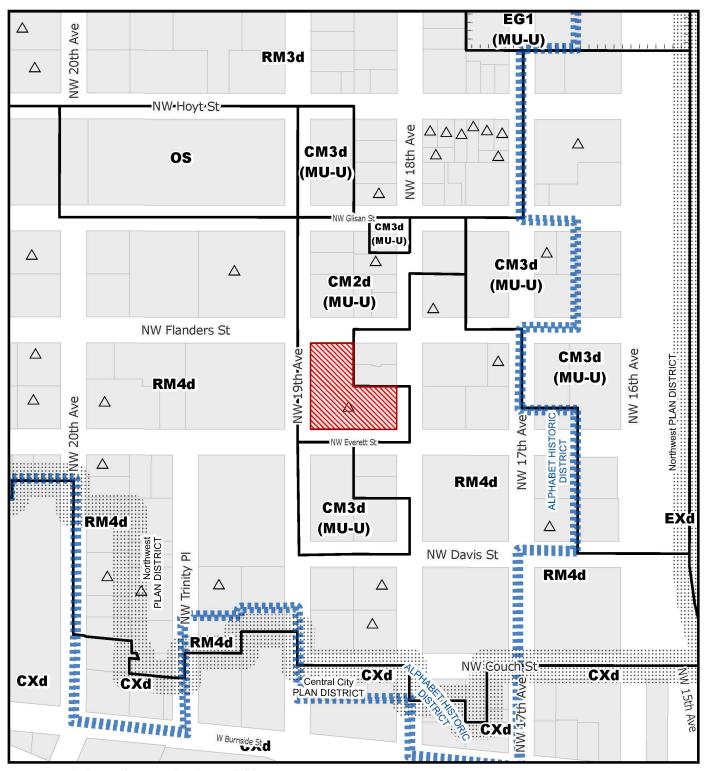
7433 (or www.trimet.org) for routes and times. Hourly rated public parking is available a half block south of the building on Fourth Ave.

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

#### **Enclosures:**

Zoning Map Site Plan

Instructions for Observing or Testifying at the Historic Landmarks Commission Webinar Hearings



For Zoning Code in Effect Post October 1, 2022

ZONING 🏺

THIS SITE LIES WITHIN THE: NORTHWEST PLAN DISTRICT ALPHABET HISTORIC DISTRICT Site

Plan District

Historic District

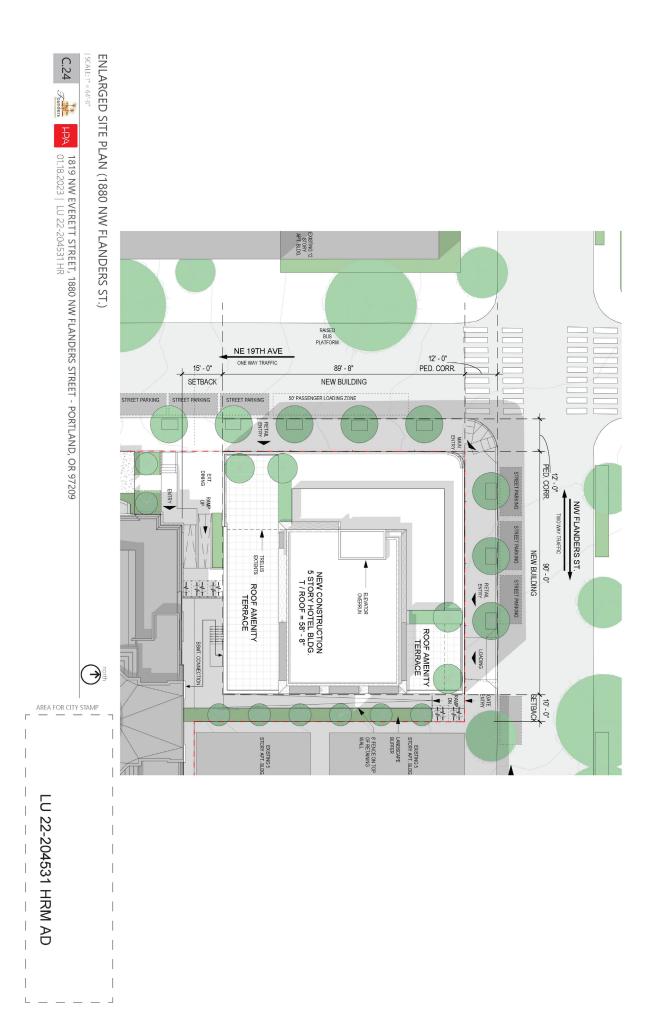
A Historic Landmark

File No. LU 22 - 204531 HRM AD

1/4 Section Scale 1 inch = 200 feet

State ID 1N1E33DB 2800

Exhibit B Jan 23, 2023



### Observing or Testifying at the Portland Design Commission, Historic Landmarks Commission, or Adjustment Committee Webinar Hearings

Thank you for your interest in attending a land use public hearing. All hearings are currently held virtually, via Zoom. The information below will help you get connected.

\*\*\*If you do not have access to the internet from a home computer or mobile phone, please see the end of this document for instructions on how to participate from a City building at 1900 SW 4th Avenue in downtown Portland.

#### Preparing for the Hearing:

- 1. To access the Zoom Webinar, please go to the online hearing Agenda, and click the link under the hearing date you are interested in participating: <a href="https://www.portlandoregon.gov/bds/42441">https://www.portlandoregon.gov/bds/42441</a>
- 2. In advance of the hearing, please review documents and drawings in the project link within the Online Agenda.
  - Please also provide comments to the planner assigned in advance of the hearing.

#### Getting into the Hearing [Registering in Zoom to observe or participate in Hearing]:

- 1. In order to observe or testify in the hearing, please be sure to Register for the Webinar as soon as possible.
  - The Webinar Link is posted to the Online Agenda typically one week prior to the hearing date.
- 2. Once you register you will receive an email notification of how to log-in or access the Webinar.
- 3. You can enter the Webinar no sooner than ten minutes before the start of the hearing.
- 4. You will be held in the Zoom waiting room until the Webinar begins. (Please note each individual agenda item has an <u>estimated</u> start time.)
- 5. If using a smartphone or tablet, download the Zoom app for easy entry into the Webinar.

#### Public participation in the Hearing:

- 1. After Staff and Applicant presentations, the Chair will announce public testimony is open, and will ask if anyone else would like to testify.
- 2. You can provide public comment in this Webinar in several ways:
  - If during registration you indicated you would like to testify, we will put your name in order of request.

    Once in the hearing, testifiers will be renamed "Testifier 1 (Your Name)"
  - Members of the public will be automatically muted except for when they are called by the Hearings Clerk for their public comment. During the Webinar, the Hearings Clerk will promote participants to "Panelists" in the order of Webinar Registrations received. When it is your turn to provide testimony, please accept the Clerk's invitation to be promoted to Panelist.
  - If you indicated in your registration that you did not want to testify but later changed your mind, when testimony is open:
    - Click the "raise your hand" function in ZOOM, and the Hearings Clerk will add you to the list of testifiers
    - If you will be participating by call-in, raise your hand by pressing \*9 the Webinar host will see this
      notification.
  - When you are moved to Panelist position for your testimony, your name will be announced by the Chair or Hearings Clerk. Please be prepared to provide testimony.
  - Each testifier is allotted 2 minutes of testimony unless the Chair grants additional time.
  - Please manage your time when testifying. As a courtesy to other testifiers and our volunteer Commissions,
     please do not attempt to exceed the allotted amount of time.
- 3. We will enable screen sharing of presentations only for Design and Landmarks Commission members, project teams, and staff participating in the Webinar.
- 4. Testifiers who engage in inappropriate behavior or language will be promptly removed from the hearing.

#### Follow-up:

1. The Webinar will be recorded and uploaded to the City of Portland Auditors website, under the Case File Number, here: <a href="https://efiles.portlandoregon.gov/Search">https://efiles.portlandoregon.gov/Search</a>.

\*\*\*If you do not have access to the internet from a home computer or mobile phone, we can provide accommodations for you to view a live video display of the hearing from 1900 SW 4<sup>th</sup> Avenue in downtown Portland. This option for participation requires you to travel to a City building where we will provide a computer for viewing the hearing. City staff will not be present at this viewing location. If you require such accommodation, please contact the BDS Hearings Clerk at 503-865-6525 before 8 AM on the day of the hearing.\*\*\*

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## RETURN SERVICE REQUESTED 1N1833DB 600 THRIVE AESTHETIC 6 ANTI-AGINC CENTER LLC 11850 SW 67TH AVE \$110 PORTLAND OR 97223-896   5 RETURN SERVICE REQUESTED 1N1833DB 600 LAW OFFICES OF DEVIN ROBINSON PC 900 WASHINGTON ST \$1.000 VANCOUVER WA 98660-34   47 RETURN SERVICE REQUESTED 1N1833DB 600 LAW OFFICES OF DEVIN ROBINSON PC 900 WASHINGTON ST \$1.000 VANCOUVER WA 98660-34   48 RETURN SERVICE REQUESTED 1N1833DB 600 LAW OFFICES OF DEVIN ROBINSON PC 900 WASHINGTON ST \$1.000 VANCOUVER WA 98660-34   48 RETURN SERVICE REQUESTED 1N183DB 600 LON SALION LLC 1766 NW GLISER MS T PORTLAND OR 97209   49 RETURN SERVICE REQUESTED 1N183DB 60001 PRICE AISLINN E 10.000 WICKERSHAM CONDO OWNERS ASSN 12503 SE MILL PLAIN BLVD \$2.60 VANCOUVER WA 98684-40   50 RETURN SERVICE REQUESTED 1N183DB 60001 PRICE AISLINN E 400 NW 18TH AVE \$4007 PORTLAND OR 97209-223   51 RETURN SERVICE REQUESTED 1N183DB 60002 JAZO LLC PO BOX 2654 JACKSON WY 83001   52 RETURN SERVICE REQUESTED 1N183DB 60003 PEISCHMAN STEVEN W TR \$ HEATH L STRCEY TR 410 NW 18TH AVE \$101 PORTLAND OR 97209-223   53 RETURN SERVICE REQUESTED 1N183DB 60005 DUNAGAN JEANNI 410 NW 18TH AVE \$101 PORTLAND OR 972107-707   54 RETURN SERVICE REQUESTED 1N183DB 60006 TOOZE CHARTSSE M 410 NW 18TH AVE \$103 PORTLAND OR 972107-707   55 RETURN SERVICE REQUESTED 1N183DB 60006 TOOZE CHARTSSE M 410 NW 18TH AVE \$103 PORTLAND OR 97209   56 RETURN SERVICE REQUESTED 1N183DB 60006 TOOZE CHARTSSE M 410 NW 18TH AVE \$103 PORTLAND OR 97209   56 RETURN SERVICE REQUESTED 1N183DB 60006 TOOZE CHARTSSE M 410 NW 18TH AVE \$103 PORTLAND OR 97209   57 RETURN SERVICE REQUESTED 1N183DB 60001 WERRETTZ GEORGE & GRRETT DIAN L 15 CAMINO ALTO VALLEJO CA 94590   56 RETURN SERVICE REQUESTED 1N183DB 60010 WERRETTZ GEORGE & GRRETT DIAN L 15 CAMINO ALTO VALLEJO CA 94590   56 RETURN SERVICE REQUESTED 1N183DB 60011 STOUT MICHAEL H & STOUT REGUESTED 1N183DB 60012 COLEMAN ANTHONY TR & SHAH SHILPA TR 410 NW 18TH AVE \$303 PORTLAND OR 97209   56 RETURN SERVICE REQUESTED 1N183DB 60014 CANADAY MARGOT & SPECTOR RACHL 10 NW 18TH AVE \$303 PORTLA	-1
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## RETURN SERVICE REQUESTED  INLESSIDE 600  LAW OFFICES OF DEVIN ROBINSON PC  900 WASHINGTON ST \$1000  VANCOUVER WA 98660-34  ## RETURN SERVICE REQUESTED  1NLESSIDE 600  INLESSIDE 600  INLESSIDE 600  INLESSIDE 600  INLESSIDE 600  INLESSIDE 600  RETURN SERVICE REQUESTED  1NLESSIDE 6000  MICKERSHAM CONDO OWNERS ASSN  1250 SE MILL PLAIN BLVD \$260  VANCOUVER WA 98680-34  ## AUTORITY WAS SERVICE REQUESTED  1NLESSIDE 60000  MICKERSHAM CONDO OWNERS ASSN  1250 SE MILL PLAIN BLVD \$260  VANCOUVER WA 98680-34  ## AUTORITY WAS SERVICE REQUESTED  1NLESSIDE 60001  INLESSIDE 60001  INLESSIDE 60001  INLESSIDE 60001  INLESSIDE 60002  JAZO LLC  PO BOX 2554  JACKSON WW 83001  52 RETURN SERVICE REQUESTED  1NLESSIDE 60004  HEATH L STACKY TR  ## AUTORITY WAS \$4007  FORTLAND OR 97209-223  ## AUTORITY WAS \$4007  ## A	
## RETURN SERVICE REQUESTED    NIE33DB 6000   ICON SALON LLC   1706 NW GLISAN ST   PORTLAND OR 97209	455
RETURN SERVICE REQUESTED	
SO RETURN SERVICE REQUESTED	
ST RETURN SERVICE REQUESTED	
S2 RETURN SERVICE REQUESTED	30
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ST RETURN SERVICE REQUESTED	
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SPETURN SERVICE REQUESTED	
60 RETURN SERVICE REQUESTED	
62 RETURN SERVICE REQUESTED  1N1E33DB 60013  SOUTHWELL PRISCILLA  2740 FAIRMOUNT BLVD  EUGENE OR 97403  63 RETURN SERVICE REQUESTED  1N1E33DB 60014  CANADAY MARGOT & SPECTOR RACHEL  6922 PRINCE GEORGES AVE  TAKOMA PARK MD 20912-  64 RETURN SERVICE REQUESTED  1N1E33DB 60015  LEA DAVID & LEA SHANNON  10330 HYLA AVE NE  BAINBRIDGE ISLAND WA  65 RETURN SERVICE REQUESTED  1N1E33DB 60016  MIZE MARY C  410 NW 18TH AVE #501  PORTLAND OR 97209	
63 RETURN SERVICE REQUESTED 1N1E33DB 60014 CANADAY MARGOT & SPECTOR RACHEL 6922 PRINCE GEORGES AVE TAKOMA PARK MD 20912-64 RETURN SERVICE REQUESTED 1N1E33DB 60015 LEA DAVID & LEA SHANNON 10330 HYLA AVE NE BAINBRIDGE ISLAND WA 55 RETURN SERVICE REQUESTED 1N1E33DB 60016 MIZE MARY C 410 NW 18TH AVE \$501 PORTLAND OR 97209	
64         RETURN SERVICE REQUESTED         1N1E33DB         60015         LEA DAVID & LEA SHANNON         10330 HYLA AVE NE         BAINBRIDGE ISLAND WA           65         RETURN SERVICE REQUESTED         1N1E33DB         60016         MIZE MARY C         410 NW 18TH AVE #501         PORTLAND OR 97209	
65 RETURN SERVICE REQUESTED 1N1E33DB 60016 MIZE MARY C 410 NW 18TH AVE #501 PORTLAND OR 97209	
	98110
69 RETURN SERVICE REQUESTED   1N1E33DB 700   MORLD CUP COFFEE & TEA SERVICE CO 920 NB 58TH AVE #100 FORTLAND OR 97213	
TO RETURN SERVICE REQUESTED INTESSUE NO.000 ASSOCIATION OF UNIT UNNERS OF GEORGE H WILLIAMS CONDO 153 NW 16TH AVE #1 PORTLAND OR 97209  THE RETURN SERVICE REQUESTED 1NIESSUE REQUESTED 1NIESSUE 70001 LEIVA DIANE M 153 NW 16TH AVE #1 PORTLAND OR 97209	
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TA RETURN SERVICE REQUESTED 111833B 70003 BEVELY CARSON TR 133 NW 18TH AVE #3 PORTLAND OR 97209	

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74	RETURN	SERVICE REQUESTED	b	1N1E33DB 70004	NORONA MARCELO & NORONA CONSTANCE	8926 NEVADA AVE	WEST HILLS CA 91304
		SERVICE REQUESTED	1N1E33DB 70005	PAPANDREW TARA & JOHNSON DUANE &	PAPANDREW WILLA TR	133 NW 18TH AVE #5	PORTLAND OR 97209
76	RETURN	SERVICE REQUESTED		1N1E33DB 70006	STELLNER RICHARD C	133 NW 18TH AVE #6	PORTLAND OR 97209
77	RETURN	SERVICE REQUESTED		1N1E33DB 70007	DOERSCHLAG MARTIN & JULIE	1318 28TH ST NW	WASHINGTON DC 20007
78	RETURN	SERVICE REQUESTED		1N1E33DB 70008	KREOFSKY LAURA & LEAHY DEBORAH	133 NW 18TH AVE #8	PORTLAND OR 97209
79	RETURN	SERVICE REQUESTED	1N1E33DB 70009	RUTENBERG ROBERT I TR &	BERKOWITZ KAREN A TR	133 NW 18TH AVE #9	PORTLAND OR 97209-2149
80	RETURN	SERVICE REQUESTED		1N1E33DB 70010	RICHARDS MATTHEW & RICHARDS KATHRYN	133 NW 18TH AVE #10	PORTLAND OR 97209
		SERVICE REQUESTED		1N1E33DB 7700	TRINITY EPISCOPAL CHURCH	147 NW 19TH AVE	PORTLAND OR 97209
		SERVICE REQUESTED		1N1E33DB 7800	BELLE COURT WI LLC	4250 SW COUNCIL CREST DR	PORTLAND OR 97239
				ASSOCIATION OF UNIT OWNERS OF	SUTTON PLACE CONDO	522 NW 23RD AVE #200	PORTLAND OR 97210
		SERVICE REQUESTED		1N1E33DB 80001	BRANAM ARON	1810 NW EVERETT ST #101	PORTLAND OR 97209-2171
		SERVICE REQUESTED SERVICE REQUESTED		1N1E33DB 80002 1N1E33DB 80003	ZEMPEL CLARE W & ZEMPEL JUDITH A ROME SHERILYN F	7516 N BOYD WAY 1810 NW EVERETT ST #103	FOX POINT WI 53217 PORTLAND OR 97209
		SERVICE REQUESTED		1N1E33DB 80004	YOSHIMURA SARAH	1810 NW EVERETT ST #103 1810 NW EVERETT ST #104	PORTLAND OR 97209 PORTLAND OR 97209
		SERVICE REQUESTED		1N1E33DB 80005	ROONEY SARA	1810 NW EVERETT ST #201	PORTLAND OR 97209-2171
		SERVICE REQUESTED		1N1E33DB 80006	CARTER ERIN E	1810 NW EVERETT ST #202	PORTLAND OR 97209
		SERVICE REQUESTED		1N1E33DB 80007	SEDANO JOSE A	1810 NW EVERETT ST #203	PORTLAND OR 97209
91	RETURN	SERVICE REQUESTED		1N1E33DB 80008	BERRY MICHAEL & KNAPP HALEY	1810 NW EVERETT ST #204	PORTLAND OR 97209-2171
92	RETURN	SERVICE REQUESTED		1N1E33DB 80009	JOHNSON JERED & DAVIDSON VANDA P	1810 NW EVERETT ST #301	PORTLAND OR 97209
93	RETURN	SERVICE REQUESTED		1N1E33DB 80010	ROCKWOOD CRAIG A	1810 NW EVERETT ST #302	PORTLAND OR 97209
94	RETURN	SERVICE REQUESTED		1N1E33DB 80011	ALLEN STEPHEN W	PO BOX 10472	PORTLAND OR 97296-0472
95	RETURN	SERVICE REQUESTED		1N1E33DB 80012	POLLOCK AARON	100 FOUNTAIN ST #5B	PROVIDENCE RI 02903-1845
96	KETURN	SERVICE REQUESTED SERVICE REQUESTED		BARR CORBETT & 1N1E33DB 900	HAGGENJOS BARR JESSALYN CALLAHAN ROBERT A	1956 NW EVERETT ST 405 NW 18TH AVE	PORTLAND OR 97209 PORTLAND OR 97209-2217
		SERVICE REQUESTED SERVICE REQUESTED		1N1E33DB 9000 1N1E33DB 9000	CALLAHAN ROBERT A 1962 EVERETT LLC	405 NW 18TH AVE 2611 NW WESTOVER RD	PORTLAND OR 97209-2217 PORTLAND OR 97210
		SERVICE REQUESTED		VICTORIAN HOUSE CONDOMINIUM	UNIT OWNERS ASSOCIATION	2011 NW WESTOVER RD 221 NW 18TH AVE	PORTLAND OR 97210 PORTLAND OR 97209-2111
		SERVICE REQUESTED		1N1E33DB 90001	HIRSCHFELD NEAL & PARKER JANET K	225 NW 18TH AVE	PORTLAND OR 97209-2111
		SERVICE REQUESTED		1N1E33DB 90002	FORESMAN JENNIFER & FORESMAN AARON	227 NW 18TH AVE	PORTLAND OR 97209-2111
		SERVICE REQUESTED		1N1E33DB 90003	LAAKSO ROBIN L	223 NW 18TH AVE	PORTLAND OR 97209
103	RETURN	SERVICE REQUESTED		1N1E33DB 90004	HARPER DENNIS M	221 NW 18TH AVE	PORTLAND OR 97209-2111
		SERVICE REQUESTED	1N1E33DB 99000	CARLTON COURT CONDOMINIUMS	OWNERS' ASSOCIATION	9450 SW GEMINI DR PMB 18251	BEAVERTON OR 97008-7105
		SERVICE REQUESTED		1N1E33DB 99001	SCOTT CONNOR & ROZMAN ELIZABETH	21830 VOLIN PL	HOUGHTON MI 49931-1061
		SERVICE REQUESTED		1N1E33DB 99002	HILL CARISSA J	1631 NW EVERETT ST #101	PORTLAND OR 97209
107	RETURN	SERVICE REQUESTED		1N1E33DB 99003	SKYFALL INVESTMENTS LLC	2029 VERDUGO BLVD #140	MONTROSE CA 91020
108	RETURN	SERVICE REQUESTED SERVICE REQUESTED		1N1E33DB 99004 1N1E33DB 99005	XANADU REALTY GROUP LLC CHANG PATTY	9450 SW GEMINI DR PMB 18251 950 BELLOMO AVE	BEAVERTON OR 97008-7105 SUNNYVALE CA 94086
110	DETURN	SERVICE REQUESTED		1N1E33DB 99006	LENOIR ANDRIA	3439 NE SANDY BLVD PMB 338	PORTLAND OR 97232-1959
111	RETURN	SERVICE REQUESTED		1N1E33DB 99007	NIELSEN BRYCE C	3519 NE 15TH AVE PMB 390	PORTLAND OR 97212-2356
112	RETURN	SERVICE REQUESTED		1N1E33DB 99008	CHINN EMILY	333 NW BIRDSDALE AVE	GRESHAM OR 97030-6459
113	RETURN	SERVICE REQUESTED		1N1E33DB 99009	SZOT THOMAS & PATTYN ERIN M	4546 SW PLUM ST	PORTLAND OR 97219-5279
		SERVICE REQUESTED		1N1E33DB 99010	GRANT PAUL R & GRANT ADELINE	1124 S PLEASANT ST	LODI CA 95240
		SERVICE REQUESTED		1N1E33DB 99011	EWEN DAVID	PO BOX 91	GOVERNMENT CAMP OR 97028
		SERVICE REQUESTED		1N1E33DB 99012	HARVEY FRANCISCO	1631 NW EVERETT ST #203	PORTLAND OR 97209
		SERVICE REQUESTED		1N1E33DB 99013	YEITER PAUL A TR	34 NW 1ST AVE #100 1631 NW EVERETT ST #205	PORTLAND OR 97209-4014 PORTLAND OR 97209
		SERVICE REQUESTED SERVICE REQUESTED		1N1E33DB 99014 1N1E33DB 99015	FUENTEVILLA WILLIAM & AURAND MARIN IRICK LAUREN K	1631 NW EVERETT ST #205 1631 NW EVERETT ST #206	PORTLAND OR 97209 PORTLAND OR 97209
		SERVICE REQUESTED		1N1E33DB 99016	KING GUY	1631 NW EVERETT ST #200	PORTLAND OR 97209
		SERVICE REQUESTED		1N1E33DB 99017	STEWART MICHAEL	1631 NW EVERETT ST #207	PORTLAND OR 97209
		SERVICE REQUESTED		1N1E33DB 99018	LINDER JOHN E & LINDER JENNIFER R	836 NW WINTERGREEN DR	MCMINNVILLE OR 97128
123	RETURN	SERVICE REQUESTED		1N1E33DB 99019	FRETZ JORDAN	PO BOX 2370	PORTLAND OR 97208-2370
124	RETURN	SERVICE REQUESTED		1N1E33DB 99020	GATLIN VAN & GATLIN DALE	1631 NW EVERETT ST #302	PORTLAND OR 97209
125	RETURN	SERVICE REQUESTED		1N1E33DB 99021	COATES CRISTIAN	1631 NW EVERETT ST #303	PORTLAND OR 97209-2146
126	RETURN	SERVICE REQUESTED		1N1E33DB 99022	SEDEY BARBARA J & SEDEY ALLEN F	2723 STONEBROOK DR	MEDFORD OR 97504
127	KETURN	SERVICE REQUESTED SERVICE REQUESTED		1N1E33DB 99023 1N1E33DB 99024	JOHNSON MARC & JOHNSON WENDY	838 SE 38TH AVE #307	PORTLAND OR 97214 PORTLAND OR 97209-2146
		SERVICE REQUESTED SERVICE REQUESTED		1N1E33DB 99024 1N1E33DB 99025	BEDARD JANA L NAIR AAKARISH	1631 NW EVERETT ST #306 1721 MARTIN LUTHER KING JR WAY #3	PORTLAND OR 97209-2146 BERKELEY CA 94709
		SERVICE REQUESTED		1N1E33DB 99026	KNOLL BRUCE N	1631 NW EVERETT ST #308	PORTLAND OR 97209
		SERVICE REQUESTED		1N1E33DB 99027	GRANT PAUL & GRANT ADELINE	1124 S PLEASANT AVE	LODI CA 95240
132	RETURN	SERVICE REQUESTED		1N1E33DB 99028	CARLTON 401 LLC	13560 NW SPRINGVILLE RD	PORTLAND OR 97229-1613
		SERVICE REQUESTED		1N1E33DB 99029	HOUNTON CINDI & HOUNTON ROGATIEN	416 NE 131ST PL	PORTLAND OR 97230
		SERVICE REQUESTED		1N1E33DB 99030	MATHER MICHELE T	8510 N RICHMOND AVE	PORTLAND OR 97203
135		SERVICE REQUESTED		1N1E33DB 99031	LYSTRUP BARBARA	1631 NW EVERETT ST #404	PORTLAND OR 97209-2163
136		SERVICE REQUESTED		1N1E33DB 99032	CANTOR AUDREY	1631 NW EVERETT ST #405	PORTLAND OR 97209-2163
		SERVICE REQUESTED SERVICE REQUESTED		1N1E33DB 99033 1N1E33DB 99034	TIBBETTS NANCY LOU LAN	1631 NW EVERETT ST #406 1631 NW EVERETT ST #407	PORTLAND OR 97209 PORTLAND OR 97209
130	RETURN	SERVICE REQUESTED		1N1E33DB 99034 1N1E33DB 99035	JAZRAWI REEM	1631 NW EVERETT ST #407 5230 CROWN ST	WEST LINN OR 97068
140	RETURN	SERVICE REQUESTED		1N1E33DB 99036	ANDERSON NICHOLAS	1631 NW EVERETT ST #A	PORTLAND OR 97008
141	RETURN	SERVICE REQUESTED		1N1E33DB 99037	SHTEYNBERG ALEX & BULOCHNIK NICOLE	1631 NW EVERETT ST #B	PORTLAND OR 97209
142	RETURN	SERVICE REQUESTED		1N1E33DB 99062	ROSE CITY RENTALS LLC	1718 SE 32ND PL	PORTLAND OR 97214
143		_			CURRENT RESIDENT	120 NW TRINITY PL #107	PORTLAND OR 97209
145 145	1				CURRENT RESIDENT	120 NW TRINITY PL #202	PORTLAND OR 97209
145					CURRENT RESIDENT	120 NW TRINITY PL #205	PORTLAND OR 97209
146	)				CURRENT RESIDENT	120 NW TRINITY PL #207	PORTLAND OR 97209

A	B.	۲	П	F	F
	Ü	C	CURRENT RESIDENT	130 NW 19TH AVE #102	PORTLAND OR 97209
47/ 48/ 49/ 50/ 51/ 52/ 53/ 54/ 55/ 55/ 55/ 55/ 55/ 56/ 57/ 66/ 66/ 66/ 66/ 66/ 66/ 66/ 6			CURRENT RESIDENT	130 NW 19TH AVE #104	PORTLAND OR 97209
40			CURRENT RESIDENT	130 NW 191H AVE #104 130 NW 19TH AVE #115	PORTLAND OR 97209
F0					
50			CURRENT RESIDENT	130 NW 19TH AVE #201	PORTLAND OR 97209
51			CURRENT RESIDENT	130 NW 19TH AVE #203	PORTLAND OR 97209
52			CURRENT RESIDENT	130 NW 19TH AVE #215	PORTLAND OR 97209
53			CURRENT RESIDENT	1631 NW EVERETT ST #100	PORTLAND OR 97209
54			CURRENT RESIDENT	1631 NW EVERETT ST #105	PORTLAND OR 97209
55			CURRENT RESIDENT	1631 NW EVERETT ST #106	PORTLAND OR 97209
56			CURRENT RESIDENT	1631 NW EVERETT ST #107	PORTLAND OR 97209
57			CURRENT RESIDENT	1631 NW EVERETT ST #300	PORTLAND OR 97209
58			CURRENT RESIDENT	1631 NW EVERETT ST #307	PORTLAND OR 97209
59			CURRENT RESIDENT	1631 NW EVERETT ST #402	PORTLAND OR 97209
0			CURRENT RESIDENT	1634 NW EVERETT ST #202	PORTLAND OR 97209
51			CURRENT RESIDENT	1706 NW GLISAN ST #1	PORTLAND OR 97209
2			CURRENT RESIDENT	1706 NW GLISAN ST #2	PORTLAND OR 97209
53			CURRENT RESIDENT	1706 NW GLISAN ST #7	PORTLAND OR 97209
4			CURRENT RESIDENT	1715 NW COUCH ST	PORTLAND OR 97209
5			CURRENT RESIDENT	1731 NW EVERETT ST #101	PORTLAND OR 97209
56			CURRENT RESIDENT	1731 NW EVERETT ST #103	PORTLAND OR 97209
7			CURRENT RESIDENT	1731 NW EVERETT ST #105	PORTLAND OR 97209
8			CURRENT RESIDENT	1731 NW EVERETT ST #107	PORTLAND OR 97209
59			CURRENT RESIDENT	1731 NW EVERETT ST #109	PORTLAND OR 97209
ā			CURRENT RESIDENT	1731 NW EVERETT ST #109	PORTLAND OR 97209
71			CURRENT RESIDENT	1731 NW EVERETT ST #110 1731 NW EVERETT ST #112	PORTLAND OR 97209
<del>j</del>			CURRENT RESIDENT	1731 NW EVERETT ST #112 1731 NW EVERETT ST #201	PORTLAND OR 97209
2			CURRENT RESIDENT	1731 NW EVERETT ST #201	PORTLAND OR 97209
<del>3</del>			CURRENT RESIDENT	1810 NW EVERETT ST #303	PORTLAND OR 97209
75			CURRENT RESIDENT	1812 NW FLANDERS ST #24	PORTLAND OR 97209
20					
77			CURRENT RESIDENT	1812 NW FLANDERS ST #25	PORTLAND OR 97209
7			CURRENT RESIDENT	1812 NW FLANDERS ST #3	PORTLAND OR 97209
<u> </u>			CURRENT RESIDENT	1812 NW FLANDERS ST #31	PORTLAND OR 97209
9			CURRENT RESIDENT	1812 NW FLANDERS ST #33	PORTLAND OR 97209
7			CURRENT RESIDENT	1812 NW FLANDERS ST #35	PORTLAND OR 97209
1			CURRENT RESIDENT	1812 NW FLANDERS ST #4	PORTLAND OR 97209
2			CURRENT RESIDENT	1812 NW FLANDERS ST #42	PORTLAND OR 97209
3			CURRENT RESIDENT	1815 NW FLANDERS ST #201	PORTLAND OR 97209
4			CURRENT RESIDENT	1819 NW EVERETT ST #105	PORTLAND OR 97209
5			CURRENT RESIDENT	1819 NW EVERETT ST #205	PORTLAND OR 97209
6			CURRENT RESIDENT	1819 NW EVERETT ST #206	PORTLAND OR 97209
7			CURRENT RESIDENT	1819 NW EVERETT ST #216	PORTLAND OR 97209
3			CURRENT RESIDENT	1936 NW FLANDERS ST #151	PORTLAND OR 97209
9			CURRENT RESIDENT	1936 NW FLANDERS ST #152	PORTLAND OR 97209
1			CURRENT RESIDENT	1936 NW FLANDERS ST #153	PORTLAND OR 97209
1			CURRENT RESIDENT	1936 NW FLANDERS ST #155	PORTLAND OR 97209
2			CURRENT RESIDENT	1936 NW FLANDERS ST #159	PORTLAND OR 97209
3			CURRENT RESIDENT	1936 NW FLANDERS ST #256	PORTLAND OR 97209
1			CURRENT RESIDENT	1936 NW FLANDERS ST #357	PORTLAND OR 97209
			CURRENT RESIDENT	1936 NW FLANDERS ST #358	PORTLAND OR 97209
6			CURRENT RESIDENT	1949 NW EVERETT ST #103	PORTLAND OR 97209
1			CURRENT RESIDENT	1949 NW EVERETT ST #106	PORTLAND OR 97209
8			CURRENT RESIDENT	1949 NW EVERETT ST #202	PORTLAND OR 97209
9			CURRENT RESIDENT	1949 NW EVERETT ST #203	PORTLAND OR 97209
0			CURRENT RESIDENT	1949 NW EVERETT ST #301	PORTLAND OR 97209
ī <b>l</b>			CURRENT RESIDENT	1962 NW EVERETT ST	PORTLAND OR 97209
2			CURRENT RESIDENT	1975 NW EVERETT ST #201	PORTLAND OR 97209
1			CURRENT RESIDENT	1975 NW EVERETT ST #211	PORTLAND OR 97209
4			CURRENT RESIDENT	1975 NW EVERETT ST #306	PORTLAND OR 97209
5			CURRENT RESIDENT	1975 NW EVERETT ST #307	PORTLAND OR 97209
1			CURRENT RESIDENT	1975 NW EVERETT ST #310	PORTLAND OR 97209
7			CURRENT RESIDENT	1975 NW EVERETT ST #402	PORTLAND OR 97209
8			CURRENT RESIDENT	1975 NW EVERETT ST #402	PORTLAND OR 97209
9			CURRENT RESIDENT	1975 NW EVERETT ST #510	PORTLAND OR 97209
1			CURRENT RESIDENT	220 NW 17TH AVE #1	PORTLAND OR 97209
i			CURRENT RESIDENT	220 NW 17TH AVE #1 220 NW 17TH AVE #2	PORTLAND OR 97209
2			CURRENT RESIDENT	325 NW 171H AVE #2 325 NW 18TH AVE #101	PORTLAND OR 97209
<del>1</del>			CURRENT RESIDENT	325 NW 18TH AVE #101 325 NW 18TH AVE #103	PORTLAND OR 97209 PORTLAND OR 97209
<del>1</del>					
			CURRENT RESIDENT	325 NW 18TH AVE #202	PORTLAND OR 97209
			CURRENT RESIDENT	325 NW 18TH AVE #204	PORTLAND OR 97209
7			CURRENT RESIDENT	325 NW 18TH AVE #206	PORTLAND OR 97209
4			CURRENT RESIDENT	325 NW 18TH AVE #308	PORTLAND OR 97209
<u> </u>			CURRENT RESIDENT	325 NW 18TH AVE #503	PORTLAND OR 97209
			CURRENT RESIDENT	325 NW 18TH AVE #504	PORTLAND OR 97209

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COMMENT RESIDENT  COMMENT PRESIDENT  COMMENT PRESIDENT  COMMENT RESIDENT  COMMENT RE	F
CORRECT RESIDENCY 33.0 (170 AM 10) PORTABO (0 170 AM 10) PORTABO (	
### CHARLES OF PART AND ADDRESS OF PART AND AD	
CHRONIC DELICATION   333 NA 1978 AV \$1.04   PORTADO OR \$7.07	
COURSEN RESIDENT 35 MM 1978 AVE \$1003 FORTHARD OR 972 AVE \$1000 FORTHA	
CHERRY REGISTERY 355 MW 1974 AVW \$11.05 PORTLAND ON 975 AVE \$10.05 PORTLAND	
COMMENT RESIDENT  CONSENT RESI	
Course   Region	
Company   Register	
CURRENT RESIDENT   335 NN 1978 AVE 1212   PORTABO ON 972	19
CURRENT HISTORIEST   33.5 NM 197H AVE \$2.05   PORTLAND OR 972   20.05	19
CURRENT MASIEMEN   335 NM 1974 AVE \$206   PONTABIO ON 972	19
CUMBRET RESIDENT  COMBRET RESI	
CURRENT MESIDONT	
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286 CURRENT RESIDENT 120 NW TRINITY PL #305 PORTLAND OR 972C	
CURRENT RESIDENT 120 NW TRINITY PL #401 PORTLAND OR 972C	
288         CURRENT RESIDENT         120 NW TRINITY PL #405         PORTLAND OR 9720           289         CURRENT RESIDENT         130 NW 19TH AVE #105         PORTLAND OR 9720	
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93 A	В	C	CURRENT RESIDENT	130 NW 19TH AVE #206	PORTLAND OR 97209
94			CURRENT RESIDENT	130 NW 191H AVE #200	PORTLAND OR 97209
95			CURRENT RESIDENT	130 NW 19TH AVE #212	PORTLAND OR 97209
96			CURRENT RESIDENT	130 NW 19TH AVE #5	PORTLAND OR 97209
97			CURRENT RESIDENT	1628 NW EVERETT ST #2	PORTLAND OR 97209
98			CURRENT RESIDENT	1631 NW EVERETT ST #102	PORTLAND OR 97209
99			CURRENT RESIDENT	1631 NW EVERETT ST #103	PORTLAND OR 97209
00			CURRENT RESIDENT	1631 NW EVERETT ST #104	PORTLAND OR 97209
01			CURRENT RESIDENT	1631 NW EVERETT ST #200	PORTLAND OR 97209
02			CURRENT RESIDENT	1631 NW EVERETT ST #305	PORTLAND OR 97209
03			CURRENT RESIDENT	1634 NW EVERETT ST #102	PORTLAND OR 97209
04			CURRENT RESIDENT	1706 NW GLISAN ST #5	PORTLAND OR 97209
05			CURRENT RESIDENT	1731 NW EVERETT ST #111	PORTLAND OR 97209
06			CURRENT RESIDENT	1731 NW EVERETT ST #206	PORTLAND OR 97209
07			CURRENT RESIDENT	1731 NW EVERETT ST #211	PORTLAND OR 97209
08			CURRENT RESIDENT	1731 NW EVERETT ST #A	PORTLAND OR 97209
10			CURRENT RESIDENT	1740 NW FLANDERS ST	PORTLAND OR 97209
10			CURRENT RESIDENT	1810 NW GLISAN ST	PORTLAND OR 97209
12			CURRENT RESIDENT CURRENT RESIDENT	1812 NW FLANDERS ST #21	PORTLAND OR 97209
99 99 99 99 99 99 99 99 99 99 99 99 99			CURRENT RESIDENT	1812 NW FLANDERS ST #23 1812 NW FLANDERS ST #34	PORTLAND OR 97209 PORTLAND OR 97209
14			CURRENT RESIDENT	1812 NW FLANDERS ST #34 1812 NW FLANDERS ST #44	PORTLAND OR 97209 PORTLAND OR 97209
 15			CURRENT RESIDENT	1812 NW FLANDERS ST #45	PORTLAND OR 97209
16			CURRENT RESIDENT	1812 NW FLANDERS ST #45	PORTLAND OR 97209
17			CURRENT RESIDENT	1815 NW FLANDERS ST #103	PORTLAND OR 97209
18			CURRENT RESIDENT	1815 NW FLANDERS ST #203	PORTLAND OR 97209
19			CURRENT RESIDENT	1815 NW FLANDERS ST #L103	PORTLAND OR 97209
20			CURRENT RESIDENT	1819 NW EVERETT ST #200	PORTLAND OR 97209
21			CURRENT RESIDENT	1819 NW EVERETT ST #203	PORTLAND OR 97209
22			CURRENT RESIDENT	1936 NW FLANDERS ST #156	PORTLAND OR 97209
23			CURRENT RESIDENT	1936 NW FLANDERS ST #158	PORTLAND OR 97209
24			CURRENT RESIDENT	1936 NW FLANDERS ST #160	PORTLAND OR 97209
25			CURRENT RESIDENT	1936 NW FLANDERS ST #251	PORTLAND OR 97209
26			CURRENT RESIDENT	1936 NW FLANDERS ST #252	PORTLAND OR 97209
27			CURRENT RESIDENT	1936 NW FLANDERS ST #253	PORTLAND OR 97209
28			CURRENT RESIDENT	1936 NW FLANDERS ST #254	PORTLAND OR 97209
29			CURRENT RESIDENT	1936 NW FLANDERS ST #353	PORTLAND OR 97209
30			CURRENT RESIDENT	1936 NW FLANDERS ST #354	PORTLAND OR 97209
31			CURRENT RESIDENT	1936 NW FLANDERS ST #355	PORTLAND OR 97209
32			CURRENT RESIDENT	1936 NW FLANDERS ST #356 1936 NW FLANDERS ST #359	PORTLAND OR 97209 PORTLAND OR 97209
33			CURRENT RESIDENT CURRENT RESIDENT	1936 NW FLANDERS ST #359 1949 NW EVERETT ST #101	PORTLAND OR 97209 PORTLAND OR 97209
25			CURRENT RESIDENT	1949 NW EVERETT ST #101	PORTLAND OR 97209
36			CURRENT RESIDENT	1949 NW EVERETT ST #102	PORTLAND OR 97209
37			CURRENT RESIDENT	1949 NW EVERETT ST #206	PORTLAND OR 97209
38			CURRENT RESIDENT	1949 NW EVERETT ST #304	PORTLAND OR 97209
39			CURRENT RESIDENT	1975 NW EVERETT ST #101	PORTLAND OR 97209
40			CURRENT RESIDENT	1975 NW EVERETT ST #103	PORTLAND OR 97209
41			CURRENT RESIDENT	1975 NW EVERETT ST #204	PORTLAND OR 97209
42			CURRENT RESIDENT	1975 NW EVERETT ST #303	PORTLAND OR 97209
43			CURRENT RESIDENT	1975 NW EVERETT ST #305	PORTLAND OR 97209
44			CURRENT RESIDENT	1975 NW EVERETT ST #406	PORTLAND OR 97209
45			CURRENT RESIDENT	1975 NW EVERETT ST #407	PORTLAND OR 97209
46			CURRENT RESIDENT	1975 NW EVERETT ST #411	PORTLAND OR 97209
47			CURRENT RESIDENT	1975 NW EVERETT ST #503	PORTLAND OR 97209
48			CURRENT RESIDENT	1975 NW EVERETT ST #506	PORTLAND OR 97209
49			CURRENT RESIDENT	1975 NW EVERETT ST #508	PORTLAND OR 97209
50			CURRENT RESIDENT	1975 NW EVERETT ST #511	PORTLAND OR 97209
51			CURRENT RESIDENT	323 NW 17TH AVE	PORTLAND OR 97209
52			CURRENT RESIDENT	325 NW 18TH AVE #207	PORTLAND OR 97209
54			CURRENT RESIDENT	325 NW 18TH AVE #208	PORTLAND OR 97209
54			CURRENT RESIDENT	325 NW 18TH AVE #305	PORTLAND OR 97209
55			CURRENT RESIDENT CURRENT RESIDENT	325 NW 18TH AVE #306 325 NW 18TH AVE #401	PORTLAND OR 97209 PORTLAND OR 97209
57			CURRENT RESIDENT CURRENT RESIDENT	325 NW 18TH AVE #401 325 NW 18TH AVE #402	PORTLAND OR 97209 PORTLAND OR 97209
58			CURRENT RESIDENT	325 NW 18TH AVE #402 325 NW 18TH AVE #404	PORTLAND OR 97209 PORTLAND OR 97209
59			CURRENT RESIDENT	325 NW 18TH AVE #404 325 NW 18TH AVE #405	PORTLAND OR 97209 PORTLAND OR 97209
60			CURRENT RESIDENT	325 NW 18TH AVE #405 325 NW 18TH AVE #406	PORTLAND OR 97209 PORTLAND OR 97209
61			CURRENT RESIDENT	325 NW 18TH AVE #406 325 NW 18TH AVE #407	PORTLAND OR 97209 PORTLAND OR 97209
62			CURRENT RESIDENT	325 NW 18TH AVE #407	PORTLAND OR 97209
63			CURRENT RESIDENT	325 NW 18TH AVE #501	PORTLAND OR 97209
				325 NW 18TH AVE #502	PORTLAND OR 97209
64			CURRENT RESIDENT		

- I			C	<b>D</b>		
266	A	В	C	CURRENT RESIDENT	325 NW 18TH AVE #506	PORTLAND OR 97209
366 367 368 369 369 370 371 377 378 378 377 378 378 379 378 378 380 381 381 382 383 384 384 389 389 391 392 393 391 392 393 391 400 401 401 402 403 404 405 406 407 408 409 409 401 401 411 412 412 414 415 416 417 418 419 419 410 411 411 412 412 414 415 416 417 418 419 419 420 421 422 423 424 424 429 429 429 429 429 429 429 429				CURRENT RESIDENT	331 NW 18TH AVE #506	PORTLAND OR 97209 PORTLAND OR 97209
368				CURRENT RESIDENT	335 NW 19TH AVE #1002	PORTLAND OR 97209
369				CURRENT RESIDENT	335 NW 19TH AVE #1006	PORTLAND OR 97209
370				CURRENT RESIDENT	335 NW 19TH AVE #101	PORTLAND OR 97209
371				CURRENT RESIDENT	335 NW 19TH AVE #106	PORTLAND OR 97209
372				CURRENT RESIDENT	335 NW 19TH AVE #107	PORTLAND OR 97209
373				CURRENT RESIDENT	335 NW 19TH AVE #1110	PORTLAND OR 97209
374				CURRENT RESIDENT	335 NW 19TH AVE #1207	PORTLAND OR 97209
375				CURRENT RESIDENT	335 NW 19TH AVE #204	PORTLAND OR 97209
376				CURRENT RESIDENT	335 NW 19TH AVE #207	PORTLAND OR 97209
377				CURRENT RESIDENT	335 NW 19TH AVE #209	PORTLAND OR 97209
378				CURRENT RESIDENT	335 NW 19TH AVE #306	PORTLAND OR 97209
379				CURRENT RESIDENT	335 NW 19TH AVE #308	PORTLAND OR 97209
380				CURRENT RESIDENT	335 NW 19TH AVE #311	PORTLAND OR 97209
381				CURRENT RESIDENT	335 NW 19TH AVE #402	PORTLAND OR 97209
382				CURRENT RESIDENT	335 NW 19TH AVE #404	PORTLAND OR 97209
383				CURRENT RESIDENT	335 NW 19TH AVE #406	PORTLAND OR 97209
384				CURRENT RESIDENT	335 NW 19TH AVE #410	PORTLAND OR 97209
386				CURRENT RESIDENT CURRENT RESIDENT	335 NW 19TH AVE #411 335 NW 19TH AVE #605	PORTLAND OR 97209 PORTLAND OR 97209
387				CURRENT RESIDENT	335 NW 19TH AVE #605 335 NW 19TH AVE #606	PORTLAND OR 97209 PORTLAND OR 97209
388				CURRENT RESIDENT	335 NW 19TH AVE #606 335 NW 19TH AVE #610	PORTLAND OR 97209 PORTLAND OR 97209
389				CURRENT RESIDENT	335 NW 19TH AVE #010	PORTLAND OR 97209
390				CURRENT RESIDENT	335 NW 19TH AVE #703	PORTLAND OR 97209
391				CURRENT RESIDENT	335 NW 19TH AVE #708	PORTLAND OR 97209
392				CURRENT RESIDENT	335 NW 19TH AVE #710	PORTLAND OR 97209
393				CURRENT RESIDENT	335 NW 19TH AVE #803	PORTLAND OR 97209
394				CURRENT RESIDENT	335 NW 19TH AVE #806	PORTLAND OR 97209
395				CURRENT RESIDENT	335 NW 19TH AVE #903	PORTLAND OR 97209
396				CURRENT RESIDENT	335 NW 19TH AVE #904	PORTLAND OR 97209
397				CURRENT RESIDENT	335 NW 19TH AVE #907	PORTLAND OR 97209
398				CURRENT RESIDENT	335 NW 19TH AVE #908	PORTLAND OR 97209
399				CURRENT RESIDENT	335 NW 19TH AVE #909	PORTLAND OR 97209
400				CURRENT RESIDENT	335 NW 19TH AVE #911	PORTLAND OR 97209
401				CURRENT RESIDENT	335 NW 19TH AVE #912	PORTLAND OR 97209
402				CURRENT RESIDENT	400 NW 17TH AVE	PORTLAND OR 97209
403				CURRENT RESIDENT	410 NW 18TH AVE #201	PORTLAND OR 97209
404				CURRENT RESIDENT CURRENT RESIDENT	410 NW 18TH AVE #202 410 NW 18TH AVE #401	PORTLAND OR 97209 PORTLAND OR 97209
405				CURRENT RESIDENT	507 NW 18TH AVE #401	PORTLAND OR 97209 PORTLAND OR 97209
407				CURRENT RESIDENT	513 NW 18TH AVE	PORTLAND OR 97209
408				CURRENT RESIDENT	515 NW 18TH AVE	PORTLAND OR 97209
409				CURRENT RESIDENT	535 NW 18TH AVE	PORTLAND OR 97209
410				CURRENT RESIDENT	550 NW 19TH AVE #106	PORTLAND OR 97209
411				CURRENT RESIDENT	550 NW 19TH AVE #111	PORTLAND OR 97209
412				CURRENT RESIDENT	550 NW 19TH AVE #203	PORTLAND OR 97209
413				CURRENT RESIDENT	550 NW 19TH AVE #209	PORTLAND OR 97209
414				CURRENT RESIDENT	550 NW 19TH AVE #210	PORTLAND OR 97209
415				CURRENT RESIDENT	550 NW 19TH AVE #211	PORTLAND OR 97209
416				CURRENT RESIDENT	550 NW 19TH AVE #216	PORTLAND OR 97209
117				CURRENT RESIDENT	550 NW 19TH AVE #217	PORTLAND OR 97209
418				CURRENT RESIDENT	550 NW 19TH AVE #302	PORTLAND OR 97209
119				CURRENT RESIDENT	550 NW 19TH AVE #303	PORTLAND OR 97209
120				CURRENT RESIDENT	550 NW 19TH AVE #304	PORTLAND OR 97209
122				CURRENT RESIDENT	550 NW 19TH AVE #306	PORTLAND OR 97209
122				CURRENT RESIDENT	550 NW 19TH AVE #307	PORTLAND OR 97209
124				CURRENT RESIDENT CURRENT RESIDENT	550 NW 19TH AVE #308 550 NW 19TH AVE #311	PORTLAND OR 97209 PORTLAND OR 97209
25				CURRENT RESIDENT	550 NW 19TH AVE #311 550 NW 19TH AVE #403	PORTLAND OR 97209 PORTLAND OR 97209
126				CURRENT RESIDENT	550 NW 19TH AVE #403 550 NW 19TH AVE #405	PORTLAND OR 97209
127				CURRENT RESIDENT	550 NW 19TH AVE #405	PORTLAND OR 97209 PORTLAND OR 97209
-28				CURRENT RESIDENT	550 NW 19TH AVE #416	PORTLAND OR 97209
29				CURRENT RESIDENT	550 NW 19TH AVE #410	PORTLAND OR 97209
130				CURRENT RESIDENT	550 NW 19TH AVE #417	PORTLAND OR 97209
31				CURRENT RESIDENT	550 NW 19TH AVE #505	PORTLAND OR 97209
132				CURRENT RESIDENT	550 NW 19TH AVE #616	PORTLAND OR 97209
133				CURRENT RESIDENT	120 NW TRINITY PL #101	PORTLAND OR 97209
34				CURRENT RESIDENT	120 NW TRINITY PL #304	PORTLAND OR 97209
135				CURRENT RESIDENT	120 NW TRINITY PL #306	PORTLAND OR 97209
136				CURRENT RESIDENT	120 NW TRINITY PL #307	PORTLAND OR 97209
137				CURRENT RESIDENT	120 NW TRINITY PL #404	PORTLAND OR 97209
				CURRENT RESIDENT	120 NW TRINITY PL #407	PORTLAND OR 97209

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A	В	C	D	E	F
439			CURRENT RESIDENT	120 NW TRINITY PL #D	PORTLAND OR 97209
440			CURRENT RESIDENT	130 NW 19TH AVE #101	PORTLAND OR 97209
441			CURRENT RESIDENT	130 NW 19TH AVE #106	PORTLAND OR 97209
442			CURRENT RESIDENT CURRENT RESIDENT	130 NW 19TH AVE #109 130 NW 19TH AVE #112	PORTLAND OR 97209 PORTLAND OR 97209
443			CURRENT RESIDENT	130 NW 19TH AVE #112 130 NW 19TH AVE #114	PORTLAND OR 97209 PORTLAND OR 97209
445			CURRENT RESIDENT	130 NW 19TH AVE #204	PORTLAND OR 97209
446			CURRENT RESIDENT	130 NW 19TH AVE #207	PORTLAND OR 97209
447			CURRENT RESIDENT	130 NW 19TH AVE #208	PORTLAND OR 97209
448			CURRENT RESIDENT	130 NW 19TH AVE #210	PORTLAND OR 97209
449			CURRENT RESIDENT	130 NW 19TH AVE #211	PORTLAND OR 97209
450			CURRENT RESIDENT	130 NW 19TH AVE #6	PORTLAND OR 97209
451			CURRENT RESIDENT	130 NW 19TH AVE #7	PORTLAND OR 97209
452			CURRENT RESIDENT	130 NW 19TH AVE #8	PORTLAND OR 97209
453			CURRENT RESIDENT	133 NW 18TH AVE #7	PORTLAND OR 97209
454			CURRENT RESIDENT	1631 NW EVERETT ST #201	PORTLAND OR 97209
455			CURRENT RESIDENT	1631 NW EVERETT ST #204	PORTLAND OR 97209
456			CURRENT RESIDENT	1631 NW EVERETT ST #400	PORTLAND OR 97209
457			CURRENT RESIDENT	1631 NW EVERETT ST #403	PORTLAND OR 97209
458			CURRENT RESIDENT	1631 NW EVERETT ST #408	PORTLAND OR 97209
459			CURRENT RESIDENT	1634 NW EVERETT ST #101	PORTLAND OR 97209
461			CURRENT RESIDENT	1731 NW EVERETT ST #203	PORTLAND OR 97209
462			CURRENT RESIDENT CURRENT RESIDENT	1731 NW EVERETT ST #209 1731 NW EVERETT ST #212	PORTLAND OR 97209 PORTLAND OR 97209
463			CURRENT RESIDENT	1731 NW EVERETT ST #212 1740 NW GLISAN ST	PORTLAND OR 97209 PORTLAND OR 97209
464			CURRENT RESIDENT	1810 NW EVERETT ST #102	PORTLAND OR 97209 PORTLAND OR 97209
439 440 441 442 443 4444 445 446 447 448 449 459 451 452 453 454 455 456 457 458 459 459 460 461 462 463 464 465 466 467 477 477 477 478 479 480 481 482 489 489 489 489 489 489 489 499 500 500 500 500 500 500 500 500 500 5			CURRENT RESIDENT	1810 NW EVERETT ST #102	PORTLAND OR 97209
466			CURRENT RESIDENT	1812 NW FLANDERS ST #1	PORTLAND OR 97209
467			CURRENT RESIDENT	1812 NW FLANDERS ST #2	PORTLAND OR 97209
468			CURRENT RESIDENT	1812 NW FLANDERS ST #22	PORTLAND OR 97209
469			CURRENT RESIDENT	1812 NW FLANDERS ST #5	PORTLAND OR 97209
470			CURRENT RESIDENT	1815 NW FLANDERS ST #102	PORTLAND OR 97209
471			CURRENT RESIDENT	1815 NW FLANDERS ST #104	PORTLAND OR 97209
472			CURRENT RESIDENT	1815 NW FLANDERS ST #L101	PORTLAND OR 97209
473			CURRENT RESIDENT	1815 NW FLANDERS ST #L102	PORTLAND OR 97209
474			CURRENT RESIDENT	1819 NW EVERETT ST #204	PORTLAND OR 97209
475			CURRENT RESIDENT	1936 NW FLANDERS ST #154	PORTLAND OR 97209
476			CURRENT RESIDENT	1936 NW FLANDERS ST #157	PORTLAND OR 97209
477			CURRENT RESIDENT	1936 NW FLANDERS ST #255	PORTLAND OR 97209
478			CURRENT RESIDENT	1936 NW FLANDERS ST #258	PORTLAND OR 97209
480			CURRENT RESIDENT CURRENT RESIDENT	1936 NW FLANDERS ST #259 1949 NW EVERETT ST #104	PORTLAND OR 97209 PORTLAND OR 97209
481			CURRENT RESIDENT	1949 NW EVERETT ST #204	PORTLAND OR 97209
482			CURRENT RESIDENT	1949 NW EVERETT ST #302	PORTLAND OR 97209
483			CURRENT RESIDENT	1949 NW EVERETT ST #305	PORTLAND OR 97209
484			CURRENT RESIDENT	1949 NW EVERETT ST #306	PORTLAND OR 97209
485			CURRENT RESIDENT	1975 NW EVERETT ST #105	PORTLAND OR 97209
486			CURRENT RESIDENT	1975 NW EVERETT ST #109	PORTLAND OR 97209
487			CURRENT RESIDENT	1975 NW EVERETT ST #202	PORTLAND OR 97209
488			CURRENT RESIDENT	1975 NW EVERETT ST #203	PORTLAND OR 97209
489			CURRENT RESIDENT	1975 NW EVERETT ST #206	PORTLAND OR 97209
490			CURRENT RESIDENT	1975 NW EVERETT ST #207	PORTLAND OR 97209
491			CURRENT RESIDENT	1975 NW EVERETT ST #208	PORTLAND OR 97209
492			CURRENT RESIDENT	1975 NW EVERETT ST #301	PORTLAND OR 97209
493			CURRENT RESIDENT	1975 NW EVERETT ST #308	PORTLAND OR 97209
494			CURRENT RESIDENT	1975 NW EVERETT ST #404	PORTLAND OR 97209
496			CURRENT RESIDENT	1975 NW EVERETT ST #405	PORTLAND OR 97209
497			CURRENT RESIDENT CURRENT RESIDENT	1975 NW EVERETT ST #410 1975 NW EVERETT ST #502	PORTLAND OR 97209 PORTLAND OR 97209
498			CURRENT RESIDENT	1975 NW EVERETT ST #504	PORTLAND OR 97209
499			CURRENT RESIDENT	1975 NW EVERETT ST #505	PORTLAND OR 97209
500			CURRENT RESIDENT	25 NW 23RD PL #6 PMB 385	PORTLAND OR 97210
501			CURRENT RESIDENT	325 NW 18TH AVE #201	PORTLAND OR 97209
502			CURRENT RESIDENT	325 NW 18TH AVE #203	PORTLAND OR 97209
503			CURRENT RESIDENT	325 NW 18TH AVE #205	PORTLAND OR 97209
504			CURRENT RESIDENT	325 NW 18TH AVE #303	PORTLAND OR 97209
505			CURRENT RESIDENT	325 NW 18TH AVE #304	PORTLAND OR 97209
506			CURRENT RESIDENT	325 NW 18TH AVE #507	PORTLAND OR 97209
507			CURRENT RESIDENT	325 NW 18TH AVE #508	PORTLAND OR 97209
508			CURRENT RESIDENT	335 NW 17TH AVE	PORTLAND OR 97209
509			CURRENT RESIDENT	335 NW 19TH AVE #104	PORTLAND OR 97209
510			CURRENT RESIDENT	335 NW 19TH AVE #105	PORTLAND OR 97209
511			CURRENT RESIDENT	335 NW 19TH AVE #109	PORTLAND OR 97209

Α	D.	-	D.	I -	
	В	C	ь	E	F
512			CURRENT RESIDENT CURRENT RESIDENT	335 NW 19TH AVE #111 335 NW 19TH AVE #1211	PORTLAND OR 97209 PORTLAND OR 97209
514			CURRENT RESIDENT	335 NW 19TH AVE #1211	PORTLAND OR 97209 PORTLAND OR 97209
514			CURRENT RESIDENT	335 NW 19TH AVE #201	PORTLAND OR 97209
516			CURRENT RESIDENT	335 NW 19TH AVE #205	PORTLAND OR 97209
517			CURRENT RESIDENT	335 NW 19TH AVE #210	PORTLAND OR 97209
518			CURRENT RESIDENT	335 NW 19TH AVE #303	PORTLAND OR 97209
519			CURRENT RESIDENT	335 NW 19TH AVE #305	PORTLAND OR 97209
520			CURRENT RESIDENT	335 NW 19TH AVE #307	PORTLAND OR 97209
521			CURRENT RESIDENT	335 NW 19TH AVE #309	PORTLAND OR 97209
522			CURRENT RESIDENT	335 NW 19TH AVE #310	PORTLAND OR 97209
523			CURRENT RESIDENT	335 NW 19TH AVE #401	PORTLAND OR 97209
524			CURRENT RESIDENT	335 NW 19TH AVE #405	PORTLAND OR 97209
525			CURRENT RESIDENT	335 NW 19TH AVE #409	PORTLAND OR 97209
526			CURRENT RESIDENT	335 NW 19TH AVE #412	PORTLAND OR 97209
527			CURRENT RESIDENT	335 NW 19TH AVE #5	PORTLAND OR 97209
528			CURRENT RESIDENT	335 NW 19TH AVE #503	PORTLAND OR 97209
529			CURRENT RESIDENT	335 NW 19TH AVE #505	PORTLAND OR 97209
530			CURRENT RESIDENT	335 NW 19TH AVE #506	PORTLAND OR 97209
531			CURRENT RESIDENT	335 NW 19TH AVE #609	PORTLAND OR 97209
532			CURRENT RESIDENT	335 NW 19TH AVE #704	PORTLAND OR 97209
533			CURRENT RESIDENT	335 NW 19TH AVE #706	PORTLAND OR 97209
534			CURRENT RESIDENT	335 NW 19TH AVE #709	PORTLAND OR 97209
535			CURRENT RESIDENT	335 NW 19TH AVE #807	PORTLAND OR 97209
530			CURRENT RESIDENT	335 NW 19TH AVE #808	PORTLAND OR 97209
537			CURRENT RESIDENT	335 NW 19TH AVE #810	PORTLAND OR 97209
538			CURRENT RESIDENT CURRENT RESIDENT	335 NW 19TH AVE #9 335 NW 19TH AVE #901	PORTLAND OR 97209 PORTLAND OR 97209
540			CURRENT RESIDENT	335 NW 19TH AVE #901	PORTLAND OR 97209 PORTLAND OR 97209
5/1			CURRENT RESIDENT	335 NW 19TH AVE #905	PORTLAND OR 97209
542			CURRENT RESIDENT	335 NW 19TH AVE #906	PORTLAND OR 97209
543			CURRENT RESIDENT	337 NW 17TH AVE	PORTLAND OR 97209
544			CURRENT RESIDENT	407 NW 17TH AVE #6	PORTLAND OR 97209
545			CURRENT RESIDENT	410 NW 18TH AVE #203	PORTLAND OR 97209
546			CURRENT RESIDENT	415 NW 18TH AVE	PORTLAND OR 97209
547			CURRENT RESIDENT	479 NW 18TH AVE	PORTLAND OR 97209
548			CURRENT RESIDENT	550 NW 19TH AVE #102	PORTLAND OR 97209
549			CURRENT RESIDENT	550 NW 19TH AVE #108	PORTLAND OR 97209
550			CURRENT RESIDENT	550 NW 19TH AVE #112	PORTLAND OR 97209
551			CURRENT RESIDENT	550 NW 19TH AVE #202	PORTLAND OR 97209
552			CURRENT RESIDENT	550 NW 19TH AVE #204	PORTLAND OR 97209
553			CURRENT RESIDENT	550 NW 19TH AVE #214	PORTLAND OR 97209
554			CURRENT RESIDENT	550 NW 19TH AVE #215	PORTLAND OR 97209
555			CURRENT RESIDENT CURRENT RESIDENT	550 NW 19TH AVE #218 550 NW 19TH AVE #305	PORTLAND OR 97209 PORTLAND OR 97209
530			CURRENT RESIDENT	550 NW 19TH AVE #305	PORTLAND OR 97209 PORTLAND OR 97209
557			CURRENT RESIDENT	550 NW 191H AVE #310	PORTLAND OR 97209
559			CURRENT RESIDENT	550 NW 19TH AVE #310 550 NW 19TH AVE #401	PORTLAND OR 97209 PORTLAND OR 97209
560			CURRENT RESIDENT	550 NW 19TH AVE #409	PORTLAND OR 97209
561			CURRENT RESIDENT	550 NW 19TH AVE #411	PORTLAND OR 97209
562			CURRENT RESIDENT	550 NW 19TH AVE #412	PORTLAND OR 97209
563			CURRENT RESIDENT	550 NW 19TH AVE #501	PORTLAND OR 97209
564			CURRENT RESIDENT	550 NW 19TH AVE #506	PORTLAND OR 97209
565			CURRENT RESIDENT	550 NW 19TH AVE #507	PORTLAND OR 97209
566			CURRENT RESIDENT	550 NW 19TH AVE #508	PORTLAND OR 97209
567			CURRENT RESIDENT	550 NW 19TH AVE #509	PORTLAND OR 97209
568			CURRENT RESIDENT	550 NW 19TH AVE #516	PORTLAND OR 97209
569			CURRENT RESIDENT	550 NW 19TH AVE #604	PORTLAND OR 97209
570			CURRENT RESIDENT	550 NW 19TH AVE #608	PORTLAND OR 97209
570			CURRENT RESIDENT	550 NW 19TH AVE #609 550 NW 19TH AVE #610	PORTLAND OR 97209 PORTLAND OR 97209
573			CURRENT RESIDENT CURRENT RESIDENT	550 NW 19TH AVE #610 550 NW 19TH AVE #611	PORTLAND OR 97209 PORTLAND OR 97209
574			CURRENT RESIDENT	550 NW 19TH AVE #611 550 NW 19TH AVE #612	PORTLAND OR 97209 PORTLAND OR 97209
575			CURRENT RESIDENT	550 NW 191H AVE #612	PORTLAND OR 97209
576			CURRENT RESIDENT	550 NW 19TH AVE #615	PORTLAND OR 97209
577			CURRENT RESIDENT	550 NW 19TH AVE #617	PORTLAND OR 97209
578			CURRENT RESIDENT	120 NW TRINITY PL #103	PORTLAND OR 97209
579			CURRENT RESIDENT	120 NW TRINITY PL #105	PORTLAND OR 97209
580			CURRENT RESIDENT	120 NW TRINITY PL #106	PORTLAND OR 97209
581			CURRENT RESIDENT	120 NW TRINITY PL #203	PORTLAND OR 97209
512 513 514 515 516 517 518 519 520 521 522 523 524 525 526 527 528 529 530 531 532 533 534 535 536 537 538 539 540 640 647 548 549 550 551 552 553 553 554 555 555 555 555 555			CURRENT RESIDENT	120 NW TRINITY PL #206	PORTLAND OR 97209
583			CURRENT RESIDENT	120 NW TRINITY PL #302	PORTLAND OR 97209
584			CURRENT RESIDENT	120 NW TRINITY PL #402	PORTLAND OR 97209

A	В	С	D	T =	Е
A	В	C	CURRENT RESIDENT	120 NW TRINITY PL #403	PORTLAND OR 97209
5			CURRENT RESIDENT	120 NW TRINITY PL #405	PORTLAND OR 97209
7			CURRENT RESIDENT	120 NW TRINITY PL #A	PORTLAND OR 97209
3			CURRENT RESIDENT	120 NW TRINITY PL #B	PORTLAND OR 97209
9			CURRENT RESIDENT	120 NW TRINITY PL #C	PORTLAND OR 97209
0			CURRENT RESIDENT	130 NW 19TH AVE #103	PORTLAND OR 97209
1			CURRENT RESIDENT	130 NW 19TH AVE #107	PORTLAND OR 97209
2			CURRENT RESIDENT	130 NW 19TH AVE #110	PORTLAND OR 97209
3			CURRENT RESIDENT	130 NW 19TH AVE #202	PORTLAND OR 97209
4			CURRENT RESIDENT	130 NW 19TH AVE #214	PORTLAND OR 97209
			CURRENT RESIDENT	133 NW 18TH AVE #4	PORTLAND OR 97209
			CURRENT RESIDENT	1631 NW EVERETT ST #202	PORTLAND OR 97209
7			CURRENT RESIDENT	1631 NW EVERETT ST #301	PORTLAND OR 97209
			CURRENT RESIDENT CURRENT RESIDENT	1631 NW EVERETT ST #304 1631 NW EVERETT ST #401	PORTLAND OR 97209 PORTLAND OR 97209
			CURRENT RESIDENT	1634 NW EVERETT ST #401	PORTLAND OR 97209
1			CURRENT RESIDENT	1731 NW EVERETT ST #102	PORTLAND OR 97209
7			CURRENT RESIDENT	1731 NW EVERETT ST #104	PORTLAND OR 97209
3			CURRENT RESIDENT	1731 NW EVERETT ST #106	PORTLAND OR 97209
4			CURRENT RESIDENT	1731 NW EVERETT ST #108	PORTLAND OR 97209
5			CURRENT RESIDENT	1731 NW EVERETT ST #202	PORTLAND OR 97209
			CURRENT RESIDENT	1731 NW EVERETT ST #204	PORTLAND OR 97209
			CURRENT RESIDENT	1731 NW EVERETT ST #205	PORTLAND OR 97209
3			CURRENT RESIDENT	1731 NW EVERETT ST #207	PORTLAND OR 97209
			CURRENT RESIDENT	1731 NW EVERETT ST #210	PORTLAND OR 97209
			CURRENT RESIDENT	1731 NW EVERETT ST #B	PORTLAND OR 97209
1			CURRENT RESIDENT	1731 NW GLISAN ST	PORTLAND OR 97209
2			CURRENT RESIDENT	1812 NW FLANDERS ST #32	PORTLAND OR 97209
3			CURRENT RESIDENT	1812 NW FLANDERS ST #41	PORTLAND OR 97209
1			CURRENT RESIDENT	1812 NW FLANDERS ST #43	PORTLAND OR 97209
5			CURRENT RESIDENT CURRENT RESIDENT	1815 NW FLANDERS ST #101 1815 NW FLANDERS ST #202	PORTLAND OR 97209 PORTLAND OR 97209
4			CURRENT RESIDENT	1818 NW GLISAN ST	PORTLAND OR 97209 PORTLAND OR 97209
7 R			CURRENT RESIDENT	1819 NW EVERETT ST #101	PORTLAND OR 97209
9			CURRENT RESIDENT	1936 NW FLANDERS ST #257	PORTLAND OR 97209
			CURRENT RESIDENT	1936 NW FLANDERS ST #260	PORTLAND OR 97209
1			CURRENT RESIDENT	1936 NW FLANDERS ST #351	PORTLAND OR 97209
2			CURRENT RESIDENT	1936 NW FLANDERS ST #352	PORTLAND OR 97209
3			CURRENT RESIDENT	1936 NW FLANDERS ST #360	PORTLAND OR 97209
4			CURRENT RESIDENT	1949 NW EVERETT ST #201	PORTLAND OR 97209
5			CURRENT RESIDENT	1949 NW EVERETT ST #205	PORTLAND OR 97209
			CURRENT RESIDENT	1949 NW EVERETT ST #303	PORTLAND OR 97209
7			CURRENT RESIDENT	1975 NW EVERETT ST #102	PORTLAND OR 97209
4			CURRENT RESIDENT	1975 NW EVERETT ST #104	PORTLAND OR 97209
9			CURRENT RESIDENT	1975 NW EVERETT ST #106	PORTLAND OR 97209
<u> </u>			CURRENT RESIDENT	1975 NW EVERETT ST #107	PORTLAND OR 97209
2			CURRENT RESIDENT CURRENT RESIDENT	1975 NW EVERETT ST #108 1975 NW EVERETT ST #205	PORTLAND OR 97209 PORTLAND OR 97209
1			CURRENT RESIDENT	1975 NW EVERETT ST #205	PORTLAND OR 97209 PORTLAND OR 97209
1			CURRENT RESIDENT	1975 NW EVERETT ST #209	PORTLAND OR 97209
5			CURRENT RESIDENT	1975 NW EVERETT ST #210	PORTLAND OR 97209
			CURRENT RESIDENT	1975 NW EVERETT ST #304	PORTLAND OR 97209
1			CURRENT RESIDENT	1975 NW EVERETT ST #309	PORTLAND OR 97209
1			CURRENT RESIDENT	1975 NW EVERETT ST #311	PORTLAND OR 97209
<u> </u>			CURRENT RESIDENT	1975 NW EVERETT ST #401	PORTLAND OR 97209
			CURRENT RESIDENT	1975 NW EVERETT ST #403	PORTLAND OR 97209
			CURRENT RESIDENT	1975 NW EVERETT ST #409	PORTLAND OR 97209
2			CURRENT RESIDENT	1975 NW EVERETT ST #501	PORTLAND OR 97209
			CURRENT RESIDENT	1975 NW EVERETT ST #507	PORTLAND OR 97209
			CURRENT RESIDENT	1975 NW EVERETT ST #509	PORTLAND OR 97209
			CURRENT RESIDENT	210 NW 17TH AVE #200	PORTLAND OR 97209
7			CURRENT RESIDENT	325 NW 17TH AVE	PORTLAND OR 97209
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707				CURRENT RESIDENT	550 NW 19TH AVE #607	PORTLAND OR 97209
708				CURRENT RESIDENT	550 NW 19TH AVE #613	PORTLAND OR 97209
709	RETURN SERVICE REQUESTED	OWNER	1N1E33DB 2800	NW NEIGHBORHOOD CULTURAL CENTER	PO BOX 96116	PORTLAND OR 97296
710	RETURN SERVICE REQUESTED	APPLICANT	HARTSHORNE PLUNKARD ARCHITECTURE	BECKER ANDREW	315 W WALTON ST	CHICAGO IL 60610
711	RETURN SERVICE REQUESTED	OWNERS AGENT	GAIA VENTURES I LLC	TOBY TANYA	5821 SE POWELL BLVD	PORTLAND OR 97206
712	RETURN SERVICE REQUESTED		NEIGHBORS WEST-NORTHWEST	GARRETT DARLENE URBAN	2257 NW RALEIGH ST	PORTLAND OR 97210
713	RETURN SERVICE REQUESTED		LAND USE CONTACT	NORTHWEST PORTLAND BA	25 NW 23RD AVE #6-PMB 217	PORTLAND OR 97210
714	RETURN SERVICE REQUESTED		NORTHWEST DISTRICT ASSOCIATION	THEISEN GREG	1846 B NW ASPEN AVE	PORTLAND OR 97210
715	RETURN SERVICE REQUESTED		LAND USE CONTACT	PEARL DISTRICT BA	PO BOX 6767	PORTLAND OR 97228
716	RETURN SERVICE REQUESTED		PEARL DISTRICT NA	FARHOODI REZA & DYSERT DAVID	2257 NW RALEIGH STREET	PORTLAND OR 97210
717	RETURN SERVICE REQUESTED		PORTLAND SCHOOL DISTRICT	LAND USE NOTICE CONTACT	501 N DIXON	PORTLAND OR 97227
718	RETURN SERVICE REQUESTED	LAND USE CONTACT	STADIUM DISTRICT BUSINESS GROUP	C/O VENTURE PORTLAND	1125 SE MADISON ST #112	PORTLAND OR 97214
	RETURN SERVICE REQUESTED		LAND USE CONTACT	CENTRAL CITY CONCERN	232 NW 6TH AVE	PORTLAND OR 97209
	RETURN SERVICE REQUESTED		LAND USE CONTACT	GOOSE HOLLOW BUSINESS ASSOCIATION	PO BOX 915	PORTLAND OR 97207
	RETURN SERVICE REQUESTED		LAND USE CONTACT	GOOSE HOLLOW FOOTHILLS LEAGUE	2257 NW RALEIGH STREET	PORTLAND OR 97210
	RETURN SERVICE REQUESTED			JUDY PETERS	6916 NE 40TH ST	VANCOUVER WA 98661
	RETURN SERVICE REQUESTED		LAND USE CONTACT	STATE HISTORIC PRESERVATION OFFICE	725 SUMMER NE #C	SALEM OR 97301
	RETURN SERVICE REQUESTED		PORTLAND METRO REGIONAL SOLUTIONS	C/O DLCD REGIONAL REPRESENTATIVE	1600 SW FOURTH AVE #109	PORTLAND OR 97201
	RETURN SERVICE REQUESTED		LAND USE CONTACT	PORT OF PORTLAND PLANNING	PO BOX 3529	PORTLAND OR 97201 PORTLAND OR 97208
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_	RETURN SERVICE REQUESTED		LAND USE CONTACT	TRANSIT DEVELOPMENT	1800 SW FIRST AVE #300	PORTLAND OR 97201
	RETURN SERVICE REQUESTED		DION C TAND DEDARMINE	KARLA MOORE-LOVE (CITY HALL)	1221 SW 4TH AVE #130	PORTLAND OR 97204
	RETURN SERVICE REQUESTED		RISK & LAND DEPARTMENT	NW NATURAL	250 SW TAYLOR ST	PORTLAND OR 97204-3038
120	RETURN SERVICE REQUESTED		LAND USE CONTACT	PACIFIC POWER & LIGHT LAND USE CONTACT	7544 NE 33RD DR PROSPER PORTLAND	PORTLAND OR 97211 129/PROSPER
123						

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731					HEARINGS CLERK	299/3100
732					DAWN KRANTZ	B299/R5000

1120 SW Fifth Avenue, Suite 613, Portland, Oregon 97204 • Mingus Mapps, Commissioner • Michael Jordan, Director

#### Land Use Response

Date: February 23, 2023

To: Hannah Bryant, BDS Land Use Services

503-865-6520, Hannah.Bryant@portlandoregon.gov

From: Abigail Cermak, BES Systems Development

503-823-7577, Abigail.Cermak@portlandoregon.gov

Case File: LU 22-204531

Location: 1819 NW EVERETT ST

**R#:** R140879, R140882

Proposal: HLC HEARING - The proposal includes two elements: First, the adaptive reuse of an existing three-

story Historic Landmark to convert a former church into a private club with a hotel, restaurants, spa and bar. Second, the construction of a new, detached, six-story hotel with a ground floor restaurant

and affordable commercial retail space on a vacant parcel within the Landmark boundary.

Modification requests [PZC 33.846.070]:1. Increase the building height from 60-feet to 69-feet 9inches (33.130.210.B.1, Table 130-2, Table 130-3); 2. Reduce the dimensions of a Standard A loading zone from 35-feet long by 10-feet wide by 13-feet high to 25-feet long by 9-feet wide by 10feet high (33.266.310.D); 3. Increase the maximum distance short-term bike parking spaces may be from a main entrance from 50-feet to 160-feet (33.266.210.E.1.a); and Adjustment request [PZC 33.805]: Reduce the quantity of short-term bike parking stalls from 19 to 17 (33.266.200.B, Table

266-6). Allow loading to be entered and exited in a rearward motion (33.266.310.F.1).

The following comments are based on the land use review plans and documents provided to the Bureau of Environmental Services (BES). Some references to Portland City Code (PCC) are included below; the applicant may also refer to the Auditor's Office Online Charter and Code page.

#### A. RESPONSE SUMMARY

BES does not object to approval of the Historic Resource Review with Modifications application.

The proposed development will be subject to BES standards and requirements during the permit review process.

#### **B. SANITARY SERVICE**

For BES to recommend approval of the historic resource review application, the applicant must demonstrate that the proposed project will accommodate sanitary disposal facilities that are approvable under PCC 17.32.

- Existing Sanitary Infrastructure: According to available GIS data, the following sewer infrastructure is located in the vicinity of the project site:
  - There is a public 18-inch concrete (RCP) combined sewer in NW Everett St (BES asa. built # E08880).
  - There is a public 15-inch PVC combined sewer in NW Flanders St (BES as-built # E08880). BES models predict that the upstream and downstream portions of this sewer may surcharge during the 2-year storm event.
  - The Lower NW Sewer and Stormwater Capital Improvement Project (CIP) to address sewer capacity issues in this area is currently in the planning stages. Please note that construction is expected to begin after Spring 2024 and your project site is within the

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vicinity of the planned sewer work. For more information, please go to <a href="https://www.portland.gov/bes/improvements/construction/lower-northwest-sewer-and-stormwater-project">https://www.portland.gov/bes/improvements/construction/lower-northwest-sewer-and-stormwater-project</a>.

- Service Availability: Sanitary connections from private property that are to be permitted
  according to PCC 17.32.090 must be separately conveyed to the property line and
  connected through individual laterals to a City sanitary or combined sewer. All discharge
  must be connected via a route of service approved by the BES Chief Engineer.
  - a. *Existing Development*: According to City records, the existing historic building to remain is currently connected to the combined sewer in NW Everett St via a lateral located approximately 220 feet east from the maintenance hole (asset ID APU267) near the intersection of NW 9<sup>th</sup> Ave and NW Everett St.
  - b. *Proposed Development*: The existing building will maintain its sanitary sewer connection. The new hotel building on the north lot will be served by a new connection to the combined sewer in NW Flanders St within its frontage. The requested adjustment does not appear to have an impact on the availability of sanitary service for this project.
- 3. Connection Requirements: Connections to the City sewer system must meet the standards of the City of Portland's Sewer and Drainage Facilities Design Manual, PCC 17.32.090, administrative rules ENB-4.07 and ENB-4.17, and all other relevant City codes and rules. Sanitary sewage from private property must be separately conveyed to the property line and connected through individual laterals for discharge to the City separate sanitary or combined sewer. Per ENB-4.07, sewer connection permits are required to make new connections to City mains and laterals, relocate or upsize existing laterals, and repair sewers in City right-of-way. The permittee is responsible for verifying the location, depth and size of an existing sewer lateral and for ensuring the lateral is clear of obstructions prior to connection.

Staff finds the applicant's proposed sanitary sewer service acceptable for the purpose of reviewing the historic resource review application.

#### C. STORMWATER MANAGEMENT

For BES to recommend approval of the historic resource review application, the applicant must demonstrate that the proposed project will accommodate stormwater management facilities that are approvable under PCC 17.38.

- 1. Existing Stormwater Infrastructure: According to available GIS data, the following stormwater infrastructure is located in the vicinity of the project site:
  - a. There is a public 42-inch HDPE storm sewer in NW Flanders St (BES as-built # 5500). Stormwater from this system discharges to the Willamette River at OF 11 (asset ID ABG369).
  - Currently, stormwater from the public right-of-way discharges to existing combined sewer infrastructure.
- 2. General Stormwater Management Requirements: Development and redevelopment sites that include any of the triggers listed in PCC 17.38.040 are subject to the policies and standards of PCC 17.38.035, Portland's <u>Stormwater Management Manual</u> (SWMM) and <u>Source Control Manual</u> (SCM). Projects must comply with the current adopted version of the SWMM as of the permit application date. A fundamental evaluation factor in the SWMM is the Stormwater Infiltration and Discharge Hierarchy (Section 1.3.3), which sets the framework that will be used to determine when a project's stormwater runoff must be infiltrated onsite and when offsite discharge will be permitted, and the parameters that must be met for either scenario.
- 3. *Private Property Stormwater Management*: Stormwater runoff from this project must comply with all applicable standards of the SWMM and SCM and be conveyed to a discharge point

along a route of service approved by the BES Director or the Director's designee.

- a. Existing Development: Based on City records and the submitted site plan, it appears stormwater runoff from the existing historic building currently discharges to weep holes at the street, to the parking lot north of the building (the area to be redeveloped with the hotel), through downspouts to lawn area, and/or to the combined sewer in NW Everett St.
- Approach stormwater report from KPFF (dated January 2023) and the geotechnical report from Hardman Geotechnical Services Inc. (dated March 4, 2022). The submitted geotechnical report includes falling head infiltration test results of 27 to 56 inches per hour on this site. Due to the addition of a third floor to the existing historic building, the applicant proposes to reroute the existing roof drainage to onsite drywells (depth of 25 ft) that appear to meet the minimum setbacks as established in the facility design standards and Table 2-1 of the SWMM. For the new hotel building, the applicant also proposes onsite infiltration using a drywell. A design infiltration rate of 23 inches per hour was used to size the drywells.

The proposed stormwater management system for the hotel includes a drywell (depth of 25ft) that appears to have reduced setbacks to the proposed building. BES has determined that the proposed facility is an allowable method of stormwater management per the SWMM and the submitted sizing information is sufficient. The drywells have been sized for the 100-year storm event. The submitted information is sufficient for the purpose of this land use review. However, to address the proposed setback encroachment, at the time of building permit review the applicant must submit additional documentation and analysis from a geotechnical engineer, structural engineer, registered geologist or other environmental professional as described in Section 2.2.4 of the SWMM. In addition, structural design of the building will need to accommodate the drywell and will be reviewed by BDS during permit review.

Additionally, it should be noted that infiltration tests were not conducted on the southern lot with the existing historic structure. While onsite infiltration appears to be feasible in this area based on the infiltration test results of the lot to the north, BES may require additional infiltration tests at building permit to confirm a design infiltration rate for the southern lot.

With the building permit submittal, please also include the basin map for each area discharging to the drywells as well as the SBUH modeling information which was not provided with the stormwater report submitted for this review.

The requested adjustment does not appear to have an impact on the location of the stormwater management system for this project.

- 4. Public Right-of-Way Stormwater Management: Stormwater runoff from public right-of-way improvements as required by the City of Portland Bureau of Transportation (PBOT) must be managed according to the standards of the SWMM and the Sewer and Drainage Facilities Design Manual.
  - a. PBOT requires construction, expansion or replacement of sidewalk behind the existing curbs in NW Everett St, NW 18<sup>th</sup> Ave, NW Flanders St, and NW 19<sup>th</sup> Ave, which qualify for payment of an offsite management fee-in-lieu of building public stormwater facilities. Based on the submitted dedication and sidewalk improvement plans, the applicant is not proposing public stormwater facilities in the right-of-way. Please note that the applicant will be required to pay the offsite management fee with the Public Works Permit if the stormwater management requirements of the SWMM are triggered by the proposed public right-of-way improvements. However, if the applicant decides to build facilities instead of paying the offsite management fee, this change may impact right-of-way dedication amounts and/or necessitate revisions to the land use decision or Public Works Permit. Payment of the offsite management fee will occur with the Public Works

### Permit.

5. Clean River Rewards Program: Clean River Rewards, Portland's stormwater discount program, offers discounts up to 100% of the City's onsite stormwater management charge to ratepayers who manage stormwater runoff on their property. Please note that this discount is not automatically applied; ratepayers must register their property and describe how stormwater is being managed to qualify. Be aware that some properties discharging to the Multnomah County Drainage Districts (within the Columbia Slough watershed) are not charged an onsite stormwater management fee by the City and are therefore ineligible to register for this program. See the <u>Clean River Rewards website</u> or call 503-823-1371 for more information.

Staff finds the applicant's proposed stormwater management plan is acceptable for the purpose of reviewing the historic resource review application.

### D. CONDITIONS OF APPROVAL

BES has no recommended conditions of approval.

### **E. PERMIT INFORMATION**

At the time of permit review the applicant should be aware of the following:

- Connection Fees: Sewer system development charges and connection fees are assessed at the time of building plan review and change every fiscal year on July 1<sup>st</sup>. For additional information on these fees use the <u>BDS Online Fee Estimator</u> or call the BES Development Review Team at 503-823-7761, option 2.
- 2. Building Plans: Building plans for this project must include a detailed site utility plan which shows proposed and existing sanitary connections, as well as stormwater management that meets the requirements of the version of the SWMM that is in effect at the time permit applications are submitted.
- 3. *UIC Registration*: The Oregon Department of Environmental Quality (DEQ) regulates underground injection control (UIC) facilities to protect groundwater. Drywells and soakage trenches are examples of UICs. It is the applicant's responsibility to register all onsite UICs with DEQ, as appropriate. To learn more visit <u>DEQ's website</u> or contact the DEQ UIC Program at 503-229-5623. The SWMM also includes general UIC information.
- 4. Source Control Requirements: Source control requirements from the Source Control Manual (SCM), Portland City Code (PCC) Title 17, and BES Administrative Rules that may be applicable to this project are listed below with the corresponding chapter, section, code, and/or rule. For specific questions on the following, please contact BES Source Control at 503-823-7122.
  - a. Site Use and Activity-Based Source Control Requirements (SCM Chapter 6): BES recommends the applicant review the following SCM sections to understand the structural, treatment, and operational BMP requirements that may impact the project design.
    - 1) Waste and Recycling Storage (SCM Section 6.1)
    - 2) Covered Vehicle Parking (SCM Section 6.3)
    - 3) Material Transfer and Loading Docks (SCM Section 6.4)
  - b. Site Dewatering Requirements (SCM Chapter 9, PCC 17.34, PCC 17.36, PCC 17.38, PCC 17.39, ENB 4.32) BES evaluates requests for temporary and long-term stormwater and groundwater dewatering discharges into the city sewer system for approval or denial. See Appendix 1 of the SCM for the Construction Dewatering Discharge Application Form. If approved, a Discharge Permit for the storm or combined sewer may be required.

- 1) Fees are assessed for temporary construction discharges to the public sewer system navigate <u>HERE</u> for current rates and information about dewatering as it relates to construction projects.
- 2) Construction discharges to City UICs are prohibited.
- 3) Construction discharges to private UICs (e.g., drywells or soakage trenches) must be authorized by DEQ's UIC Program.
- c. Grease Management Program (<u>PCC 17.34</u>, <u>ENB 4.26</u>): The City requires grease management (GM) devices in all food service establishments and for any business/industry that may introduce fats, oils, or grease (FOG) into the public sewer. A monitoring access structure (MAS) may also be required. Please refer to the MAS discussion below regarding MAS requirements. For additional information, go to the <u>Cut Through the FOG</u> webpage.
- d. Extra Strength Sewer Charge Program (PCC 17.34, PCC 17.36, ENB 4.25): Food service establishments and some other industries incur a surcharge on their sewer bills. A monitoring access structure (MAS) may also be required. Please refer to the MAS discussion below regarding MAS requirements. Establishments that employ best management practices can apply for a rate reduction. Call 503-823-7093 for more information.
- e. Monitoring Access Structures (<u>PCC 17.34</u>, <u>PCC 17.39</u>, <u>ENB 4.03</u>, <u>ENB 4.25</u>, <u>ENB 4.26</u>, <u>ENB 4.35</u>): Monitoring Access Structures (MAS) for sampling wastewater or stormwater discharges may be required for users that introduce, or are expected to introduce, pollutants into the public sewer from any non-domestic source. Please refer to the <u>MAS Administrative Rules</u> for additional information regarding MAS requirements.
- f. Water Features (<u>PCC 17.34</u>, <u>PCC 17.39</u>): If a water feature, such as a fountain or pool, uses treated water (e.g., chlorinated or biocides), the feature water and filter backwash must discharge to the sanitary waste line. If the feature water is not treated, under most circumstances the water can discharge to the storm sewer. If the source of the feed water is rainwater and required to discharge to the City sanitary sewer system or groundwater, a meter is required to assess sewer user fees; current rates are located HERE.

#### F. ADMINISTRATIVE REVIEW

The applicant may request a modification of a decision presented in this response, as applicable, via an administrative review as outlined in PCC sections 17.06.050, 17.32.150, 17.33.100, 17.34.115, 17.36.110, 17.38.060 and 17.39.120 and in those sections' associated administrative rules. Some portions of this response are not decisions, but guidance related to requirements that this proposal may be subject to during City review of other processes, such as a building permit or public works permit review. While these are not decisions that are ripe to be considered through an administrative review, if the outcome of a future administrative review needs to be anticipated at this time in order to inform the land use action, the administrative review process may be utilized. Some items, such as technical standards, are not reviewable. For guidance on whether a modification can be requested and whether the land use process is the proper time to request it, consult with the BES staff identified above prior to submitting a request.

There is no fee charged for an administrative review, and all BES penalties and late fees will be stayed pending the outcome of the review process, as applicable. To request an administrative review, the applicant must complete the Administrative Review Request Form (located here: <a href="https://www.portlandoregon.gov/bes/68285">www.portlandoregon.gov/bes/68285</a>) and submit it to the Systems Development staff listed above within 20 business days of the mailing date of this response. The applicant should coordinate with the BDS planner to determine whether applying for an administrative review would have an impact on state-mandated land use timelines.



1900 SW Fourth Ave., Suite 5000 Portland, OR 97201 503-823-5185

Fax 503-823-7576 TTY 503-823-6868 www.portlandoregon.gov/transportation

Mingus Mapps Commissioner Tara Wasiak Interim Director

### RESPONSE TO THE BUREAU OF DEVELOPMENT SERVICES LAND USE REVIEW REQUEST

# Portland Transportation Development Review Bureau of Transportation Engineering & Development

LU: 22-204531-000-00-LU Date: February 23, 2023

To: Hannah Bryant, Bureau of Development Services, B299/R5000

From: Michael Pina, B1720206/800, 503-823-4249, michael.pina@portlandoregon.gov

Applicant: Andrew Becker, HARTSHORNE PLUNKARD ARCHITECTURE

315 W. WALTON ST CHICAGO IL 60610

Location: 1819 NW EVERETT ST

TYPE OF REQUEST: Type 3 procedure HRM - Historic Resource Review w/Modifications

### **DESCRIPTION OF PROJECT**

HLC HEARING - Type III Historic Resource Review to convert the former Landmark church into a club with restaurants, spa and bar, and construction of a new, detached, six-story hotel on a vacant parcel within the Landmark boundary. One loading space and no parking is proposed in the hotel. Three Modifications and two Adjustments are requested: to increase building height; reduce the dimensions of the loading space; increase the distance between short-term bike parking and main entrances; reduce the quantity of short-term bike parking, and allow loading to be entered/exited in a rearward motion. Historic Resource Review is required for non-exempt exterior alterations and non-exempt development in a Historic District.

### RESPONSE

Portland Bureau of Transportation (PBOT) Development Review has reviewed the application for its potential impacts regarding the public Right-of-Way (ROW), traffic impacts and conformance with adopted policies, street designations, Title 33, Title 17, and for potential impacts upon transportation services.

### Historic Resource Review Approval Criteria (33.846.060.G)

There are no applicable transportation-related approval criteria associated with the proposed Historic Resource Review, however, the project impacts the public right-of-way, therefore warrants a review from PBOT.

The applicant proposes to renovate the existing Northwest Neighborhood Cultural Center building, which fronts onto NW Everett St; and construct a new hotel building oriented to the NW 19<sup>th</sup> Ave and NW Flanders St intersection. The project constitutes both an increase in trip generation to the site and a exceeds the Significant Alteration threshold in accordance with 17.88.020. Therefore, dedication and frontage improvements are required surrounding the three-quarter block site. Required improvements include a 4-ft dedication on NW Everett St around the existing historic stairs, reconstructed pedestrian corridors on all four streets, and three new ADA corners. Final Right-of-Way configuration and improvement details will be determined through the Public Works permit process, which the applicant has yet to initiate at time of this response.

In addition, the applicant submitted a Loading Demand Study (22-180156 TR) requesting a reduction in on-site loading space from a Standard A to a Standard B. Following the PBOT's analysis, it was found that forty percent of the anticipated loadings to the site will be larger than a Standard B space. The on-site loading space dimensions necessary to accommodate these vehicles would be roughly 25-ft long. There is anticipated to be 2.4 daily loading trips to the site. This equates to about 17 weekly trips, 7 of which would need a space larger than a Standard B. Therefore, PBOT has approved a modified Standard A space measuring 25-ft in length, 10-ft in width, and an 11-ft vertical clearance. Furthermore, while not a Title 17 requirement, PBOT reviewed the request to access the site in a rearward motion and found that due the NW Flanders St Local Service Traffic Classification, the Modification request was not found to be detrimental to the transportation system.

### RECOMMENDATION

PBOT has no objection to the proposed Historic Resource Review or associated Modifications.



### City of Portland, Oregon - Bureau of Development Services

1900 SW Fourth Avenue • Portland, Oregon 97201 | 503-823-7300 | www.portland.gov/bds



To: Hannah Bryant

From: Chanel Horn, Life Safety Plans Examiner

Date: February 21, 2023

RE: 1819 NW EVERETT ST, 22-204531-LU

### LIFE SAFETY PLAN REVIEW RESPONSE

The following comments are based on the plans and documents provided to the Life Safety Plan reviewer. They are intended to provide the applicant with preliminary Building Code information that could affect the Land Use Review, Public Records request and/or future Building Permit reviews. The comments may not identify all conflicts between the Land Use proposal and the Building Codes. A complete Life Safety plan review will be provided at the time of Building Permit submittal at which time any additional Building Code issues will be noted. The comments are based on the Oregon Structural Specialty Code (OSSC), the International Existing Building Code (IEBC), the Oregon Mechanical Specialty Code (OMSC), or the Oregon Residential Specialty Code (ORSC).

### **RESPONSE SUMMARY**

🖄 Life Safety Plan Review does not object to the approval of this proposal. The applicant should be aware
that several building code requirements may impact the final design of this building. For information regarding future
compliance, see the GENERAL LIFE SAFETY COMMENTS below.
Life Safety Plan Review does not object to the approval of this proposal. This approval is conditional on the
finalization of the property line adjustment approved through this LUR/PR. If this public record is not finalized, a
Covenant Not to Sell the Properties Separately must be established for this project. For information regarding future
compliance, see the GENERAL LIFE SAFETY COMMENTS below.
Life Safety Plan Review does not object to the approval of this proposal. Prior to Life Safety approval of the
final plat or Land Use proposal, the applicant must address the Building Code issues listed as part of the GENERAL
LIFE SAFETY COMMENTS below.
Life Safety Plan Review cannot support approval of the current Land Use proposal. Prior to Life Safety
approval of the final plat, the applicant must address the Building Code issues listed as part of the GENERAL LIFE
SAFETY COMMENTS below.

Item #	GENERAL LIFE SAFETY COMMENTS
1	<b>Building Permits Required -</b> Separate Building Permits are required for the work proposed and the proposal must be designed to meet all applicable building codes and ordinances. Information about submitting a permit application request is available online at: <a href="https://www.portland.gov/bds/permit-review-process/apply-or-pay-permits">https://www.portland.gov/bds/permit-review-process/apply-or-pay-permits</a> .
2	<b>Preliminary Meeting Recommended-</b> It is recommended the applicant request a Preliminary Life Safety Meeting to verify building code requirements. Information about the Preliminary Meeting process is available online at: <a href="https://www.portland.gov/bds/early-assistance/life-safety-preliminary-meetings">https://www.portland.gov/bds/early-assistance/life-safety-preliminary-meetings</a> .
3	<b>Assumed Property Line</b> - For the purposes of determining the required wall, opening protection, and roof-covering requirements, buildings on the same lot shall be assumed to have an imaginary property line somewhere between them. (OSSC 503.1.1, 705.3)
4	Allowable Building Height – The height and number of stories of a building shall be determined based on the type of construction, occupancy classification, and whether there is an automatic sprinkler system installed throughout the building. (OSSC 504.1)

Jo Ann Hardesty, Commissioner Sara Boone, Fire Chief AJ Jackson, Fire Marshal Prevention Division 1300 SE Gideon Street Portland, OR, 97202

Phone: (503) 823-3770

### LAND USE REVIEW RESPONSE

TO: Hannah Bryant, City of Portland, Land Use Review FROM: Dawn Krantz, Portland Fire Bureau 503-823-3718

DATE: February 17, 2023 SUBJECT: LU 22-204531 HRM

**SITE LOCATION: 1819 NW EVERETT ST** 

The following conditions of approval and informational comments are based on the land use review information provided to the Fire Bureau. Fire Bureau requirements are generated from the 2021 Portland Fire Code. All current Fire Code requirements apply and are required to be met. If these conditions cannot be met, an appeal providing an alternative method is an option for the applicant. If the applicant chooses to appeal a requirement, the appeal must be listed as a condition in the decision. Fire Code Appeals can be obtained at the Fire Bureau web page, <a href="https://www.portlandonline.com">www.portlandonline.com</a>.

### CONDITIONS OF APPROVAL AT TIME OF DEVELOPMENT

A separate building permit is required for this proposal. All applicable Fire Code requirements shall be met at the time of permit review and development.



1900 SW 4<sup>th</sup> Ave. Ste 5000, Portland, OR 97201 Tel: 503-823-TREE (8733) Fax: 503-823-4493 email: trees@portlandoregon.gov web: portlandoregon.gov/trees

# Urban Forestry Land Use Review Response

Date: February 17, 2023 From: Mariano Masolo

503-823-4560, Mariano.Masolo@portlandoregon.gov

Case File: 22-204531-000-00-LU
Location 1819 NW EVERETT ST

Proposal: HLC HEARING - Type III Historic Resource Review to convert the former Landmark church into a club with restaurants, spa and bar, and construction of a new, detached, six-story hotel on a vacant parcel within the Landmark boundary. One loading space and no parking is proposed in the hotel. Three Modifications and two Adjustments are requested: to increase building height; reduce the dimensions of the loading space; increase the distance between short-term bike parking and main entrances; reduce the quantity of short-term bike parking, and allow loading to be entered/exited in a rearward motion.

Historic Resource Review is required for non-exempt exterior alterations and non-exempt development in a Historic District.

Urban Forestry has reviewed the proposal for its impact on existing city trees, street trees and heritage trees, street tree planting requirements and related mitigation in accordance with Title 11, Trees and for potential impacts upon urban tree canopy. It is the applicant's responsibility to disclose all aspects of their land use proposal that may impact required street tree plantings and existing street trees during the land use review process.

UNLESS EXPLICITLY STATED HEREIN, THIS REVIEW DOES NOT APPROVE STREET TREE REMOVALS AND DOES NOT PROVIDE ANY EXEMPTIONS TO TITLE 11 REQUIRMENTS.

Permits required after land use approval are subject to all applicable development standards and all provisions of the City Code, including Title 11. Title 11 regulations will be applied during the permit review process.

### PLEASE NOTE THERE MAY BE OTHER APPLICABLE TREE REQUIREMENTS AS PER TITLE 33 PLANNING & ZONING.

### A. Response Summary

Urban Forestry does not object to approval of the land use proposal. The proposed development will be subject to Title 11 regulations during the permit review process.

### A. Tree Plan (11.50.070)

A Title 11 compliant tree plan must be submitted with each phase of development review and permitting including land use reviews, building permits, and public works permits. The same tree plan shall be included with each permit.

A tree plan was submitted with the land use proposal.



The following information is required for street trees:

- a. Existing improvements
- a. Construction staging areas
- b. Proposed alterations including structures, impervious areas, grading, and utilities
- c. Exiting trees
  - 1. Private trees at least 12 inches in diameter
  - 2. Street trees at least 3 inches in diameter
- d. Proposed tree activity
  - 1. Indicate trees to be retained and proposed tree protection
  - 2. Indicate trees to be removed
  - 3. Show location, species, planting size and number of trees to be planted
- e. Protection plan
  - i. The protection plan must describe the potential impacts of construction methods, staging areas, equipment usage, loading areas, and building materials that will impact regulated trees. The plan must describe how the existing street, heritage, and city trees will have continued protection, in accordance with the protection requirements of 11.60, during the proposed development.

### **B.** Street Trees

1. Existing Street Conditions

According to available GIS data, the frontages have the following configuration.

a. NW Everett St: The site has approximately 200 feet of street frontage. The right-of-way is improved with pavement, curbs, planting cutouts, sidewalks. There are no overhead high voltage power lines. There are 3 street trees.

A field visit by Urban Forestry staff have confirmed the following trees:

- i. 1 Newport plum (Prunus cer. 'Newport') 25" DBH. Tree is in poor condition.
- ii. 2 elm (*Ulmus 'Frontier'*) 5" DBH. Trees are in good condition.
- b. NW 18<sup>th</sup> Ave: The site has approximately 100 feet of street frontage. The right-of-way is improved with pavement, curbs, planting strip, sidewalks There are no overhead high voltage power lines. There are 2 street trees.

A field visit by Urban Forestry staff have confirmed the following:

- i. 2 linden (Tilia ssp) 23"-35" DBH. Trees are in fair condition.
- c. NW Flanders St: The site has approximately 100 feet of street frontage. The right-of-way is improved with pavement, curbs, planting strip, sidewalks. There are overhead high voltage power lines. There are 2 street trees.

A field visit by Urban Forestry staff have confirmed the following trees:

- i. 2 elm (*Ulmus americana*) 31" 35" DBH. Trees are in fair to poor condition.
- d. NW 19<sup>th</sup> Ave: The site has approximately 200 feet of street frontage. The right-of-way is improved with pavement, curbs, planting strip, sidewalks. There are overhead high voltage power lines. There are 2 street trees.

A field visit by Urban Forestry staff have confirmed the following trees:

- i. 2 elm (*Ulmus americana*) 31" 35" DBH. Trees are in fair to poor condition.
- 2. Street Tree Preservation (11.50.040)

The applicant proposes to remove the following 5 trees:

• 1 (One) Newport plum along NW Everett St, 2 (two) elm trees along NW Flanders St. and 2 (two) elm trees along NW 19<sup>th</sup> Ave.

The proposed tree removals may be permitted for removal during the appropriate development permit.



As per the current species, size, and condition of trees removed, it is estimated eight (8) trees may be required in accordance with the <u>Administrative Rule PRK-2.04</u> to mitigate the tree canopy loss as a result of the project.

- Each approved elm tree will require 2:1 mitigation if removed.
- No mitigation will be required for the removal of the Newport plum.

Mitigation for trees removed shall occur in the street planter strip, on site, or in the same watershed either by planting or by paying a fee in lieu of planting for each tree not planted. Mitigation will be required through the appropriate development permit. Fees-in-lieu will be charged in accordance with the <u>Title 11 Trees Fee Schedule</u>. Street tree planting standards must also be met in accordance with 11.50.060.C.

All trees not approved for removal are required to be retained and protected during all stages of the development.

### 3. Street Tree Protection Specifications (11.60.030)

The applicant has not provided a street tree protection plan. Tree protection is required for all trees required to be retained in accordance with Title 11 Trees, Protection Methods (11.60.030). Tree protection shall follow either the Prescriptive or Performance path. Protection methods must be shown on the tree plan. If using the Performance path, the alternate tree protection plan must be prepared by an arborist who has visited the site. The protection plan must describe the potential impacts of construction methods, staging areas, equipment usage, loading areas, and building materials that will impact regulated trees.

### 4. *Street Tree Planting* (11.50.060.C)

The applicant has provided a street tree planting plan. One street tree must be planted or retained for each full increment of 25 linear feet (11.50.060.C.1).

Street trees must be planted at a minimum 2.5 caliper inches.

- a. NW Everett St: There are approximately 200 feet of street frontage at this location, the existing utilities will reduce the required number of trees by 2 requiring 6 trees to be planted or retained.
- a. NW 18<sup>th</sup> Ave: There are approximately100 feet of street frontage at this location, the existing tree's setting will reduce the required number of trees by 1 requiring 3 trees to be planted or retained.
- b. NW Flanders St: There are approximately 100 feet of street frontage at this location, the existing utilities will reduce the required number of trees by 1 requiring 3 trees to be planted or retained.
- c. NW 19<sup>th</sup> Ave: There are approximately 200 feet of street frontage at this location, requiring 8 trees to be planted or retained.

Street tree planting may be exempt under <u>11.50.060.B</u> when existing above or below grade utilities prevent planting street trees or when the existing planting strip is less than 3-feet wide.

When the required number of trees cannot be planted, a fee in lieu of planting may be required during the permit review process. Fees-in-lieu will be charged in accordance with the <u>Title 11 Trees Fee Schedule</u>.

### C. Recommendations

Urban Forestry has no objection to the proposed project.



### **Land Use Review Response**

Site Development Section, BDS

To: Hannah Bryant, LUR Division

From: Ye Zhuang, Site Development (503-823-7901)

Location/Legal: BLOCK 172 LOT 1-4, COUCHS ADD; BLOCK 172 LOT 6&7, COUCHS ADD

Land Use Review: LU 22-204531

Proposal: HLC HEARING - Type III Historic Resource Review to convert the former Landmark

church into a club with restaurants, spa and bar, and construction of a new, detached, six-story hotel on a vacant parcel within the Landmark boundary. One loading space and no parking is proposed in the hotel. Three Modifications and two Adjustments are requested: to increase building height; reduce the dimensions of the loading space; increase the distance between short-term bike parking and main entrances; reduce the quantity of short-term bike parking, and allow loading to be entered/exited in a rearward motion. Historic Resource Review is required for non-exempt exterior alterations and

non-exempt development in a Historic District.

Quarter Sec. Map: 3026

Date: February 28, 2023

Site topography. It is relatively flat.

<u>Geotechnical</u>. At the time of building permit application, a geotechnical investigation and engineering report will be required. The report shall provide recommendations for foundation support, seismic design parameters, remediation of potential impact on adjoining properties and adjacent structures, and general earthwork. The geotechnical report must be prepared in compliance with the 2019 Oregon Structural Specialty Code and by a professional engineer licensed in the State of Oregon.

If temporary excavation shoring is necessary, the construction plans and design calculation must be included in the submittal as they are non-deferrable items.

Please direct questions to Ye Zhuang, at 503-823-7901.

<u>Erosion control</u>. Erosion prevention and sediment control requirements found in Title 10 apply to both site preparation work and development. Full compliance with the erosion control requirements of Title 10, as well as maintenance of the erosion control elements, such as silt fences on private property, storm drain inlet protection and bio bags in the public right-of-way, is the responsibility of the property owner, the developer of the land division and the builders of structures on the individual lots. Please refer to the City of Portland *Erosion and Sediment Control Manual* for additional information regarding erosion and sediment control requirements.

### Case file LU 22-204531 HRM AD PC #22-128290

Shannon <shannonlea23@comcast.net>

Fri 2/24/2023 1:38 PM

To: Bryant, Hannah < Hannah. Bryant@portlandoregon.gov>

Cc: Andrew Becker <abecker@hparchitecture.com>

Hello. We are writing about the above case number as neighbors that reside at 410 NW Flanders in the historic Wickersham Bldg.

We approve of this project and also approve of the current proposal. We think it will have a very positive impact on the neighborhood and revitalize the area which has take a hit with the current conditions. We hope the investors/developers will make something architecturally interesting and classy. Six floors will be a good thing if done right. I have spoken to several neighbors and they all support this proposal.

We are wondering about the lack of parking on some level and hope it will not be in our very small K parking permit area. Other than that we are excited about this new Hotel in the historic neighborhood!

Our hope is that the city will make the process easy for the applicants but hold aesthetics to a high standard.

Thank you,

Shannon and David Lea 410 NW Flanders unit 403 Portland

### Case File LU 22-204531 HRM AD - 1819 NW Everett

DennisHarper < DennisHarper@protonmail.com>

Sat 2/25/2023 4:30 AM

To: Bryant, Hannah < Hannah. Bryant@portlandoregon.gov>

(Modified and re-submitted with additional comments)

Hello Hannah,

As today is the deadline to submit comments for the Request for Response relating to the proposed project, I am trying to determine if I should only be commenting on the Modification and Adjustment Requests, or should I also delve into other design issues. I will first look at the former.

Mod Request #1 seems reasonable because it is very important to attain higher ground level ceiling heights. It is a stated goal of the NW Portland design guidelines.

Mod Request #2 - no comment.

Mod Request #3 - Would it be possible to have the developer install bike racks in the sidewalk zone near the hotel entrance on the NW corner, along NW Flanders and NW 19th?

Adj Request for bike rack reduction - This seems acceptable because it is a small reduction that is requested.

Adj Request for rearward movement at Loading - no comment.

Regarding the attached drawings, starting first with the Color Elevation - 1819 NW Everett, Note 2 for existing steel windows and leaded glass has no information included. The public must be given assurance that the historic steel windows and leaded glass will be retained and restored. Obviously interior double glazing will probably be required to meet the Energy Code.

On the Landscape Planting Plan, it is not clear what is proposed on west beside the perennial garden. Along the NW 19th Avenue sidewalk, is that a seating terrace that is shown? If so, what kind of privacy screening or fencing is planned? It must be pleasing to guests seated on the terrace and to the passersby on the sidewalk.

Also on the Landscape Planting Plan, the southern of the two outdoor terraces along NW 18th Avenue does not have wheelchair access either from the sidewalk or from the building. It only has steps.

On Color Elevation - 1880 NW Flanders, Note 5 for exterior cladding above the granite base states Wood Paneling for Paint. The Landmarks Commission should require samples to be submitted and require an explanation of how the material will weather in 5 or 10 years. Is it the right exterior material for a ground floor?

On Sheet C.23 it appears that you are illustrating the difficulty of providing an ADA accessible entrance on the south while showing that there are accessible entrances on the west and east sides at those respective grades. I do not know if the Landmarks Commission can rule on any ADA matter, but

would it be possible to relocate the lamp post southward & eastward just enough to create a clear accessible path to the entrance door? If that would work, then the lamp post twin on the eastern plinth would be located symmetrically. The historic locations of the lamp posts on the symmetrical plinths would be lost, but perhaps this would be an acceptable alternative.

The wrap-around glazed canopy over the roof terrace of the new hotel building is a gigantic improvement and an exciting addition. From the street below and from the apartments in the taller building to the west, the canopy will be an attractive feature. Wrapping the canopy around all four sides really unifies the roof terrace space.

Please include my comments in the official proceedings of the Landmarks Commission meeting on March 13, 2023.

Regards, Dennis Harper ret. Architect NW Portland

Sent from Proton Mail mobile



October 7, 2022

**Hannah Bryant Bureau of Development Services** 1900 SW 4th Avenue Portland, Oregon 97201

RE: EA 22-161762 DA - 1819 NW Everett St. DAR Submittal Review

The NWDA Planning Committee reviewed the DAR drawing submittal dated Sep 26 2022 and the draft Summary Memo dated Oct 3 2022 for the above referenced project on Oct 6 2022. We offer the following comments for your consideration.

### Overall

The NWDA strongly supports the project concept, including the renovation and repurposing of the existing landmark building, and the proposed new hotel building on the adjacent parcel.

### **Requested Modifications**

We have no objections to the potential modifications, including those regarding building height, parapets, elevator overrun, mechanical equipment and loading requirements.

### **Project Design – Existing Landmark**

- Windows the NWDA requests specific detailed information on the proposed replacement windows, their materials, configuration, appearance and performance. We have significant concern about the historic windows and are unlikely to support their replacement;
- Chimney we have no objection to the removal of the existing chimney;
- Existing Elevator Overrun we have no objection to the removal of the existing elevator overrun;
- Sidewalk the NWDA requests clarification on the width of the proposed dedication and its purpose.

### **Project Design - New Hotel**

Entry, Materials, Fenestration – the NWDA does not support the proposed façade composition and strategy. The current iteration of the hotel building, as depicted in the view from the northwest, appears to have regressed from the far clearer, better organized and more coherent approach to the façade design that was proposed at the time of the project's Pre-application Conference in April:





PreApp DAR

The design elements in the previous scheme that are more successful include the corner arrangement, the street level materials, openings and rhythm, the upper floor window composition and types, and the building cornice. The current scheme appears to have added a number of arbitrarily applied design elements that do not work together create a coherent whole;

- **Guest Drop-off** the NWDA requests clarification on how the guest drop-off, curb valet, and other related functions would be accommodated;
- Connection Between the Buildings the NWDA requests clarification on why there is no connection between the two buildings. Their apparent programmatic and functional relationship would seem to suggest such a connection.
- **Street Level Grading** the NWDA requests confirmation that the interior street level floor elevations step up to correspond to the sidewalk grades on NW 19<sup>th</sup> Ave.

Best Regards, the Northwest District Association

+---

Steve Pinger member, Planning Committee

### Case File # LU 22-204531 HRM AD

Charisse Tooze <charisse@theanimalbodyworks.com> Mon 3/6/2023 6:13 PM To: Bryant, Hannah <Hannah.Bryant@portlandoregon.gov>

Dear Ms. Bryant,

In reference to the proposal of the former Landmark church conversion into a club with restaurants, spa and bar, and the addition of a six-story hotel, I want to stress the importance of creating plenty of designated parking spaces in a parking lot for hotel guests and those visiting the restaurants, spa and bar. With the multitude of activities proposed for both overnight stays and daily patrons visiting the hotel, restaurants, spa and bar--both during the day and night—parking is a must in this already carcongested neighborhood. Many of us who live and work in this neighborhood, as well as our guests and clients, park on the street. As it is, we have a difficult time finding places to park at various times of the days, even more during events at Providence Park where the parking spaces are taken over with the influx of stadium people, as well as on Sunday mornings when people are attending the many church services in this area. Without designated parking spaces for your guests, they will most definitely have a difficult time parking a well, and the draw to your facility will predictably reduce, as the competition for restaurants, spas and bars is very high in Portland.

Thank you, Charisse Tooze



### **City of Portland, Oregon - Bureau of Development Services**



1900 SW Fourth Avenue · Portland, Oregon 97201 | 503-823-7300 | www.portland.gov/bds

Land Use Review Ap	plication	File Number:					
FOR INTAKE, STAFF USE ONLY	Y	Qtr Sec Map(s) _	Zoning				
Date Recby		Plan District					
■ Type I □ Type Ix □ Type II □ Type IIx □ Type II	II □ Tvpe IV ELD						
LU Reviews		Historic and/or Design District  Neighborhood  District Coalition  Business Assoc					
[Y] [N] Unincorporated MC							
[Y] [N] Flood Hazard Area (LD & PD o	nly)						
[Y] [N] Potential Landslide Hazard Are	• /						
[Y] [N] 100-year Flood Plain	[Y] [N] DOGAMI	Related File #					
APPLICANT: Complete Email this application a		at apply to the proposal ents to: LandUseIntake					
Development Site Address or Location							
Cross Street		Sq. ft./Ad	creage				
Site tax account number(s)							
R	R	<u>R</u>					
R	R	R					
Describe proposed stormwater dispo	sal methods						
Identify requested land use reviews							
Design & Historic Reviews - For ne  For renovation, p.	ew development, pro		\$ \$_				
<b>AND</b> provide total	project valuation.		\$				
Land Divisions - Identify number of		existing development).					
New street (public	• •		☐ yes ☐ no				
incomes equa	of the units be afforda	able to households with of the median family	☐ yes ☐ no ☐ N/A continued / over 1				

- Applicant Information
   Identify the primary contact person, applicant, property owner and contract purchaser. Include any person that has an interest in your property or anyone you want to be notified. Information provided, including telephone numbers and e-mail addresses, will be included in public notices.
- For all reviews, the applicant must sign the Responsibility Statement.
- For land divisions, all property owners must sign the application.

PRIMARY CONTACT:						l a also accela deca dhèa h sa a d
Typed Full Name	·····					I acknowledge this typed name as my signature
Company/Organization_						
Mailing Address						
City					Zip Code_	
Day Phone		FAX		email		
Check all that apply	☐ Applicant	Owner	☐ Other			
Typed Full Name						I acknowledge this typed name as my signature
Company/Organization						
Mailing Address						
City			State		Zip Code	
Day Phone		FAX		email		
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Check all that apply	☐ Applicant	☐ Owner	Other			
Responsibility Statement of the information submitted gaining the permission of the statement with them. If the Deed Records for the properties property. In order to proper of the review. I undersunder-standing and agreent Name of person submitting	d. The information owner(s) of the proposal is apprently. The City of ocess this review tand that the connent to the Resp	on being submit the property liste toved, the decis Portland is not or, City staff may inpleteness of the onsibility Stater agrees to the ab	ted includes a desc d above in order to ion and any conditi- liable if any of these visit the site, photo his application is de- ment.	ription of the site con apply for this review apply for the approval me actions are taken we graph the property, cotermined by the Direct	ditions. I am and for revieust be recordithout the cour otherwise of the cour of the cours of th	also responsible for wing the responsibility ded in the County nsent of the owner(s) of document the site as ignature, I indicate my
				il this application and	Submitta	al of locked or password
Phone number:			sup	porting documents to @portlandoregon.gov	protecte	d documents will delay your application. 2





# City of Portland, Oregon Bureau of Development Services Land Use Services

FROM CONCEPT TO CONSTRUCTION

Dan Ryan, Commissioner Rebecca Esau, Director Phone: (503) 823-7300 Fax: (503) 823-5630 TTY: (503) 823-6868 www.portland.gov/bds

December 7, 2022

Andrew Becker HARTSHORNE PLUNKARD ARCHITECTURE 315 W. Walton St Chicago Il 60610 Usa

Re: Land Use Review LU 22-204531 HR - 1819 NW Everett

### Dear Andrew:

The Bureau of Development Services received your application for a Historic Resource Review located at 1819 NW EVERETT ST on November 16, 2022. Your case has been assigned to me, Hannah Bryant. In order to continue to review your application, additional information is needed. Once you submit this information, your application will be considered complete, and I will proceed with a full review of your proposal. Up to this point, your application has been reviewed only to determine if all required information has been submitted. The application has not been fully reviewed to determine if it meets the relevant approval criteria, however some issues you may want to consider are identified in Section II below.

### I. Information Necessary to Complete Application

The following information must be submitted before your proposal can be evaluated:

- 1. <u>Neighborhood Contact</u> All Neighborhood Contact requirements must be fulfilled prior to deeming this application complete.
- 2. <u>Modifications & Adjustment Reviews</u> Please provide clarity on any Modification or Adjustment reviews requested, including the code requirements and the proposal. These requests should include a narrative response articulating how the proposal meets the Modification approval criteria of 33.846.070.
- 3. <u>Details & Cutsheets</u> Provide detail drawings, including sections, enlarged elevations and other details needed to demonstrate how materials meet and are flashed; dimensional offsets between materials (ex: thickness of façade detailing, window punch within the wall section; loading door etc.). All materials should be detailed with the finish, gauge, dimensions and need corresponding cutsheets. (Cutsheets should be 1-2 pages long and include the proposed finish/style/etc. Do not send entire product catalogs.)
- 4. <u>Code Responses</u> Please provide a code summary, with a key demarcating which drawings in the submittal demonstrate compliance with the relevant code standard. Ensure that drawings are dimensioned or keyed to provide adequate detail to allow the planner to verify that code standards are met. (Example: to demonstrate compliance with ground floor windows, provide calculations of window area, and provide a dimensioned diagram showing that window calculations are being measured per the code standard.) Additional drawings will be necessary to demonstrate a response to all code sections. (For example, a landscape plan will be necessary in order to demonstrate compliance with the landscaped setback requirement.)

- 5. <u>Utility Plan</u> Please refer to the Portland Bureau of Transportation's pre-application notes for requirements on the UVE submittal. Vaults must be located on-site. Please coordinate early and often with Portland General Electric to understand and design for all necessary vault clearances.
- 6. <u>Clarity on Height/Affordable Commercial</u> Staff understands from previous conversations that the intent is to use the affordable commercial bonus to obtain the desired height. However, the floor plan does not demonstrate where the affordable commercial space is intended to be located. The site plan must demonstrate compliance with the design standards of the Affordable Commercial Administrative Rule.
- 7. <u>All Application Requirements</u> Please review the list of application requirements in 33.730.060.C.3 and make sure the next submittal includes all the required elements. Many of the required elements are missing from this submittal package, including dimensions from most drawings, existing and proposed tree and landscape plan, utility plan, elevations from non-street facing facades of the new hotel, context of adjacent buildings, bicycle parking requirements.
- 8. Transportation & Environmental Services Requirements The Portland Bureau of Transportation and the Bureau of Environmental Services have not yet submitted responses to the completeness review. Staff will forward these responses as soon as they are completed. In the meantime, please review the pre-application conference notes from these bureaus and make sure that all requested information is provided, and that any additional reviews needed (Examples include loading study, driveway design exception) are underway. Please keep staff apprised of the timing of these additional reviews and do not deem the application complete until these parallel reviews are completed and approved.

Applications for additional reviews will not be accepted unless accompanied by the required fees. Please note that failure to submit the needed application with the required fee may result in a denial of your proposal.

#### II. Issues to Address

While not necessary to determine the application complete, additional information may be needed to show that your proposal meets the applicable approval criteria. You are encouraged to address the following issues regarding the approvability of your proposal:

- Wait to deem complete. Staff understands that this is a placeholder application, and that more detailed information is forthcoming. Since this submittal lacked adequate detail to determine approvability or whether code standards are met, staff strongly recommends that when you submit a more comprehensive submittal package, that you request the application remain incomplete until the new submittal has been distributed and reviewed, and all bureaus have indicated that they are able to approve within the 51-day timeline between deeming the case complete and scheduling a hearing.
- Landmark Windows. The submittal indicates that almost all of the existing, original and character-defining windows in the Historic Landmark are proposed to be replaced with new glass and simulated, applied grid patterns. The only windows proposed to be repaired are those located above the front door. While these are significant, character-defining windows, the deep roof above the primary entrance precludes pedestrians from benefitting from a close-up view of the unique opalescent glass in these windows. Removal of all other windows is not supportable. As directed by the Historic Landmarks Commission, a thorough window repair analysis by a qualified historic window repair company needs to be the basis of any proposal to replace the original glazing in the

historic street-facing windows. The expectation is that all original street facing windows will be repaired and retained unless a window survey indicates that some of the windows are too compromised to be repairable. The Landmarks Commission was open to replacing the glazing in the non-street facing windows with clear glazing. If energy efficiency is a concern, interior storm windows may be installed without Historic Resource review. (Guidelines P1, P2, D6, D8)

- **Hotel Windows.** Aluminum clad wood windows may be supportable provided they are crafted to simulate historic wood window dimensions and profiles. Provide more information to demonstrate this contextual relationship. (Guidelines P2, D8)
- **Canopies.** Guidelines require that canopies provide weather protection for pedestrians. Please provide more information about the proposed fabric canopies. Staff supports the use of fabric canopies in these locations provided they are fixed and a minimum of fourfeet deep, to ensure they provide meaningful year-round weather protection for pedestrians. (Guidelines E3, E5, D8)
- **NW Everett Frontage.** The Historic Landmarks Commission supports requesting a reduction in the dedication on NW Everett to prevent the existing historic stairs from protruding into the pedestrian realm. Please provide more information on the status of this request. If the applicant does not want to pursue this Public Works Alternative Review, please provide a detailed landscape plan indicating how the area will be treated to minimize the awkward topography and exposure of the stair sidewalls. (Guidelines D3, D8)
- Landmark ADA Access. The Landmarks Commission suggested exploring opportunities to use the elevation change between NW 19<sup>th</sup> and NW 18<sup>th</sup> to incorporate an ADA ramp from the west side of the site to the front entrance of the Landmark. Please explore this concept to ensure equitable access for all guests into the primary entrance of this prominent building. Forcing some guests to access the building via a rear service door is not equitable and does not comply with City of Portland Core Values. (Guideline D2)
- **Loading Door.** Provide more information on the design and detailing of this very visible, inactive space. (Guideline D4, D7, D8)
- Contextual Analysis. As requested by the Historic Landmarks Commission, provide a
  contextual analysis to demonstrate how the proposed architecture is derived from its
  Alphabet Historic District context. (Guideline D7)
- **DAR Response.** Provide a response to the DAR comments, with context analysis, drawings or other diagrams to demonstrate how the DAR comments were incorporated into the current design.

### III. Time to Complete Application

The Portland Zoning Code allows you up to 180 days to complete your application. Since the 180-day period began on the day we received the application, the deadline to make your application complete is **Monday May 15, 2023.** 

### IV. Determination of a Complete Application

The application will be determined complete when you have submitted:

1. All of the requested information included in Section I, above. If you cannot provide all of the requested information at one time and intend to submit additional information,

please include a <u>written</u> statement with each separate submittal indicating that you still intend to provide the additional missing information by the **Monday May 15**, **2023** deadline, **or** 

- 2. Some of the requested information included in Section I, above, and a <u>written</u> statement that no additional information will be provided; **or**
- 3. A <u>written</u> statement that none of the requested information included in Section I, above, will be provided.

Please be aware that not submitting the requested information may result in your application being denied. The information is needed to demonstrate the approval criteria are met. Once the application is deemed complete, review of your application can proceed using the information you have provided.

Your application will be approved if it meets the relevant land use review approval criteria. It is your responsibility to document how the approval criteria are met. The items listed above will help provide that documentation.

### **Voiding of Application**

If your application is not complete by **Monday May 15, 2023**, it will be voided, and the application fee will not be refunded. The City's land use review procedures are outlined in Chapter 33.730 of the Portland Zoning Code.

Please contact me if you have any questions about this letter. My telephone number is **503-865-6520**, and my e-mail address is Hannah.Bryant@portlandoregon.gov. Requested information noted above should be emailed to me. Please e-mail me for file dropbox instructions if document or drawing file sizes are greater than 5MB. Please label all correspondence and materials you submit with the case number LU 22-204531.

Sincerely,

Hannah Bryant, Planner Land Use Services Division

cc: (deedholder/owner/applicant if different from addressee)
Application Case File



# City of Portland, Oregon Bureau of Development Services

### **Land Use Services**

FROM CONCEPT TO CONSTRUCTION

Carmen Rubio, Commissioner Rebecca Esau, Director Phone: (503) 823-7310 TTY: (503) 823-6868 www.portland.gov/bds

## STAFF REPORT AND RECOMMENDATION TO THE LANDMARKS COMMISSION

CASE FILE: LU 22-204531 HRM

PC # 22-128290

NW Everett Landmark + Hotel

REVIEW BY: Landmarks Commission WHEN: March 13, 2023; 1:30pm

REMOTE ACCESS: Historic Landmarks Commission Agenda

https://www.portlandoregon.gov/bds/HLCagenda

This land use hearing will be limited to remote participation via Zoom. Please refer to the instructions included with this notice to observe and participate remotely.

**Bureau of Development Services Staff:** Hannah Bryant 503-865-6520 / Hannah.Bryant@portlandoregon.gov

### GENERAL INFORMATION

**Applicant:** Andrew Becker | Hartshorne Plunkard Architecture

315 W. Walton St Chicago, IL 60610

abecker@hparchitecture.com

**Developer:** Tanya Toby | Gaia Ventures I, LLC

5821 SE Powell Blvd Portland, OR 97026

**Owner:** Northwest Neighborhood Cultural Center

PO Box 96116 Portland, OR 97296

Site Address: 1819 NW EVERETT ST

**Legal Description:** BLOCK 172 LOT 1-4, COUCHS ADD; BLOCK 172 LOT 6&7,

COUCHS ADD

**Tax Account No.:** R180215530, R180215610

**State ID No.:** 1N1E33DB 02800, 1N1E33DB 02900

Quarter Section: 3026

**Neighborhood:** Northwest District, contact Greg Theisen at

planningchair@northwestdistrictassociation.org

**Business District:** Northwest Portland, contact at nobhillportland@gmail.com. **District Coalition:** Neighbors West/Northwest, contact at admin@nwnw.org

Plan District: Northwest

**Other Designations:** Historic Landmark – First Church of Christ Scientist

**Zoning:** CM2(MU-U)d – Commercial Mixed-Use 2 with a Design Overlay **Case Type:** HRM – Historic Resource Review w/ Modification Review

**Procedure:** Type III, with a public hearing before the Landmarks

Commission. The decision of the Landmarks Commission can be

appealed to City Council.

### Proposal:

Type III **Historic Resource Review** to convert the former Landmark church into a club with restaurants, spa and bar, and construction of a new, detached, six-story hotel on a vacant parcel within the Landmark boundary. One loading space and no parking is proposed in the hotel. Four **Modifications** are requested: to increase building height; reduce the dimensions of the loading space; increase the distance between short-term bike parking and main entrances; and allow loading to be entered/exited in a rearward motion.

Historic Resource Review is required for non-exempt exterior alterations and non-exempt development in a Historic District.

### Relevant Approval Criteria:

In order to be approved, this proposal must comply with the criteria of Title 33, Portland's Zoning Code. The relevant criteria are:

Community Design Guidelines

 33.846.070 – Modifications That Better Meet Design Guidelines  Alphabet Historic District Addendum to the Community Design Guidelines

### **ANALYSIS**

**Site and Vicinity:** The site is a 30,000 square foot L-shaped parcel located in the Alphabet Historic District, the Northwest Plan District and the Northwest District Pedestrian District. The southern portion of the site is developed with the existing First Church of Christ Scientist Historic Landmark. This individually listed Landmark building was built in 1909 to be the first permanent home of the earliest Christian Scientist congregation in Portland. The northern portion of the site is approximately ten thousand square feet. It is currently developed as a surface parking area. There is no prior development history for this parcel in city records.

The site fronts NW Flanders to the north, NW 18th Avenue to the east, NW Everett Street to the south and NW 19th Avenue to the west. At this location, NW Flanders is classified as a Local Service Traffic Street, a Local Service Transit Street, a Major City Bikeway, a Major City Walkway, a Local Service Freight Street and a Minimum Emergency Response Street. NW 18th Avenue is classified as a Local Service Traffic Street, a Transit Access Street, a Major City Bikeway, a City Walkway, a Local Service Freight Street, and a Secondary Emergency Response Street. NW Everett Street is classified as a Local Service Traffic Street, a Transit Access Street, a City Bikeway, a Major City Walkway, a Local Service Freight Street, and a Major Emergency Response Street. NW 19th Avenue is classified as a Local Service Traffic Street, a Transit Access Street, a Major City Bikeway, a City Walkway, a Local Service Freight Street, and a Secondary Emergency Response Street.

The <u>Alphabet Historic District</u> is an area of Portland significant for its concentration of intact late 19<sup>th</sup> and early 20<sup>th</sup> Century, mostly middle class, housing stock and small-scale commercial buildings. Of special note are the many mid-sized apartment and

institutional buildings. Many of these are in the various Period Revival styles, e.g. Tudor, Spanish Colonial, Byzantine, Jacobean, etc. and this is especially the case in the immediate vicinity of the proposed new development. The area is characterized by a grid of narrower, more tree-lined, east-west residential streets, named alphabetically after prominent Portlanders of the day, which are crossed by generally more robust north-south avenues. Two of these, NW 21st Avenue and NW 23rd Avenue are low-scale business corridors featuring a mix of purpose-built commercial structures and converted houses.

The Northwest Plan District implements the Northwest District Plan, providing for an urban level of mixed-use development including commercial, office, housing, and employment. Objectives of the plan district include strengthening the area's role as a commercial and residential center. The regulations of this chapter: promote housing and mixed-use development; address the area's parking scarcity while discouraging auto-oriented developments; enhance the pedestrian experience; encourage a mixed-use environment, with transit supportive levels of development and a concentration of commercial uses, along main streets and the streetcar alignment; and minimize conflicts between the mixed-uses of the plan district and the industrial uses of the adjacent Guild's Lake Industrial Sanctuary.

**Zoning:** The <u>Commercial/Mixed Use 2</u> (CM2) zone is a medium-scale zone intended for sites in a variety of centers, along corridors, and in other mixed-use areas that have frequent transit service. The zone allows a wide range and mix of commercial and residential uses, as well as employment uses that have limited off-site impacts. Buildings in this zone will generally be up to four stories tall unless height and floor area bonuses are used, or plan district provisions specify other height limits. Development is intended to be pedestrian-oriented, provide a strong relationship between buildings and sidewalks, and complement the scale of surrounding residentially zoned areas.

The <u>historic resource overlay zone</u> protects historic resources that have been identified as significant to the history of the city and region. The regulations implement Portland's Comprehensive Plan policies that address historic preservation. These policies recognize the role historic resources have in promoting education and enjoyment for those living in and visiting the region. The regulations foster awareness, memory, and pride among the region's current and future residents in their city and its diverse architecture, culture, and history. Historic preservation recognizes social and cultural history, retains significant architecture, promotes economic and environmental health, and stewards important resources for the use, education, and enjoyment of future generations.

The <u>Design overlay zone</u> ensures that Portland is both a city designed for people and a city in harmony with nature. The Design overlay zone supports the city's evolution within current and emerging centers of civic life. The overlay promotes design excellence in the built environment through the application of additional design standards and design guidelines that:

- Build on context by enhancing the distinctive physical, natural, historic and cultural qualities of the location while accommodating growth and change;
- Contribute to a public realm that encourages social interaction and fosters inclusivity in people's daily experience; and
- Promotes quality and long-term resilience in the face of changing demographics, climate and economy.

**Land Use History:** City records indicate that prior land use reviews include:

• <u>LU 12-170628 HDZ</u> – Historic Design Review approval for the removal of a chimney at the Landmark Church, includes conditions of approval that must be maintained regarding the cataloging and storage of the historic chimney materials.

**Agency Review:** A "Notice of proposal in Your Neighborhood" was mailed February 21, 2023. The following Bureaus have responded with no issue or concerns:

- Fire Bureau (E.1)
- Bureau of Environmental Services (E.2)
- Portland Bureau of Transportation (E.3)
- Urban Forestry (E.4)
- Life Safety (E.5)
- Site Development (E.6)
- Water Bureau

**Neighborhood Review:** A Notice of Proposal in Your Neighborhood was mailed on February 21, 2023. Three written response has been received from either the Neighborhood Association or notified property owners in response to the proposal.

- <u>Shannon and David Lea</u>, February 24, 2023, neighbors support the proposal, including the height. Some concerns about on-street parking impacts.
- <u>Dennis Harper</u>, February 25, 2023, neighbor has some concern about the proposed fence and lack of landscaping details. Supports the rooftop canopy design proposed for the penthouse of the new hotel.
- <u>Steve Pinger (Northwest District Association)</u>, February 23, 2023. The NWDA strongly supports the project concept, including the renovation and repurposing of the existing landmark building, and the proposed new hotel building on the adjacent parcel.

**Staff Response**: Staff appreciates these neighbors for taking the time to share their thoughtful and nuanced comments. While potential off-street parking impacts are not approval criteria for new development, the concerns about quality and durability of proposed materials are subject to review. Staff has requested additional information related to building materials and detailing in order to determine whether the proposal meets the approval criteria related to quality of construction detailing and materials.

### PROCEDURAL HISTORY

This application was submitted on November 16, 2022. The applicant deemed it complete on January 23, 2023. A hearing was scheduled for March 13, 2023 (49 days after deeming the application complete). The applicant submitted a full extension. The land use application will expire on January 24, 2024.

### ZONING CODE APPROVAL CRITERIA

### Chapter 33.846.060 - Historic Resource Review

### Purpose of Historic Resource Review

Historic Resource Review ensures the conservation and enhancement of the special characteristics of historic resources.

### Historic Resource Review Approval Criteria

Requests for Historic Resource Review will be approved if the review body finds the applicant has shown that all of the approval criteria have been met.

**Findings:** The site is a designated Historic Landmark outside the Central City Plan District and not within in a Historic or Conservation District, and the proposal is for non-exempt treatments. Therefore, the proposal requires Historic Resource Review approval. The approval criteria are those listed in *33.846.060 G – Other Historic Approval Criteria*.

Staff has considered all of the approval criteria and addressed only those applicable to this proposal.

### Historic Alphabet District - Community Design Guidelines Addendum

- **1. Historic Changes.** Most properties change over time; those changes that have acquired historic significance will be preserved.
- **2. Differentiate New from Old.** New additions, exterior alterations, or related new construction will retain historic materials that characterize a property to the extent practicable. Replacement materials should be reasonable facsimiles of the historic materials they replace. The design of new construction will be compatible with the historic qualities of the district as identified in the Historic Context Statement.

**Findings for 1 and 2:** The two-part proposal includes the restoration of the existing First Church of Christ Scientist Historic Landmark building, and the construction of a new freestanding hotel on an existing surface parking lot within the Landmark boundary. While the new hotel is designed to complement the Landmark, it is clearly differentiated from the Landmark in architectural style, material, fenestration pattern and massing.

The existing three-story, masonry Landmark building is notable for its character-defining domed roof form, arched leaded windows with opalescent glass and massive midblock portico. The proposed renovations are primarily limited to the interior, however a few exterior alterations are proposed. These include the repair and restoration of existing original windows on the three street-facing facades; repair or partial replacement of the existing tin roof; new frameless glass guardrail around the perimeter of the level three roof; new stucco rainscreen cladding at the level three mass; two new fabric awnings over basement level doors at the east façade; removal of an original chimney and an elevator overrun on the north side of the roof and construction of new stair overruns at the location of the original chimneys; replacement of all windows on the north (non-street facing) façade on levels one through three; a new basement level service entry on the north façade, and modification of the primary arched windows on the east and west level three facades to incorporate a person door within the existing opening.

While there are numerous elements proposed to be altered, the scope of the proposed changes is modest in proportion to the scale of the existing building, its many years of deferred maintenance, and the complexity of adaptively reusing a building designed for such a different use. The alterations are carefully located to minimize visibility from the street level, while maintaining the character-defining features on three street-facing facades, and facilitating spill-out activation spaces that will add vibrancy to the public realm.

Therefore, these guidelines are met.

**3. Hierarchy of Compatibility.** Exterior alterations and additions will be designed to be compatible primarily with the original resource, secondarily with adjacent properties, and finally, if located within a historic or conservation district, with the rest of the District. Where practical, compatibility will be pursued on all three levels. New development will seek to incorporate design themes characteristic of similar buildings in the Historic Alphabet District.

**Findings for 3:** The proposal includes the removal of one remaining chimney on the north side, as well as a non-original elevator overrun also located on the north side of the roof. The chimney is one of the original pair that flanked the central dome and reinforced the building's symmetry. One of the paired chimneys was removed years ago, and the remaining unreinforced masonry chimney no longer serves the building functionally and detracts from the symmetrical architecture. The removal of the chimney and elevator overrun will facilitate the addition of two symmetrical stair overruns at the location of the original chimneys. The overruns are tucked in toward the center of the roof edge, furthest from the east and west street facades. They are proposed to be clad in a stucco cladding that will serve as a discreet background for the original cut stone cladding without drawing attention to itself.

As noted above, the bulk of the proposed alterations are targeted to the areas that are not visible from the public realm. The interior program and floor plans have been designed to ensure retention of the original architectural elements on the street-facing facades. The alterations and additions do not diminish the Landmark's iconic architecture, nor do they detract from the district.

Therefore, this guideline is met.

### **Community Design Guidelines**

- **P1. Plan Area Character.** Enhance the sense of place and identity by incorporating site and building design features that respond to the area's desired characteristics and traditions.
- **P2. Historic and Conservation Districts.** Enhance the identity of historic and conservation districts by incorporating site and building design features that reinforce the area's historic significance. Near historic and conservation districts, use such features to reinforce and complement the historic areas.

**Findings for P1 and P2:** The Alphabet Historic District is notable for its diversity of architectural styles and building types. However, some architectural themes are present throughout the district, including cornice treatments; a consistent fenestration pattern and visual transition between floor levels – particularly between the ground level and upper stories.

The proposed new hotel enhances the district's identity by incorporating numerous building features that respond to the traditional Alphabet District context and reinforce its significance. These include the active, pedestrian-oriented ground level, defined by large operable windows and granite pilasters, that is strongly differentiated from the upper levels through a change in cladding material and fabric awnings.

Above the ground level, the windows repeat the regular fenestration pattern typical throughout the district. Ornate metal balcony railings add rich detailing in front of French doors at the level two balconies and at all windows at the building's corner. Additional stucco detailing at the cornice is typical of many

historic buildings throughout the district. Above the cornice, a conservatory-inspired penthouse is set back from the roof edges. While the penthouse screens the building's mechanical equipment, it also contains a food service area to serve the adjacent roof deck. This activation of all available outdoor area is consistent with the highly walkable, pedestrian-oriented context, and will provide an asset to the neighborhood with rare view of the rooftops of numerous Historic Landmarks, as well as incredible vistas of the surrounding mountains and hills.

Overall, the hotel strikes the appropriate balance for new development within a Landmark boundary. It is an elegant stand-alone addition to the neighborhood while also being a graceful neighbor to the iconic adjacent Landmark.

Therefore, these guidelines are met.

- **E1. The Pedestrian Network.** Create an efficient, pleasant, and safe network of sidewalks and paths for pedestrians that link destination points and nearby residential areas while visually and physically buffering pedestrians from vehicle areas.
- **E2. Stopping Places.** New large-scale projects should provide comfortable places along pedestrian circulation routes where people may stop, visit, meet, and rest.
- **E3.** The Sidewalk Level of Buildings. Create a sense of enclosure and visual interest to buildings along sidewalks and pedestrian areas by incorporating small scale building features, creating effective gathering places, and differentiating street level facades.
- **E4.** Corners that Build Active Intersections. Create intersections that are active, unified, and have a clear identity through careful scaling detail and location of buildings, outdoor areas and entrances.
- **E5.** Light, Wind, and Rain. Enhance the comfort of pedestrians by locating and designing buildings and outdoor areas to control the adverse effects of sun, shadow, glare, reflection, wind, and rain.
- **D2. Main Entrances.** Make the main entrances to houses and buildings prominent, interesting, pedestrian-accessible, and transit-oriented.

**Findings for E1, E2, E3, E4, E5 and D2:** The proposed hotel is designed to facilitate an indoor-outdoor experience for its guests and to maximize activation of the pedestrian realm. The hotel introduces an active ground level, with operable storefront windows and recessed bays between pilasters to facilitate cafe seating along both street frontages (NW 19th Ave. and NW Flanders Street). Fixed fabric awnings provide year-round weather protection for pedestrians, while reinforcing the building's European-inspired style. The primary entrance is located at the curved corner, where it is most visible to guests arriving via one-way streets and it activates the corner. Wrapped by a large canopy, the tall entry doors accentuate the building's unique curved corner. The design utilizes operable windows, French doors, balconies and a roof deck on upper levels to ensure that the entire building serves to activate the pedestrian realm.

Therefore, these guidelines are met.

- **D1. Outdoor Areas.** When sites are not fully built on, place buildings to create sizable, usable outdoor areas. Design these areas to be accessible, pleasant, and safe. Connect outdoor areas to the circulation system used by pedestrians;
- **D3.** Landscape Features. Enhance site and building design through appropriate placement, scale, and variety of landscape features.

**Findings for D1 & D3:** The new hotel building is built to the property lines on both street frontages, however it is set back fifteen feet from the Landmark to

the south as well as ten feet from the shared property line with the existing multifamily apartment building to the east. A service path and code-required landscape buffer along the east side facilitate access between the loading space in the new building and the new basement level service door into the north façade of the existing Landmark. At the south side, a sunken courtyard between the basement levels of the new hotel and the existing Landmark serves to screen mechanical equipment, reducing the requirements for rooftop mechanical and facilitating a more active rooftop on the hotel. On the west side of this setback, the grade aligns with the sidewalk and the ADA access ramp to the Landmark building doubles as access to short-term bike parking.

The existing Landmark has deep, landscaped setbacks between the building and all street lot lines. On the east and west sides, the building is set back more than twenty feet from the sidewalks. New, occupiable outdoor spill out spaces are proposed on the east and west side of the Landmark. On the east side, a stepped courtyard is proposed between the building and the sidewalk, to serve as outdoor seating for a new basement-level café. On the west side, an outdoor garden with seating is proposed along the sidewalk edge. The enlarged site plan (C.2) indicates that the outdoor seating for both areas is proposed to be enclosed by a six-foot fence at the sidewalk edge, although this fence is not shown in the building elevations. Staff has concerns that the fence will create a fortress-like effect at the sidewalk edge, and that its presence may offset the public benefits of activating these outdoor spaces. Staff suggests that setting the fence back three feet from the sidewalk could allow a landscape buffer along the sidewalk edge that would better maintain the original landscape design, softening the fence and allowing it to visually recede from view.

The proposal maximizes the use of the site on behalf of both buildings, while utilizing grade and the buildings to screen necessary service and mechanical needs. The required bike parking is located on three street frontages, ensuring that hotel, club and retail guests are all able to easily locate convenient bike racks.

Therefore, these guidelines are met with an added nominal landscape buffer between the public sidewalk edge and the fences.

**D5. Crime Prevention.** Use site design and building orientation to reduce the likelihood of crime through the design and placement of windows, entries, active ground level uses, and outdoor areas.

**Findings for D5:** The proposal for both buildings is exemplary in its focus on activating the ground level, including exterior spaces. As described above, the new hotel fronts both street lots lines, with its undeveloped outdoor space located between the buildings, where it can be easily secured and illuminated. Both street frontages are lined with operable windows, and the ground level has been designed to facilitate café seating along the sidewalks. Ground level lighting embedded in each pilaster at the new hotel ensures the spaces are well lit throughout the day.

Spill out seating in the east and west setbacks of the Landmark building incentivize more people to spend time outside, adjacent to the sidewalk, where they can provide 'eyes on the street' and enhance pedestrian perceptions of safety. The proposed fences are intended to ensure that the outdoor spaces may be secured after hours and do not feel like dark, unsafe spaces for pedestrians.

Therefore, this guideline is met.

- **D6. Architectural Integrity.** Respect the original character of buildings when making modifications that affect the exterior. Make additions compatible in scale, color, details, material proportion, and character with the existing building.
- **D7. Blending into the Neighborhood.** Reduce the impact of new development on established neighborhoods by incorporating elements of nearby, quality buildings such as building details, massing, proportions, and materials.

**Findings for D6 & D7:** The two-part proposal includes the restoration of the existing First Church of Christ Scientist Historic Landmark building, and the construction of a new freestanding hotel on an existing surface parking lot within the Landmark boundary. While the new hotel is designed to complement the Landmark, it is clearly differentiated from the Landmark in architectural style, material, fenestration pattern and massing.

### Existing Landmark

The existing three-story, masonry Landmark building is notable for its character-defining domed roof form, arched leaded windows with opalescent glass and massive midblock portico. The proposed exterior alterations include the repair and restoration of existing original windows on the three street-facing facades; repair or partial replacement of the existing tin roof; new frameless glass guardrail around the perimeter of the level three roof; new stucco rainscreen cladding at the level three mass; two new fabric awnings over basement level doors at the east façade; removal of an original chimney and an elevator overrun on the north side of the roof and construction of new stair overruns at the location of the original chimneys; replacement of all windows on the north (non-street facing) façade on levels one through three; a new basement level service entry on the north façade, and modification of the primary arched windows on the east and west level three facades to incorporate a person door within the existing opening.

While there are numerous elements proposed to be altered on the existing Landmark, the scope of the proposed changes is modest in proportion to the scale of the existing building, its many years of deferred maintenance, and the complexity of adaptively reusing a building designed for such a different use. The alterations are carefully located to minimize visibility from the street level, while maintaining the character-defining features on three street-facing facades, and facilitating spill out activation spaces that will add vibrancy to the public realm.

### New Hotel

The new hotel incorporates numerous elements of nearby, quality commercial and mixed-use buildings. These include the material palette that comprises of operable storefront windows and French doors overlooking the street; stucco detailing and material changes that differentiate the top, middle and bottom of the building; custom artistic balustrades, and fabric awnings providing pedestrian weather protection at every storefront bay. The proposed hotel design includes fine-grain aesthetic detailing, particularly at the bottom two levels, that are reminiscent of many of the nearby contributing resources.

The new building's massing is proposed to be built to the property line on both of its street facing frontages, while it is set back from both the contributing resource to its east and the Landmark building to the south. This differential setback respects the existing context while also responding to the

neighborhood's desired character. The active ground level program at the sidewalk frontage serves to enliven the sidewalk environment, providing views of interior activity, new lighting, visual interest and opportunity for human interaction to the pedestrian realm.

The proposed hotel massing, architectural style, material palette and ground floor programming are an excellent model of respectful infill development in this historic district.

Therefore, these guidelines are met.

**D8. Interest, Quality, and Composition.** All parts of a building should be interesting to view, of long-lasting quality, and designed to form a cohesive composition.

**Findings for D8:** The submittal lacks sufficient building details to determine response to this guideline. Details necessary include, but are not limited to, those demonstrating how windows and doors are proposed to be set in the wall section and trimmed; flashing details, wood stem wall and ground-level parapet detailing, rooftop surfaces, fabric awning details, lighting cutsheets, and drawings that show how differing materials meet or intersect.

Further, notes on elevations need sufficient detail to determine which elements correspond to the provided cutsheets.

This guideline is not yet met. However, if sufficient and approvable detail is provided ahead of the hearing and if time allows, staff may revise this Staff Report to recommend approval.

### 33.846.060 G - Other Historic Approval Criteria

- **1. Historic character.** The historic character of the landmark or contributing resource will be retained and preserved. Removal of historic materials or alteration of features and spaces that contribute to the historic significance of the landmark or contributing resource will be avoided.
- **2. Record of its time.** The landmark or contributing resource will remain a physical record of its time, place, and use. Changes that create a false sense of historic development, such as adding conjectural features or architectural elements from other buildings will be avoided.
- **3. Historic changes.** Most resources change over time. Those changes that have acquired historic significance will be preserved.
- **4. Historic features.** Generally, deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement, the new feature will match the historic feature in design, color, texture, and other visual qualities and, where practical, in materials. Replacement of missing features must be substantiated by documentary, physical, or pictorial evidence.
- **5. Historic materials.** Historic materials will be protected. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials will not be used.
- **7. Differentiate new from old.** New additions, exterior alterations, or related new construction will not destroy historic materials that characterize a landmark or contributing resource. New work may be differentiated from the old if the differentiation does not diminish the character, features, materials, form, or integrity of the landmark or contributing resource and, if in a Historic District, the district as a whole.
- **8. Architectural compatibility.** New additions, exterior alterations, or new construction will be compatible with the massing, size, scale, and architectural features

of the landmark or contributing resource and, if in a district, the district as a whole. When retrofitting to improve accessibility for persons with disabilities or accommodate seismic improvements, design solutions will not compromise the architectural integrity of the landmark or contributing resource.

- **9. Preserve the form and integrity of historic resources.** New additions, exterior alterations, or new construction will be undertaken in such a manner that if removed in the future, the essential form and integrity of the landmark or contributing resource and, if in a district, the district as a whole would be unimpaired.
- **10. Hierarchy of compatibility.** New additions, exterior alterations, or new construction will be designed to be compatible primarily with the landmark or contributing resource and, if located within a district, secondarily with contributing resources within 200 feet and, finally, with the rest of the district. Where practical, compatibility in districts will be pursued on all three levels.

**Findings for 1, 2, 3, 4, 5, 7, 8, 9, and 10:** The existing First Church of Christ Scientist, an individually-listed Historic Landmark is being retained and preserved. Its character-defining leaded windows and domed roof are being retained and restored, and it will continue to serve as a record of its time. The proposed exterior alterations are minor elements intended to allow the adaptive reuse of a challenging structure, and are mostly limited to the north façade, which is the building's only non-street facing façade. Extensive restoration and repair work is proposed to the existing windows, the cut stone cladding and the roofing.

Due to building code requirements, activation of the flat rooftops to create terraces with outdoor seating required the introduction of multiple new egress doors. The applicant proposes modest new stair and elevator overruns tucked toward the center of the rear façade, furthest from the street-facing facades. These additions are proposed to be clad in stucco, to complement the color of the cut stone cladding but to differentiate these minor service elements from the original architecture.

Additionally, a secondary egress is needed from both the east and west side of the roof in order to facilitate the rooftop conversion to publicly accessible outdoor space. At staff's direction, the applicant explored multiple locations for this additional door, including replacing south-facing windows with doors, or modifying the tallest windows on level three of the east and west facades. With the goal of limiting the introduction of new regulating lines caused by the door frame, the applicant is proposing to insert a custom ten-foot door into the center of both of these windows. At that height, the door will infill the entire height of the existing window opening, eliminating the need for an infill panel above the door. The original window glazing will be modified and retained on either side of the door to maintain the character-defining leaded window pattern.

At the Design Advice Request, a Commissioner suggested the applicant explore opportunities to utilize the site's slope to introduce a new ADA ramp to the front door. The applicant has illustrated this explorative exercise on C.23. However, the investigation ultimately revealed that due to an eight-inch step at the main entrance door, and the location of existing historic columns, a ramp at this location does not meet the requirements for ADA turning radius and ramp width, and meeting the requirements would require fundamentally changing the design of the original entrance. Therefore, the applicant is proposing accessible entrances off both the east and the west facades. An accessible path through the new east courtyard provides access from NW 18<sup>th</sup> Avenue to the proposed basement level coffee shop at this frontage. At the west entrance, located at the

north end of the west façade, the ramped entry will bring users into the main level, and doubles as a primary entrance for guests arriving via the hotel or those accessing the new proposed spill-out garden seating on the west side of the hotel. While it would be more equitable to ensure all guests are able to arrive via the same entrance, the design, location and programming of the west ADA entrance demonstrate that it will be a primary entrance for users of all abilities and that guests arriving via these doors will not be navigating back of house conditions.

Therefore, these criteria are met.

### 33.846.070 Modifications Considered During Historic Resource Review

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the historic resource review process. These modifications are done as part of historic resource review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through historic resource review may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

- A. **Better meets historic resource review approval criteria.** The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and
- B. Purpose of the standard.
  - 1. The resulting development will meet the purpose of the standard being modified; or
  - 2. The preservation of the character of the historic resource is more important than meeting the purpose of the standard for which a modification has been requested.

Modification #1: Building Height, PZC 33.130.210.B.1 / Table 130-2 / Table 130-3 -increase the allowed building height by ten feet, from the allowed 60-feet to 70-feet above basepoint.

Purpose Statement: The height limits are intended to control the overall scale of buildings. The height limits in the CM2 and CE zones allow for a greater building height at a scale that can accommodate the growth intended for centers and corridors, while relating to the low- to mid-rise scale of neighborhood residential areas. Exceptions to height limit standards accommodate minor projections that do not significantly increase the visual scale of buildings; provide flexibility in the height of parapets and railings to facilitate rooftop outdoor spaces and equipment screening; and accommodate ground-floor spaces with high ceilings to encourage ground-floor commercial uses, mechanical parking, and other uses that benefit from high ceilings.

Standard: 33.130.210.B.1 - The base height standards for all structures, except detached accessory structures, are stated in Table 130-2. Table 130-2 states that the base height in the CM2 zone is 45 feet, and bonus height is in Table 130-3. Table 130-3 states that the maximum height in the CM2 zone with bonus is 55 feet.

Note: the applicant has combined numerous code-allowed height exceptions with allowed bonus heights to achieve an allowable height of 60-feet. These include a 45-

foot base height, plus a five-foot high ceilings height exception, plus a ten-foot height bonus for providing an affordable commercial space, for a total of sixty-feet of allowable height. The ten-foot Modification request is in addition to that sixty-feet.

**A.** Better meets historic resource review approval criteria. The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and

**Findings for A:** The modification is to increase the height of the building to allow for occupiable space at the rooftop penthouse level (in addition to the mechanical, stair and elevator overruns that are already allowed by code). The code allows for height exceptions for mechanical and stair/elevator overruns, provided they fall within specific height limits and are set back a minimum of fifteen feet from roof edges on all street facing facades. The proposed occupiable space is set back more than fifteen feet from the street facing roof edges, and meets the ten-foot height limitation allowed for rooftop mechanical, except for where the stair and elevator overrun are located. These elements meet the sixteen-foot height exception in the code. Only the occupiable floor area at the rooftop level necessitates this modification request.

The modification better meets the guidelines D1 – Outdoor Areas and D3 – Landscape Features. The benefits of allowing the penthouse-level interior space are that it facilitates the active use of a rooftop, creating an additional publicly accessible commercial space with a rooftop restaurant, and opportunity for outdoor activity in a vibrant and densifying neighborhood. As the city grows, it is increasingly important to maximize the use of outdoor spaces. Private outdoor space is a privilege many do not have access to, and rooftops are one of the underutilized opportunities for introducing new places for the public to enjoy Portland's mild climate and phenomenal views.

**B.** Purpose of the standard. The resulting development will meet the purpose of the standard being modified.

**Findings for B:** The proposal meets the purpose of the standard because the proposed interior penthouse level space meets the height limits and setbacks required for allowed rooftop mechanical and overruns and does not dramatically alter the building's perceived height or scale, particularly within its immediate context. Sight line diagrams indicate that the mass is set back sufficiently to not be seen from the adjacent rights of way, and is only slightly visible from a block away, where the higher grade allows pedestrians to see the roofline of the new mass. It will have little impact on the building's perceived scale. Further, immediately across NW 19th Avenue, an existing non-conforming apartment building is over 129-feet tall, with additional mechanical elements extending above that height.

The proposal better meets the approval criteria and the purpose statement for the standard being modified.

Therefore, this Modification merits approval.

Modification #2: Loading Space Size, PZC 33.266.310.D.a – decrease the standard A loading space size from the required 35-feet long, 10-feet wide and 13-foot clearance to 25-feet long, 10-feet wide and 13-foot clearance.

Modification #3: Forward Motion, PZC 33.266.310.F.1 – allow loading space to be accessed and exited in a rearward motion.

Purpose Statement: A minimum number of loading spaces are required to ensure adequate areas for loading for larger uses and developments. These regulations ensure that the appearance of loading areas will be consistent with that of parking areas. The regulations ensure that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

*Standard:* 33.266.310.D.a: Standard A: the loading space must be at least 35 feet long, 10 feet wide, and have a clearance of 13 feet.

Standard 33.266.310.F.1: Outside the Central City plan district. Outside the Central City plan district, loading facilities generally must be designed so that vehicles enter and exit the site in a forward motion. Standard B loading spaces that are accessed from a Local Service Traffic Street are exempt from this requirement

**A.** Better meets historic resource review approval criteria. The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and

**Findings for Modification #2.A:** The modification to reduce the length of the proposed loading space better meets approval criteria E1 – The Pedestrian Network and E3 – The Sidewalk Level of Buildings. The modification request is based on loading data collected by the applicant and reviewed by the Portland Bureau of Transportation engineer. The collected data demonstrated that the vehicles utilized to service this size and type of program do not require the 35-foot length of a Standard A space, but are too large to be served by a Standard B space. Therefore, the modified dimensions are proposed to ensure that the space is adequately sized to serve the vehicles that will be accessing the space but that it is not unnecessarily large. The reduced size allows the floor area to be better utilized for other uses accessory to the retail sales and service program. Maximizing the active uses at the ground floor benefits the sidewalk environment and pedestrian realm, facilitating more opportunity for visual interest at the sidewalk level of the building.

**Findings for Modification #3.A:** The modification to reduce the length of the proposed loading space better meets approval criteria E1 – The Pedestrian Network and E3 – The Sidewalk Level of Buildings. In order to meet the code standard requiring a loading space may be entered and exited in a forward motion, the proposal would need to have a one-way driveway wrapping around the building. Neither PBOT nor BDS supported this proposal, as it would have required a large percentage of the site area to be allocated to surface vehicle area, and PBOT determined that having a truck exit in a rearward motion onto NW Flanders Street will create fewer traffic conflicts than having vehicles exit onto NW 19th Avenue near the complex intersection of NW 19th Avenue and NW Everett streets.

Minimizing the amount of on-site vehicle area, with a design that internalizes the loading space into the building mass upholds the approval criteria dedicated to fostering an active and vibrant pedestrian realm.

**B. Purpose of the standard.** The resulting development will meet the purpose of the standard being modified.

**Findings for Modifications #2 and #3 B:** The proposal meets the purpose of the standard, since the requested loading space dimension reflects the size of trucks servicing the site, and is derived from data collected specific to this use and size of program. The appropriately sized loading space ensures that access to and from

loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

Allowing vehicles accessing the loading space to enter or exit in a rearward motion has been deemed the safest option, and will result in fewer conflicts with other vehicles, bicyclists and pedestrians than if the standard were met with a one-way driveway around the site.

The proposal better meets the approval criteria and meets the purpose statement of the standard proposed to be modified.

Therefore, these Modifications merit approval.

Modification #4: Short Term Bike Parking Locations, PZC 33.266.210.E.1.a.(3)—increase the maximum distance that short-term bike parking may be located from a main entrance, from 50 feet to 65 feet.

Standard 33.266.210: These standards ensure that required bicycle parking is designed so people of all ages and abilities can access the bicycle parking and securely lock their bicycle without undue inconvenience. Bicycle parking is in areas that are reasonably safeguarded from theft and accidental damage. The standards allow for a variety of bicycle types, including but not limited to standard bicycles, tricycles, hand cycles, tandems, electric motor assisted cycles and cargo bicycles. Long-term bicycle parking is in secure, weather protected facilities and is intended for building and site occupants, and others who need bicycle parking for several hours or longer. Short-term bicycle parking is located in publicly accessible, highly visible locations that serve the main entrance of a building. Short-term bicycle parking is visible to pedestrians and bicyclists on the street and is intended for building and site visitors.

**A.** Better meets historic resource review approval criteria. The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and

**Findings for A:** The proposal to increase the maximum distance allowed between the required short-term bike parking and the main entrances on both buildings better meets historic resource review approval criteria E1 – The Pedestrian Network. Because the new hotel is built to the property line on both street frontages and does not have any on-site space within fifty feet of the main entrances where short term bike parking could meet the locational standards, the applicant had the option to buy into the short-term bike parking fund. This would have eliminated the requirement to provide any short-term bike parking on site, however there is no guarantee that PBOT will be able to use the associated fees to locate bike parking in the adjacent right-of-way. The applicant desires to provide convenient, on-site bike parking accessible to each of their retail sales and service uses, despite not all the short-term bike parking being located within the maximum 50-foot distance from a main entrance.

This solution better meets the approval criteria, as the resulting proposal cumulatively provides all the required short-term bike parking spaces but separates them into three distinct locations, located off three separate street frontages. The site has four street frontages, and while locating all the bike parking in one location may have met the code standard, it would not have resulted in convenient, findable bike parking facilities for all guests to the site. The dispersed bike parking proposal is more efficient, pleasant and safe, as it locates bike parking in convenient areas

that are adjacent to windows, sidewalks and other active spaces where many people can see into the bike parking areas.

**B.** Purpose of the standard. The resulting development will meet the purpose of the standard being modified;

**Findings for B:** The proposed short-term bike parking proposal is designed so people of all ages and abilities can access the bike parking. It is conveniently located adjacent to three of the site's four frontages. The locations are highly visible, close to primary entrances to many of the retail sales and service uses in both buildings.

The proposal better meets the approval criteria and meets the purpose statement of the standard proposed to be modified.

Therefore, this Modification merits approval.

#### **DEVELOPMENT STANDARDS**

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

#### CONCLUSIONS

The purpose of the Historic Resource Review process is to ensure that additions, new construction, and exterior alterations to historic resources do not compromise their ability to convey historic significance. This proposal respectfully converts an existing Landmark church building into a complicated program of multiple new retail sales and service uses. It will adaptively reuse the existing building to house numerous active uses, while prioritizing minimal exterior alterations. The new hotel is designed to complement both the adjacent Landmark and the surrounding historic district context. It models an excellent ground level and maximizes opportunities for activation adjacent to the streets. However, the proposal lacks necessary details to determine quality and resilience guidelines. Therefore, it does not yet warrant approval.

#### TENTATIVE STAFF RECOMMENDATION

(May be revised upon receipt of new information at any time to the Landmarks Commission decision)

Staff cannot yet recommend approval due to a lack of material and building details.

When staff is able to recommend approval, at least the following conditions of approval will be recommended.

A. As part of the building permit application submittal, the following development-related conditions (B through C) must be noted on each of the 4 required site plans or included as a sheet in the numbered set of plans. The sheet on which this information appears must be labeled "ZONING COMPLIANCE PAGE - Case File LU 22-204531 HRM AD". All requirements must be graphically represented on the site plan, landscape, or other required plan and must be labeled "REQUIRED."

- B. At the time of building permit submittal, a signed Certificate of Compliance form (<a href="https://www.portlandoregon.gov/bds/article/623658">https://www.portlandoregon.gov/bds/article/623658</a>) must be submitted to ensure the permit plans comply with the Design/Historic Resource Review decision and approved exhibits.
- C. No field changes allowed.

**Procedural Information.** The application for this land use review was submitted on November 16, 2022, and was determined to be complete on January 23, 2023.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore, this application was reviewed against the Zoning Code in effect on November 16, 2022.

*ORS* 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant waived the 120-day review period, as stated with Exhibit (Exhibit A.2) Unless further extended by the applicant, **the 120 days will expire on: January 23, 2024** 

Some of the information contained in this report was provided by the applicant. As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with

the applicable approval criteria. This report is the recommendation of the Bureau of

Development Services with input from other City and public agencies.

This report is not a decision. The review body for this proposal is the Landmarks Commission who will make the decision on this case. This report is a recommendation to the Landmarks Commission by the Bureau of Development Services. The review body may adopt, modify, or reject this recommendation. The Landmarks Commission will make a decision about this proposal at the hearing or will grant a continuance. Any new written testimony should be emailed to Hannah Bryant at Hannah.Bryant@PortlandOregon.gov. If you cannot email comments and must mail comments via USPS mail, your comments to the Landmarks Commission can be mailed c/o the Landmarks Commission, 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201.

Please note regarding USPS mail: USPS mail is only received a couple times a week, and testimony must be received before the close of the record. Therefore, please mail testimony well in advance of the hearing date.

If you are interested in viewing information in the file, please contact the planner listed on this decision. The planner can provide information over the phone or via email. Please note only digital copies of material in the file are available for viewing. A digital copy of the Portland Zoning Code is available on the internet at <a href="http://www.portlandoregon.gov/zoningcode">http://www.portlandoregon.gov/zoningcode</a>.

You will receive mailed notice of the decision if you write a letter received before the hearing or testify at the hearing, or if you are the property owner or applicant. This Staff Report will be posted on the Bureau of Development Services website. Look at www.portlandonline.com. On the left side of the page use the search box to find Development Services, then click on the Zoning/Land Use section, select Notices and Hearings. Land use review notices are listed by the District Coalition shown at the beginning of this document.

**Appeal of the decision.** The decision of the Landmarks Commission may be appealed to City Council, who will hold a public hearing. If you or anyone else appeals the decision of the Landmarks Commission, City Council will hold an evidentiary hearing, one in which new evidence can be submitted to them. Upon submission of their application, the applicant for this land use review chose to waive the 120-day time frame in which the City must render a decision. This additional time allows for any appeal of this proposal to be held as an evidentiary hearing.

Who can appeal: You may appeal the decision only if you write a letter which is received before the close of the record for the hearing, if you testify at the hearing, or if you are the property owner/applicant. Appeals must be filed within 14 days of the decision. An appeal fee of \$5,789.00 will be charged.

Additional information on how to file and the deadline for filing an appeal will be included with the decision. Assistance in filing the appeal and information on fee waivers are available from the Bureau of Development Services website: <a href="https://www.portlandoregon.gov/bds/article/411635">https://www.portlandoregon.gov/bds/article/411635</a>. Neighborhood associations recognized by the Office of Neighborhood Involvement may qualify for a waiver of the appeal fee provided that the association has standing to appeal. The appeal must contain the signature of the Chair person or other person\_authorized by the association, confirming the vote to appeal was done in accordance with the organization's bylaws.

Neighborhood associations, who wish to qualify for a fee waiver, must complete the Type III Appeal Fee Waiver Request for Organizations Form and submit it prior to the appeal deadline. The Type III Appeal Fee Waiver Request for Organizations Form contains instructions on how to apply for a fee waiver, including the required vote to appeal.

#### Recording the final decision.

If this Land Use Review is approved the final decision will be recorded with the Multnomah County Recorder.

• *Unless appealed*, the final decision will be recorded after **March 14, 2022**, by the Bureau of Development Services.

The applicant, builder, or a representative does not need to record the final decision with the Multnomah County Recorder.

For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

**Expiration of this approval.** An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Zone Change and Comprehensive Plan Map Amendment approvals do not expire.

**Applying for your permits.** A building permit, occupancy permit, or development permit must be obtained before carrying out this project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed here.
- All applicable development standards, unless specifically exempted as part of this land use review.
- All requirements of the building code.
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the city.

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

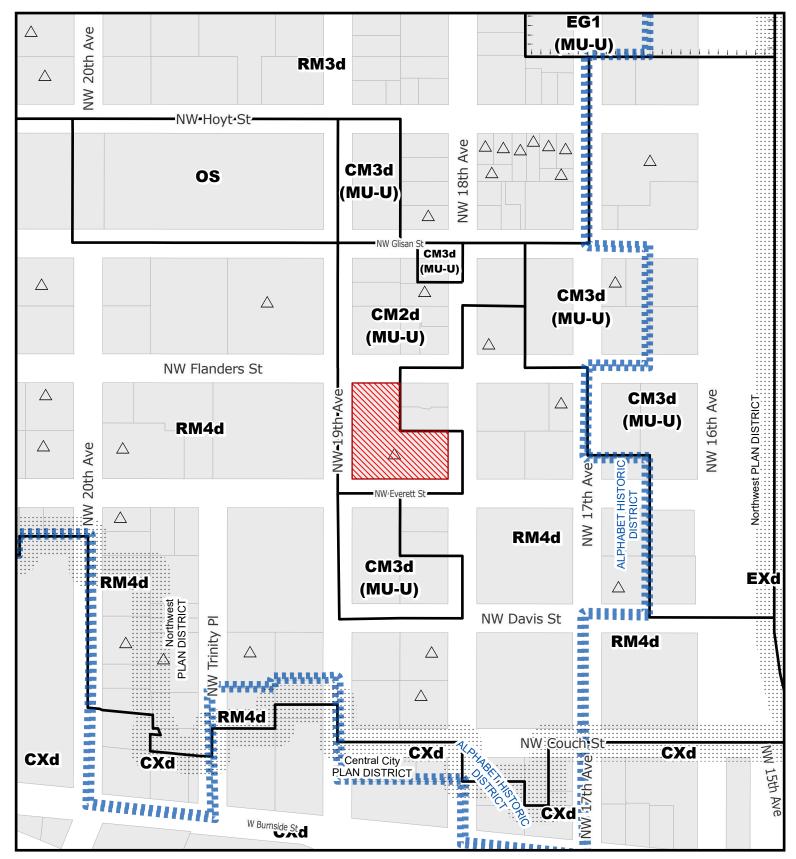
Hannah Bryant March 3, 2023

#### **EXHIBITS** – NOT ATTACHED UNLESS INDICATED

- A. Applicant's Submittals
  - 1. Original Submittal
  - 2. Full Extension, November 28, 2022
  - 3. Rooftop Egress Door Options, January 13, 2023
  - 4. Request from Applicant to deem complete, January 19, 2023
  - 5. Response to Incomplete, January 23, 2023
  - 6. Final Submittal
- B. Zoning Map (attached)
- C. Plan & Drawings
  - 1. Not used
  - 2. Site Plan (Landmark) (attached)
  - 3. Basement Floor Plan (Landmark)
  - 4. First Floor Plan (Landmark)
  - 5. Second Floor Plan (Landmark)
  - 6. Third Floor Plan (Landmark)
  - 7. Roof Plan (Landmark)
  - 8. Black and White South Elevation (Landmark)
  - 9. Black and White West Elevation (Landmark)
  - 10. Black and White East Elevation (Landmark)
  - 11. Black and White North Elevation (Landmark)
  - 12. Color South Elevation (Landmark)
  - 13. Color West Elevation (Landmark)
  - 14. Color East Elevation (Landmark)
  - 15. Color North Elevation (Landmark)
  - 16. Demolition South Elevations (Landmark)
  - 17. Demolition West Elevations (Landmark)
  - 18. Demolition East Elevations (Landmark)
  - 19. Demolition North Elevations (Landmark)
  - 20. Existing Conditions (Landmark)

- 21. Details new door at Landmark level 3 (Landmark)
- 22. Rooftop addition Landmark level 3 (Landmark)
- 23. Not used
- 24. Not used
- 25. Window Repair and Replacement Scope (Landmark)
- 26. Site Plan (New Hotel)
- 27. Floor Plans (New Hotel)
- 28. Floor Plans (New Hotel)
- 29. Black and White West Elevations (New Hotel)
- 30. Black and White North Elevation (New Hotel)
- 31. Black and White South Elevation (New Hotel)
- 32. Black and White East Elevation (New Hotel)
- 33. Color West Elevations (New Hotel)
- 34. Color North Elevation (New Hotel)
- 35. Color South Elevation (New Hotel)
- 36. Color East Elevation (New Hotel)
- 37. Details Storefront (New Hotel)
- 38. Details Wall Section (New Hotel)
- 39. Details Rooftop Penthouse & Canopy (New Hotel)
- 40. Affordable Commercial Space
- 41. FAR Summary
- 42. Short Term Bike Parking
- 43. Long Term Bike Parking
- 44. Ground Floor Windows (New Hotel)
- 45. Height Diagram (New Hotel)
- 46. Window and Door Cutsheets
- 47. Garage Door Cutsheet
- 48. Mechanical Screen & Stucco Cutsheet
- 49. Glass Railing Cutsheet
- 50. Rooftop Mechanical Cutsheet
- 51. Rooftop Mechanical Cutsheet
- 52. Landscape Tree Plan
- 53. Landscape Site Plan
- 54. Landscape Roof Plan
- 55. Landscape Planting Plan
- 56. Landscape Planting Plan Roof Decks
- 57. Existing Civil Plan (New Hotel)
- 58. Existing Civil Plan (Landmark)
- 59. Civil Plan (Hotel)
- 60. Civil Plan (Landmark)
- 61. Utility Plan (Hotel)
- 62. Utility Plan (Landmark)
- 63. Stormwater Plan (Hotel)
- 64. Stormwater Plan (Landmark)
- D. Notification information:
  - 1. Request for response
  - 2. Posting letter sent to applicant
  - 3. Notice to be posted
  - 4. Applicant's statement certifying posting
  - 5. Mailed notice
  - 6. Mailing list
- E. Agency Responses:
  - 1. Bureau of Environmental Services
  - 2. Bureau of Transportation Engineering and Development Review
  - 3. Water Bureau

- 4. Fire Bureau
- 5. Bureau of Parks, Forestry Division
- 6. Site Development Review Section of BDS
- F. Letters
  - 1. Shannon and David Lea, February 24, 2023, neighbors support the proposal, including the height. Some concerns about on-street parking impacts.
  - 2. Dennis Harper, February 25, 2023, neighbor has some concern about the proposed fence and lack of landscaping details. Supports the rooftop canopy design proposed for the penthouse of the new hotel.
  - 3. Steve Pinger (NWDA), February 23, 2023. The NWDA strongly supports the project concept, including the renovation and repurposing of the existing landmark building, and the proposed new hotel building on the adjacent parcel.
- G. Other
  - 1. Original LUR Application
  - 2. Incomplete Letter, December 7, 2022
- Η.



For Zoning Code in Effect Post October 1, 2022



THIS SITE LIES WITHIN THE: NORTHWEST PLAN DISTRICT ALPHABET HISTORIC DISTRICT Site

Plan District

Historic District

A Historic Landmark

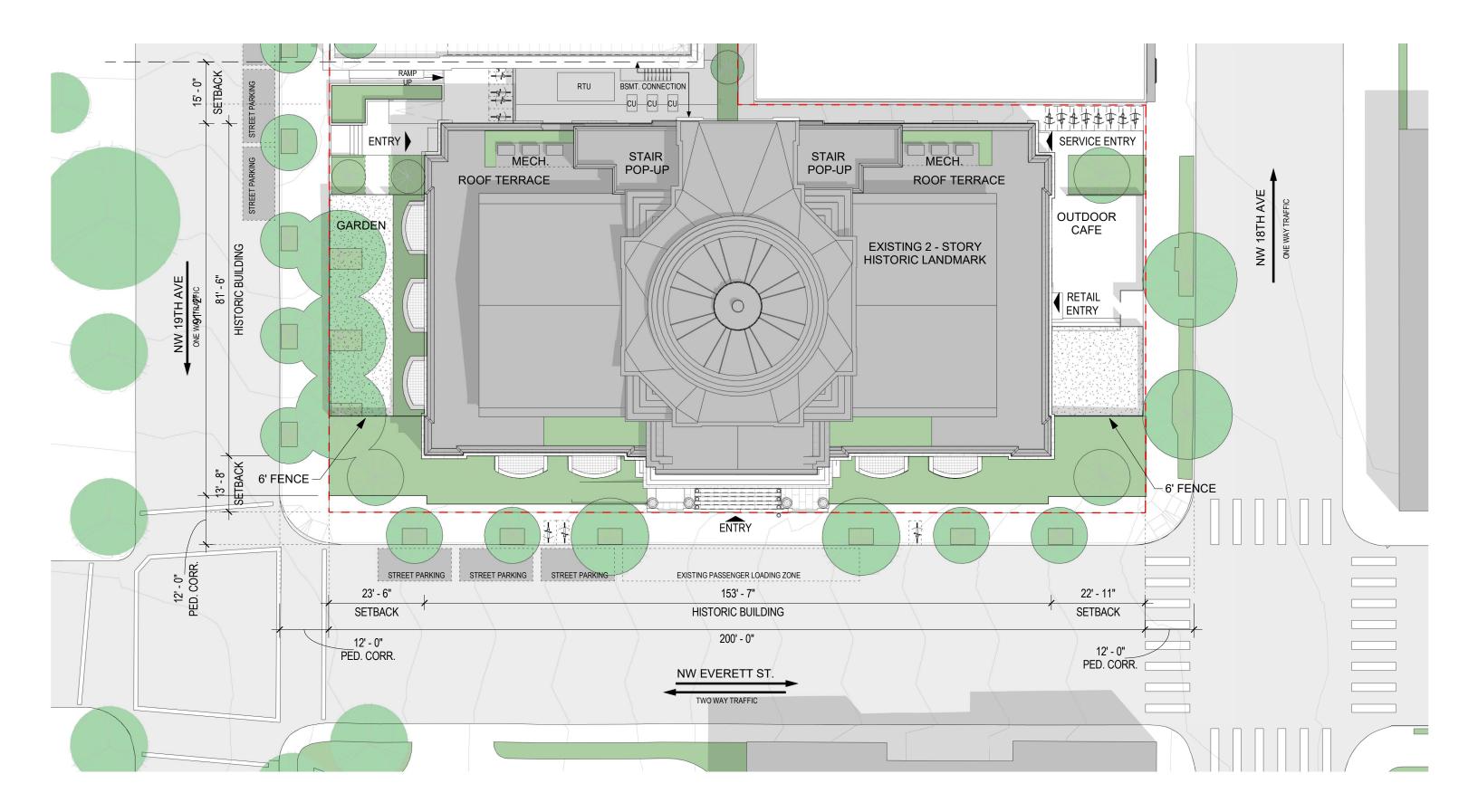
File No. LU 22 - 204531 HRM AD

1/4 Section 3028

Scale 1 inch = 200 feet

State ID 1N1E33DB 2800

Exhibit B Jan 23, 2023



#### ENLARGED SITE PLAN (1819 NW EVERETT ST.)

| SCALE: 1" = 64'-0"











## City of Portland, Oregon Bureau of Development Services

#### **Land Use Services**

FROM CONCEPT TO CONSTRUCTION

Carmen Rubio, Commissioner Rebecca Esau, Director Phone: (503) 823-7310 TTY: (503) 823-6868 www.portland.gov/bds

## REVISED STAFF REPORT AND RECOMMENDATION TO THE LANDMARKS COMMISSION

CASE FILE: LU 22-204531 HRM

PC # 22-128290

NW Everett Landmark + Hotel

REVIEW BY: Landmarks Commission WHEN: March 13, 2023; 1:30pm

REMOTE ACCESS: Historic Landmarks Commission Agenda

https://www.portlandoregon.gov/bds/HLCagenda

This land use hearing will be limited to remote participation via Zoom. Please refer to the instructions included with this notice to observe and participate remotely.

**Bureau of Development Services Staff:** Hannah Bryant 503-865-6520 / Hannah.Bryant@portlandoregon.gov

\***Note**: This REVISED Staff Report includes revisions to the original Staff Report published on March 7, 2023. This revised report now recommends approval of the project based on new information submitted by the applicant, included in Exhibits C.38-C.42.

#### **GENERAL INFORMATION**

**Applicant:** Andrew Becker | Hartshorne Plunkard Architecture

315 W. Walton St Chicago, IL 60610

abecker@hparchitecture.com

**Developer:** Tanya Toby | Gaia Ventures I, LLC

5821 SE Powell Blvd Portland, OR 97026

**Owner:** Northwest Neighborhood Cultural Center

PO Box 96116 Portland, OR 97296

Site Address: 1819 NW EVERETT ST

**Legal Description:** BLOCK 172 LOT 1-4, COUCHS ADD; BLOCK 172 LOT 6&7,

**COUCHS ADD** 

**Tax Account No.:** R180215530, R180215610

**State ID No.:** 1N1E33DB 02800, 1N1E33DB 02900

Quarter Section: 3026

**Neighborhood:** Northwest District, contact Greg Theisen at

planningchair@northwestdistrictassociation.org

**Business District:** Northwest Portland, contact at nobhillportland@gmail.com. **District Coalition:** Neighbors West/Northwest, contact at admin@nwnw.org

**Plan District:** Northwest

Other Designations: Historic Landmark - First Church of Christ Scientist

**Zoning:** CM2(MU-U)d – Commercial Mixed-Use 2 with a Design Overlay **Case Type:** HRM – Historic Resource Review w/ Modification Review **Procedure:** Type III, with a public hearing before the Landmarks

Commission. The decision of the Landmarks Commission can be

appealed to City Council.

#### Proposal:

Type III **Historic Resource Review** to convert the former Landmark church into a club with restaurants, spa and bar, and construction of a new, detached, six-story hotel on a vacant parcel within the Landmark boundary. One loading space and no parking is proposed in the hotel. Four **Modifications** are requested: to increase building height; reduce the dimensions of the loading space; increase the distance between short-term bike parking and main entrances; and allow loading to be entered/exited in a rearward motion.

Historic Resource Review is required for non-exempt exterior alterations and non-exempt development in a Historic District.

#### Relevant Approval Criteria:

In order to be approved, this proposal must comply with the criteria of Title 33, Portland's Zoning Code. The relevant criteria are:

Community Design Guidelines

 33.846.070 – Modifications That Better Meet Design Guidelines  Alphabet Historic District Addendum to the Community Design Guidelines

#### **ANALYSIS**

**Site and Vicinity:** The site is a 30,000 square foot L-shaped parcel located in the Alphabet Historic District, the Northwest Plan District and the Northwest District Pedestrian District. The southern portion of the site is developed with the existing First Church of Christ Scientist Historic Landmark. This individually listed Landmark building was built in 1909 to be the first permanent home of the earliest Christian Scientist congregation in Portland. The northern portion of the site is approximately ten thousand square feet. It is currently developed as a surface parking area. There is no prior development history for this parcel in city records.

The site fronts NW Flanders to the north, NW 18<sup>th</sup> Avenue to the east, NW Everett Street to the south and NW 19<sup>th</sup> Avenue to the west. At this location, NW Flanders is classified as a Local Service Traffic Street, a Local Service Transit Street, a Major City Bikeway, a Major City Walkway, a Local Service Freight Street and a Minimum Emergency Response Street. NW 18<sup>th</sup> Avenue is classified as a Local Service Traffic Street, a Transit Access Street, a Major City Bikeway, a City Walkway, a Local Service Freight Street, and a Secondary Emergency Response Street. NW Everett Street is classified as a Local Service Traffic Street, a Transit Access Street, a City Bikeway, a Major City Walkway, a Local Service Freight Street, and a Major Emergency Response Street. NW 19<sup>th</sup> Avenue is classified as a Local Service Traffic Street, a Transit Access Street, a Major City

Bikeway, a City Walkway, a Local Service Freight Street, and a Secondary Emergency Response Street.

The <u>Alphabet Historic District</u> is an area of Portland significant for its concentration of intact late 19<sup>th</sup> and early 20<sup>th</sup> Century, mostly middle class, housing stock and small-scale commercial buildings. Of special note are the many mid-sized apartment and institutional buildings. Many of these are in the various Period Revival styles, e.g. Tudor, Spanish Colonial, Byzantine, Jacobean, etc. and this is especially the case in the immediate vicinity of the proposed new development. The area is characterized by a grid of narrower, more tree-lined, east-west residential streets, named alphabetically after prominent Portlanders of the day, which are crossed by generally more robust north-south avenues. Two of these, NW 21<sup>st</sup> Avenue and NW 23<sup>rd</sup> Avenue are low-scale business corridors featuring a mix of purpose-built commercial structures and converted houses.

The Northwest Plan District implements the Northwest District Plan, providing for an urban level of mixed-use development including commercial, office, housing, and employment. Objectives of the plan district include strengthening the area's role as a commercial and residential center. The regulations of this chapter: promote housing and mixed-use development; address the area's parking scarcity while discouraging auto-oriented developments; enhance the pedestrian experience; encourage a mixed-use environment, with transit supportive levels of development and a concentration of commercial uses, along main streets and the streetcar alignment; and minimize conflicts between the mixed-uses of the plan district and the industrial uses of the adjacent Guild's Lake Industrial Sanctuary.

**Zoning:** The <u>Commercial/Mixed Use 2</u> (CM2) zone is a medium-scale zone intended for sites in a variety of centers, along corridors, and in other mixed-use areas that have frequent transit service. The zone allows a wide range and mix of commercial and residential uses, as well as employment uses that have limited off-site impacts. Buildings in this zone will generally be up to four stories tall unless height and floor area bonuses are used, or plan district provisions specify other height limits. Development is intended to be pedestrian-oriented, provide a strong relationship between buildings and sidewalks, and complement the scale of surrounding residentially zoned areas.

The <u>historic resource overlay zone</u> protects historic resources that have been identified as significant to the history of the city and region. The regulations implement Portland's Comprehensive Plan policies that address historic preservation. These policies recognize the role historic resources have in promoting education and enjoyment for those living in and visiting the region. The regulations foster awareness, memory, and pride among the region's current and future residents in their city and its diverse architecture, culture, and history. Historic preservation recognizes social and cultural history, retains significant architecture, promotes economic and environmental health, and stewards important resources for the use, education, and enjoyment of future generations.

The <u>Design overlay zone</u> ensures that Portland is both a city designed for people and a city in harmony with nature. The Design overlay zone supports the city's evolution within current and emerging centers of civic life. The overlay promotes design excellence in the built environment through the application of additional design standards and design guidelines that:

- Build on context by enhancing the distinctive physical, natural, historic and cultural qualities of the location while accommodating growth and change;
- Contribute to a public realm that encourages social interaction and fosters inclusivity in people's daily experience; and

• Promotes quality and long-term resilience in the face of changing demographics, climate and economy.

Land Use History: City records indicate that prior land use reviews include:

 <u>LU 12-170628 HDZ</u> – Historic Design Review approval for the removal of a chimney at the Landmark Church, includes conditions of approval that must be maintained regarding the cataloging and storage of the historic chimney materials.

**Agency Review:** A "Notice of proposal in Your Neighborhood" was mailed February 21, 2023. The following Bureaus have responded with no issue or concerns:

- Fire Bureau (E.1)
- Bureau of Environmental Services (E.2)
- Portland Bureau of Transportation (E.3)
- Urban Forestry (E.4)
- Life Safety (E.5)
- Site Development (E.6)
- Water Bureau

**Neighborhood Review:** A Notice of Proposal in Your Neighborhood was mailed on February 21, 2023. Three written response has been received from either the Neighborhood Association or notified property owners in response to the proposal.

- <u>Shannon and David Lea</u>, February 24, 2023, neighbors support the proposal, including the height. Some concerns about on-street parking impacts.
- <u>Dennis Harper</u>, February 25, 2023, neighbor has some concern about the proposed fence and lack of landscaping details. Supports the rooftop canopy design proposed for the penthouse of the new hotel.
- <u>Steve Pinger (Northwest District Association)</u>, February 23, 2023. The NWDA strongly supports the project concept, including the renovation and repurposing of the existing landmark building, and the proposed new hotel building on the adjacent parcel.
- <u>Charisse Tooze</u>, March 6, 2023, neighbor has concerns about the impact of the new development on on-street parking availability.

**Staff Response**: Staff appreciates these neighbors for taking the time to share their thoughtful and nuanced comments. While potential on-street parking impacts are not approval criteria for new development, the concerns about quality and durability of proposed materials *are* subject to review. Staff shared the concerns, and requested additional information related to building materials and detailing in order to determine whether the proposal meets the approval criteria related to quality of construction detailing and materials.

#### PROCEDURAL HISTORY

This application was submitted on November 16, 2022. The applicant deemed it complete on January 23, 2023. A hearing was scheduled for March 13, 2023 (49 days after deeming the application complete). The applicant submitted a full extension. The land use application will expire on January 23, 2024. Prior to the first hearing, a staff

report published on March 3, 2023 did not recommend approval due to a lack of necessary building and material details. The applicant submitted additional materials, and a revised staff report recommending approval was published on March 10, 2023.

#### ZONING CODE APPROVAL CRITERIA

#### Chapter 33.846.060 - Historic Resource Review

#### **Purpose of Historic Resource Review**

Historic Resource Review ensures the conservation and enhancement of the special characteristics of historic resources.

#### Historic Resource Review Approval Criteria

Requests for Historic Resource Review will be approved if the review body finds the applicant has shown that all of the approval criteria have been met.

**Findings:** The site is a designated Historic Landmark outside the Central City Plan District and not within in a Historic or Conservation District, and the proposal is for non-exempt treatments. Therefore, the proposal requires Historic Resource Review approval. The approval criteria are those listed in *33.846.060 G – Other Historic Approval Criteria*.

Staff has considered all of the approval criteria and addressed only those applicable to this proposal.

#### Historic Alphabet District - Community Design Guidelines Addendum

- **1. Historic Changes.** Most properties change over time; those changes that have acquired historic significance will be preserved.
- **2. Differentiate New from Old.** New additions, exterior alterations, or related new construction will retain historic materials that characterize a property to the extent practicable. Replacement materials should be reasonable facsimiles of the historic materials they replace. The design of new construction will be compatible with the historic qualities of the district as identified in the Historic Context Statement.

**Findings for 1 and 2:** The two-part proposal includes the restoration of the existing First Church of Christ Scientist Historic Landmark building, and the construction of a new freestanding hotel on an existing surface parking lot within the Landmark boundary. While the new hotel is designed to complement the Landmark, it is clearly differentiated from the Landmark in architectural style, material, fenestration pattern and massing.

The existing three-story, masonry Landmark building is notable for its character-defining domed roof form, arched leaded windows with opalescent glass and massive midblock portico. The proposed renovations are primarily limited to the interior, however a few exterior alterations are proposed. These include the repair and restoration of existing original windows on the three street-facing facades; repair or partial replacement of the existing tin roof; new frameless glass guardrail around the perimeter of the level three roof; new stucco rainscreen cladding at the level three mass; two new fabric awnings over basement level doors at the east façade; removal of an original chimney and an elevator overrun on the north side of the roof and construction of new stair overruns at the location of the original chimneys; replacement of all windows on the north (non-street facing) façade on levels one through three; a new basement level service entry on the north façade, and modification of the primary arched windows on the east and west level three facades to incorporate a person door

within the existing opening.

While there are numerous elements proposed to be altered, the scope of the proposed changes is modest in proportion to the scale of the existing building, its many years of deferred maintenance, and the complexity of adaptively reusing a building designed for such a different use. The alterations are carefully located to minimize visibility from the street level, while maintaining the character-defining features on three street-facing facades, and facilitating spill-out activation spaces that will add vibrancy to the public realm.

Therefore, these guidelines are met.

**3. Hierarchy of Compatibility.** Exterior alterations and additions will be designed to be compatible primarily with the original resource, secondarily with adjacent properties, and finally, if located within a historic or conservation district, with the rest of the District. Where practical, compatibility will be pursued on all three levels. New development will seek to incorporate design themes characteristic of similar buildings in the Historic Alphabet District.

**Findings for 3:** The proposal includes the removal of one remaining chimney on the north side, as well as a non-original elevator overrun also located on the north side of the roof. The chimney is one of the original pair that flanked the central dome and reinforced the building's symmetry. One of the paired chimneys was removed years ago, and the remaining unreinforced masonry chimney no longer serves the building functionally and detracts from the symmetrical architecture. The removal of the chimney and elevator overrun will facilitate the addition of two symmetrical stair overruns at the location of the original chimneys. The overruns are tucked in toward the center of the roof edge, furthest from the east and west street facades. They are proposed to be clad in a stucco cladding that will serve as a discreet background for the original cut stone cladding without drawing attention to itself.

As noted above, the bulk of the proposed alterations are targeted to the areas that are not visible from the public realm. The interior program and floor plans have been designed to ensure retention of the original architectural elements on the street-facing facades. The alterations and additions do not diminish the Landmark's iconic architecture, nor do they detract from the district.

Therefore, this guideline is met.

#### **Community Design Guidelines**

- **P1. Plan Area Character.** Enhance the sense of place and identity by incorporating site and building design features that respond to the area's desired characteristics and traditions.
- **P2. Historic and Conservation Districts.** Enhance the identity of historic and conservation districts by incorporating site and building design features that reinforce the area's historic significance. Near historic and conservation districts, use such features to reinforce and complement the historic areas.

**Findings for P1 and P2:** The Alphabet Historic District is notable for its diversity of architectural styles and building types. However, some architectural themes are present throughout the district, including cornice treatments; a consistent fenestration pattern and visual transition between floor levels – particularly between the ground level and upper stories.

The proposed new hotel enhances the district's identity by incorporating numerous building features that respond to the traditional Alphabet District context and reinforce its significance. These include the active, pedestrian-oriented ground level, defined by large operable windows and granite pilasters, that is strongly differentiated from the upper levels through a change in cladding material and fabric awnings.

Above the ground level, the windows repeat the regular fenestration pattern typical throughout the district. Ornate metal balcony railings add rich detailing in front of French doors at the level two balconies and at all windows at the building's corner. Additional stucco detailing at the cornice is typical of many historic buildings throughout the district. Above the cornice, a conservatory-inspired penthouse is set back from the roof edges. While the penthouse screens the building's mechanical equipment, it also contains a food service area to serve the adjacent roof deck. This activation of all available outdoor area is consistent with the highly walkable, pedestrian-oriented context, and will provide an asset to the neighborhood with rare view of the rooftops of numerous Historic Landmarks, as well as incredible vistas of the surrounding mountains and hills.

Overall, the hotel strikes the appropriate balance for new development within a Landmark boundary. It is an elegant stand-alone addition to the neighborhood while also being a graceful neighbor to the iconic adjacent Landmark.

Therefore, these guidelines are met.

- **E1. The Pedestrian Network.** Create an efficient, pleasant, and safe network of sidewalks and paths for pedestrians that link destination points and nearby residential areas while visually and physically buffering pedestrians from vehicle areas.
- **E2. Stopping Places.** New large-scale projects should provide comfortable places along pedestrian circulation routes where people may stop, visit, meet, and rest.
- **E3.** The Sidewalk Level of Buildings. Create a sense of enclosure and visual interest to buildings along sidewalks and pedestrian areas by incorporating small scale building features, creating effective gathering places, and differentiating street level facades.
- **E4.** Corners that Build Active Intersections. Create intersections that are active, unified, and have a clear identity through careful scaling detail and location of buildings, outdoor areas and entrances.
- **E5.** Light, Wind, and Rain. Enhance the comfort of pedestrians by locating and designing buildings and outdoor areas to control the adverse effects of sun, shadow, glare, reflection, wind, and rain.
- **D2. Main Entrances.** Make the main entrances to houses and buildings prominent, interesting, pedestrian-accessible, and transit-oriented.

**Findings for E1, E2, E3, E4, E5 and D2:** The proposed hotel is designed to facilitate an indoor-outdoor experience for its guests and to maximize activation of the pedestrian realm. The hotel introduces an active ground level, with operable storefront windows and recessed bays between pilasters to facilitate café seating along both street frontages (NW 19<sup>th</sup> Ave. and NW Flanders Street). Fixed fabric awnings provide year-round weather protection for pedestrians, while reinforcing the building's European-inspired style. The primary entrance is located at the curved corner, where it is most visible to guests arriving via one-way streets and it activates the corner. Wrapped by a large canopy, the tall entry doors accentuate the building's unique curved corner. The design utilizes

operable windows, French doors, balconies and a roof deck on upper levels to ensure that the entire building serves to activate the pedestrian realm.

Therefore, these guidelines are met.

- **D1. Outdoor Areas.** When sites are not fully built on, place buildings to create sizable, usable outdoor areas. Design these areas to be accessible, pleasant, and safe. Connect outdoor areas to the circulation system used by pedestrians;
- **D3.** Landscape Features. Enhance site and building design through appropriate placement, scale, and variety of landscape features.

**Findings for D1 & D3:** The new hotel building is built to the property lines on both street frontages, however it is set back fifteen feet from the Landmark to the south as well as ten feet from the shared property line with the existing multifamily apartment building to the east. A service path and code-required landscape buffer along the east side facilitate access between the loading space in the new building and the new basement level service door into the north façade of the existing Landmark. At the south side, a sunken courtyard between the basement levels of the new hotel and the existing Landmark serves to screen mechanical equipment, reducing the requirements for rooftop mechanical and facilitating a more active rooftop on the hotel. On the west side of this setback, the grade aligns with the sidewalk and the ADA access ramp to the Landmark building doubles as access to short-term bike parking.

The existing Landmark has deep, landscaped setbacks between the building and all street lot lines. On the east and west sides, the building is set back more than twenty feet from the sidewalks. New, occupiable outdoor spill out spaces are proposed on the east and west side of the Landmark. On the east side, a stepped courtyard is proposed between the building and the sidewalk, to serve as outdoor seating for a new basement-level café. On the west side, an outdoor garden with seating is proposed along the sidewalk edge. The enlarged site plan (C.2) indicates that the outdoor seating for both areas is proposed to be enclosed by a six-foot fence at the sidewalk edge, although this fence is not shown in the building elevations. Staff has concerns that the fence will create a fortress-like effect at the sidewalk edge, and that its presence may offset the public benefits of activating these outdoor spaces. Staff suggests that setting the fence back three feet from the sidewalk could allow a landscape buffer along the sidewalk edge that would better maintain the original landscape design, softening the fence and allowing it to visually recede from view.

The proposal maximizes the use of the site on behalf of both buildings, while utilizing grade and the buildings to screen necessary service and mechanical needs. The required bike parking is located on three street frontages, ensuring that hotel, club and retail guests are all able to easily locate convenient bike racks.

Therefore, these guidelines are met with an added nominal landscape buffer between the public sidewalk edge and the fences.

**D5. Crime Prevention.** Use site design and building orientation to reduce the likelihood of crime through the design and placement of windows, entries, active ground level uses, and outdoor areas.

**Findings for D5:** The proposal for both buildings is exemplary in its focus on activating the ground level, including exterior spaces. As described above, the

new hotel fronts both street lots lines, with its undeveloped outdoor space located between the buildings, where it can be easily secured and illuminated. Both street frontages are lined with operable windows, and the ground level has been designed to facilitate café seating along the sidewalks. Ground level lighting embedded in each pilaster at the new hotel ensures the spaces are well lit throughout the day.

Spill out seating in the east and west setbacks of the Landmark building incentivize more people to spend time outside, adjacent to the sidewalk, where they can provide 'eyes on the street' and enhance pedestrian perceptions of safety. The proposed fences are intended to ensure that the outdoor spaces may be secured after hours and do not feel like dark, unsafe spaces for pedestrians.

Therefore, this guideline is met.

- **D6. Architectural Integrity.** Respect the original character of buildings when making modifications that affect the exterior. Make additions compatible in scale, color, details, material proportion, and character with the existing building.
- **D7. Blending into the Neighborhood.** Reduce the impact of new development on established neighborhoods by incorporating elements of nearby, quality buildings such as building details, massing, proportions, and materials.

**Findings for D6 & D7:** The two-part proposal includes the restoration of the existing First Church of Christ Scientist Historic Landmark building, and the construction of a new freestanding hotel on an existing surface parking lot within the Landmark boundary. While the new hotel is designed to complement the Landmark, it is clearly differentiated from the Landmark in architectural style, material, fenestration pattern and massing.

#### Existing Landmark

The existing three-story, masonry Landmark building is notable for its character-defining domed roof form, arched leaded windows with opalescent glass and massive midblock portico. The proposed exterior alterations include the repair and restoration of existing original windows on the three street-facing facades; repair or partial replacement of the existing tin roof; new frameless glass guardrail around the perimeter of the level three roof; new stucco rainscreen cladding at the level three mass; two new fabric awnings over basement level doors at the east façade; removal of an original chimney and an elevator overrun on the north side of the roof and construction of new stair overruns at the location of the original chimneys; replacement of all windows on the north (non-street facing) façade on levels one through three; a new basement level service entry on the north façade, and modification of the primary arched windows on the east and west level three facades to incorporate a person door within the existing opening.

While there are numerous elements proposed to be altered on the existing Landmark, the scope of the proposed changes is modest in proportion to the scale of the existing building, its many years of deferred maintenance, and the complexity of adaptively reusing a building designed for such a different use. The alterations are carefully located to minimize visibility from the street level, while maintaining the character-defining features on three street-facing facades, and facilitating spill out activation spaces that will add vibrancy to the public realm.

#### New Hotel

The new hotel incorporates numerous elements of nearby, quality commercial and mixed-use buildings. These include the material palette that comprises of operable storefront windows and French doors overlooking the street; stucco detailing and material changes that differentiate the top, middle and bottom of the building; custom artistic balustrades, and fabric awnings providing pedestrian weather protection at every storefront bay. The proposed hotel design includes fine-grain aesthetic detailing, particularly at the bottom two levels, that are reminiscent of many of the nearby contributing resources.

The new building's massing is proposed to be built to the property line on both of its street facing frontages, while it is set back from both the contributing resource to its east and the Landmark building to the south. This differential setback respects the existing context while also responding to the neighborhood's desired character. The active ground level program at the sidewalk frontage serves to enliven the sidewalk environment, providing views of interior activity, new lighting, visual interest and opportunity for human interaction to the pedestrian realm.

The proposed hotel massing, architectural style, material palette and ground floor programming are an excellent model of respectful infill development in this historic district.

Therefore, these guidelines are met.

**D8. Interest, Quality, and Composition.** All parts of a building should be interesting to view, of long-lasting quality, and designed to form a cohesive composition.

**Findings for D8:** The applicant has provided numerous building details and cutsheets to reflect that the proposal is well-detailed, and utilizes materials that will both reflect the high-quality historic architecture of the surrounding context while ensuring a durable, long-lasting contribution to the city.

The new hotel is proposed to be primarily a triple-coat, field applied stucco rainscreen, with integrated stucco detailing. While pre-fabricated stucco panels are not always durable in our climate, high-quality, field applied stucco has been utilized in Portland for over a century, and has held up well through the temperature and weather extremes that are typical in our climate. The thick walls and applied stucco mouldings around the hotel windows foster the deeply recessed window punch typical of historic buildings and consistent with buildings of this style. A wall section demonstrates that the punch from the face of the exterior wall to the face of the window sash will be approximately sixinches deep, and will appear deeper due to the additional depth created by the stucco window mouldings. At level two, where bays are framed with additional stucco detailing, the transom windows and operable French doors are recessed one-foot two-inches from the face of the exterior surrounds. The deep recess of the French doors, creates an elegant condition that draws attention to the rich detailing at this level, including custom aluminum balusters at each French door bay, designed to relate to the unique muntin pattern of the Landmark's leaded windows.

At the ground level, the applicant proposes to recess the storefront bays between pilasters by approximately two feet. A granite stone sill beneath the storefront bays matches the granite stone base of each pilaster, and serves to protect the storefront bays from moisture from the sidewalk. Beneath the storefront windows, the applicant proposes painted wood paneling. While wood is not

always recommended at the ground level, in this situation it is proposed in a location and design that are consistent with many nearby commercial storefronts that have demonstrated durability for more than a century. To ensure resilience, the applicant has proposed red oak panels, with a paint finish. The stone sill below, the two-foot deep recessed condition between pilasters, and the fixed overhead awnings at each storefront bay serve to minimize the wood's weather exposure as well as its exposure to pedestrian impacts. The limited deployment of the wood elements and their accessibility to the sidewalk ensure ease of maintenance access, should repair or repainting be necessary.

The hotel's primary street-facing corner, at the intersection of NW 19th Avenue and NW Flanders, is accentuated with curved stucco and baluster detailing and the main entrance located at the chamfered corner. The entrance is highlighted with a unique, custom aluminum and glass canopy that provides deep coverage while also reinforcing the curved detailing of upper levels at the pedestrian realm.

The details provided demonstrate careful consideration of the materials proposed and that they are applied in a manner that ensures their durability and facilitates ease of access for maintenance and repair. The deep punches and rich detailing are consistent with the historic masonry aesthetic and with the surrounding historic context. The proposed design strikes the appropriate balance between being an attractive stand-alone contribution to the Alphabet Historic District while also being a deferential neighbor to the existing adjacent Landmark.

Therefore, this guideline is met.

#### 33.846.060 G - Other Historic Approval Criteria

- **1. Historic character.** The historic character of the landmark or contributing resource will be retained and preserved. Removal of historic materials or alteration of features and spaces that contribute to the historic significance of the landmark or contributing resource will be avoided.
- **2. Record of its time.** The landmark or contributing resource will remain a physical record of its time, place, and use. Changes that create a false sense of historic development, such as adding conjectural features or architectural elements from other buildings will be avoided.
- **3. Historic changes.** Most resources change over time. Those changes that have acquired historic significance will be preserved.
- **4. Historic features.** Generally, deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement, the new feature will match the historic feature in design, color, texture, and other visual qualities and, where practical, in materials. Replacement of missing features must be substantiated by documentary, physical, or pictorial evidence.
- **5. Historic materials.** Historic materials will be protected. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials will not be used.
- **7. Differentiate new from old.** New additions, exterior alterations, or related new construction will not destroy historic materials that characterize a landmark or contributing resource. New work may be differentiated from the old if the differentiation does not diminish the character, features, materials, form, or integrity of the landmark or contributing resource and, if in a Historic District, the district as a whole.

- **8. Architectural compatibility.** New additions, exterior alterations, or new construction will be compatible with the massing, size, scale, and architectural features of the landmark or contributing resource and, if in a district, the district as a whole. When retrofitting to improve accessibility for persons with disabilities or accommodate seismic improvements, design solutions will not compromise the architectural integrity of the landmark or contributing resource.
- **9. Preserve the form and integrity of historic resources.** New additions, exterior alterations, or new construction will be undertaken in such a manner that if removed in the future, the essential form and integrity of the landmark or contributing resource and, if in a district, the district as a whole would be unimpaired.
- **10. Hierarchy of compatibility.** New additions, exterior alterations, or new construction will be designed to be compatible primarily with the landmark or contributing resource and, if located within a district, secondarily with contributing resources within 200 feet and, finally, with the rest of the district. Where practical, compatibility in districts will be pursued on all three levels.

Findings for 1, 2, 3, 4, 5, 7, 8, 9, and 10: The existing First Church of Christ Scientist, an individually-listed Historic Landmark is being retained and preserved. Its character-defining leaded windows and domed roof are being retained and restored, and it will continue to serve as a record of its time. The proposed exterior alterations are minor elements intended to allow the adaptive reuse of a challenging structure, and are mostly limited to the north façade, which is the building's only non-street facing façade. Extensive restoration and repair work is proposed to the existing windows, the cut stone cladding and the roofing.

Due to building code requirements, activation of the flat rooftops to create terraces with outdoor seating required the introduction of multiple new egress doors. The applicant proposes modest new stair and elevator overruns tucked toward the center of the rear façade, furthest from the street-facing facades. These additions are proposed to be clad in stucco, to complement the color of the cut stone cladding but to differentiate these minor service elements from the original architecture.

Additionally, a secondary egress is needed from both the east and west side of the roof in order to facilitate the rooftop conversion to publicly accessible outdoor space. At staff's direction, the applicant explored multiple locations for this additional door, including replacing south-facing windows with doors, or modifying the tallest windows on level three of the east and west facades. With the goal of limiting the introduction of new regulating lines caused by the door frame, the applicant is proposing to insert a custom ten-foot door into the center of both of these windows. At that height, the door will infill the entire height of the existing window opening, eliminating the need for an infill panel above the door. The original window glazing will be modified and retained on either side of the door to maintain the character-defining leaded window pattern.

At the Design Advice Request, a Commissioner suggested the applicant explore opportunities to utilize the site's slope to introduce a new ADA ramp to the front door. The applicant has illustrated this explorative exercise on C.23. However, the investigation ultimately revealed that due to an eight-inch step at the main entrance door, and the location of existing historic columns, a ramp at this location does not meet the requirements for ADA turning radius and ramp width, and meeting the requirements would require fundamentally changing the design of the original entrance. Therefore, the applicant is proposing accessible entrances off both the east and the west facades. An accessible path through the

new east courtyard provides access from NW 18th Avenue to the proposed basement level coffee shop at this frontage. At the west entrance, located at the north end of the west façade, the ramped entry will bring users into the main level, and doubles as a primary entrance for guests arriving via the hotel or those accessing the new proposed spill-out garden seating on the west side of the hotel. While it would be more equitable to ensure all guests are able to arrive via the same entrance, the design, location and programming of the west ADA entrance demonstrate that it will be a primary entrance for users of all abilities and that guests arriving via these doors will not be navigating back of house conditions.

Therefore, these criteria are met.

#### 33.846.070 Modifications Considered During Historic Resource Review

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the historic resource review process. These modifications are done as part of historic resource review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through historic resource review may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

- A. **Better meets historic resource review approval criteria.** The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and
- B. Purpose of the standard.
  - 1. The resulting development will meet the purpose of the standard being modified; or
  - 2. The preservation of the character of the historic resource is more important than meeting the purpose of the standard for which a modification has been requested.

## Modification #1: Building Height, PZC 33.130.210.B.1 / Table 130-2 / Table 130-3 -increase the allowed building height by ten feet, from the allowed 60-feet to 70-feet above basepoint.

Purpose Statement: The height limits are intended to control the overall scale of buildings. The height limits in the CM2 and CE zones allow for a greater building height at a scale that can accommodate the growth intended for centers and corridors, while relating to the low- to mid-rise scale of neighborhood residential areas. Exceptions to height limit standards accommodate minor projections that do not significantly increase the visual scale of buildings; provide flexibility in the height of parapets and railings to facilitate rooftop outdoor spaces and equipment screening; and accommodate ground-floor spaces with high ceilings to encourage ground-floor commercial uses, mechanical parking, and other uses that benefit from high ceilings.

Standard: 33.130.210.B.1 - The base height standards for all structures, except detached accessory structures, are stated in Table 130-2. Table 130-2 states that the base height in the CM2 zone is 45 feet, and bonus height is in Table 130-3. Table 130-3 states that the maximum height in the CM2 zone with bonus is 55 feet.

Note: the applicant has combined numerous code-allowed height exceptions with allowed bonus heights to achieve an allowable height of 60-feet. These include a 45-foot base height, plus a five-foot high ceilings height exception, plus a ten-foot height bonus for providing an affordable commercial space, for a total of sixty-feet of allowable height. The ten-foot Modification request is in addition to that sixty-feet.

**A.** Better meets historic resource review approval criteria. The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and

**Findings for A:** The modification is to increase the height of the building to allow for occupiable space at the rooftop penthouse level (in addition to the mechanical, stair and elevator overruns that are already allowed by code). The code allows for height exceptions for mechanical and stair/elevator overruns, provided they fall within specific height limits and are set back a minimum of fifteen feet from roof edges on all street facing facades. The proposed occupiable space is set back more than fifteen feet from the street facing roof edges, and meets the ten-foot height limitation allowed for rooftop mechanical, except for where the stair and elevator overrun are located. These elements meet the sixteen-foot height exception in the code. Only the occupiable floor area at the rooftop level necessitates this modification request.

The modification better meets the guidelines D1 – Outdoor Areas and D3 – Landscape Features. The benefits of allowing the penthouse-level interior space are that it facilitates the active use of a rooftop, creating an additional publicly accessible commercial space with a rooftop restaurant, and opportunity for outdoor activity in a vibrant and densifying neighborhood. As the city grows, it is increasingly important to maximize the use of outdoor spaces. Private outdoor space is a privilege many do not have access to, and rooftops are one of the underutilized opportunities for introducing new places for the public to enjoy Portland's mild climate and phenomenal views.

**B.** Purpose of the standard. The resulting development will meet the purpose of the standard being modified.

**Findings for B:** The proposal meets the purpose of the standard because the proposed interior penthouse level space meets the height limits and setbacks required for allowed rooftop mechanical and overruns and does not dramatically alter the building's perceived height or scale, particularly within its immediate context. Sight line diagrams indicate that the mass is set back sufficiently to not be seen from the adjacent rights of way, and is only slightly visible from a block away, where the higher grade allows pedestrians to see the roofline of the new mass. It will have little impact on the building's perceived scale. Further, immediately across NW 19th Avenue, an existing non-conforming apartment building is over 129-feet tall, with additional mechanical elements extending above that height.

The proposal better meets the approval criteria and the purpose statement for the standard being modified.

Therefore, this Modification merits approval.

Modification #2: Loading Space Size, PZC 33.266.310.D.a – decrease the standard A loading space size from the required 35-feet long, 10-feet wide and 13-foot clearance to 25-feet long, 10-feet wide and 13-foot clearance.

### Modification #3: Forward Motion, PZC 33.266.310.F.1 – allow loading space to be accessed and exited in a rearward motion.

Purpose Statement: A minimum number of loading spaces are required to ensure adequate areas for loading for larger uses and developments. These regulations ensure that the appearance of loading areas will be consistent with that of parking areas. The regulations ensure that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

Standard: 33.266.310.D.a: Standard A: the loading space must be at least 35 feet long, 10 feet wide, and have a clearance of 13 feet.

Standard 33.266.310.F.1: Outside the Central City plan district. Outside the Central City plan district, loading facilities generally must be designed so that vehicles enter and exit the site in a forward motion. Standard B loading spaces that are accessed from a Local Service Traffic Street are exempt from this requirement

**A.** Better meets historic resource review approval criteria. The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and

**Findings for Modification #2.A:** The modification to reduce the length of the proposed loading space better meets approval criteria E1 – The Pedestrian Network and E3 – The Sidewalk Level of Buildings. The modification request is based on loading data collected by the applicant and reviewed by the Portland Bureau of Transportation engineer. The collected data demonstrated that the vehicles utilized to service this size and type of program do not require the 35-foot length of a Standard A space, but are too large to be served by a Standard B space. Therefore, the modified dimensions are proposed to ensure that the space is adequately sized to serve the vehicles that will be accessing the space but that it is not unnecessarily large. The reduced size allows the floor area to be better utilized for other uses accessory to the retail sales and service program. Maximizing the active uses at the ground floor benefits the sidewalk environment and pedestrian realm, facilitating more opportunity for visual interest at the sidewalk level of the building.

**Findings for Modification #3.A:** The modification to reduce the length of the proposed loading space better meets approval criteria E1 – The Pedestrian Network and E3 – The Sidewalk Level of Buildings. In order to meet the code standard requiring a loading space may be entered and exited in a forward motion, the proposal would need to have a one-way driveway wrapping around the building. Neither PBOT nor BDS supported this proposal, as it would have required a large percentage of the site area to be allocated to surface vehicle area, and PBOT determined that having a truck exit in a rearward motion onto NW Flanders Street will create fewer traffic conflicts than having vehicles exit onto NW 19<sup>th</sup> Avenue near the complex intersection of NW 19<sup>th</sup> Avenue and NW Everett streets.

Minimizing the amount of on-site vehicle area, with a design that internalizes the loading space into the building mass upholds the approval criteria dedicated to fostering an active and vibrant pedestrian realm.

**B. Purpose of the standard.** The resulting development will meet the purpose of the standard being modified.

**Findings for Modifications #2 and #3 B:** The proposal meets the purpose of the standard, since the requested loading space dimension reflects the size of trucks servicing the site, and is derived from data collected specific to this use and size of program. The appropriately sized loading space ensures that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

Allowing vehicles accessing the loading space to enter or exit in a rearward motion has been deemed the safest option, and will result in fewer conflicts with other vehicles, bicyclists and pedestrians than if the standard were met with a one-way driveway around the site.

The proposal better meets the approval criteria and meets the purpose statement of the standard proposed to be modified.

Therefore, these Modifications merit approval.

Modification #4: Short Term Bike Parking Locations, PZC 33.266.210.E.1.a.(3)—increase the maximum distance that short-term bike parking may be located from a main entrance, from 50 feet to 65 feet.

Standard 33.266.210: These standards ensure that required bicycle parking is designed so people of all ages and abilities can access the bicycle parking and securely lock their bicycle without undue inconvenience. Bicycle parking is in areas that are reasonably safeguarded from theft and accidental damage. The standards allow for a variety of bicycle types, including but not limited to standard bicycles, tricycles, hand cycles, tandems, electric motor assisted cycles and cargo bicycles. Long-term bicycle parking is in secure, weather protected facilities and is intended for building and site occupants, and others who need bicycle parking for several hours or longer. Short-term bicycle parking is located in publicly accessible, highly visible locations that serve the main entrance of a building. Short-term bicycle parking is visible to pedestrians and bicyclists on the street and is intended for building and site visitors.

**A.** Better meets historic resource review approval criteria. The resulting development will better meet the approval criteria for historic resource review than would a design that meets the standard being modified; and

**Findings for A:** The proposal to increase the maximum distance allowed between the required short-term bike parking and the main entrances on both buildings better meets historic resource review approval criteria E1 – The Pedestrian Network. Because the new hotel is built to the property line on both street frontages and does not have any on-site space within fifty feet of the main entrances where short term bike parking could meet the locational standards, the applicant had the option to buy into the short-term bike parking fund. This would have eliminated the requirement to provide any short-term bike parking on site, however there is no guarantee that PBOT will be able to use the associated fees to locate bike parking in the adjacent right-of-way. The applicant desires to provide convenient, on-site bike parking accessible to each of their retail sales and service uses, despite not all the short-term bike parking being located within the maximum 50-foot distance from a main entrance.

This solution better meets the approval criteria, as the resulting proposal cumulatively provides all the required short-term bike parking spaces but separates them into three distinct locations, located off three separate street frontages. The

site has four street frontages, and while locating all the bike parking in one location may have met the code standard, it would not have resulted in convenient, findable bike parking facilities for all guests to the site. The dispersed bike parking proposal is more efficient, pleasant and safe, as it locates bike parking in convenient areas that are adjacent to windows, sidewalks and other active spaces where many people can see into the bike parking areas.

**B.** Purpose of the standard. The resulting development will meet the purpose of the standard being modified;

**Findings for B:** The proposed short-term bike parking proposal is designed so people of all ages and abilities can access the bike parking. It is conveniently located adjacent to three of the site's four frontages. The locations are highly visible, close to primary entrances to many of the retail sales and service uses in both buildings.

The proposal better meets the approval criteria and meets the purpose statement of the standard proposed to be modified.

Therefore, this Modification merits approval.

#### **DEVELOPMENT STANDARDS**

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

#### CONCLUSIONS

The purpose of the Historic Resource Review process is to ensure that additions, new construction, and exterior alterations to historic resources do not compromise their ability to convey historic significance. This proposal respectfully converts an existing Landmark church building into a complicated program of multiple new retail sales and service uses. It will adaptively reuse the existing building to house numerous active uses, while prioritizing minimal exterior alterations. The new hotel is designed to complement both the adjacent Landmark and the surrounding historic district context. It models an excellent ground level and maximizes opportunities for activation adjacent to the streets. However, the proposal lacks necessary details to determine quality and resilience guidelines. Therefore, it does not yet warrant approval.

#### TENTATIVE STAFF RECOMMENDATION

(May be revised upon receipt of new information at any time to the Landmarks Commission decision)

Staff recommends approval of the restoration of an existing Historic Landmark to a private club with rooftop dining and retail sales and service uses, and the construction of a new six-story hotel within the Landmark boundary.

Staff recommends approval for the following Modification requests:

1. Increase the allowed building height by ten feet, from the allowed 60-feet to 70-feet above basepoint (PZC 33.130.210.B.1 / Table 130-2 / Table 130-3);

- 2. Decrease the standard A loading space size from the required 35-feet long, 10-feet wide and 13-foot clearance to 25-feet long, 10-feet wide and 13-foot clearance (PZC 33.266.310.D.a):
- 3. Allow loading space to be accessed and exited in a rearward motion (PZC 33.266.310.F.1); and
- 4. Increase the maximum distance that short-term bike parking may be located from a main entrance, from 50 feet to 65 feet (PZC 33.266.210.E.1.a.(3)).

Staff recommends the following conditions of approval:

- A. As part of the building permit application submittal, the following development-related conditions (B through C) must be noted on each of the 4 required site plans or included as a sheet in the numbered set of plans. The sheet on which this information appears must be labeled "ZONING COMPLIANCE PAGE Case File LU 22-204531 HRM AD". All requirements must be graphically represented on the site plan, landscape, or other required plan and must be labeled "REQUIRED."
- B. At the time of building permit submittal, a signed Certificate of Compliance form (<a href="https://www.portlandoregon.gov/bds/article/623658">https://www.portlandoregon.gov/bds/article/623658</a>) must be submitted to ensure the permit plans comply with the Design/Historic Resource Review decision and approved exhibits.

C.	Nο	field	changes	allowed.	

\_\_\_\_\_

**Procedural Information.** The application for this land use review was submitted on November 16, 2022, and was determined to be complete on January 23, 2023.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore, this application was reviewed against the Zoning Code in effect on November 16, 2022.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant waived the 120-day review period, as stated with Exhibit (Exhibit A.2) Unless further extended by the applicant, **the 120 days will expire on: January 23, 2024.** 

Some of the information contained in this report was provided by the applicant.

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the recommendation of the Bureau of Development Services with input from other City and public agencies.

This report is not a decision. The review body for this proposal is the Landmarks Commission who will make the decision on this case. This report is a recommendation to the Landmarks Commission by the Bureau of Development

Services. The review body may adopt, modify, or reject this recommendation. The Landmarks Commission will make a decision about this proposal at the hearing or will grant a continuance. Any new written testimony should be emailed to Hannah Bryant at Hannah.Bryant@PortlandOregon.gov. If you cannot email comments and must mail comments via USPS mail, your comments to the Landmarks Commission can be mailed c/o the Landmarks Commission, 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201.

Please note regarding USPS mail: USPS mail is only received a couple times a week, and testimony must be received before the close of the record. Therefore, please mail testimony well in advance of the hearing date.

If you are interested in viewing information in the file, please contact the planner listed on this decision. The planner can provide information over the phone or via email. Please note only digital copies of material in the file are available for viewing. A digital copy of the Portland Zoning Code is available on the internet at <a href="http://www.portlandoregon.gov/zoningcode">http://www.portlandoregon.gov/zoningcode</a>.

You will receive mailed notice of the decision if you write a letter received before the hearing or testify at the hearing, or if you are the property owner or applicant. This Staff Report will be posted on the Bureau of Development Services website. Look at www.portlandonline.com. On the left side of the page use the search box to find Development Services, then click on the Zoning/Land Use section, select Notices and Hearings. Land use review notices are listed by the District Coalition shown at the beginning of this document.

**Appeal of the decision.** The decision of the Landmarks Commission may be appealed to City Council, who will hold a public hearing. If you or anyone else appeals the decision of the Landmarks Commission, City Council will hold an evidentiary hearing, one in which new evidence can be submitted to them. Upon submission of their application, the applicant for this land use review chose to waive the 120-day time frame in which the City must render a decision. This additional time allows for any appeal of this proposal to be held as an evidentiary hearing.

Who can appeal: You may appeal the decision only if you write a letter which is received before the close of the record for the hearing, if you testify at the hearing, or if you are the property owner/applicant. Appeals must be filed within 14 days of the decision. An appeal fee of \$5,789.00 will be charged.

Additional information on how to file and the deadline for filing an appeal will be included with the decision. Assistance in filing the appeal and information on fee waivers are available from the Bureau of Development Services website: <a href="https://www.portlandoregon.gov/bds/article/411635">https://www.portlandoregon.gov/bds/article/411635</a>. Neighborhood associations recognized by the Office of Neighborhood Involvement may qualify for a waiver of the appeal fee provided that the association has standing to appeal. The appeal must contain the signature of the Chair person or other person\_authorized by the association, confirming the vote to appeal was done in accordance with the organization's bylaws.

Neighborhood associations, who wish to qualify for a fee waiver, must complete the Type III Appeal Fee Waiver Request for Organizations Form and submit it prior to the appeal deadline. The Type III Appeal Fee Waiver Request for Organizations Form contains instructions on how to apply for a fee waiver, including the required vote to appeal.

#### Recording the final decision.

If this Land Use Review is approved the final decision will be recorded with the Multnomah County Recorder.

• *Unless appealed*, the final decision will be recorded after **March 14, 2022**, by the Bureau of Development Services.

The applicant, builder, or a representative does not need to record the final decision with the Multnomah County Recorder.

For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

**Expiration of this approval.** An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Zone Change and Comprehensive Plan Map Amendment approvals do not expire.

**Applying for your permits.** A building permit, occupancy permit, or development permit must be obtained before carrying out this project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed here.
- All applicable development standards, unless specifically exempted as part of this land use review.
- All requirements of the building code.
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the city.

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

Hannah Bryant March 10, 2023

#### **EXHIBITS** – NOT ATTACHED UNLESS INDICATED

- A. Applicant's Submittals
  - 1. Original Submittal
  - 2. Full Extension, November 28, 2022
  - 3. Rooftop Egress Door Options, January 13, 2023
  - 4. Request from Applicant to deem complete, January 19, 2023
  - 5. Response to Incomplete, January 23, 2023
  - 6. Final Submittal, dated February 21, 2023
  - 7. Revised Final Submittal, dated March 8, 2023
- B. Zoning Map (attached)
- C. Plan & Drawings
  - 1. Not used
  - 2. Site Plan (Landmark) (attached)

- 3. Basement Floor Plan (Landmark)
- 4. First Floor Plan (Landmark)
- 5. Second Floor Plan (Landmark)
- 6. Third Floor Plan (Landmark)
- 7. Roof Plan (Landmark)
- 8. Black and White South Elevation (Landmark)
- 9. Black and White West Elevation (Landmark)
- 10. Black and White East Elevation (Landmark)
- 11. Black and White North Elevation (Landmark)
- 12. Color South Elevation (Landmark)
- 13. Color West Elevation (Landmark)
- 14. Color East Elevation (Landmark)
- 15. Color North Elevation (Landmark)
- 16. Demolition South Elevations (Landmark)
- 17. Demolition West Elevations (Landmark)
- 18. Demolition East Elevations (Landmark)
- 19. Demolition North Elevations (Landmark)
- 20. Existing Conditions (Landmark)
- 21. Details new door at Landmark level 3 (Landmark)
- 22. Rooftop addition Landmark level 3 (Landmark)
- 23. Window Awning Details
- 24. Exterior Materials
- 25. Window Repair and Replacement Scope (Landmark)
- 26. Not used
- 27. Site Plan (New Hotel)
- 28. Floor Plans (New Hotel)
- 29. Floor Plans (New Hotel)
- 30. Black and White West Elevations (New Hotel)
- 31. Black and White North Elevation (New Hotel)
- 32. Black and White South Elevation (New Hotel)
- 33. Black and White East Elevation (New Hotel)
- 34. Color West Elevations (New Hotel)
- 35. Color North Elevation (New Hotel)
- 36. Color South Elevation (New Hotel)
- 37. Color East Elevation (New Hotel)
- 38. Details Storefront (New Hotel)
- 39. Details Awning and Baluster (New Hotel)
- 40. Details Wall Section
- 41. Details Penthouse Wall Section and Awning
- 42. Details Main Entrance Canopy
- 43. Affordable Commercial Space
- 44. Exterior Materials
- 45. FAR Summary
- 46. Short Term Bike Parking
- 47. Long Term Bike Parking
- 48. Ground Floor Windows (New Hotel)
- 49. Height Diagram (New Hotel)
- 50. Window and Door Cutsheets
- 51. Oasis Multifold Door
- 52. Oasis Multifold Door
- 53. French Door
- 54. French Door
- 55. Garage Door Cutsheet
- 56. Stucco Cutsheet
- 57. Mechanical Screen & Exterior Lighting

- 58. Glass Railing Cutsheet
- 59. Rooftop Mechanical Cutsheet
- 60. Rooftop Mechanical Cutsheet
- 61. Landscape Tree Plan
- 62. Landscape Site Plan
- 63. Landscape Roof Plan
- 64. Landscape Planting Plan
- 65. Landscape Planting Plan Roof Decks
- 66. Landscape Planting Sections
- 67. Existing Civil Plan (New Hotel)
- 68. Existing Civil Plan (Landmark)
- 69. Civil Plan (Hotel)
- 70. Civil Plan (Landmark)
- 71. Utility Plan (Hotel)
- 72. Utility Plan (Landmark)
- 73. Stormwater Plan (Hotel)
- 74. Stormwater Plan (Landmark)
- D. Notification information:
  - 1. Request for response
  - 2. Posting letter sent to applicant
  - 3. Notice to be posted
  - 4. Applicant's statement certifying posting
  - 5. Mailed notice
  - 6. Mailing list
- E. Agency Responses:
  - 1. Bureau of Environmental Services
  - 2. Bureau of Transportation Engineering and Development Review
  - 3. Water Bureau
  - 4. Fire Bureau
  - 5. Bureau of Parks, Forestry Division
  - 6. Site Development Review Section of BDS
- F. Letters
  - 1. Shannon and David Lea, February 24, 2023, neighbors support the proposal, including the height. Some concerns about on-street parking impacts.
  - 2. Dennis Harper, February 25, 2023, neighbor has some concern about the proposed fence and lack of landscaping details. Supports the rooftop canopy design proposed for the penthouse of the new hotel.
  - 3. Steve Pinger (NWDA), February 23, 2023. The NWDA strongly supports the project concept, including the renovation and repurposing of the existing landmark building, and the proposed new hotel building on the adjacent parcel.
  - 4. Charisse Tooze, March 6, 2023, neighbor has concerns about the impacts of the new development on on-street parking availability.
- G. Other
  - 1. Original LUR Application
  - 2. Incomplete Letter, December 7, 2022
- H. Not used





## City of Portland Historic Landmarks Commission

## Type III Land Use Review

LU 22-204531 HRM

## 1819 NW Everett + 1880 NW Flanders

March 13, 2023

**Staff Presentation** 

### **Context**

Location
Zoning
Approval Criteria
Context
Program Overview
Project History
Modifications

## **Applicant Presentation**

## **Approvability Items**

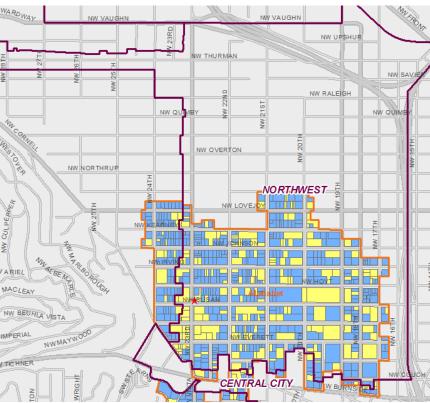
Micro Mid Macro Modifications

## **Staff Recommendation**

Conditions of Approval Next Steps



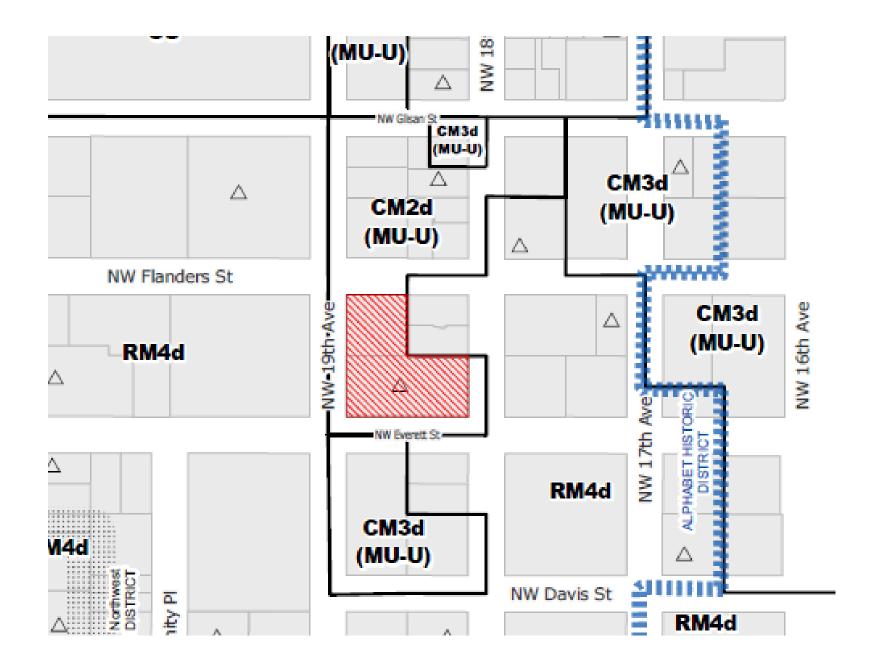
# PETTYGROVE HOYT N MACLEAY



## Location

Northwest Plan District

Alphabet Historic District



## Zoning

**Base Zone:** 

CM2d -

Commercial Mixed-Use 2

Overlay:

Historic Resource Overlay Design Overlay

Floor Area Ratio:

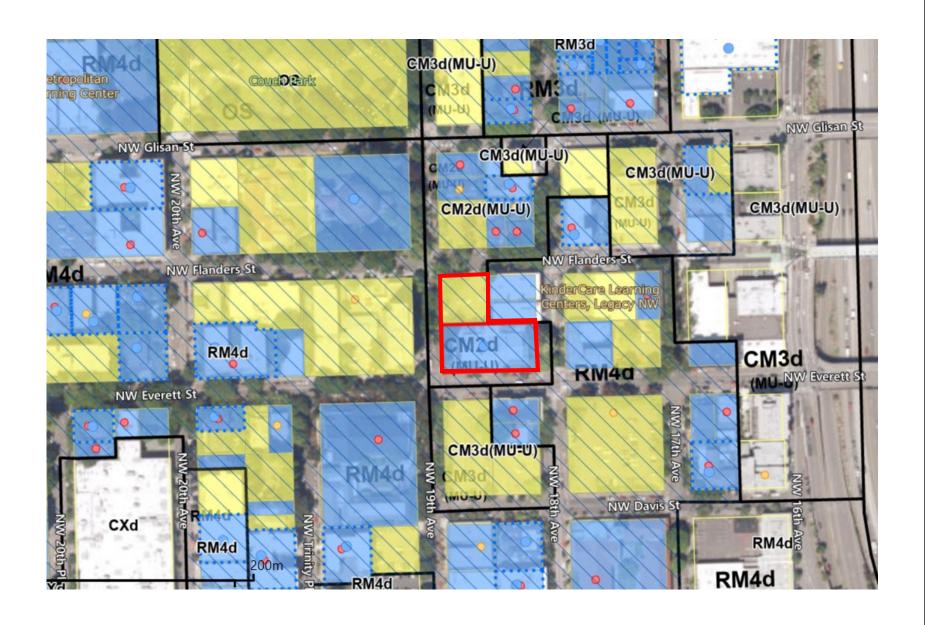
2.5:1 base

2.49:1 proposed

Height:

45' max base

- + 5' tall ceilings exception
- + 10' affordable comm. bonus
- = 60' max allowed per zoning 70' proposed



# Approval Criteria

Historic Alphabet District Addendum

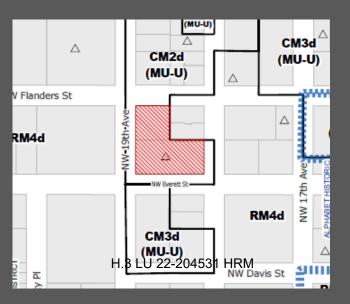
Community Design Guidelines

33.846.060.G – for existing Landmark only



#### Context

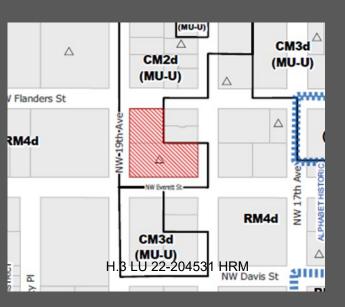
- I-405 2 blocks east
- Couch Park 2 blocks north
- NW 21<sup>st</sup> Ave 2 block west





# Context

- Historic Landmark
- Contributing Resource
- Built 1909
- Former Church

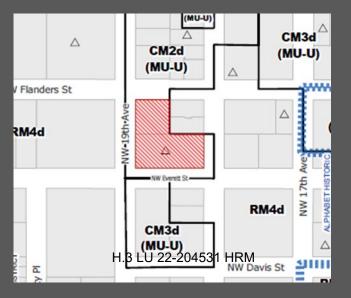




# Program Overview

- Landmark Building:

   private club with
   restaurants, coffee shop,
   spa, event space, roof
   deck.
- New hotel: restaurants, 80 hotel rooms, affordable commercial space, roof deck.
- 1 loading space





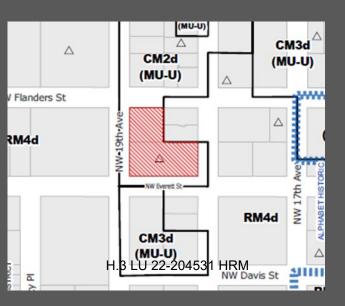




Design Advice Request – September 2022

# **Project History**

- Pre-App April 2022
- DAR September 2022

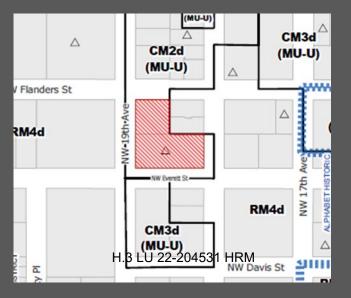




Staff Report #1– March 3, 2023 Did <u>NOT</u> recommend approval Staff Report #2– March 10, 2023 Recommends <u>approval</u>

# **Project History**

- Staff Report #1 Denial March 3, 2023 due to lack of building details
- Revised Drawings
   Submitted
   March 8, 2023 includes
   new building details
- Staff Report #2 Approval March 10, 2023





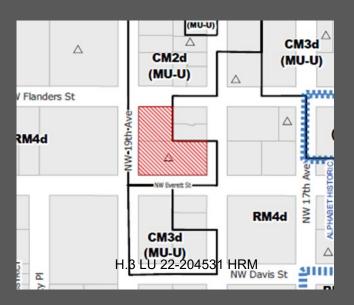
# **Modification 1: Building Height**

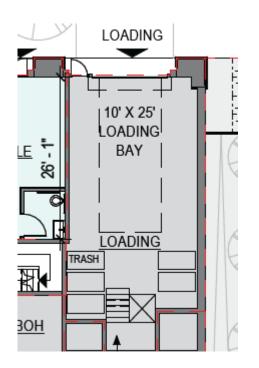
The Standard: A combination of code-allowed height exceptions plus bonuses results in an allowable height of 60-feet. (45' base height + 5' tall ground floor height exception + 10' affordable commercial height bonus)

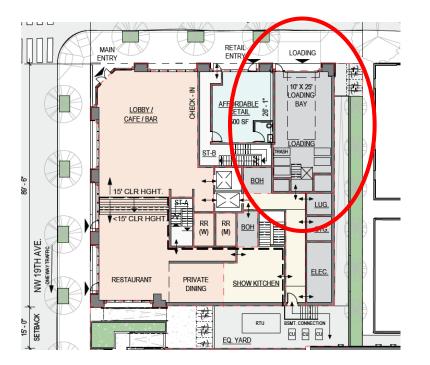
The Purpose: To control the overall scale of buildings. The height limits in the CM2 zone allows for a greater building height at a scale that can accommodate the growth intended for centers and corridors, while relating to the low- to mid-rise scale of neighborhood residential areas.

The Proposal: The applicant proposes a height of 70-feet to facilitate an occupiable rooftop penthouse level with rooftop seating.

- 1. Modification 1 Height
- 2. Modification 2
- 3. Modification 3
- 4. Modification 4







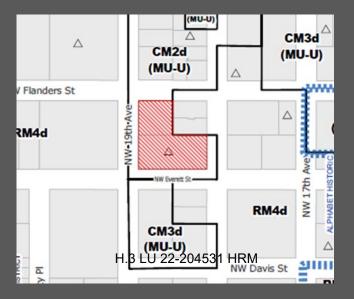
## **Modification 2: Loading Space Dimension**

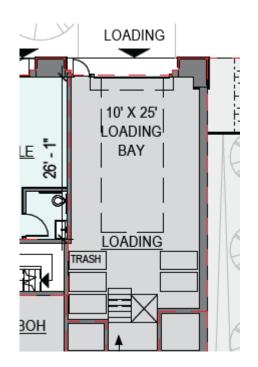
The Standard: Standard A: the loading space must be at least 35 feet long, 10 feet wide, and have a clearance of 13 feet.

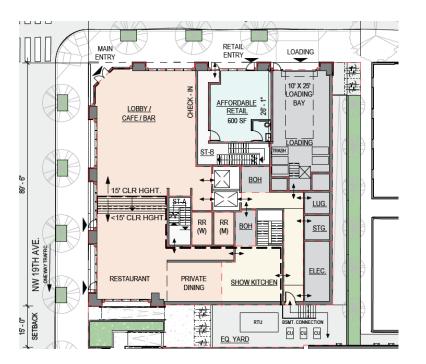
The Purpose: The regulations ensure that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

The Proposal: decrease the standard A loading space size from the required 35-feet long, 10-feet wide and 13-foot clearance to 25-feet long, 10-feet wide and 13-foot clearance.

- 1. Modification 1 Height
- 2. Modification 2 Loading Space Size
- 3. Modification 3
- 4. Modification 4







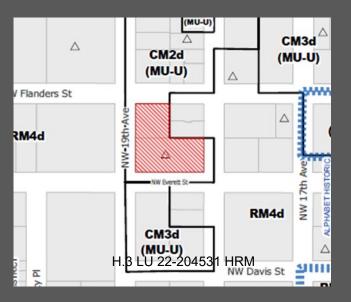
#### **Modification 3: Forward Motion**

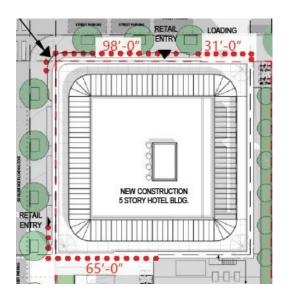
The Standard: Outside the Central City plan district, loading facilities generally must be designed so that vehicles enter and exit the site in a forward motion.

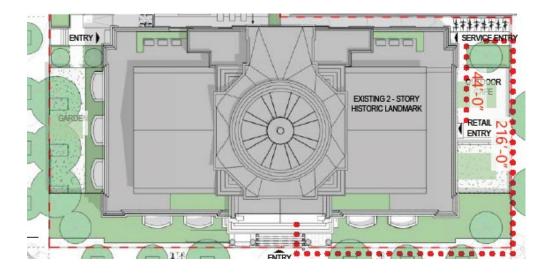
The Purpose: The regulations ensure that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

The Proposal: Allow loading space to be accessed and exited in a rearward motion.

- 1. Modification 1 Height
- 2. Modification 2 Loading Space Size
- 3. Modification 3 Forward Motion
- 4. Modification 4







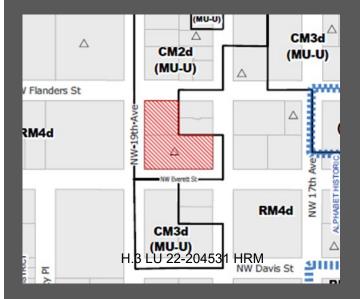
# **Modification 4: Short-Term Bike Parking**

The Standard: For a building with more than one main entrance, the bicycle parking must be along all façades with a main entrance, and within 50 feet of at least one main entrance on each façade that has a main entrance, as measured along the most direct pedestrian access route.

The Purpose: Short-term bicycle parking is located in publicly accessible, highly visible locations that serve the main entrance of a building. Short-term bicycle parking is visible to pedestrians and bicyclists on the street and is intended for building and site visitors.

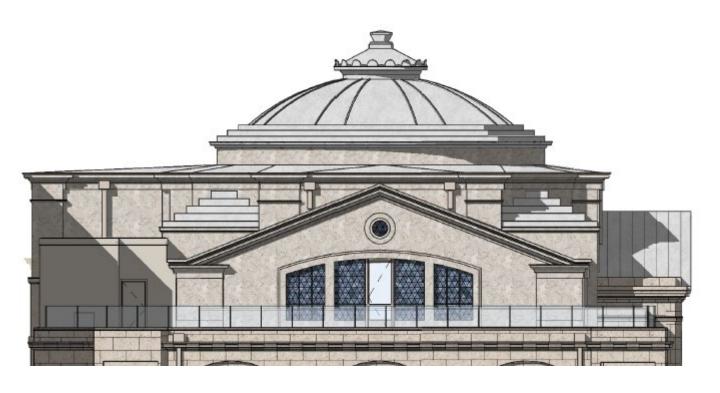
The Proposal: Increase the maximum distance that short-term bike parking may be located from a main entrance, from 50 feet to 65 feet.

- 1. Modification 1 Height
- 2. Modification 2 Loading Space Size
- 3. Modification 3 Forward Motion
- 4. Modification 4 Short-Term Bike Parking Location



# Applicant Presentation H.3 LU 22-204531 HRM

# Discussion Topics H.3 LU 22-204531 HRM



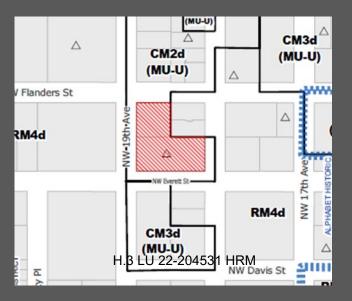


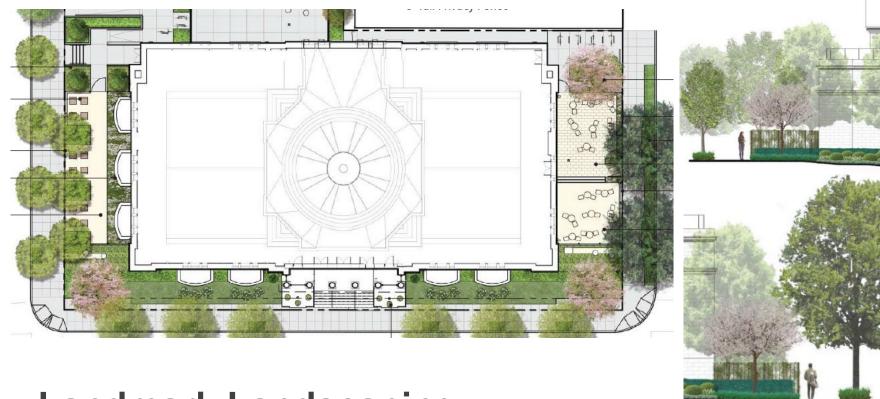
# **Landmark Materials and Detailing**

Exterior alterations on the Landmark include:

- Modifying the level three east and west windows to include a clear glass door;
- Adding glass railings behind the level 3 parapet to enclose a roof deck;
- Removing north (full and partial) chimneys;
- Removing non-original elevator overrun on north side;
- Stair overrun additions on north side;
- Replace north basement window with door;
- New awnings over east and west doors.

- 1. Exterior Alterations
- 2. Landscaping
- 3. Hotel Details



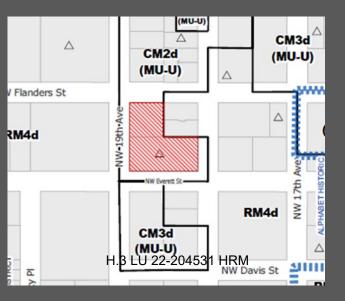


# **Landmark Landscaping**

Site alterations on the Landmark include:

- Addition of courtyards on east and west façade;
- Addition of 6' tall metal fence at east and west property lines.
- New ADA ramp at west entry; New ADA accessible hardscaping at east entry.

- 1. Exterior Alterations
- 2. Landscaping
- 3. Hotel Details



# 6' TALL METAL FENCE PROPERTY LINE 12' SIDEWALK AND PLANTING ZONE 1' 1'

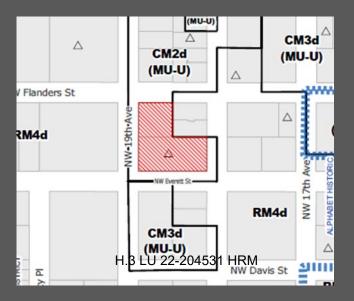


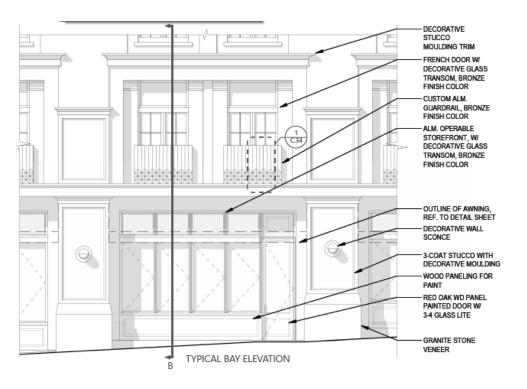




# **Landmark Landscaping**

- 1. Exterior Alterations
- 2. Landscaping
- 3. Hotel Details









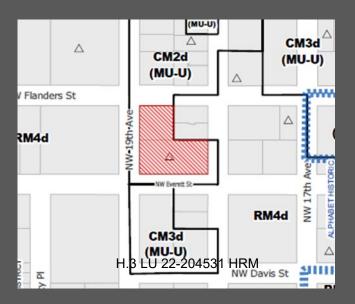
WOOD STOREFRONT SURROUND

# **Hotel Materials and Detailing**

Additional details were provided by the applicant in an updated drawing package on March 8<sup>th</sup>. They were uploaded and a link shared with Commission on March 9<sup>th</sup>. A revised Staff Report was uploaded and a link shared with Commission on March 10<sup>th</sup>.

With the addition of new building details and cutsheets, staff found that guideline D8 – Interest, Quality and Composition is met. The revised Staff Report reflects this and recommends approval.

- 1. Exterior Alterations
- 2. Landscaping
- 3. Hotel Details





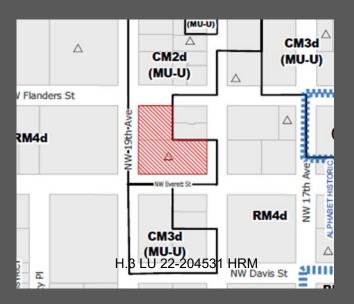
# **Modification 1: Building Height**

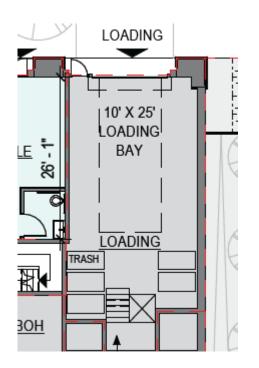
The Standard: A combination of code-allowed height exceptions plus bonuses results in an allowable height of 60-feet. (45' base height + 5' tall ground floor height exception + 10' affordable commercial height bonus)

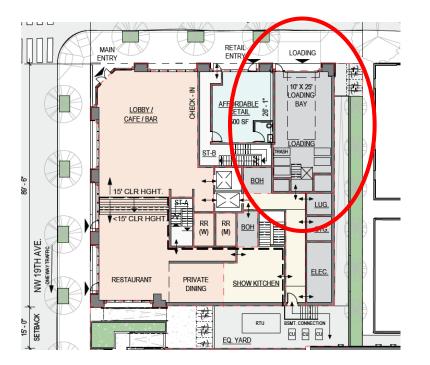
The Purpose: To control the overall scale of buildings. The height limits in the CM2 zone allows for a greater building height at a scale that can accommodate the growth intended for centers and corridors, while relating to the low- to mid-rise scale of neighborhood residential areas.

The Proposal: The applicant proposes a height of 70-feet to facilitate an occupiable rooftop penthouse level with rooftop seating.

- 1. Modification 1 Height
- 2. Modification 2
- 3. Modification 3
- 4. Modification 4







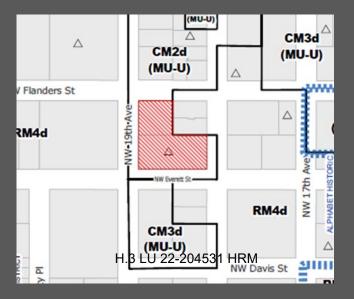
## **Modification 2: Loading Space Dimension**

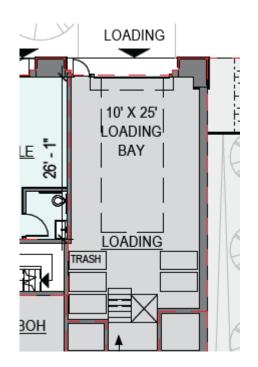
The Standard: Standard A: the loading space must be at least 35 feet long, 10 feet wide, and have a clearance of 13 feet.

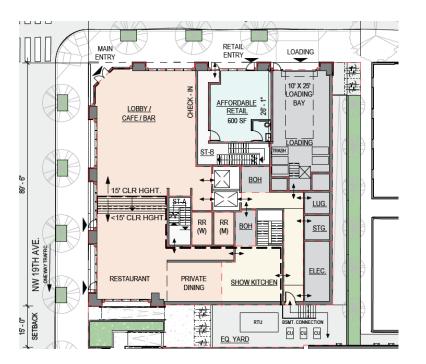
The Purpose: The regulations ensure that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

The Proposal: decrease the standard A loading space size from the required 35-feet long, 10-feet wide and 13-foot clearance to 25-feet long, 10-feet wide and 13-foot clearance.

- 1. Modification 1 Height
- 2. Modification 2 Loading Space Size
- 3. Modification 3
- 4. Modification 4







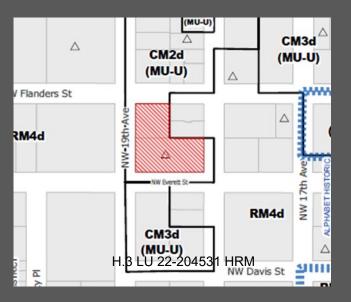
#### **Modification 3: Forward Motion**

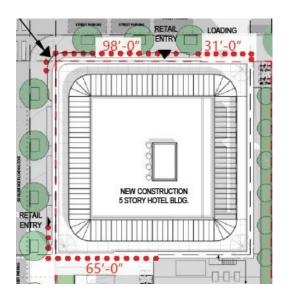
The Standard: Outside the Central City plan district, loading facilities generally must be designed so that vehicles enter and exit the site in a forward motion.

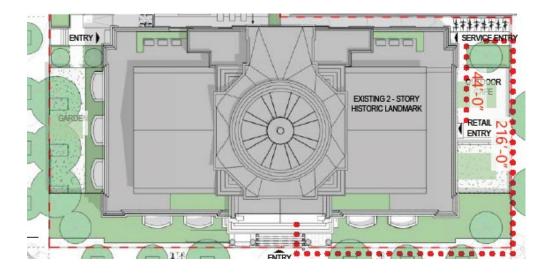
The Purpose: The regulations ensure that access to and from loading facilities will not have a negative effect on the traffic safety or other transportation functions of the abutting right-of-way.

The Proposal: Allow loading space to be accessed and exited in a rearward motion.

- 1. Modification 1 Height
- 2. Modification 2 Loading Space Size
- 3. Modification 3 Forward Motion
- 4. Modification 4







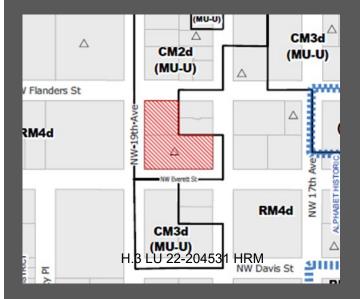
# **Modification 4: Short-Term Bike Parking**

The Standard: For a building with more than one main entrance, the bicycle parking must be along all façades with a main entrance, and within 50 feet of at least one main entrance on each façade that has a main entrance, as measured along the most direct pedestrian access route.

The Purpose: Short-term bicycle parking is located in publicly accessible, highly visible locations that serve the main entrance of a building. Short-term bicycle parking is visible to pedestrians and bicyclists on the street and is intended for building and site visitors.

The Proposal: Increase the maximum distance that short-term bike parking may be located from a main entrance, from 50 feet to 65 feet.

- 1. Modification 1 Height
- 2. Modification 2 Loading Space Size
- 3. Modification 3 Forward Motion
- 4. Modification 4 Short-Term Bike Parking Location



# Staff Recommendations H.3 LU 22-204531 HRM

# Staff recommends approval



#### **Type III Land Use Review**

#### **MEMORANDUM**

Date: March 3, 2023

To: Historic Landmarks Commission

From: Hannah Bryant, Design / Historic Review Team

503.865.6520 | Hannah.Bryant@portlandoregon.gov

Re: LU 23-204531 HRM – Landmark Restoration + New Hotel

Type III Design Review- March 13, 2023

Attached is a drawing set for the Type III Historic Resource review scheduled on March13, 2023. Please contact me with any questions or concerns.

#### I. PROGRAM OVERVIEW

The proposal includes two elements: First, the adaptive reuse of an existing three-story Historic Landmark to convert a former church into a private club with a hotel, restaurants, spa and bar. Second, the construction of a new, detached, six-story hotel with a ground floor restaurant and affordable commercial retail space on a vacant parcel within the Landmark boundary. One loading space is proposed in the new hotel. No parking is proposed. Four Modifications are requested. Modifications are requested to increase the building height, reduce the dimensions of the Standard A loading space, to increase the maximum distance between short-term bike parking and main entrances, and to allow loading to be entered/exited in a rearward motion.

#### II. DEVELOPMENT TEAM BIO

**Architect** Andrew Becker | Hartshorne Plunkard Architecture

Owner Tanya Toby | Gaia Ventures I, LLC

Project Valuation \$ 30 million

**III. HISTORIC REVIEW APPROVAL CRITERIA –** Community Design Guidelines + Alphabet Historic District Addendum to the Community Design Guidelines. See attached matrix.

#### **III. MODIFICATIONS**

Subject to the following approval criteria:

- A. The resulting development will meet the purpose of the standard being modified; or
- B. The preservation of the character of the historic resource is more important than meeting the purpose of the standard for which a modification has been requested.

Modifications requested:

1. To increase building height from 60-feet to 70-feet;

- 2. Reduce the length of the Standard A loading space;
- 3. Increase the distance between short-term bike parking and main entrances;
- 4. Allow loading to be entered/exited in a rearward motion.

#### IV. STAFF RECOMMENDATION

Staff cannot yet recommend approval due to a lack of material and building details. Once additional building details and cutsheets are provided, Staff had the following suggestions for Commission to consider:

Fencing around seating areas to the east and west of the Landmark. The landscaped setback around this building is a primary component of the setting that contributes to the Landmark's historic significance. Staff suggests that setting the fence in at least two feet from the sidewalk edge to allow a planting buffer between the sidewalk and the fence. The planting area should be deep enough to allow for irrigation and shrubs.

Staff anticipates receiving additional building details and cutsheets prior to the hearing. If additional information is received with enough time to determine approvability, Staff may revise the staff report. An updated staff report will be published to TRIM and an updated link will be emailed to all Commissioners.

#### V. CONDITIONS OF APPROVAL

The standard three conditions of approval have been added.

#### VI. PROCEDURAL NOTES

- The subject proposal was heard before at a voluntary Design Advice Requests (DAR) meeting, held on October 10, 2022 (Commissioners present: Kristen Minor, Matthew Roman, Peggy Moretti, Maya Foty, Kimberley Moreland, Andrew Smith). See the attached summary of Commission comments.
- The application was deemed complete on January 23, 2023.
- A first hearing was scheduled for March 13, 2023, 49 days after the applicant deemed the application complete.

Reference Materials: Efiles - LU 22-204531 HRM AD - 1918 NW Everett (23/EF/774) (portlandoregon.gov)

- Drawing Set dated March 13, 2023
- Guidelines Matrix
- Summary Notes from the DAR held on October 10, 2022 (EA 22-161762 DA)

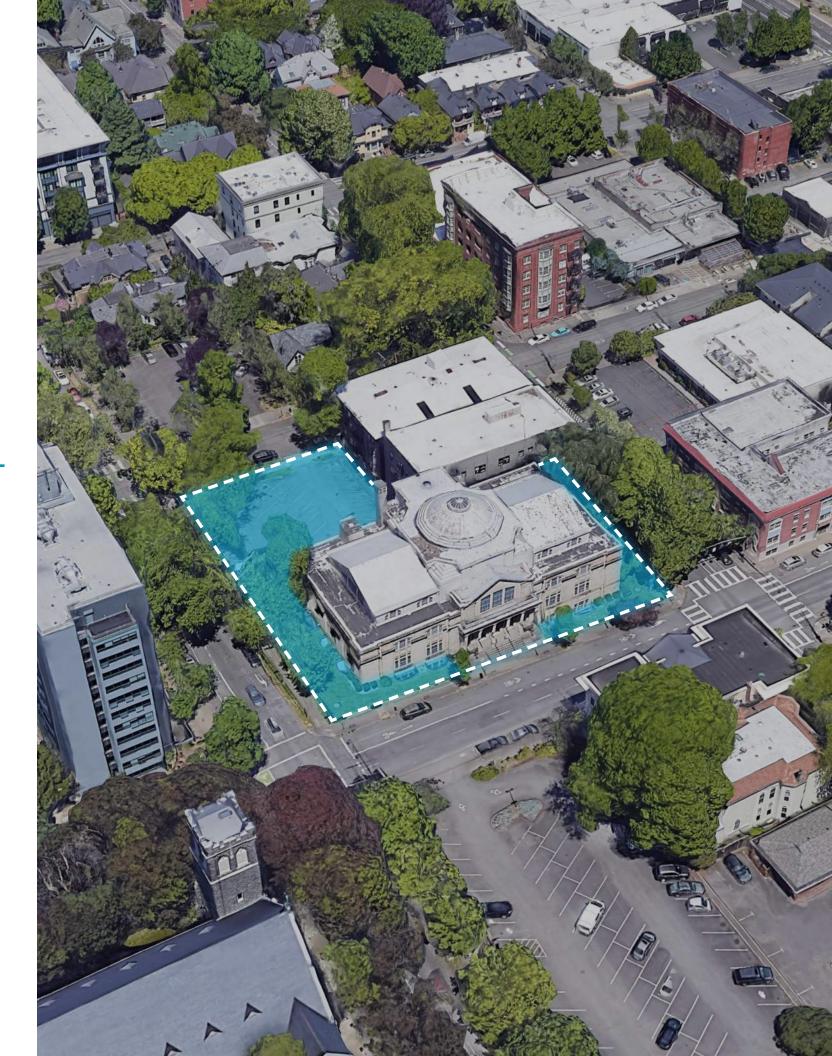
#### TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

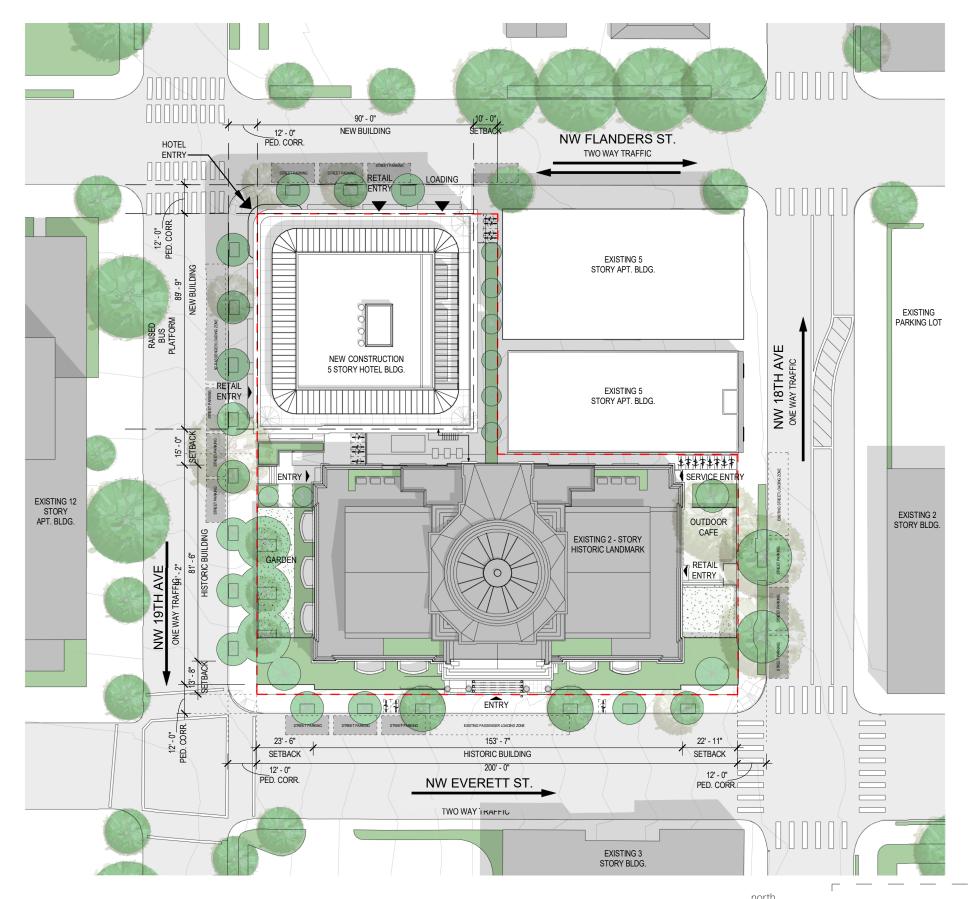
## 1819 NW EVERETT STREET

Historic Construction - Drawings

March 13th, 2023







#### **ENLARGED SITE PLAN**

| SCALE: 1" = 64'-0"









AREA FOR CITY STAMP

#### **MATERIAL KEY**

- 1 EXISTING STONE FACADE TO REMAIN, REPAIR AS REQ'D
- 2 EXISTING STEEL WINDOW WITH LEADED GLASS (SEE WINDOW SCOPE PAGE FOR DTLS)
- 3 EXISTING WOOD WINDOW FOR REPAIR AND PAINT
- 4 EXISTING METAL CLADDING TO REMAIN; REPAIR AND PAINT TO MATCH EXISTING
- 5 EXISTING COATED TIN ROOFING TO BE REPAIRED AND/OR REPLACED IN KIND
- BE 6 EXISTING WOOD DOOR FOR REPAIR AND STAIN
  - DR PAINT 12 NEW METAL DOOR FOR PAINT TO MATCH WALL

- 7 EXISTING SCUPPER AND DOWNSPOUT TO REMAIN FOR PAINT
- 8 NEW FABRIC AWNING IN EXISTING LOCATION
- 9 NEW FRAMELESS GLASS GAURDRAIL, CLEAR LOW-IRON GLASS
- 10 NEW STUCCO RAINSCREEN

NEW METAL COPING CAP FOR PAINT TO MATCH WALL

AREA FOR CITY STAMP

THRO PLOOR
EL +66-0'

THRO PLOOR
EL +27'-10'

SECOND ROOR
EL +13'-4'

FIRST FLOOR
EL +10'-0'

BASEMENT
EL -11'-3'

BASEMENT
EL -11'-3'

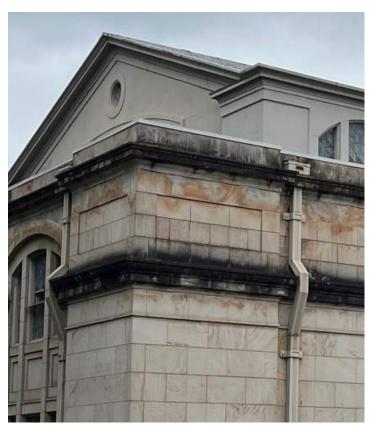
SOUTH ELEVATION

COLOR ELEVATIONS (1819 NW EVERETT ST.)

| SCALE: 1/16" = 1'-0"











NORTH WINDOW BAY/FACADE MATERIAL



EXISTING METAL SHEET CLADDING OVER HISTORIC CEMENTITIOUS FINISH



WINDOW CONDITION EXAMPLE



ENLARGED WINDOW DETAIL



EXISTING STONE TO REMAIN





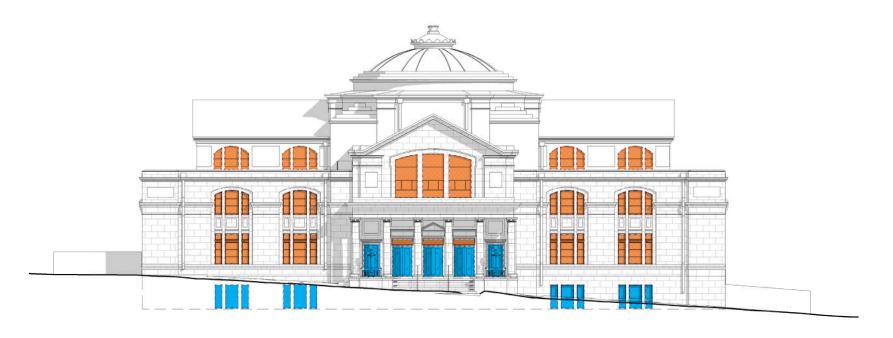
STAIR TOWER ADDITIONS - STUCCO AND METAL COPING

#### EXTERIOR MATERIALS (1819 NW EVERETT ST.)

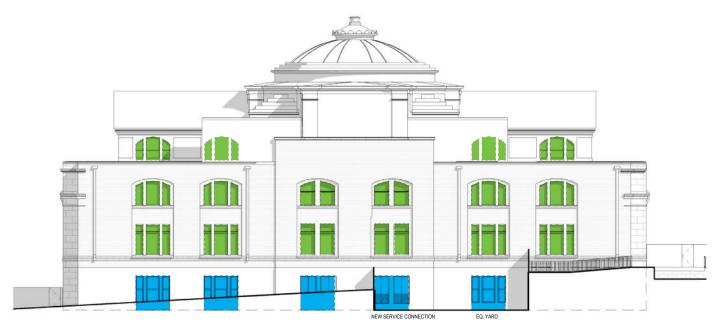








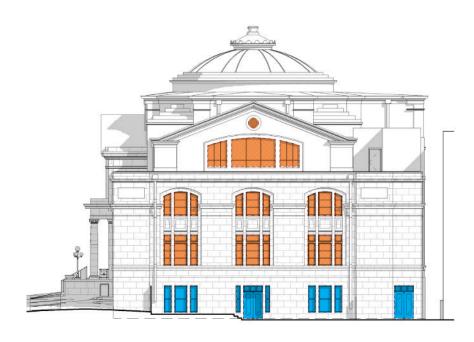
#### SOUTH ELEVATION



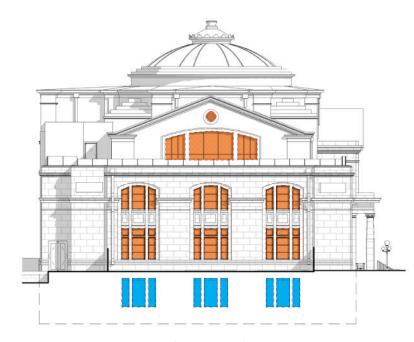
NORTH ELEVATION



EXISTING CLEAR GLASS AND WOOD FRAME TO BE REPAIRED AND/OR REPLACED IN KIND TO MATCH HISTORIC CONDITION



#### EAST ELEVATION



WEST ELEVATION

EXISTING GLASS TO BE REPLACED WITH SEMI-TRANSPARENT PRIVACY GLASS WITHOUT GEOMETRIC PATTERN. FRAME AND MULLIONS TO BE REPLACED WITH NEW IN HISTORIC CONFIGURATION

#### WINDOW REPAIR AND REPLACEMENT SCOPE (1819 NW EVERETT ST.)

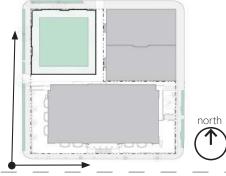








3D PERSPECTIVE FROM THE INTERSECTION OF NW EVERETT ST. AND NW 19TH AVE.











NW 18TH AVE

NW EVERETT ST

#### THIRD FLOOR PLAN (1819 NW EVERETT ST.)

NW 19TH AVE

| SCALE: 1" = 20-0"



**EXISTING NORTHEAST** CHIMNEY - PROPOSED TO BE REMOVED AND REPLACED WITH STAIR ADDITION



**EXISTING NORTHWEST** REMOVED AND REPLACED WITH STAIR ADDITION



EXISTING, NON-HISTORIC, EXISTING NORTHWEST

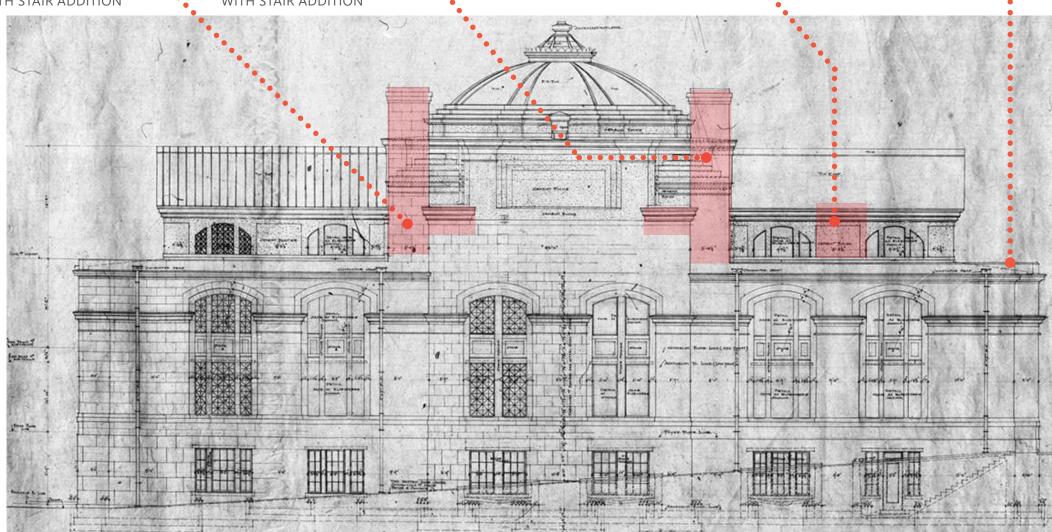
• EXISTING, NON-HISTORIC,

CHIMNEY - PROPOSED TO BE

• ELEVATOR OVERRUN TO BE DEMOLISHED



12" HIGH EXISTING PARAPET CONDITION



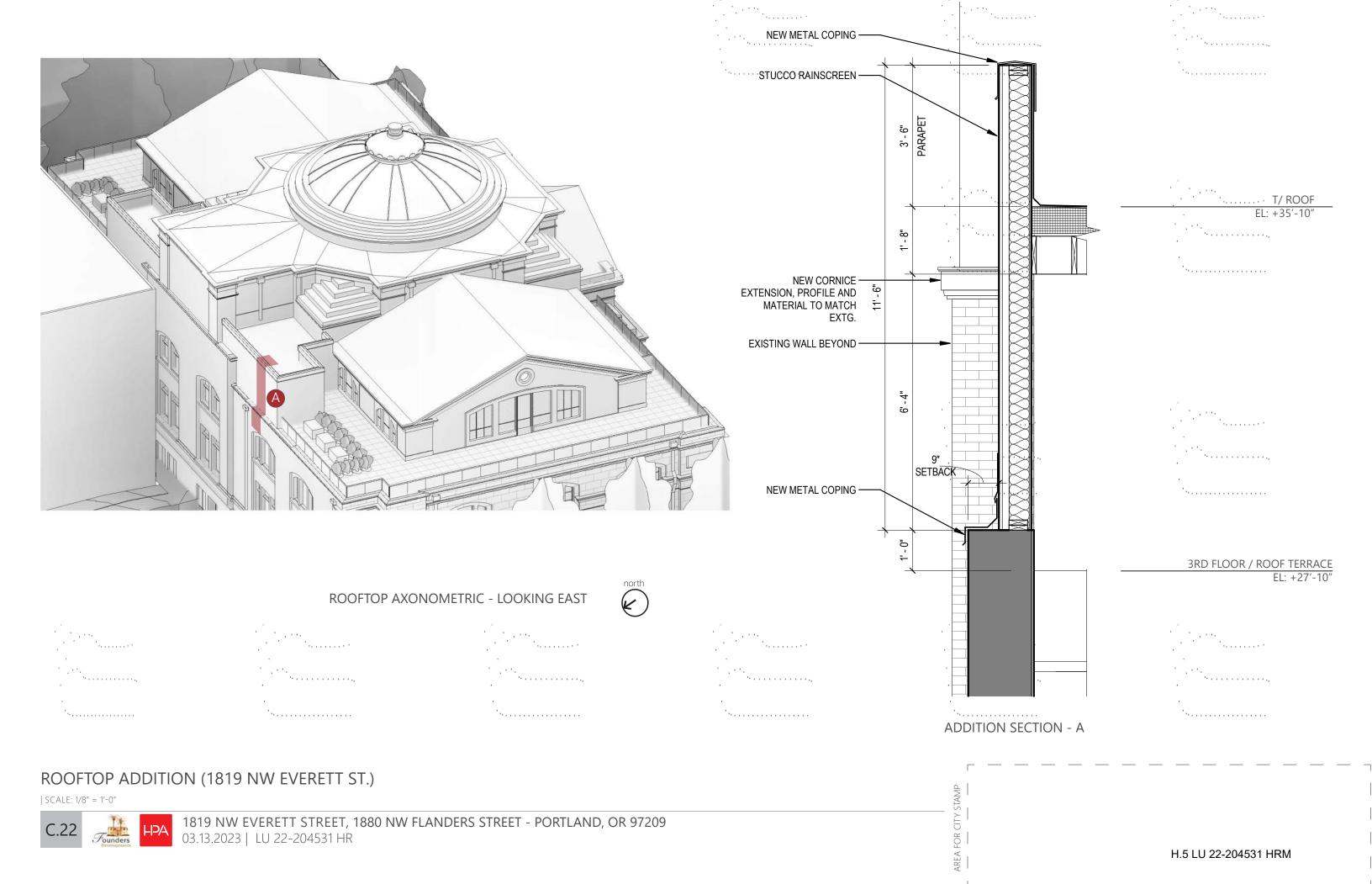
ORIGINAL NORTH ELEVATION DRAWING

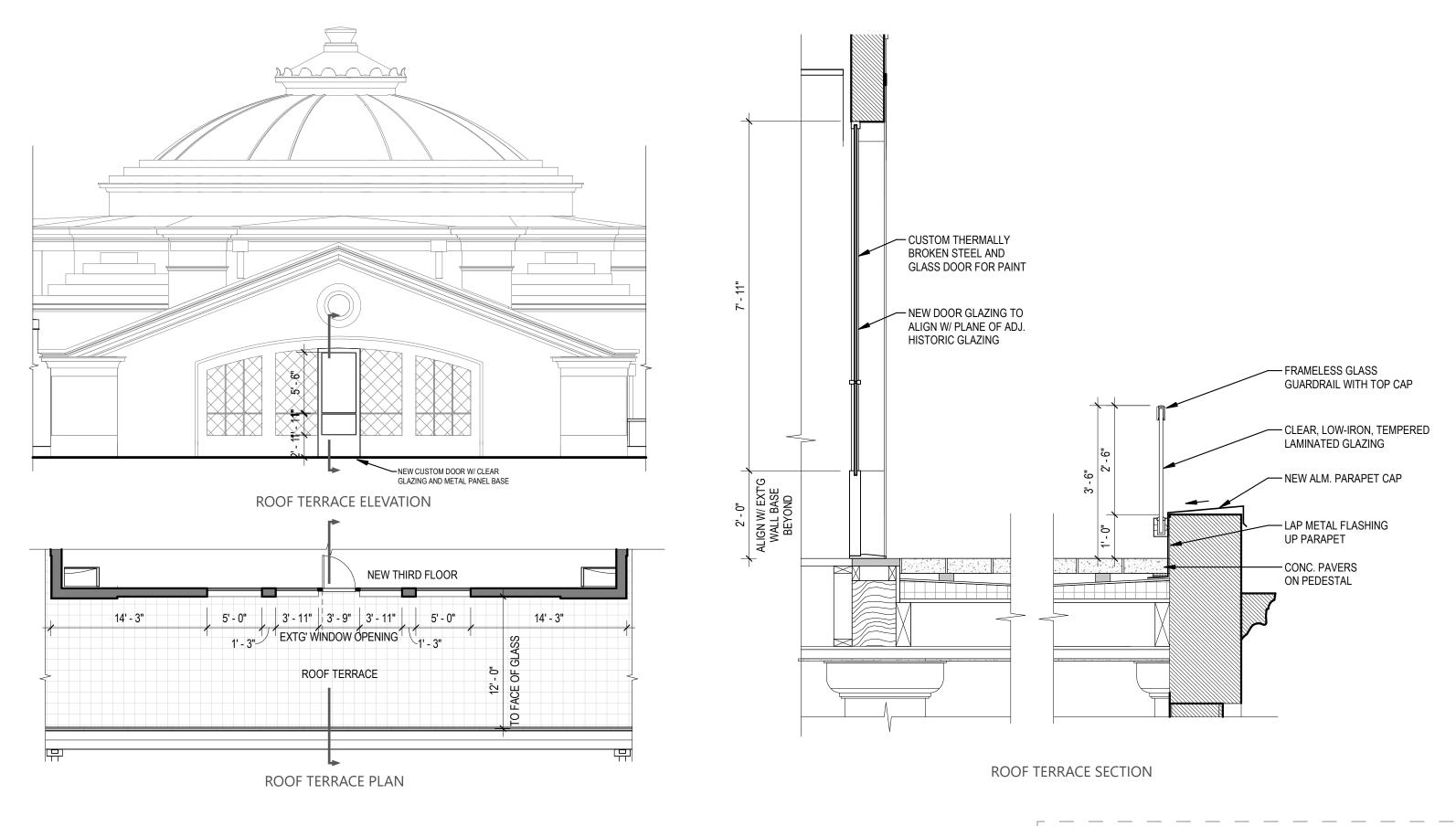
#### EXISTING CONDITIONS (1819 NW EVERETT ST.)











#### DETAILS (1819 NW EVERETT ST.)

| SCALE: 1/8" = 1'-0" & 1/2" = 1'-0"



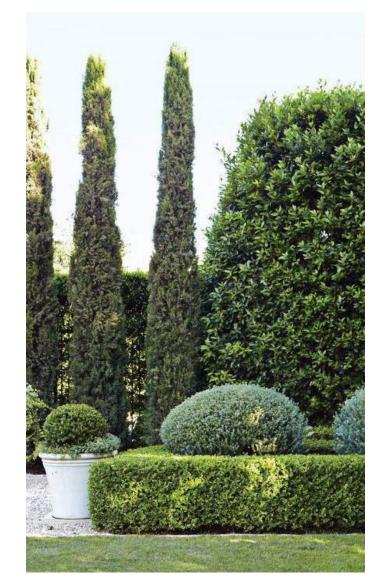








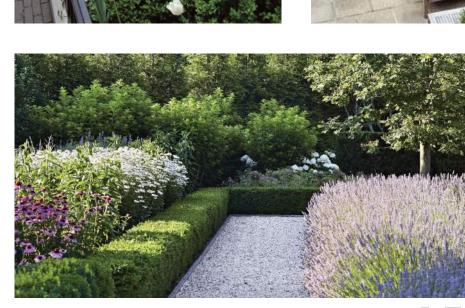
H.5 LU 22-204531 HRM



















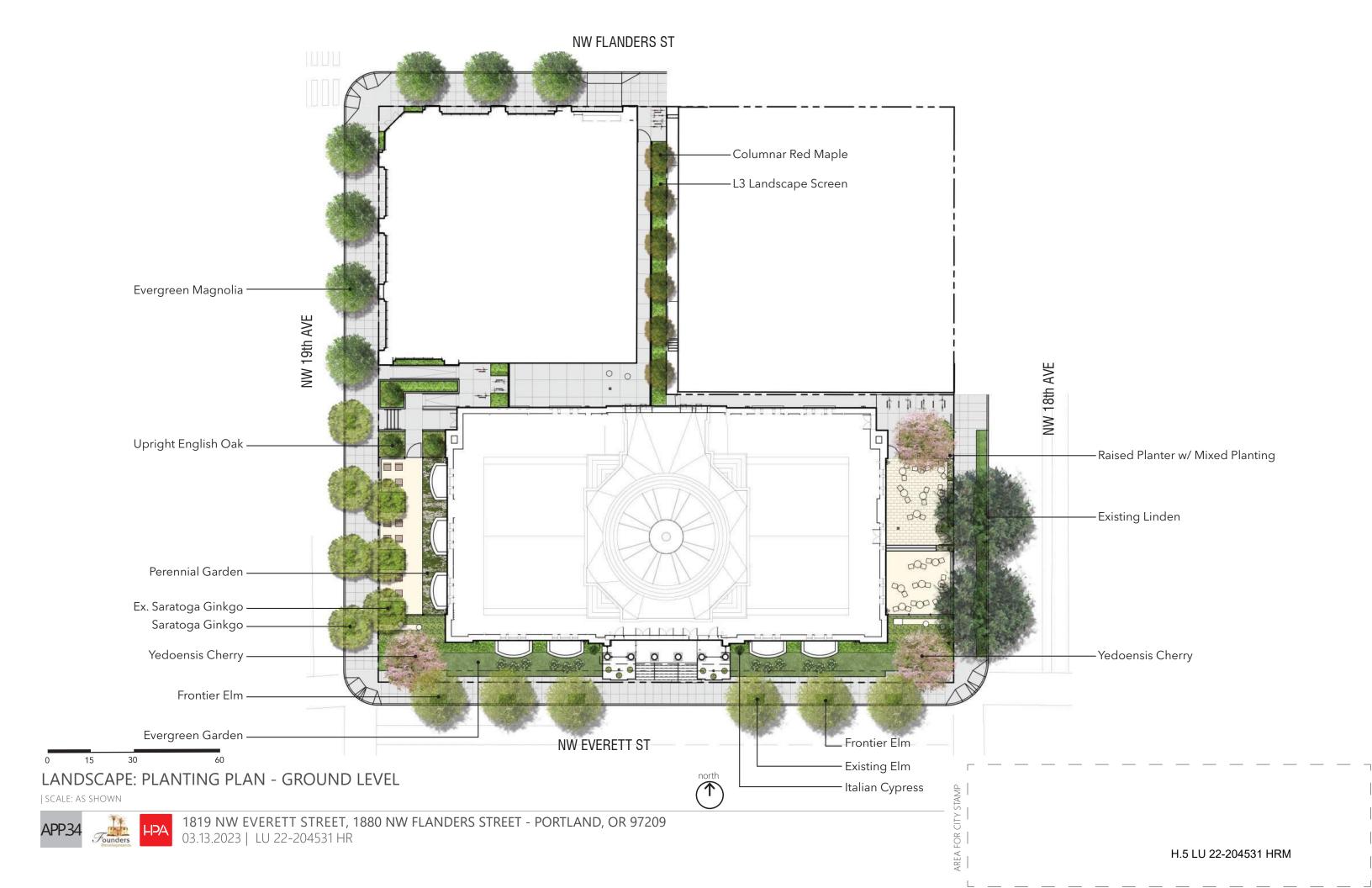


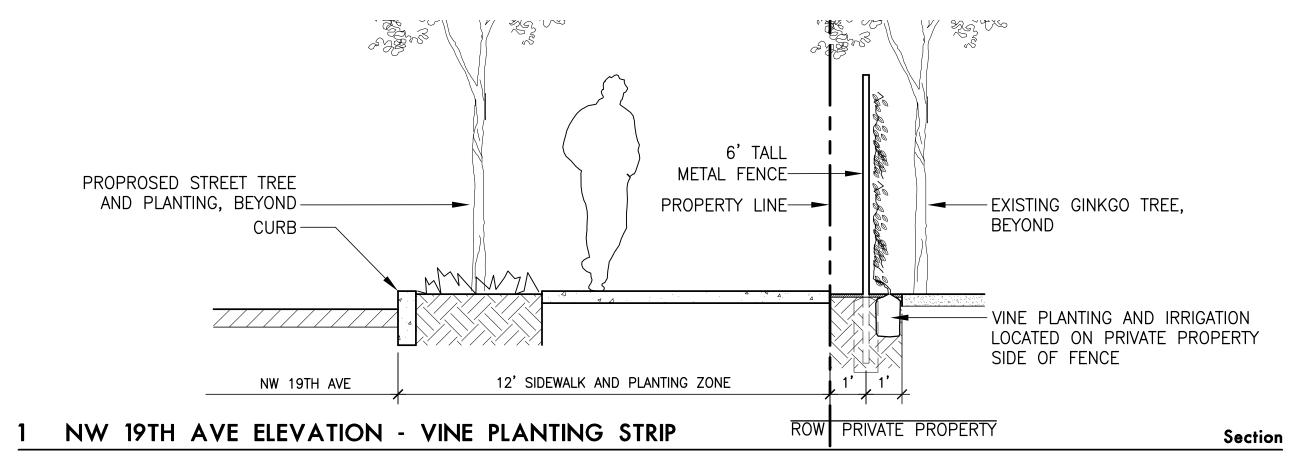
LANDSCAPE: PRECEDENT IMAGES

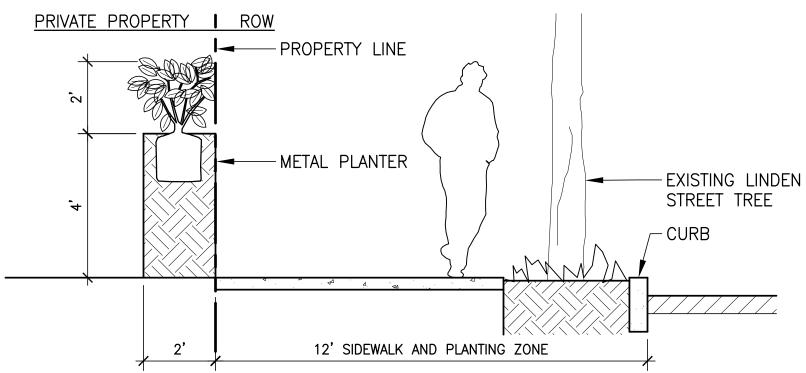












## 2 NW 18TH AVE ELEVATION - RAISED METAL PLANTER

Section

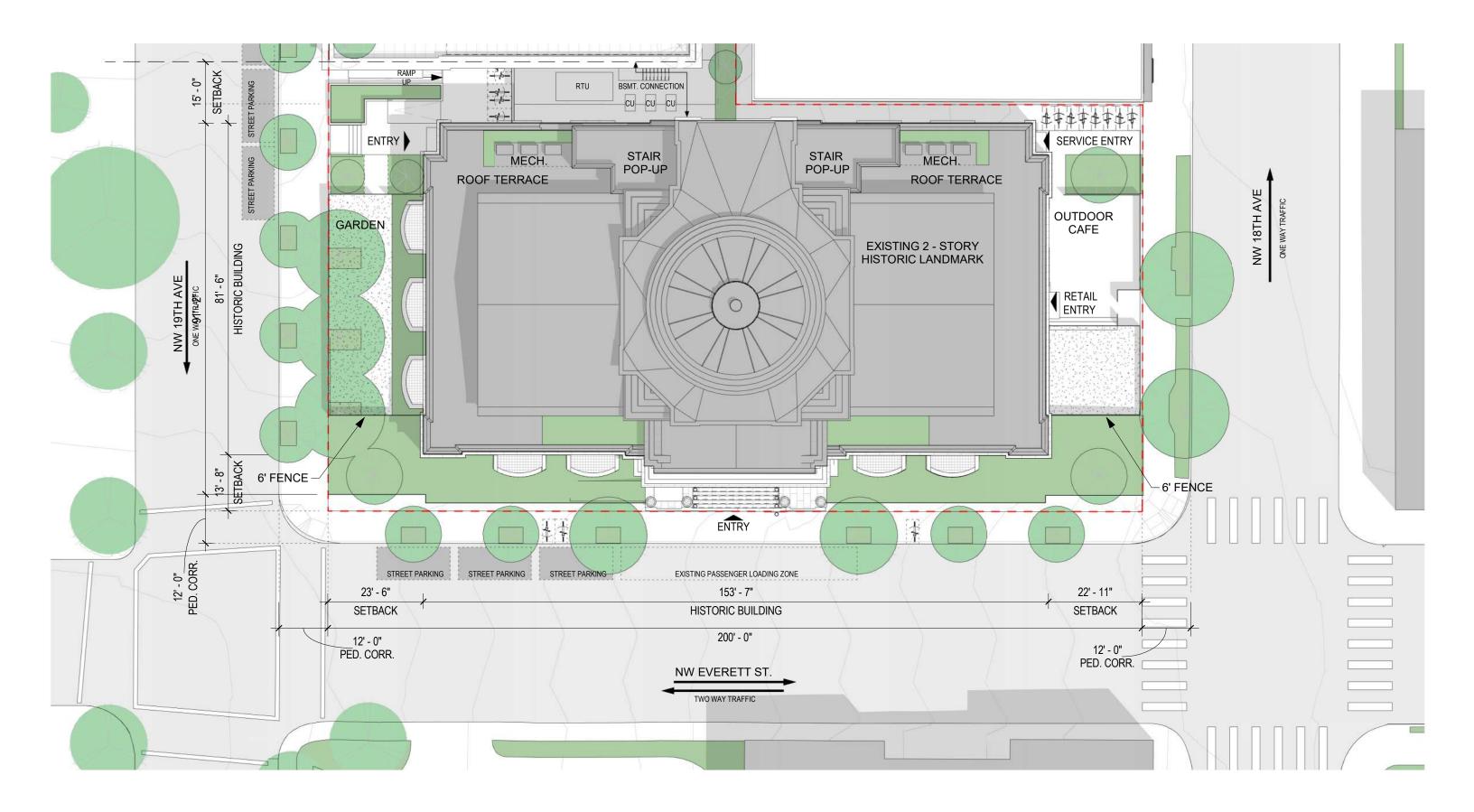
LANDSCAPE: PLANTING ELEVATIONS



| SCALE: 3/8" = 1'-0"







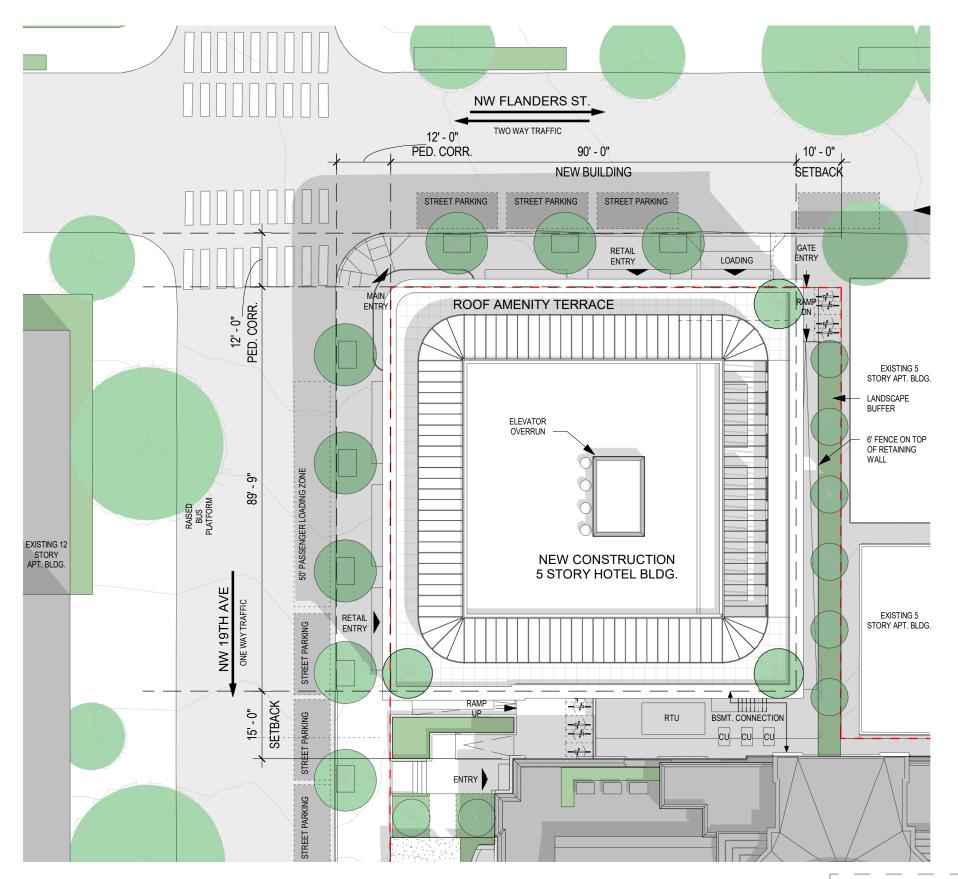
## ENLARGED SITE PLAN (1819 NW EVERETT ST.)

| SCALE: 1" = 64'-0"











03.13.2023 | LU 22-204531 HR

| SCALE: 1" = 64'-0"





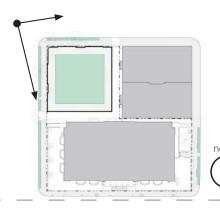


1819 NW EVERETT STREET, 1880 NW FLANDERS STREET - PORTLAND, OR 97209





3D PERSPECTIVE FROM THE INTERSECTION OF NW 19TH AVE. AND NW FLANDERS ST.



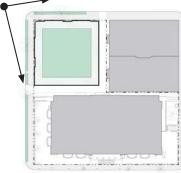
## 3D PERSPECTIVES (1880 NW FLANDERS ST.)













3D PERSPECTIVE FROM THE INTERSECTION OF NW 19TH AVE. AND NW FLANDERS ST.

## 3D PERSPECTIVES (1880 NW FLANDERS ST.)





#### MATERIAL KEY



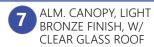
ALM. WINDOW, LIGHT BRONZE FINISH















10 ALM. COPING CAP, PAINT 11 ALM. DOOR, PAINT TO TO MATCH STUCCO MATCH WALL

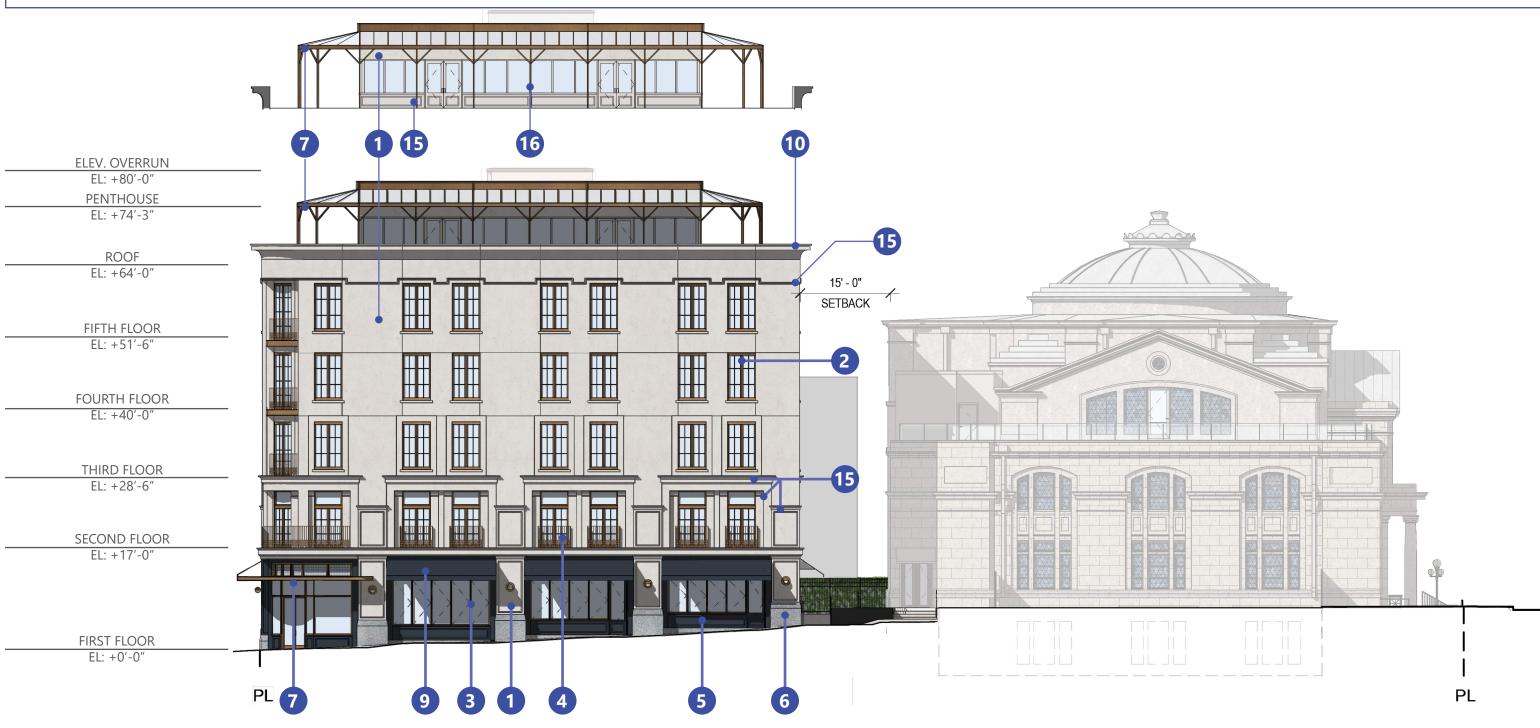
ALM. OVERHEAD COILING DOOR 4LM. UVEKTIEAD COILING DOCUMENT OF THE STATE TRANSLUCENT GLAZING PANELS

DECORATIVE ALM. PANEL MECH. SCREEN WALL, MATTE PAINT FINISH TO MATCH

CONCRETE W/ ELASTOMERIC 14 PAINT , COLOR TO MATCH STUCCO

DECORATIVE STUCCO MOULDING TRIM

16 ALM. STOREFRONT, PAINT TO MATCH STUCCO



**WEST ELEVATION** 

COLOR ELEVATIONS (1880 NW FLANDERS ST.)

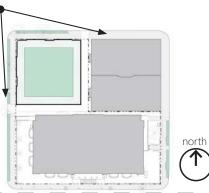
| SCALE: 1/16" = 1'-0"







3D PERSPECTIVE - AERIAL VIEW OF PENTHOUSE LOOKING SOUTHEAST





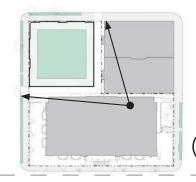








3D PERSPECTIVE - AERIAL VIEW OF PENTHOUSE LOOKING NORTHWEST





3D PERSPECTIVES (1880 NW FLANDERS ST.)







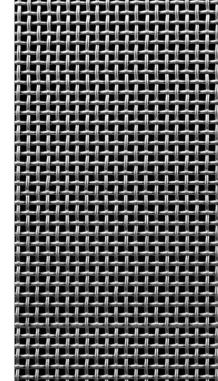




WOOD STOREFRONT SURROUND



CUSTOM RAILING EXAMPLE-LIGHT BRONZE FINISH



MESH SCREEN



EXISTING HISTORIC STONE FACADE



STUCCO RAINSCREEN, SMOOTH FINISH



**DECORATIVE GLASS** 



CHARCOAL GRANITE STONE VENER -FLAMED FINISH

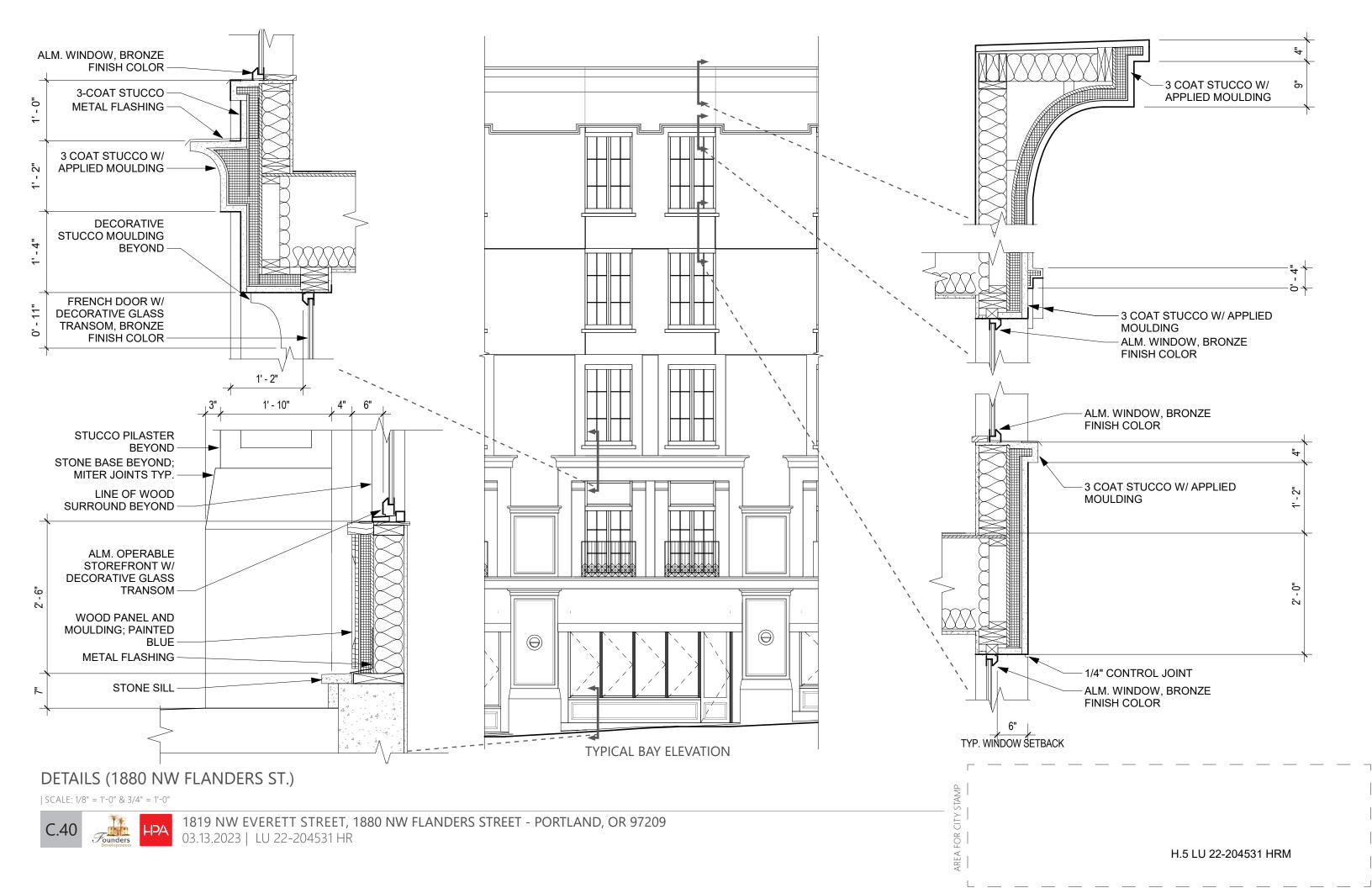


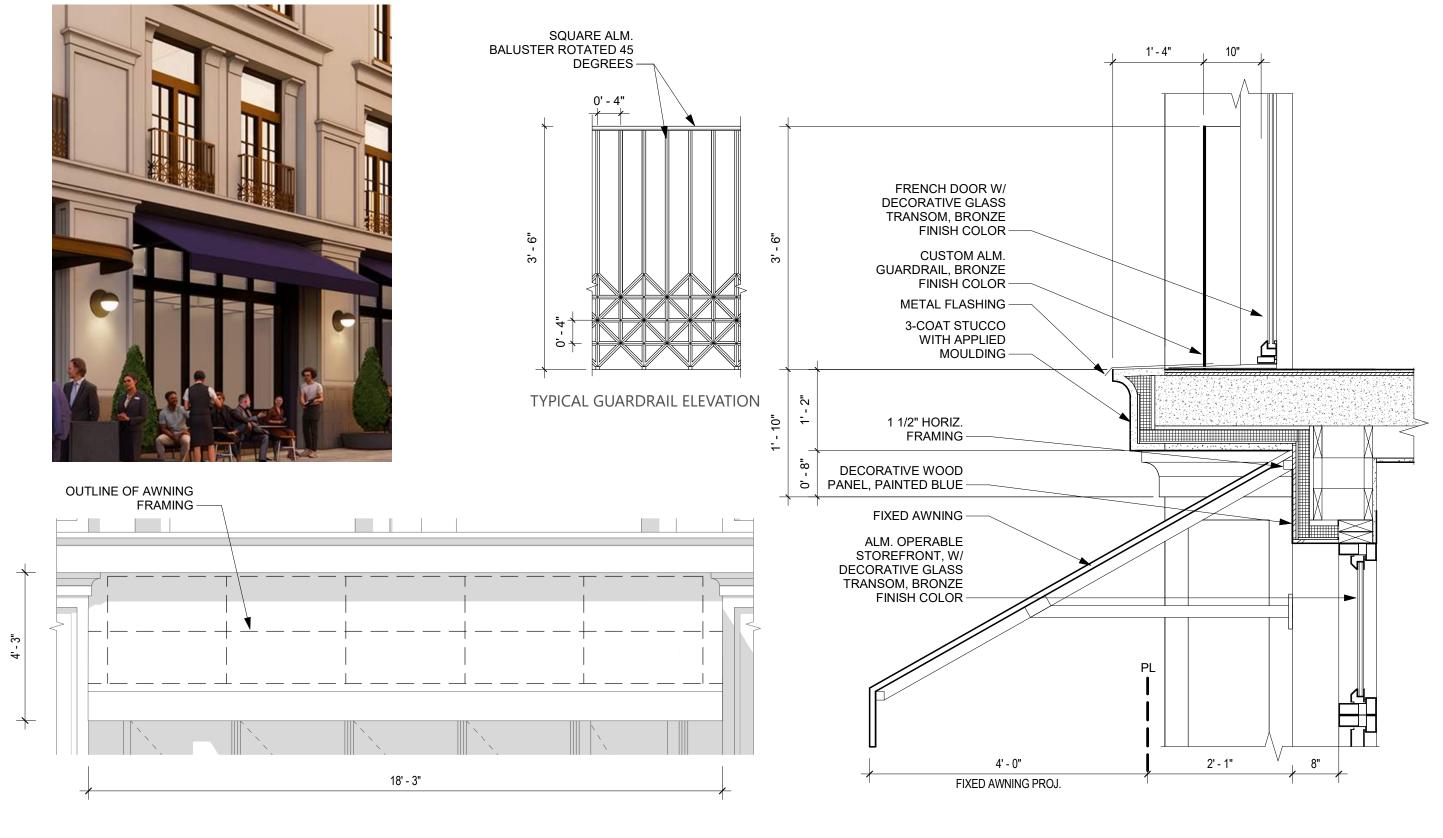
METAL - LIGHT BRONZE

## FACADE MATERIALS (1880 NW FLANDERS ST.)









TYPICAL AWNING ELEVATION FIXED AWNING SECTION

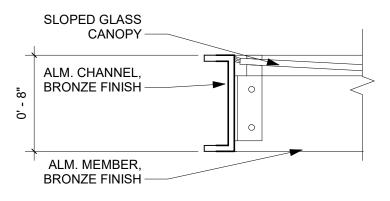
### DETAILS (1880 NW FLANDERS ST.)

| SCALE: 3/8" = 1'-0" & 3/4" = 1'-0"

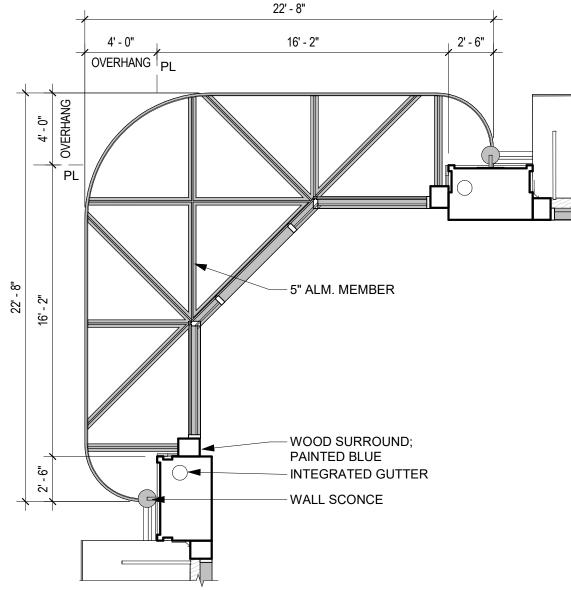




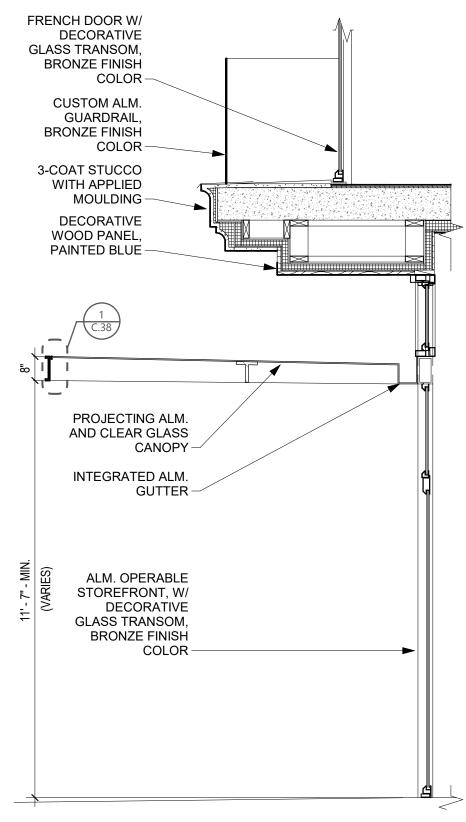




**CANOPY DETAIL SECTION 1** 



FIRST FLOOR CANOPY RCP



FIRST FLOOR CANOPY SECTION

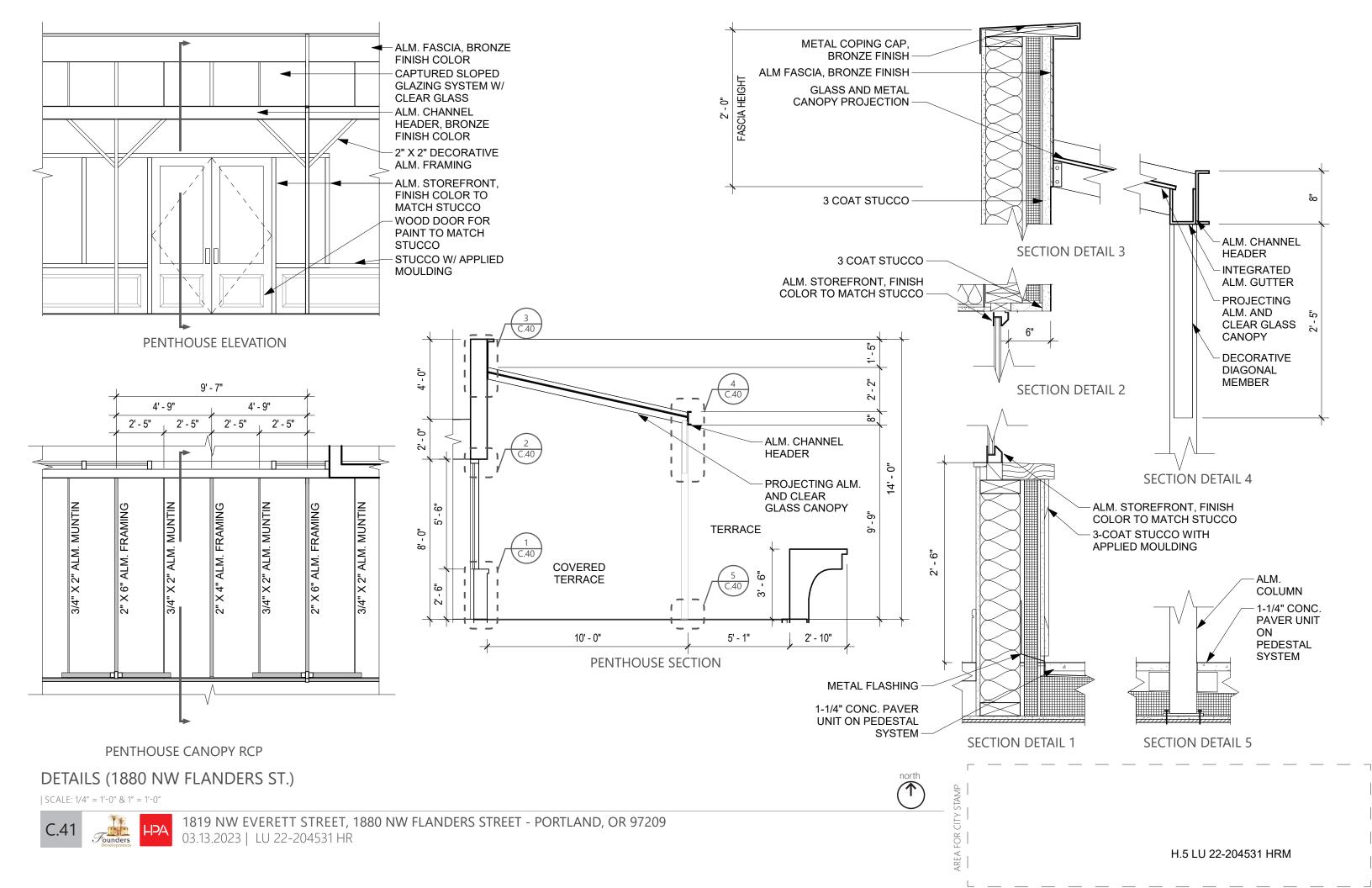
DETAILS (1880 NW FLANDERS ST.)

| SCALE: 3/16" = 1'-0", 3/8" = 1'-0", & 1-1/2" = 1'-0"









## TYPE III HISTORIC RESOURCE REVIEW SUBMITTAL

# 1819 NW EVERETT STREET 1880 NW FLANDERS STREET

Historic & New Construction Drawings

March 13th, 2023