



## **STAFF REPORT AND RECOMMENDATION TO THE DESIGN COMMISSION**

CASE FILE: LU 13-189059 DZM – Abigail Apartments  
Pre-App.: EA 12-180268 PC  
REVIEW BY: Design Commission  
WHEN: Thursday November 21, 2013 @ 1:30 pm  
WHERE: 1900 SW Fourth Ave., Room 2500A  
Portland, OR 97201

It is important to submit all evidence to the Design Commission. City Council will not accept additional evidence if there is an appeal of this proposal.

**Bureau of Development Services Staff:** Mark Walhood 503-823-7806 / mark.walhood@portlandoregon.gov

Throughout this staff report, outstanding areas of concern that must be addressed prior to a staff recommendation of approval are identified in areas of boxed text.

### **GENERAL INFORMATION**

**Applicant:** Isaac Johnson  
Ankrom Moison Associated Architects  
6720 SW Macadam Ave.  
Portland, OR 97219

**Purchaser/Operator:** Nicole Peterson  
Bridge Housing Corporation  
925 NW 19th, Studio B  
Portland, OR 97209

**Property Owner:** Hoyt Street Properties LLC  
1022 NW Marshall St., #709  
Portland, OR 97209-2989

**Site Address:** Northeast Corner of NW 13<sup>th</sup> & Raleigh Streets

**Legal Description:** BLOCK 2 TL 708, WATSONS ADD  
**Tax Account No.:** R883800160  
**State ID No.:** 1N1E28DD 00708  
**Quarter Section:** 2828

**Neighborhood:** Pearl District, contact Patricia Gardner at 503-243-2628.  
**Business District:** Pearl District Business Association, contact Adele Nofield at 503-223-0070.  
**District Coalition:** Neighbors West/Northwest, contact Mark Sieber at 503-823-4212.

**Zoning:** **EXd** (Central Employment base zone with Design overlay zone),  
**Central City plan district/River District Subdistrict/North Pearl Subarea**

**Case Type:** **DZM** (Design Review with Modifications)  
**Procedure:** Type III, with a public hearing before the Design Commission.  
 The decision of the Design Commission can be appealed to City Council.

**Proposal:** The applicant is proposing the development of a six-story apartment building on a block-sized portion of a larger site at the north end of the Pearl District. The site boundary includes a south edge in alignment with NW Raleigh Street, a west edge in alignment with NW 13<sup>th</sup> Avenue, and a northeast diagonal edge abutting the rail corridor between the property and NW Front Avenue. The proposal includes 143 apartments, ground floor lobby and tenant community room, and a small retail café. The building is arranged with an east and west wing, connection through an enclosed pedestrian bridge. The plan of the building creates a series of outdoor courtyards on the site south of the bridge, north of the bridge, and east of the angled east wing. The main entry lobby and corner retail/café space have doors oriented to NW 13<sup>th</sup> Avenue, with a secondary entry door at the bridge from the south courtyard, and three units having direct entries to the south in alignment with Raleigh Street. A basement parking level, with access from NW 13<sup>th</sup> Avenue at the northwest corner of the building, accommodates one loading stall, 81 cars and 238 long-term bike parking spaces.

The exterior design and materials of the buildings are similar, but differentiated in color and details between the east and west wings. The west wing has buff colored brick and inset earth-toned vertical metal panel materials, and the east wing has dark gray brick with light gray vertical metal panel materials. The connecting bridge element has metal composite panel skin and clear and colored window elements. Upper story windows are vinyl, with the exception of some fiberglass windows on the bridge, and ground floor windows are typically metal storefront systems on the west wing, and vinyl/residential windows on the east wing.

The applicant proposes to dedicate the full 60' public right-of-way of NW 13<sup>th</sup> Avenue to the City on the west edge of the site between NW Raleigh and Xavier Streets. Improvements in this dedicated right-of-way would include a raised concrete dock along the entire frontage with stairs near the main lobby entry, and a ramp at the south end. The applicant proposes to construct a temporary 36'-wide asphalt roadway in NW 13<sup>th</sup> Avenue, without sidewalks on the west side of the roadway. A gravel turnaround would be provided to allow trucks to turn around just north of the improved section of NW 13<sup>th</sup> Avenue, until future projects north and west of the site are developed. In alignment with NW Raleigh Street on the south edge of the site, the applicant proposes a 26'-wide fire access and utility easement with a 6'-wide concrete sidewalk and curb, and a 20'-wide asphalt roadway with stormwater planter.

The Design overlay zoning and site location require the project to receive approval through a Type III Design Review process. Concurrently, the applicant has requested three Modifications through Design Review to several development standards. These include the following:

- Perpendicular parking stalls must be 8'-6" wide (33.266.130.F.2). Nine of the 81 parking spaces have structural columns that reduce the width of portions of a space near the column to a range of from 7'-8" to 8'-5";

- Bike parking stalls must be at least 2'-0" wide (33.266.220.C.3). ~~100~~ ~~208~~ of the long-term bike parking spaces are provided in vertical racks that stagger and put individual bikes in a 'stall' that is only 1'-6" wide; and
- The required loading bay must be 35' long, 10' wide, and have 13' clearance. As proposed, the basement loading stall is only 21'-6" long, 9'-0" wide and with 8'-2" clearance.

**Approval Criteria:** In order to be approved, this proposal must comply with the approval criteria of Title 33, Portland Zoning Code. The applicable approval criteria are:

- The *Central City Fundamental Design Guidelines*; and
- The *River District Design Guidelines*.

## ANALYSIS

**Site and Vicinity:** The applicant has proposed development on a small section of a large vacant site in the former railyards at the north edge of the River District. Created through Lot Confirmations and Property Line Adjustments, the lot lines align with the surrounding grid on the west (east side of NW 13<sup>th</sup>), but slightly further into the Raleigh (10'-0"), 12<sup>th</sup> (5'-0") and Savier (5'-0") alignments when compared to surrounding blocks to the west and south. At the northeast edge the property angles where abutting the adjacent rail corridor between the site and NW Front Avenue beyond. The proposed site area is approximately 39,909 square feet, slightly under the 40,000 square feet of a typical city block.

The property is currently vacant, although portions of the adjacent property are being used as construction-related storage and fencing areas. The recently-constructed Ramona Apartments with integral school and community center is directly across the site on the diagonal to the southwest. One block directly to the south, between NW Overton and Pettygrove, the Parker Apartments are in the final stages of construction. One block southeast of the site is the northernmost tip of the new Fields Park. To the west of the site, along NW 14<sup>th</sup> Avenue, a cluster of smaller one- and two-story warehouse buildings hold office space, small retail, and vehicle repair uses.

The abutting intersection of NW 13<sup>th</sup> and Raleigh Streets is improved with asphalt surfacing. The adjacent Ramona Apartments to the southwest feature a raised dock along NW 13<sup>th</sup> Avenue, a concrete roadway in NW 13<sup>th</sup> abutting the Ramona north to the south edge of Raleigh Street, and new public sidewalks and street trees along the south side of Raleigh Street west of 13<sup>th</sup>. The north side of Raleigh west of the site has a curb-tight concrete sidewalk but no street trees. The east side of NW 13<sup>th</sup> in the block south of the site is unimproved.

The abutting intersection of NW 13<sup>th</sup> and Raleigh has only Local Service designations for the bike, pedestrian, traffic, and transit designations in Portland's Transportation System Plan, although the entire site is within the Northwest Triangle Pedestrian District.

**Zoning:** The Central Employment (EX) base zone allows mixed-uses and is intended for areas in the center of the City that have predominantly industrial-type development. The intent of the zone is to allow industrial and commercial uses which need a central location. Residential uses are allowed, but are not intended to predominate or set development standards for other uses in the area.

The Design Overlay Zone [d] promotes the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. This is

achieved through the creation of design districts and applying the Design Overlay Zone as part of community planning projects, development of design guidelines for each district, and by requiring design review. In addition, design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area.

The Central City plan district provides a set of special use regulations and development standards that address circumstances unique to the downtown area, and in conformance with various adopted plans including the downtown plan, the Central City Plan, and transportation-related plans. This site is within the North Pearl Subarea of the River District of the Central City Plan.

**Land Use History:** City records indicate that prior land use reviews include the following:

- *LUR 92-00798 MS LA:* Approved Late Acceptance of a Central City Master Plan and Zone Change to approve development of the Hoyt Street Yards area, and changing the zoning on the site from IG1 to EXd, subject to multiple conditions of approval. All but one of the conditions of approval have already been satisfied or no longer apply, with one exception at condition E. Condition E notes that ‘Possible additional east-west and north-south pedestrian/bicycle paths may be considered and required by design review if they would complement development/renovation of areas to the east and west of this site. If the the Avenue connection to Front Avenue is not made, a pedestrian connection shall be made to Front Avenue in the general vicinity, provided P.U.C. and Office of Transportation approval can be obtained.’ **STAFF NOTE:** This condition has been addressed by the applicant by providing a diagram of likely future pedestrian connections through the remainder of the larger property from which this site was created, including west along Savier to NW 14<sup>th</sup>, and east along Raleigh with connections south to both NW 12<sup>th</sup> and 11<sup>th</sup> Avenues. Because the applicant is avoiding development immediately east of the site in alignment with the vacated NW 12<sup>th</sup> Avenue right-of-way where the 1992 Master Plan proposed a crossing over NW Front, we are not addressing the requirement for an alternate crossing location at this time. Future development with improvements in the area of the vacated NW 12<sup>th</sup> Avenue east of the site that does not include a pedestrian crossing over Front Avenue will trigger a Central City Master Plan Amendment to consider this issue.
- *LUR 99-00541 MS:* Master Plan Amendment that was withdrawn.
- *LUR 99-00542 SU MS:* Preliminary Plat approval for a 43-lot, five phase subdivision with amendments to the Central City Master Plan (LUR 92-00798 MS LA). This site was identified as Phase 5 of the subdivision, which is the only phase that has not yet received final plat approval. The applicant is pursuing the division of the remainder of the site through Lot Confirmations and Property Line Adjustments instead of pursuing final plat approval for Phase 5.

**Agency Review:** A “Notice of proposal in Your Neighborhood” was mailed **October 31, 2013**. The following Bureaus have responded:

- The *Fire Bureau* has reviewed the proposal and responded with standard comments noting that all provisions from the 2007 Oregon Fire Code must be met, and that applicable Fire Code provisions will be applied and evaluated during the mandatory building permit review process. No specific objections or concerns have been raised regarding the requested Design Review and Modifications. Exhibit E.1 contains staff contact and additional information.

- The *Life Safety Section of the Bureau of Development Services* has reviewed the proposal and responded with standard comments noting that the proposal must comply with all applicable building codes and ordinances. A separate building permit is required for the project, and a preliminary meeting to explore code-related issues is advised. No specific objections or concerns have been raised regarding the requested Design Review and Modifications.
- The *Urban Forestry Division of Portland Parks and Recreation* has reviewed the proposal and responded without comments, objections or concerns. Exhibit E.3 is a hard copy print-out of their electronic 'no concerns' response.
- The *Water Bureau* has reviewed the proposal and has no objections to the requested Design Review and Modifications, but does have comments to be included in regards to future permitting and water services. The Water Bureau finds that there is no water available to the site and currently no City rights-of-way to provide water service to this location. After the extension of NW 13<sup>th</sup> Avenue is dedicated as a public street, a water main extension will be required. Payment in full to the Water Bureau for this extension must happen prior to release of any building permits for the project. The specifics of installation details, permitting, timing, and other technical issues are explored further in the memo provided by the Water Bureau. The applicant is encouraged to contact the Water Bureau for a water system logistics and information meeting in order to ensure that all planning time frames for this site's water system development can be met. Exhibit E.4 contains staff contact and additional information.

- The *Bureau of Environmental Services* has reviewed the proposal and recommends that two specific elements of the stormwater management plan be clarified. To address the 4,645 square feet of unmanaged courtyard (non-roof) paving on the site, the applicant should either gain the required Special Circumstances request or revise the plans to show how the courtyard surfaces are being managed per standards. Also, the stormwater swale serving the Raleigh driveway appears to be undersized, so the site plans should be corrected or this issue should otherwise be acknowledged in the final decision if changes are not made. The BES staff recommends that these issues be addressed prior to final Design Review approval.

The remainder of the BES response identifies technical requirements related to the necessary Public Sewer Extension that will be required through the Public Works Permitting process, information about an abandoned sewer in NW Quimby just to the south of the site, and general stormwater management and permitting details. Exhibit E.5 contains staff contact and additional information.

- The *Site Development Section of the Bureau of Development Services* has reviewed the proposal and provided technical information on geotechnical, floodplain, stormwater discharge and treatment, and erosion control issues. A geotechnical report will be required at the time of permitting, including recommendations for temporary excavation support adjacent to private property, the railroad and the right-of-way. It may be determined that additional geotechnical information is required following Site Development review of the building permit drawings. THE project is partially located within the Potential Flood Hazard Area and is subject to the applicable requirements of Title 24.50, including a standard that the structure has a lowest floor, including basements, elevated to the flood protection elevation of 32 feet City of Portland datum. The exhibits appear to

show the garage ramp at the proper elevation, with all residential use spaces at or above the flood protection elevation. Additional flood-related technical requirements and the erosion control regulations of Title 10 will apply during permitting. Site Development has no objections or requested conditions of approval related to this design review. Exhibit E.6 contains staff contact and additional information.

- The *Development Review Section of Portland Transportation (PBOT)* has reviewed the proposal for conformance with applicable policies, street designations, Titles 17 and 33, and for potential overall impacts on the transportation system. The pedestrian connections (in NW Raleigh) will be required to have a public access easement placed over them as a condition of building permit approval. The applicant will also be required to dedicate the extension of NW 13<sup>th</sup> Avenue in a 60-foot wide right-of-way as a condition of building permit approval. The PBOT will allow NW 13<sup>th</sup> to be constructed with a temporary asphalt roadway instead of the concrete. Hoyt Street Properties will be required to provide a development agreement that obligates them to replace the asphalt with concrete as a condition of approval for the future development on the west side of NW 13<sup>th</sup> Avenue. The development agreement will be a condition of building permit approval for this application. A temporary on-site turnaround at the northern end of NW 13<sup>th</sup> must be provided until NW 13<sup>th</sup> connects to NW Savier in the future. The initial PBOT letter noted no objection to approval, subject to the following advisory condition: “As a condition of building permit approval the applicant shall be required to dedicate the 60-feet of right-of-way for the extension of NW 13<sup>th</sup> Avenue and provide a developer’s agreement to replace the temporary asphalt paving with concrete when the site on the west side of the extended NW 13<sup>th</sup> redevelops. NOTE: Additional conditions of building permit approval include public access easements and financial guarantees for the public works permit for NW 13<sup>th</sup>.” Exhibit E.7 contains staff contact and additional information.”

Unfortunately, the initial PBOT response did not specifically respond to the three transportation-related Modifications. Generally we rely on PBOT staff to approve all transportation-related Adjustments and Modifications, so a supplemental response will be requested for the hearing.

**Neighborhood Review:** A Notice of Proposal in Your Neighborhood was mailed on **October 31, 2013**. One written response has been received. A resident wrote a letter with several questions about the long-term/basement level bike parking, including specific comments on the floor rack design and spacing. This letter also made the specific comment that many people do not have enough strength to lift their bikes up to use wall racks, and argued against replacing floor racks with wall racks, suggesting that more than 20% of the long-term bike parking should be floor racks.

**STAFF NOTE:** Staff clarified the number of required long-term spaces for the project (217), and discrepancies in the graphic device used to show individual racks. In response to the concern about wall rack accessibility, the applicant modified their basement bike parking proposal to increase the total number of long-term bike parking stalls (238 instead of 232), and to reduce the number of wall racks (180 versus 208). The original e-mail letter and staff response are included in the case file as Exhibit F.1

## DESIGN ADVICE REQUEST COMMENTS

This memo summarizes Design Commission design direction provided at the February 21, 2013 DAR. Overall, Design Commissioners were excited to see this project on such a compelling site, and find the parti compelling.

### Responsiveness to the Site

- The courtyard locations and design warrants further consideration:
  - North courtyard may not be best spot for children’s play area given lack of sun exposure for much of the year;
  - East courtyard is isolated and needs better integration for users of the building;
  - Noise should be considered. Given the adjacent rail corridor you don’t want to create north- and east-facing ‘noise traps’; and
  - Overall, light and sunlight access should be considered carefully with regards to the courtyard spaces.
- A larger south-facing courtyard with the bridge element moved further north might create a more successful courtyard space.
- Altering the shape of the east wing and ‘flipping’ the angled mass to step back further from the internal courtyards and hug the eastern edge of the site might strengthen the complementary/antagonistic relationship between the two wings.
- The parti should forge sophisticated relations between the two wings or buildings. Playful is an adjective that could be used for the concept drawings, but move as much as you can towards sophistication as well (cheerios AND rocket science). Look for an intellectual as well as color/material connection between the two buildings in light of your parti.
- A more intentional, identifiable pedestrian entry for the east building off the south courtyard could be helpful.
- Pedestrian entries are important. Look at loading dock entries elsewhere on NW 13<sup>th</sup> closely.
- The Fremont Bridge itself and views to the bridge should be integral to the project, including views from adjacent streets, through the buildings, and looking north in alignment with NW 13<sup>th</sup> and 12<sup>th</sup> Avenues.
- Rail traffic and noise is not necessarily a bad thing to be designed away. Celebrating the adjacent rail corridor versus putting it to the ‘back’ of the project should be considered.
- The raised platform and docks on the west and south sides are successful, make sure pedestrian access stairs, etc. are functional and direct.
- The hard edge or continuous wall along NW 13<sup>th</sup> Avenue is successful, other projects with courtyards off this street do not support the street character as well.
- Commissioners were split on the two wings versus two buildings discussion.

### Building Massing & ‘Bridge’ Connection

- The two building idea is interesting, and the idea of two separate identities is intriguing. This could also be achieved through a distinction in building height between the two wings, or segregating the units by type in each building, one building could be narrower and the other wider, etc.
- Make sure that the bridge is a necessary, integral part of the project that makes the whole stronger. If two wings with a connecting bridge is the best solution considering the potential impact on the daily experience of the future residents then pursue the bridge. Otherwise, look at two separate buildings or a more low-key connector. Consider impacts to views out from the units, what the

visual experience is from inside the bridge, how does the bridge function with families and other users, etc.

- The bridge needs to be used to strengthen the parti and antagonistic & complementary opposites. Push a little harder on the original design concept for the parti keeping this in mind. Material hardness vs. softness and the bridge relationship between the two could be elaborated upon.
- Look carefully at the math of doing two distinct buildings versus the bridge connection. The site offers a fantastic opportunity to frame views of the Fremont Bridge with two separate buildings from Raleigh Street.
- Consider having a staggered placement of the buildings in relation to Raleigh Street, perhaps the mass of the east building projects further south than the west, to invite people in from NW 13<sup>th</sup> and create another layer of distinction for the two wings, as well as interest for the courtyard space between. Layered projections into the ‘street’ in Raleigh could create interesting views.

### **Design and Materials**

- The buildings could use more complementary design elements, or just one or more common elements more clearly developed. Is the binding element the ground floor finishes? Color panel and balcony elements? The connecting ‘DNA’ between the buildings should be clearer.
- Perhaps the buildings could have design elements that programmatically express unit size and type, such as windows or other architectural elements.
- Brick historically was often used in part as a tool to create light, shadow, and depth for building walls, while contemporary brick designs often suffer from a flattened out, less interesting appearance. Consider providing depth to the window openings to add shadow and interest, and allowing window openings to offer a textural element on the façade.
- A roof deck or roof access would be fantastic at this site. Green or eco-roof elements would also be desirable.
- Consider air conditioning carefully, especially in family buildings. Natural ventilation and children with large windows can be a bad combination.
- Give further thought to window type, size and orientation on the project, especially in how various walls face the courtyard(s), access to sunlight, and tenant responses to a desire for privacy.
- Understanding this project will face budgetary rigor, consider clamping down on the material palette early, for example by using three or so primary materials very intentionally and well.

### **Streets and Pedestrian Connectivity**

- Alignment with historic north-south lot lines of underlying block pattern works well;
- Full-width public dedication of NW 13<sup>th</sup> Avenue is appropriate, especially if built out with a raised dock as found elsewhere to the south along this important street.
- Think carefully about fire access and development into the adjacent rights-of-way. Development in NW 12<sup>th</sup> Avenue may raise other challenging issues (Central City Master Plan Amendment for pedestrian crossing over Front, COA E from LUR99-00542 SU MS).
- The design and size of the Raleigh Street private driveway/walkway should not preclude vehicle access further to the east and south in the future. All streets abutting the site should be designed and considered in terms of future access to adjacent sites and beyond.
- Consider the relationship to proposed and future axial termination points in 13<sup>th</sup>/Savier and 12<sup>th</sup>/Raleigh abutting the project so these spaces are

intentionally framed and not ignored or blocked off. Even if you're not developing them at this time, they should be addressed as intentional people spaces for the future.

## **ZONING CODE APPROVAL CRITERIA**

### **(1) DESIGN REVIEW (33.825)**

#### **Chapter 33.825 Design Review**

##### **Section 33.825.010 Purpose of Design Review**

Design review ensures that development conserves and enhances the recognized special design values of a site or area. Design review is used to ensure the conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district or area. Design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area. Design review is also used in certain cases to review public and private projects to ensure that they are of a high design quality.

##### **Section 33.825.055 Design Review Approval Criteria**

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design guidelines for the area.

**Findings:** The site is designated with design overlay zoning (d), therefore the proposal requires Design Review approval. Because of the site's location, the applicable design guidelines are the Central City Fundamental Design Guidelines and River District Guidelines.

#### **River District Design Guidelines and Central City Fundamental Design Guidelines**

The River District is a remarkable place within the region. The area is rich with special and diverse qualities that are characteristic of Portland. Further, the River District accommodates a significant portion of the region's population growth. This area emphasizes the joy of the river, connections to it, and creates a strong sense of community. The goals frame the urban design direction for Central City and River District development.

The Central City Fundamental Design Guidelines and the River District Design Guidelines focus on four general categories. **(A) Portland Personality**, addresses design issues and elements that reinforce and enhance Portland's character. **(B) Pedestrian Emphasis**, addresses design issues and elements that contribute to a successful pedestrian environment. **(C) Project Design**, addresses specific building characteristics and their relationships to the public environment. **(D) Special Areas**, provides design guidelines for the four special areas of the Central City.

#### **River District Design Goals**

1. Extend the river into the community to develop a functional and symbolic relationship with the Willamette River.
2. Create a community of distinct neighborhoods that accommodates a significant part of the region's residential growth.
3. Enhance the District's character and livability by fostering attractive design and activities that give comfort, convenience, safety and pleasure to all its residents and visitors.
4. Strengthen connections within River District, and to adjacent areas.

#### **Central City Plan Design Goals**

1. Encourage urban design excellence in the Central City;

2. Integrate urban design and preservation of our heritage into the development process;
3. Enhance the character of the Central City’s districts;
4. Promote the development of diversity and areas of special character within the Central City;
5. Establish an urban design relationship between the Central City’s districts and the Central City as a whole;
6. Provide for a pleasant, rich and diverse pedestrian experience for pedestrians;
7. Provide for the humanization of the Central City through promotion of the arts;
8. Assist in creating a 24-hour Central City which is safe, humane and prosperous;
9. Ensure that new development is at a human scale and that it relates to the scale and desired character of its setting and the Central City as a whole.

*Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project.*

Throughout this staff report, outstanding areas of concern that must be addressed prior to a staff recommendation of approval are identified in areas of boxed text. The findings are organized into two sections: an unboxed first section with guidelines that are fully met, and a boxed second section with unmet or partially met guidelines and the related outstanding issues.

**A1. Integrate the River.** Orient architectural and landscape elements including, but not limited to lobbies, entries, balconies, terraces, and outdoor areas to the Willamette River and greenway. Develop access ways for pedestrians that provide connections to the Willamette River and Greenway.

**A1-1. Link the River to the Community.** Link the Willamette River to the community reinforcing the river’s significance. This guideline may be accomplished by:

- 1) Organizing land areas and groupings of buildings to visually define the river’s linkage to the community.
- 2) Focusing and articulating roadways and pedestrianways to emphasize the river.
- 3) Developing projects that celebrate the river and contribute to creating centers of interest and activity that focuses on the Willamette.
- 4) Connecting the internal areas of the District to the Willamette Greenway Trail.

**Findings for A1 & A1-1:** The Abigail site abuts and is ‘cut’ by the railroad, which runs parallel with adjacent NW Naito Parkway and the Willamette River just to the northeast. The Abigail is keyed into the ‘cut’ site in a way that orients the north and east facades towards the river. Units facing north and east will have direct river views from the building, and residents on the outdoor balconies on the west and south sides will have the ability to look north or east in alignment with NW 13<sup>th</sup> and Raleigh towards the river, as well. The ‘bridge’ element connecting the two building wings is semi-transparent, allowing some views through the building north towards the river the south side of the project. The north and east courtyards are also both oriented to the river, with upper-floor units gaining views out over the railroad tracks towards the river. New pedestrian walkways on the south and west sides of the building will connect to the adjacent neighborhood and river and greenway trail beyond. *Therefore, these guidelines are met.*

**A2. Emphasize Portland Themes.** When provided, integrate Portland-related themes with the development’s overall design concept.

**Findings:** Located in a former industrial district, the proposed design recognizes the history and character of this area of Portland through the orientation of the

main building entries along a 'loading dock' in NW 13<sup>th</sup> Avenue, through the rectangular massing of the building and punched brick window openings, and by the use of brick, concrete and steel building materials. *Therefore, this guideline is met.*

**A3. Respect the Portland Block Structures.** Maintain and extend the traditional 200-foot block pattern to preserve the Central City's ratio of open space to built space. Where superblocks exist, locate public and/or private rights-of-way in a manner that reflects the 200-foot block pattern, and include landscaping and seating to enhance the pedestrian environment.

**A3-1. Provide Convenient Pedestrian Linkages.** Provide convenient linkages throughout the River District that facilitate movement for pedestrians to and from the river, and to and from adjacent neighborhoods. This guideline may be accomplished by:

- 1) Using visual and physical cues within the design of the building and building entries to express connections to the river and to adjacent neighborhoods.
- 2) Orienting integrated open spaces and trails that physically and visually link the river and/or surrounding neighborhoods.
- 3) Reusing or retaining cobblestone within the design of new development.
- 4) Encouraging flexibility and creativity along streets enhancing their historic or cultural role.
- 5) Creating visual and physical links across major corridors such as I-405, Burnside, and Front/Naito to strengthen connections to the river and other neighborhoods.

**Findings for A3 & A3-1:** The site is being carved out of a larger parcel at the north edge of the Pearl District, abutting the railroad tracks and Front Avenue beyond. The street grid terminates into the railroad tracks north and east of the site. The building placement is in direct alignment with the east boundary of NW 13<sup>th</sup> Avenue, clearly respecting this historic street frontage. Along the south edge of the site, abutting what would be NW Raleigh Street, the first floor building is in alignment with the traditional block structure, but the upper floors project up to an additional 10' past the traditional block pattern. This narrower north-south dimension of 40' is typical of other east-west pedestrian connections in the Pearl District, for example NW Kearney Street between 10<sup>th</sup> and 12<sup>th</sup> Avenues. The building overhang in what would be NW Raleigh Street does allow pedestrians to pass underneath in what functions as a public sidewalk at the building corner, maintaining a sense of continuity with the street pattern on the block to the west. The north and east edges of the building are located in such a way as to align with the traditional grid pattern of adjacent blocks nearby to the west (Savier St.) and the south (12<sup>th</sup> Avenue).

Pedestrian linkages are provided on the south and west sides of the project connecting to the intersection of NW 13<sup>th</sup> and Raleigh. Future connections will be made to the north in alignment with Savier Street, and to the east in alignment with Raleigh, 12<sup>th</sup> and 11<sup>th</sup>, as the remainder of the site develops in the future.

*Therefore, these guidelines are met.*

**A4. Use Unifying Elements.** Integrate unifying elements and/or develop new features that help unify and connect individual buildings and different areas.

**Findings:** The Abigail intends to be a strong fabric building, and reflective of its position on the city grid and the railroad tracks as they cut through the grid. Both buildings relate to the natural materials that make up the bulk of historic buildings in the area, for example extensive use of brick, concrete and steel. Two colors of brick are used to play light against dark and establish a relationship

between the two wings. Striated vertical metal panels harken to corrugation covering existing historic loading platform canopies, railroad station passenger canopies, and long-gone metal-skinned machine and mill buildings in the district. Window openings are in a regular, gridded pattern as often found on simple industrial buildings. The six story mass is common in the Pearl District and establishes a spatial relationship with other nearby development to the south.

*Therefore, this guideline is met.*

**A5. Enhance, Embellish and Identify Areas.** Enhance an area by reflecting the local character within the right-of-way. Embellish an area by integrating elements in new development that build on the area’s character. Identify an area’s special features or qualities by integrating them into new development.

**A5-1. Reinforce Special Areas.** Enhance the qualities that make each area distinctive within the River District, using the following “Special Area Design Guidelines” (A5-1-1 – A5-1-5).

**A5-1-1. Reinforce the Identity of the Pearl District Neighborhood.** This guideline may be accomplished by:

- 1) Recognizing the urban warehouse character of the Pearl District when altering existing buildings and when designing new ones.
- 2) Recognizing the urban warehouse character of the Pearl District within the design of the site and open spaces.
- 3) Designing buildings which provide a unified, monolithic tripartite composition (base/middle/top), with distinct cornice lines to acknowledge the historic building fabric.
- 4) Adding buildings which diversify the architectural language and palette of materials.
- 5) Celebrating and encouraging the concentration of art and art galleries and studios with design features that contribute to the Pearl District’s “arts” ambiance. Consider features that provide connectivity and continuity such as awnings, street banners, special graphics, and streetscape color coordination, which link shops, galleries, entrances, display windows and buildings. Active ground level retail that opens onto and/or uses the sidewalk can contribute to the attraction of the “arts” concentration.

**Findings for A5, A5-1 and A5-1-1:** The proposal brings building walls to the typical street edges in alignment with the traditional grid, with upper stories extending 10’-0” further into the alignment of NW Raleigh Street to create interest and enclosure along the south edge. Brick building skin is also used in keeping with quality buildings in the district, and the project proposes to continue the raised loading dock pattern in NW 13<sup>th</sup> Avenue. The design creates a distinct ground floor and upper-story volume that diversifies the traditional tri-partite forms found in the district with a simple, clean building ‘top’ of metal coping on the parapet, similar to many of the utilitarian warehouse buildings in the neighborhood. *Therefore, these guidelines are met.*

**A5-3. Incorporate Water Features.** Incorporate water features or water design themes that enhance the quality, character, and image of the River District. This guideline may be accomplished by:

- 1) Using water features as a focal point for integrated open spaces.
- 2) Taking cues from the river, bridges, and historic industrial character in the design of structures and/or open space.
- 3) Integrating stormwater management into the development.

**Findings:** Stormwater planters are integrated into the site and landscape plan, including roof drainage to swales in the courtyards and along the south property edge that include exposed stainless steel runnels that make the stormwater visible as it travels from downspouts to the planters. *Therefore, this guideline is met.*

**A5-4. Integrate Works of Art.** Integrate works of art or other special design features that increase the public enjoyment of the District. This guideline may be accomplished by:

- 1) Integrating art into open spaces or along pathways.
- 2) Incorporating art within the structure of the building.
- 3) Using “found objects” that are remnants from the area’s history.

**Findings:** No specific public art projects are proposed in the project. Nevertheless, the proposal includes special design features that have an art-like quality, and which will increase the public enjoyment of the district. A variety of colored glazing panels are included in the connecting bridge element, creating a sense of playfulness and vibrancy to the design. Exposed stainless steel stormwater runnels, playful striped paving patterns in the courtyard, and integrated site seating and lighting also have an integrated, artistic effect on the surroundings. *Therefore, this guideline is met.*

**A7. Establish and Maintain a Sense of Urban Enclosure.** Define public rights-of-way by creating and maintaining a sense of urban enclosure.

**Findings:** The building placement directly aligns with the re-established street lot line on the east side of NW 13<sup>th</sup> Avenue, defining this public right-of-way and maintaining a sense of urban enclosure. A private driveway and sidewalk on the south side of the project will have a public access easement, but is not technically a public right-of-way. Nevertheless, the building establishes and maintains a sense of urban enclosure in keeping with the surrounding district and traditional street grid, except the buildings extend 10’-0” further into the alignment of NW Raleigh than is the case on NW Raleigh Street west of the site. *Therefore, this guideline is met.*

**A8. Contribute to a Vibrant Streetscape.** Integrate building setbacks with adjacent sidewalks to increase the space for potential public use. Develop visual and physical connections into buildings’ active interior spaces from adjacent sidewalks. Use architectural elements such as atriums, grand entries and large ground-level windows to reveal important interior spaces and activities.

**A8-1. Design Fences, Walls and Gateways to be Seen Over.** Design fences, walls and gateways located between a building and the sidewalk to be seen over to allow for social interaction. This guideline may be accomplished by:

- 1) Elevating building entries higher than the public sidewalk or path.
- 2) Creating a low fence or wall to visually separate but not hide semi-private spaces.
- 3) Using a low or stepped-down planting area or terraces to separate private development from a public sidewalk.

**Findings for A8 & A8-1:** There are no significant building setbacks, with the exception of the courtyard spaces created by the building massing. These courtyard spaces are intentionally designed with raised planters, integrated seating, lighting, and paving materials that distinguish yet connect these spaces to the adjacent pedestrian zone. Large areas of glazing, especially on the more prominent and publicly-accessible western wing of the building, allow extensive indoor-outdoor visual connections, with the small retail space directly at the primary southwest building corner. Large windows and well-marked building

entries with canopies abut the adjacent sidewalks. There are no fences or walls between a building and the sidewalk, with the exception of the visually permeable steel cable railing at the ‘dock’ elements, and a series of low landscape planters along the south building edge. All the dock railings and vegetated planters at the perimeter can be seen over to allow for social interaction. Taller, sight-obscuring fencing is proposed along the edges of the north and east courtyards, but these space do not abut a public sidewalk. *Therefore, this guideline is met.*

**B1. Reinforce and Enhance the Pedestrian System.** Maintain a convenient access route for pedestrian travel where a public right-of-way exists or has existed. Develop and define the different zones of a sidewalk: building frontage zone, street furniture zone, movement zone, and the curb. Develop pedestrian access routes to supplement the public right-of-way system through superblocks or other large blocks.

**B1-1. Provide Human Scale to Buildings along Walkways.** Provide human scale and interest to buildings along sidewalks and walkways. This guideline may be accomplished by:

- 1) Providing street furniture outside of ground floor retail, such as tables and chairs, signage and lighting, as well as large windows and balconies to encourage social interaction.
- 2) Providing stoops, windows, and balconies within the ground floors of residential buildings.

**Findings for B1 & B1-1:** Adjacent sidewalks in NW 13<sup>th</sup> Avenue and on private land in alignment with NW Raleigh Street will be built to River District right-of-way standards, including the loading dock with ramps and stairs in NW 13<sup>th</sup> Avenue. There is direct access from the building to these adjacent pedestrian connections in NW 13<sup>th</sup> and NW Raleigh, including the two main building entries, the corner retail space, and secondary exiting. The applicant has identified future pedestrian connections to be made on the larger surrounding site in the future, including a connection west to NW 14<sup>th</sup> in NW Savier, and connections to the southeast in NW Raleigh, 12<sup>th</sup> and 11<sup>th</sup> Avenues. There are building recesses near the main entry facing NW 13<sup>th</sup>, where integral seating is provided, as well as at the angled wall of the corner retail space, with room provided for covered outdoor seating along the walkway of the ‘dock’ structure. Large windows provide views in and out of the building to encourage social interaction, and the ground floor dwelling units have vertically enlarged windows compared to identical units directly above in the upper floors, further emphasizing interaction and human scale at the ground floor. *Therefore, these guidelines are met.*

**B2. Protect the Pedestrian.** Protect the pedestrian environment from vehicular movement. Develop integrated identification, sign, and sidewalk-oriented night-lighting systems that offer safety, interest, and diversity to the pedestrian. Incorporate building equipment, mechanical exhaust routing systems, and/or service areas in a manner that does not detract from the pedestrian environment.

**Findings:** The garage entry point is at the northernmost edge of the building along NW 13<sup>th</sup> Avenue, keeping vehicle movement into the garage as far as possible from the primary NW 13<sup>th</sup> & Raleigh intersection. The sidewalk ‘dock’ feature is raised above the adjacent roadway in NW 13<sup>th</sup> Avenue, ramping down to grade at the driveway on the north end, further protecting pedestrians from auto traffic. The raised dock treatment continues around the corner to the east in alignment with Raleigh Street. Simple wall sconce lighting, soffit downlights, step lights and integral lighting of the rounded benches present a unified, integrated appearance for pedestrians in adjacent streets. Mechanical equipment is generally located on the rooftop, and the emergency generator will have a vertical exhaust pipe that

vents through the roof. A series of four louvers are included on the street-facing within a raised planter along the south project edge, but these louvers only provide garage ventilation and will be partially concealed by the landscaping materials in the planter. Loading and service areas are generally kept in the basement of the building, or on the very north end of the project, allowing the two primary faces of the building to present active spaces or individual dwelling units.

*Therefore, this guideline is met.*

**B3. Bridge Pedestrian Obstacles.** Bridge across barriers and obstacles to pedestrian movement by connecting the pedestrian system with innovative, well-marked crossings and consistent sidewalk designs.

**Findings:** The sidewalks will be built to River District standards, and there are no identified barriers to pedestrian movement. The applicant has presented a conceptual circulation plan for development when the ‘blocks’ to the south, southeast, west, and northwest develop in the future. *Therefore, this guideline is met.*

**B4. Provide Stopping and Viewing Places.** Provide safe, comfortable places where people can stop, view, socialize and rest. Ensure that these places do not conflict with other sidewalk uses.

**Findings:** There will be a new public sidewalk in a dock-like configuration along the extension of NW 13<sup>th</sup> Avenue, and a public walkway in an easement on private land along the south edge. The dock itself, including the integral cable railings, provides an opportunity to stop and view the surroundings, as do the areas adjacent to the main entry and café space along NW 13<sup>th</sup>, where seating is provided directly adjacent to the sidewalk. Along the south edge there are raised planters and stairs to the south courtyard and three individual unit ‘stoops’ that provide space for pedestrians to get out of the movement zone on the sidewalk. Safe, comfortable places are provided along the sidewalks for people to stop, view, socialize and rest. *Therefore, this guideline is met.*

**B6. Develop Weather Protection.** Develop integrated weather protection systems at the sidewalk-level of buildings to mitigate the effects of rain, wind, glare, shadow, reflection, and sunlight on the pedestrian environment.

**Findings:** The main building entry and corner retail entry occur under protective cover of an entry cover or building overhang. The south courtyard building entry also has a shallow (2'-0") canopy projection over the entry door, providing some protection from the elements. The entire south edge of the upper floors of the west wing project 10'-0" out over the dock and sidewalk along the south edge, creating a deep year-round weather-protected area. A small angled section of the east wing building also projects out beyond the first floor wall on the south edge, providing some rain protection and shade for the entry patio/stoop for one of the dwelling units. *Therefore, this guideline is met.*

**B7. Integrate Barrier-Free Design.** Integrate access systems for all people with the building’s overall design concept.

**Findings:** All public areas of the building, parking and courtyard spaces are designed for accessibility. The basement and upper floors are accessible by elevator from the main lobby. The docks/sidewalks are also ramped on at least one end, providing convenient access. *Therefore, this guideline is met.*

**C1. Enhance View Opportunities.** Orient windows, entrances, balconies and other building elements to surrounding points of interest and activity. Size and place new buildings to protect existing views and view corridors. Develop building façades that create visual connections to adjacent public spaces.

**C1-1. Increase River View Opportunities.** Increase river view opportunities to emphasize the River District ambiance. This guideline may be accomplished by:

- 1) Designing and locating development projects to visually link their views to the river.
- 2) Providing public stopping and viewing places which take advantage of views of River District activities and features.
- 3) Designing and orienting open space and landscape areas to emphasize views of the river.

**Findings for C1 & C1-1:** The main building lobby, corner retail space and community room are all oriented directly to NW 13<sup>th</sup> Avenue or the adjacent sidewalk in alignment with NW Raleigh. The building maintains historic views in alignment with NW 13<sup>th</sup> Avenue looking north. Large areas of clear glazing provide generous indoor-outdoor visual connections to the surroundings, especially at the more active lobby/retail/common room spaces on the southwest corner.

Upper stories of the building have balconies that can take advantage of river views, including on the south and west sides where you can look east or north towards the river. The glazed bridge element also allows views north towards the river through the north courtyard. *Therefore, this guideline is met.*

**C2. Promote Quality and Permanence in Development.** Use design principles and building materials that promote quality and permanence.

**Findings:** The material palette includes two colors of brick, concrete, metal panel and aluminum, vinyl and fiberglass window systems. Masonry is a common building material in the district, and smaller areas with metal panel or concrete are also common in the district. Applied intentionally within punched brick openings, the vinyl windows on the upper floors being used are of a more durable, commercial character than other types (VPI Windows). The building materials and design concept for the exterior skin of the building are durable and will hold up over time. *Therefore, this guideline is met.*

**C3-1. Integrate Parking.** Design parking garage exteriors to visually integrate with their surroundings. This guideline may be accomplished by:

- 1) Designing street facing parking garages to not express the sloping floors of the interior parking.
- 2) Designing the sidewalk level of parking structures to accommodate active uses, display windows, public art or other features which enhance the structure's relationship to pedestrians.

**Findings for C3 & C3-1:** The garage access door is located on the northernmost edge of the project, adjacent to NW 13<sup>th</sup> Avenue. The door itself has a painted steel frame with staggered and colored metal panel sections providing visual screening of the driveway when closed, with material and tonal qualities that integrate with the metal panel used elsewhere on the building. All the parking is below grade and adjacent sidewalks. *Therefore, this guideline is met.*

**C4. Complement the Context of Existing Buildings.** Complement the context of existing buildings by using and adding to the local design vocabulary.

**Findings:** The project incorporates elements of scale, materials, and height found elsewhere in the River District, including the six-story form, extensive use of brick, and division of the building into separate wings. The use of a connecting ‘bridge’ element between the two buildings is a novel concept for the district that has not been seen before, adding to the local design vocabulary. Other complementary moves include the raised docks along NW 13<sup>th</sup>, exposed stormwater management planters with visible runnels, and vertically striated and colored metal panel materials, which is found on other nearby buildings. *Therefore, this guideline is met.*

**C6. Develop Transitions between Buildings and Public Spaces.** Develop transitions between private development and public open space. Use site design features such as movement zones, landscape elements, gathering places, and seating opportunities to develop transition areas where private development directly abuts a dedicated public open space.

**Findings:** The raised dock is a transitional element, along with the recesses that are cut into and under the building at ground level, especially at the prominent and most active southwest corner retail and lobby/common room areas. The raised dock in NW 13<sup>th</sup> Avenue returns partly around the corner along the south project edge, which extends further into the open south courtyard and raised planters to the east. Gathering spaces with seating near the main entry, under cover at the corner retail space, and in the open south courtyard provide transition areas at the edge of the project. *Therefore, this guideline is met.*

**C7. Design Corners that Build Active Intersections.** Use design elements including, but not limited to, varying building heights, changes in façade plane, large windows, awnings, canopies, marquees, signs and pedestrian entrances to highlight building corners. Locate flexible sidewalk-level retail opportunities at building corners. Locate stairs, elevators, and other upper floor building access points toward the middle of the block.

**Findings:** The corner at NW 13<sup>th</sup> & Raleigh is activated by the placement of the corner retail space, and the main lobby entry is immediately adjacent to the retail space. The entire SW corner of the building is treated distinctly through the use of large windows, signage panels in the wall, and the use of projecting canopies and building overhangs. Retail is placed at the corner and the upper-floor building access is nearby toward the middle of the block. *Therefore, this guideline is met.*

**C8. Differentiate the Sidewalk-Level of Buildings.** Differentiate the sidewalk-level of the building from the middle and top by using elements including, but not limited to, different exterior materials, awnings, signs, and large windows.

**Findings:** The sidewalk level of the building is distinguished by inset building walls at the southwest corner, larger windows and more glazing in general, and a taller floor-to-ceiling height. Storefront window systems are used only at the ground floor, which is also where projecting entry canopies and signage are to be located. *Therefore, this guideline is met.*

**C9. Develop Flexible Sidewalk-Level Spaces.** Develop flexible spaces at the sidewalk-level of buildings to accommodate a variety of active uses.

**Findings for C9:** The building is primarily an apartment building, with a small corner retail space. The applicant envisions a café or bistro for the corner retail

space, but the design could accommodate a variety of office or retail tenants given the open plan, flexible layout. The entry lobby and common room are of a size that they can serve a variety of support and community functions for future residents. *Therefore, this guideline is met.*

**C10. Integrate Encroachments.** Size and place encroachments in the public right-of-way to visually and physically enhance the pedestrian environment. Locate permitted skybridges toward the middle of the block, and where they will be physically unobtrusive. Design skybridges to be visually level and transparent.

**Findings:** There are no significant encroachments into the public right-of-way. Standard projection of open balconies on the west façade and a portion of the main lobby entry canopy project into the right-of-way in NW 13<sup>th</sup> Avenue, but the building otherwise is confined to the private property. The balconies are successful at bringing energy and focus to the southwest corner and NW 13<sup>th</sup> Avenue façade, and the projecting entry canopy provides helpful visual identification of the entry door with vivid yellow coloration. *Therefore, this guideline is met.*

**C11. Integrate Roofs and Use Rooftops.** Integrate roof function, shape, surface materials, and colors with the building’s overall design concept. Size and place rooftop mechanical equipment, penthouses, other components, and related screening elements to enhance views of the Central City’s skyline, as well as views from other buildings or vantage points. Develop rooftop terraces, gardens, and associated landscaped areas to be effective stormwater management tools.

**Findings:** The roof is simple and functional, with a built-up asphalt roofing and aggregate surface treatment. Rooftop mechanical units are limited to four centrally-located common space HVAC units, two each near the center of both the east and west wings. These four HVAC units are the only mechanical equipment on the rooftop, and are joined by a single centralized elevator over-run and trash chute exhaust structure. Provisions have been made for future kitchen exhaust in the south central portion of the west wing rooftop, but the specific unit is not shown. Because of the limited size and number of rooftop elements, no supplemental screening is proposed or necessary. *Therefore, this guideline is met.*

**C12. Integrate Exterior Lighting.** Integrate exterior lighting and its staging or structural components with the building’s overall design concept. Use exterior lighting to highlight the building’s architecture, being sensitive to its impacts on the skyline at night.

**Findings:** Several types of lighting fixtures are proposed for the project, and the application package includes six lighting plans. Wall sconce lights are used on the main exterior building walls and courtyard fencing, with soffit downlighting in the areas of building overhang at the southwest corner. Step lights are used near stairs and ramps at the raised loading docks. Round seating circles have integral halo-effect tape lighting, and the on-site courtyards feature bollard lighting. Three vertical pole lights are proposed in the sidewalk along the south project edge, and fence-mounted spot lights are shown oriented inwards to the playground in the north courtyard. The exterior lighting all occurs on the ground or first floor level, and helps to highlight the building’s architecture, without any skyline impacts at night. *Therefore, this guideline is met.*

**B5. Make Plazas, Parks and Open Space Successful.** Orient building elements such as main entries, lobbies, windows, and balconies to face public parks, plazas, and open spaces. Where provided, integrate water features and/or public art to enhance the public open space. Develop locally oriented pocket parks that incorporate amenities for nearby patrons.

**Findings for B5:** There are no directly adjacent public parks or open spaces, although the The Fields Park is located two blocks diagonally to the southeast. Although a direct connection is not being provided to this park in the short-term, the applicant has provided a future circulation plan to show additional pedestrian connections that will be made as the remainder of the site develops over time, including walkways extending east another block in NW Raleigh and south along NW 11<sup>th</sup> Avenue to the Fields.

This guideline also speaks to the creation of successful open spaces on private property, as indicated by the majority of the examples used in the guidelines document. At the Design Advice Request, Design Commissioners suggested that the north courtyard was not the best spot for a children’s playground because of limited sunlight access, and that the east courtyard feels like leftover space that could be improved. Some concern was also raised that the north and east courtyards are creating ‘noise traps’ for train noise. Enlarging the south courtyard space by moving the bridge to the north, and/or re-shaping the building to hug the eastern edge of the site with a bigger courtyard between the two wings were suggested as ideas to consider. Another specific solution might be to pull the angular northern portion of the east wing over to the southeast, filling in all or a portion of the east courtyard by re-shaping the floor plan and unit layout.

The courtyard size and placement and the relationship of the bridge is the same in the current proposal as that presented at the DAR, with a narrow north courtyard and children’s playground that ranges in width from 30 to 40 feet. While the exterior materials and building skin design have improved dramatically since the DAR, the courtyard size and placement remains a concern in terms of making the on-site open spaces successful.

Without further consideration of the building placement, including the size and location of the exterior courtyards, the project is creating a challenged outdoor courtyard environment on the north and east edges of the project. *Therefore, this guideline is not yet met.*

**C5. Design for Coherency.** Integrate the different building and design elements including, but not limited to, construction materials, roofs, entrances, as well as window, door, sign, and lighting systems, to achieve a coherent composition.

**Findings:** The applicant has improved and refined the building design since the DAR in terms of building skin, materials, and the complementary/antagonistic relationship between the two wings of the building. The connecting bridge element has also been treated distinctly with a unique pattern of fiberglass windows and colored glass. All materials and colors are in natural, neutral tones allowing for the colorful yellow entry elements and bridge ‘wedge’ to sing without competition. The three building elements of east wing, west wing and bridge speak in relation to each other while presenting a stylistically unified whole.

At the Design Advice Request, Design Commissioners suggested that the north courtyard was not the best spot for a children’s playground because of limited

sunlight access, and that the east courtyard feels like leftover space that could be improved. Some concern was also raised that the north and east courtyards are creating ‘noise traps’ for train noise. Enlarging the south courtyard space by moving the bridge to the north, and/or re-shaping the building to hug the eastern edge of the site with a bigger courtyard between the two wings were suggested as ideas to consider. Another specific solution might be to pull the angular northern portion of the east wing over to the southeast, filling in all or a portion of the east courtyard by re-shaping the floor plan and unit layout.

The courtyard size and placement and the relationship of the bridge is the same in the current proposal as that presented at the DAR, with a narrow north courtyard and children’s playground that ranges in width from 30 to 40 feet. While the exterior materials and building skin design have improved dramatically since the DAR, the courtyard size and placement remains a concern in terms of making the on-site open spaces successful.

The two entry locations for the residential units are identified through the use of bold yellow steel panel canopies, seating, doors, and vertical wall treatments near each entry. This is most notable along NW 13<sup>th</sup> Avenue at the main lobby entry, where these elements clearly define the main entry and integrate with the L at the end of the building identification signage immediately adjacent. The yellow color is used again in a more subtle way at the inset south courtyard entry door. However, this yellow color is used again as a vertical feature on the south façade of the east wing in a location that does not signify building entry, and the pre-hearing design packet also shows the yellow now appearing on the first floor walls of the west wing at the south courtyard. These non-entry locations for the yellow panel are inconsistent and distracting features of an otherwise well-considered approach to signifying building entry. The use of the yellow metal panel deserves specific discussion by Design Commission to ensure that this prominent design feature is cohesive and integrated. Confining the yellow pattern to areas of entry only, and eliminating this surface material elsewhere, may increase the cohesiveness of the project.

Without further consideration of the building placement, including the size and location of the exterior courtyards, the project is creating a challenged outdoor courtyard environment on the north and east edges of the project. The use of bright yellow metal panel to highlight building entries is successful, but diluted by the application of the material on the common area courtyard walls and the south face of the east wing. Although the project is generally cohesive, the current courtyard arrangement and bright yellow panel application deserve further consideration before Design Commission. *Therefore, this guideline is not yet met.*

**C13. Integrate Signs.** Integrate signs and their associated structural components with the building’s overall design concept. Size, place, design, and light signs to not dominate the skyline. Signs should have only a minimal presence in the Portland skyline.

**Findings:** The proposed signage includes raised letter building identification signage near the lobby entry, two small blade signs near the corner retail space, and two large panel wall signs on the corner retail space of 40 square feet each. The building identification sign and the blade signs appear to be less than 32 square feet, and therefore exempt from Design Review.

Sign Code for this site allows 1.5 times the length of the primary building wall as retail signage. With a primary building wall of approximately 30 feet, the retail space is allowed a maximum of approximately 45 square feet. The two 40 square foot panel wall signs result in too much signage for this retail space per the Sign Code, not counting the blade signs. The two panel signs on the retail space are also not clearly integrated with the ground floor design, appearing in the rendering (Exhibit C.14) as both in and out of the transom zone at the ground floor. The size and placement of these signs is not characteristic of the district, and their size and location where a window panel would typically be located should be further examined. It may be to the benefit of the project to eliminate these signage panels and replace them with clear glazing, limiting the retail signage so smaller blade signs or signage hung inside the glass.

The corner retail signage bears further consideration to comply with Sign Code size limits and the overall building design concept. One option might be to defer the sign package to a future Type II review after receiving input from Design Commission, replacing the large sign panels at the corner retail space to storefront glass for this application. *Therefore, this guideline is not yet met.*

**(2) MODIFICATION REQUESTS (33.825)**

**33.825.040 Modifications That Will Better Meet Design Review Requirements:**

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the design review process. These modifications are done as part of design review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through design review may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

- A. Better meets design guidelines.** The resulting development will better meet the applicable design guidelines; and
- B. Purpose of the standard.** On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.

**Findings:** The applicant has requested Modifications through Design Review to allow the following:

1. Reduce the width of nine of the parking stalls from a minimum 8'-6" wide to a range of from 7'-8" to 8'-5" wide (33.266.13-F.2);
2. Reduce the width of 100 (was 208) of the long-term bike parking stalls from 2'-0" to 1'-6" (33.266.220.C.3); and
3. Reduce the loading bay from 35'-0" long, 10'-0" wide and 13'-0" clear to 21'-6" long, 9'-0" wide and 8'-2" clear (33.266.310.D).

**Modification #1 – Parking Stall Width:** The purpose of the parking stall dimensional standards is to promote safe circulation in the parking area and direct traffic in the parking area. The purpose of the bike parking stall dimensions is to ensure that required bicycle parking is designed so that bicycles may be securely locked without undue inconvenience and will be

reasonably safeguarded from intentional or accidental damage. The purpose of the loading stall size standards is to ensure that larger uses and developments have adequate loading spaces.

The parking stall width reduction is relatively minimal, and only occurs for small portions of nine of the parking stalls where structural columns are located. The structural columns generally occur at the inside or outside edges of the parking stall as opposed to the center, reducing the need for the extra width because the door swing/access area is more towards the center of a parking space.

Modification #2 – Bike Parking Stall Width: Bike parking is provided in excess of the code requirement, and the applicant has revised the basement bike parking layout to reduce the number of reduced width stalls from 208 to 100. The wall-mounted staggered vertical rack system has a stagger and allowance for sliding hangers that will assist in the hanging and locking of bikes. Additionally, the loops to which the bike are hung project out of the wall 27” to further ease bike hanging. A 5’-0” aisle is still provided adjacent to each rack, and the Zoning Code does not yet establish a distinction between minimum dimensions for wall hanging spaces versus floor mounted spaces.

Modification #3 Loading Stall Dimensions: Loading functions are often achieved in a building of this size through the provision of a smaller, van-sized loading space. Commercial deliveries are expected to be minimal given the size of the retail space.

Containing all the required parking and loading functions within the building basement helps reduce the impacts of these functions on the streetscape, consistent with C3-1, Integrate Parking.

Staff from Portland Transportation has reviewed the proposal for potential impacts on the public right-of-way, and for overall transportation system impacts. However, the initial response did not specifically provide findings or information on these transportation-related Modifications. Until a more detailed response is received addressing these Modifications specifically, staff cannot find that the approval criteria are met. *Therefore, these criteria are not met.*

## **DEVELOPMENT STANDARDS**

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

Pertinent development standards include the following:

- Maximum 4:1 FAR (Approx. 3.9:1 FAR proposed, Residential FAR bonus available);
- Maximum Height of 100’ (Approx. 68’-0” proposed);
- No Required Building Lines;
- No Ground Floor Active Use or Minimum Active Floor Area;
- Ground Floor Windows apply on west and south (met/exceeded);
- North Pearl Subarea Standards with Public Open Area (apply to sites over 40,000 square feet, site is 39,909 square feet);

- Bike Parking – 217 long-term, 10 short-term required (238 long-term, 12 short-term proposed);
- Loading – one ‘A’ space at least 35’-0” long, 10’-0” wide with 13’-0” clearance is required (reduced size proposed through Modification);
- Bike Parking Stall and Vehicle Parking Stall Dimensions (reduced sizes proposed through Modifications); and
- No minimum vehicle parking, no maximum for residential uses (82 residential stalls proposed).

## CONCLUSIONS

The Design Review process exists to promote the conservation, enhancement, and continued vitality of areas of the City with special architectural, scenic, or cultural value.

This project has commendably evolved since the Design Advice Request, including the addition of an integrated, quality building exterior of brick, metal panel, and regular punched window openings. The building creates a sense of street enclosure along NW 13<sup>th</sup> Avenue, and the design of the ground level reinforces and supports a robust pedestrian experience. Temporary asphalt roadways will be replaced with permanent surfacing in the future as adjacent properties redevelop, but the raised dock feature and public sidewalk on the west and south building edges will meet current River District standards. The paving materials, seating, and lighting concept used for the outdoor spaces are visually interesting, durable, and well-considered.

However, three specific issues have presented the project from fully meeting the applicable design guidelines, and merit further consideration before Design Commission. These include the form and layout of the open spaces on the site, in particular regarding the north and east courtyards. The yellow metal accents are used effectively to highlight the building entries, but their use on the common room courtyard walls and on the south edge of the east wing are inconsistent with the visual hierarchy used elsewhere and dilute the sense of entry. Finally, the retail signage appears to be larger than allowed, and the two large wall panel signs in merit reconsideration or removal. If these projects can be addressed to the satisfaction of Design Commission, the project will merit approval.

## TENTATIVE STAFF RECOMMENDATION

(May be revised upon receipt of new information at any time to the Design Commission decision)

At this time, staff cannot recommend full approval as the following design guidelines are not yet met:

- B5, Make Plazas, Parks and Open Space Successful;
- C5, Design for Coherency; and
- C13, Integrate Signs.

Staff recommends the following three areas of discussion at the November 21<sup>st</sup> hearing:

1. **North and East Courtyards** (Guidelines B5, C5): Consider changes to the size and location of these key outdoor spaces, improving access to sunlight, and generally improving their utility and spatial layout in the context of future residents/users and the adjacent railroad tracks;

- 2. **Yellow Panels/Entries** (Guideline C5): Consider limiting the yellow metal panel accents to the main lobby and courtyard entry to avoid diluting the power of this material and design gesture; and
- 3. **Retail Signage** (Guideline C13): The size of the retail space does not allow the amount of signage proposed, and the two large wall panel signs should be eliminated, reconsidered, or reduced in size.

Should the applicant choose to address and satisfy the above concerns before Design Commission, staff could recommend full approval at a future hearing date. If a favorable response is received from Portland Transportation, full approval would include the three requested Modifications, as well.

**NOTE:** Bureau of Environmental Services (BES) has requested clarifications from the applicant on two outstanding items which should be addressed prior to any Design Review approval, since they impact the site/landscape plans. These include getting an appeal to dispose of the courtyard paving/non-roof stormwater runoff instead of managing it on-site, and increased/proper size for the temporary Raleigh driveway stormwater planter. With resolution of these issues at or before the hearing, BES staff expects to support approval of the project.

**NOTE:** Portland Transportation will provide supplemental findings addressing the approvability of the three transportation-related Modifications prior to or at the November 21<sup>st</sup>, hearing, at which time the findings for the Modifications can be finalized.

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**Procedural Information.** The application for this land use review was submitted on August 6, 2013, and was determined to be complete on October 4, 2013.

*Zoning Code Section 33.700.080* states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on August 6, 2013.

*ORS 227.178* states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period.

**Some of the information contained in this report was provided by the applicant.** As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the recommendation of the Bureau of Development Services with input from other City and public agencies.

**This report is not a decision. The review body for this proposal is the Design Commission who will make the decision on this case.** This report is a recommendation to the Design Commission by the Bureau of Development Services. The review body may adopt, modify, or reject this recommendation. The Design

Commission will make a decision about this proposal at the hearing or will grant a continuance. Your comments to the Design Commission can be mailed, c/o the Design Commission, 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201 or faxed to 503-823-5630.

You will receive mailed notice of the decision if you write a letter received before the hearing or testify at the hearing, or if you are the property owner or applicant. You may review the file on this case by appointment at the Development Services Building, 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201.

**Appeal of the decision.** The decision of the Design Commission may be appealed to City Council, who will hold a public hearing. If you or anyone else appeals the decision of the review body, only evidence previously presented to the review body will be considered by the City Council.

**Who can appeal:** You may appeal the decision only if you write a letter which is received before the close of the record for the hearing, if you testify at the hearing, or if you are the property owner/applicant. **Appeals must be filed within 14 days of the decision. An appeal fee of \$5,000.00 will be charged** (one-half of the BDS application fee for this case, up to a maximum of \$5,000.00).

Additional information on how to file and the deadline for filing an appeal will be included with the decision. Assistance in filing the appeal and information on fee waivers are available from the Bureau of Development Services in the Development Services Center, 1900 SW Fourth Ave., First Floor. Neighborhood associations recognized by the Office of Neighborhood Involvement may qualify for a waiver of the appeal fee provided that the association has standing to appeal. The appeal must contain the signature of the Chair person or other person authorized by the association, confirming the vote to appeal was done in accordance with the organization's bylaws.

Neighborhood associations, who wish to qualify for a fee waiver, must complete the Type III Appeal Fee Waiver Request for Organizations Form and submit it prior to the appeal deadline. The Type III Appeal Fee Waiver Request for Organizations Form contains instructions on how to apply for a fee waiver, including the required vote to appeal.

**Recording the final decision.**

If this Land Use Review is approved the final decision must be recorded with the Multnomah County Recorder. A few days prior to the last day to appeal, the City will mail instructions to the applicant for recording the documents associated with their final land use decision.

- A building or zoning permit will be issued only after the final decision is recorded.

The applicant, builder, or a representative may record the final decision as follows:

- By Mail: Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- In Person: Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County

Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034  
For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

**Expiration of this approval.** An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

**Applying for your permits.** A building permit, occupancy permit, or development permit must be obtained before carrying out this project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed here.
- All applicable development standards, unless specifically exempted as part of this land use review.
- All requirements of the building code.
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the city.

**The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).**

Mark Walhood  
November 12, 2013

#### **EXHIBITS**

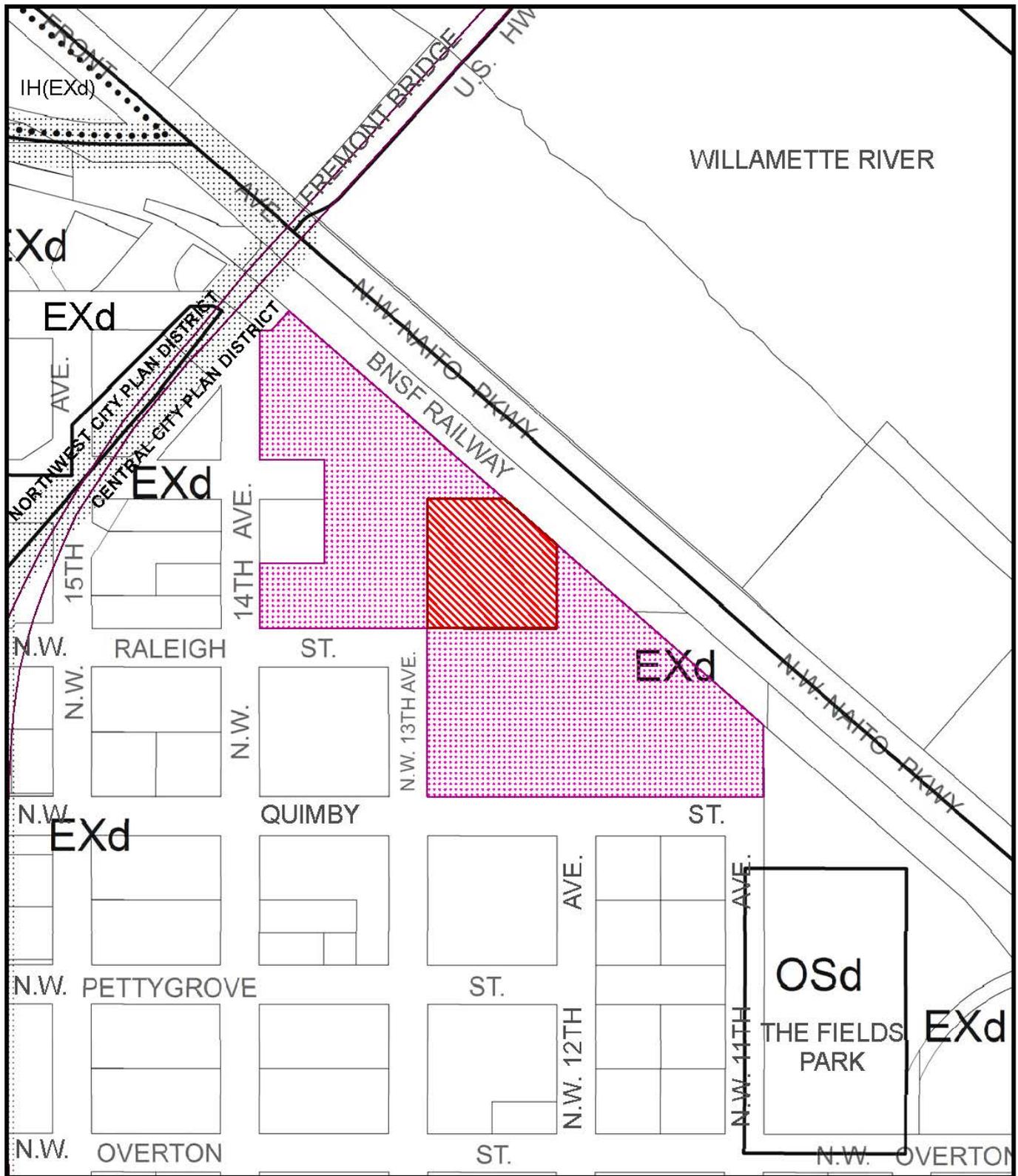
NOT ATTACHED UNLESS INDICATED

- A. Applicant's Statements
  1. Original narrative, including copies of Design Advice Request and Pre-Application Conference Summary Notes
  2. Supplemental narrative submitted with revisions, and including stormwater report and HHPR memo and plans discussing right-of-way improvements, received October 1, 2013
  3. Original plan set – reference only – not approved
  4. Revised plan set, received 10/1/13 – reference only – not approved
  5. Cover memo submitted with first hearing drawing packet, received 11/6/13
  6. E-mail with Raleigh Street improvement details and drawing, received 8/22/13
  7. Cover memo submitted with final drawings making case complete, received 10/4/13
- B. Zoning Map (attached)

### C. Plan & Drawings

1. Vicinity Map
2. Site Photos
3. <blank>
4. Diagrams
5. Site Plan
6. Perspective View – West
7. Perspective View – Southwest Corner
8. Perspective View – South
9. Perspective View – East
10. Perspective View – North Courtyard
11. Perspective View – Northeast Corner
12. Solar Studies
13. Sign Package – Building Entry
14. Sign Package – Retail
15. Basement Plan (attached)
16. Level 1 Plan (attached)
17. Level 2-6 Plan
18. Roof Plan
19. West Elevation (attached)
20. West Courtyard Elevation
21. East Courtyard Elevation
22. East Elevation
23. South Elevation (attached)
24. North Elevation
25. Materials
26. Building Section
27. Enlarged Section/Details – Entry
28. Enlarged Section/Details – Bridge
29. Enlarged Section/Details – East Building Corner
30. Enlarged Section/Details – West Building Corner
31. Garage Exhaust/Balcony Details
32. Details – entry canopy, board formed concrete, dock guardrail
33. Garage Gate Details
34. Landscape Plan
35. Courtyard Section A
36. Courtyard Section B
37. Courtyard Section D
38. Courtyard Section C
39. Courtyard Section E
40. Furnishings
41. Paving Materials
42. Seating Elements
43. Planting Plan – Trees
44. Planting Plan – Tree Images
45. Planting Plan – Shrubs and Groundcover
46. Planting Plan – Shrub and Groundcover Images
47. Stormwater Runnels
48. Planting Plan – Stormwater and Vines
49. Perimeter Fence
50. Perimeter Fence – Elevations
51. Lighting Plan - overall
52. Lighting Plan – wall sconces
53. Lighting Plan – wall sconce and linear uplight
54. Lighting Plan – wall sconce and recessed downlight

55. Lighting Plan – pole and steplights
  56. Lighting Plan – bollard and tape lights
  57. Roof Mechanical
  58. Vinyl Window Cut Sheet
  59. Fiberglass Window Cut Sheet
  60. Lot Configuration
  61. Hoyt Street Yards Phase 5 – Pedestrian Circulation
  62. Raleigh Street Section
  63. Street Improvement Plan
  64. Site Grading
  65. Site Utility Plan
  66. Stormwater Planter Details
  67. <blank>
  68. Site Fire Plan
  69. Materials Board
- D. Notification information:
1. Request for response
  2. Posting information and notice as sent to applicant
  3. Applicant’s statement certifying posting
  4. Mailed public hearing notice
  5. Mailing list for public hearing notice
- E. Agency Responses:
1. Fire Bureau
  2. Life Safety Section of the Bureau of Development Services
  3. Urban Forestry Division of Portland Parks and Recreation
  4. Water Bureau
  5. Bureau of Environmental Services
  6. Site Development Review Section of the Bureau of Development Services
  7. Development Review Section of Portland Transportation
- F. Letters
1. E-mail with comments from Doug Klotz, including staff responses, 11/5/13
- G. Other
1. Original LU application form and receipt
  2. Incomplete letter from staff to applicant, sent 8/30/13
  3. Request for Completeness information and response: PBOT Comments
  4. Request for Completeness information and response: BES Comments



# ZONING

-  Site
-  Also Owned
-  Historic Landmark



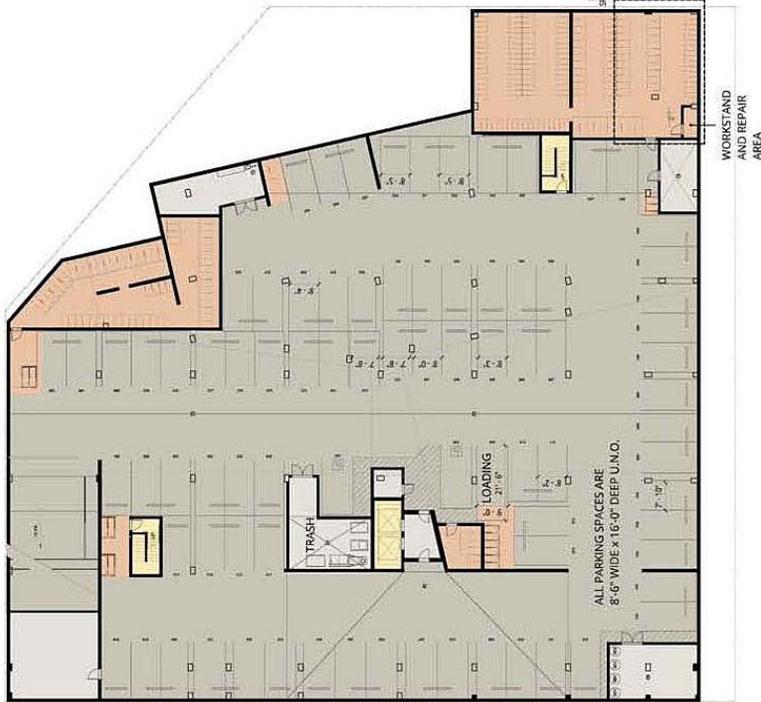
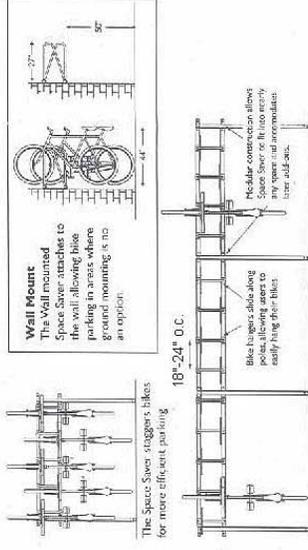
This site lies within the:  
**CENTRAL CITY PLAN DISTRICT**  
**RIVER SUB DISTRICT**  
**NORTH PEARL SUB AREA**

File No. LU 13-189059 DZM  
 1/4 Section 2828  
 Scale 1 inch = 200 feet  
 State\_Id 1N1E28DD 708  
 Exhibit B (Aug 29, 2013)

**C.15** Basement Plan

THE ABIGAIL/ PORTLAND, OR

Below grade parking garage with access ramp along 13th Avenue provides 82 car parking stalls, 1 loading space, and long term bike parking spaces



Typical Wall Rack



Typical Floor Rack

**Modification:**

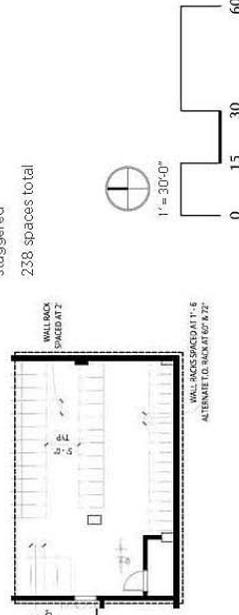
33,266 220.C Standards for Bicycle Parking:  
A space 2'-0" by 6'-0" must be provided for each required bicycle parking space.

**Required Bicycle Parking:**

There are 143 apartments in The Abigail.  
215 bicycle parking spaces are required.  
The required bicycle can be provided in the space shown by providing:  
48 floor racks at 24" oc  
167 wall racks at 24" oc  
216 spaces total

**Alternate Proposal:**

We would like to provide more bike parking and more of those spaces as floor racks within the allocated space. This project is oriented towards families with children. This means that we will have larger extended bikes and trailers that are best secured to floor racks. At the same time, we will have children's bikes that take up less space. We propose the following alternate:  
58 floor racks at 24" oc  
80 wall racks at 18" oc vertically staggered  
238 spaces total



BRIDGE HOUSING  
ANKHROM MOISAN ARCHITECTS, INC.

DESIGN REVIEW EXHIBITS  
11.06.2013

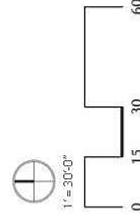


**Outdoor Spaces**

The building form divides the green spaces into three distinct courtyards. The south courtyard is open and more public, and connects to the NW Raleigh pedestrian street. The north courtyard is private, secure and more protected with a fence along the railroad tracks. This space provides a play area for kids and a visual connection to the Fremont bridge. The east courtyard contains private patios and a large planted area.

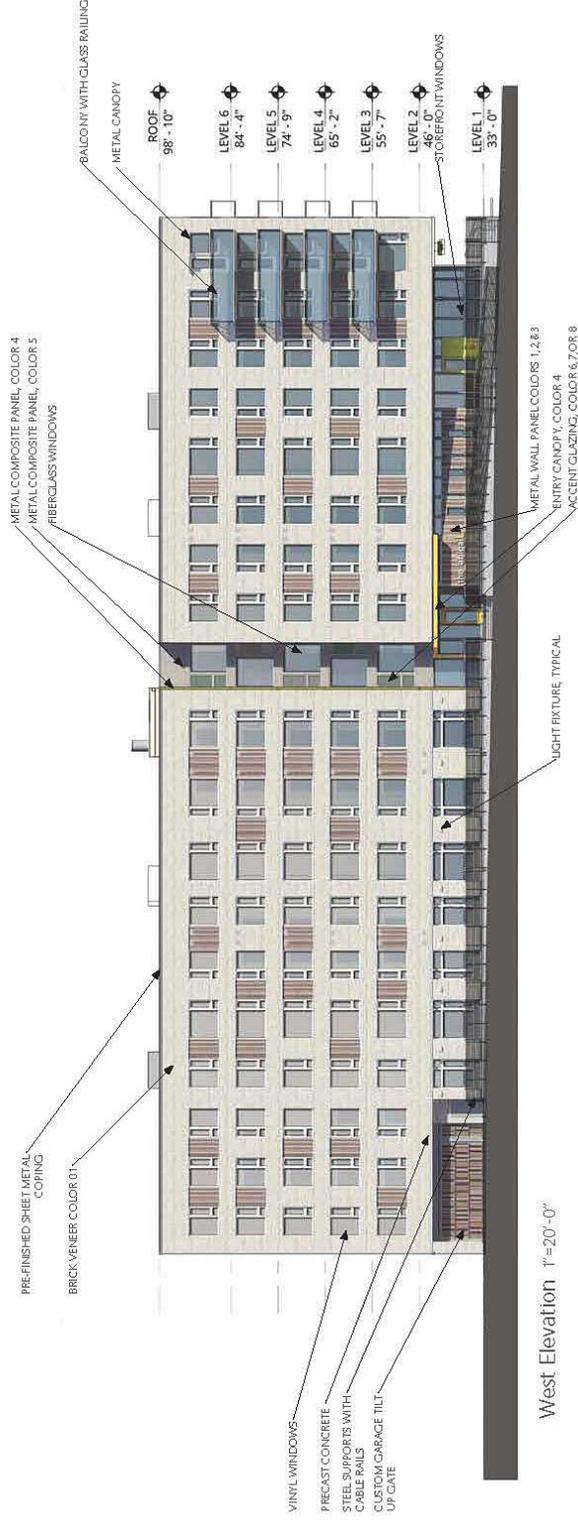
**Program:**

- 18 Residential Units
- 12 short term bike spaces
- Community Room
- Cafe
- Play Area



**C.19** Building Elevations

THE ABIGAIL / PORTLAND, OR



BRIDGE HOUSING  
ANKROM MOISAN ARCHITECTS, INC.

DESIGN REVIEW EXHIBITS  
11.06.2013

### C.23 Building Elevations

THE ABIGAIL / PORTLAND, OR



BRIDGE HOUSING  
ANKROM MOISAN ARCHITECTS, INC.

DESIGN REVIEW EXHIBITS

11.06.2013