

10/23/13  
Submitted by  
Scott Somohano

## Sumner Association of Neighbors (SAN)

NE Portland, Oregon

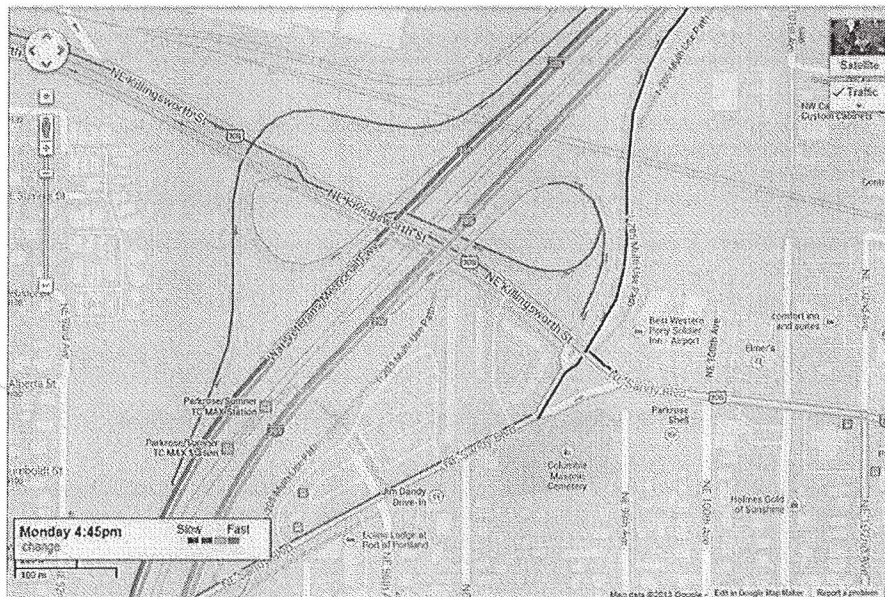


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### Special meeting with ODOT yields new info — and next steps — about onramp widening project

21. October 2013 · [Write a comment](#) · Categories: [health](#), [land use](#), [planning](#), [safety](#), [san](#), [transportation](#)

*Update (on 10/23/13):* Added this animated GIF from Google Traffic Map showing traffic levels at 4:45 PM during the most recent five weekdays. ([Click here for full size.](#))



Normally only the Sumner Association of Neighbors (SAN) board meets at the board meetings. However, this month (on Oct 8) we held a special meeting about the proposal to add a third lane to the onramp southbound from Killingsworth St onto I-205. The proposal is the traffic mitigation portion of a rezone proposal for 48 acres of Colwood Golf Course on NE Cornfoot Road. (For more info, [see our previous item.](#))

### **What's the next step?**

Testimony at the Portland City Hall, 1221 SW Fourth Avenue (downtown) on Wed, Oct 23, at 2 PM ([agenda](#)). All members of the public are allowed to testify. Sumner neighborhood residents are welcome to attend and/or testify. Showing up can make the difference!



*Caption: Facing south, showing the bend in the onramp after the traffic lights for the two existing metered lanes on the ramp.*

## Key points about how this became the mitigation proposal and the project status

At our Oct 8 special board meeting, we had three special guests: Don Goldberg from Trust for Public Lands, Kurt Krueger from PBOT and Marah Danielson from ODOT to discuss the project. The SAN board thanks them for coming to talk on such short notice (at the request of the Portland City Council at their hearing on Sept 25).

Kurt did most of the talking. At the start of his comments, He very graciously apologized for the oversight of not consulting or including our neighborhood in the discussion to date. The meeting was very well attended by neighborhood residents — the room was full — and everyone was neighborly and civil. I'm proud of how everyone treated Don, Kurt and Mara. Some key points:

- **How we got here:** A new process due to a new state law requiring PBOT and ODOT to work together on significant development projects. Also a recent SCOTUS ruling that limits how much developers can be asked to do related to any given project.
- **Where is the project in the design process:** Concept plan is approved, ODOT design to begin this month.
- **When would construction begin:** Spring 2014 at soonest, but tied to proposal to develop the 48 acres (on Cornfoot Road) proposed for industrial zoning.
- **How long would construction last:** 3-4 months
- **Would the SAN Lot be used to stage for the construction:** Unknown, too soon to say, but neighborhood interest in minimizing disruption at the SAN Lot was understood.

## What was the outcome on Oct 8

Three things:

1. The SAN board voted to work with ODOT to mitigation the onramp proposal for livability factors (safety, odor, noise, etc) during the design and construction periods.
2. In return, ODOT agreed to share design drawing for neighborhood input, and consult the neighbors during construction.
3. However, given the unusual nature of the traffic mitigation proposal — and its possible ineffectiveness as a traffic mitigation solution — SAN will again request that the Portland City Council (on Oct 23) consider altering the current proposal to move road improvements to Alderwood Road (at Cornfoot and NE 82nd).

## What makes the proposal unsuitable

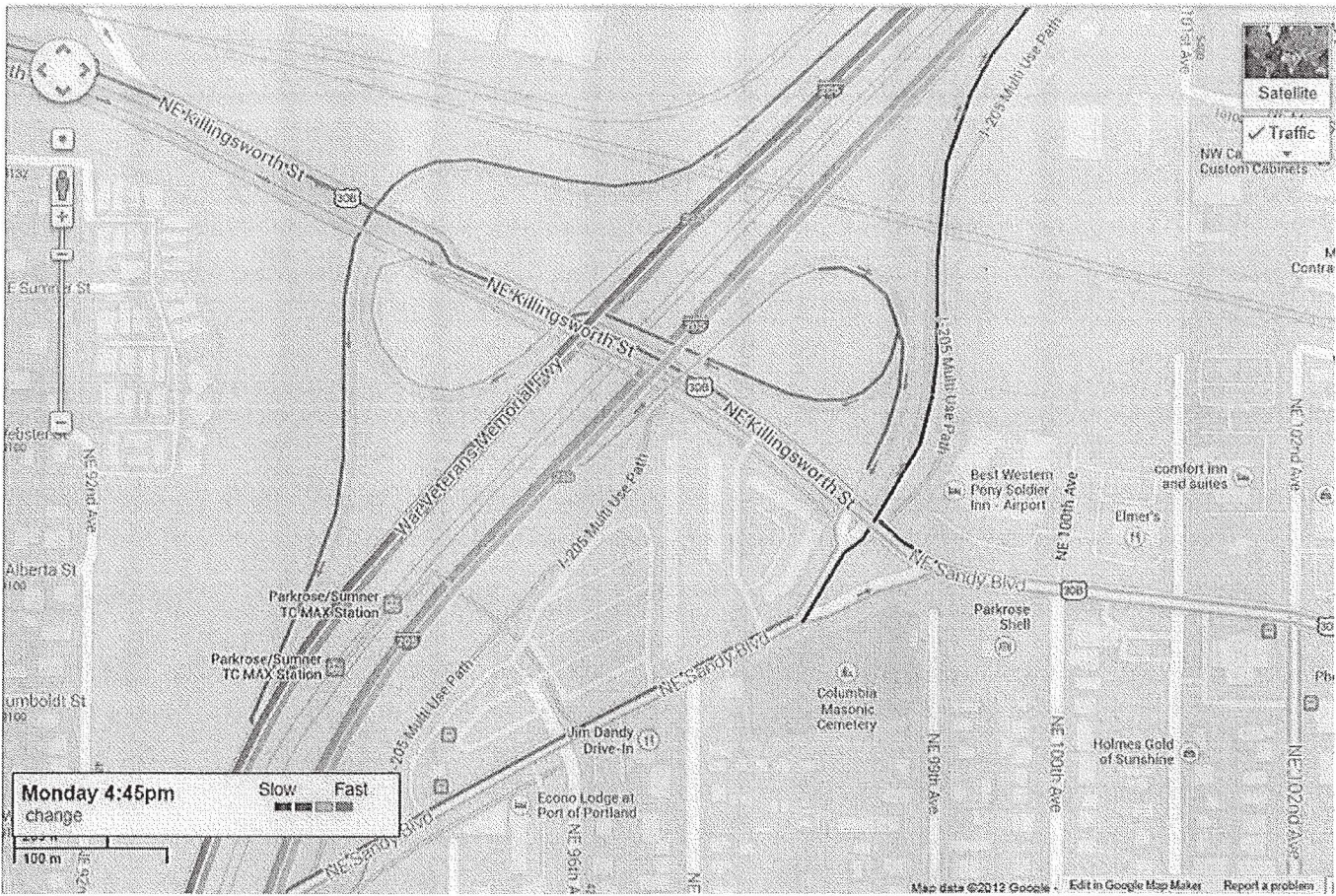
By widening the southbound onramp, traffic related to the 48 acre property on NE Corfoot Road is supposed to be mitigated. Several reasons it does not:

**Proximity:** The proposal is more than 1.5 miles away from the development site. This is unusual. Indeed, Kurt confirmed that this was the first time in his 8-10 years working on such projects that a mitigation proposal has been this far away from the site it's mitigating.

**% of traffic mitigated:** According to the Traffic Impact Analysis (TIA) report produced by Kittelson Associations in 2012 for the rezone proposal, the property on NE Cornfoot Road will generate 2430 trips per day. The TIA also estimates that 50 of those 2,340 trips would use the southbound onramp each day. That's 2.1% of all vehicle trips. By contrast about 66% of trips will use the two intersections nearest to the property (NE Alderwood at NE Cornfoot Road and at NE 82nd Ave).

**Traffic count data does not match other ODOT data:** The Kittelson TIA report showed the southbound onramp at its full capacity (its vehicle-to-capacity ratio), yet shows the northbound onramp (next to the Pony Soldier Hotel in Parkrose) at 2/3 of its capacity. That contradicts the 2013 ODOT Corridor Bottleneck Study, which shows the northbound onramp with a 3 hour delay daily, and local motorist experience.

**Traffic count data does not match itself internally:** We found a repeated internal inconsistency in figures in the Kittelson TIA regarding the southbound onramp intersection. Specifically, in Figures 4, 5, 9 & 10, westbound traffic *leaving* the n/b ramp intersection (next to the Pony Soldier Hotel in Parkrose) was 30-50%



## Parsons, Susan

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**From:** Marc Butorac [MBUTORAC@kittelton.com]  
**Sent:** Tuesday, October 22, 2013 3:38 PM  
**To:** Parsons, Susan  
**Cc:** Haley, Robert  
**Subject:** FW: Colwood Transportation Response - CASE FILE: LU 12-213885 CP ZC EN  
**Attachments:** Colwood Response - FINAL102213.pdf

Susan,

Can you please enter the attached letter into the Colwood file for tomorrow's hearing.

Thanks

Marc Butorac

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**From:** Haley, Robert [mailto:Robert.Haley@portlandoregon.gov]  
**Sent:** Tuesday, October 22, 2013 2:32 PM  
**To:** Marc Butorac  
**Cc:** Krueger, Kurt; Michael.Cerbone@cardno.com; don.goldberg@tpl.org; Frugoli, Sheila  
**Subject:** RE: Colwood Transportation Response - CASE FILE: LU 12-213885 CP ZC EN

Marc,

Please send your letter to the assistant council clerk, Susan Parsons at [susan.parsons@portlandoregon.gov](mailto:susan.parsons@portlandoregon.gov) to have the letter entered into the record.

Bob

---

**From:** Marc Butorac [mailto:MBUTORAC@kittelton.com]  
**Sent:** Tuesday, October 22, 2013 12:53 PM  
**To:** Haley, Robert  
**Cc:** Krueger, Kurt; Michael.Cerbone@cardno.com; don.goldberg@tpl.org; Frugoli, Sheila  
**Subject:** Colwood Transportation Response - CASE FILE: LU 12-213885 CP ZC EN

Bob,

Could you please include the attached response letter into the record for the Colwood CASE FILE: LU 12-213885 CP ZC EN.

Thanks

Marc

**From:** Sumner NeighborhoodAssn [mailto:sumner.neighborhood@gmail.com]  
**Sent:** Friday, October 18, 2013 11:45 AM  
**To:** Krueger, Kurt  
**Cc:** Harry Sanger; Stoll, Alison  
**Subject:** Draft letter: From Sumner NA to Council on Colwood rezone & onramp proposal

Hi Kurt --

Thank you for your call Wed evening & sorry I did not get this to you yesterday as expected. Attached is the draft letter to council. I'd call it 90-95% complete. It includes the two elements I

10/22/2013

described to you on the phone:

1. Re-iteration of our previous request to move the mitigation closer to the Colwood property
2. Our commitment to work with ODOT to mitigate the onramp for livability factors, if Council approves that option.

**One thing that will probably be added:** We found a repeated internal inconsistency in figures in the Kittelson TIA regarding the southbound onramp intersection. Specifically, in Figures 4, 5, 9 & 10 westbound traffic *leaving* the n/b ramp intersection was 30-50% lower than the westbound traffic *arriving* at the s/b ramp intersection.

There is no intervening intersection where those vehicles could turn off. Presumably this was a transcription error of some sort. But that's a significant unexplained flaw in the data on which to base this traffic mitigation recommendation.

I am available this afternoon and early next week if you have questions.

--

Thanks!

Scott Somohano

chair and webmaster

Sumner Association of Neighbors (SAN)

[SumnerPDX.org](http://SumnerPDX.org)

503-254-1449



# KITTELSON & ASSOCIATES, INC.

TRANSPORTATION ENGINEERING / PLANNING

610 SW Alder Street, Suite 700, Portland, OR 97205 P 503.228.5230 F 503.273.8169

October 22, 2013

Project #: 12248

Honorable Mayor and Commissioners  
City of Portland  
1900 SW 4th Avenue  
Portland, OR 97201

**RE: CASE FILE: LU 12-213885 CP ZC EN (Colwood)**

Dear Honorable Mayor and Commissioners,

This letter is submitted in response to the comments raised by the Sumners Association of Neighbors (SAN) at the September 25<sup>th</sup> hearing and in their follow-up letter and email dated October 18<sup>th</sup>. The primary comments appear to be associated with why the improvement should be located at the I-205/Killingsworth Southbound On-Ramp rather than closer to the subject site of the comprehensive plan amendment, and not regarding any direct impacts to the neighborhood itself. The SAN letter raises questions in regards to suitability, effectiveness, and the necessity of the proposed on-ramp improvements. The SAN email raises questions regarding the figures within the transportation impact analysis. The remainder of this letter addresses these comments and questions.

## **SAN October 18<sup>th</sup> Letter Response**

It is important to remember that the proposed I-205/Killingsworth Southbound On-Ramp improvement is the result of the Transportation Planning Rule (TPR) (660-012-0060) which requires an amendment to an acknowledged comprehensive plan, or a land use regulation (including a zoning map) to determine if the existing or planned transportation system would be significantly affected. In assessing the significant effects, the application is required to compare the horizon year (2035) with and without the proposed comprehensive plan and zone map amendment at a system level. The transportation impact analysis (November 2012) examined these effects and found that three intersections, including the I-205/Killingsworth Southbound On-Ramp were affected in year 2035. As such, a measure(s) needed to be identified to address these effects per the TPR.

As part of the most recent update of the TPR, Section 660-012-0060 (2)e allows the City to determine that the proposed land use action is consistent with the identified function, capacity, and performance of the facilities and require improvement measures that would create system-wide benefits that on balance address the identified significant effects. As a result, the City, ODOT, and the applicant looked at the three affected intersections and determined that I-205/Killingsworth Southbound On-Ramp created the greatest system-wide benefit as it has been previously identified on the RTP, represents an

arterial and interstate intersection, and relieved standing queues on Killingsworth that create vehicular, bicycle, and pedestrian conflicts.

To respond directly to the suitability, effectiveness, and the necessity issues raised by SAN in their letter dated October 18<sup>th</sup>, we have listed the concern in *italics* and our response below in standard text:

**Suitability:** *Given the new industrial area resides north of the slough, the proposed traffic mitigation runs counter to the existing comprehensive plan 6A Policy – Regional, as stated:*

*Encourage the use of major arterials for regional traffic and separate this traffic from local traffic.*

*6A-1 Objective: Support the existing growth at the airport for industrial and airport-related services without increasing traffic through the neighborhood.*

*Strategies:*

- 1. Encourage use of I-205 and Airport Way as the primary access routes to the airport, airport related and industrial uses north of the slough*

*Adding an additional lane to the Southbound I-205 on-ramp at Killingsworth would encourage additional traffic in an area where local traffic volume is already high.*

**RESPONSE:** The traffic generated from the proposed land use action and destined northeast and southeast, respectively, uses regional traffic ways, major streets and collectors to access both the I-205/Airport Way (via NE Alderwood Drive, NE 82<sup>nd</sup> Avenue, and NE Airport Way) and I-205/Killingsworth (via NE Alderwood Drive, NE Columbia Boulevard, and NE Killingsworth) interchanges. It should be noted that NE Columbia Boulevard is a Major Street and NE Killingsworth is a Regional Traffic Way and state highway. As such, the transportation impact analysis does not anticipate traffic to travel on local streets within the SAN coverage area.

**Effectiveness:** *Let's set aside the limited suitability for a moment. Is the southbound onramp really a significant bottleneck? No, not according to neighborhood residents on Oct 8. The real bottleneck – well-known by motorists & confirmed by the [2013 ODOT Corridor Bottleneck Study](#) – is located at the northbound onramp (where Killingsworth and Sandy converge). That report cites 3 hours of congestion at the NB ramp daily, but does not mention the southbound onramp.*

*One final point: According to the May 2012 traffic impact analysis, the widened onramp will handle 2.1% of the estimated 2,340 vehicle trips generated by this development. Two point one percent is 50 vehicles per day.*

**RESPONSE:** The TPR requires the improvement measure to be of system-wide benefit and consider those intersection significantly affected by the proposed land use action. While the I-205/Killingsworth Northbound On-Ramp experiences operational issues, it is not anticipated to experience traffic associated directly with the proposed land use action as the northbound I-205 destined traffic will utilize the I-205/Airport Way interchange due to the travel timing savings. As such, the City and ODOT concluded that the proposed I-205/Killingsworth Southbound On-Ramp improvement would provide both system-wide benefit and address traffic impacts associated with the land use action.

**Necessity:** NE Columbia Parkway (aka [capital improvement project PDT000046](#)) -- which connects Columbia Blvd. at NE 82<sup>nd</sup> to NE Killingsworth at NE 87<sup>th</sup> – was completed in 2008 (as the recession hit) for \$16.8 million. This four lane connector added significant capacity on the very route we're discussing. It was completed with significant public outreach. A third onramp lane was not part of that major recent project.

**RESPONSE:** The NE Columbia Parkway was a separate project and its' scope did not include the I-205/Killingsworth Southbound On-Ramp. The necessity for the I-205/Killingsworth Southbound On-Ramp is both documented in the RTP and the transportation impact analysis prepared for the Colwood application, and provides additional capacity to accommodate the increased traffic demand created by NE Columbia Parkway project.

### **SAN October 18<sup>th</sup> Email Response**

To respond directly to the transportation impact analysis figure questions raised by SAN in their email dated October 18<sup>th</sup>, we have listed the concern in *italics* and our response below in standard text:

#### *Analysis Accuracy:*

*We found a repeated internal inconsistency in figures in the Kittelson TIA regarding the southbound onramp intersection. Specifically, in Figures 4, 5, 9 & 10 westbound traffic leaving the n/b ramp intersection was 30-50% lower than the westbound traffic arriving at the s/b ramp intersection.*

*There is no intervening intersection where those vehicles could turn off. Presumably this was a transcription error of some sort.*

**RESPONSE:** Based on the questions raised in the Sumner Association of Neighbors email date October 18<sup>th</sup>, the Transportation Impact Analysis and figures referenced were reviewed. The northbound to westbound I-205 loop off-ramp onto NE Killingsworth loads traffic between these two study intersections (#15 I-205/Killingsworth Southbound Ramp traffic signal and #16 I-205/Killingsworth Northbound Ramp traffic signal), which accounts for the additional westbound traffic arriving at the I-205/Killingsworth Southbound Ramp signalized intersection that does not leave the I-205/Killingsworth Northbound Ramp signalized intersection. All intersections are based on actual traffic counts measured during typical weekday peak periods. As such, the transportation impact analysis and figures accurately reflects the correct traffic volumes.

### **ADDITIONAL MITIGATION PROPOSAL FOR CONSIDERATION**

While it is not typical practice to include off-site landscaping and screening as part of Transportation Planning Rule based mitigation treatments, the applicant is willing to plant up to \$10,000 in screening vegetation in the proximity of the I-205 southbound on-ramp. The planting, if deemed acceptable to the City and the Sumners Association of Neighbors, would occur outside the ODOT right-of-way and within property controlled by Sumners Association of Neighbors.



I trust this letter adequately addresses the comments raised by the Sumners Association of Neighbors regarding the proposed Colwood Comprehensive Plan and Zoning Map Amendment. Please feel free to contact me if you have any further questions.

Sincerely,  
KITTELSON & ASSOCIATES, INC.

A handwritten signature in blue ink, appearing to read 'M. Butorac', with a stylized flourish at the end.

Marc Butorac, PE, PTOE  
Senior Principal

## Parsons, Susan

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**From:** Sumner NeighborhoodAssn [sumner.neighborhood@gmail.com]  
**Sent:** Tuesday, October 22, 2013 11:50 AM  
**To:** Parsons, Susan; Krueger, Kurt; Frugoli, Sheila  
**Cc:** Harry Sanger; Stoll, Alison  
**Subject:** Letter to Portland City Council on LU 12-213885 CP ZC EN (Onramp for Colwood rezone)  
**Attachments:** 2013\_10\_22\_ColwoodLetterToCouncil\_Final.pdf

Hi Susan, Kurt and Sheila --

Attached is our NA letter to Council for tomorrow's second hearing on the Colwood rezone proposal.

I will be there tomorrow to testify in person on these points, as may or may not be allowed under the circumstances.

Please feel free to contact me with any questions.

Thanks!

Scott Somohano

chair and webmaster

Sumner Association of Neighbors (SAN)

[SumnerPDX.org](http://SumnerPDX.org)

503-254-1449

TO: Portland City Council  
1900 SW 4th Avenue  
Portland, OR 97201  
22 October 2013

FROM: Scott Somohano, chair  
4523 NE 89<sup>th</sup> Ave  
Portland OR 97220  
Sumner Association of Neighbors (SAN)  
[sumner.neighborhood@gmail.com](mailto:sumner.neighborhood@gmail.com)

## CASE FILE: LU 12-213885 CP ZC EN (Colwood)

Honorable Mayor and Portland City Commissioners –

Thank you for allowing testimony today. And thank you for asking staff to meet with our neighborhood association about the traffic mitigation plan for the Colwood rezone proposal.

I'd like to salute – and thank – Mara, Kurt and Don for visiting our neighborhood association (NA) board meeting on Tuesday, Oct 8.

### Filling in the big blank

At that meeting on Oct 8, Kurt provided valuable information that was not previously available. He answered an important question: “How did this onramp become the mitigation for the Colwood rezone 1.5 miles away?”

The answer had two parts: New state law (requiring collaboration between ODOT and PBOT) and a new SCOTUS ruling (limiting mitigation expenses asked of developers). Kurt also noted that this was the first big project since those two factors came into play. It was also the first time in his 8-10 years that he saw a traffic mitigation so far away from a site.

### Committee choice: Neither suitable nor effective nor necessary

With this information, it's easy to see what happened: The committee convened with a checklist:

1. Within a certain range
2. Within a certain dollar amount
3. Something they could build in one shot, instead of putting small sums into individual intersection projects

This onramp, as one of 1,100 items in Metro's 2035 regional transportation plan (RTP), checked the boxes. But it fails to be suitable for mitigating effects on local traffic, effective for reducing congestion, nor necessary for route capacity.

**Suitability:** Given the new industrial area resides north of the slough, the proposed traffic mitigation runs counter to the existing comprehensive plan 6A Policy – Regional, as stated:

*Encourage the use of major arterials for regional traffic and separate this traffic from local traffic.*

6A-1 Objective: Support the existing growth at the airport for industrial and airport-related services without increasing traffic through the neighborhood.

Strategies:

1. Encourage use of I-205 and Airport Way as the primary access routes to the airport, airport related and industrial uses north of the slough

Adding an additional lane to the Southbound I-205 on-ramp at Killingsworth would encourage additional traffic in an area where local traffic volume is already high.

**Effectiveness:** Let's set aside the limited suitability for a moment. Is the southbound onramp really a significant bottleneck? No, not according to neighborhood residents on Oct 8. The real bottleneck – well-known by motorists & confirmed by the [2013 ODOT Corridor Bottleneck Study](#) – is located at the northbound onramp (where Killingsworth and Sandy converge). That report cites 3 hours of congestion at the NB ramp daily, but does not mention the southbound onramp.

One final point: According to the May 2012 traffic impact analysis, the widened onramp will handle 2.1% of the estimated 2,340 vehicle trips generated by this development. Two point one percent is 50 vehicles per day.

**Necessity:** NE Columbia Parkway (aka [capital improvement project PDT000046](#)) -- which connects Columbia Blvd. at NE 82<sup>nd</sup> to NE Killingsworth at NE 87<sup>th</sup> – was completed in 2008 (as the recession hit) for \$16.8 million. This four lane connector added significant capacity on the very route we're discussing. It was completed with significant public outreach. A third onramp lane was not part of that major recent project.

In addition, our ex-transportation chair, Jacob Warren works professionally as a civil engineer. He says the traffic signal timing is part of any current issue:

*My observation of the problem (I drive through M-F at about 5pm) has been that there is adequate capacity on the onramp itself. The backup appears to be caused by a combination of inadequate queuing capacity on Killingsworth and signal timing that does not prioritize the right turn movement to the on-ramp. I often see the ramp in free flow condition while there is still a long backup on Killingsworth.*

**Traffic impact analysis data accuracy:** In reviewing the Kittelson TIA, we found a repeated internal inconsistency in figures in regarding the southbound onramp intersection. Specifically, in Figures 4, 5, 9 & 10 westbound traffic *leaving* the n/b ramp intersection was 30-50% lower than the westbound traffic *arriving* at the s/b ramp intersection.

There is no intervening intersection where those vehicles could turn off. Presumably this was a transcription error of some sort. But that's a significant unexplained flaw in the data on which to base this traffic mitigation recommendation.

[Our neighborhood's request: Move the mitigation to the two Alderwood intersections](#)

If the onramp proposal was an effective and suitable mitigation for the proposed industrial rezone on Cornfoot Road, Sumner Association of Neighbors would withdraw our previous testimony. However, the additional information shared by Mr. Krueger on Oct 8 confirms our initial reaction in June. This proposal is an oddity, as it turns out the first such proposal resulting not from the needs of the traffic flow or this proposal, but from the new rules guiding traffic mitigation costs and process.

With that context in mind, we must reaffirm our previous request: Please consider moving the traffic mitigation plan to NE Alderwood Road, at its intersections with NE Cornfoot and NE 82<sup>nd</sup>. This will:

- (a) Mitigate traffic both inbound and outbound

- (b) Mitigate an estimated 66% share of all industrial traffic related to the rezoned property whether it is bound for NE 82<sup>nd</sup> Ave, the Killingsworth onramps or the Airport Way onramps

Please note: We take no pleasure in opposing this traffic mitigation plan. The Colwood rezone is a worthy & noble project, and it has our support.

[Our neighborhood’s commitment: To work with ODOT and PBOT on livability mitigation](#)

However, if the city does not alter the traffic mitigation plan, the SAN board committed at the Oct 8 meeting to work with ODOT and PBOT during the design and construction phases of the onramp project to mitigate the following livability issues (with examples)

Livability issue	Example
Safety	Enhanced bicycle and pedestrian crossings at I-205 onramp and along Killingsworth/Columbia
Noise	Sound barriers, natural or artificial
Odor & air quality	Preserve SAN lot as a natural buffer zone
Visual screening	Trees, additional plantings on SAN lot
Pedestrian access across Killingsworth at NE 89th	Should the traffic mitigation go through as planned, Sumner residents would benefit from dedicated pedestrian access to the rezoned open space

At a time when Portland is extolled for its bike culture and green outlook, our little neighborhood is hidebound by three state highways. It may seem like just one lane on one onramp, but nothing poses a greater risk or defines the challenges for our neighborhood’s livability than these busy roadways.

Thank you again for the opportunity to share our views on this vital issue to our neighborhood and its livability.

Respectfully submitted,

Scott Somohano  
 chair, Sumner Association of Neighbors (SAN)  
 on behalf of our NA board

**SUPPORT PROPOSAL**

**DON GOLDBERG & THE TRUST FOR PUBLIC LANDS**

**COLWOOD GOLF COURSE LU 12-213885 CP ZC EN**

IF YOU WISH TO SPEAK TO CITY COUNCIL, **PRINT** YOUR NAME, ADDRESS, AND EMAIL.

NAME (print)	ADDRESS AND ZIP CODE	Email
✓ TERRY PARKER	P. O. Box 13503 97213 349 SW First Ave Portland, OR 97239	perkert2012@gmail.com
✓ Laura Campos-Gleason	1300 SW 5th Ave, Suite 3050 Portland 97201	midlewiwin@juno.com
✓ Steve Wells	5330 NE HOLMAN PORTLAND, OR 97218	swells@stammellcrow.com
✓ Erwin Bergman	4800 NE 72nd Avenue Portland, OR 97221	lbergman503@q.com
✓ Howard N. Kenyon	610 SW Alder, Ste 700 Portland OR, 97205	howard@hnkconnect.com
No MARC BUTORAC <sup>APPLICANT TERM</sup>	5536 W Maryland #4 Portland, OR 97217	mbutorac@kittelson.com
✓ Demi Espinoza	6899 NE Columbia Blvd, Pox 97218	demi@coalitioncommunitiescolor.org
✓ ALAN HIPÓLITO		ALAN@VENNEDW.org
✓ Victor Mered <del>TOMY DE FALLA</del>		TomyDeFalla@vennedw.org
✓ REY ESPAÑA <del>Kate Allen</del>	3308 N. Farragut St Pdx 97217	kadev@earthlink.net

**SUPPORT  
PROPOSAL**

**DON GOLDBERG & THE TRUST FOR PUBLIC LANDS**

**COLWOOD GOLF COURSE LU 12-213885 CP ZC EN**

IF YOU WISH TO SPEAK TO CITY COUNCIL, PRINT YOUR NAME, ADDRESS, AND EMAIL.

NAME (print)	ADDRESS AND ZIP CODE	Email
✓ Rich Gundersen	5717 NE 55 <sup>TH</sup> Ave. Portland OR 97218	richgund@comcast.net
✓ DON TARBUTTON	4653 NE KILLINGSWORTH ST, UNIT 27	don.tarbutton@gmail.com
✓ Jon Ostar	2407 SE 49 <sup>th</sup> Ave PDX OR 97206	jon@opalpdx.org
✓ Midge Purcell	2710 SE Long St Pdx	mpurcell@olpdx.org
✓ <del>KATE ALLEN</del> REG ESPANA	5135 NE COLUMBIA, POX 97219	REGESPANAPOX.ORG
✓ <del>VICTOR MENCHES</del> TOMY DETALCO	5136 NE 42ND AVE POX 97218	VICTOR@KOSVIE-MA.CAL.ORG
✓ Bob Sallinger	9151 NW Cornell Row	bsallinger@auribopstla.org
NO ✓ Ty Schweefferman	10 N. Russell Portland	tyschw@olpdx.org
✓ William Saunders	4111 Black Point Rd Honolulu, HI 96816	BRainbow@aol.com
✓ JASON MINER	135 SW SECOND AVE. PORTLAND, OR 97204	jminer@friends.org

**SUPPORT  
PROPOSAL**

**DON GOLDBERG & THE TRUST FOR PUBLIC LANDS**

**COLWOOD GOLF COURSE LU 12-213885 CP ZC EN**

IF YOU WISH TO SPEAK TO CITY COUNCIL, PRINT YOUR NAME, ADDRESS, AND EMAIL.

NAME (print)	ADDRESS AND ZIP CODE	Email
✓ Kayse Jama	908 North Rosa Parks Way PDX 97217	kaysejama@yahoo.com
✓ Donita Fry	5135 NE Columbia Blvd, 9728	donitastf@naya-pdx.org
✓ Marcelo MARCELO BONTA	10233 NW LONGWORTHY TER., PORTLAND 97229	mbonta@environmentaldefenders.org
✓ Evans Martin	6325 NE Fogelun St, PDX 97218	evans7martin@gmail.com
✓ JIM MIGNANO	704 SE 34TH AVE, PORTLAND 97214	jim@emmasgarden.org
✓ Joseph Santos-Lyon	ADANO	josephcapano.org
✓ ANDREW COLN	COLN C.	



**OPPOSE  
PROPOSAL**

**DON GOLDBERG & THE TRUST FOR PUBLIC LANDS**

**COLWOOD GOLF COURSE LU 12-213885 CP ZC EN**

IF YOU WISH TO SPEAK TO CITY COUNCIL, **PRINT** YOUR NAME, ADDRESS, AND EMAIL.

NAME (print)

ADDRESS AND ZIP CODE

Email

NAME (print)	ADDRESS AND ZIP CODE	Email
✓ DOUG LEISI	8511 N. PENINSULAR AV. PORT, OR <sup>97217</sup>	CANDACEBARR@PEOPLEPC.COM
ALISON STOLL	4415 NE 87 <sup>th</sup> AVE 97222	alison@denncoalition.org
✓ HARRY SANGER	4545 NE 83 <sup>RD</sup> AVE 97220	HARRY.SANGER@CRAFTBREW.COM

**Moore-Love, Karla**

---

**From:** Shelley Peters [peters.sm@comcast.net]  
**Sent:** Tuesday, September 24, 2013 7:51 PM  
**To:** Moore-Love, Karla; Parsons, Susan  
**Subject:** [User Approved] [Zip File Attached]LU 12-213885 CP ZC EN.

**Attachments:** cully neighborhood letter.pages.zip



cully  
hborhood letter.pdf

I am unable to be at the Colwood National Golf Course re-zoning hearing, but would like to contribute this letter to the conversation.

Thank you, Shelley Peters

\*\*\*\*\*

\*\*\*\*\* Please Do Not Reply To This Message \*\*\*\*\*

As per The City of Portland email policy, The email security appliance (SonicWall) has quarantined an email that contains a zip file attachment. For your protection this file has been placed in your personal Junk Box.

As always, please exercise caution when retrieving attached zip files. Unsolicited email or emails from an unknown source can contain files in the form of malware and viruses.

For questions please call the BTS Helpdesk at 503-823-5199

\*\*\*\*\*  
\*\*\*\*\*

9/24/13

Dear City Council,

I would like to express concern about Colwood National Golf Course having its zoning changed to industrial. I live 8/10th of a mile from this golf course. A change in zoning could result in increased pollution and noise impacts for the Cully Neighborhood in which I live. Many residents live within five miles of the golf course and would be impacted by increased industry in our neighborhood.

My father died from a type of cancer that was associated with chemicals he was exposed to in his job and I am acutely aware of the risks of environmental exposure to carcinogenic substances. Even businesses that wouldn't have negative effects on air or water quality may impact the liveability of our neighborhood with increased traffic or noise. The beauty of the golf course would undoubtedly be replaced with buildings or factories which are less attractive.

Residents of the Cully Neighborhood live in one of the more affordable areas of Portland but many (probably most) wouldn't be able to afford to move. Therefore, it is a major concern that our environment might become more polluted as a result of a zoning change. I have lived in this neighborhood for over 15 years. Please help preserve the environmental integrity of our neighborhood.

Thank you, Shelley Peters  
6720 NE Roselawn St.  
Portland, Or 97218

September 24, 2013

Don Goldberg  
Trust for Public Land  
808 SW 3<sup>rd</sup>, #570  
Portland, OR 97204

Mike Abbaté  
Portland Parks and Recreation  
1120 SW 5<sup>th</sup> Avenue, Suite 1300  
Portland, OR 97204

Re: Colwood

Mr. Goldberg and Mr. Abbaté:

We, the undersigned organizations, write to request that Trust for Public Land/TPL and Portland Parks and Recreation/PPR work with the **1-Parque, 1-Modelo Coalition** to adapt the Let Us Build Cully Park! model to the Colwood site.

The **1-Parque, 1-Modelo Coalition** is led by Verde, Hacienda CDC and the Native American Youth & Family Center/NAYA, three strong Cully-based nonprofits which represent and organize with people of color and low-income people. Since early 2012, Verde, Hacienda CDC and NAYA have regularly, constructively engaged the Colwood process, meeting many times with TPL and PPR, and have consistently presented the following messages:

- We greatly appreciate and are thankful for the hard work done by TPL to bring/preserve this significant environmental asset for the Cully Neighborhood and Portland.
- We strongly support the proposed zoning change. Verde, Hacienda CDC and NAYA testified in favor of the proposed zoning change before the Hearing Officer on 6.12.2013.
- We seek a commitment to adapt the Let Us Build Cully Park! model to Colwood. This model – implemented at the City's Cully Park property, directly across Columbia Boulevard from Colwood – educates youth, enhances the environment, involves hundreds of community members in Park design, and creates local jobs and diverse contracting opportunities. Established by a 2012 public-private partnership agreement between Verde (on behalf of the Let Us Build Cully Park! Coalition) and the City, this model has produced the following benefits:
  - *Community-led fundraising*, resulting in \$3 million in grant funding and in-kind support, including many new sources that had not previously supported park development.
  - *Community-based design*, involving 324 community members in FY13 (7.1.2012-6.30.2013) in Park design, including 126 youth, 143 low-income community members, and 231 people of color.
  - *Target Business Contracting*. FY13, 75% of the project's design contracting dollars were awarded to minority-owned and women-owned businesses. Over 60% of the project's construction contracting dollars were awarded to minority-owned businesses, women-owned businesses and nonprofits.
  - *Jobs*. FY13, over 20% of the project's construction wages went to local workers, and over 50% of the project's construction wages went to low-income people and people of color.

Our first two messages have been acknowledged and well-received by TPL and PPR. To date, neither organization has committed to adapting the Let Us Build Cully Park! model to Colwood.

This Let Us Build Cully Park! model is an acknowledged best practice which can lessen the City's acquisition and development costs for Colwood by leveraging significant outside funding, deeply engage community members in project design, provide direct economic benefits to Cully's low-income people and people of color through future mitigation and construction activities on Colwood's southern parcel, and help prevent involuntary displacement of low-income people. And, it is already happening right across the street.

We strongly urge TPL and the City of Portland to publicly commit to adapting this best practice to Colwood.

Commissioner Loretta Smith  
District 2  
**Multnomah County**

Jason Miner  
Executive Director  
**1000 Friends of Oregon**

Lee Po Cha  
Director  
**Asian Family Center**

Joseph Santos-Lyons  
Executive Director  
**Asian Pacific American Network of Oregon**

Marcelo Bonta  
Executive Director  
**Center for Diversity and the Environment**

Kayse Jama  
Executive Director  
**Center for Intercultural Organizing**

Julia Meier  
Director  
**Coalition of Communities of Color**

Mara Gross  
Executive Director  
**Coalition for a Livable Future**

Andrew Colas  
President  
**Colas Construction**

Cheryl Bickle  
Principal  
**Community Transitional School**

Evans Martin  
Volunteer  
**Cully Community Market**

Linda Castillo  
Division Director  
**El Programa Hispano**

Casey Barnard  
Program Director  
**Emerald Cities Portland**

Jim Mignano  
Executive Director  
**Emma's Garden**

Roberto Jimenez  
Executive Director  
**Farmworker Housing Development Corporation**

Scott Fogarty  
Executive Director  
**Friends of Trees**

Cassie Cohen  
Executive Director  
**Groundwork Portland**

Steve Messinetti  
President and CEO  
**Habitat for Humanity Portland/Metro**

Victor Merced  
Executive Director  
**Hacienda CDC**

Jimmy Hood  
President  
**Helensview Heights Homeowners Assn.**

Judy BlueHorse Skelton  
Faculty  
**Indigenous Nations Studies – PSU**

Kali Thorne Ladd  
Executive Director  
**KairosPDX**

Carmen Caballero Rubio  
Executive Director  
**Latino Network**

Tony Jones  
Executive Director  
**Minority Contractor Improvement Partnership**

Andrew Colas  
President  
**National Association of Minority Contractors – Oregon**

Matthew Morton  
Executive Director  
**Native American Youth & Family Center**

Howard N. Kenyon  
Senior Program Manager  
**NE Emergency Food Program**

Jon Ostar  
Executive Director  
**OPAL**

Eli Spevak  
Owner  
**Orange Splot**

Andrea Durbin  
Executive Director  
**Oregon Environmental Council**

Connie Ashbrook  
Executive Director  
**Oregon Tradeswomen Inc.**

Cyreena Boston Ashby  
Director  
**Portland African-American Leadership Forum**

Maxine Fitzpatrick  
Executive Director  
**Portland Community Reinvestment Initiative**

Donita Fry  
Organizer  
**Portland Youth & Elders Council**

Ron White  
Owner  
**Probity Builders & Best HQ**

Kristie Lindholm  
Principal  
**Rigler School**

Nick Sauvie  
Executive Director  
**ROSE CDC**

Verenice Gutierrez  
Principal  
**Scott School**

Gerald Deloney  
Director of Program Advancement  
**Self Enhancement, Inc.**

Michael Alexander  
President & CEO  
**Urban League of Portland**

Alan Hipolito  
Executive Director  
**Verde**

Romeo Sosa  
Executive Director  
**VOZ Workers' Rights Education Project**

September 25, 2013

Mayor Hale, Commissioners

My name is Erwin Bergman; I have lived 43 years in the Cully neighborhood at 5330 NE Holman.

I am responsible for resolving Quality of life issues for the Cully Association of Neighbors (CAN).

I am here to testify in support of the rezoning of the northern 48 acre parcel of Colwood Golf Course to an Industrial IG2 zone, with the remaining 90 acre southern parcel remaining open space, ultimately to become a City park.

My testimony reflects the August 21 findings and recommendation of Gregory Frank, the Hearing Officer, in his 140 page report. I, and others from CAN, had testified at that hearing.

At the September monthly CAN meeting I summarized rezoning details, and CAN members present (approximately 40) unanimously voted to support rezoning. Obviously I and other CAN members would have preferred to have the entire golf course property remain open space to become a park, the overriding concern being IF and WHEN. As you may remember, CAN efforts to get a Colwood park go back to 2007. It appears now to become a reality soon. Surrounded by 5700 acres (9 square miles) of INDUSTRIAL SANCTUARY, a 90 acre sanctuary for people and animals should acknowledge to some degree that people have a need and greatly benefit from getting away from it all for at least a few hours and enjoy the few remaining vestiges of nature such as the Columbia Slough environs.

Last, but not least, I would like to express my deep gratitude to the Trust for Public Land (TPL) and especially to Mr. Don Goldberg; to grab the opportunity to work out a deal with Colwood owners and to now be in the position to deliver an exquisite piece of Portland history to become a jewel in our neighborhood for all of Portland. Thank you!

After you hopefully approve the rezoning plans, in time management and eventual ownership of Colwood will transition from TPL as the contract owner now to the Bureau of Parks. During such time TPL's goals, their care and efforts must not be abandoned or diluted. With Colwood being identified as a SPECIAL HABITAT AREA, an important Wildlife Connectivity corridor and also an important element in the Portland Trail System, a high and diverse level of planning expertise is essential under the guidance of TPL as the new owner and the Bureau of Parks as the future owner and caretaker.

*Erwin Bergman*



Asian Pacific American Network of Oregon

(971) 340-4861

info@apano.org

www.apano.org

  @apanonews

September 25, 2013

LV 12-2138885-CP

2C-EN

Mayor Hales  
Commissioner Saltzman  
Commissioner Fritz  
Commissioner Fish  
Commissioner Novick

Dear Mayor and Commissioners,

My name is Joseph Santos-Lyons, Executive Director of APANO, the Asian Pacific American Network of Oregon. I'd like to highlight two values of our organization that brings us here today.

The first is the value that strong relationships are the foundation for meaningful change. Our experiences as Asians and Pacific Islanders inspire our partnerships with each other and promote our coalition building in order to find solutions to shared concerns.

The second value is about leadership, and how leadership must emerge from, be reflective of, and be accountable to the diverse needs, interests and heritages of our communities.

APANO has endorsed the Colwood Letter. We support the zoning change because it can produce equity benefits. It is entirely reasonable to expect the Trust for Public Land and the City of Portland to work with Verde and their partners to generate those benefits.

As you no doubt know, Cully lacks public parks.

Nearly half of Americans get less than the recommended minimum amount of physical activity—more than one-third engage in no leisure-time physical activity at all. In the movement to improve the health and wellness of adults and children across the country, parks have a critical role to play.

Our community needs this park, and yet without meaningful partnerships with community based organizations such as Verde, we know from our history that equity issues fall short.

I want to take a moment to urge us all to think about the value of community-driven solutions. In our city, we have a rich history of leadership from grassroots communities. As we



become more racially and culturally diverse, I ask us to be even more intentional in developing solutions that come from our communities. Our communities are well served by solutions that come from people who are most affected by the problem.

When we as city leaders engage in community-driven solutions, we empower and transform communities. There are many benefits from this relationship and cycle -- and today you have before you an important opportunity.

We request that the City of Portland work with the One Park One Model Coalition that includes Verde, Hacienda CDC, NAYA and other partners to adapt the Let Us Build Cully Park Model. This is an opportunity to develop a public-private partnership for the acquisition, design and construction of Colwood's Southern parcel that is alignment with community needs.

This request recognizes and taps the power and creativity of community members to realize their own vision for a better neighborhood, and provides a clear mechanism for community and government to work together.

This request is consistent with the city's equity goals and policies. Community members and community organizations have come together to present a consensus equity vision/position, and are reaching out to the city for recognition of that vision/position and to work together with the community to make it happen.

Thank you

Rev. Joseph Santos-Lyons  
Executive Director



Coalition of  
Communities of  
Color

September 25, 2013

City Council 1221 SW 4th Avenue Room 110, 97204

LU 12-213885 CP ZC EN Colwood Golf Course

Dear members of the City Council,

I'm writing to you on behalf of the Coalition of Communities of Color. The mission of the Coalition of Communities of Color (CCC) is to address the socioeconomic disparities, institutional racism, and inequity of services experienced by our families, children and communities; and to organize our communities for collective action resulting in social change to obtain self-determination, wellness, justice and prosperity. Formed in 2001, the CCC is an alliance of culturally-specific community based organizations in the Portland metropolitan region with representatives from six communities of color: African, African American, Asian/Pacific Islander, Latino, Native American and Slavic.

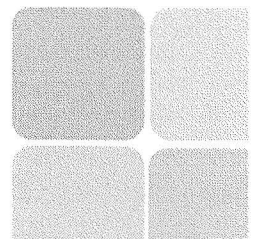
The Coalition of Communities of Color is a signatory to the Colwood letter, and we support the 1-Parque 1-Modelo Coalition's efforts to adapt a community-based approach to building a neighborhood park by and for low-income people and people of color. The model produces outputs that are all in line with the CCC's community and economic development subcommittee strategic plan goals, which include: Workforce diversification, 2. Access to capital and investments for minority-owned businesses, 3. Strategizes to combat displacement and 4. Participation in sustainable initiatives.

We appreciate the Trust for Public Land's dedication and hard work to bring the zoning change forward. Now, we seek a commitment from the City of Portland Parks and Recreation Bureau to adapt the *Let Us Build Cully Park!* model to Colwood by working with 1-Parque, 1-Modelo Coalition. The CCC supports the adaption of this model because it increases sustainability /environmental funding and investment in communities of color. Replication of the Cully Park Model means we can provide better opportunities for communities of color in our area by providing access to contracting jobs and local hiring to increase economic opportunities.

This model re-instills community pride through a collective design process and community stewardship of the park. We thank the Trust for Public Land for their hard work so far. We support the efforts made by 1-parque, 1-Modelo Coalition, and urge the City of Parks and Recreation to do the same.

Respectfully,

The Coalition of Communities of Color



# 922

Council Clerk  
1221 SW 4<sup>th</sup> Room 140  
Portland, OR 97204

AUDITOR 09/25/13 AM 10:00

To Whom It May Concern:

I am writing in reference to case file number: LU 12-213885 CP ZC EN. My name is Jimmy Hood and I am a volunteer Garden Manager at the 72<sup>nd</sup> Ave Community Garden. Since the Garden's grand opening of last year I have seen a dramatic shift in the dynamics of the Cully Neighborhood where I live. Before the Gardens were built the normal foot traffic in that area consisted of criminal activities that left no room of safety for pedestrians or a healthy environment for children. However, with the model created by Verde and Parks and Recreation we now have an arena where families of diverse cultural and economic backgrounds can provide themselves with affordable organic produce. The model that's been used to create the community garden is a solid example of success in regards to providing a healthy environment for community growth. Where there once was an empty lot that layed invitation to criminal activity is now an area where Cully residents can gather for many generations in the future. I hope that this testimony on the success of the 72<sup>nd</sup> Ave Community Gardens will be the inspiration to the City Council to please help us replicate this model for Colwood. At long last this area of Portland is receiving the attention it needs for positive growth. Please help us maintain that vision for a better future for this area.

Sincerely,

Jimmy Hood

TO: Portland City Council  
1900 SW 4th Avenue  
Portland, OR 97201  
20 September 2013

FROM: Scott Somohano  
4523 NE 89<sup>th</sup> Ave  
Portland OR 97220  
chair and webmaster  
Sumner Association of Neighbors (SAN)  
[sumner.neighborhood@gmail.com](mailto:sumner.neighborhood@gmail.com)

**CASE FILE: LU 12-213885 CP ZC EN (Colwood)**

Honorable Mayor and Portland City Commissioners –

Thank you for receiving my written testimony. I regret that I cannot be there to testify in person on September 25, 2013 about this matter.

As a layman and volunteer chair of the Sumner Association of Neighbors (SAN), I represent the neighborhood between NE 82nd and I-205 on Killingsworth Street, including the southbound onramp to I-205.

The Sumner neighborhood board and I support the overall proposal to rezone part of Colwood Golf Course. We applaud this effort and our friends and neighbors in Cully neighborhood for a job well done.

However, the traffic mitigation portion of this rezone proposal is not acceptable. As currently written, the traffic mitigation plan is located entirely in Sumner neighborhood – over 1.5 miles away from the proposed rezone site on Cornfoot Road. It requires adding a third lane to the southbound onramp from Killingsworth onto I-205.

The traffic mitigation portion of the rezone proposal is unacceptable because approval criteria requiring “Neighborhood Involvement” and “Careful planning to enhance neighborhood livability” were not met. At all.

It’s true that Cully neighborhood was consulted, but this traffic mitigation proposal is in our neighborhood, and directly impacts our livability, not Cully’s.

More completely: Our neighborhood association, our residents and our businesses were never contacted, never consulted -- and apparently never even considered -- as explicitly required by approval criteria. Not by the applicant. Not their traffic engineering firm. And not by any of the government agencies (BDS, PBOT, ODOT) who crafted and vetted this traffic mitigation plan, even though the plan is entirely located in Sumner neighborhood, not Cully neighborhood.

Sumner works hard to be good neighbors. Some of you already know us as good neighbors. We won the Spirit of Portland award in 2010. Commissioners Fritz and Fish helped us celebrate the opening of new community garden this March. But in this case, we simply did not exist.

On June 20, I learned about the onramp widening proposal via The Oregonian newspaper. On June 26, 2013, I submit comments on behalf of Sumner neighborhood to the Hearings Officer citing five specific approval criteria that the traffic mitigation proposal did not satisfy. His final report makes no mention of any of my comments, and changed nothing related to transportation criteria from the original report.

After submitting comments, I contacted people in the Governor's office, PBOT, ODOT and BDS about this traffic mitigation plan. None of them – not one – could give me a direct answer about why traffic should be mitigated at this outbound onramp in our neighborhood rather than at or near the site itself.

In other words, I – a volunteer – conducted all of the outreach in reverse! Despite not being able to explain the relation of this onramp widening to the property 1.5 miles away, not one official offered to change their agencies support. None offered to consider changing their support. Mr Rian Windsheimer at ODOT Region 1 said it's in the city's hands. (And so here we are.)

Only the applicant, Mr Cerbone, in his written response dated July 23, offered any explanation whatsoever: That "additional improvements are planned over the 20-year planning horizon." None of those additional improvements are part of this traffic mitigation proposal. They may or may not happen someday.

In contrast, the proposal requires widening of the onramp – which is located directly adjacent to residential dwellings – before any permits can be issues to develop the rezoned property.

Other approval criteria require directing traffic from north of the slough – as this property is on Cornfoot – to the I-205 Airport Way interchange. That's explicitly meant to prevent increased traffic through neighborhoods. Yet this mitigation plan directs extra traffic through our neighborhood instead. And ODOT is being encourage to explore adding "one additional traffic lane to Columbia/Killingsworth between NE 82<sup>nd</sup> and I-205."

Even on a practical level, the benefit of this onramp widening is questionable. Specifically:

1. It aids only outbound traffic from the new industrial site, not inbound traffic.
2. One eastbound lane of Killingsworth is already a queuing lane for the southbound onramp. It does not cross the overpass into Parkrose.
3. At six lanes, Killingsworth is already wider than any of the arterials leading to it from Cornfoot (NE 82<sup>nd</sup>, Columbia Blvd, and Alderwood).

**Sumner neighborhood requests** that the current traffic mitigation plan be replaced by mitigation at or much closer to the rezone site itself (on Cornfoot Road and at Alderwood Road). Such a change would both improve the efficiency of the mitigation plan and reduce its impact on Sumner neighborhood.

**Failing that, as a backup, Sumner neighborhood requests** that the current traffic mitigation plan be amended to explicitly require neighborhood involvement and careful planning to enhance the livability of residents whose homes and livability will be directly impacted at NE Killingsworth and I-205.

Again, Sumner wants to be good neighbors. Please give us the chance denied to us before it's too late.

Respectfully submitted,  
Scott Somohano  
4523 NE 89th Ave  
Portland, OR 97220



Additional written testimony:  
Scott Somohano, Sumner Association of Neighbors  
4523 NE 89<sup>th</sup> Ave, Portland, OR 97220  
Submit on 20 September 2013

**CASE FILE: LU 12-213885 CP ZC EN (Colwood)**

Three photos with captions



**26 June 2013: Facing south toward I-205 onramp on NE Killingsworth.** Note proximity of house on the right. There are no residential dwelling at Cornfoot/Alderwood/NE 82<sup>nd</sup> near the rezone site itself.



**26 June 2013: Facing east on Killingsworth at the s/b I-205 onramp.** Please note the dwelling on the left. Also please note the first lane on Killingsworth already serves only as a queuing lane. It does not continue east across the overpass.

**4 August 2013: Facing east on NE Killingsworth.** Note that the signage and road striping indicates that the first lane is designated only for southbound onramp use.

