



CON-WAY MASTER PLAN

MASTER PLAN DOCUMENT

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ORIGINAL SUBMISSION: APRIL 24, 2012

GBD ARCHITECTS *Incorporated*

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OVERVIEW
CON-WAY MASTER PLAN

OVERVIEW

Con-way Inc. traces its history to Leland James, who, in 1929, founded a small regional trucking company in Portland, Oregon, named Consolidated Truck Lines. The company enjoyed great success through expansion and acquisition, and was later renamed Consolidated Freightways (CF).

Today, Con-way delivers industry-leading services through its primary operating companies of Con-way Freight, Con-way Truckload, and Menlo Worldwide Logistics. These operating units provide industry-leading regional, inter-regional and national less-than-truckload (LTL); logistics, warehousing and supply chain management services; as well as full truckload freight transportation to the best companies across North America and around the globe. Con-way and its subsidiaries operate from more than 500 locations across North America and in 20 countries across five continents.

CON-WAY BUSINESS GOALS

Con-way has several business goals. Through responsible business practices, Con-way hopes to accomplish the following:

- Create a lasting community legacy through enhanced sustainability, livability, and design.
- Design a place which helps retain and attract the best and brightest employees – a place where people can live, work, shop and play.
- Develop effective and feasible parking and transit strategies for Con-way employees and other users and visitors.
- Create value for the Con-way shareholders.
- Promote economic and job growth in Portland.

CON-WAY COMMUNITY GOALS

Although Con-way's headquarters are now located in Ann Arbor, Michigan, they still control 25 acres in Northwest Portland, and will continue to maintain a presence in two large office buildings they currently occupy. Con-way hopes to create a Master Plan development framework and then sell the parcels to private developers for redevelopment. Con-way does

not plan to act as a developer on any of the parcels. Development will be mixed-use and market driven, and the Master Plan must be complete for land sales to take place. Through the Master Plan and development framework, Con-way hopes to meet the following community goals:

- Design a mixed-use quarter that includes employment, housing, retail, open space and community uses.
- Embrace sustainable planning, design and construction in all facets of the development.
- Support a multi-modal transportation system that accommodates auto, pedestrians, bicyclists and transit.
- Enhance the livability of the entire area.
- Create memorable places and spaces that give the site a distinct character and offer places for public activity.
- Encourage both market and affordable family housing.
- Pursue partnerships with government, institutions and other groups to achieve common objectives.

NEIGHBORHOOD PARTNERSHIP

Con-way's property is located within the boundaries of the Northwest District Association (NWDA). The NWDA is a city-recognized neighborhood organization that represents portions of northwest Portland. NWDA provides advise to the city of Portland, its Bureaus, Commissions and Council on affairs affecting the character and quality of life in the neighborhood.

Con-way has actively engaged with the NWDA through the development of the Con-way Master Plan document, to accomplish the following goals:

- Integrating the vision of the Slabtown Urban Design Concept, an NWDA planning tool that looked to enhance the Northwest District plan.
- Weekly meetings with the NWDA looked to achieve an open and collaborative process.
- Collaboration looked to focus on both quantitative (heights, density, uses, open spaces and massing) and qualitative (quality and character) issues.



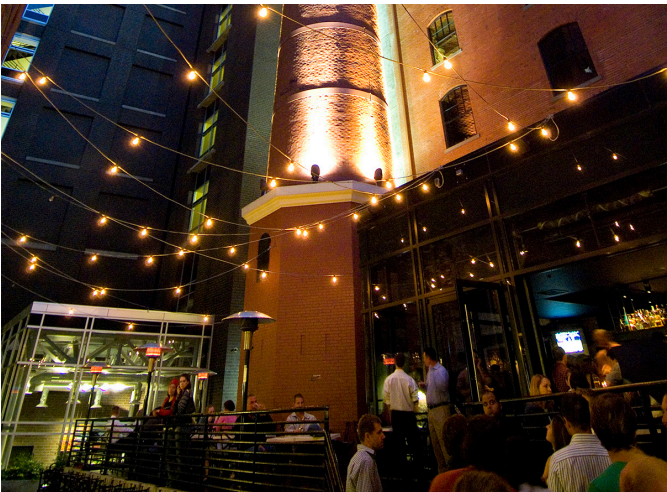
One of Con-way's two office buildings in Northwest Portland.



Con-way employees.



Con-way hopes to accommodate for opens spaces like Pioneer Courthouse Square within the Master Plan framework.



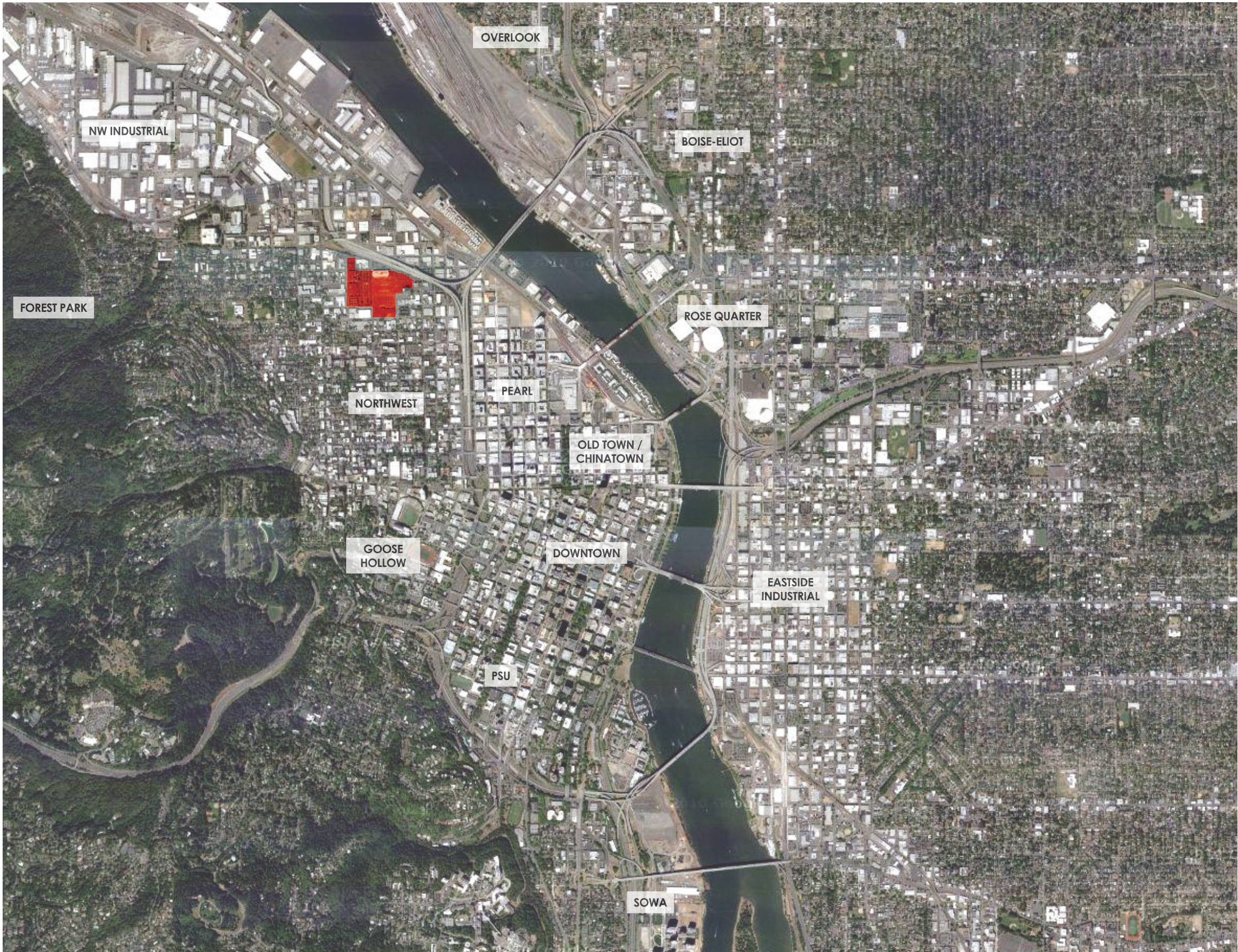
Con-way hopes to accommodate for mixed-use areas and uses within the Master Plan framework; similar to those in Portland's Brewery Blocks.

MOMENTUM

Momentum in the marketplace for urban infill development has increased significantly in recent time, giving Con-way the confidence that the time is right for viable development to occur on its property. Several parcels are under contract today, awaiting completion of this Master Plan—the first of which will be highly catalytic with the inclusion of a neighborhood grocery store as a key element of a large, mixed-use project. Establishing and maintaining momentum for desired development within this market-driven window of opportunity is key to the viability of implementing this overall master plan.



Several Con-way parcels are under contract, awaiting completion of the Master Plan.



VICINITY MAP

The Con-way property is located adjacent to the Northwest Industrial district and is located in the northern portion of the Northwest District, a densely populated residential neighborhood.

The site is located at the intersection of Highway I-405, Highway 30, and the Fremont Bridge.



NEIGHBORHOOD IMAGERY

These images represent the character of the surrounding neighborhoods. There is an eclectic mix of housing retail, and some office. There is a great diversity in the age and materiality of the architecture of the surrounding areas.





SITE / CONTEXT IMAGES

These images represent the character of the surrounding context in the blocks directly surrounding the Con-way property.

SECTION 1

BOUNDARIES

33.562.300.D

1. Boundaries. The boundaries of the area to be included in the Northwest Master Plan. The area must include all contiguous lots within the area shown on Map 562-9 that are owned by the same person, partnership, association, or corporation. This also includes lots that are in common ownership but are separated by a shared right-of-way.

12

MAP 01-1 MASTER PLAN AERIAL

13

MAP 01-2 MASTER PLAN BOUNDARY



BOUNDARIES:

0 01-1 describes the limits of the site to be considered for this Master Plan application. The proposed Master Plan boundary is shown in red.

All of the parcels within the limits described are presently owned and controlled by Con-way.

The aggregate site area contained within these limits, excluding current right-of-ways, is 762,138 sq. feet, or 17.49 acres.

Aerial image with Master Plan boundary overlaid. Block numbers are shown in red.

MAP 01-1
MASTER PLAN AERIAL

SECTION 2

OVERALL SCHEME

33.562.300.D

2. Overall Scheme. An overall scheme, including both written and graphic elements, that describes and ties together existing, proposed, and possible development and uses, height and massing of development, phasing of development, review procedures for each development or phase, and what standards, guidelines, and approval criteria will be used to evaluate each development or phase.

15	OVERALL SCHEME SUMMARY
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19	MAP 02-2 OPEN SPACE PLAN
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21	MAP 02-4 PHASING DIAGRAM
22	MAP 02-5 STREET PLAN MAP 02-6 POTENTIAL STREETCAR ALIGNMENT

SECTION 2 OVERALL SCHEME

Con-way owns all of the land parcels within the boundaries described in the preceding section (Section 1 Boundaries). New development within these boundaries is subject to required Master Plan criteria in accordance with Chapter 33.562.300 of the Northwest District plan.

Con-way no longer needs all of their land to support its Portland-based business operations. They seek to sell unneeded parcels to interested developers for private development. Con-way does not intend to develop additional buildings for its own use nor does it intend to participate as a developer on any of the properties it may sell. Accordingly, new development will be strongly influenced by market forces. Con-way does, however, intend that these properties be developed in a manner that generates a vibrant mixed-use urban environment. They intend to control the overall development in a manner that both in-fills and complements the densely-populated and richly-established urban neighborhood that surrounds it. This Master Plan is intended to serve as a framework for each new development.

This section is a summary of the overall Con-way Master Plan described in this application. It includes both narratives and graphic elements that describe existing and proposed uses, height and massing (densities) of development, phasing, review procedures for each stage of development and a description of design standards, guidelines and approval criteria proposed.

EXISTING & PROPOSED USES

The Con-way Master Plan integrates new mixed-use development with existing commercial office and warehouse buildings. Map 02-1 illustrates locations of existing buildings and sites where new development (or re-development) may occur.

Con-way intends to maintain its presence in and ownership of its present office facilities on Blocks 293 and 294. Possible future uses on other properties are proposed to include all of those allowed under the base zone as modified by 33.562 Northwest Plan District, intended to provide for an urban level of mixed-use development including commercial, office, housing and employment. Public and institutional uses are also possible but are expected to be part of mixed-use building developments if they are to be realized.

The entire Con-way Master Plan property will remain subject to all applicable provisions of the City of Portland Zoning Code, including the base zone regulations, except as follows:

- The alternative design review procedure provided in Chapter 33.218.015 shall not apply;
- Underlying base zone allowed uses for the property zoned IG1 on Block 16 shall be expanded to also include all allowed uses for the EXd zone;
- Parking standards of Chapter 33.266 shall apply except for instances wherein there is a conflict between Con-way parking ratios provided in Section 5 of this Master Plan and Chapter 33.266; and in those cases, the Con-way parking ratios provided in Section 5 shall supersede Chapter 33.266 and shall control; and
- If conflicts occur between the NW Master Plan and the City of Portland Zoning Code requirements, the requirements of the NW Master Plan shall prevail.
- Interim phase neighborhood square development shall not be subject to Design Review.
- Parcels owned by Con-way are not subject to non-conforming upgrades per Chapter 33.258.
- Amendments to the Master Plan will be processed pursuant to Section 8.

New open space uses where illustrated on Map 02-2 are a significant component of Con-way's Master Plan. Approximately 25% of the total land area owned by Con-way is designated to become open space as a part of this application. All open space will be accessible to the public. These spaces have not yet been designed but guiding principles have been established for their future development as described in section 5.

Block 290 will be the site for two of the major open spaces being proposed. A neighborhood park will be located on the easterly portion of this block—property that may be conveyed to the Parks Bureau. A privately owned and developed, but publicly accessible, urban square will be located on the westerly side of this block. The square will be fronted on two sides by mixed-use buildings with ground-level, commercial retail uses; these will be complementary to the varied functions and activities that will occur in the square.

HEIGHT AND MASSING (DENSITIES)

Consistent with Con-way's approach to development described above, specific building heights and dimensional characteristics for each proposed new structure will be described at the time development applications are submitted for each project. Map 02-3 describes the maximum heights that are allowed within the Master Plan boundary.

Massing is carefully addressed to ensure that new structures are compatible with desired neighborhood characteristics via a series of Design Standards and Guidelines described in Section 5. These criteria attempt to balance desired densities with livability and positive urban qualities, with a strong emphasis on the quality of the pedestrian realm.

The development program described below achieves an overall density (floor area ratio / FAR) of 3:1 throughout the Con-way Master Plan area. This density level is

currently allowed in the Northwest Plan district for residential uses only. To enable development to achieve a truly vibrant mixed-use environment, the Con-way Master Plan expands the mix of uses allowed so that commercial, office, employment and other allowed uses be allowed at a 3:1 FAR. Provisions explaining the uses that are allowed in the Con-way Master Plan are described in Section 5, Development and Design Standards and Criteria, of this application.

At 3:1 FAR, the overall development program for this Master Plan is as follows:

- Total FAR = 2,280,850 square feet
- Existing Floor area = 330,850 square feet (Includes floor area for existing buildings on Blocks 293 and 294, Adtech I and Adtech II respectively. See Appendix.)
- New floor area = 1,950,000 square feet

Proposed allocations of FAR between uses are described in Section 3, bearing in mind that these are approximate allocations that will ultimately be market driven.

PHASING OF DEVELOPMENT

Map 02-4 describes the proposed project phasing. As described in the introduction to this section, Con-way will sell its vacant properties to a variety of developers upon completion and adoption of this Master Plan. Development is anticipated to be largely private in nature for both commercial and residential uses. Civic and/or institutional uses are also possible but will likely be included as a component of larger mixed-use buildings that might stem from public/private development partnerships.

Given these parameters, market forces will play a significant role in determining the phasing and timing of the overall development. Market needs, economics and developer capacity will clearly influence the pace and viability of every parcel. As such, Con-way cannot make any specific commitments at this time for the eventual placement, program or timing of any of the individual parcels within the Master Plan boundary.

A key driver for phasing will be the integration of replacement parking for Con-way. Ultimately, all of the surface parking currently serving Con-way will be displaced by vertical development. The parking that serves this new vertical development will likely need to be co-mingled with replacement parking for Con-way, anticipated to be in underground structures that underlie new mixed-use or commercial buildings on properties that adjoin Con-way's office current office buildings. For this reason we expect that properties that are closest to Con-way's current buildings will be developed in the latter phases of development and, those perimeter properties furthest from Con-way's current offices, will be developed in the earlier phases.

One of these perimeter properties is block 296, a parcel that is under contract and ready to begin detailed design upon approval of this Master Plan application. The project is catalytic in that it involves a neighborhood grocery store as its key program element.

REVIEW PROCEDURES

Con-way proposes to use the City's existing Design Review process for review and approval of all future development proposals within the Con-way Master Plan area. While they are carefully controlling the disposition of their property to be in keeping with goals established by Con-way and the neighborhood, they do not intend to overlay a separate Owner's design review process.

DEVELOPMENT AND DESIGN STANDARDS

DESIGN STANDARDS

The standards and design guidelines described in Section 5 apply to new development and major remodels of existing buildings in the NW Master Plan Area and are in addition to the base zone standards, the requirements of Chapter 33.562-Northwest Plan District, and the Community Design Guidelines. They address the character and quality of development that are derived specifically from goals established by both Con-way and the neighborhood.

Following are summarized descriptions for Design Standards included in this NW Master Plan application:

1. Maximum height - Approximately 25% of the total land area owned by Con-way is designated to become open space as a part of this application. In order to accommodate desired densities on the reduced buildable land area remaining, maximum heights have generally been increased over NW Plan District standards. These heights are described on map 05-1. Maximum heights have also been adjusted to accommodate taller standards and guidelines for ground floor heights.
2. Maximum and minimum Floor Area Ratio (FAR) and uses - The maximum 3:1 FAR allowed in the NW District Plan currently applies to residential uses only. Commercial uses are capped at 1:1 FAR. This Master Plan application requests approval for all permitted uses to be built to 3:1 FAR. It also requires all development to meet a minimum FAR of 1.5:1.
3. Retail sales and service uses - This Master Plan application request approval for a single retail use (grocery store) of no more than 40,000 square feet.
4. Transfer of floor area within the NW Master Plan area - The Master Plan makes provision for FAR to be freely transferred between sites within the Master Plan limits, provided an overall cap of 3:1 is maintained for the limits of the plan area. Sites do not have to be abutting.
5. Neighborhood facilities within the NW Master Plan area - As an incentive to developers, FAR from certain neighborhood facilities will not count against the maximum FAR allowed on the site.
6. Required building lines - Certain building frontages require that building faces be built to the lot line to enhance the urban quality of these locations - as shown on Map 05-4.
7. Special ground floor retail sales and service, or neighborhood facility uses and setbacks on NW 21st Avenue - Retail uses are required along NW 21st frontages to reinforce a lively and vibrant public realm and extends the retail environment that exists to the south. It also requires taller ground floor heights and other characteristics conducive to good retail space.
8. Standards on streets, squares, parks, and other open spaces - These standards focus on urban characteristics for ground floor development that are active, interesting and pedestrian scaled.
9. Parking standards - These standards describe a sliding ratio of maximum parking spaces that are allowed. The ratios reduce as more transit is provided in the neighborhood.
10. Square standards - These standards describe requirements associated with development of the square on Block 290.

DESIGN GUIDELINES

Design guidelines are mandatory approval criteria that must be met as part of design review and historic design review. They inform developers and the community as to what issues will be addressed during the design review process. The guidelines state broader concepts than typical development standards in order to provide flexibility to designers, yet they are requirements. Applicants are responsible for explaining, in their application, how their design meets each applicable guideline.

Following are summarized descriptions for Design Guidelines included in this NW Master Plan application:

- 1. Provide human scale to buildings and edges along sidewalks, squares and pedestrian accessways.
- 2. Develop urban edge variety adjacent to parks, pedestrian accessways and greenstreets.
- 3. Develop weather protection.
- 4. Develop buildings that are appropriately scaled to the neighborhood. Façades should be well articulated and offer diversity in volume and form along the street edge.
- 5. Provide transitions between the public and private realms when residential structures abut streets, parks and pedestrian accessways.
- 6. Integrate high-quality materials and design details.
- 7. Open space standards.
 - A. Provide private open spaces that are well integrated with adjacent development, act as gathering places designed to adapt to a variety of activities, are linked together and to other nearby open spaces, are accessible to the public and provide distinctive neighborhood identity.
 - B. Square. Design the square to be a significant iconic urban place and include commercial focal points as adjacent uses.
 - C. NW Quimby Parcel. Provide a multi-use street and open space that links the neighborhood park and square to the south and development to the north, and serves primarily as a pedestrian and bicycle connection.
 - D. Pedestrian accessways. Provide a network of pedestrian accessways that, together with public greenstreets and building forecourts, form a special pedestrian circuit or network of connected open spaces in the neighborhood, in addition to providing pedestrian access to adjacent development.
 - E. Building forecourts. Provide building forecourts on specific blocks that serve as multi-use outdoor spaces open to the public.
 - F. Pocket park. Provide land for a small pocket park west of St. Patrick's Church.

DESIGN PRINCIPLES FOR NEIGHBORHOOD PARK

In lieu of BDS Design Review, a public process will determine the programming of uses and activities, concepts and design of the neighborhood park. Con-way will undertake a developer solicitation/design process, closely coordinated with the parks design process, for the square and related development.

- 1. Provide a neighborhood park designed as a green space in an urban neighborhood; integrate the park with the square, adjacent streets and abutting development.

TRANSPORTATION

Map 02-6 describes the proposed street plan and potential Streetcar alignment in concert with proposed open spaces, illustrating that the established street grid will be restored where it has been compromised by past uses. Development criteria have been carefully written to support the desired nature of designated Neighborhood Community Main Streets and Local Service Streets.

Negotiations to bring an extension of the Portland Streetcar system to serve the Con-way Master Plan area are actively underway. Routes where shown are preliminary and may happen on a phased basis, setting the stage for future expansions to extend westward to Montgomery Park.

Section 6 describes the results of a required Multi-Modal Transportation Impact Study prepared in concert with and informing this Master Plan application. it addresses several key transportation issues including:

- Location and amount of motor vehicle and bicycle parking.
- Strategies to reduce the number of motor vehicle miles traveled
- Planned improvements to routes used by transit patrons
- A street plan providing multi-modal street connections
- A multi-modal transportation impact study
- Identification of traffic impacts, mitigation measures and other recommendations

KEY

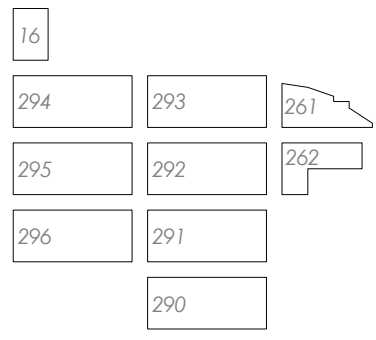
- EXISTING BUILDINGS TO REMAIN UNCHANGED
- NEW DEVELOPMENT SITES
- EXISTING BUILDINGS LIKELY TO BE RE-DEVELOPED OR DEMOLISHED

LOCATION, SIZE & DIMENSIONS OF EXISTING & PROPOSED STRUCTURES

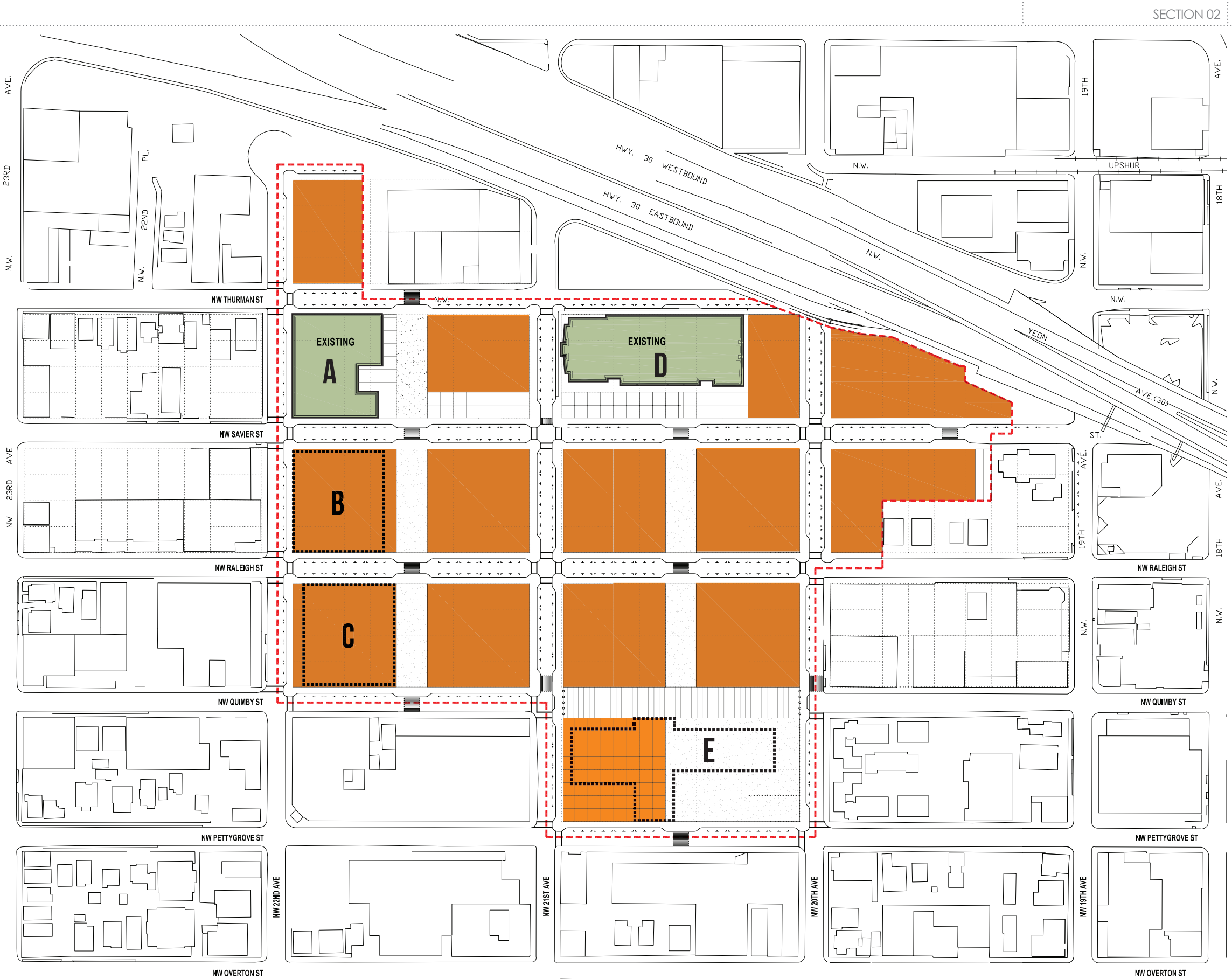
Map 02-1 illustrates the locations for existing and proposed structures. Consistent with Con-way's speculative approach to development described in Section 1, size and dimensions for proposed new structures cannot yet be described. Section 5, Design Standards and Guidelines, does however describe the characteristics of the development "envelope" that will govern size and dimensions of new structures.

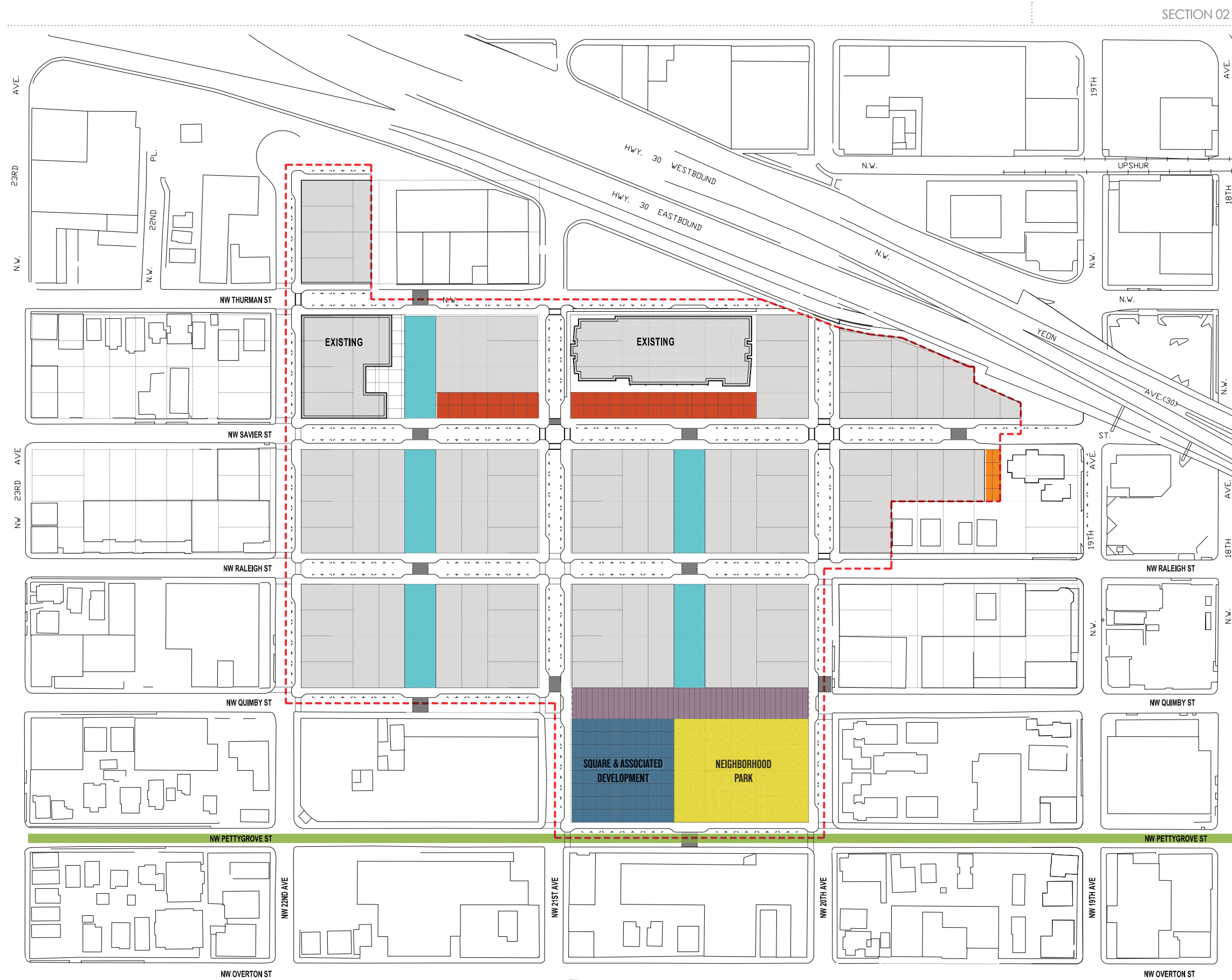
EXISTING BUILDINGS

- A. Office building - occupied by Con-way (See Appendix for floor area)
- B. Office building - vacant - This building may either be re-developed or demolished.
- C. Warehouse - occupied by Con-way (37,000 sf) - This building may either be re-developed or demolished.
- D. Office building - occupied by Con-way (See Appendix for floor area)
- E. Truck service building -vacant - This building will be demolished.



MAP 02-1
EXISTING & PROPOSED
STRUCTURES

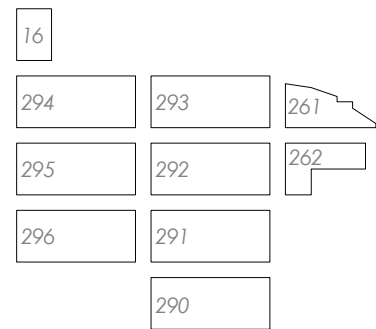




- POCKET PARK
- NEIGHBORHOOD PARK
- CITY DESIGNATED GREEN STREET
- PEDESTRIAN ACCESSWAY
- BUILDING FORECOURT
- SQUARE & ASSOCIATED DEVELOPMENT
- QUIMBY ST PARCEL

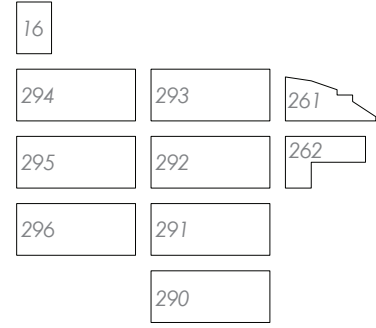
Map 02-2 illustrates the locations for various proposed open areas. Refer to Section 5 Design Standards and Design Guidelines for a comprehensive description of the Vision and Guiding Principles related to the formation of these spaces.

A portion of Block 290 is labeled as neighborhood park.



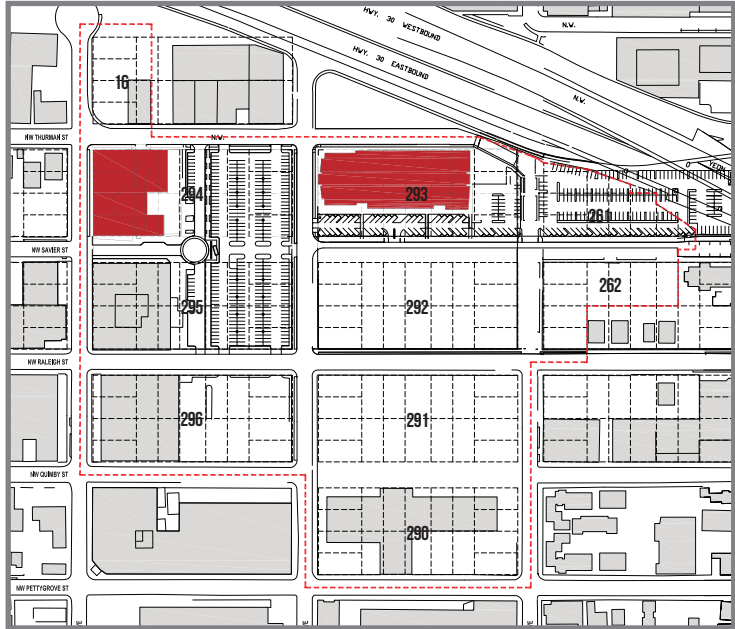
MAP 02-2
OPEN SPACE PLAN





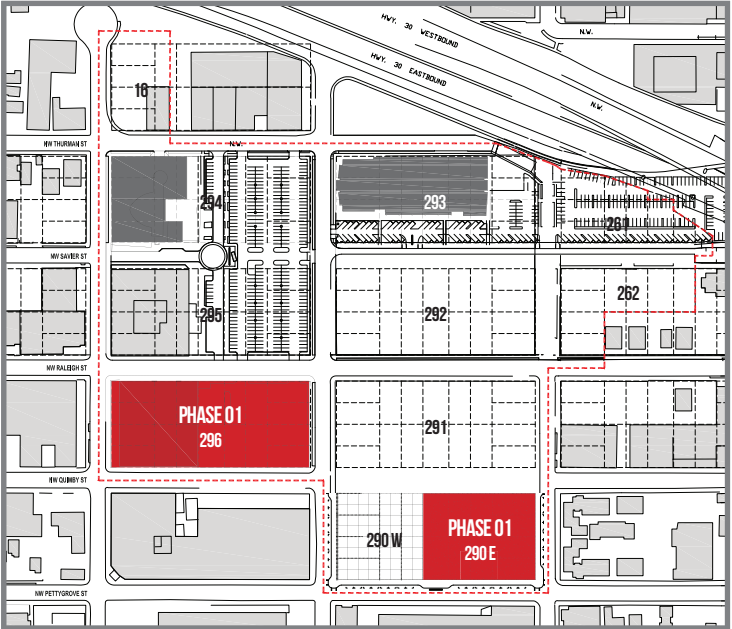
MAP 02-3
MAXIMUM HEIGHT





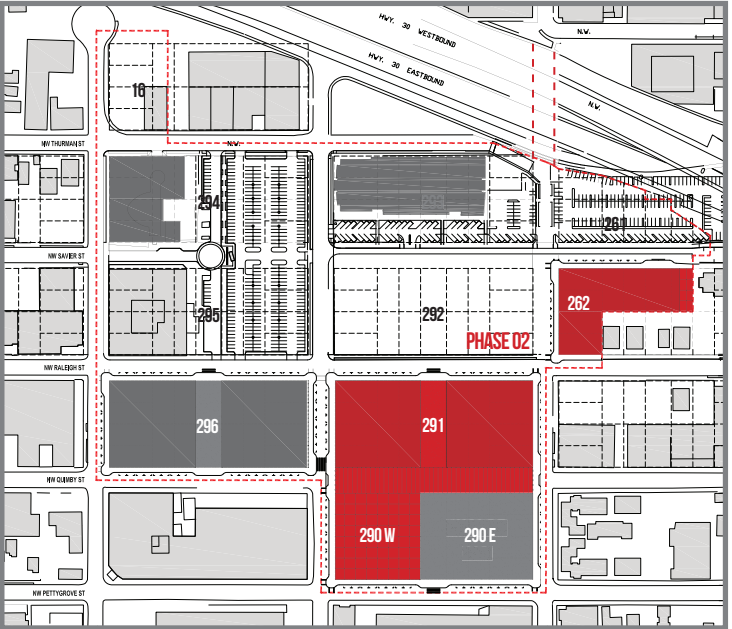
EXISTING CONDITIONS

Con-way is to remain in two of their existing office buildings. Buildings on blocks 295, 296, and 290 may or may not be reused during initial and later phases of development.



PHASE 01

Adaptive re-use of an existing building plus first new catalyst development on Block 296. Demolition of existing truck maintenance building and development of neighborhood park on Block 290 E.



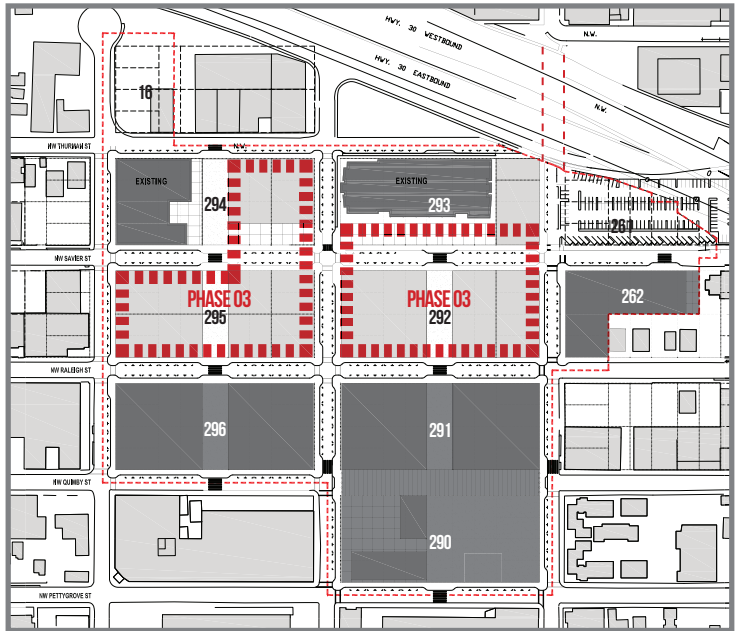
PHASE 02

Development of parcels that adjoin the neighborhood park. Development of other fringe parcels. Traffic/street improvements per Section 6.

NOTES: Street improvements are shown to occur in conjunction with each adjacent property.

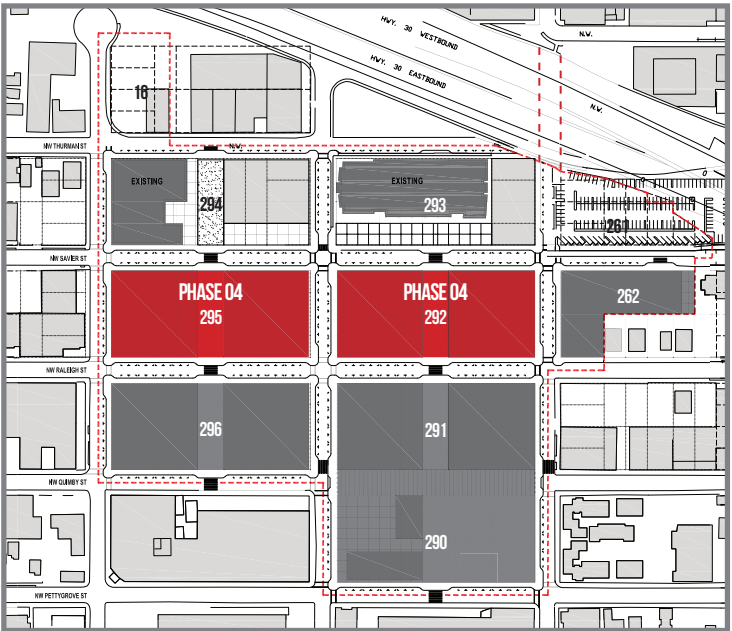
NW 20th, from NW Upshur to Sawyer, will be improved pursuant to Section 6.

A more complete description of each phase is detailed in Section 7.



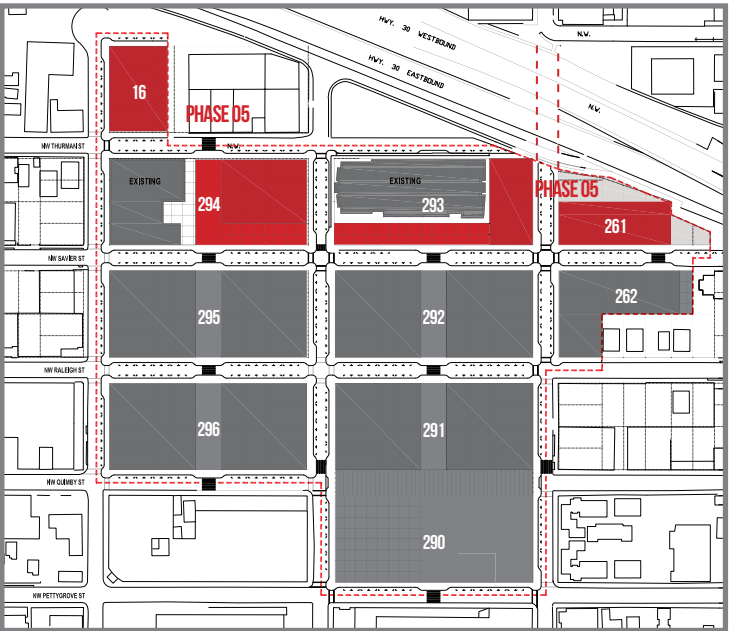
PHASE 03

Underground parking built to replace Con-way's surface lots



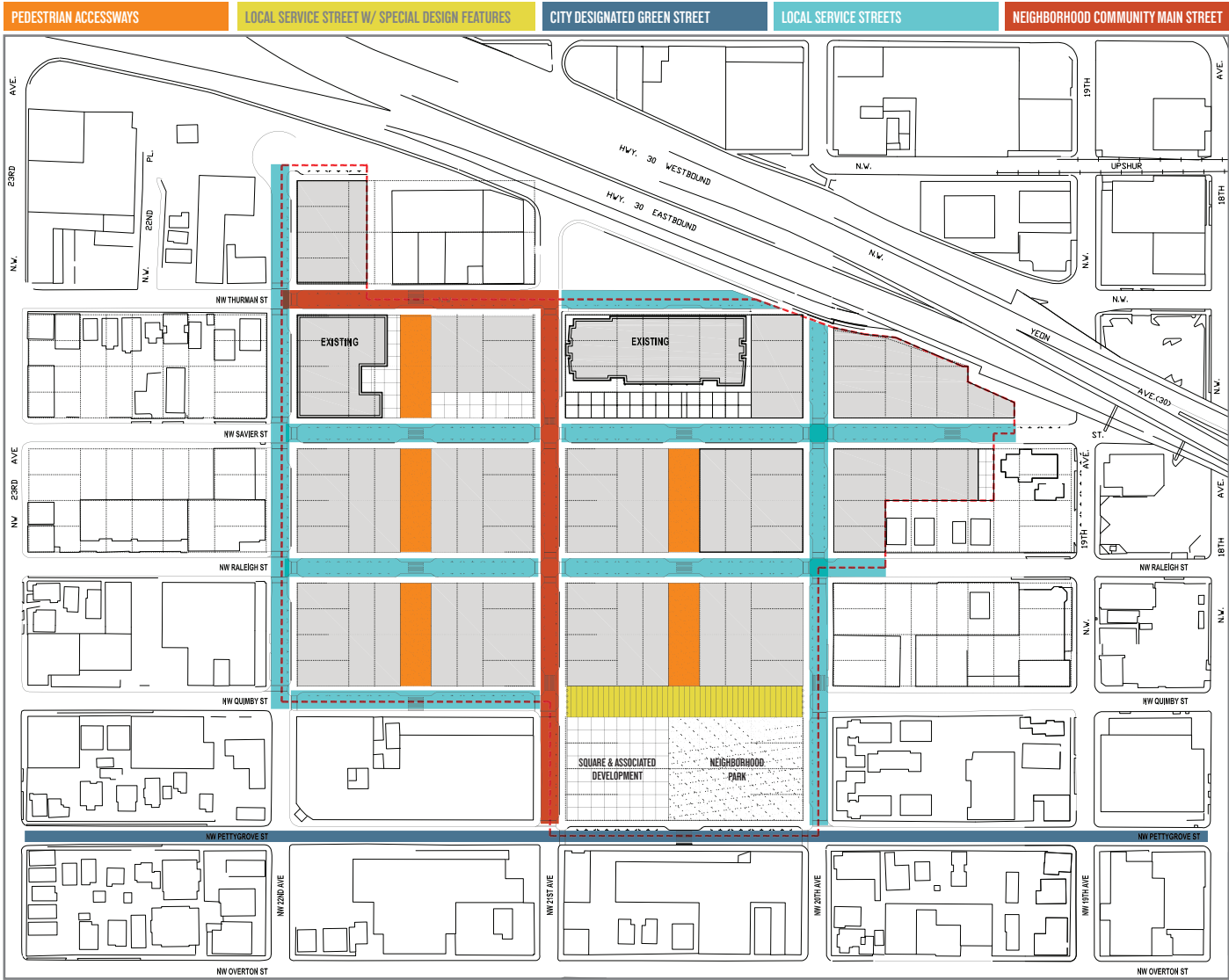
PHASE 04

Development occurs over underground garages.

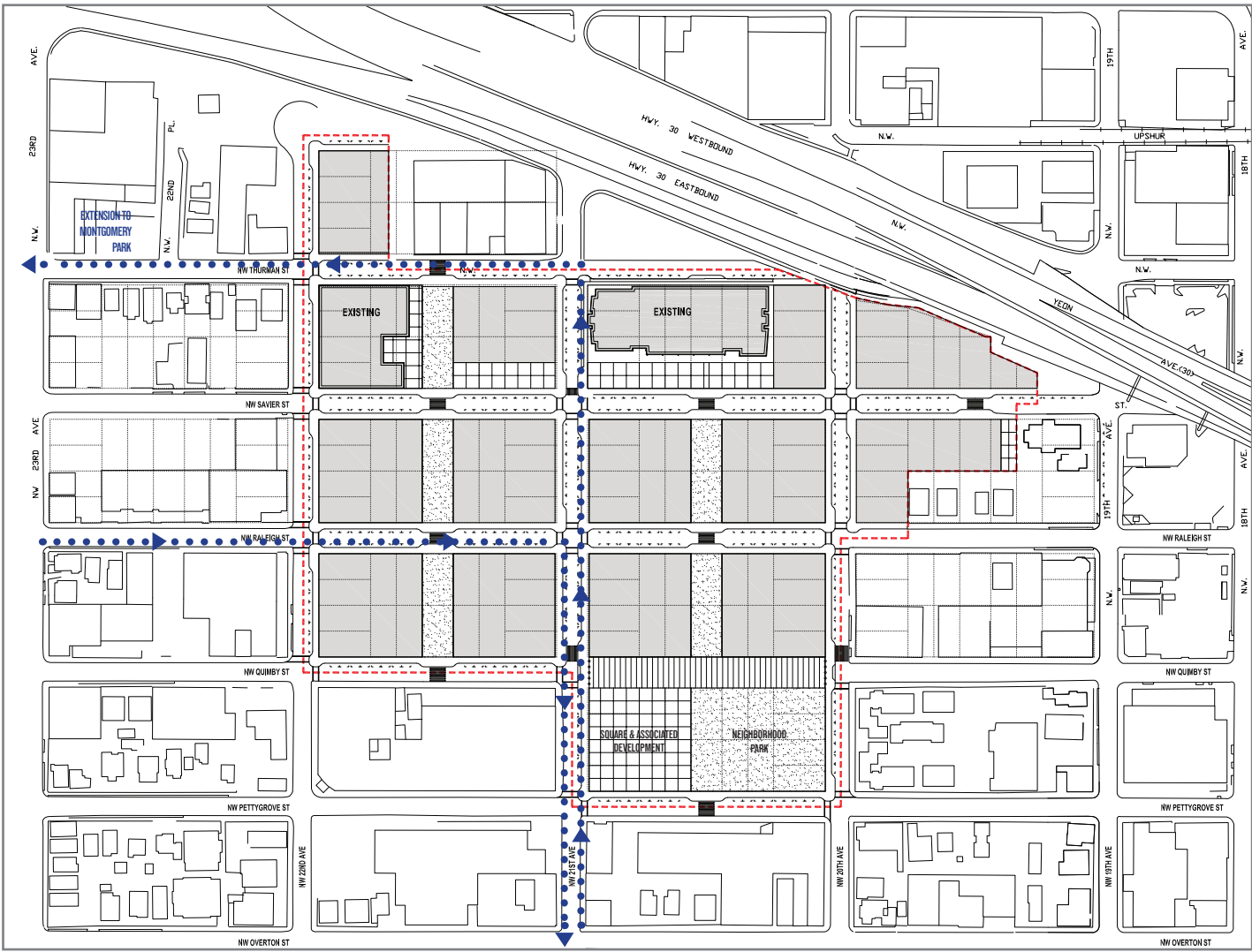


PHASE 05

Parcels at the north end of the property are redeveloped. These could be office or institutional uses due to proximity to freeway.



MAP 02-5
STREET PLAN



MAP 02-6
POTENTIAL STREETCAR
ALIGNMENT

SECTION 3

USES AND ACTIVITIES

33.562.300.D

3. Uses and Activities. A description of present uses, affiliated uses, proposed uses, interim uses, and possible future uses. The description must include information as to the general amount and type of all uses such as office, warehousing, retail, residential, and parking; number of employees, and number of dwelling units.

- 24** MAP 03-1 EXISTING USES
- 25** SUMMARY OF PROPOSED USES
- 26** MAP 03-2 PROPOSED USES

KEY



PRESENT USES

Present uses of the Con-way property include office, industrial, warehousing and surface parking (in locations indicated on Map 03-1). The property originally accommodated a trucking depot and truck maintenance facilities; it evolved over time to include headquarters office facilities that supported the trucking operations. Trucking operations have moved to off-site locales.

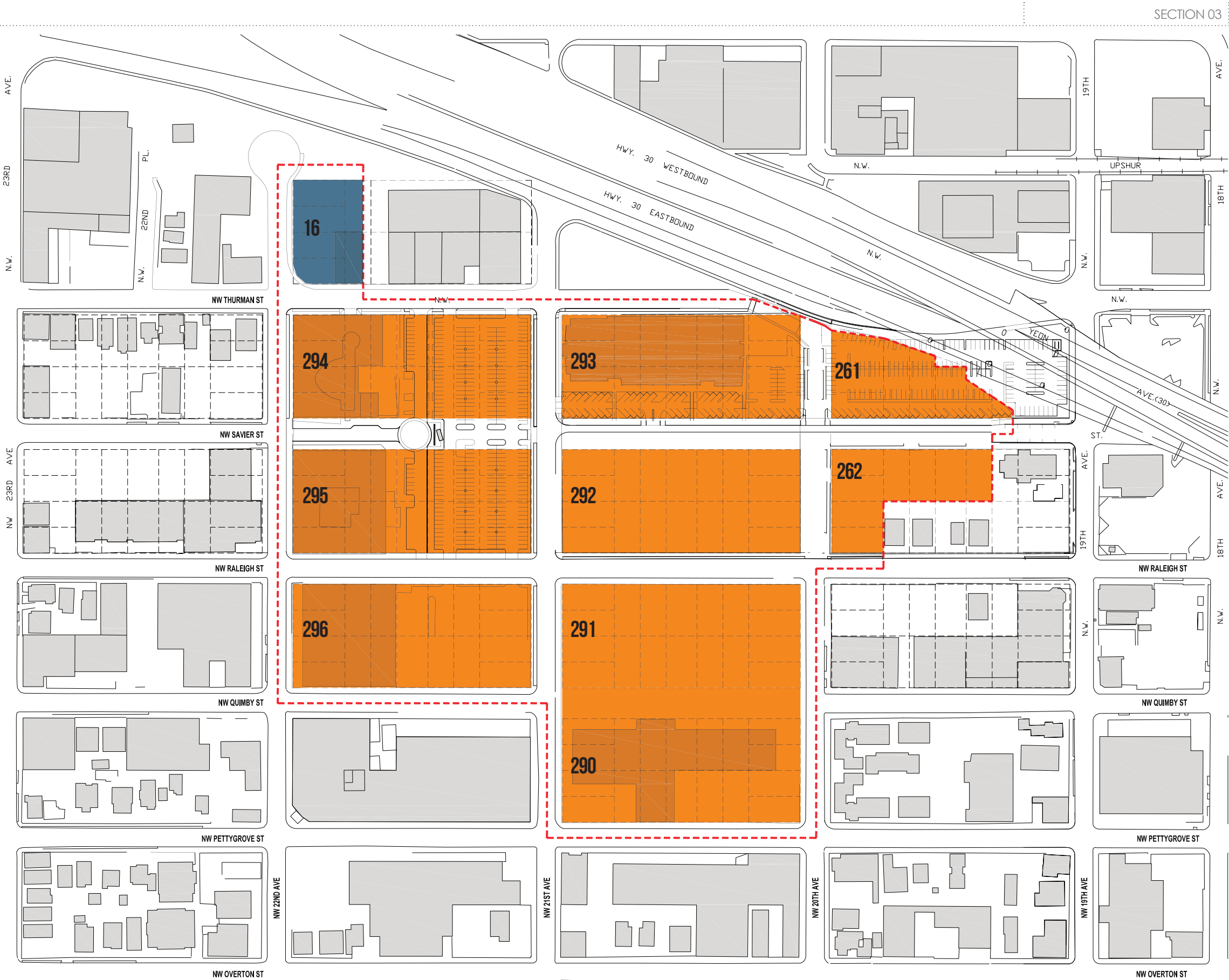
Today, Con-way owns and occupies office buildings on the site on Blocks 293 and 294. It has largely vacated a third office building that resides on Block 295.

Con-way also owns and occupies a warehouse building on Block 296, a vacant truck maintenance building on Block 290, and a small, leased industrial building on Block 16.

The balance of Con-way's site area includes paved lots which are used as parking.

INTERIM USES

Several of the existing surface parking lots will be maintained and used as such until development momentum causes them to be reclaimed for vertical development. Section 7 of this application describes the proposed sequencing of development as it relates to maintaining parking facilities for Con-way. Other interim uses are limited to an interim square, construction staging and parking areas, which will occupy adjoining undeveloped parcels during the various construction periods for each project.



MAP 03-1
EXISTING USES



PROPOSED USES

Proposed uses include all of those allowed under the base zone (Exd), as modified by Chapter 33.562 Northwest Plan District, providing for an urban level of mixed-use development including commercial, office, housing and employment. Anticipated program areas are summarized below on Table 03-1, and are described in detail in Appendix A.

The floor areas referenced in Table 03-1 are approximate in nature and for informational purposes only. Exact floor areas will ultimately be determined by future development applications. However, this floor area summary does serve as the maximum amount of development allowed throughout the Con-way Master Plan.

POSSIBLE FUTURE USES:

In addition to the uses described below, the Master Plan supports the possible development of public and institutional uses allowed within the base Exd zoning. These uses, if developed, will likely be accommodated within a mixed-use building project—the result of a public/private partnership. As an example, Portland Public Schools has expressed their long-term interest in the development in a capacity yet to be determined. Other possibilities might include a public library or community center at the base of a mixed-use building. Aside from a developing agreement between Con-way and the Parks Bureau relative to open space development, there are no public/private partnerships in play at this time; the stage has been set for them to flourish, however.

DENSITY:

The development program described below, and as shown on Table 03-1, achieves an overall density (floor area ratio / FAR) of 3:1 throughout the Con-way Master Plan area. This density level currently is allowed in the Northwest Plan district for residential uses. To enable development to achieve a true and vibrant mixed-use environment, the Con-way Master Plan expands the mix of uses allowed so that in addition to residential uses, a mix of commercial, office and employment will be allowed at a 3:1 FAR. This is necessary to create a vibrant mixed-use neighborhood. Provisions explaining the uses that are allowed in the Con-way Master Plan are described in Section 5, Development and Design Standards and Criteria, of this application.

Minimum densities that are currently described in the NW District plan (33.562.220.B.1.b.) are inadequate to ensure a true mixed-use environment. The Con-way Master Plan includes guidelines and standards (Section 5) for density maximums and minimums that ensure a successful mixed-use environment will be achieved. These include:

- Establish a min. 1.5:1 FAR on every block except 290.
- Establish a maximum of 150,000 sq. feet of new retail.
- Establish a maximum of 450,000 sq. feet of new office.

HOUSING - MULTI-FAMILY:

The overall development program calls for a range of 1,100 - 1,500 new housing units within the plan limits. The exact number will ultimately be determined by the market.

The majority of new housing development will be realized as high-density, multi-family, multi-story products comprised of flats, townhouses, lofts and studios. All classes of housing uses are allowed and encouraged, including: condominiums, apartments, senior, assisted care, empty nester, student, singles and couples, family, market rate and affordable. Ultimately, Con-way will have an influence on the type of housing developed, as they see a strong demand for housing for its employees.

Map 03-1 describes the locations for current uses within the plan boundary. Map 03-2 describes anticipated locations for proposed uses within the plan boundary. These locations are intended to be flexible in nature rather than prescribed—the exception being the requirement for retail along NW 21st Avenue. Office uses are anticipated to occupy the northernmost blocks, where they can adjoin existing office uses and serve to buffer the visual and acoustic impact of the Fremont Bridge approach.

TABLE 03-1. SUMMARY OF PROPOSED USES (FOR INFORMATIONAL PURPOSES ONLY)*1

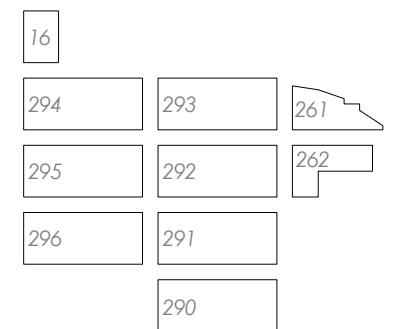
Proposed Uses	Existing Floor Area	New Floor Area	Total Floor Area	% of Total
Commercial retail	0	146,000	146,000	6%
Housing (SF)	0	1,382,775	1,382,775	61%
Housing (# Units)	0	1100 - 1500 units	1100 - 1500 units	
Office	330,850 ²	421,225	752,075	31%
Total	330,850	1,950,000	2,280,850	100%

¹See Appendix A to see a detailed program area model that describes these numbers in greater detail.
²Floor area for existing Con-way buildings that will remain (Adtech I on Block 294; Adtech II on Block 293).

ALLOWED USES UNDER CON-WAY MASTER PLAN

The Con-way Master Plan is proposing that proposed uses to each site may vary on a project-by-project basis, and that allowed uses match those uses allowed under the EX base zone criteria.

The allowed uses for the sites shown in this diagram are to be the same uses allowed for Central Employment (EX) base zone as described in the City of Portland's Title 33, section **33.140.100 PRIMARY USES.**



MAP 03-2 PROPOSED USES

SECTION 4

SITE PLAN

33.562.300.D

4. Site Plan. A site plan, showing the location, size, and dimensions of existing and proposed structures, the pedestrian, bicycle, and vehicle circulation system, rights-of-way proposed for dedication or vacation, vehicle and bicycle parking areas, open areas, infrastructure improvements, landscaping, and any proposed temporary uses during construction and phasing of development.

28	MAP 04-1 EXISTING & PROPOSED STRUCTURES
29	MAP 04-2 CIRCULATION SYSTEMS
30	MAP 04-3 POTENTIAL STREETCAR ALIGNMENT
31	MAP 04-4 BIKE CIRCULATION SYSTEM
32	MAP 04-5 DEDICATIONS & VACATIONS
33	MAP 04-6 PARKING LEVEL PLAN
34	MAP 04-7 OPEN AREAS
35	MAP 04-8 CITY-WIDE OPEN SPACE NETWORK
36	MAP 04-9 INFRASTRUCTURE DIAGRAM

KEY

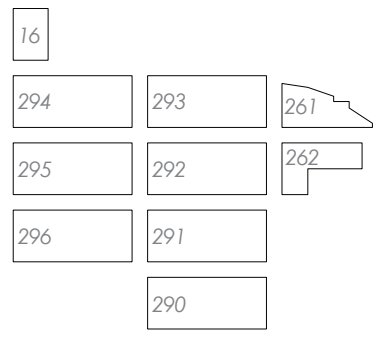
- EXISTING BUILDINGS TO REMAIN UNCHANGED
- NEW DEVELOPMENT SITES
- EXISTING BUILDINGS LIKELY TO BE RE-DEVELOPED OR DEMOLISHED

LOCATION, SIZE & DIMENSIONS OF EXISTING & PROPOSED STRUCTURES

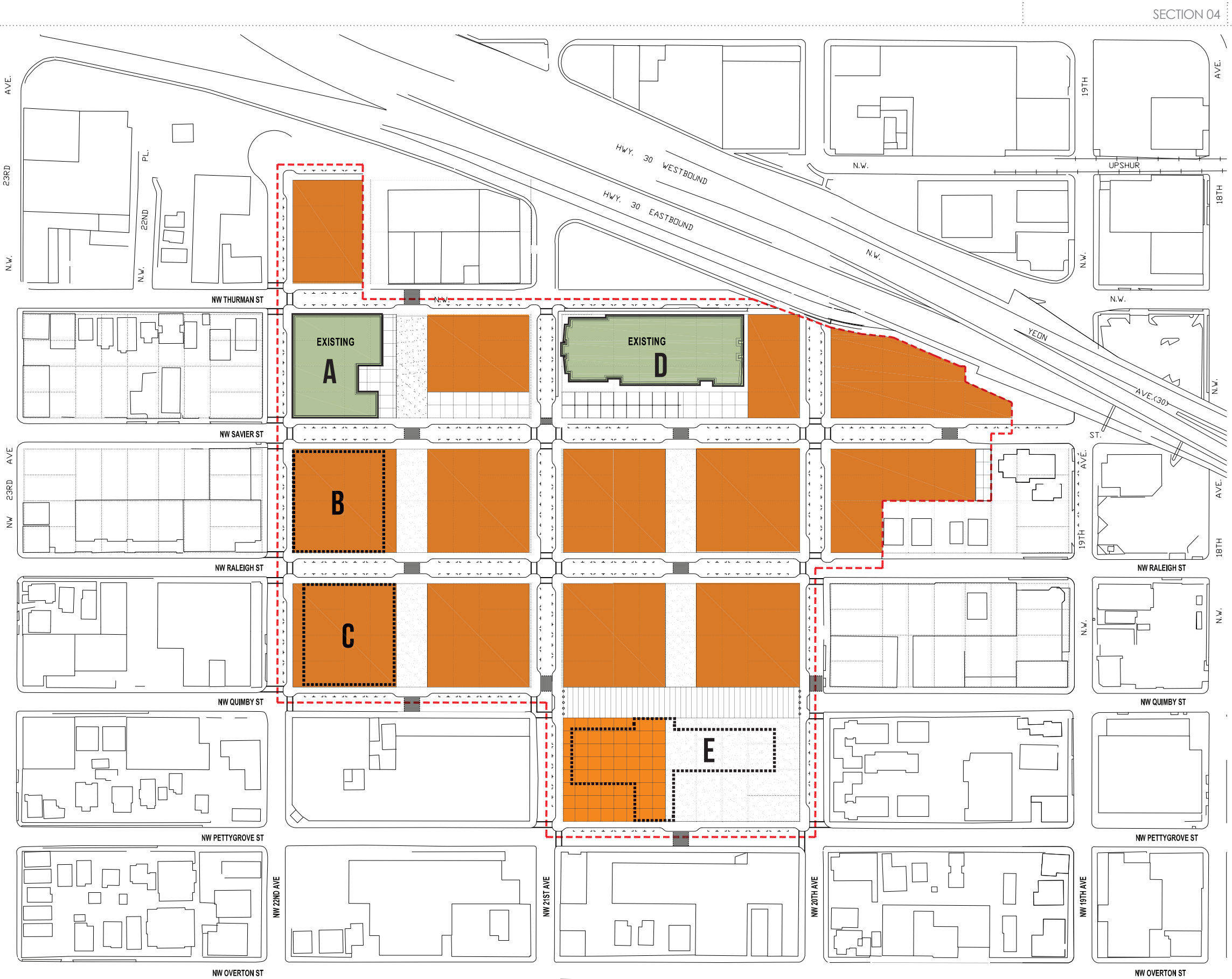
Map 04-1 illustrates the locations for existing and proposed structures. Consistent with Con-way's speculative approach to development described in Section 1, size and dimensions for proposed new structures cannot yet be described. Section 5, Design Standards and Guidelines, does however describe the characteristics of the development "envelope" that will govern size and dimensions of new structures.

EXISTING BUILDINGS

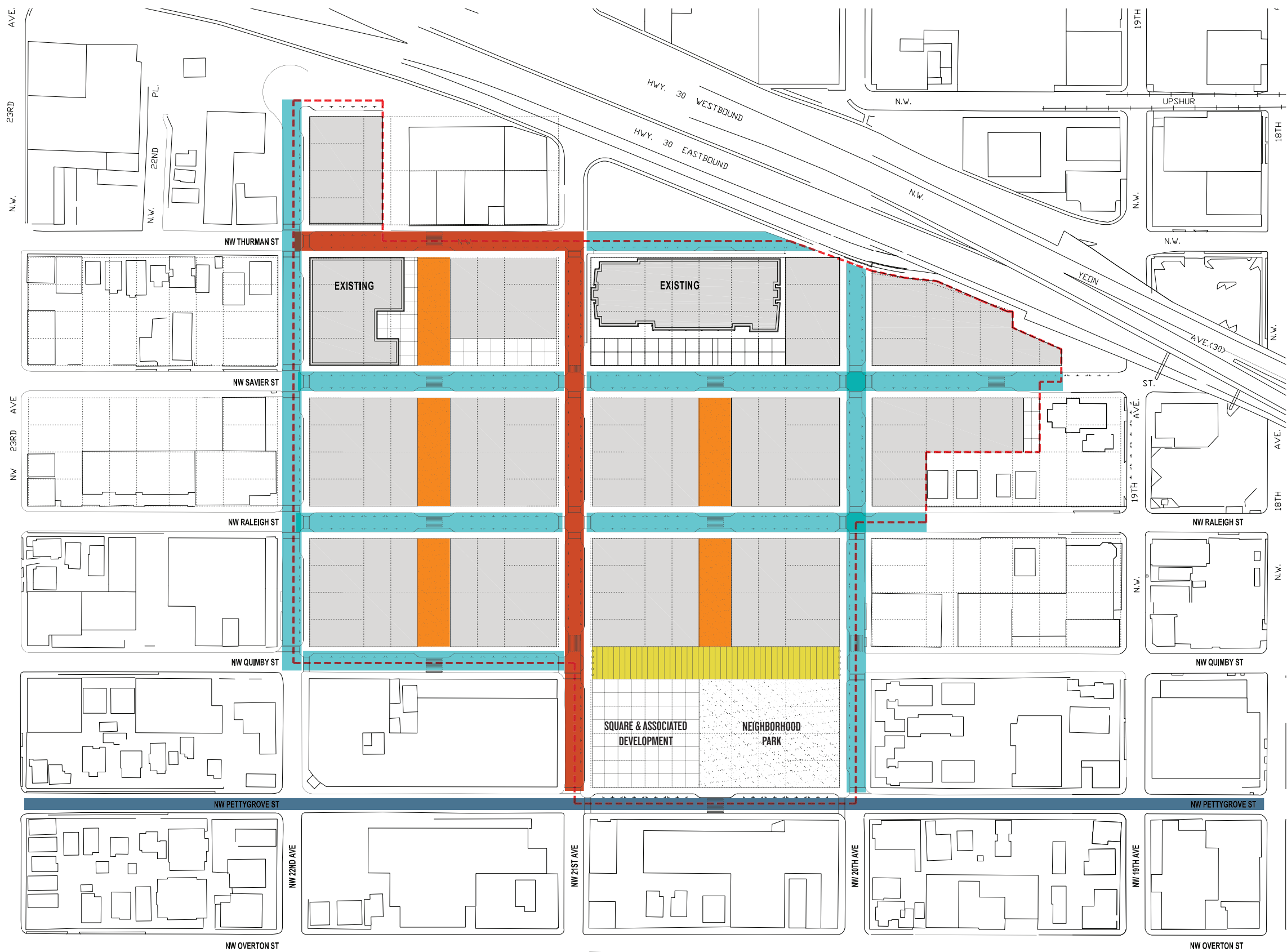
- A. Office building - occupied by Con-way (See Appendix for floor area)
- B. Office building - vacant - This building may either be re-developed or demolished.
- C. Warehouse - occupied by Con-way (37,000 sf) - This building may either be re-developed or demolished.
- D. Office building - occupied by Con-way (See Appendix for floor area)
- E. Truck service building -vacant - This building will be demolished.



MAP 04-1
EXISTING & PROPOSED
STRUCTURES



- PEDESTRIAN ACCESSWAYS
- LOCAL SERVICE STREET W/ SPECIAL DESIGN FEATURES
- CITY DESIGNATED GREEN STREET
- LOCAL SERVICE STREETS
- NEIGHBORHOOD COMMUNITY MAIN STREET



CIRCULATION SYSTEM:
PEDESTRIAN & VEHICULAR

Map 04-2 illustrates the proposed configuration for the various circulation systems within the Con-way Master Plan area. The plan has been developed to comply with City of Portland plans, policies and characteristics for pedestrian, bike and vehicular circulation systems. Details include:

Pedestrians:

The Con-way plan reinforces the entire district's designation as a Pedestrian District.

- All sidewalks within the right-of-way within the plan area will be constructed to comply with the policies of the Portland Comprehensive Plan and PBOT standards for Neighborhood Collector a Community Main Streets and Local Service Streets.
- New mid-block "Pedestrian Accessways" enhance pedestrian access through the plan area by providing public access through private land and connections to open spaces within the plan area.
- NW Quimby between 20th and 21st will be designated as a local service street with special design features. The design for this street will be coordinated with the square and the neighborhood park.

Vehicles:

All streets and sidewalks within the Con-way plan area will be constructed to comply with the policies, street classifications, and street designations of the Transportation Element of the Portland Comprehensive Plan. The proposed street plan provides a series of multi-modal street connections that match the surrounding street grid pattern, where feasible, and as supported by the Portland Bureau of Transportation. Streets within the Con-way property will continue to operate consistent with their current functional classification as designated in the Portland Transportation System Plan. See Section 6 Transportation for additional detail described in the Multi-Modal Transportation Impact Analysis

- NW 21st Avenue and Thurman Street will continue to function and operate as Neighborhood Collectors and Community Main Streets.

MAP 04-2
CIRCULATION SYSTEMS



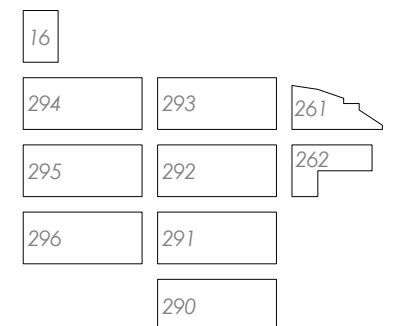
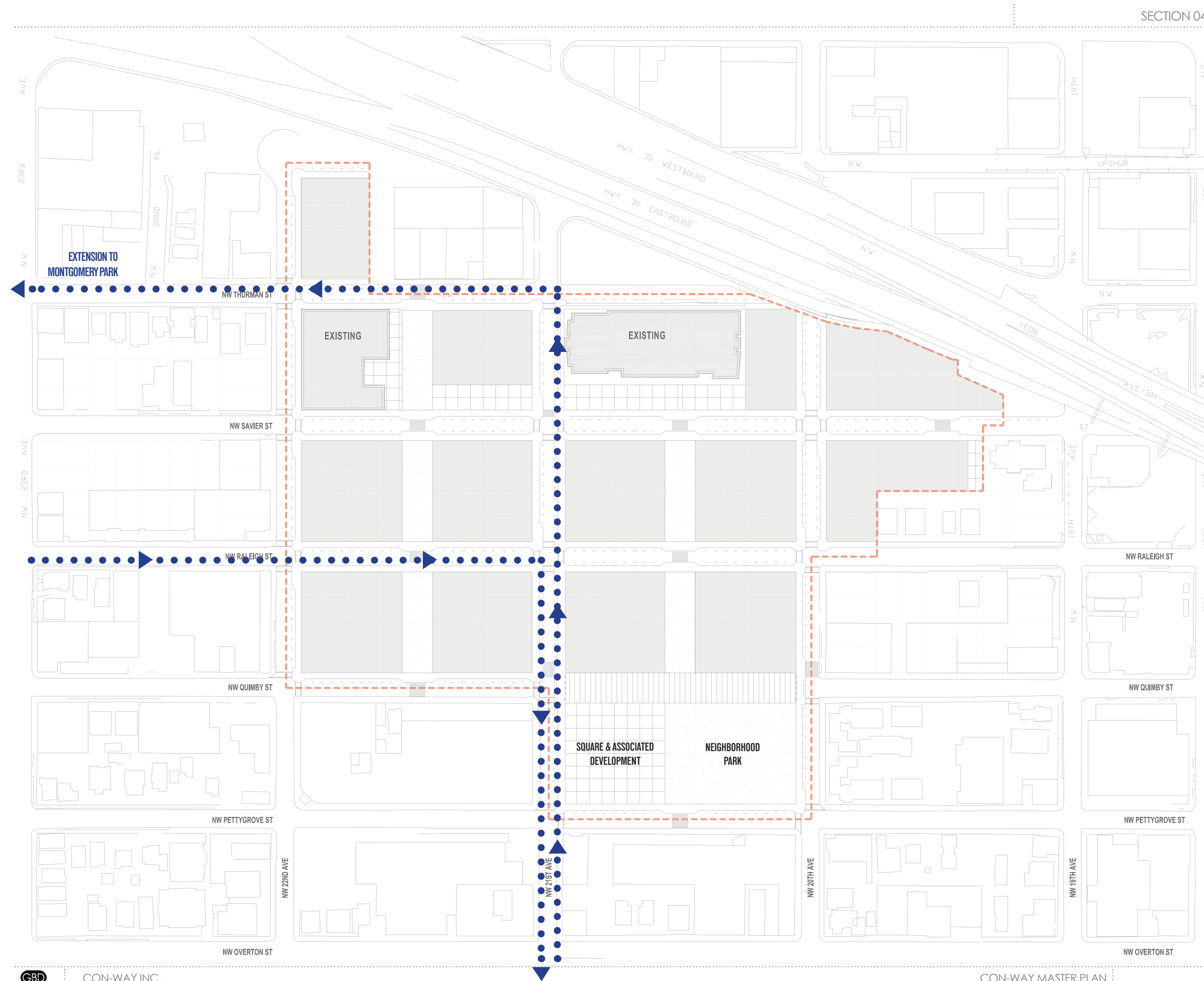
KEY

POTENTIAL STREETCAR ALIGNMENT



CIRCULATION SYSTEMS: STREETCAR

The Con-way plan includes provisions for an extension of the Portland Streetcar system to be constructed when funds become available. Potential route is shown on Map 04-3.



**MAP 04-3
POTENTIAL STREETCAR
ALIGNMENT**



KEY

FUTURE SEPARATED IN-ROADWAY

EXISTING BIKE LANE SEPARATED IN-ROADWAY

EXISTING OR FUNDED BICYCLE BOULEVARD

FUTURE BICYCLE BOULEVARD

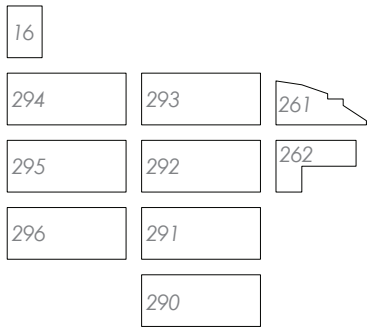
CON-WAY BIKE FRIENDLY STREETS

CON-WAY POSSIBLE FUTURE BIKE HUB / STATION, GENERAL LOCATIONS

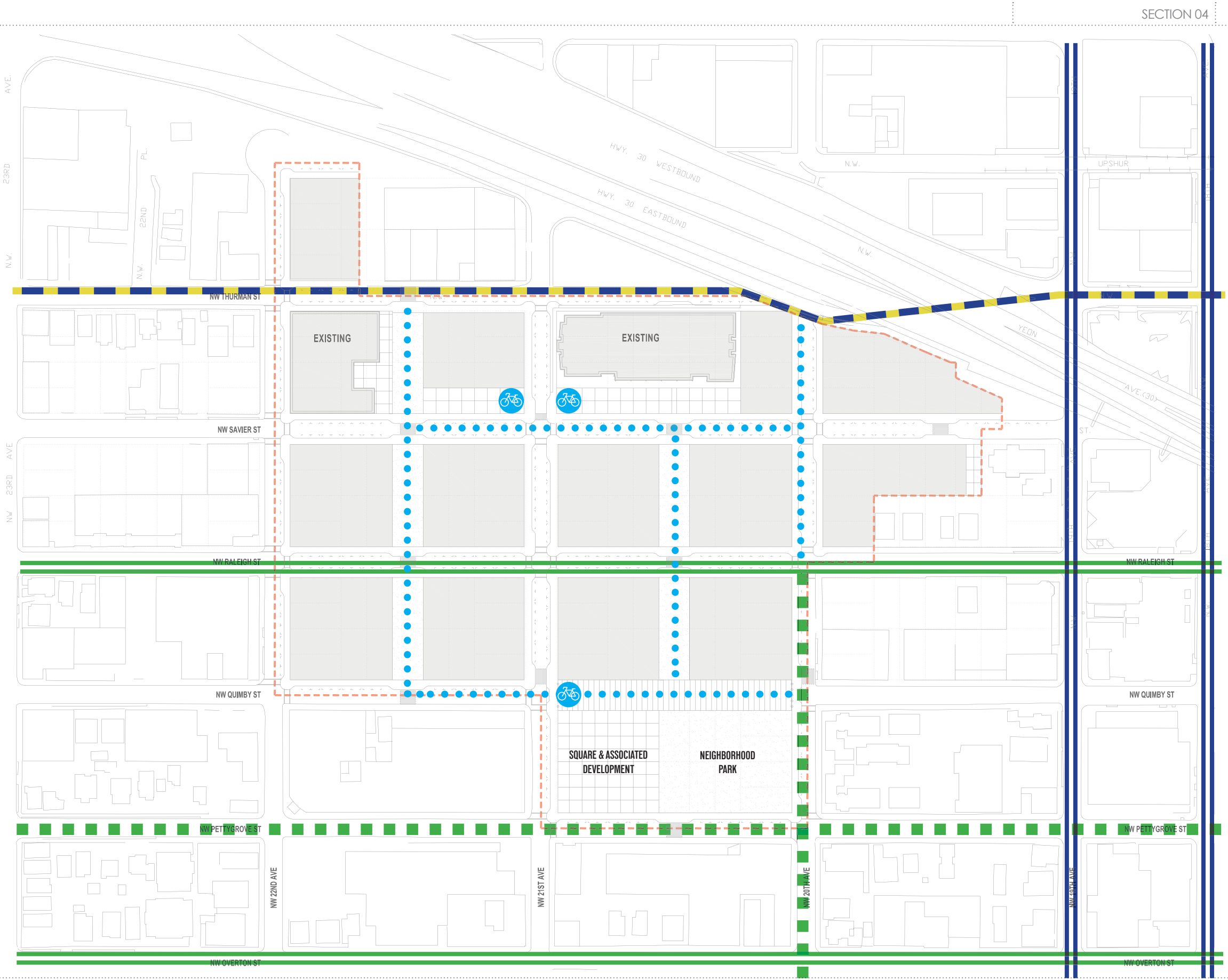
BIKE CIRCULATION SYSTEM

The Con-way plan builds on the recommended bicycle network described in Portland Bicycle plan for 2030. Existing bikeways will be retained and new bikeways will be constructed where development projects involve the construction or re-development of adjoining streets that are designated as existing or future bikeways.

- City designated green street. The new open space designated for parks (Block 290) has been located so as to adjoin and complement the future redevelopment of NW Pettygrove as a City designated green street.
- New mid-block "Pedestrian Accessways" enhance bicycle access through the plan area by providing public access through private land and connections to open spaces within the plan area.



MAP 04-4
BIKE CIRCULATION SYSTEM



KEY

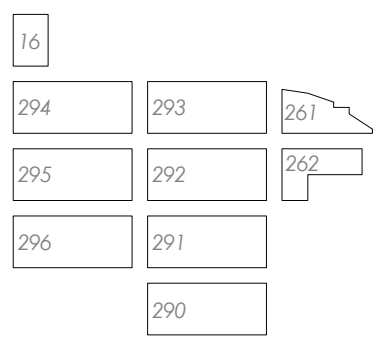
PRIVATE PROPERTY TO BECOME
RIGHT-OF-WAY

RIGHTS-OF-WAY PROPOSED FOR
DEDICATION

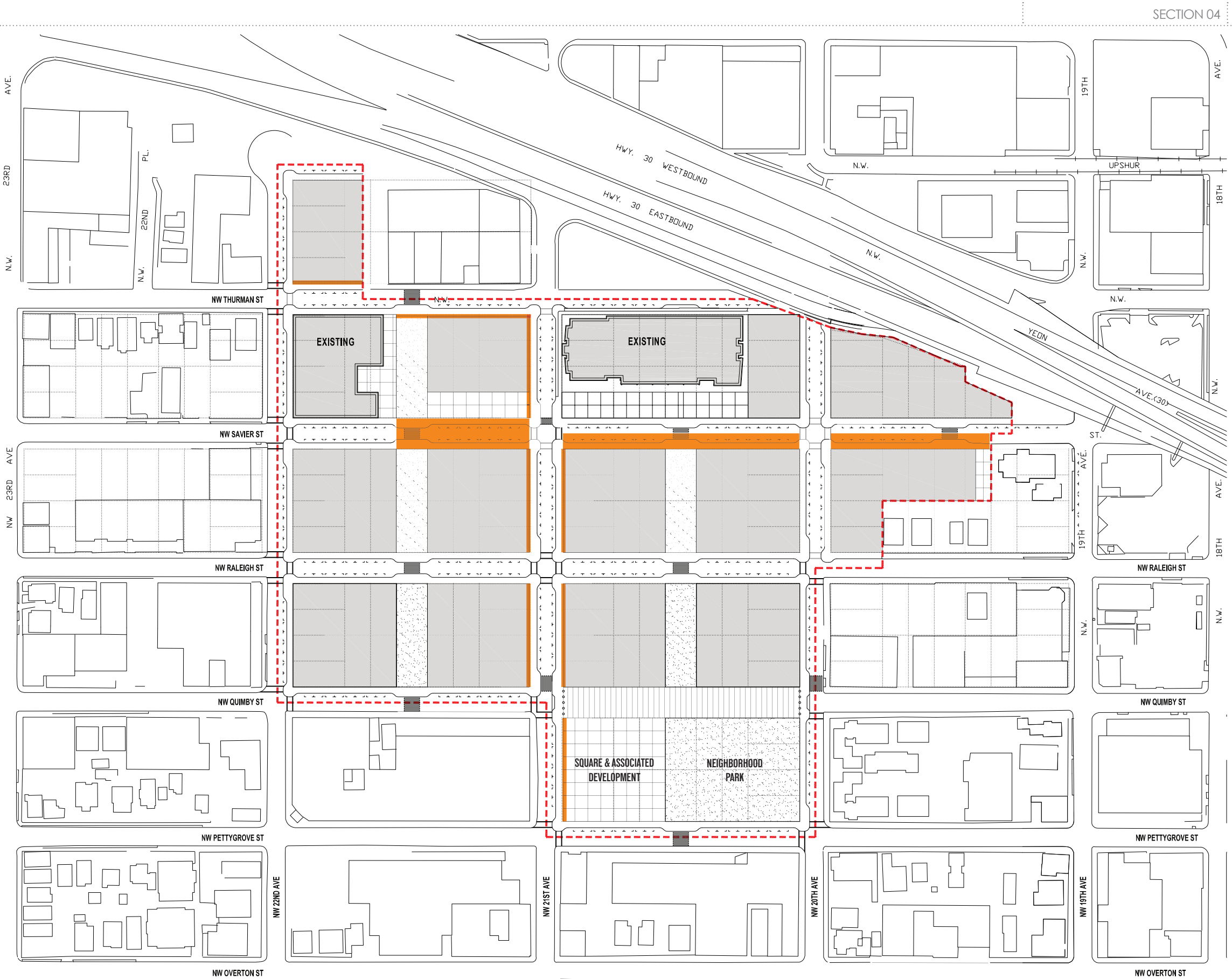
TOTAL AREA: 42,800 SF

Map 04-5 illustrates locations where private property will be conveyed to the City for its use as open space or expanded rights-of-way at the time a development application is proposed. These locations include:

- Dedicate portions of NW Savier Street owned by Con-way to the City. This street will be re-constructed to meet city standards for a local service street.
- Dedicate 3 feet of private property on block faces abutting both sides of NW 21st Street and NW Thurman Street for expanded right-of-way to meet main street standards.



MAP 04-5
DEDICATIONS



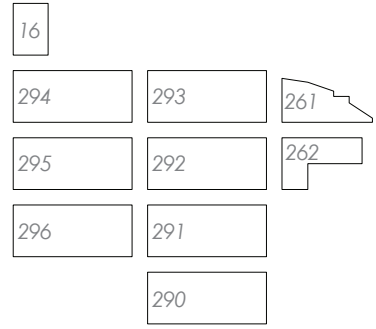
VEHICLE & BICYCLE
PARKING AREAS

Map 04-6 illustrates several locations where underground parking structures can be located. The large land parcels enhance the viability of underground parking by allowing for greater efficiencies in layout and thereby greater densities than can be configured on traditional 200' x 200' Portland blocks. Ground water levels are deep enough in this locale so as to not encumber the constructability of underground parking.

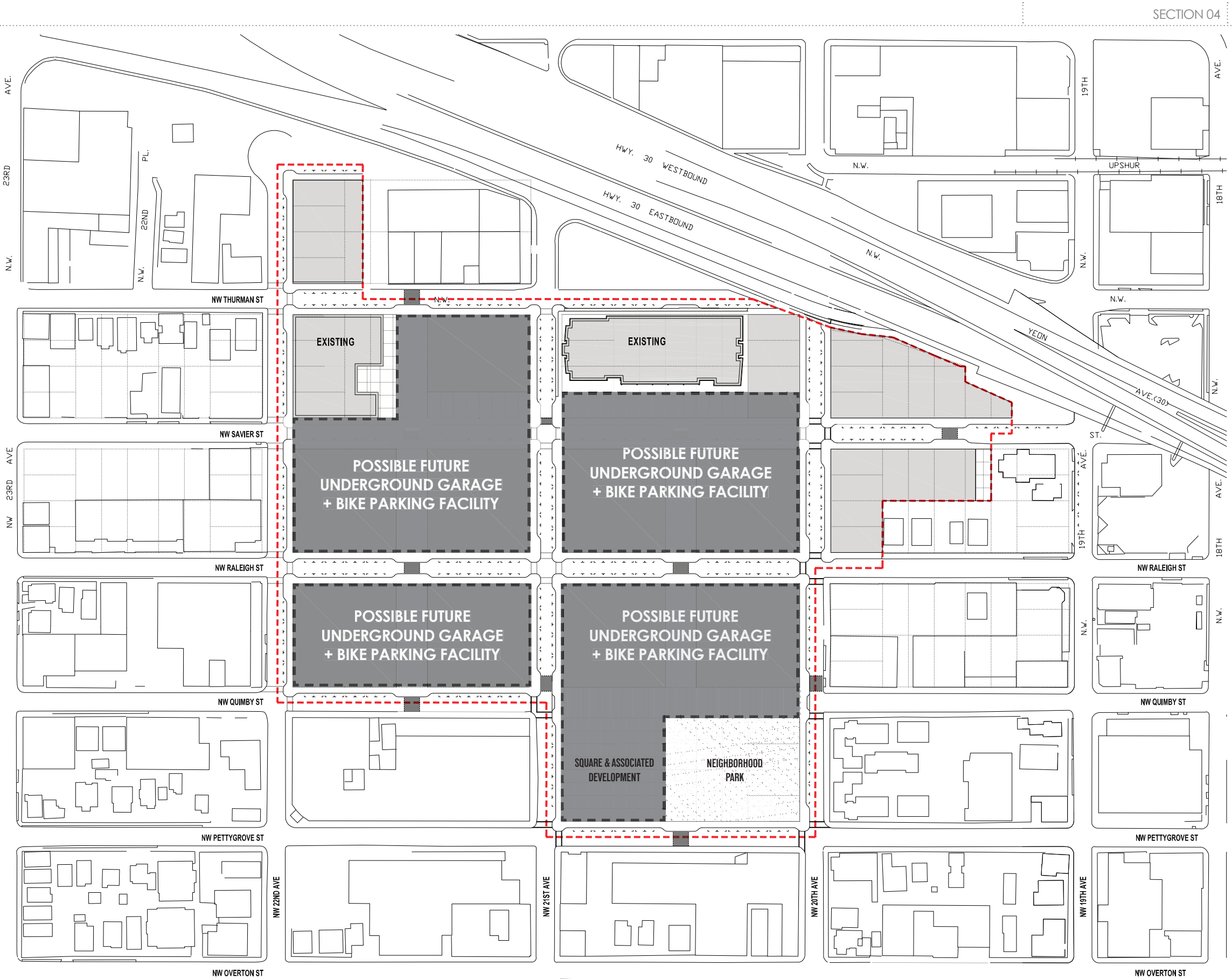
Vehicle parking facilities will be developed on a project-by-project basis to support the proposed development program for each block. Stall counts will comply with parking standards described in Section 6 Transportation. Con-way will progressively develop parking to replace its current surface parking inventory in concert with future development projects that adjoin its office buildings. Con-way will integrate shared parking strategies as a key element of the land transactions for those future developments.

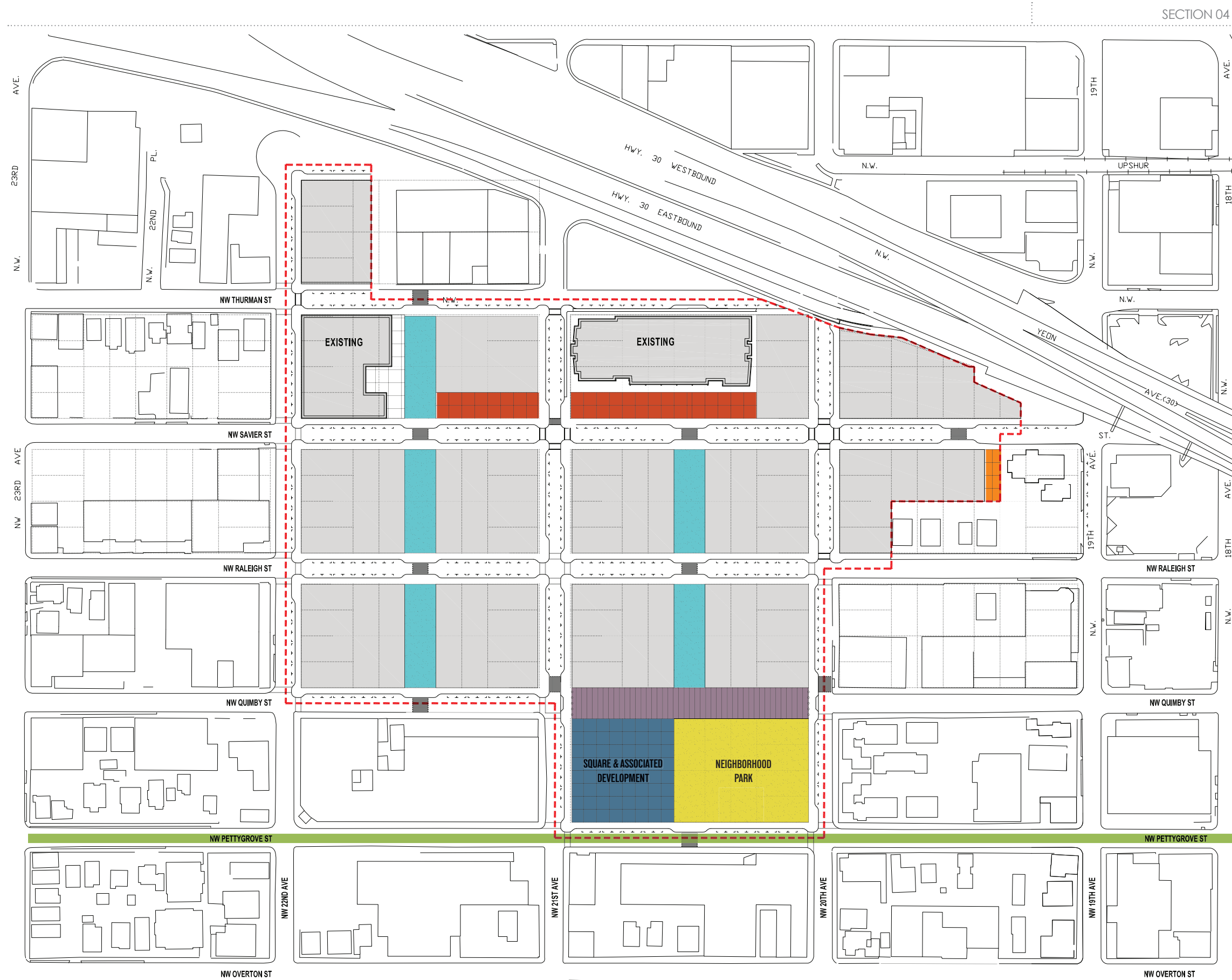
Bicycle parking facilities will be provided on a project-by-project basis in compliance with City of Portland requirements and as described in Section 6 Transportation. Con-way currently has a large long-term bicycle parking facility that accommodates approximately 60 bikes. Con-way plans to maintain and possibly expand this facility as the Master Plan builds out.

Pedestrian accessways will also serve as locations for above-ground, short-term bike parking and bike corrals.



MAP 04-6
PARKING LEVEL PLAN



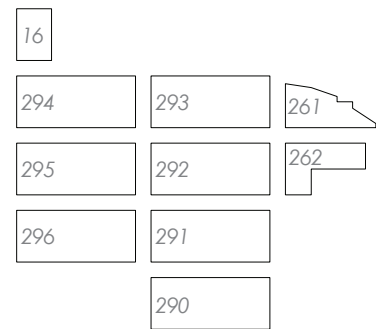


- POCKET PARK
- NEIGHBORHOOD PARK
- CITY DESIGNATED GREEN STREET
- PEDESTRIAN ACCESSWAY
- BUILDING FORECOURT
- SQUARE & ASSOCIATED DEVELOPMENT
- QUIMBY ST PARCEL

OPEN AREAS

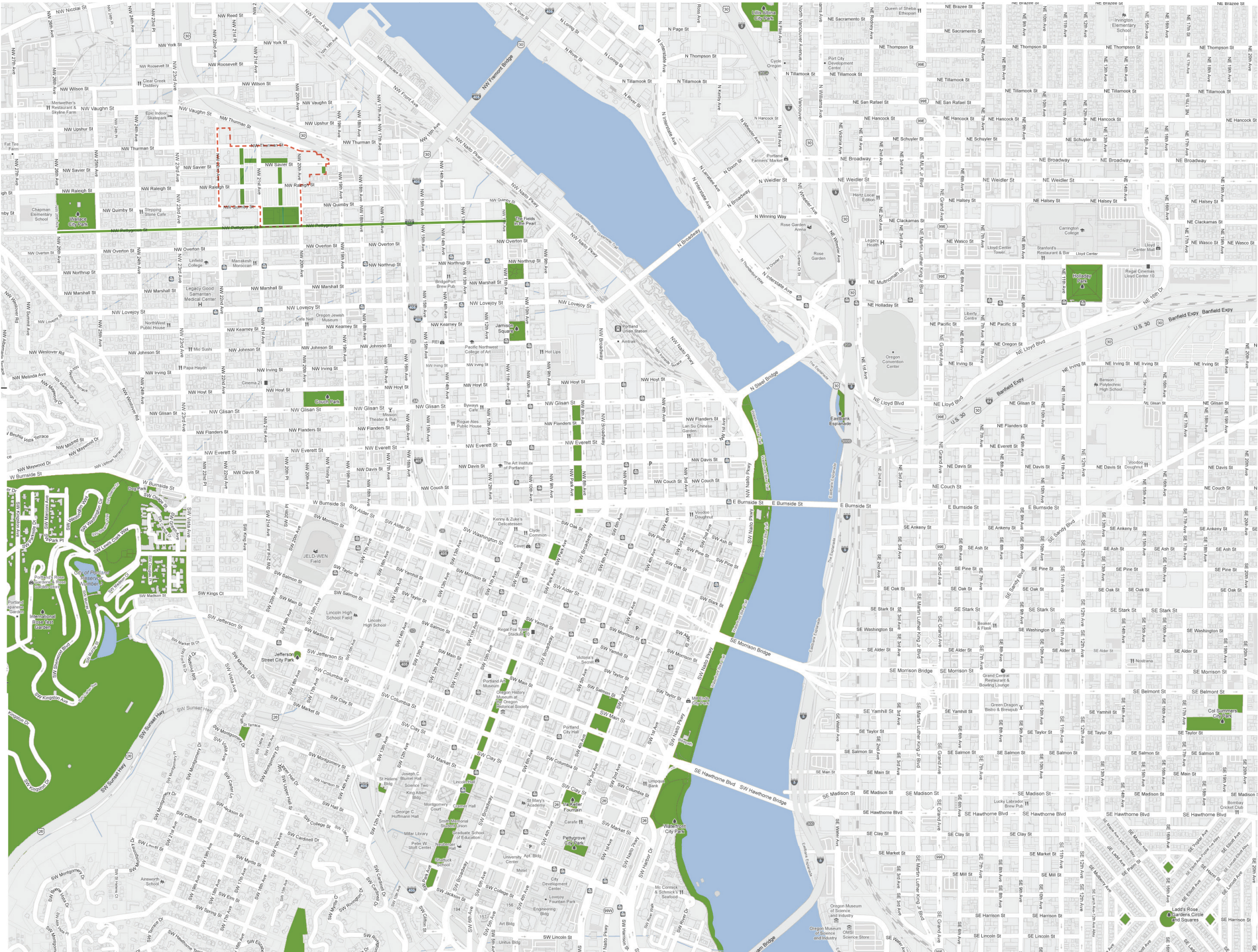
Map 04-7 illustrates the locations for various proposed open areas. Refer to Section 5 Design Standards and Design Guidelines for a comprehensive description of the Vision and Guiding Principles related to the formation of these spaces.

A portion of Block 290 is labeled as neighborhood park.



MAP 04-7
OPEN AREAS





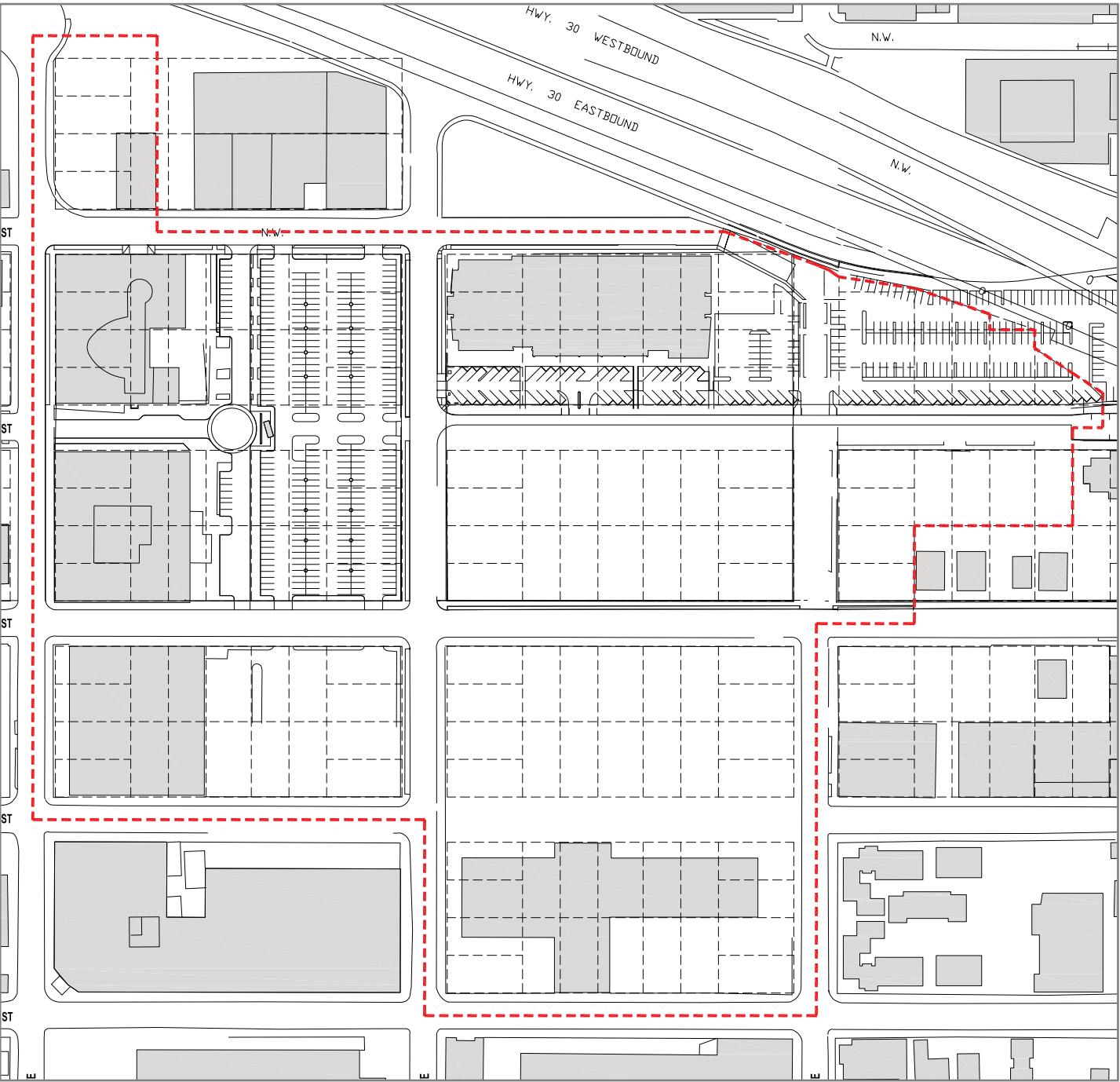
This diagram shows the Con-way property, and how it sits in relation to other parks and open spaces throughout the city.

NW Pettygrove is currently envisioned as a "green street" by the city of Portland, helping link Wallace Park in Northwest Portland to The Fields Park in the Pearl District.

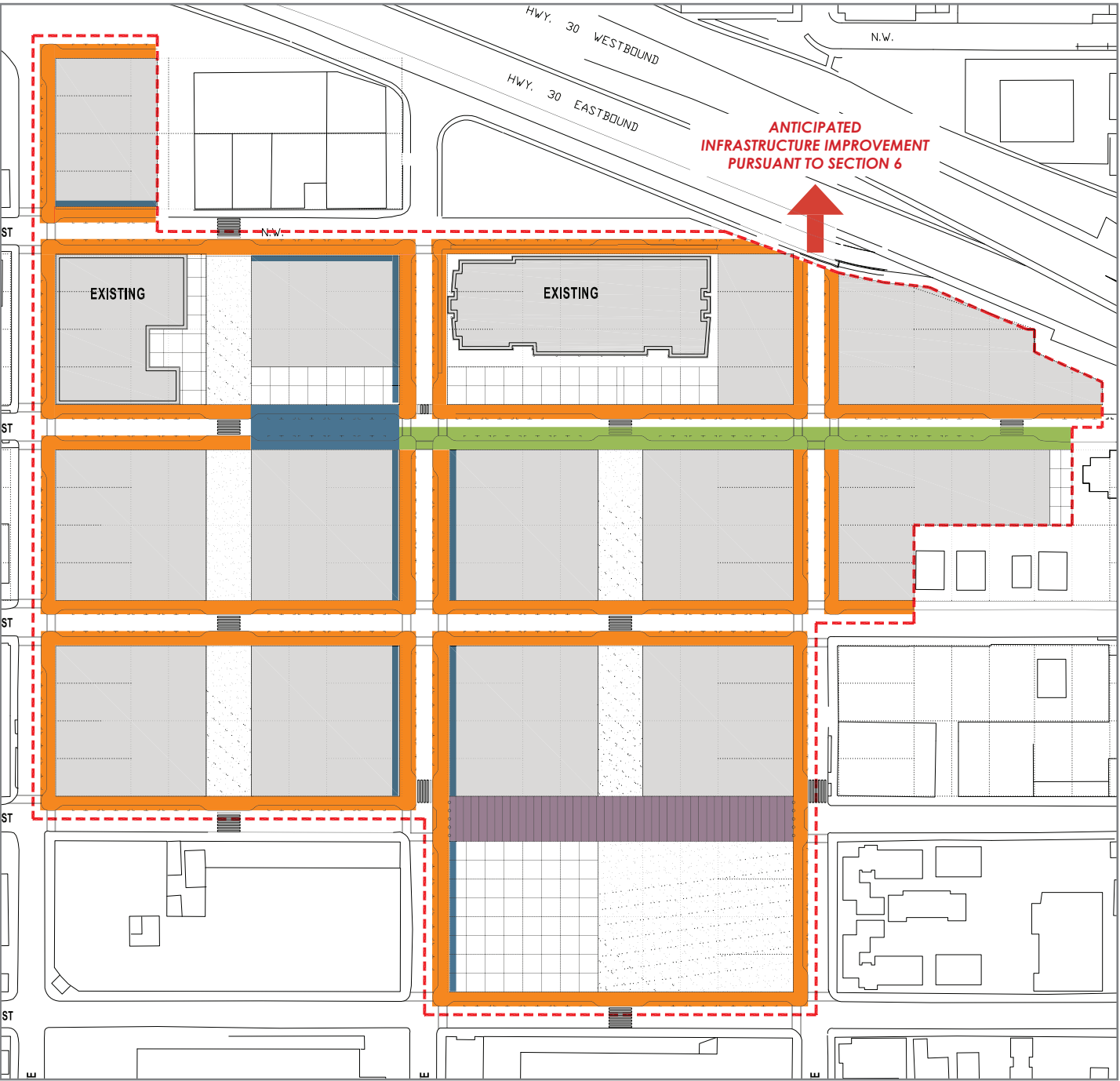
The Con-way site abuts this green street on one block face. The proposed open spaces are shown in green.

MAP 04-8
CITY-WIDE OPEN
SPACE NETWORK

EXISTING



PROPOSED



RIGHT-OF-WAY DEDICATIONS	HALF STREET IMPROVEMENTS
QUIMBY ST PARCEL	PEDESTRIAN ENVIRONMENT UPGRADES

MAP 04-9
INFRASTRUCTURE DIAGRAM



SECTION 5

DEVELOPMENT AND DESIGN STANDARDS AND CRITERIA, PARKS AND OPEN SPACE GUIDING PRINCIPLES

33.562.300.D

5. Development and design standards and criteria. The Northwest Master Plan must set out how specific development and use proposals will be reviewed, and the standards, guidelines, and approval criteria used to evaluate each proposal. The Northwest Master Plan may include standards that are in addition to or instead of standards in other sections of the Zoning Code. The Northwest Master Plan must address such things as height limits, setbacks, FAR limits, landscaping requirements, parking requirements, entrances, sign programs, view corridors and façade treatments.

Because the Northwest Master Plan is used in the EX zone, design review is required. The Northwest Master Plan must describe how design review will be implemented in the plan area. Generally, the Community Design Guidelines and Community Design Standards will apply; however, the Northwest Master Plan may augment those standards and guidelines for the area covered by the Northwest Master Plan.

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43	MAP 05-1 MAXIMUM HEIGHT
44	MAP 05-2 MAXIMUM FLOOR AREA RATIO
45	MAP 05-3 RETAIL SALES AND SERVICE - SINGLE USE OVER 20,000 SF
46	MAP 05-4 REQUIRED BUILDING LINES
47	MAP 05-5 REQUIRED GROUND FLOOR RETAIL SALES, SERVICE, OR NEIGHBORHOOD FACILITY USES
48	MAP 05-6 STANDARDS ON STREETS, SQUARES, PARKS, AND OTHER OPEN SPACES.
49	NW MASTER PLAN DESIGN GUIDELINES
72	OPEN SPACE DESIGN GUIDELINES
79	DESIGN PRINCIPLES FOR PARKS & OPEN SPACES
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NW MASTER PLAN DEVELOPMENT AND DESIGN STANDARDS, PARKS AND OPEN SPACE DESIGN GUIDELINES

The entire Con-way Master Plan property will remain subject to all applicable provisions of the City of Portland Zoning Code, including the base zone regulations, except as follows:

- The alternative design review procedure provided in Chapter 33.218.015 shall not apply;
- Underlying base zone allowed uses for the property zoned IG1 on Block 16 shall be expanded to also include all allowed uses for the EXd zone;
- Parking standards of Chapter 33.266 shall apply except for instances wherein there is a conflict between Con-way parking rations provided in Section 5 of this Master Plan and Chapter 33.266; and in those cases, the Con-way parking ratios provided in Section 5 shall supersede Chapter 33.266 and shall control;
- If conflicts occur between the NW Master Plan and the City of Portland Zoning Code requirements, the requirements of the NW Master Plan shall prevail;
- Interim phase neighborhood square development shall not be subject to Design Review; and
- Parcels owned by Con-way are not subject to non-conforming upgrades per Chapter 33.258.
- Amendments to the Master Plan will be processed pursuant to Section 8.

The following standards and design guidelines apply to all development proposals and major remodels of existing buildings in the NW Master Plan Area and are in addition to all other applicable sections of the City of Portland Zoning Code, except as outlined above.

Development applications within the NW Master Plan are subject to the requirements of Chapter 33.825-Design Review. The procedures for design review, per Chapter 33.825.025 apply within the NW Master Plan area.

MODIFICATIONS

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the design review process, as provided for in Chapter 33.825.040- Modifications That Will Better Meet Design Review Requirements. These modifications are done as part of design review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through design review

may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

- A. Better meets design guidelines. The resulting development will better meet the applicable design guidelines; and
- B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.

If the location or configuration of existing buildings make compliance with these standards and guidelines difficult or not possible, the specific standard may be modified or guideline may be waived through Design Review if the review body finds that the applicant has shown that the following approval criteria are met:

- A. Existing Building Constraints. The existing building to be redeveloped is constrained in such a manner that meeting the standards and guidelines will not be practicable; and
- B. Purpose of the standard. On balance, the redevelopment proposal will be consistent with the purpose of the standard or guidelines for which a waiver is requested

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8.

STANDARDS ON STREETS AND OPEN SPACES

9.

PARKING STANDARDS

10.

SQUARE STANDARDS

NW MASTER PLAN DESIGN STANDARDS

1. MAXIMUM HEIGHT

The maximum building heights allowed are shown on Map 05-01.

2. MAXIMUM AND MINIMUM FLOOR AREA RATIO AND USES

- 2.A. The maximum floor area ratios for the entire Master Plan area are 3:1 and are shown on Map 05-02.
- 2.B. Floor area ratios may exceed 3:1 on individual sites through floor area transfer

options described in Standard 4 below.

- 2.C. The minimum floor area ratios for all individual blocks except 290 are 1.5:1. The term “block” for purposes of this Master Plan shall mean the numbered land parcels shown on Map 01-1.
- 2.D. The total new retail sales and service uses within the NW Master Plan area shall not exceed 150,000 square feet of net building area.
- 2.E. The total new commercial office uses within the NW Master Plan area shall not exceed 450,000 square feet of net building area. New office uses shall be allowed in addition to Con-way’s current use, as indicated on Map 02-1.

3. RETAIL SALES AND SERVICE USES ON BLOCK 296

The following standards apply in addition to the requirements of Section 33.562.110.

- 3.A. One retail sales and service use exceeding 20,000 square feet is allowed within the NW Master Plan Area on Block 296 as long as the following conditions are met:

3.A.1. The single use shall not exceed 40,000 square feet of net building area;

3.A.2. The single use must be a supermarket as defined in title 33.910.030; and

3.A.3. The block containing the single use supermarket must be the site shown on Map 05-03.
- 3.B. The single use on block 296 is permitted to transition to other uses in the future without amending the NW Master Plan provided that the single use space on block 296 is subdivided into multiple spaces. Each single retail use shall be no larger than 20,000 square feet of net building area.
- 3.C. The single use supermarket may be transferred to another block if the original single use is demolished or reconfigured as described in Section 3.B. If transferred, Section 3.A.1 and 3.A.2 shall be met. The approval process described in Chapter 33.562.300.F shall be used in order to approve the transfer of the single use to another block within the Master Plan area.
- 3.D. If a single use supermarket is not proposed for block 296, the requirements of Standard 3 do not apply.

4. TRANSFER OF FLOOR AREA WITHIN THE NW MASTER PLAN AREA¹

In the NW Master Plan Area, floor area may be transferred between sites. The sites are not required to be abutting; however, both the sending site and the receiving site must be located within the NW Master Plan Area. Floor area transfers are subject to the following:

- 4.A. Buildings on each site may not exceed the height limit established for that site by the regulations of this Master Plan;
- 4.B. The property owners must execute a covenant with the City that is attached to and recorded with the deed of both the sending and receiving sites reflecting the respective increase and decrease of potential floor area. The covenant must meet the requirements of 33.700.060.

5. NEIGHBORHOOD FACILITIES WITHIN THE NW MASTER PLAN AREA²

- 5.A. Purpose. This regulation encourages creation of facilities to serve those who live and work in the NW Master Plan Area. These facilities are necessary elements of a neighborhood.
- 5.B. Standards. In the NW Master Plan Area, floor area used for specified neighborhood facilities is not counted towards maximum FAR for the NW Master Plan area. The specified neighborhood facilities are public schools, public community centers, daycare facilities for children, public libraries and full service bike stations. To qualify for this provision, the following requirements must be met:

5.B.1. Schools. Floor area to be used for public schools does not count towards maximum FAR if the school will be operated by or for a public school district.

5.B.2. Daycare. Floor area to be used for daycare facilities for children does not count towards maximum FAR.

5.B.3. Libraries. Floor area to be used for public libraries does not count towards maximum FAR if the library will be operated by the Multnomah County Library or does not charge membership fees.

5.B.4. Public community centers. Floor area to be used for community centers does not count towards maximum FAR. Public community centers are not for exclusive use by residents of a site and their guests.

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10.

SQUARE STANDARDS

1 Adapted from Central City Plan District, South Waterfront Subdistrict 33.510.200.G.

2 Adapted from Central City Plan District, North Pearl Subdistrict 33.510.200.I.

- 5.B.5

Full service bike stations. Floor area up to 2,500 square feet per station to be used for full service bike stations does not count towards maximum FAR. Full service bike stations provide a wide range of amenities, including secure indoor bicycle parking, ADA-compliant showers, restrooms, lockers, bicycle self-repair stand with tools, and public transit information. Access is open to the public, or may be limited to members and users can choose from a range of membership options ranging from daily to annual plans.
- 5.B.6.

All facilities. All neighborhood facilities must meet the following:

5.B.6.a.

The floor area of the facility must be reserved for the exclusive use of the neighborhood facility for at least 20 years from the date a certificate of occupancy is issued for the qualifying floor area. No uses other than those listed in this subsection are allowed.

5.B.6.b.

The applicant must document that there is a binding agreement with an operator for each facility. This documentation must be submitted with the application for design review;

5.B.6.c.

The property owner must execute a covenant with the City which is attached to and recorded with the deed of the site. The covenant must ensure that the owner will reserve the floor area as specified in 5.B.6.a; and

5.B.6.d

The covenant must comply with the requirements of Section 33.700.060.

5.B.6.e

In the event that the neighborhood facility is no longer operational for a period of at least 12 months due to loss of funding or other economic factor, then the property may be used for another permitted use other than a neighborhood facility. Prior to using the property for a non-neighborhood facility, the property owner will provide documentation to the City that demonstrates that the facility has not been used as a neighborhood facility for a minimum of 12 months and the City will then abandon the covenant and shall promptly record documentation releasing the covenant so that the covenant no longer runs with the land.

6. REQUIRED BUILDING LINES³

- 6.A.

Purpose. Required building lines are intended to enhance the urban quality of the NW Master Plan Area.

- 6.B.

Sites and development subject to the building line standard. Sites subject to this standard are shown on Map 05-4.
- 6.C.

Building line standards. Development proposals and major remodeling projects, along a frontage containing a required building line, must comply with this standard. The building must extend to the street lot line along at least 75 percent of the lot line.

7. SPECIAL REQUIRED GROUND FLOOR RETAIL SALES, SERVICE, OR NEIGHBORHOOD FACILITY USES ON NW 21ST AVENUE AND BUILDINGS THAT FRONT THE SQUARE⁴

- 7.A.

Purpose. This requirement ensures that Retail Sales, Service, or Neighborhood Facility uses are developed along NW 21st Avenue; these uses activate and enrich the public realm. The requirement specifically focuses on Retail Sales and Service uses because they generate more activity and interaction within the public realm than do other active ground floor uses, and help to establish and reinforce a lively and vibrant public realm along NW 21st Avenue.
- 7.B.

Where this regulation applies. This regulation applies to the areas shown on Map 05-5 and on buildings that front the square. Development proposals, or major remodeling on the portion of a site within the areas shown on Map 05-5, must meet the standards of this subsection.
- 7.C.

Standards. Buildings must be designed and constructed to accommodate Retail Sales and Service uses, or Neighborhood Facilities as described in Standard 5. This standard must be met along at least 75 percent of the ground floor walls, as depicted on Map 05-5. Ground floor wall areas include the exterior wall areas up to 12 feet above the finished grade.
- 7.D

Areas designed to accommodate Retail Sales and Service or Neighborhood Facilities uses must meet the following standards:

7.D.1.

The distance from the finished floor to the bottom of the structure above must be at least 16 feet. The bottom of the structure above includes supporting beams;

7.D.2.

The area must be at least 50 feet deep, measured from the street facing façade (building services, vertical shafts, and underground garage entrances may intrude up to 10 percent of the required area;

7.D.3.

At least 75 percent of the area of the ground floor wall area must be windows and doors;

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3 Adapted from 33.510.215, Central City Plan District.

4 Adapted from 33.510.252.D Central City Plan District, South Waterfront.

- 7.D.4. The area may be designed to accommodate a single tenant or multiple tenants. In either case, the area must meet the standard of the Accessibility Chapter of the State of Oregon Structural Specialty Code. This code is administered by BDS; and
- 7.D.5. Parking is not allowed in the ground floor areas designed to meet the standards of this subsection.

8. STANDARDS ON STREETS AND OPEN SPACES⁵

- 8.A. Purpose. These regulations reinforce the continuity of the pedestrian-oriented environment, provide a pleasant, rich and diverse pedestrian experience by connecting activities occurring within a structure to adjacent sidewalk areas, and also help to maintain a healthy urban district with architectural elements or improvements that provide visual interest and interrelate with the pedestrian environment.
- 8.B. Where these regulations apply. These regulations apply to sites shown on Map 05-6.
- 8.C. Required windows above the ground floor. When above-grade buildings are proposed, windows must cover at least 35 percent of the area of the required façade above the ground floor wall area. This requirement is in addition to any required ground floor windows. Ground floor wall areas include all exterior wall areas up to 12 feet above grade.
- 8.D. Ground floor active use standard. In order to accommodate active uses that include but are not limited to residential, retail, lobbies, commercial, office, schools and colleges, community service, religious institutions, daycare, community centers, and libraries, the ground floor of buildings (when proposed) must be designed and constructed as follows. This standard must be met along at least 50 percent of the ground floor of walls in designated sites, per Map 05-6. Areas designed to accommodate active uses must meet the following standards:

8.D.1. The distance from the finished floor to the bottom of the structure above must be at least 16 feet. The bottom of the structure above includes supporting beams;

8.D.2. The area must be at least 25 feet deep, measured from the façade; and

8.D.3. At least 35 percent of the ground floor wall area must be windows and doors.
- 8.E. Private entrances and terraces facing pedestrian accessways. Areas to provide

for stairs, stoops, and other private entrance features are permitted within a pedestrian accessway up to a depth of 10 feet. The area shall be delineated at the boundary with the accessway by including a low fence, wall, hedge, or similar feature

- 8.F. Buildings. The top floor of all buildings taller than 75 feet shall be setback a minimum of 5 feet.
- 8.G. When above-grade buildings are not proposed, architectural elements or improvements must be provided so that they create visual interest and are inter-related with the pedestrian environment.

9. PARKING STANDARDS

The following outline describes the parking strategy for the Con-way Master Plan. Con-way will utilize parking standards and a reduction of maximum parking requirements over the life of the Master Plan to control parking in the neighborhood. The maximum parking requirements will reduce as transit, paid on-street parking and complementary land uses are constructed within the Master Plan area.

The strategy recognizes that in early phases of development parking ratios will be higher in order to reflect the existing level of transit service and relatively low amount of surrounding development. As more transit service is provided, and as the district matures with more complementary development, maximum parking ratios are reduced. The maximum parking ratio “tiers” reflect parking standards now in effect in several Central City districts. The sources of the standards are noted in footnotes. These tiered standards apply to districts that have very high transit service, high density mixed use development and paid on-street parking; conditions that are not currently found in the NW Master Plan district. Threshold actions and activities are identified that will trigger moving from one standard tier to another.

- 9.A. Proposed Parking Standards. The following parking standards are proposed for the NW Master Plan area. Three levels of maximum parking standards are identified, and threshold activities are defined that will trigger enforcement of the next level of standards. The more restrictive parking standards (levels 2 and 3) reflect standards that currently apply to other areas of the Central City that have mixed uses, high densities, paid on-street parking, and excellent transit service. For any uses not referenced below, the Standards of Chapter 33.266 apply.

- 9.A.1. Base Parking Level 1. These standards improve upon the existing regulations. These will be the parking standards for the Master Plan area until thresholds for Level 2 are met:

Phase 1 Parking Standards
Minimum requirements for all uses: None
Maximum parking standards:
All Office at 2.5 stalls per 1,000 SF
All Retail at 2.5 stalls per 1,000 SF
All Housing at 1 stall per unit

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⁵ Expands on 33.562.240 that currently applies only to main streets and streetcar within the Northwest Plan District, to streets, squares, parks and open spaces throughout the Master Plan area. Incorporates active use definitions from 33.510.226 minimum active floor area for streetcar alignments in the Central City Plan District.

9.A. 2. Parking Level 2 - Based on Lloyd District Standards. These will automatically become the parking standards for the Master Plan area when the following occurs:

Thresholds for Level 2 – each of the following must be in place:

- (a) A binding commitment to construct streetcar,
- (b) Operation of streetcar will commence in one year,
- (c) The City has installed meters for paid on-street parking.

Phase 2 Parking Standards

Minimum requirements for all uses: None

Maximum parking standards:

- All Office at 2.0 stalls per 1,000 SF
- All Retail at 2.0 stalls per 1,000 SF
- All Housing at 1 stall per unit

9.A. 3. Parking Level 3 - Based on River District 2 (CCTMP) Ultimate parking standards. These will automatically become the parking standards for the Master Plan area when the following occur:

Threshold for Level 3 - each of the following must be met:

- (a) A binding commitment to construct Con-way replacement parking garage,
- (b) Operation of the parking garage will commence in one year.

Phase 3 (Final) Parking Standards

Minimum requirements for all uses: None

Maximum parking standards:

- All Office at 1.5 stalls per 1,000 SF
- All Retail at 1.5 stalls per 1,000 SF
- All Housing at 1 stall per unit

clear height of at least 25 feet.

10.D A public access easement shall be required for the square and ground plane connection.

10.E For buildings subject to the 47-foot maximum height shown on Map 05-1, the north facing elevation of the top floor shall be set back from the main building façade a distance of no less than the height of the top floor elevation.

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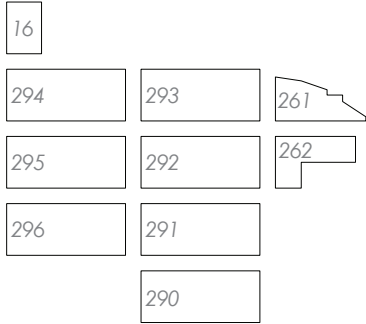
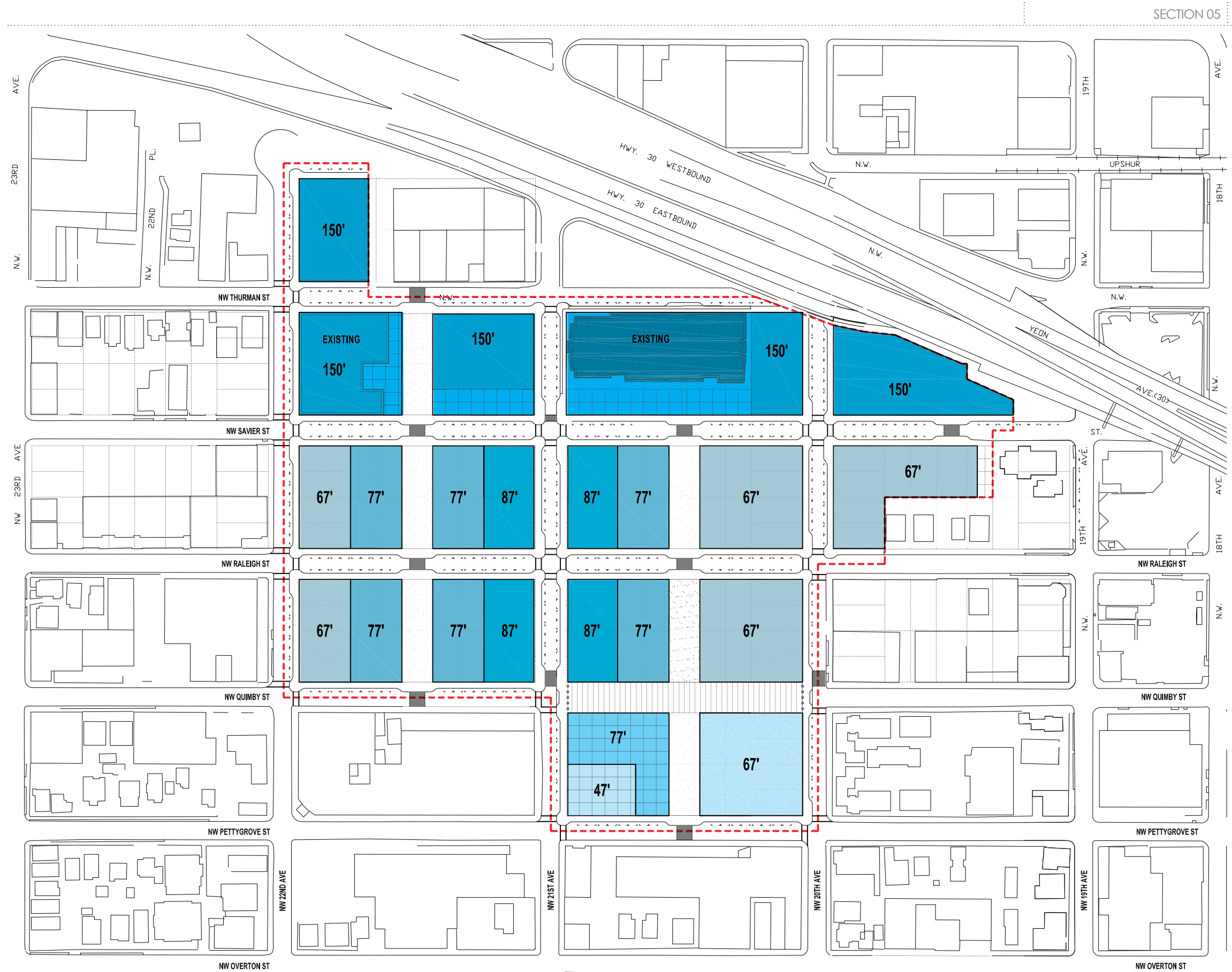
10. SQUARE STANDARDS

- 10.A Purpose. The square shall be a significant, iconic urban place, framed by active buildings on at least three sides⁶, and connected to nearby, open spaces.
- 10.B Development of the square and associated development projects shall include a square that has no dimension less than 100 feet and shall be at least 16,000 square feet in size.
- 10.C Ground plane connection between the square and neighborhood park shall be a minimum of 30-feet-wide, and if included within a building, shall have a

⁶ Framing buildings include buildings directly fronting the square and buildings fronting the north side of NW Quimby Street facing the square.

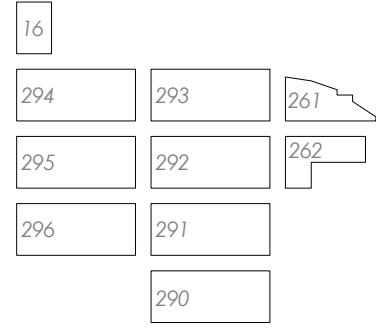
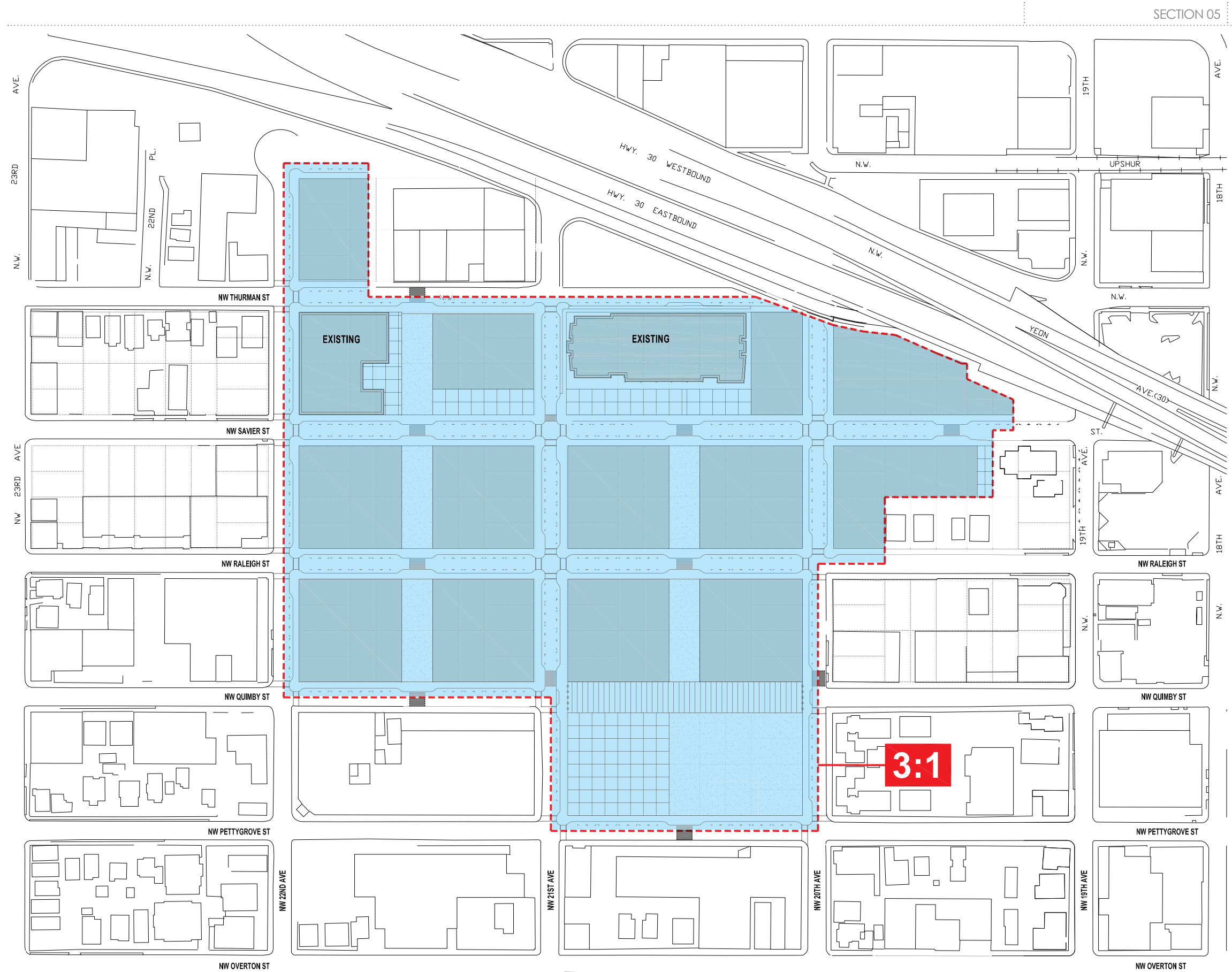
MAP 05-1
MAXIMUM HEIGHT

REFERENCED IN STANDARD 1:
MAXIMUM HEIGHT



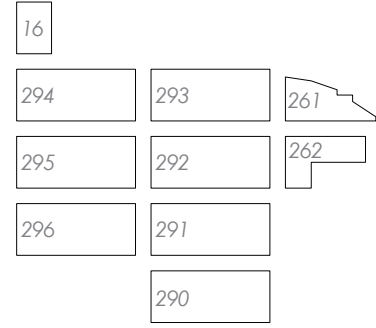
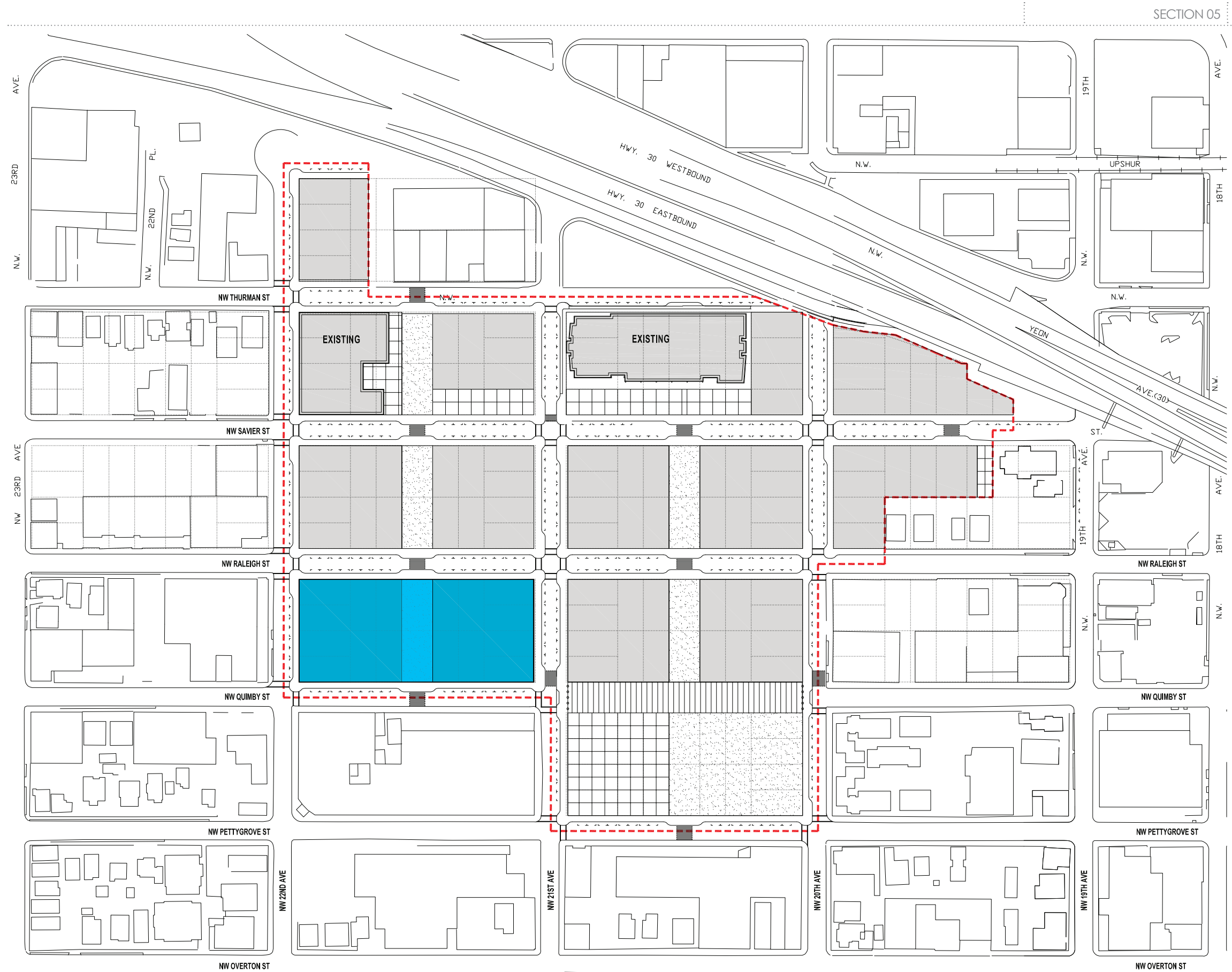
MAP 05-2
MAXIMUM FLOOR
AREA RATIO

REFERENCED IN STANDARD 2:
MAXIMUM AND MINIMUM FLOOR
AREA RATIO AND USES



MAP 05-3
RETAIL SALES AND
SERVICE - SINGLE USE
OVER 20,000 SF

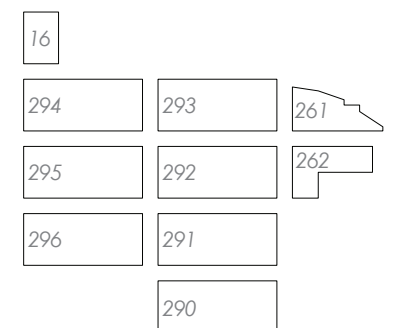
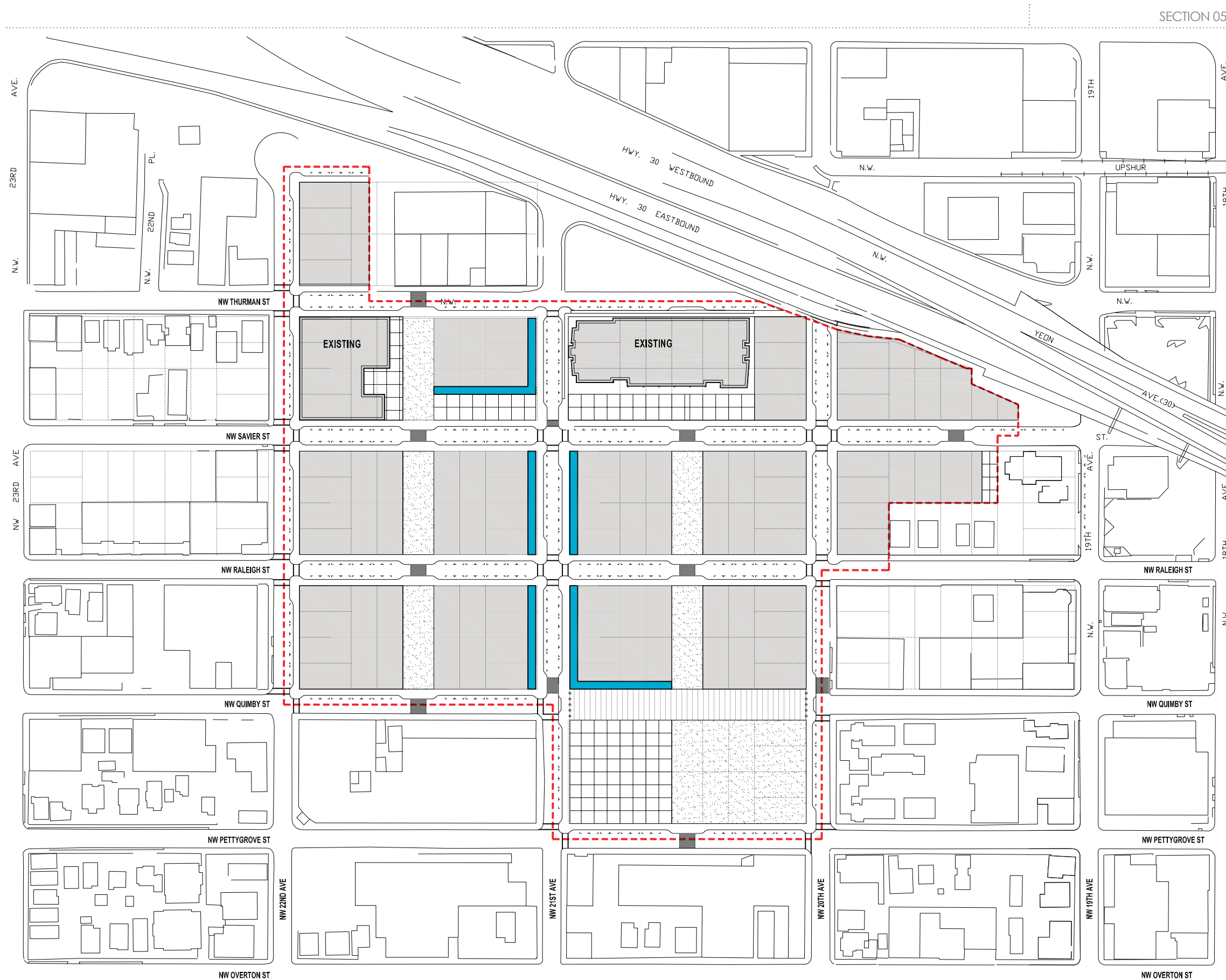
REFERENCED IN STANDARD 3:
RETAIL SALES AND SERVICE USES



MAP 05-4
REQUIRED BUILDING LINES

**REFERENCED IN STANDARD 6:
REQUIRED BUILDING LINES**

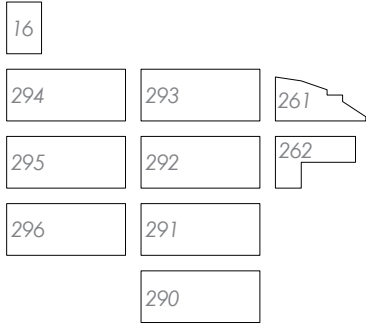
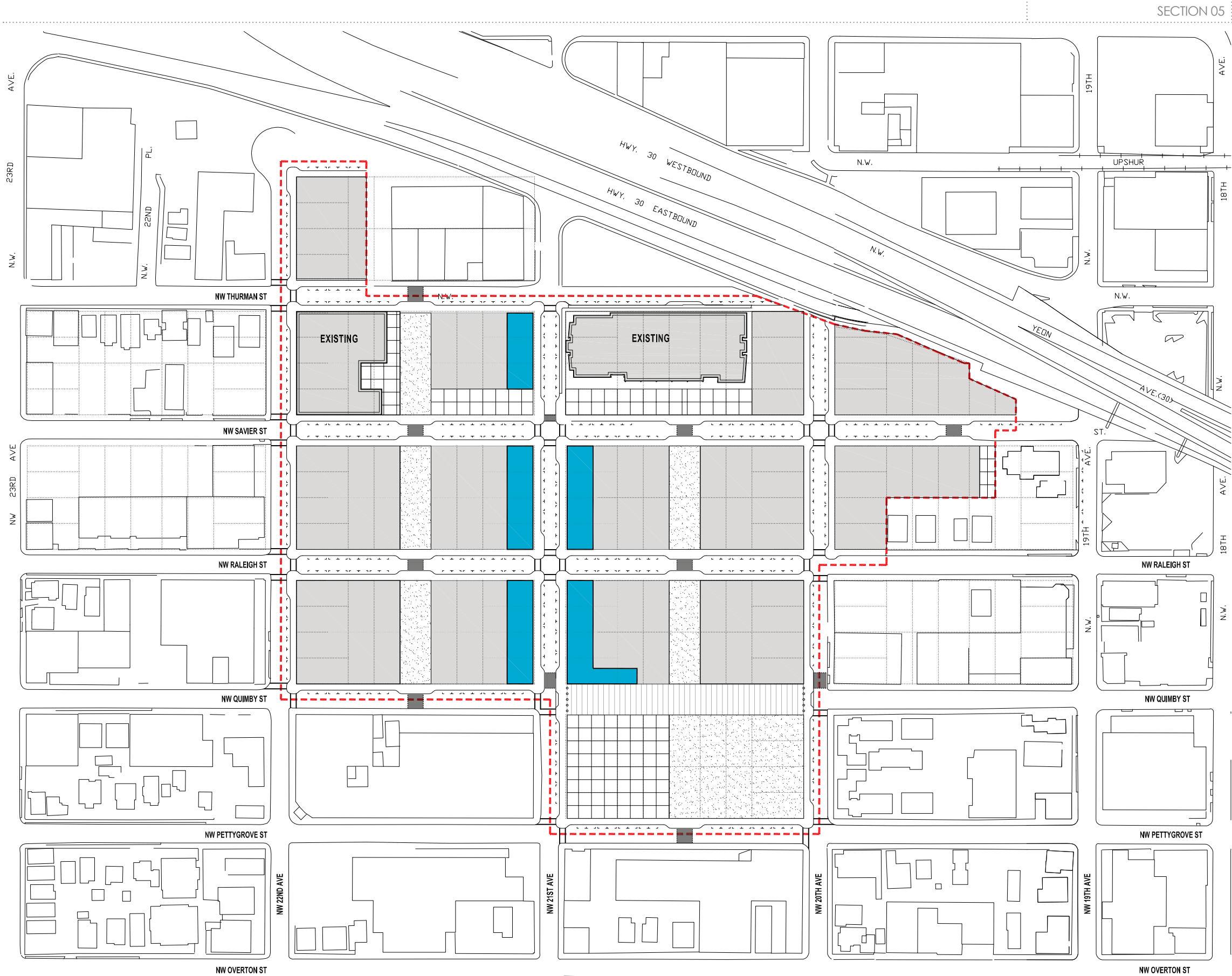
Required building lines will apply to any buildings planned as part of a park Master Plan that front NW 21st Avenue or a public square.



MAP 05-5
REQUIRED GROUND FLOOR
RETAIL SALES, SERVICE, OR
NEIGHBORHOOD FACILITY
USES

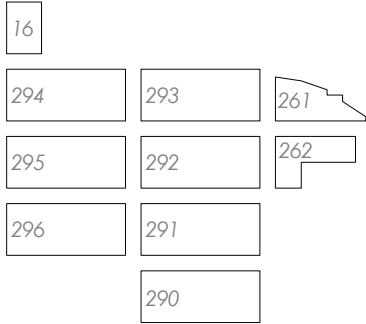
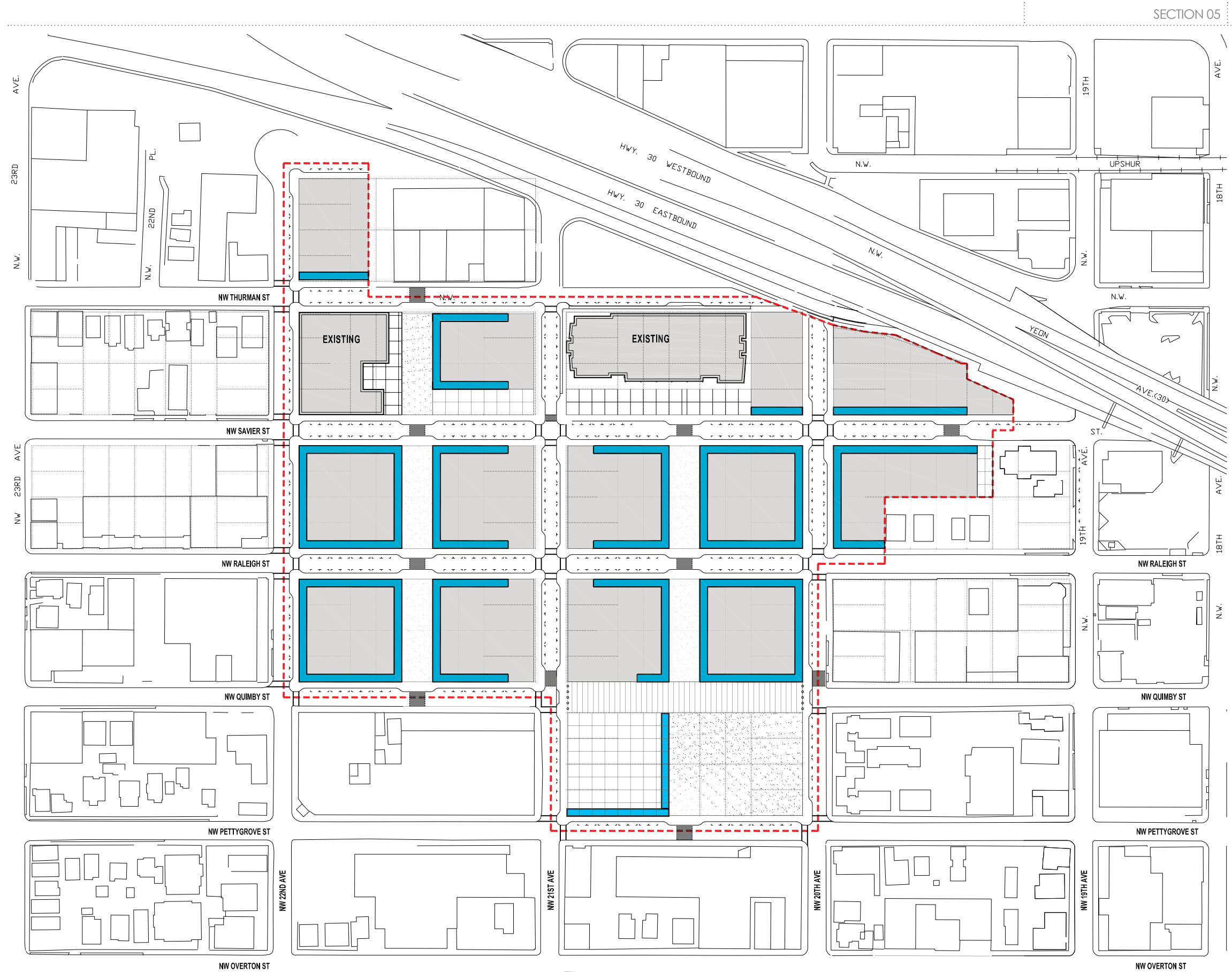
**REFERENCED IN STANDARD 7:
SPECIAL REQUIRED GROUND
FLOOR RETAIL SALES, SERVICE, OR
NEIGHBORHOOD FACILITY USES ON
NW 21ST AVENUE AND BUILDINGS
THAT FRONT THE SQUARE**

*Ground Floor Retail and Neighborhood
Facility Requirements will apply to any
buildings that front NW 21st Avenue or
the square.*



MAP 05-6
STANDARDS ON STREETS
AND OPEN SPACES.

REFERENCED IN STANDARD 8:
STANDARDS ON STREETS AND OPEN
SPACES



NW MASTER PLAN DESIGN GUIDELINES

INTRODUCTION

The existing Community Design Guidelines, along with these new seven (7) Con-way Master Plan design guidelines, are the applicable approval criteria for Design Review. Design guidelines are mandatory approval criteria that must be met as part of design review and historic design review. They inform developers and the community as to what issues will be addressed during the design review process. The guidelines state broader concepts than typical development standards in order to provide flexibility to designers, yet they are requirements.

Applicants are responsible for explaining, in their application, how their design meets each applicable guideline.

The design review process is flexible. It is intended to encourage designs that are innovative and appropriate for their locations. For this reason design guidelines are qualitative statements. Unlike objective design standards, there are typically many acceptable ways to meet each design guideline. Examples of how to address specific guidelines are included in this section for each design guideline. It is not the City's intent to prescribe any specific design solution through the design guidelines.

During the design review process, the review body must find that the proposal meets each of the applicable design guidelines. Proposals that meet all applicable guidelines will be approved; proposals that do not meet all of the applicable guidelines will not be approved.

If the review body approves the proposed design, they may add conditions to their approval to ensure the proposal's compliance with the guidelines. If the review body does not approve the proposed design, they would prefer that the applicants revise the design to address deficiencies rather than have the city impose a specific solution through conditions. They may find that such action is necessary to better achieve the goals for design review.

In some cases, a design guideline may be waived during the design review process. An applicable guideline may be waived as part of the design review process when the proposed design better meets the goals of design review than would a project that had complied with the guideline. If a waiver is requested, the applicants must explain, in their application, how the goals of design review are better met in the proposed design than would be possible if each guideline being considered for waiver was followed. Allowing the waiver of one or more guidelines during the design review process reflects the City's concern that the design guidelines not become a rigid set of requirements that stifle innovation.

GOALS OF DESIGN REVIEW:

1. Encourage urban design excellence;
2. Integrate urban design and preservation of our heritage into the development process;
3. Promote the development of diversity and areas of special character within the district;
4. Establish an urban design relationship between the district and the Northwest District as a whole;
5. Provide for a pleasant, rich and diverse experience for pedestrians;
6. Assist in creating a 18-hour district which is safe, humane and prosperous; and
7. Ensure that development proposals are at a human scale and that they relate to the scale and desired character of its setting and the Northwest District as a whole.

DESIGN GUIDELINE INDEX

1. PROVIDE HUMAN SCALE TO BUILDINGS AND EDGES ALONG SIDEWALKS, SQUARES AND PEDESTRIAN ACCESSWAYS
2. DEVELOP BUILDING EDGE VARIETY ADJACENT TO PARKS, PEDESTRIAN ACCESSWAYS AND GREENSTREETS
3. DEVELOP WEATHER PROTECTION
4. DEVELOP BUILDINGS THAT ARE APPROPRIATELY SCALED TO THE NEIGHBORHOOD. FAÇADES SHOULD BE WELL ARTICULATED AND OFFER DIVERSITY IN VOLUME AND FORM ALONG THE STREET EDGE
5. PROVIDE TRANSITIONS BETWEEN THE PUBLIC AND PRIVATE REALMS WHEN RESIDENTIAL STRUCTURES ABUT STREETS, PARKS AND PEDESTRIAN ACCESSWAYS
6. INTEGRATE HIGH-QUALITY MATERIALS AND DESIGN DETAILS THAT ARE IN LINE WITH THE CHARACTER OF THE SURROUNDING NEIGHBORHOOD
7. PROVIDE PRIVATE OPEN SPACES THAT ARE WELL INTEGRATED WITH ADJACENT DEVELOPMENT, ACT AS GATHERING PLACES DESIGNED TO ADAPT TO A VARIETY OF ACTIVITIES, ARE LINKED TOGETHER AND TO OTHER NEARBY OPEN SPACES, ARE ACCESSIBLE TO THE PUBLIC AND PROVIDE DISTINCTIVE NEIGHBORHOOD IDENTITY

GUIDELINE 1:

PROVIDE HUMAN SCALE TO BUILDINGS AND EDGES ALONG SIDEWALKS, SQUARES AND PEDESTRIAN ACCESSWAYS¹

BACKGROUND

In order to reinforce and enhance the pedestrian realm in the NW Master Plan District, buildings and edges have a responsibility to contribute to pedestrian comfort and enjoyment. Buildings and edges adjacent to walkways should provide human scale and relationships which support pedestrian needs and interests. This can be accomplished with appropriate building configuration, details, proportions, materials, and activities, and by breaking up large elevations into smaller areas.

In particular, door, window and wall treatments adjacent to pedestrianways should relate and contribute to pedestrian interest and enjoyment. Large expanses of mirrored glazing and blank flat walls are discouraged due to their impersonal character. Variation and sensitive detailing, related to the pedestrian environment, is especially encouraged within building bases. Ground floors should be prominent at the street level, with ceiling heights that are taller than the floors above. Porches and balconies are particularly encouraged. These elements provide human scale, allow for interaction between residents and passers-by, and allow for informal surveillance of the street.

In areas where vehicles are allowed (such as parking, loading, mews and driveways) use interesting architectural elements, consistent paving patterns, landscaping and site furnishings to soften the edge where they abut open spaces and to create multi-use areas that are pedestrian friendly.

GUIDELINE 1.A: PROVIDE HUMAN SCALE AND INTEREST TO BUILDINGS AND EDGES ALONG SIDEWALKS, SQUARES AND PEDESTRIAN ACCESSWAYS.

¹Derived from the North Pearl District design guidelines

GUIDELINE 1: PROVIDE HUMAN SCALE AND INTEREST TO BUILDINGS AND EDGES ALONG SIDEWALKS SQUARES AND PEDESTRIAN ACCESSWAYS.

This Guideline may be accomplished by:

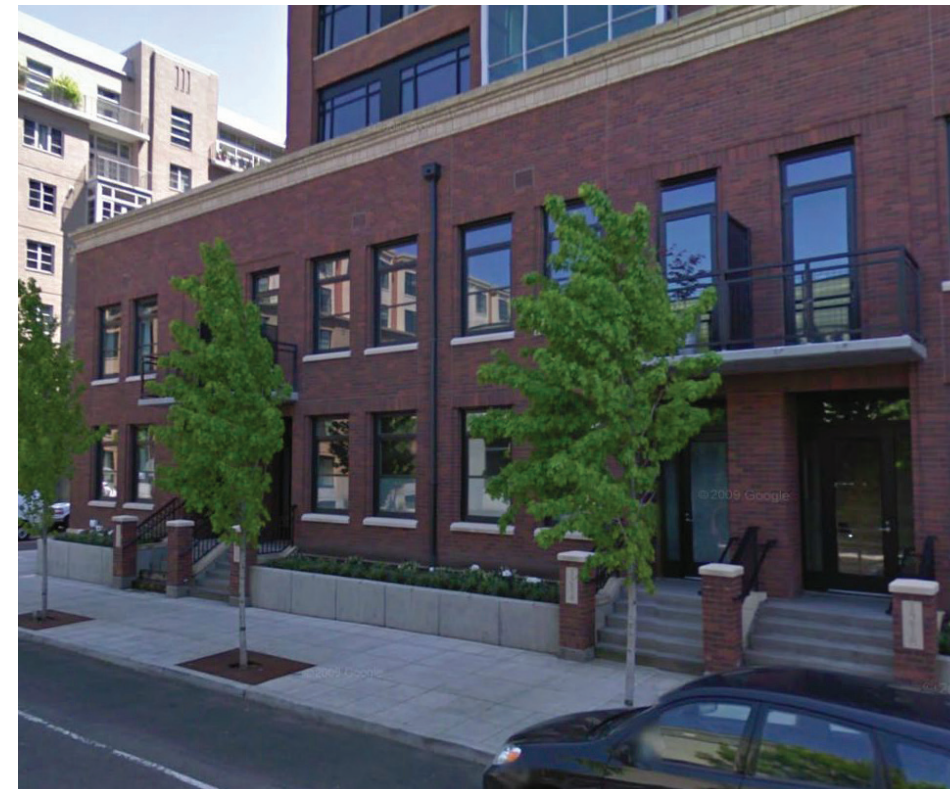
- 1.1 Where retail is incorporated, create an active outdoor environment by providing street furniture outside of ground floor retail, such as tables and chairs, signage and lighting, as well as large windows and balconies to encourage social interaction.



A fine-grained pedestrian environment is created by providing outdoor seating and large windows to its ground floor retail. Tall ground floor ceiling heights as well as a differentiated brick color add prominence to the ground level. Residential balconies offer some weather protection and articulation

This Guideline may be accomplished by:

- 1.2 Providing stoops, windows, and balconies within the ground floors of residential buildings.



This building façade achieves interest by having at least three layers, each differentiated by more than three feet in depth.

The three layers are:
1. The inset building entrance
2. The large windows
3. The balconies / stoops



Large windows, ceiling heights, balconies, private porches, and stoops along ground floor residential units allow social interaction between residents and passersby

GUIDELINE 1: PROVIDE HUMAN SCALE AND INTEREST TO BUILDINGS AND EDGES ALONG SIDEWALKS SQUARES AND PEDESTRIAN ACCESSWAYS.

This Guideline may be accomplished by:

- 1.3. Articulating building façades with human scale elements and activity.



These building incorporates many human-scale elements, including large windows, canopies, street furniture, signage, lighting, and low plants.

This Guideline may be accomplished by:

- 1.4. Incorporating human-scaled design details at the ground floor and enhanced pedestrian experience in the street environment.



Weather protection, signage, street trees, and on-street parking all contribute to a human-scaled experience at the ground floor

GUIDELINE 1: PROVIDE HUMAN SCALE AND INTEREST TO BUILDINGS AND EDGES ALONG SIDEWALKS SQUARES AND PEDESTRIAN ACCESSWAYS.

This Guideline may be accomplished by:

- 1.5. Softening the edge and creating strong visual connections where public open spaces abut private streets, parking areas and other private urban spaces.



Portland's Director Park, where paving materials and furnishings extend from the plaza into the public right-of-way, helping to blur the urban edges.



A well-designed urban edge to this parking lot at Portland's EcoTrust headquarters allows for multiple uses and activities to occur during the week.

GUIDELINE 2: DEVELOP URBAN EDGE VARIETY ADJACENT TO PARKS, PEDESTRIAN ACCESSWAYS AND GREENSTREETS²

BACKGROUND

Many different qualities and opportunities exist along the Pedestrian Accessways and Parks within the NW Master Plan Area. Developing edge variety within development proposals will make these spaces some of the most lively and enjoyable within the district. Building mass and/or edges should express a diversity of building forms to avoid the creation of a “wall” along accessways and parks.

Buildings should be programmed with ground floor active uses that contribute to the public realm.

Active ground floor uses and articulation of these building façades with human-scale elements will contribute to the diversity and variety of these edges, strengthening the livelihood of these open spaces.

Building elements, such as bay windows, balconies, roof terraces, awnings or large windows with locations for plants (such as in window boxes or on window sills), are encouraged to create a human scale at the lower levels of buildings.

In areas where vehicles are allowed (such as parking, loading, mews and driveways) use interesting architectural elements, consistent paving patterns, landscaping and site furnishings to soften the edge where they abut open spaces and to create multi-use areas that are pedestrian friendly.

GUIDELINE 2: PROGRAM USES ON THE GROUND LEVEL OF BUILDINGS ADJACENT TO PARKS, ACCESSWAYS AND GREENSTREETS THAT ACTIVATE AND EXPAND THE PUBLIC REALM. DESIGN THE LOWER STORIES OF BUILDINGS TO INCLUDE ELEMENTS THAT ACTIVATE USES AND ADD VARIETY AND INTEREST TO THE BUILDING FAÇADES.

²Derived from the South Waterfront design guidelines

**GUIDELINE 2:
PROGRAM USES ON THE GROUND LEVEL OF BUILDINGS ADJACENT TO PARKS, ACCESSWAYS AND GREENSTREETS THAT ACTIVATE AND EXPAND THE PUBLIC REALM. DESIGN THE LOWER STORIES OF BUILDINGS TO INCLUDE ELEMENTS THAT ACTIVATE USES AND ADD VARIETY AND INTEREST TO THE BUILDING FAÇADES.**

This Guideline may be accomplished by:

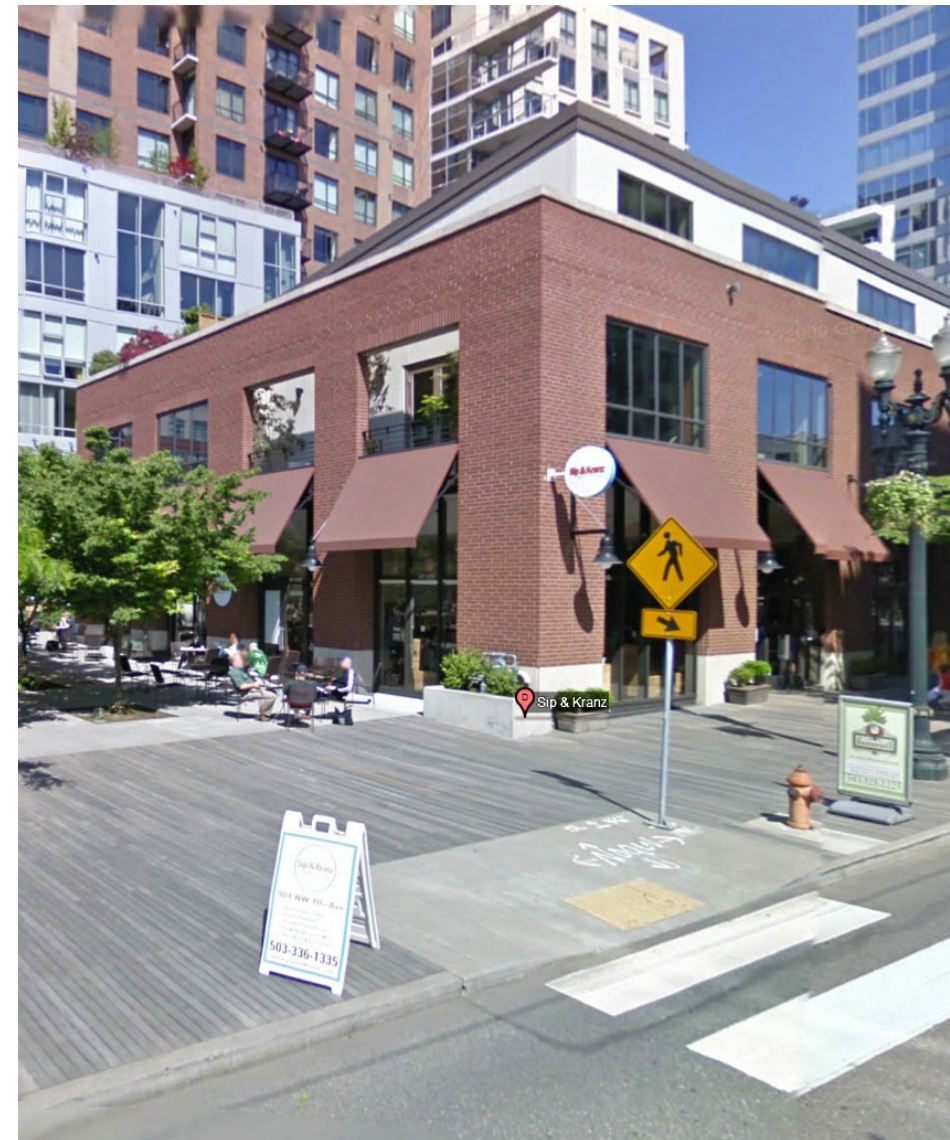
- 2.1. Configuring the building's mass to be perpendicular to the park, pedestrian accessway, or greenstreet.



This is the end of one wing of the Tanner Place Condominiums in the River District. Narrower sections of the building face the pedestrian accessway to the south, reducing the overall mass of the building facing the pedestrian and bicycle-only environment. This plan layout would produce a dynamic set of building façades facing the open space and would also create opportunities for semi-public outdoor spaces, such as the entry courtyard to the right.

This Guideline may be accomplished by:

- 2.2. Articulating the façade plane to step down to the open space.



This building on the north end of Jamison Square park in Portland's Pearl District steps down toward the south. The larger volumes are biased toward the north. This allows the building to scale down to a human scale as it gets closer to the public realm.

**GUIDELINE 2:
PROGRAM USES ON THE GROUND LEVEL OF BUILDINGS ADJACENT TO PARKS, ACCESSWAYS AND GREENSTREETS THAT ACTIVATE AND EXPAND THE PUBLIC REALM. DESIGN THE LOWER STORIES OF BUILDINGS TO INCLUDE ELEMENTS THAT ACTIVATE USES AND ADD VARIETY AND INTEREST TO THE BUILDING FAÇADES.**

This Guideline may be accomplished by:

- 2.3. Programming the ground floor of buildings along the open space with active uses, especially at intersections with public streets.



Active ground floor uses, such as those along this accessway, strengthen the sense of public ownership and activity of the public realm.



Active uses, such as this restaurant's outdoor seating area will help to establish important transitions between open spaces and more active streets, contributing to the movement and livelihood of the district.

This Guideline may be accomplished by:

- 2.4. Providing stoops, windows, balconies and decks facing the open space while defining private and public uses.



This image shows an "eyes on the street" approach toward the public right-of-way. Windows, balconies, and stoops are all oriented toward the open space, and a clear demarcation of private and public spaces is included.

**GUIDELINE 2:
PROGRAM USES ON THE GROUND LEVEL OF BUILDINGS ADJACENT TO PARKS, ACCESSWAYS AND GREENSTREETS THAT ACTIVATE AND EXPAND THE PUBLIC REALM. DESIGN THE LOWER STORIES OF BUILDINGS TO INCLUDE ELEMENTS THAT ACTIVATE USES AND ADD VARIETY AND INTEREST TO THE BUILDING FAÇADES.**

This Guideline may be accomplished by:

- 2.5. Developing stormwater management facilities within the pedestrian accessways.



This image is of a landscaped pedestrian accessways in the South Waterfront. The swale and movement paths on either side give an indication of a stormwater design that could be developed within pedestrian accessways.

This Guideline may be accomplished by:

- 2.6. Providing a range of pedestrian experiences, ranging from more passive greenways to more active pedestrian corridors.



This image is of a hardscaped pedestrian street. Although trees and plants are integrated into the street design, the focus is on hard surfaces. These allow people to walk, bike, and move around freely. This also offers space for tables, chairs, and other amenities to spill out into the public right of way, activating the street.

GUIDELINE 3: DEVELOP WEATHER PROTECTION³

BACKGROUND

The design of buildings and their relationships to the sidewalk environment are critical factors in the development of an active and vital pedestrian environment. This relationship is enhanced when the effects of environmental factors such as rain, wind, glare, shadow, reflection, and sunlight on pedestrian movement are engaged in the design process. Developing buildings with sidewalk-level façades that balance the different aspects of these environmental factors strengthens the pedestrian environment.

The size and placement of building elements such as awnings,arcades, trellises, recessed windows or entries, and landscaping contribute to the successful engagement of environmental factors at the sidewalk-level of a building. In addition, at higher building elevations, the incorporation of exterior sun-shading components that respond to different façade orientations can significantly reduce a building's overall energy costs. The successful integration of these and other building elements with the building's design concept provide weather protection for the pedestrian, enhance the character of the building, and add to the overall diversity of neighborhood development.

GUIDELINE 3: DEVELOP INTEGRATED WEATHER PROTECTION SYSTEMS AT THE SIDEWALK LEVEL OF BUILDINGS TO MITIGATE THE EFFECTS OF RAIN, WIND, GLARE, SHADOW, REFLECTION, AND SUNLIGHT ON THE PEDESTRIAN ENVIRONMENT.

³Adapted from the Central City Fundamental Design Guidelines B.6

GUIDELINE 3: DEVELOP INTEGRATED WEATHER PROTECTION SYSTEMS AT THE SIDEWALK LEVEL OF BUILDINGS TO MITIGATE THE EFFECTS OF RAIN, WIND, GLARE, SHADOW, REFLECTION, AND SUNLIGHT ON THE PEDESTRIAN ENVIRONMENT.

This Guideline may be accomplished by:

- 3.1. Incorporating comprehensive weather protection.

This section of canopy is only part of the entire system that has been designed into the overall concept of Union Station. This canopy not only provides complete rain protection at the main entrance to the building, but it also creates deep shade in the summer and has incorporated hanging flower baskets.



This Guideline may be accomplished by:

- 3.2. Providing weather protection at and near building access points.



This trellis at the Starbucks coffee shop on Pioneer Courthouse Square acts as a transition from the outdoors into the building, and vice versa. On the eastern side of the building (inset images) the trellis provides an excellent framework for the growth of vines. The vines provide different types of weather protection depending on the season; the upper image was taken in the summer, while the lower image was captured in the winter.



GUIDELINE 3: DEVELOP INTEGRATED WEATHER PROTECTION SYSTEMS AT THE SIDEWALK LEVEL OF BUILDINGS TO MITIGATE THE EFFECTS OF RAIN, WIND, GLARE, SHADOW, REFLECTION, AND SUNLIGHT ON THE PEDESTRIAN ENVIRONMENT.

This Guideline may be accomplished by:

- 3.3. Incorporating multifunctional weather protection.

These two examples of different awning systems (at the South Park Restaurant on the top, and at the Brewery Blocks below) provide weather protection for the different seasons.



GUIDELINE 4: DEVELOP BUILDINGS THAT ARE APPROPRIATELY SCALED TO THE NEIGHBORHOOD. FAÇADES SHOULD BE WELL ARTICULATED AND OFFER DIVERSITY IN VOLUME AND FORM ALONG THE STREET EDGE⁴

BACKGROUND

Northwest Portland is made up of blocks that contain multiple buildings with small footprints placed close together. Certain portions of this neighborhood are built to much higher densities than others. The Con-way Master Plan seeks to build to density levels consistent with the highest density levels of the neighborhood. These high-density areas of the NW Plan District neighborhood are a diverse patchwork of buildings at various heights and densities. Block faces in the neighborhood are extremely varied, with multiple buildings along each block face, providing a wonderfully diverse urban environment. The goal for the Northwest Master Plan area is to have buildings that contribute to this existing character, but are modern and contemporary in form and functionality. This guideline gives examples of how this can be achieved.

A quarter-block massing language is predominant in the majority of larger, high-density, residential buildings that exist in the NW neighborhood, which contributes strongly to the rich human scale of the neighborhood. New buildings are to be designed to be consistent with this massing language where possible. Where buildings are larger than a quarter-block (100 feet) in length, use multi-dimensional design features to break up any large façade surfaces that face the street. There are several design features or approaches that can be used to accomplish this, including, but not limited to: balconies, bay windows, changes in material, changes in façade plane of at least 3 feet to create multiple façade elements, and limiting any single flat façade plane to approximately 50 feet in any direction. These same design features are encouraged in the make-up of quarter-block façades but critical in larger façades.

Integrate the different building and design elements including, but not limited to, construction materials, roof, entrances, as well as window, doors, signs and lighting systems to achieve a coherent composition.

Suggested ways to accomplish this include:

1. Developing coherent compositions (using similar building façade elements throughout the various components—parking, retail, tower, etc.);
2. Integrating existing buildings into new building compositions;
3. Developing an integrated strategy at the sidewalk level of the project (e.g., Portland's Pacwest Tower has rounded corners on the building's base and tower; other sidewalk elements: canopies);
4. Incorporating unifying elements (e.g., Mackenzie Lofts has an industrial character at all levels of the building composition);
5. Integrating different building volumes with similar materials; and
6. Using a common system of building forms.

GUIDELINE 4: DEVELOP BUILDINGS THAT ARE APPROPRIATELY SCALED TO THE MOST DENSE AREAS OF THE NEIGHBORHOOD. FAÇADES SHOULD BE WELL ARTICULATED AND OFFER DIVERSITY IN VOLUME AND FORM ALONG THE STREET EDGE.

⁴New guideline specific to the NW Master Plan

GUIDELINE 4: DEVELOP BUILDINGS THAT ARE APPROPRIATELY SCALED TO THE NEIGHBORHOOD. FAÇADES SHOULD BE WELL ARTICULATED AND OFFER DIVERSITY IN VOLUME AND FORM ALONG THE STREET EDGE.

This guideline may be accomplished by:

- 4.1. Develop buildings with a maximum floor plate size of 10,000 square feet, resulting in at least two buildings per block face.



These housing buildings in Northwest Portland are each developed on parcels that are roughly one-quarter block in size. The result is a street edge that contains multiple buildings, and lots of visual diversity.

In principle, this guideline can be met by designing building façades along full block (200 ft) faces that reflect a quarter block vernacular: at least two distinctive elevation compositions. Distinction between elevations may be achieved with different heights, materials, fenestration patterns, setbacks and separation, or other significant design features that accomplish the same goals.

This guideline may be accomplished by:

- 4.2. Provide a series of multiple, narrow, and deep spaces along streets in order to guarantee diversity along a block face.

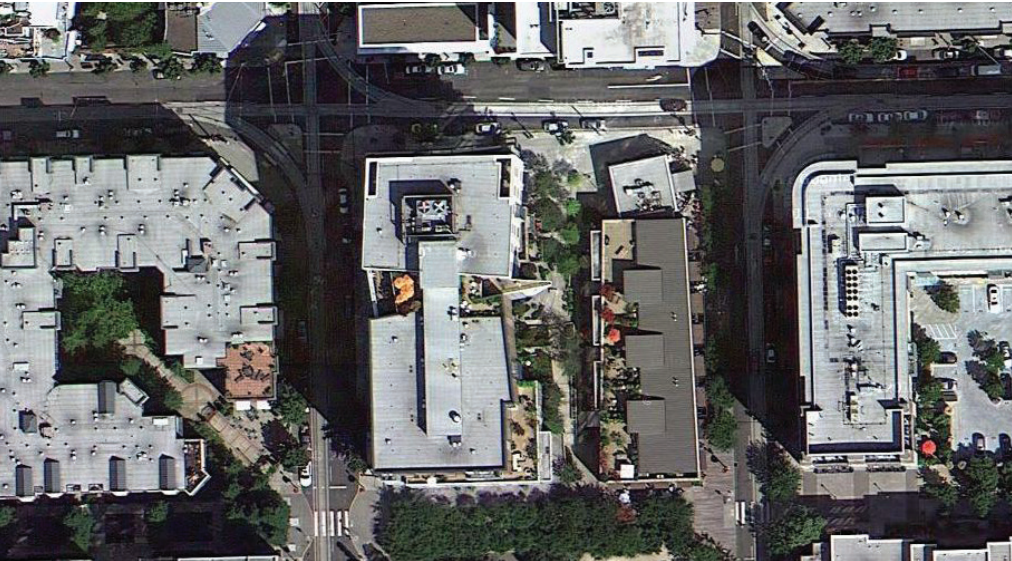


This building in southeast Portland takes up a full city block face, but has multiple narrow retail storefronts to help create diversity and interest along the street.

GUIDELINE 4: DEVELOP BUILDINGS THAT ARE APPROPRIATELY SCALED TO THE NEIGHBORHOOD. FAÇADES SHOULD BE WELL ARTICULATED AND OFFER DIVERSITY IN VOLUME AND FORM ALONG THE STREET EDGE.

This guideline may be accomplished by:

- 4.3. Provide a series of publicly accessible courtyards or accessways that cut through large developments.



This large development in Portland's Pearl district is an appropriate response for a large building project. The development is split up into multiple buildings, and public accessways in between buildings allow for inter-block connectivity and interest.



This row-house development in southeast Portland is an appropriate response for developments that are only housing. Central courtyards provide access for units and help to break up the building, allowing it to blend into the neighborhood.

This guideline may be accomplished by:

- 4.4. Breaking up buildings with façades longer than 100' into multiple building volume configurations. Different building volumes can be articulated through different building materials, colors, heights, or setbacks from the property line.

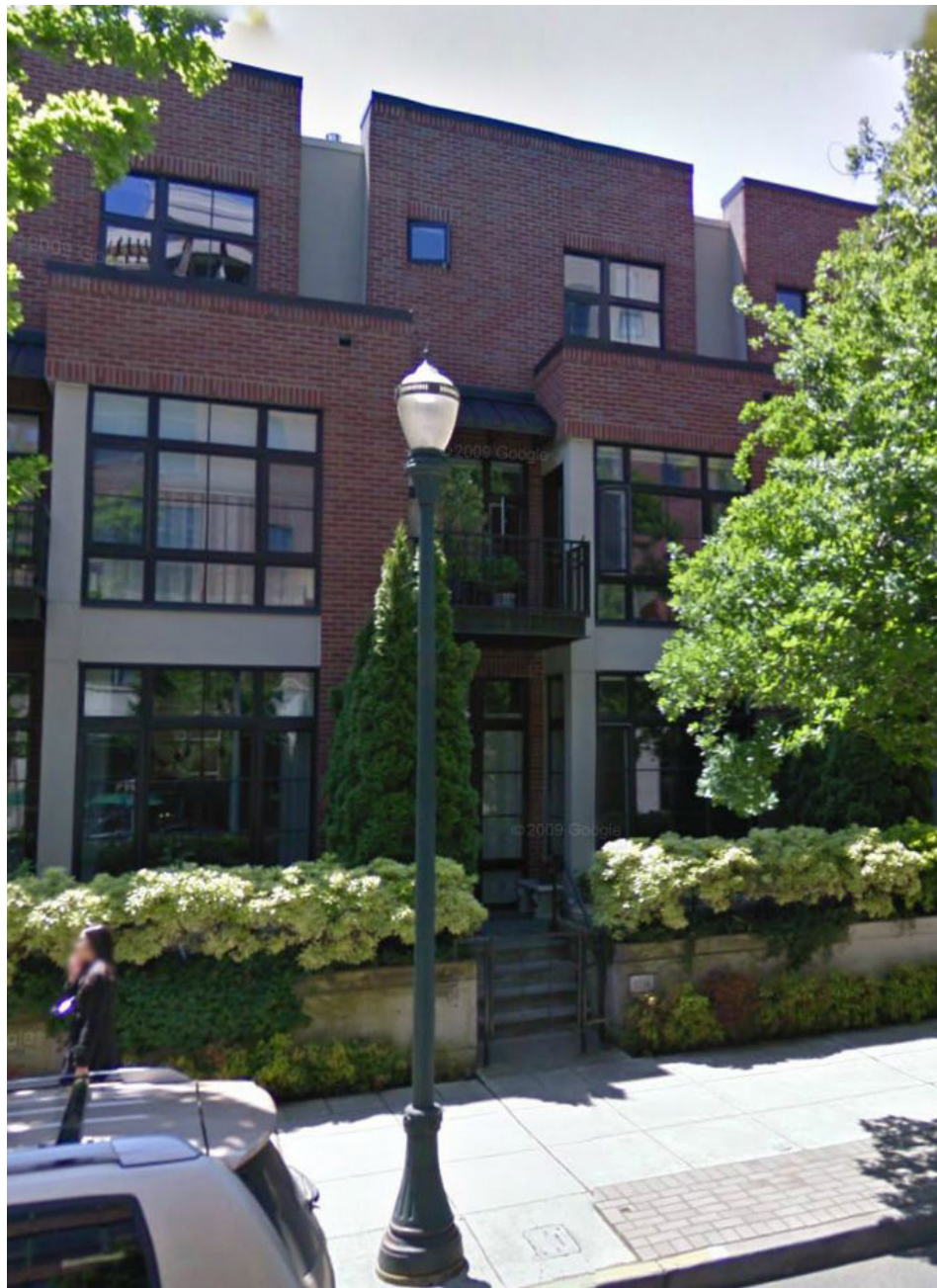


This is one large development that has been successfully articulated as different building volumes. The buildings are articulated using different heights, as well as a change in materials.

GUIDELINE 4: DEVELOP BUILDINGS THAT ARE APPROPRIATELY SCALED TO THE NEIGHBORHOOD. FAÇADES SHOULD BE WELL ARTICULATED AND OFFER DIVERSITY IN VOLUME AND FORM ALONG THE STREET EDGE.

This Guideline may be accomplished by:

- 4.5 Using divisions inherent to the building type to break up potentially monolithic building forms with horizontal plane and façade shifts.



The designers of this residential development have used projecting bays to accentuate the divisions between individual dwelling units as well as a series of planters and seating ledges. The cumulative effect of this technique results in an undulating façade plane, offering lots of visual texture.

This can also be accomplished by limiting single planes of the building to human dimensions, less than 50 feet in any direction.

This Guideline may be accomplished by:

- 4.6 Using bay windows and balconies at upper levels to break up building mass and provide eyes on the street.



Bay windows and balconies at upper levels help articulate the façade and break up the scale of the building. Different materials are used to express these parts of the buildings and make them special.

GUIDELINE 4: DEVELOP BUILDINGS THAT ARE APPROPRIATELY SCALED TO THE NEIGHBORHOOD. FAÇADES SHOULD BE WELL ARTICULATED AND OFFER DIVERSITY IN VOLUME AND FORM ALONG THE STREET EDGE.

This Guideline may be accomplished by:

- 4.7 Creating urban spaces such as alleys, gardens, courtyards by allowing breaks in the building volume to reach the ground plane. Allowing these spaces to connect to a network of streets and pedestrian passageways.



This courtyard serves as a semi-private entry for the rowhouses along the courtyard.



The mid-block connection in this image functions as a loading dock during the day, and an outdoor seating patio for an adjacent restaurant at night.

This Guideline may be accomplished by:

- 4.8 Along the NW 21st Avenue Main street and at public squares, a strong street-wall condition is desired to provide continuity to the street and to accommodate outdoor activities related to uses within ground floor spaces. Ground floor setbacks of at least 5 feet accommodate these outdoor activities.



5' setbacks on the ground floor can accommodate a variety of activities, such as these outdoor dining tables, while still allowing for pedestrian movement on the sidewalk.

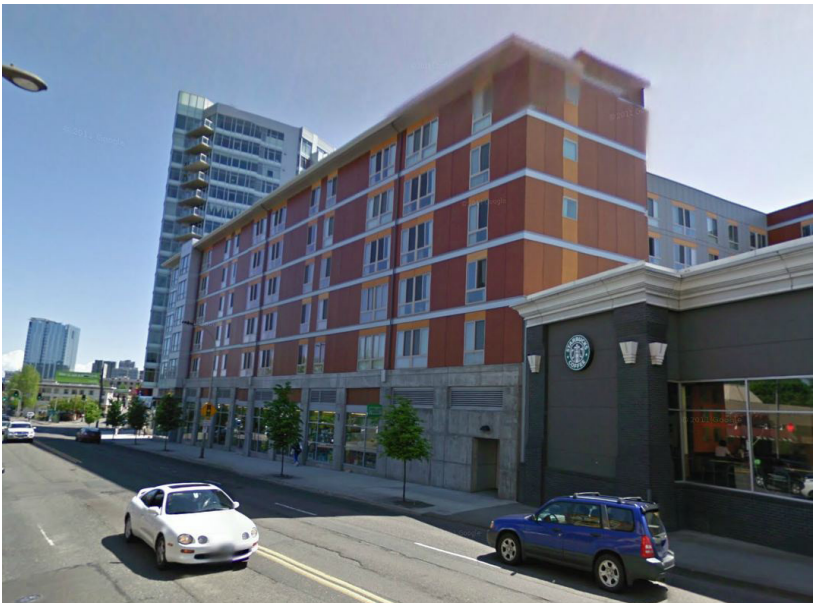
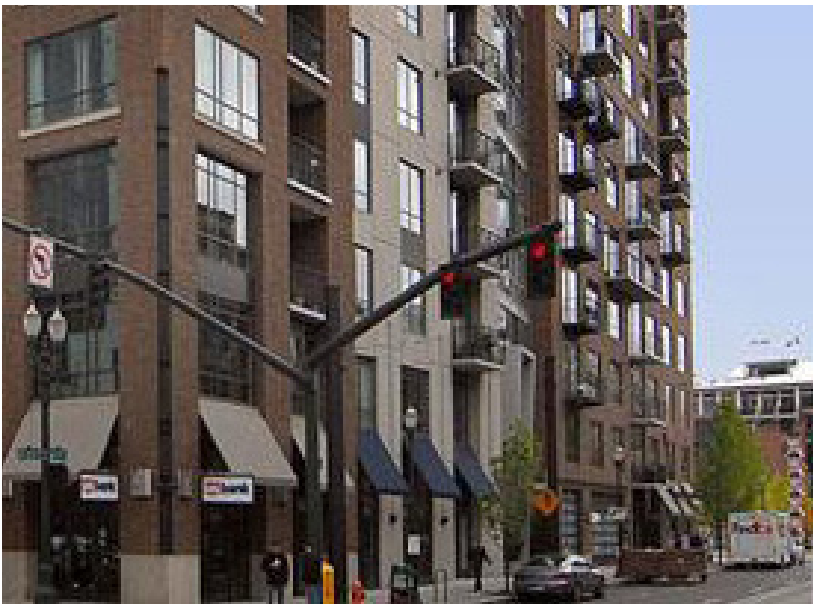
GUIDELINE 4: DEVELOP BUILDINGS THAT ARE APPROPRIATELY SCALED TO THE NEIGHBORHOOD. FAÇADES SHOULD BE WELL ARTICULATED AND OFFER DIVERSITY IN VOLUME AND FORM ALONG THE STREET EDGE.

This Guideline may be accomplished by:

This is one large development that has been successfully articulated as different building volumes. The buildings are articulated using different heights, as well as a change in materials.

This building is broken up into multiple volumes by stepping the building back from the property line and lowering the scale at the street edge.

This building is broken up into distinct parts with a change in materials, color, and depth in the façade.



This building uses all cold and unnatural materials. The depth of the façade is covered up with metal panels so that the building reads as a flat façade.

This building façade is very flat, and offers little texture or visual diversity.

This development is meant to look like multiple buildings that were designed over time, although they were designed and built at the same time.

GUIDELINE 5: PROVIDE TRANSITIONS BETWEEN THE PUBLIC AND PRIVATE REALMS WHEN RESIDENTIAL STRUCTURES ABUT STREETS, PARKS AND PEDESTRIAN ACCESSWAYS⁵

BACKGROUND

The relationships of residential only uses to the sidewalk grade and to the property line are important in establishing zones that are public and zones that are private. The public and private realms should be clearly defined via the following:

Where the first floor of a structure is a residential use, that floor should be raised above the sidewalk grade and set back from the property line to allow for the delineation of the public and private realms. The height of the first floor above the sidewalk should be approximately 3 feet, or enough to adequately delineate between public and private realms. Care should be taken on sloped sites to ensure the relationship between the first floor and sidewalk at the front and rear of the site does not create blank wall conditions. The setback from the property line can vary from 4 - 15 feet to accommodate stairs, stoops and other entrance features where living units can be accessed from the streets, parks and pedestrian accessways. Where it isn't practical to raise the first residential floor level above the sidewalk grade, the setback should be used for providing a landscaped buffer, low fence, wall, hedge or similar feature to create the delineation between public and private realms.

Implementation of this guideline should not conflict with the requirements of 2010 Oregon Structural Specialty Code, the Federal Fair Housing Act, Oregon State Statutes (ORS Chapter 659a) and ICC/ANSI A117.1-2003. If conflicts with any of these requirements arise, conformance with this guideline shall be waived.

GUIDELINE 5: PROVIDE TRANSITIONS BETWEEN THE PUBLIC AND PRIVATE REALMS WHEN RESIDENTIAL STRUCTURES ABUT STREETS, PARKS AND PEDESTRIAN ACCESSWAYS

⁵New guideline specific to the NW Master Plan

GUIDELINE 5: PROVIDE TRANSITIONS BETWEEN THE PUBLIC AND PRIVATE REALMS WHEN RESIDENTIAL STRUCTURES ABUT STREETS, PARKS AND PEDESTRIAN ACCESSWAYS

This guideline may be accomplished by:

- 5.1. Changing the relationship of residential structures to finished grade .



The entrance to this townhouse project is several feet above the sidewalk grade;a clear demarcation of private and public spaces is achieved. The entrance is also setback from the property line so that a stair and stoop can lead to the entrance, attractive landscaping at the sidewalk edge can be incorporated, and access to a lower level unit can be provided.

This guideline may be accomplished by:

- 5.2 Setbacks for residential structures fronting streets and parks.



The example described for 5.2 shows how a modest setback can define the public and private spaces. In this example, the front elevation is set back from the property line creating space for a small terrace and landscaping. The entryway is further setback creating a generous landing area and weather protection. The low wall and landscaping along the sidewalk edge further defines the public and private spaces.

GUIDELINE 6: INTEGRATE HIGH-QUALITY MATERIALS AND DESIGN DETAILS⁶

BACKGROUND

High-quality exterior materials help establish a sense of permanence and contribute to an enjoyable pedestrian experience within the public realm. These materials should be carefully and artfully detailed in the composition of building façades. There are preferred materials and those that are discouraged. Discouraged materials are not prohibited but allowed only in limited locations as secondary materials away from façades that face street frontages and open spaces.

Special attention to well-crafted detailing may allow discouraged materials to meet this guideline in limited locations.

Preferred materials for exterior cladding include natural materials, such as wood and stone. Other preferred materials for exterior cladding include brick, metal panels, pre-cast concrete, stucco and tiles. Wood-clad, steel and aluminum assemblies are preferred for storefront systems and building entrances. Stone, masonry and cementitious tiles are preferred for exterior paving systems. For window systems, high-quality wood, wood-clad, fiberglass and metal systems are preferred. If vinyl windows are proposed they must be commercial-grade and detailed in a careful manner that visually differentiates the window from surrounding trim elements or adjoining siding materials.

Discouraged materials for exterior cladding include plain concrete block, unfinished concrete, corrugated metal, plywood and sheet pressboard (sheet pressboard is fiber cement siding that is more than six-inches-wide). Residential-grade vinyl windows are discouraged for all window applications.

Foundation materials, where exposed, may be unfinished concrete or honed concrete block—when the foundation material is not revealed for more than 3 feet.

New exterior cladding materials are constantly being developed. Where new materials are proposed, they should be consistent with the characteristics described in this section for preferred materials.

The following pages demonstrate how these materials, both preferred and discouraged, might be used and detailed in a capacity that meets this guideline and, where they don't meet this guideline.

⁶New guideline specific to the NW Master Plan, partially adapted from Community Design Standards.

GUIDELINE 6: INTEGRATE HIGH-QUALITY MATERIALS AND DESIGN DETAILS

GUIDELINE 6: INTEGRATE HIGH-QUALITY MATERIALS AND DESIGN DETAILS

This guideline may be accomplished by:

6.1. Using the following materials in an appropriate manner:

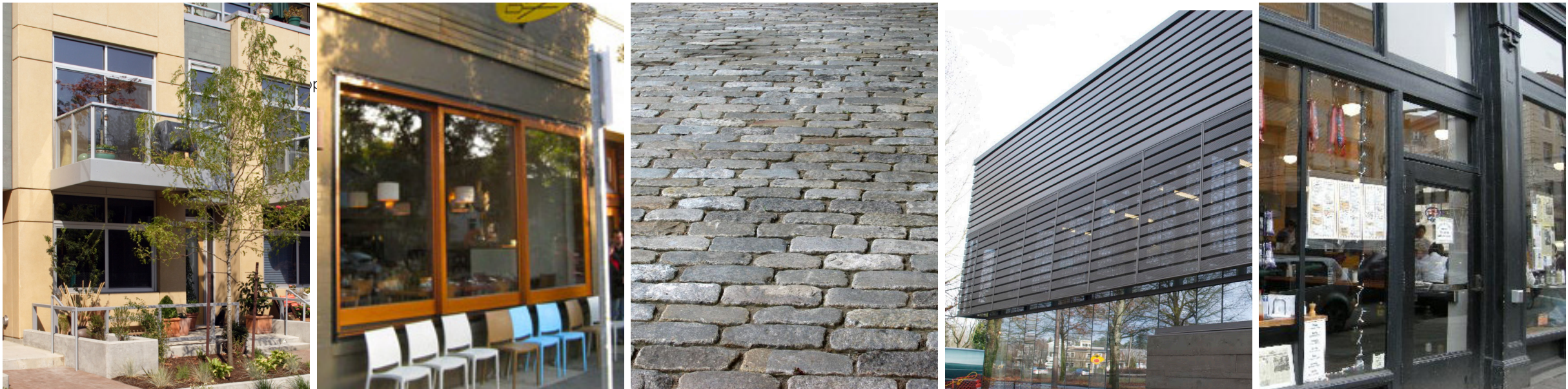


<p>BRICK</p> <p>Brick should be well detailed. Window and door openings should be appropriately scaled. Bricks should not span across a large opening without an appropriately scaled spandrel, lintel or arch. Use of “jumbo” brick should be avoided except where scale can be shown to be appropriate to the overall façade.</p>	<p>WOOD SIDING</p> <p>Wood should be real, and should be stained as opposed to painted. Windows in wood siding should offer depth and be neatly trimmed. Rough textured wood and trim boards should be avoided. Outside corners should be mitered or trimmed with metal corners. Avoid vertical timboards at outside corners.</p>	<p>METAL PANEL</p> <p>Metal panel should be detailed to offer layers and depth to the façade. Flat façades with flat windows are not desired.</p>	<p>FIBER CEMENT SIDING</p> <p>Fiber cement siding should be detailed to offer layers and depth to the façade. Flat façades with flat windows are not desired.</p>	<p>STUCCO</p> <p>Stucco should be used as a simple, textural building material that can be painted to meet design intent.</p>
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GUIDELINE 6: INTEGRATE HIGH-QUALITY MATERIALS AND DESIGN DETAILS

This guideline may be accomplished by:

6.1. Using the following materials in an appropriate manner:



STONE / STAINED CONCRETE

Stone should be well detailed and used to represent mass in a building. Matching stacked stone with vinyl windows is not desired.

WINDOWS

Wood windows are preferred over metal windows at the ground floor levels of buildings.

PAVING MATERIALS

Paving materials should have variety and texture, while providing for universal accessibility. Large areas of asphalt paving are not desired.

CORRUGATED METAL

Corrugated metal should be used as a building accent material, but should not cover large expanses of façade.

STOREFRONTS

Storefronts should be substantial and well detailed.

OPEN SPACE DESIGN GUIDELINES

GUIDELINE 7A: PROVIDE PRIVATE OPEN SPACES THAT ARE WELL INTEGRATED WITH ADJACENT DEVELOPMENT, ACT AS GATHERING PLACES DESIGNED TO ADAPT TO A VARIETY OF ACTIVITIES, ARE LINKED TOGETHER AND TO OTHER NEARBY OPEN SPACES, ARE ACCESSIBLE TO THE PUBLIC AND PROVIDE DISTINCTIVE NEIGHBORHOOD IDENTITY.

BACKGROUND

Given the urban context of this neighborhood, several different types of public parks and private open spaces open to the public are proposed within the Con-Way Master Plan development area. These spaces consist of a public neighborhood park, a square, a pocket park, building forecourts and three multi-block pedestrian accessways. As a framework, these spaces will be well-integrated with development and provide distinctive neighborhood identity. They will become gathering places that are programmed to adapt to a variety of activities in this park-deficient part of the city. They will link to other open spaces, parks and green streets within the Northwest and Pearl District neighborhoods, and add new public pedestrian connectivity to the district.

See Map 05-07 showing the Con-way Master Plan area and the relations to surrounding parks and recreation facilities, and Map 05-08 showing the open space elements.

NEIGHBORHOOD PARK AND SQUARE

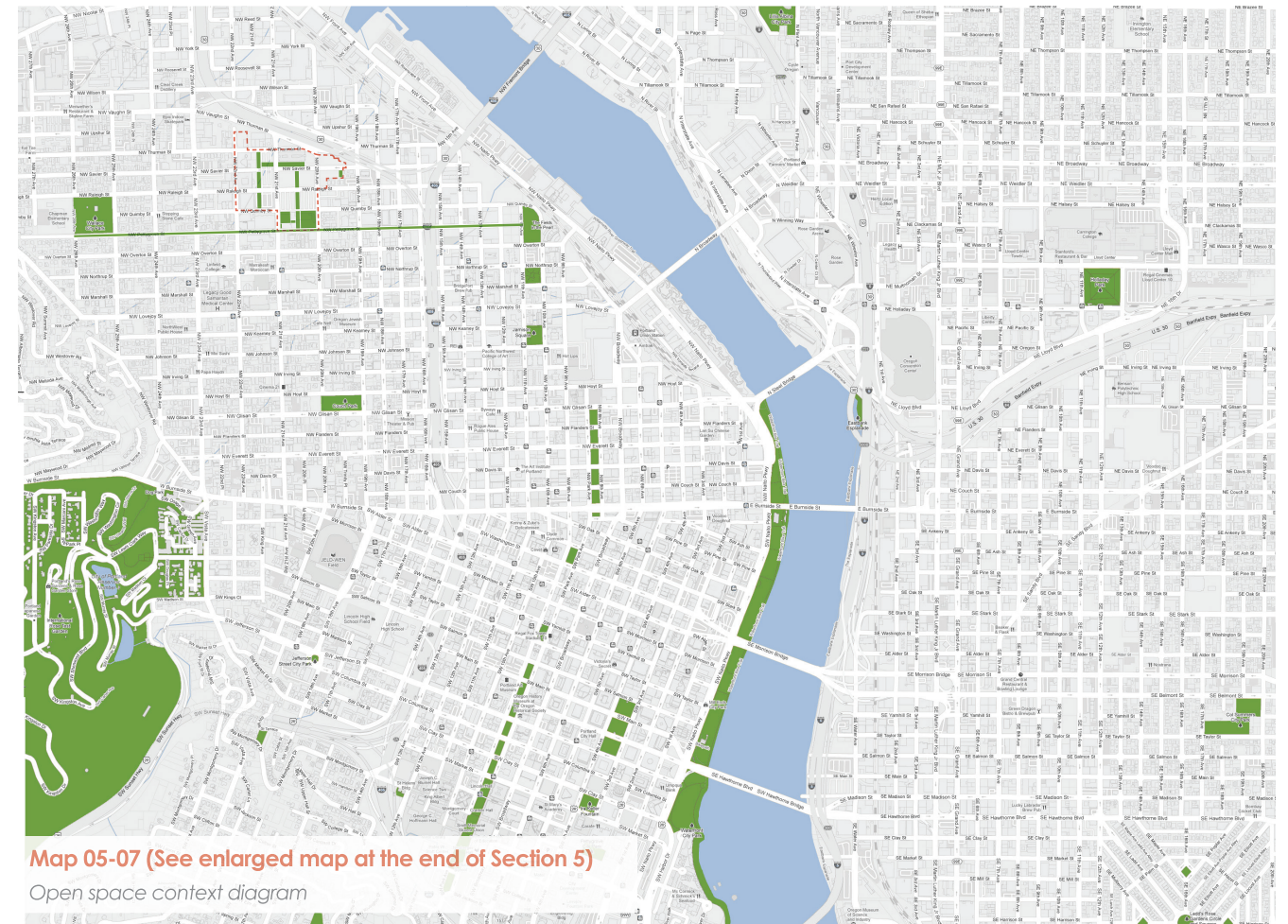
The neighborhood park and square are located on Block 290. The eastern portion of the site will be developed as a public neighborhood park. The park will be primarily green space situated in an urban neighborhood.

The western portion of the site will be developed privately as a square open to the public. The square will be an urban hardscape space that provides for everyday use, a variety of neighborhood activities, a farmer's market, outdoor seating and dining associated with adjacent developments and programmed events in all seasons. This space will be formed by mixed-use buildings as an urban framework to activate the square, particularly at the ground level.

These blocks also abut the adjacent 60' wide parcel that extends NW Quimby between NW 20th and 21st Ave. as an east/west accessway or potential "Festival



A heavily landscaped open space that invites engagement and activity



Street.” This parcel is designated as a local street. Final design is subject to BDS Design Review and approval by the Portland Bureau of Transportation. This accessway will offer pedestrian and bicycle connectivity and possible service or emergency access. If it is programmed as a “Festival Street” the majority would be likely paved with flexible hardscape that supports or extends the park and square activities. Ideas for this space will be included in the programming and design efforts for the public park.

In lieu of BDS Design Review, a public process, including extensive outreach led by or done in collaboration with Portland Parks and Recreation, will determine the programming of uses and activities, concepts and design of the neighborhood park. Depending on available funding, the neighborhood park will either be constructed by Portland Parks and Recreation or by a private developer during Phase 1. Con-way will undertake a developer solicitation/ design process, closely coordinated with the parks design process, for the square and related development. Map 05-09, Open Space and Massing Diagram, illustrates an overall concept for the square and neighborhood park.

PRIVATE PEDESTRIAN ACCESSWAYS OPEN TO THE PUBLIC

Together with public sidewalks, the pedestrian accessways form a linked circuit of walkable open spaces within the Con-way Master Plan area. They will provide enhanced, connecting pedestrian experiences that are currently lacking in the neighborhood.

New pedestrian accessways break down the superblocks in a north/south direction throughout the Con-Way properties. Although the accessways will be privately developed and owned as part of the site development, they will be offered for public use through permanent access easements.

The north/south pedestrian accessways will provide the ability to enter the inner portions of the privately developed blocks, whether they are commercial uses, townhomes, and mixed-use, work/live spaces at the ground floor. The accessways will have continuously paved walkways and seating areas, ornamental lighting, bicycle parking and landscape or garden elements. Stormwater run-off from adjacent buildings and paved hardscape may be treated in the accessways, adding a seasonal displays and effects. In addition, the accessways may have works of art or small water features. Special signage will create identity and convey the appropriate uses of the accessways. Shade trees and ornamental plantings will be an important part of the pedestrian accessways, adding a distinguished identity and character. There may be variation in design from block to block, depending on the adjacent ground floor uses. The accessways will be privately maintained by adjacent properties.

BUILDING FORECOURTS

The existing parking lot and open space on the south side of the Con-way office building on block 293 presents an interesting opportunity to become a more active and pedestrian-oriented building forecourt. Generally open to the public use, the space can function as drop-off and pedestrian entries to the building, along with some handicapped parking and short term parking for deliveries. The space will have special paving, ornamental lighting, seating areas with furnishings and landscape improvements. It may also have a water feature, stormwater treatment areas and/or public art.

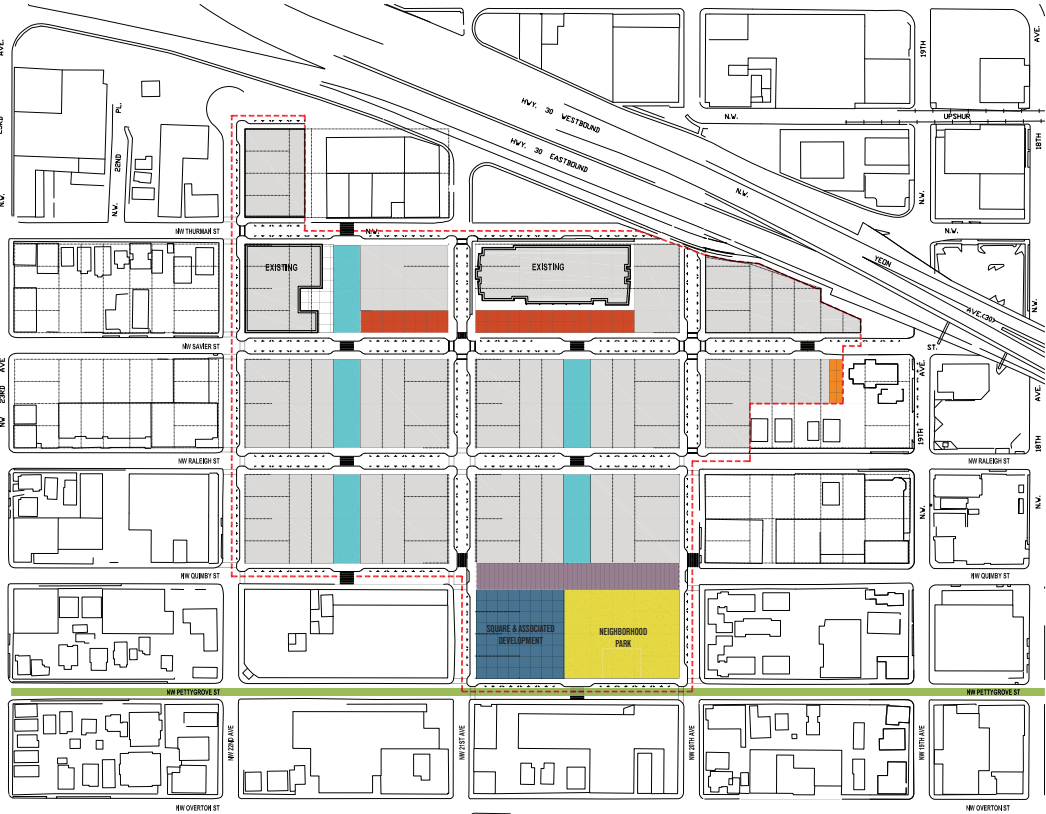
This building forecourt element is planned to extend along the southern edge of block 294, providing a linkage between Con-way’s two existing office buildings and the north/south pedestrian accessways. Functions, materials and details will be similar to the planned forecourt on block 293.

POCKET PARK

A small pocket park is proposed west of St. Patrick’s Church. This space will give the historic structure some “breathing room” and provide opportunities for informally programmed activities organized primarily by adjacent development, the church or neighborhood. The pocket park will be part of a negotiation regarding ownership and maintenance with the adjacent development and church.

See Map 05-08 which shows the conceptual open space plan for the Master Plan area.

GUIDELINE 7: PROVIDE PRIVATE OPEN SPACES THAT ARE WELL INTEGRATED WITH ADJACENT DEVELOPMENT, ACT AS GATHERING PLACES DESIGNED TO ADAPT TO A VARIETY OF ACTIVITIES, ARE LINKED TOGETHER AND TO OTHER NEARBY OPEN SPACES, ARE ACCESSIBLE TO THE PUBLIC AND PROVIDE DISTINCTIVE NEIGHBORHOOD IDENTITY.



Map 05-08 (See enlarged map at the end of Section 5)

Open areas map



GUIDELINE 7B: SQUARE—DESIGN THE SQUARE TO BE A SIGNIFICANT ICONIC URBAN PLACE AND INCLUDE COMMERCIAL FOCAL POINTS AS ADJACENT USES.

BACKGROUND

The design of the square and related development will be undertaken privately in collaboration with the public design process for the neighborhood park managed by Portland Parks & Recreation, and with participation by neighborhood representatives and the public in general.

The square will be designed to promote the following social outcomes: (i) to bring together old and new residents on a frequent basis so that social networks develop; (ii) to promote a safe and lively environment at most times of the day and night; (iii) to create a welcoming and hospitable environment for all socio-economic, ethnic and age groups that is designed to enhance their co-presence; and (iv) to foster a sense of community membership, increase civic engagement and community pride.

All applicable design standards and design guidelines described earlier in this section will apply to buildings associated with the square. More detailed design guidelines for design of the square itself include:

- 7.B.1 Provide architectural context around the perimeter of the square. Activate the square with active ground floor uses that offer opportunities such as outdoor dining from private establishments that adjoin the square.
- 7.B.2 Provide ground level sight lines and pedestrian access from the square into the neighborhood park.
- 7.B.3 Mass adjacent buildings to enclose the square and to optimize solar exposure.
- 7.B.4 If possible, provide additional commercial space and/or multi-family housing at the upper levels of the surrounding development, to help put "eyes" on the square.
- 7.B.5 If/when commercial uses such as cafes are located on the second floors, provide balconies for outdoor dining to activate the square from the upper levels.
- 7.B.6 Design the square to be flexible and to support commerce, activities, and events such as farmers/public markets, dining, fairs, art shows, and small musical performances, etc.

- 7.B.7 Consider opportunities for neighborhood facilities such as schools, libraries, meeting places, full service bike station and community centers to abut the square and provide for 18 hour activity.
- 7.B.8 Design the square as a simple and flexible urban space; include high quality furnishings and materials particularly at the ground plane. The square should be appropriately sized for the activities and functions envisioned in the space.
- 7.B.9 Incorporate elements that evoke the history of the neighborhood such as a water feature or public art to give identity to the square.
- 7.B.10 Provide landscape elements consisting primarily of shade trees, possibly including low shrubs and ground covers that allow for surveillance and security.
- 7.B.11 Provide furnishings such as lighting, trash containers, fixed benches, movable tables and chairs, bollards and planters.
- 7.B.12 Design the eastern edge of the site so that it is well integrated with the neighborhood park.
- 7.B.13 Provide for universal accessibility.
- 7.B.14 Provide public access easements for the square.
- 7.B.15 In the event that construction of the square significantly lags construction of the neighborhood park, interim improvements shall be allowed. Interim improvements include activities and treatments, such as demolition, grading, seeding, installing temporary paving, allowing public access and the like. Phase 1 improvements are to be mutually agreed upon by Con-way, property owner, and Portland Parks and Recreation. Phase 1 improvements, as described above, shall not be subject to Design Review and shall be allowed outright on the square.

GUIDELINE 7B: SQUARE—DESIGN THE SQUARE TO BE A SIGNIFICANT ICONIC URBAN PLACE AND INCLUDE COMMERCIAL FOCAL POINTS AS ADJACENT USES.



A market in an active public square



Concept images for a public square



Activity at multiple levels



Activity at the ground floor.

GUIDELINE 7.C: NW QUIMBY PARCEL

PROVIDE A MULTI-USE STREET AND OPEN SPACE THAT LINKS THE NEIGHBORHOOD PARK AND SQUARE TO THE SOUTH AND DEVELOPMENT TO THE NORTH, AND SERVES PRIMARILY AS A PEDESTRIAN AND BICYCLE CONNECTION.

BACKGROUND

A parcel that would extend NW Quimby Street between NW 20th and 21st Avenues is currently private property and owned by Con-way. The parcel is designated as a local street in the Transportation Element of the City of Portland Comprehensive Plan.

There will be a comprehensive Master Planning process to design the neighborhood park and develop design ideas for the Quimby parcel. The design of this parcel will be important in that it is envisioned to have special qualities that allow it to successfully link the neighborhood park and square to the south with private development to the north. Design improvements proposed for the Quimby parcel are subject to BDS Design Review and approval by the Portland Bureau of Transportation.

Based on the traffic analysis conducted for the Master Plan and on the desire to carefully integrate the Quimby parcel with design of a neighborhood park and adjacent development, through pedestrian and bicycle access is the priority transportation function for the parcel. The following design guidelines will be used during the design process, to guide specific design proposals for the Quimby parcel:

- 7.C.1 Provide through pedestrian and bicycle connections between NW 21st and 20th.
- 7.C.2 Provide emergency and service access as needed to adjacent developments.
- 7.C.3 As needed, provide access to building entrances and pedestrian accessways to the north of the parcel.

- 7.C.4 Provide transitions to hard and landscape elements included in the neighborhood park to the south of the parcel.
- 7.C.5 Provide public access easements.
- 7.C.6 Accommodate underground public utilities as needed.
- 7.C.7 Provide a location for a flexible festival street to host a farmers market, art walk or other programmed neighborhood events.
- 7.C.8 Design the festival street to reflect the character of the potential square on the west end as well as the neighborhood park on the east end.

GUIDELINE 7.C: NW QUIMBY PARCEL
PROVIDE A MULTI-USE STREET AND OPEN SPACE THAT LINKS THE NEIGHBORHOOD PARK AND SQUARE TO THE SOUTH AND DEVELOPMENT TO THE NORTH, AND SERVES PRIMARILY AS A PEDESTRIAN AND BICYCLE CONNECTION.

GUIDELINE 7.D: PEDESTRIAN ACCESSWAYS—
PROVIDE A NETWORK OF PEDESTRIAN
ACCESSWAYS THAT, TOGETHER WITH PUBLIC
GREENSTREETS AND BUILDING FORECOURTS,
FORM A SPECIAL PEDESTRIAN CIRCUIT OR
NETWORK OF CONNECTED OPEN SPACES
IN THE NEIGHBORHOOD, IN ADDITION TO
PROVIDING PEDESTRIAN ACCESS TO ADJACENT
DEVELOPMENT.

BACKGROUND

Provide for pedestrian accessways that are privately owned but open to the public. Pedestrian accessways are located mid-block between NW 20/21st and NW 21/22nd. See Map 05-08.

- 7.D.1 Design pedestrian accessways as mid-block linear greens and pedestrian walkways that provide continuous informal green open spaces helping to reduce the scale of large blocks
- 7.D.2 Design pedestrian accessways so that the distance between building elevations is generally 60 feet. Pedestrian accessways should be accessible to the public through public access easements that are generous (for example, 40 feet wide) and extend continuously between public streets to the north and south.
- 7.D.3 Single north-south pedestrian pathways within an accessway should be large enough to accommodate shared bicycle and pedestrian traffic (for example, 16 feet wide).
- 7.D.4 Design the character of pedestrian accessways east of NW 21st to extend elements of the neighborhood park north to the forecourt of the Con-way office building. Residential uses are expected to be the predominant adjacent uses.

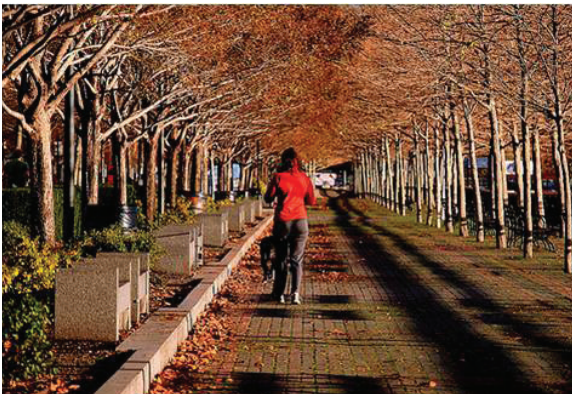
- 7.D.5 Design these accessways to include paved walkways, fixed seating, lighting, benches and bollards.
- 7.D.6 Include landscape elements such as shade trees, low shrubs and ground covers. Consider opportunities for gardens
- 7.D.7 Consider bioswales or places for stormwater treatment from adjacent private development.
- 7.D.8 Express the character of pedestrian accessways west of NW 21st to reflect the variety of commercial, office and residential uses expected as adjacent uses along the accessways, and design to accommodate a variety of activities.
- 7.D.9 Design the entries to the accessways with signage and to be welcoming to the public.
- 7.D.10 Connect the pedestrian accessways with designated crosswalks at mid-block on the local east/west streets.



A successful pedestrian accessway in the South Waterfront



Quiet recreation



A pedestrian accessway.

GUIDELINE 7.D: PEDESTRIAN ACCESSWAYS—PROVIDE A
NETWORK OF PEDESTRIAN ACCESSWAYS THAT, TOGETHER
WITH PUBLIC GREENSTREETS AND BUILDING FORECOURTS,
FORM A SPECIAL PEDESTRIAN CIRCUIT OR NETWORK OF
CONNECTED OPEN SPACES IN THE NEIGHBORHOOD, IN
ADDITION TO PROVIDING PEDESTRIAN ACCESS TO ADJACENT
DEVELOPMENT.



Places to sit.



GUIDELINE 7.E: BUILDING FORECOURTS—
PROVIDE BUILDING FORECOURTS ON
SPECIFIC BLOCKS THAT SERVE AS MULTI-
USE OUTDOOR SPACES OPEN TO THE
PUBLIC.

BACKGROUND

Provide for building forecourts shown on Map 05-08 that are privately owned but open to the public.

- 7.E.1 Create a building forecourt by re-designing the existing parking lot at the northeast corner of NW 21st and Savier on block 293 as a multi-use private outdoor space.
- 7.E.2 Design a building forecourt at the northwest corner of NW 21st and Savier on block 294 as a continuation of the forecourt planned for block 293.
- 7.E.3 Design the character of the pedestrian-oriented spaces with limited drop-off and parking for delivery vehicles, as a terminus for the north/south pedestrian accessways, as a link between the two existing Con-way office buildings, and to feature paved spaces for building entries and outdoor seating.
- 7.E.4 Include special lighting, furnishings and landscape elements.

GUIDELINE 7.E: BUILDING FORECOURTS—PROVIDE BUILDING FORECOURTS ON SPECIFIC BLOCKS THAT SERVE AS MULTI-USE OUTDOOR SPACES OPEN TO THE PUBLIC.



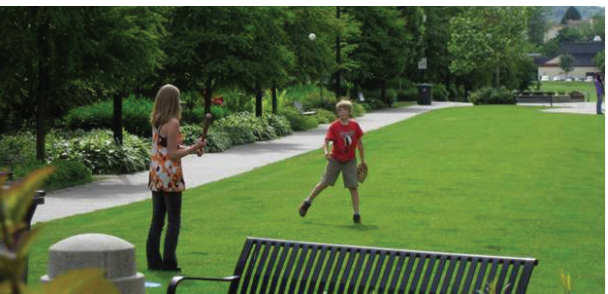
Treating rainwater at the building entrance through gardens.



Art that encourages interaction and engagement.



Capturing and treating rainwater.



Casual play

GUIDELINE 7.F: POCKET PARK—PROVIDE LAND FOR A SMALL POCKET PARK WEST OF ST. PATRICK'S CHURCH

- 7.F.1 Design the space to give the historic church some "breathing room" and to provide opportunities for informally programmed activities organized primarily by adjacent development, the church or neighborhood.
- 7.F.2 Design of this pocket park shall be in collaboration with St. Patrick's Church.

GUIDELINE 7.F: POCKET PARK—PROVIDE LAND FOR A SMALL POCKET PARK WEST OF ST. PATRICK'S CHURCH



A small park that uses water, trees, and provides seating.

DESIGN PRINCIPLES FOR PARKS & OPEN SPACES

PROVIDE A NEIGHBORHOOD PARK DESIGNED AS A GREEN SPACE IN AN URBAN NEIGHBORHOOD; INTEGRATE THE PARK WITH THE SQUARE, ADJACENT STREETS AND ABUTTING DEVELOPMENT.

BACKGROUND

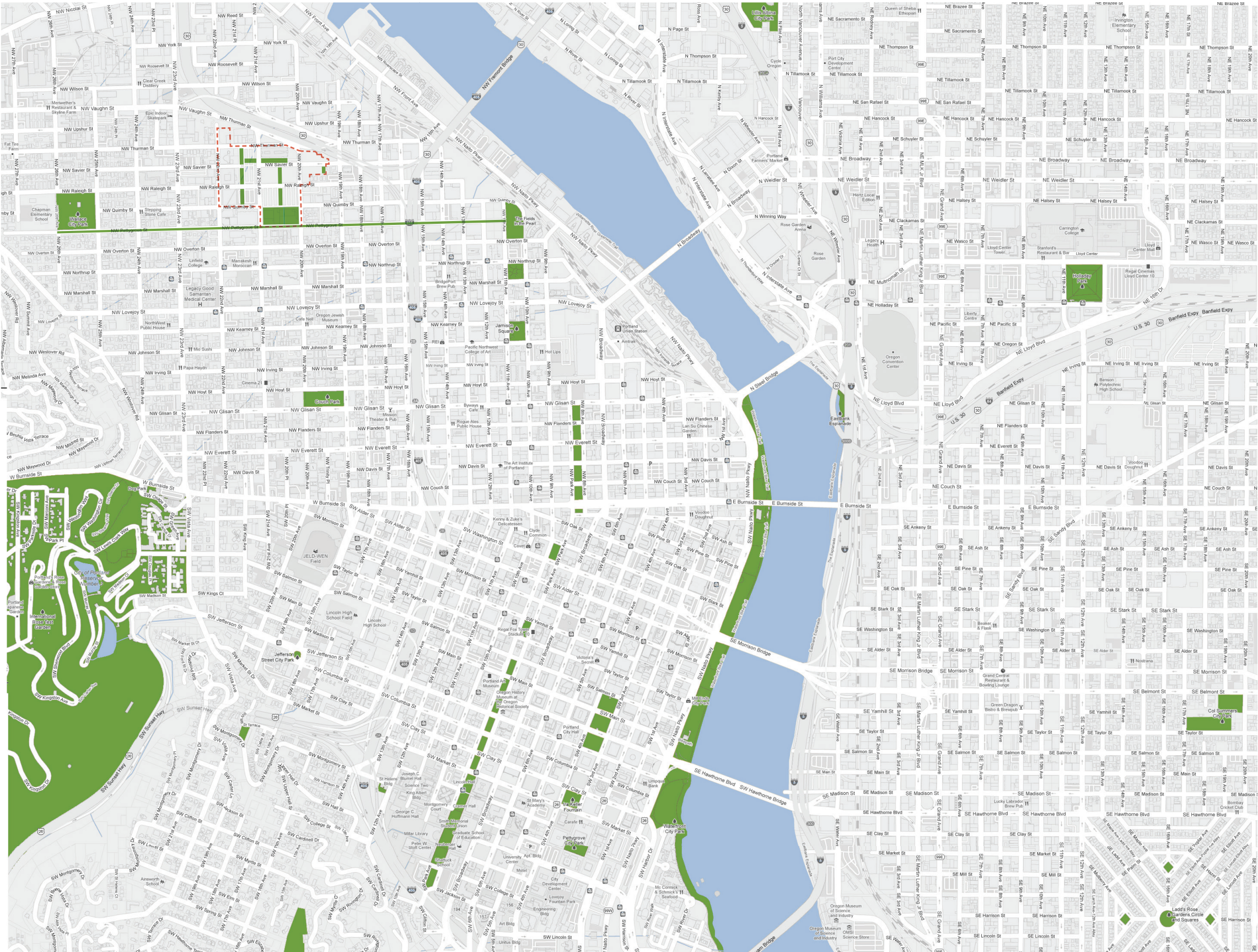
The primary public open space within the Con-way Master Plan area is a 2.75 acre site located between NW 21st and 20th Avenues north of NW Pettygrove St. This open space is composed of three elements:

1. The eastern portion of the site will be developed as a public neighborhood park. The park will be primarily green space situated in an urban neighborhood. The design principles described below apply specifically to the neighborhood park.
2. The western portion of the site will be developed by Con-way and a development partner as a square open to the public, with private development enclosing the square.
3. The 60' wide parcel that extends NW Quimby between NW 20th and 21st Ave. as an east/west accessway or potential "Festival Street." This parcel is designated as a local street. Final design is subject to design review and approval by the Portland Bureau of Transportation. This accessway will offer pedestrian and bicycle connectivity and possible service or emergency access.

A public process including extensive outreach led by or done in collaboration with Portland Parks and Recreation will determine the ultimate programming of uses and activities, concepts and design of the neighborhood park. The following Design Principles will help to inform that master planning process, but should not be considered absolute design criteria or design requirements.

1. Design the park as primarily green space situated in an urban neighborhood that is attractive to a broad range of ages and abilities.
2. Consider flexible elements such as seating areas, lawn areas and informal areas for passive recreation or games.

3. Consider a variety of places to rest, linger and oversee activities in the park.
4. Consider tree canopy over a portion of the park, or at perimeter locations.
5. Provide lighting throughout the park for safety and security.
6. Integrate design of the neighborhood park with the Square, NW Pettygrove Greenstreet and the NW Quimby parcel.
7. Consider all aspects of sustainability in the design and operation of the park.
8. Collaborate with the design of private development so that the neighborhood park appropriately connects with adjacent building entrances, ground level active uses, and terraces and balconies around the perimeter of the park.
9. Work with private development of adjacent commercial and/or residential buildings to optimize solar exposure in the park.

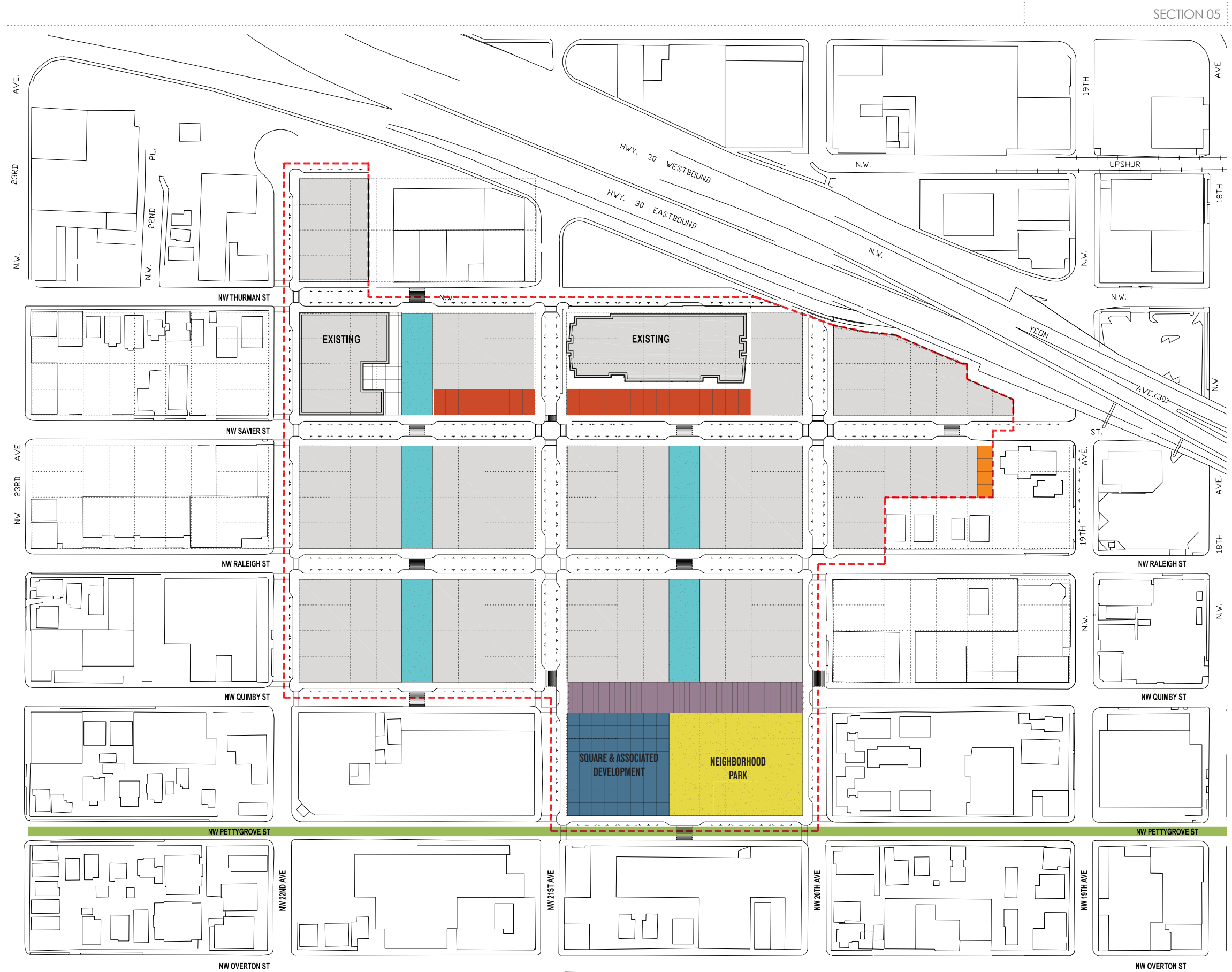


This diagram shows the Con-way property, and how it sits in relation to other parks and open spaces throughout the city.

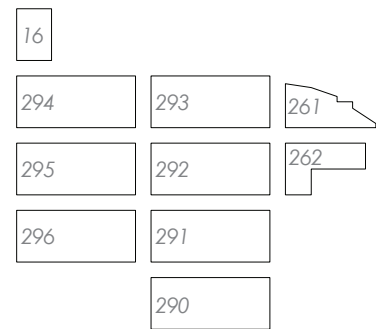
NW Pettygrove is currently envisioned as a "green street" by the city of Portland, helping link Wallace Park in Northwest Portland to The Fields Park in the Pearl District.

The Con-way site abuts this green street on one block face. The proposed open spaces are shown in green.

MAP 05-7
OPEN SPACE
CONTEXT DIAGRAM



- POCKET PARK
- NEIGHBORHOOD PARK
- CITY DESIGNATED GREEN STREET
- PEDESTRIAN ACCESSWAY
- BUILDING FORECOURT
- SQUARE & ASSOCIATED DEVELOPMENT
- QUIMBY ST PARCEL



MAP 05-8
OPEN SPACE PLAN





MAP 05-9
CONCEPTUAL OPEN SPACE &
MASSING DIAGRAM

SECTION 6

TRANSPORTATION

84	TRANSPORTATION: OVERVIEW
90	MAP 06-1 POSSIBLE FUTURE UNDERGROUND GARAGE LOCATIONS AND ACCESS
91	MAP 06-2 PARKING COUNTS
93	MAP 06-4 POTENTIAL STREETCAR ALIGNMENT
94	MAP 06-5 STREET PLAN
95	STREET SECTIONS
98	MAP 06-9 PROPOSED IMPROVEMENTS AT 23RD AND VAUGHN: OPT 1
99	MAP 06-10 PROPOSED IMPROVEMENTS AT 23RD AND VAUGHN: OPT 2

33.562.300.D

6. *Transportation. For each phase of Northwest Master Plan development the following must be addressed:*
- A.** *The location and amount of motor vehicle and bicycle parking;*
 - B.** *Strategies to reduce the number of motor vehicle miles traveled by those regularly traveling to and from the Northwest Master Plan area, including:*
 - 1. *Measures to encourage those traveling to and from the Northwest Master Plan area to use alternatives to single-occupant auto trips (walking, bicycling and public transit);*
 - 2. *Car or van pool programs;*
 - 3. *Incentives to be offered to employees to use public transit for travel to and from the Northwest Master Plan area;*
 - 4. *Incentives to be offered to employees to travel on foot or by bicycle to and from the Northwest Master Plan area. This may include incentives for employees to live within walking distance of the area;*
 - C.** *Planned improvements to the routes used by transit patrons between transit stops and buildings in the Northwest Master Plan area;*
 - D.** *A street plan for the Northwest Master Plan area that provides multimodal street connections to match the surrounding street grid pattern where feasible;*
 - E.** *A multi-modal transportation impact study. The study must follow the guidelines of the Portland Bureau of Transportation; and*
 - F.** *Traffic impacts on the streets surrounding the Northwest Master Plan area, and mitigating measures to ensure that the surrounding streets will function consistent with their designations as found in the Comprehensive Plan Transportation Element.*

SECTION 6: TRANSPORTATION

In order for expansion of existing structures or construction of new buildings, Chapter 33.562.300 of the Northwest Plan District requires that a Northwest Master Plan be approved by the City of Portland. This requirement applies to all properties owned by Con-way shown on Map 01-1. This section of the Northwest Master Plan addresses the requirements of Chapter 33.562.300.C.6-Transportation that include:

- Location and amount of motor vehicle and bicycle parking
- Strategies to reduce the number of motor vehicle miles traveled
- Planned improvements to routes used by transit patrons
- A street plan providing multi-modal street connections
- A multi-modal transportation impact study
- Identification of traffic impacts, mitigation measures and other recommendations

The proposed Northwest Master Plan is planned to include a mixed-use development of retail, office, residential housing, open space, and community uses. Construction of new development is expected to begin in the spring of 2013 with the potential for full build-out of the Master Plan to occur over the next 10 to 15+ years in multiple phases.

A Multi-Modal Transportation Impact Study, Kittelson & Associates, Inc., April 2012 (T.I.S.) was prepared to address all of the requirements of Chapter 33.562.300.C.6. In addition, a Memorandum dated August 8, 2012 was prepared by Kittelson & Associates as an addendum to the Multi-Modal Transportation Study. These documents are attached to and included as part of the Northwest Master Plan (See Appendix E and E.1). In this section, the primary findings, conclusions and recommendations found in the Transportation Impact Study and Memorandum are summarized.

For purposes of the multi-modal transportation impact study, future conditions assuming full build-out at maximum allowed density have been assumed for year 2022. The results of this T.I.S. indicate that the proposed development within the Northwest Master Plan area achieves the following:

1. Complies with the policies, street classifications, and street designations of the Transportation Element of the Portland Comprehensive Plan;
2. The transportation system is capable (with planned improvements provided for herein) of safely supporting the proposed uses in the Master Plan, in addition to

the existing and planned uses in the area; and,

3. The proposed street plan provides a series of multi-modal street connections that match the surrounding street grid pattern.

The findings and recommendations of the T.I.S. are summarized in the following pages.

A. LOCATION AND AMOUNT OF MOTOR VEHICLE AND BICYCLE PARKING

It is anticipated that below grade structured parking will be provided with each development project, and that a central below grade replacement parking structure will be constructed for Con-way's employees and visitors. Several potential sites are identified on Map 06-1 for the Con-way replacement parking.

Parking will be provided at diminishing levels as the development matures and opportunities for shared parking and TDM (Transportation Demand Management) grow. Maximum parking ratios in early phases of development will reflect existing levels of transit service and relatively limited opportunities for shared parking and complementary uses. As more transit service is provided, and as the district matures with more complementary development, maximum parking ratios will be reduced.

The City of Portland is encouraged to institute paid on-street parking at the earliest possible time. On-street parking should be intended primarily for retail customers. The Con-way Master Plan supports the recommendations included in the Mayor's Proposed NW Parking Plan, including recommendations for paid on-street parking and formation of a NW District Transportation and Parking Management Association (NWTPMA).

Parking standards for the Northwest Master Plan area are included in Section 5-Development and Design Standards and Criteria, Parks and Open Space Design Guidelines and are repeated below.

Proposed Parking Standards

The following parking standards are proposed for the NW Master Plan area. Three levels of maximum parking standards are identified, and threshold activities are summarized that will trigger enforcement of the next level of standards. The more restrictive parking standards (levels 2 and 3) reflect standards that currently apply to other areas of the Central City that have mixed uses, high densities, and excellent transit service. For any uses not referenced below, the Standards of Chapter 33.266 apply.

1. Base Parking Level 1

These standards improve upon the existing regulations. These will be the parking standards for the Master Plan area until thresholds for Level 2 are met:

Phase 1 Parking Standards

Minimum requirements for all uses: None

Maximum parking standards:

All Office at 2.5 stalls per 1,000 SF

All Retail at 2.5 stalls per 1,000 SF

All Housing at 1 stall per unit

2. Parking Level 2 - Based on Lloyd District Standards

These will automatically become the parking standards for the Master Plan area when the following occurs:

Thresholds for Level 2 – each of the following must be in place:

- (a) A binding commitment to construct streetcar,
- (b) Operation of streetcar will commence in one year
- (c) The City has installed meters for paid on-street parking

Phase 2 Parking Standards:

Minimum requirements for all uses: None

Maximum parking standards:

All Office at 2.0 stalls per 1,000 SF

All Retail at 2.0 stalls per 1,000 SF

All Housing at 1 stall per unit

3. Parking Level 3 - Based on River District 2 (CCTMP)Ultimate parking standards.

These will automatically become the parking standards for the Master Plan area when the following occur:

Threshold for Level 3—each of the following must be met:

- (a) A binding commitment to construct Con-way replacement parking garage
- (b) Operation of the parking garage will commence in one year

Phase 3 (Final) Parking Standards:

Minimum requirements for all uses: None

Maximum parking standards:

All Office at 1.5 stalls per 1,000 SF

All Retail at 1.5 stalls per 1,000 SF

All Housing at 1 stall per unit

Map 06-2 shows the location and estimated amount of vehicular parking for each block within the Northwest Master Plan area. The amount of parking assumes that the maximum parking ratio is provided for each phase, and a specific development program is assumed for each block. The block by block development program may vary, but the overall FAR of 3:1 is maintained. Also, developers of individual blocks may choose to provide parking at levels below the maximum allowed. Map 06-2 is meant to illustrate one of many examples of how vehicular parking will be provided, and represents a maximum parking plan.

Bicycle parking will be provided with each development project per city requirements. Bicycle parking improvements will also be included within North/South and East/ West accessways. See Design Guidelines for pedestrian accessways in Section 5 – Development and Design Standards and Criteria, Parks and Open Space Design Guidelines. Incentives for full service bike stations are also included; see Neighborhood Facilities within the NW Master Plan area in Section 5.

B. STRATEGIES TO REDUCE THE NUMBER OF MOTOR VEHICLE MILES TRAVELED

Strategies to reduce vehicle miles traveled will be employed, as prescribed in the Transportation Demand Management (TDM) plan found in Section 5-Transportation Demand Management in the T.I.S.. In order to implement the TDM Plan, the Northwest Master Plan proposes the formation of a Community Resource Association (CRA) made up of Con-way, business and property owners located within and near the Master Plan area. The CRA will serve as a transportation management association. This Community

¹Bus transit service on NW 21st, free parking both private and on-street
²Lloyd District standards based on very good transit (rail/bus), paid on-street parking
³River District standards are based on very good transit (rail, bus, streetcar), paid on-street parking.

Resource Association will coordinate with or merge with a proposed neighborhood-wide transportation parking and demand management association if it is created.

The TDM Plan establishes a baseline mode split based on resident and employee travel characteristics currently experienced within and in the vicinity of the Master Plan area. The baseline mode split results in an estimated 19,200 daily vehicular trips at full build-out assuming maximum densities are achieved. Targets are set for reductions in the baseline vehicular trips based on Department of Environmental Quality (DEQ) rules and objectives of the City of Portland and Multnomah County Climate Action Plan (2009). These targets are a 10% reduction within 3 years, and a 30% reduction by 2030.

The TDM Plan identifies a large menu of measures to reduce reliance on single family vehicles and to increase the use of alternative transportation modes. TDM measures are organized into four categories: parking management; facilities for bicyclists, pedestrians and transit riders; incentive programs; and education programs.

The TDM Plan will be implemented by the CRA. The CRA will be responsible for appointing a TDM Coordinator, setting fees and coordinating fundraising, designing and administering TDM programs, conducting surveys and studies to monitor results, and preparing annual progress reports provided to the Portland Bureau of Transportation (PBOT). PBOT will determine whether the goals set forth in the TDM plan have been met, and whether further actions by the CRA are required.

The CRA will also coordinate with providers, and where feasible, electric car charging stations will be strategically located near employment centers within the development to facilitate their convenient use.

C. PLANNED IMPROVEMENTS TO ROUTES USED BY TRANSIT PATRONS

TriMet currently provides bus transit service on NW 21st to NW Thurman, and NW Thurman Street west of NW 21st. Portland Streetcar may be extended through the Northwest Master Plan area to serve Montgomery Park on a yet-to-be determined alignment. NW 21st is a likely streetcar route. See Map 06-4 for potential alignment.

NW 21st and Thurman Street are designated Neighborhood Collectors/Community Main Streets. PBOT standards for these streets require a 3 foot right-of-way dedication and construction of a 15 foot sidewalk on each side of these streets, providing significant pedestrian capacity along these transit routes.

All other streets within the Master Plan area will have 12 foot sidewalks which is the local street standard for the Central City. All streets will have two travel lanes, low vehicle speeds, pedestrian crossing treatments, on-street parking, and wide sidewalks. Mid-block pedestrian crossing treatments will be provided on all east/west streets.

In addition to wide sidewalks, the Northwest Master Plan is proposing an inter-connected system of pedestrian and bicycle corridors through both north/south and east/west pedestrian accessways. The effect of these accessways is to create a 200 foot block pattern for pedestrians in an area where east/west blocks measure 460 feet. This network will provide a more "fine-grained" pedestrian and bicycle system than

currently exists, and will increase pedestrian access opportunities to existing and future transit corridors.

Many of the design standards and guidelines included in Section 5- Development and Design Standards and Criteria, Parks and Open Space Design Guidelines, focus on ensuring that a high quality pedestrian environment is created throughout the Master Plan area, and particularly on NW 21st and NW Thurman. Standards and guidelines call for building entrances to be located on streets and accessways, retail and other active uses to be located in the ground floor of buildings, and weather protection to be provided, especially on NW 21st. These standards and guidelines ensure that residential, commercial and employment uses will be easily accessible by transit patrons, and that pedestrian routes used by transit patrons will be safe, convenient and enjoyable.

D. A STREET PLAN PROVIDING MULTI-MODAL STREET CONNECTIONS

PROPOSED STREET PLAN

The proposed street plan preserves the existing street classifications and street designations of the Transportation Element of the Portland Comprehensive Plan. See Map 06-5. The street plan provides a framework for development within the Master Plan area, and robust connections to the surrounding areas.

Streets within the Northwest Master Plan area will continue to operate consistent with their current functional classification as designated in the Portland Transportation System Plan. Accordingly, NW 21st Avenue and Thurman Street will continue to function and operate as Neighborhood Collectors and Community Main Streets, and NW 20th, 22nd Avenues, and NW Savior, Raleigh, Quimby, and Pettygrove Streets will continue to function and operate as Local Service Streets. See Figure 06-6 and 06-7 for street sections.

The southern portion of the Northwest Master Plan is currently designated as a Pedestrian District. The entire area is recommended to be included as a Pedestrian District when the Transportation System Plan is next updated.

Streets outside of Northwest Master Plan area that are impacted by traffic generated by the development will continue to function and operate consistent with their classifications in the Portland Transportation System Plan. These impacted streets include Neighborhood Collectors NW Vaughn Street, 23rd Avenue, and Lovejoy Street. In addition, NW 18th Avenue and 19th Avenue will continue to operate as Local Service streets.

All streets within the Northwest Master Plan area will continue to serve the modal designations that they currently serve. Accordingly, these streets will have two travel lanes, low vehicle speeds, pedestrian crossing treatments, on-street parking and wide sidewalks. As the entire Northwest Master Plan area is planned for Pedestrian District designation, all streets will have wide sidewalks: Local Streets-12 feet and Community Main Streets-15 feet.

NW 21st Avenue, Thurman Street and Raleigh Street will be designed to accommodate

future Portland Streetcar, pending the City's plan for routing in the district. Both NW21st Avenue and Thurman Street are appropriately designated as Community Main Streets to facilitate streetcar routing, and are currently served by TriMet bus routes.

In addition to wide sidewalks on all streets, the Northwest Master Plan is proposing an inter-connected system of pedestrian and bicycle corridors through both north/south and east/west public accessways. See Figure 06-8 for a street section of these streets. This network will provide a more “fine-grained” pedestrian and bicycle system than currently exists, and will increase pedestrian access to transit corridors. As noted earlier, sidewalks along current bus routes and potential streetcar service, NW 21st and Thurman, will be 15 feet wide providing additional pedestrian capacity for transit patrons.

Other than in the proposed accessways, no separated bicycle facilities are proposed. NW Thurman and Raleigh will continue to serve as City Bikeways, and other local streets will serve as Local Service Bikeways. The Northwest Master Plan provides incentives for full service bike stations that could be located in any number of future redevelopment sites, including the Con-way replacement parking structure and near the proposed neighborhood park.

IMPROVEMENTS TO CURRENTLY UNIMPROVED STREETS

NW Quimby Street

A parcel that would extend NW Quimby Street between NW 20th and 21st Avenues is currently private property and owned by Con-way. The parcel is designated as a local street in the Transportation Element of the City of Portland Comprehensive Plan. The design of this parcel will be important in that it is envisioned to have special qualities that allow it to successfully link the neighborhood park and square to the south with private development to the north. Design improvements proposed for the Quimby parcel are subject to approval by the Portland Bureau of Transportation.

The Multi-modal Transportation Impact Study discussed in the following sub-section, did not include the Quimby parcel in its analysis of traffic access and circulation within the Master Plan area. Existing and future traffic on surrounding streets was found to be appropriate for the street designations assuming the parcel was unavailable for through traffic movements.

Based on the traffic study and on the desire to carefully integrate the Quimby parcel with design of a neighborhood park and adjacent development, through pedestrian and bicycle access is the priority transportation function for the Quimby parcel. Guideline 7.C: NW Quimby Parcel found in Section 5 provides design guidance for the parcel which will be owned and maintained by adjacent development. Final design is subject to design review approval.

Other Unimproved Streets

NW Savier between NW 21st and 22nd does not currently exist as a public street but rather as private property. It will be restored as a public street/R.O.W. and re-dedicated to the City in concert with the development of Blocks 294 and 295.

NW Savier between NW 19th and 21st exists as a public ROW but is not fully improved in accordance with PBOT street standards. It will be expanded to meet City street

standards in concert with the development of Blocks 261, 262 and 292. The eastern-most portion of NW Savier to NW 19th is outside of the Master Plan boundary. ODOT owns the abutting property to the north, and St. Patrick’s church owns the abutting property to the south.

NW 20th between NW Raleigh and Savier does not currently exist as a public street but as private property. It will be redeveloped to meet City street standards in concert with the development of Blocks 292 and 262.

NW 20th between NW Raleigh and Thurman does not currently exist as a fully improved public street. The street will be improved as part of the preferred mitigation option to address traffic congestion at the NW 23rd and Vaughn intersection.

NW 20th between NW Upshur and NW Thurman does not currently exist as a public street but as ODOT ROW beneath the freeway ramps. This street is outside the Con-way Master Plan area. The street will be improved as part of the preferred mitigation option to address traffic congestion at the NW 23rd and Vaughn intersection. See the discussion of mitigation measures in a following section.

Con-way expects to burden the pro-rata share of cost for each street improvement to the properties that front or adjoin the right-of-way in question. They will require as a part of sales agreements that streets and related improvements will be made on a block-by-block basis to coincide with each development project. Efforts to establish an LID for all necessary street improvements are being discussed with PBOT, including the segment of NW Savior to NW 19th outside the Master Plan boundary. However no commitments have been made at this time.

E. DESIGN PRINCIPLES FOR PUBLIC STREETS

DESIGN AND FURNISH PUBLIC STREETS USING CITY ADOPTED DESIGN STANDARDS AND DETAILS.

BACKGROUND

The public streets within the Con-way Master Plan will be improved as adjacent development is constructed. Section 6-Transportation of the Master Plan describes the street plan on Map 06-05 and street cross sections for Neighborhood Community Main Streets on Figure 06-6 and Local Service Streets on Figure 06-7. Improvements between the property line and curb will be constructed consistent with adopted City of Portland street right-of-way standards.

The basis for selecting specific design standards and details shall be the Updated River District Right-of-Way Standards, February 2004. Refinement of standards and details, selection of specific features and improvements will be subject to design review approval when the first full block development proposal is submitted to the city for approval after the Con-way Master Plan is adopted. Subsequent development proposals shall comply with the adopted standards and details.

Design principles for public street improvements are described below. Considerations

for selecting specific standards and details include appearance and character, energy efficiency, color, spacing, durability and cost. References to applicable criteria from the Updated River District Right-of-Way Standards are provided as a basis for the design principles.

1. Concrete sidewalks consistent with City standard will be installed throughout the Master Plan area. (Pedestrian Criteria 2.5.1.1 Twelve Foot Sidewalks and 2.5.1.3 Fifteen Foot Sidewalks; Design Standards RD-6: 12' Sidewalk Base Condition and RD-11: 15' Sidewalk Base Condition). Sand-set Pavers within the furnishing zone will be required. (Design Standards 3.1.3: Sidewalk Pavement Treatments: Flexible)
2. Curb extensions consistent with City standards will be installed at all intersections as shown on Map 06-05 unless traffic operational considerations preclude their use. (Standard Plan RD-8: 12' Sidewalk Extended Curb).
3. Mid-block pedestrian crossings consistent with City standards will be installed on all east/west streets as shown on Map 06-05.
4. Pedestrian-scaled lighting consistent with City standards will be installed on all streets. Cobra fixtures will not be used unless the City determines that these fixtures are necessary for safety and security. (Configuration and Separation Criteria per 2.6.1.2: Street Lighting Criteria for Single Ornamental; fixtures such as post top products including Lumec Zenith Series, Lumec Metroscape and Hadco Single Ornamental will be considered for application throughout the Master Plan Area).
5. Street trees will be installed on all streets consistent with City standards. The City Forester will be consulted on street tree selection. (Street Tree Performance Criteria: 2.8.1 Uniform Plantings/Upright Street Trees for east/west streets; 2.8.2 Uniform Plantings/Broad-headed Street Trees for north/south streets; 2.8.3 Infill/Established Street Trees where significant existing street tree plantings currently exist).
6. Street furnishings including but not limited to benches, trash receptacles, and the like will be selected and installed consistent with City standards. The number and location of furnishings will be approved through design review.

F. MULTI-MODAL TRANSPORTATION IMPACT STUDY

The transportation impact study described in the T.I.S. and Addendum Memo were prepared in accordance with the Portland Bureau of Transportation (PBOT) and Oregon Department of Transportation (ODOT) requirements for transportation impact studies as well as consultation with PBOT staff. Manual traffic counts including turning volumes were taken in 2008 at a number of locations in Northwest Portland, additional traffic data was collected in August and October 2011 to verify and calibrate earlier counts, and further counts were completed in June 2012 to assess the preferred mitigation option.

EXISTING TRAFFIC CONDITIONS

Traffic volumes at the study intersections were evaluated against City of Portland and ODOT operating standards. The results of the study are:

- All of the study intersections under City of Portland jurisdiction operate acceptably during the weekday a.m. and p.m. peak hours, except the NW 23rd/Vaughn

intersection.

- The NW 23rd Avenue/Vaughn Street (US 30) intersection exceeds the 0.99 volume-to-capacity ratio (v/c) ODOT standard as well as the City of Portland standard for level of service (LOS) during the weekday p.m. peak hour. This intersection currently operates at LOS E, while the City's standard is for a minimum LOS D.
- A review of historical crash data did not reveal any patterns or trends in the site vicinity that require safety mitigation associated with this project.

2022 BACKGROUND TRAFFIC CONDITIONS

Background traffic was forecast using Metro regional models and historical traffic count data. Although little traffic growth has occurred over the past 5 years, a 1% per year growth rate was applied to existing traffic volumes to estimate 2022 background traffic conditions. All assumptions were made in consultation with PBOT staff. The 2022 planning horizon year was chosen because the Northwest Master Plan, when adopted, is valid for 10 years.

The results of the 2022 background traffic conditions are:

- All but one of the study intersections are forecast to continue to operate at acceptable levels of service during the weekday a.m. and p.m. peak hours according to City of Portland and ODOT mobility standards.
- The NW 23rd Avenue/Vaughn Street (US 30) intersection is forecast to continue to exceed the 0.99 volume/capacity ratio (v/c) ODOT standard as well as the City of Portland LOS D standard for level of service (LOS) during both the weekday a.m. and p.m. peak hours prior to site development.

PROPOSED NORTHWEST MASTER PLAN DEVELOPMENT PLAN

The Northwest Master Plan area is proposed to include an estimated 2.28 million square feet of floor area, which includes approximately 330,000 square feet of office uses that currently exists on the property. This development plan assumes that all blocks will be fully developed at a maximum FAR of 3:1 and that no FAR bonuses are available. The trip generation from full build-out assumes a 15% reduction in auto trips compared to the suburban ITE trip rates, due to the urban location of the site and existing transit service.

- The proposed Northwest Master Plan development (excluding the existing 330,000 square feet of office) is estimated to generate approximately 17,090 daily net new trips;
- 1,145 net new vehicle trips (585 inbound, 560 outbound) are projected to occur during the weekday a.m. peak hour;
- 1,535 net new vehicle trips (740 inbound, 795 outbound) are projected to occur during the weekday p.m. peak hour.

A forecast of trip generation was conducted based on development permitted under existing zoning, including a partial utilization of FAR bonuses currently allowed. Planned uses in the Northwest Master Plan development generate 8% and 4% less trips during typical weekday a.m. and p.m. peak hours, respectively, than would reasonably be generated by development of the property under its current comprehensive plan and zoning designations.

2022 TOTAL TRAFFIC CONDITIONS

A forecast of total traffic conditions combined the 2022 background traffic with the full build-out of the proposed Northwest Master Plan development. This forecast represents a “worst-case” traffic scenario because:

- Utilization of 100% of available FAR was assumed in the Northwest Maser Plan area. Actual development projects in the area rarely utilize all of the FAR available.
- Full development of the entire Northwest Master Plan area was assumed by 2022. This represents a very aggressive time frame for the market to absorb almost 2 million square feet of residential, office and commercial development at one location.
- No trip reductions were assumed resulting from TDM measures. As part of this Northwest Master Plan, Con-way has committed to implement the TDM plan discussed earlier. The TDM plan targets a 30% reduction in vehicle trips.

The total traffic conditions in 2022 with the Northwest Master Plan fully developed results in the following:

- Upon site build-out, all of the study intersections are forecast to continue to operate acceptably during the weekday a.m. and p.m. peak hours with the following exceptions:
- The NW 23rd Avenue/Vaughn Street (US 30) intersection is forecast to continue to operate unacceptably during the a.m. and p.m. peak hours.
- The NW 23rd Avenue/Raleigh Street un-signalized intersection is forecast to operate unacceptably during the weekday a.m. and p.m. peak hours.

G. IDENTIFICATION OF TRAFFIC IMPACTS, MITIGATION MEASURES AND OTHER RECOMMENDATIONS

The following provides a summary of the mitigation measures recommended as part of the Northwest Master Plan. The traffic impacts summarized above include unacceptable operation at the NW 23rd Avenue/Vaughn Street intersection, and at the NW 23rd Avenue/Raleigh Street intersection.

A description of mitigation measures at both intersections are included below. Maps 06-9 and 06-10 show conceptual plan options for the NW 23rd/Vaughn intersection improvements. The first option separates the existing westbound left turn to NW 23rd from the U-turn movement while the second option eliminates the U-turn. Both options increase the northbound right turn capacity from NW 23rd. Both options result in acceptable traffic operations at the intersection for the forecast period.

NW 23RD AVENUE/NW VAUGHN STREET MITIGATIONS – OPTION 1:

A description of mitigation measures at both intersections are included below. Map 06-9 shows a conceptual plan for the NW 23rd/Vaughn intersection improvements.

- Re-stripe the westbound approach so that one through lane and two left-turn lanes are provided. The dual left turn lanes will be designated for one exclusive U-turn only lane and one exclusive left-turn lane. The existing channelized right-turn lane will remain unchanged.
- Re-stripe the northbound approach so that the right-most lane becomes a channelized free right-turn lane, allowing the northbound right turn movement to operate independent of the traffic signal. A concrete island will be designed and constructed at the intersection in conjunction with the changes to facilitate the channelization. The existing northbound left-turn and through-lanes will remain unchanged.
- Re-stripe the eastbound approach to provide one through-lane and one right-turn lane. This would be facilitated by converting the outermost of the two through lanes to an exclusive right turn lane.

The Oregon Department of Transportation has indicated that due to weaving issues on the east-bound approaches to the freeway, this option is not supported.

NW 23RD AVENUE/NW VAUGHN STREET MITIGATIONS – OPTION 2 (PREFERRED OPTION):

The Appendix Memorandum found in Appendix E.1 describes the option 2 mitigation and the analysis performed to evaluate the effectiveness of the proposal. Maps 06-10, 10b and 10c show a conceptual plan for the mitigation. The mitigation plan is summarized below:

- Modify the northbound approach to provide a separate shared through/left-turn lane and two northbound right-turn lanes. The right-turn lanes will run with green arrows (protected movement) concurrent with the northbound left-turn/through phase and when the westbound left-turn is operating (overlap phase).
- Modify the southbound approach to provide a left-turn lane and a shared left-turn/through/right-turn lane.
- Operate the northbound and southbound approaches as split phases, along with a right-turn overlap for the northbound right-turn lane.
- Reconstruct the existing pedestrian crosswalks on the northbound NW 23rd Avenue approach to reflect elimination of the westbound U-turns, shifting the crosswalk and stop bar north and introducing a pedestrian island. The new crosswalk location will shorten the intersection crossing distance for both pedestrians and vehicle traffic, improving intersection operational efficiency.
- Construct an extension of NW 20th Avenue below US 30 from NW Upshur to NW Raleigh. Either lower the grade of the one-way eastbound segment of NW Thurman between NW 21st and 20th and form a new intersection with the extended NW 20th or vacate the existing one-way eastbound segment of NW Thurman between NW 21st and 20th.
- Maintain the existing westbound approach striping with two through lanes, one right-turn lane, and one left-turn lane. The left-turn lane will be designated for an exclusive left-turn lane only. U-turns will not be allowed. Signage will be provided directing traffic wanting to access NW Thurman, 19th, 16th, Naito Parkway and the Con-way area to utilize the existing

channelized right-turn lane, which will remain unchanged.

- Provide new roadway guide signs to direct motorists on US 30 westbound to turn right at NW 23rd Avenue to access NW Thurman Street, NW 19th Avenue, NW 16th Avenue, Naito Parkway and the Con-way area using the existing channelized right-turn lane.
- The eastbound approach providing one through-lane and one shared through/right-turn lane, would remain unchanged.

NW 23RD AVENUE/NW RALEIGH STREET MITIGATIONS:

Install a traffic signal to accommodate the increase in traffic to and from the Con-way property development. This traffic signal should be coordinated with the 23rd/Thurman traffic signal. The redesign of the intersection should discourage through westbound traffic into the adjacent residential neighborhood. The 23rd/Raleigh signal could accommodate Portland Streetcar crossings of 23rd Avenue, although the routing of streetcar in the area is yet to be determined.

STREET PLAN MULTI-MODAL CONNECTION RECOMMENDATIONS:

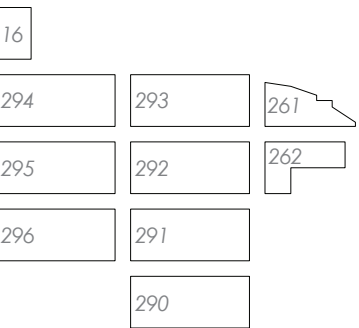
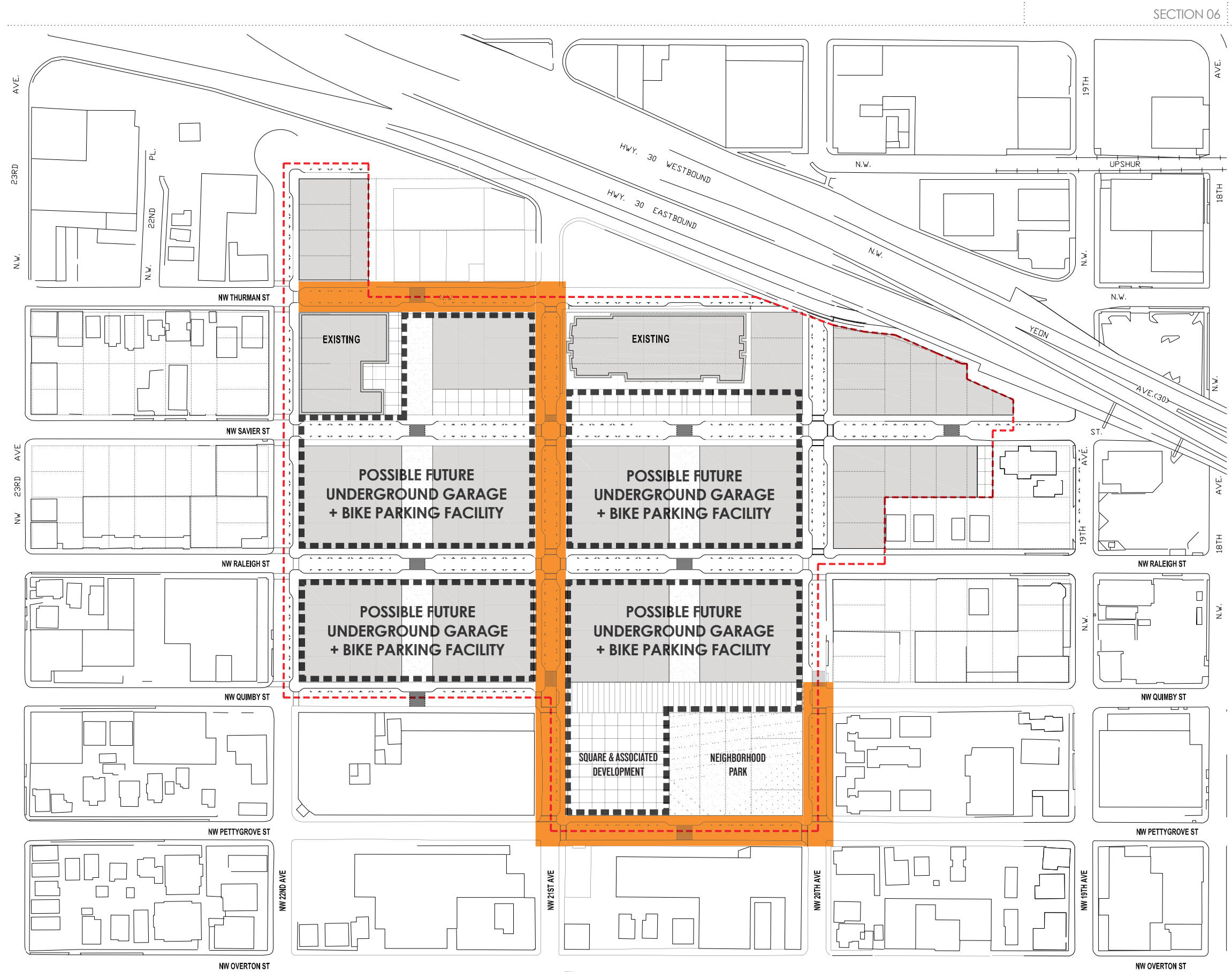
TriMet currently provides bus transit service on NW 21st to NW Thurman, and NW Thurman Street west of NW 21st. Portland Streetcar may be extended through the Northwest Master Plan area to serve Montgomery Park on a yet-to-be determined alignment. NW 21st is a likely streetcar route. See Map 06-4 for potential alignment.

Bus transportation to the Northwest Master Plan area should be coordinated with TriMet to restart Route #17: 21st Avenue and increase Route 77 service through the Master Plan area.

Wide sidewalks will be provided on all streets within the Northwest Master Plan area to improve the ability of residents, patron, and employees in the development to move throughout the area. In addition, particular attention will be given to walking routes from transit stops to attractions within the district. Sidewalks will be constructed with minimum widths of 12 feet, and 15 feet on NW 21st Avenue and Thurman. NW Quimby Street, between NW 20th and 21st, and the mid-block accessways will be designed for pedestrian and bicycle use, open to vehicle use for necessary service and emergency purposes.

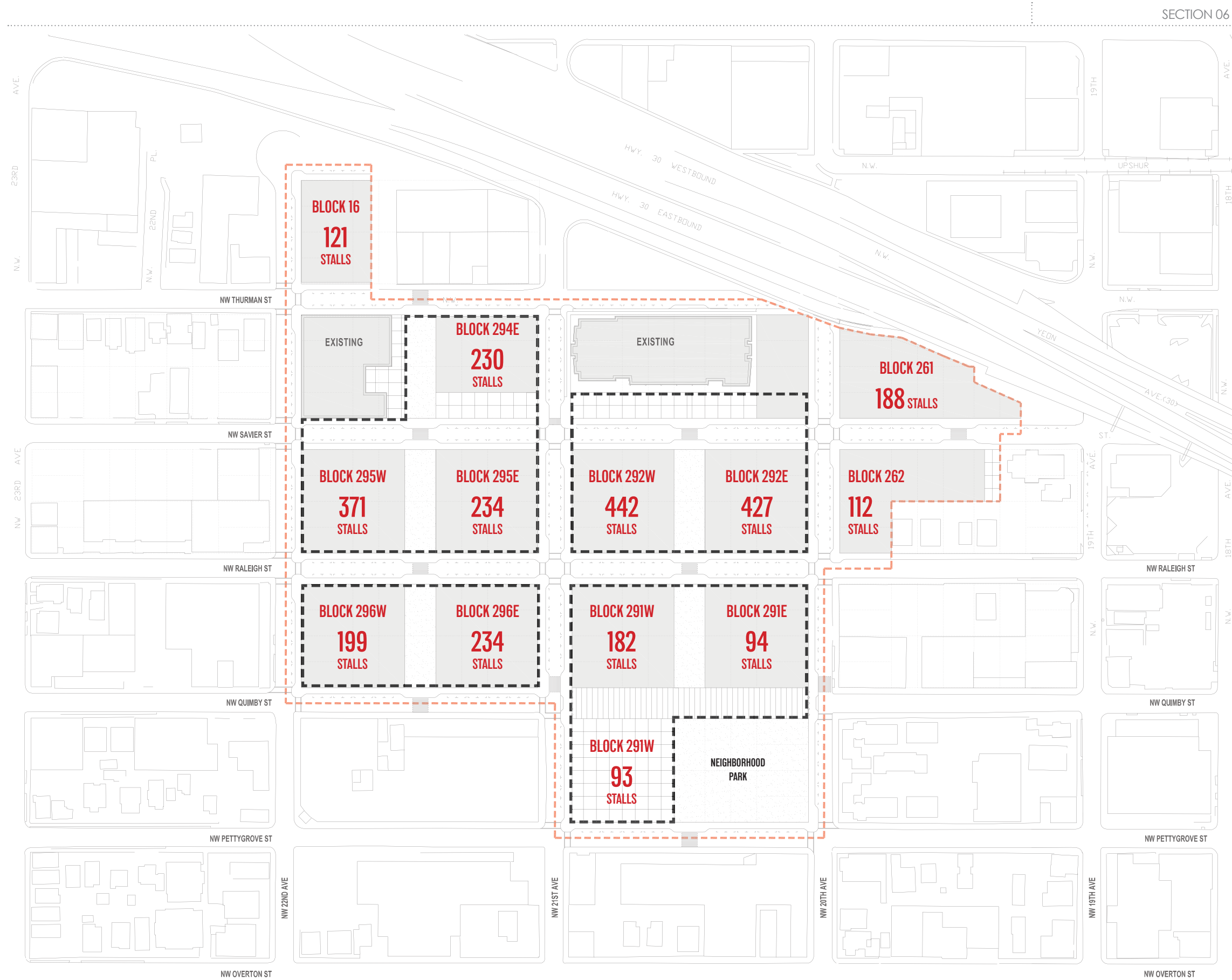
KEY

STREETS WHERE GARAGE ACCESS
AND LOADING DOCKS ARE PROHIBITED



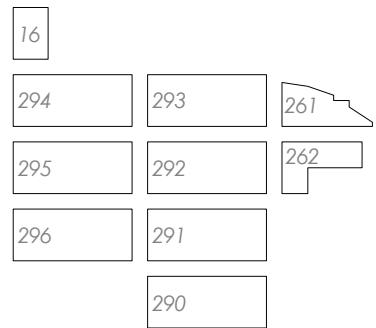
MAP 06-1
POSSIBLE FUTURE
UNDERGROUND GARAGE
LOCATIONS AND ACCESS





Please see Appendix A for a detailed model showing the area calculations that inform these numbers.

All information provided on this diagram is approximate in nature and will ultimately depend on future development plans.



MAP 06-2
POSSIBLE FUTURE PARKING
COUNTS

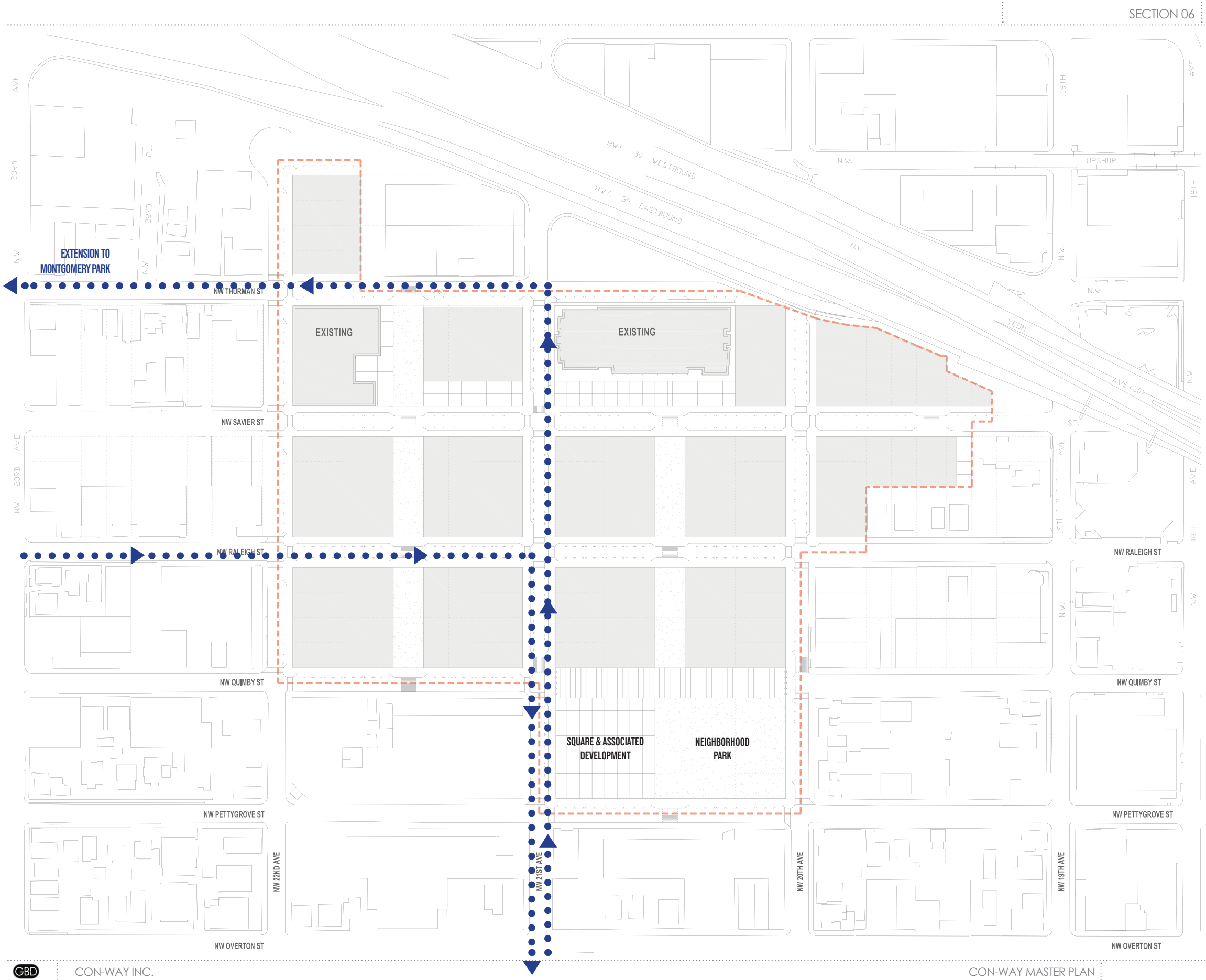


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KEY

POTENTIAL STREETCAR ALIGNMENT

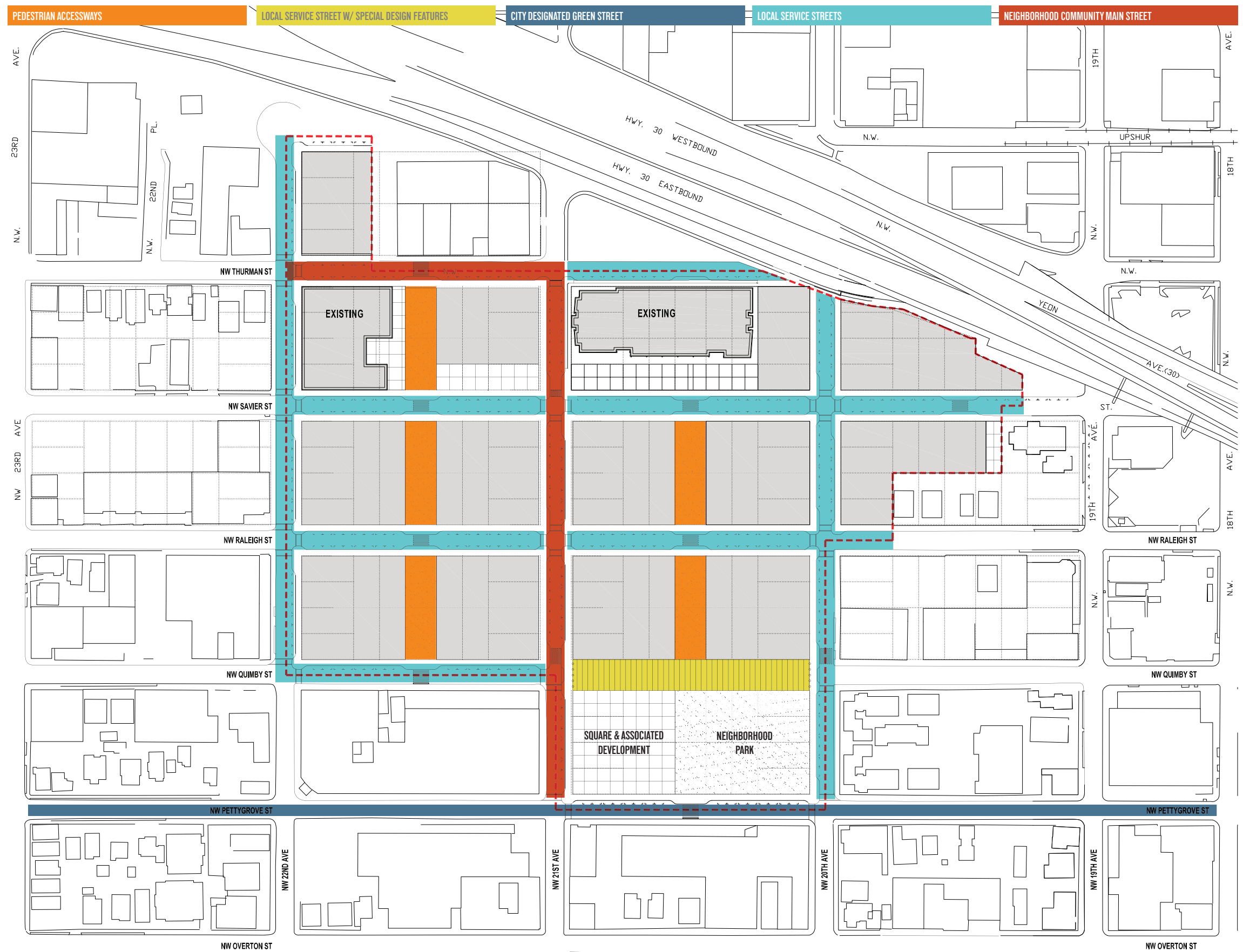
The Con-way plan includes provisions for an extension of the Portland Streetcar system to be constructed when funds become available. Potential route is shown on Map 06-4.



16		
294	293	261
295	292	262
296	291	
	290	

MAP 06-4
POTENTIAL
STREETCAR ALIGNMENT





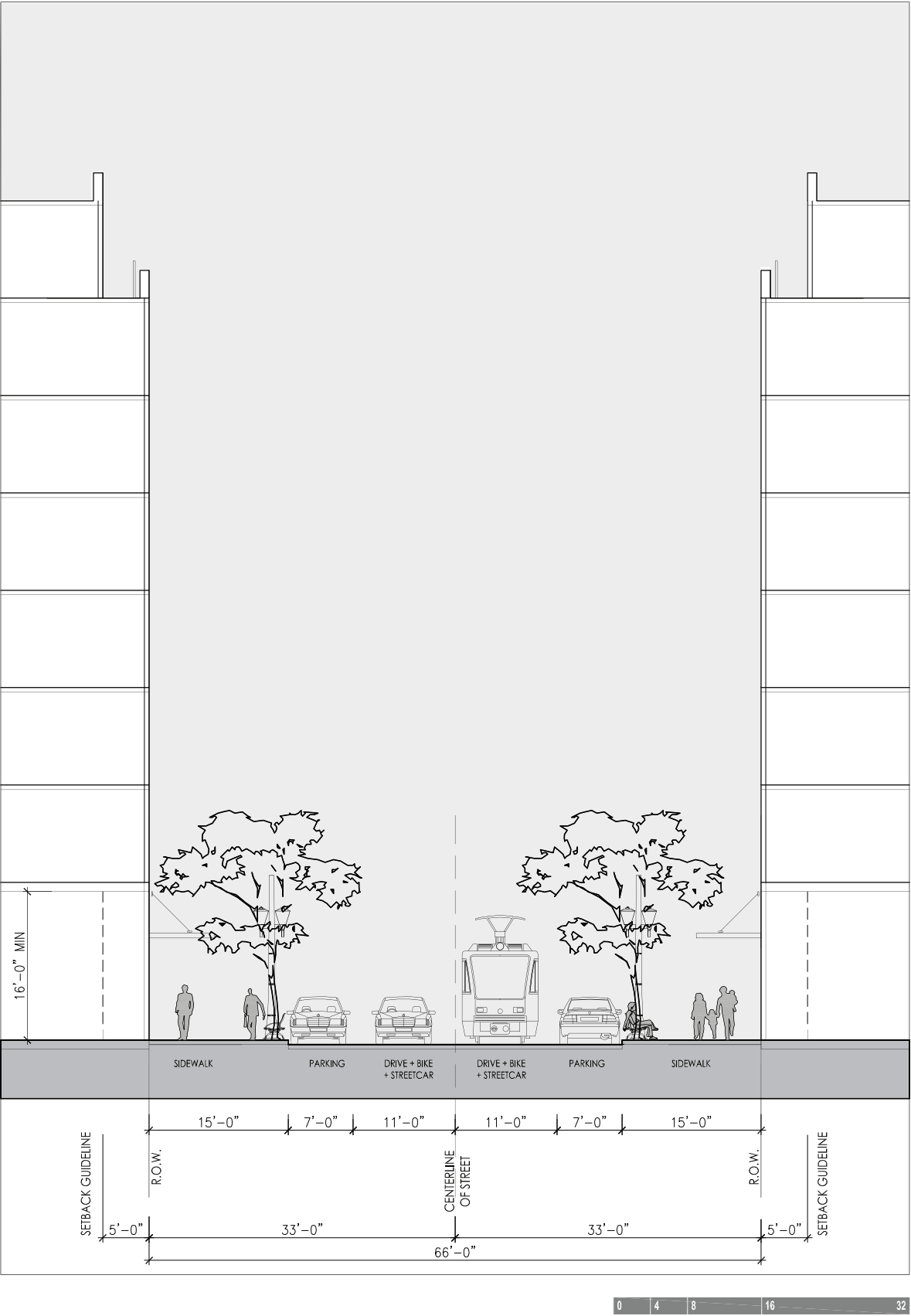
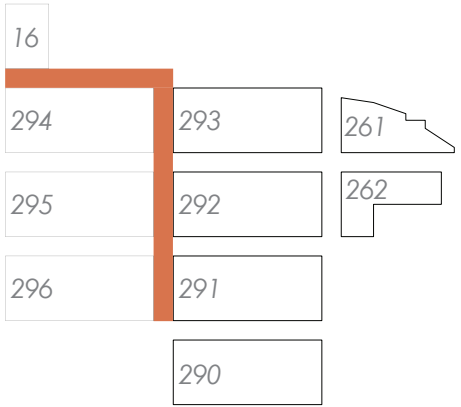


FIGURE 06-6
STREET SECTION

NEIGHBORHOOD COMMUNITY MAIN STREET	
Right of Way	66'
Roadway	36'
Sidewalks	15'
Curbline	Extensions
Circulation	2-way w/ Streetcar
Parking	Both sides
Setbacks	5' at ground flr (guideline)
Lighting	Yes
Furnishings	Yes



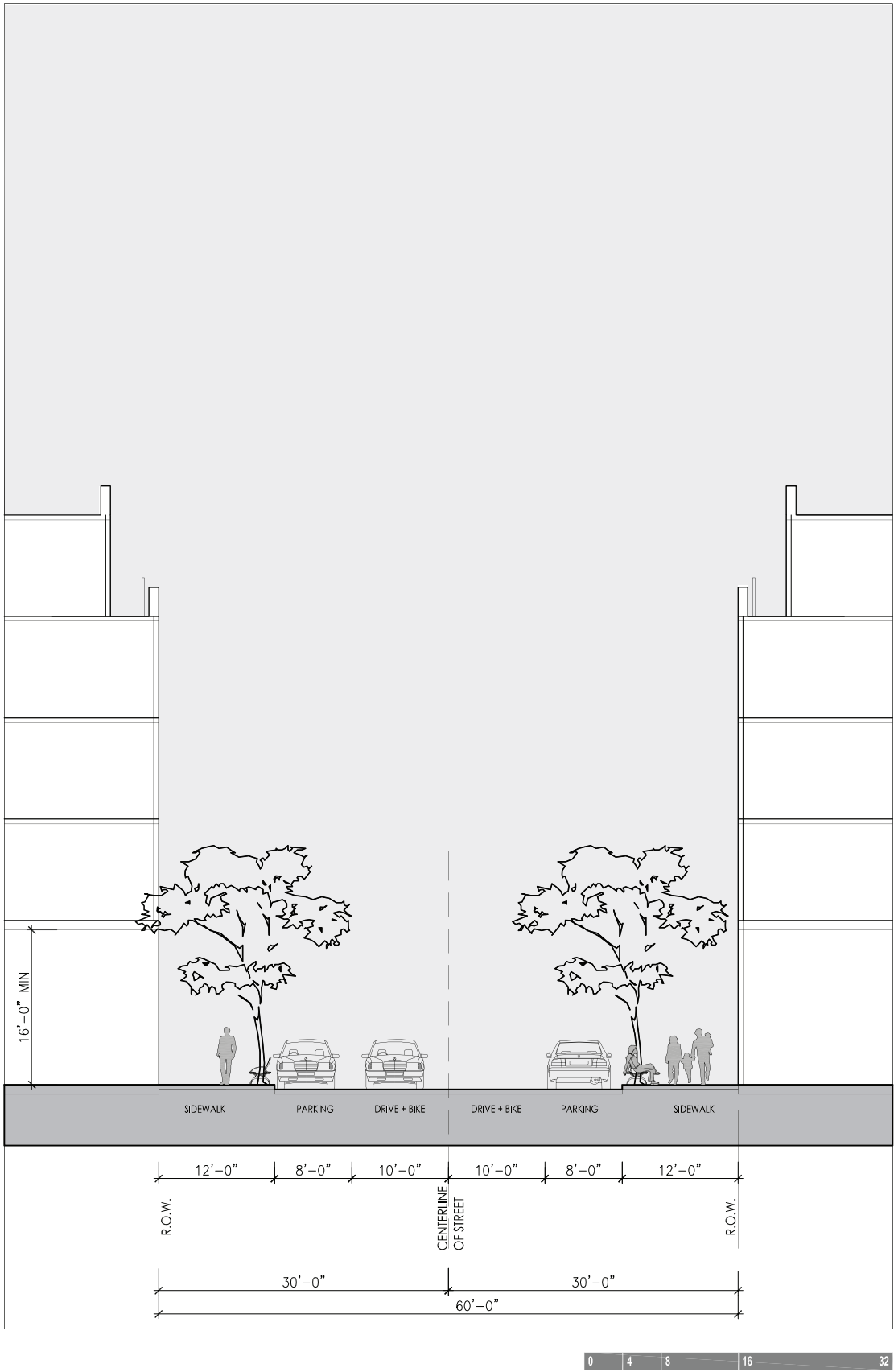
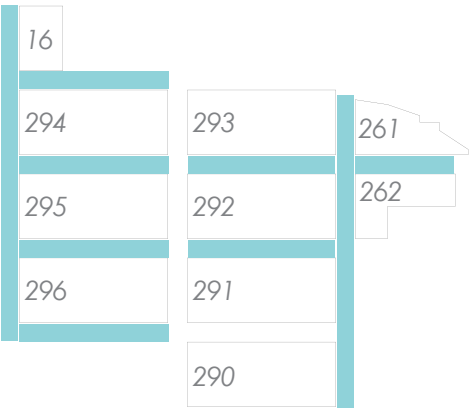


FIGURE 06-7
STREET SECTION

LOCAL SERVICE STREETS	
Right of Way	60'
Roadway	36'
Sidewalks	12'
Curbline	Extensions
Circulation	2-way
Parking	Both sides
Setbacks	Residential only
Lighting	Yes
Furnishings	Yes



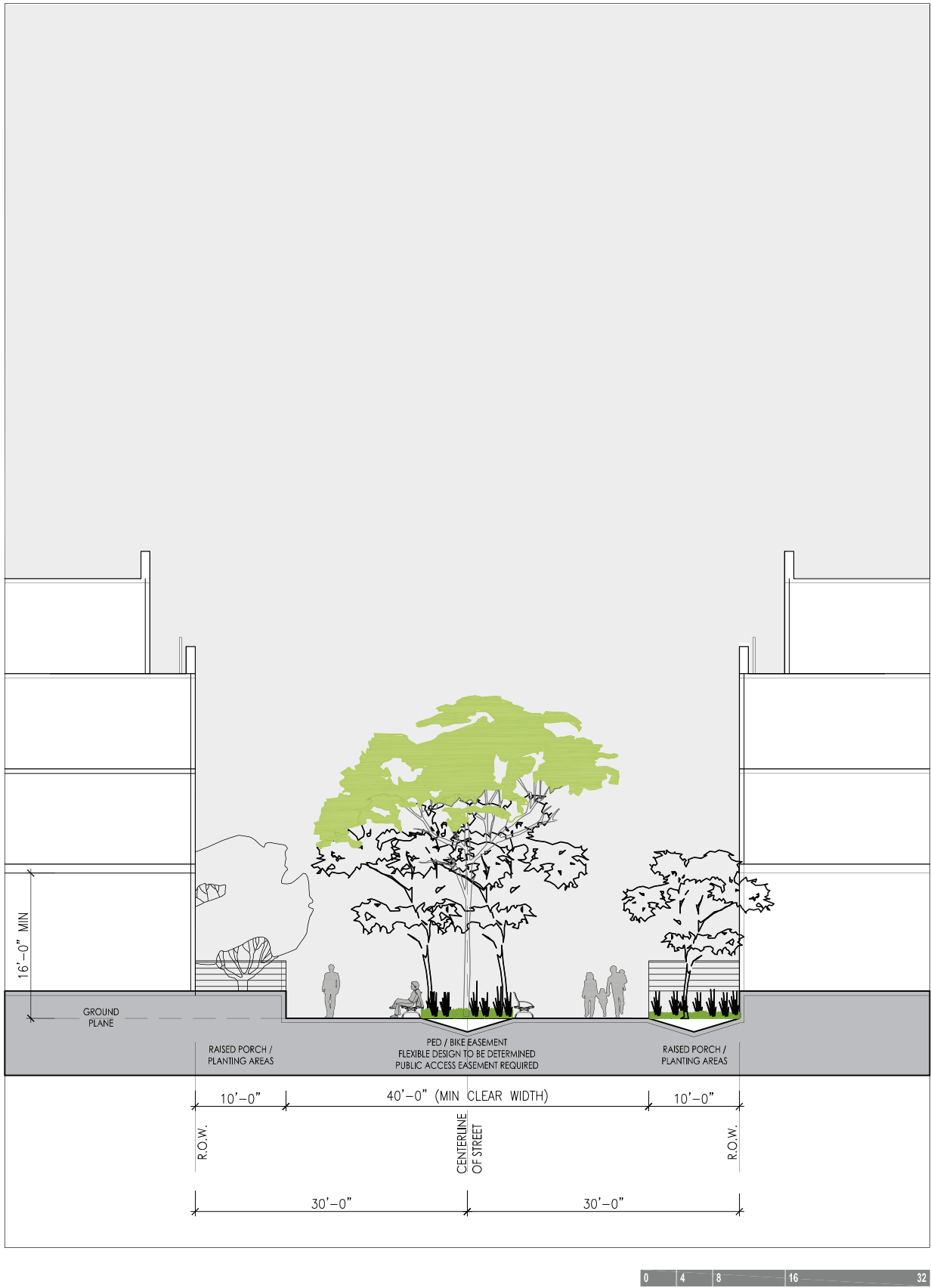
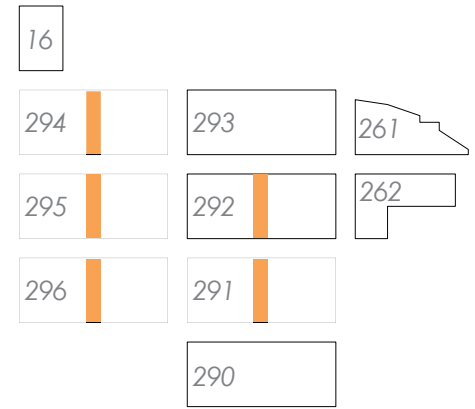


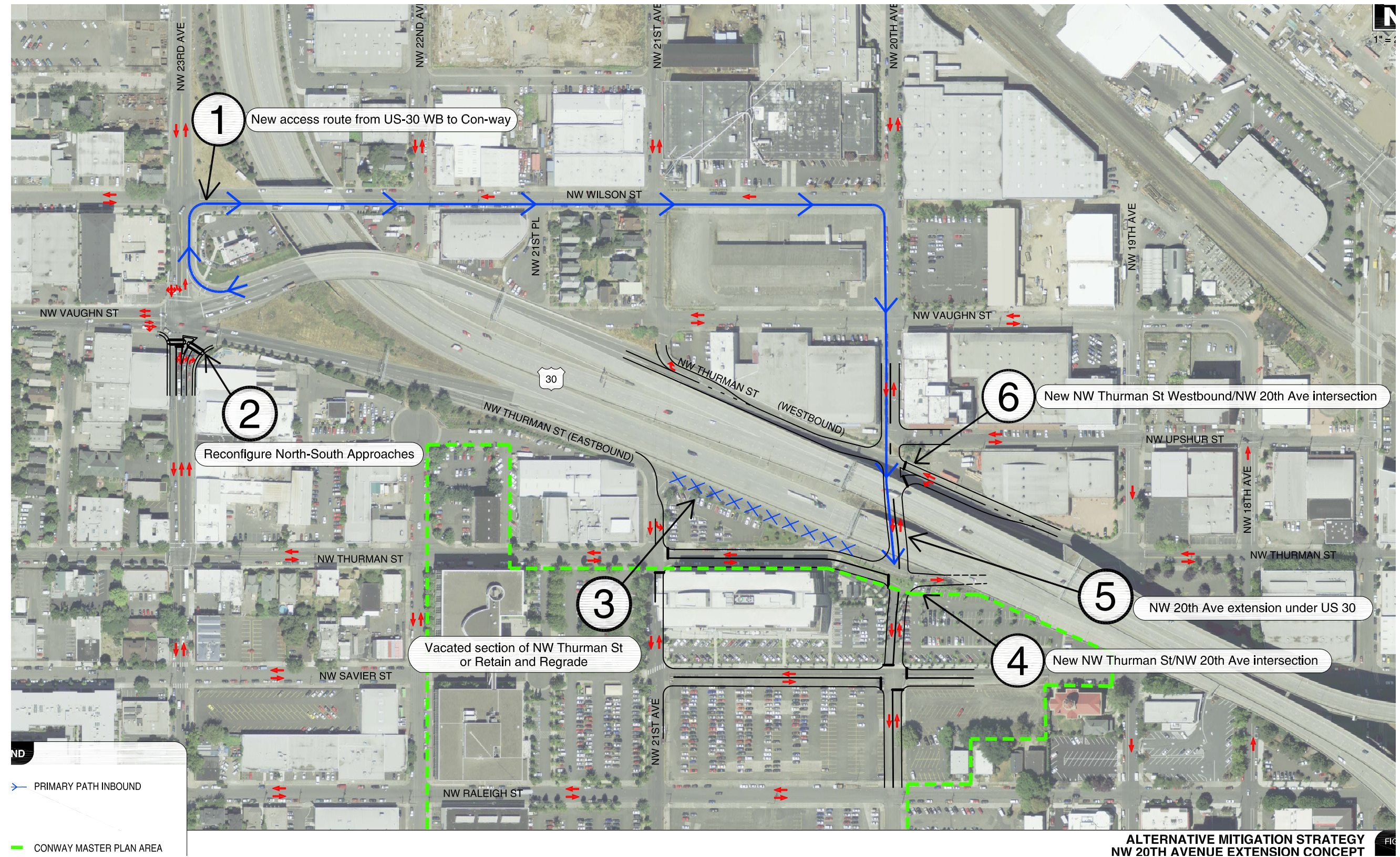
FIGURE 06-8
STREET SECTION

PEDESTRIAN ACCESSWAYS	
Right of Way	60'
Ped / Bike Easement	40' flexible design
Sidewalks	Flush
Curbline	No curb
Circulation	Limited vehicle access
Parking	None
Setbacks	10' plant / porch zone
Lighting	Yes
Furnishings	Yes





MAP 06-9
OPTION 1: IMPROVEMENTS
AT 23RD AND VAUGHN

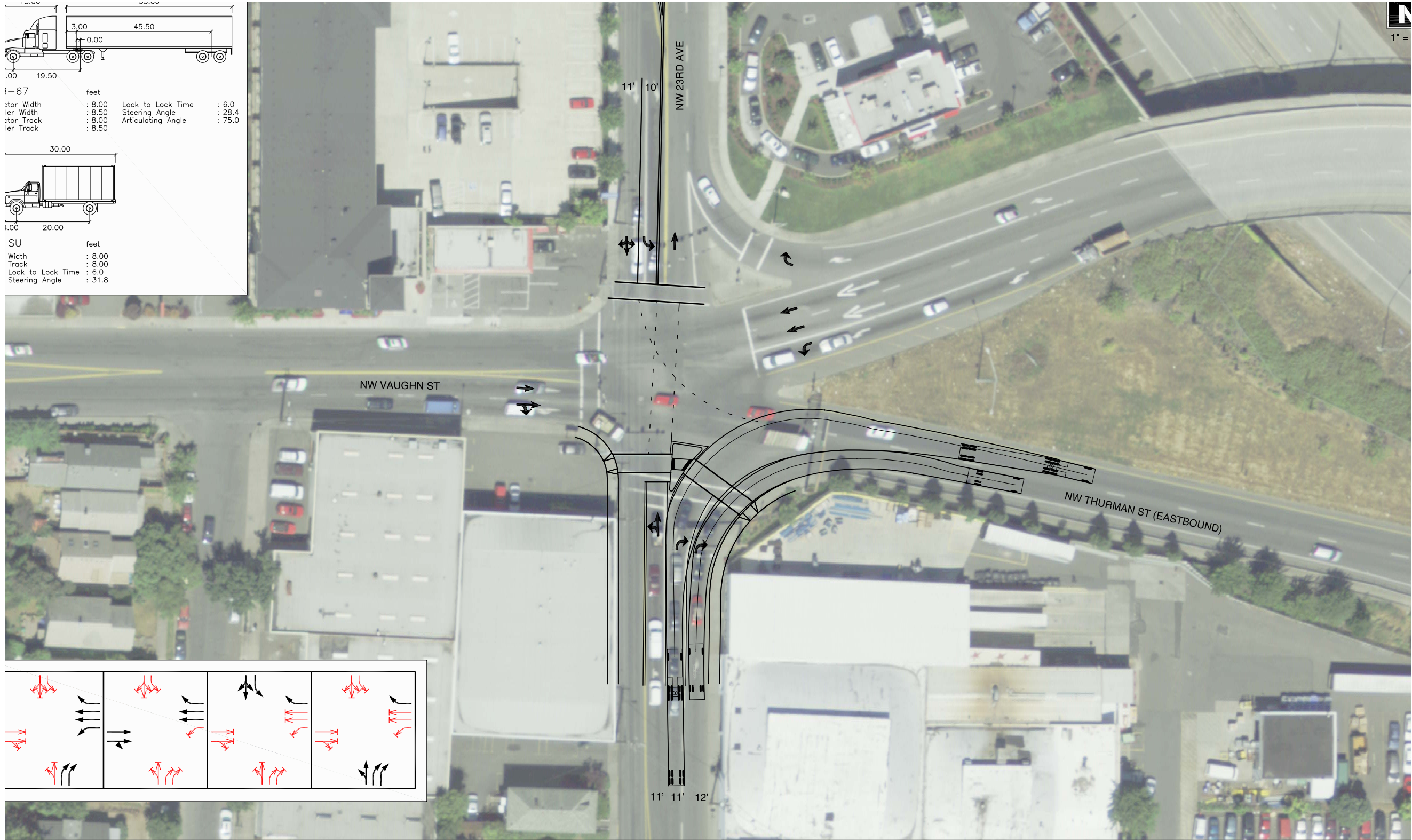
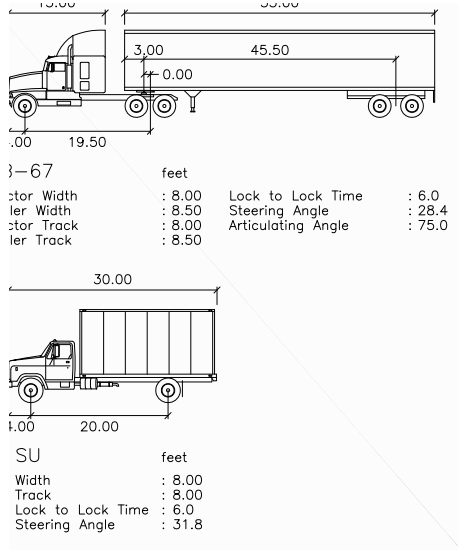


MAP 06-10
OPTION 2: IMPROVEMENTS AT 23RD AND VAUGHN



KITTELSON & ASSOCIATES, INC.
TRANSPORTATION ENGINEERING / PLANNING

KITTELSON & ASSOCIATES, INC.
TRANSPORTATION ENGINEERING / PLANNING



NW 23RD AVENUE/NW VAUGHN STREET RECONFIGURATION CONCEPT

MAP 06-10c
OPTION 2: IMPROVEMENTS AT 23RD AND VAUGHN

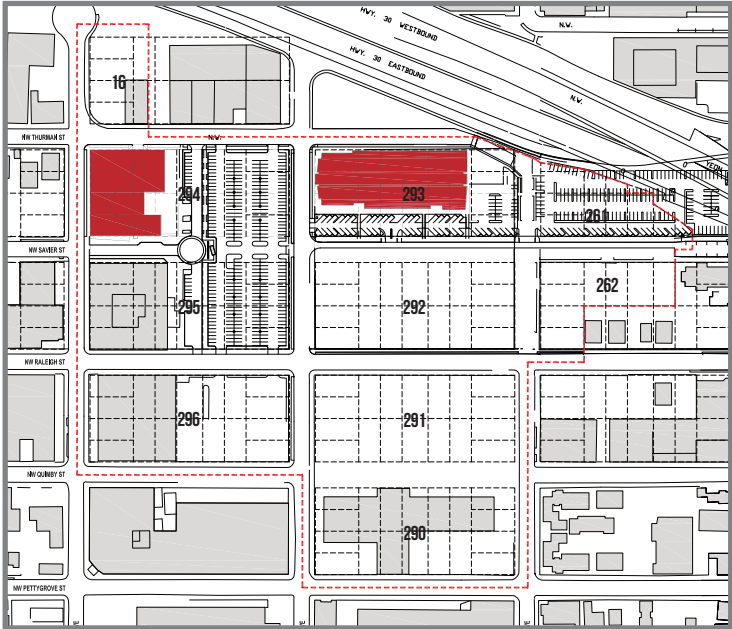
SECTION 7

PHASING OF DEVELOPMENT

33.562.300.D

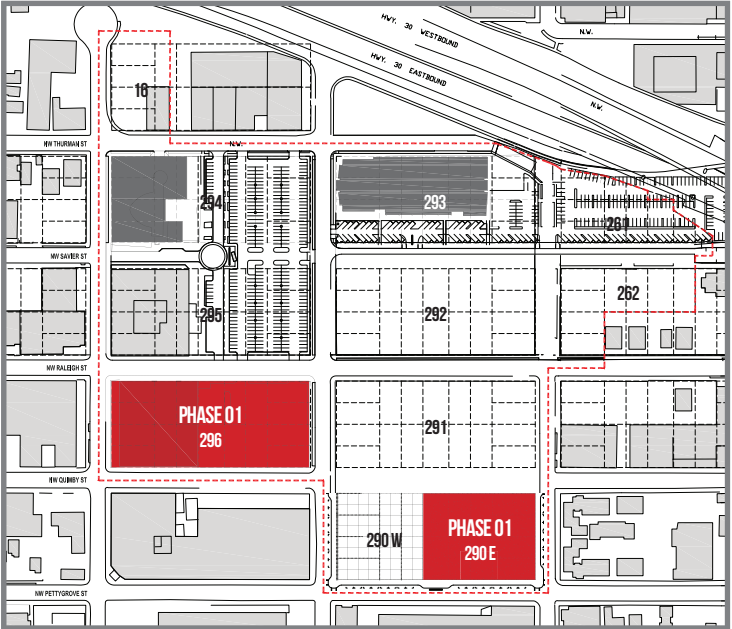
7. Phasing of development. The Northwest Master Plan must include the proposed development phases, probable sequence for proposed developments, estimated dates, and interim uses of property awaiting development. In addition, the plan should address any proposed temporary uses or locations of uses during construction periods.

103	MAP 07-1 PHASING OF DEVELOPMENT
104	PHASING OF DEVELOPMENT
105	EXISTING CONDITIONS
106	PHASE 01
107	PHASE 02
108	PHASE 03
109	PHASE 04
110	PHASE 05



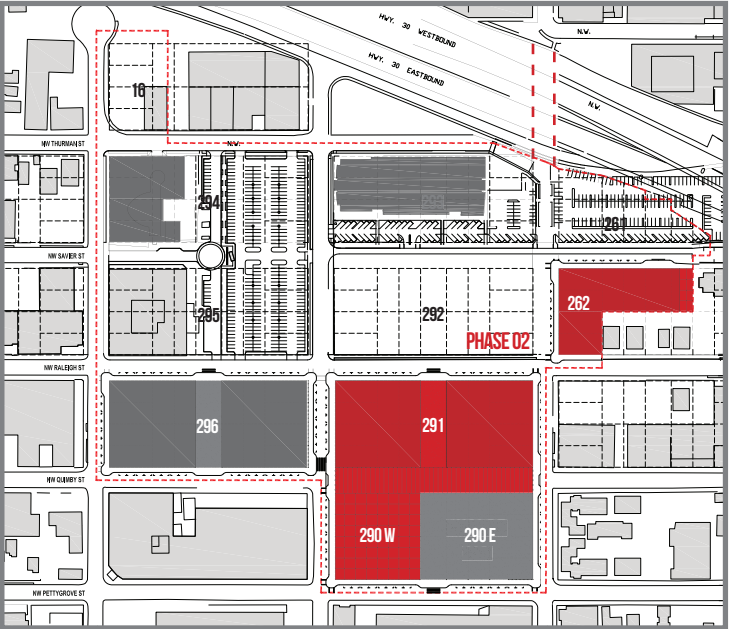
EXISTING CONDITIONS

Con-way is to remain in two of their existing office buildings. Buildings on blocks 295, 296, and 290 may or may not be reused during initial and later phases of development.



PHASE 01

Adaptive re-use of an existing building plus first new catalyst development on Block 296. Demolition of existing truck maintenance building and development of neighborhood park on Block 290 E.

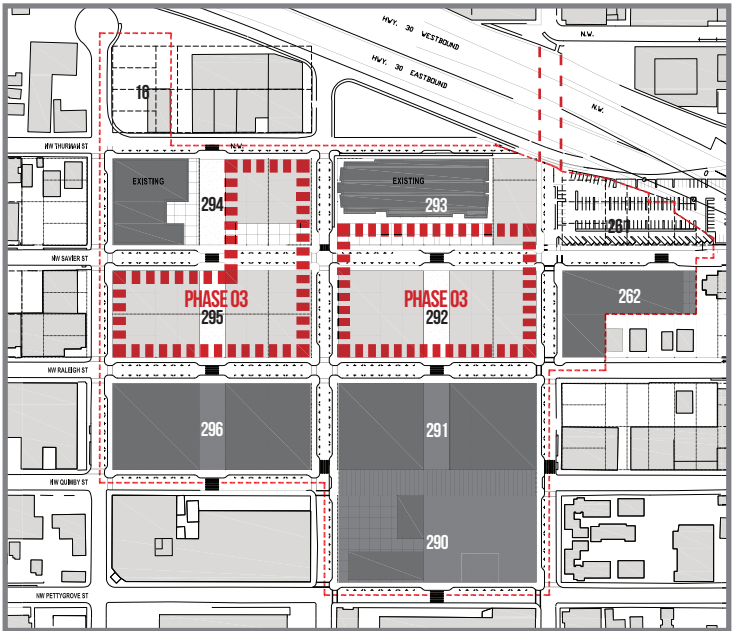


PHASE 02

Development of parcels that adjoin the neighborhood park. Development of other fringe parcels. Traffic/street improvements per Section 6.

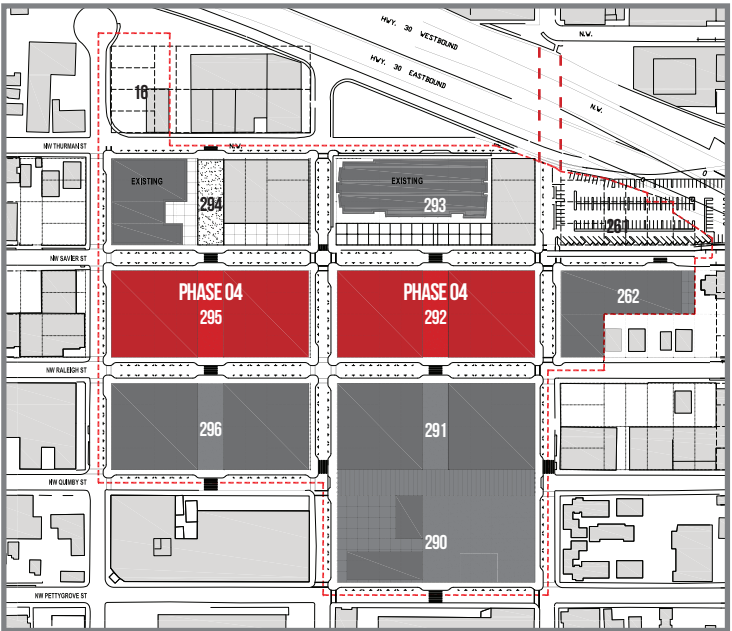
NOTES: Street improvements are shown to occur in conjunction with each adjacent property.

NW 20th, from NW Upshur to Savier, will be improved pursuant to Section 6.



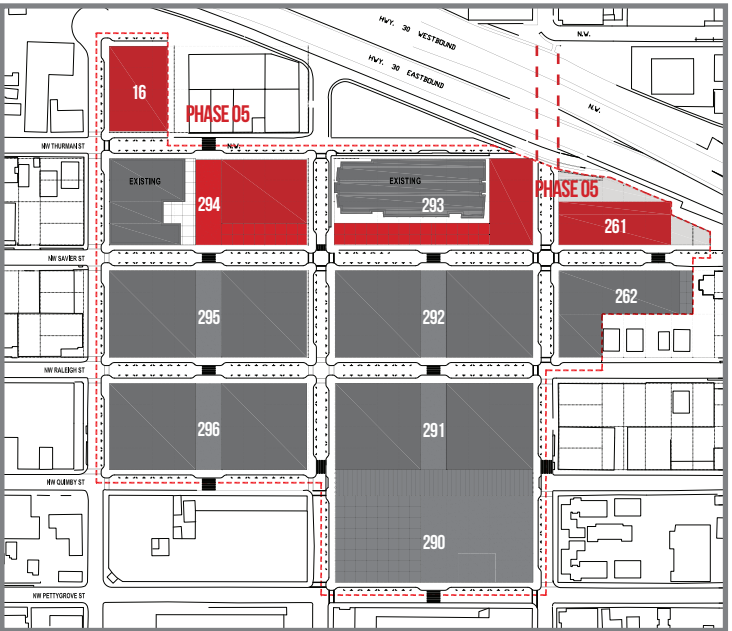
PHASE 03

Underground parking built to replace Con-way's surface lots



PHASE 04

Development occurs over underground garages.



PHASE 05

Parcels at the north end of the property are redeveloped. These could be office or institutional uses due to proximity to freeway.

PHASING OF DEVELOPMENT

As described in the **OVERVIEW** to this application, Con-way will sell its vacant properties to a variety of developers in a highly controlled fashion. Neither final design nor construction can occur on any property until the Master Plan is approved. Con-way does not intend to act as a developer on any of its parcels. Development is anticipated to be largely private in nature for both commercial and residential uses. Civic and/or institutional uses, involving public/private partnerships, are also quite possible but will likely be included as a component of larger mixed-use buildings.

Given these parameters, market forces will play a significant role in determining the phasing and timing of the overall development. Market needs, economics and developer capacity will clearly influence the pace and viability of every parcel. As such, Con-way cannot commit to specific plans for the eventual placement, program or timing of any of the individual parcels within the Master Plan boundary, beyond Phase 1. Changes to the phasing approach proposed in this Master Plan do not require an Amendment to the Master Plan.

What Con-way will do, however, is to maintain a controlling position in the sale of all of its properties. Con-way will develop a framework for phasing that is ultimately controlled by the careful selection of developers and staging of acquisitions. The framework will include special development conditions as a part of the sales agreement for each parcel. As a long-term Northwest District neighbor, Con-way has every incentive to attend to the details of each transaction and ensure that each developer is bound to produce the attributes described in this Master Plan.

TIMING

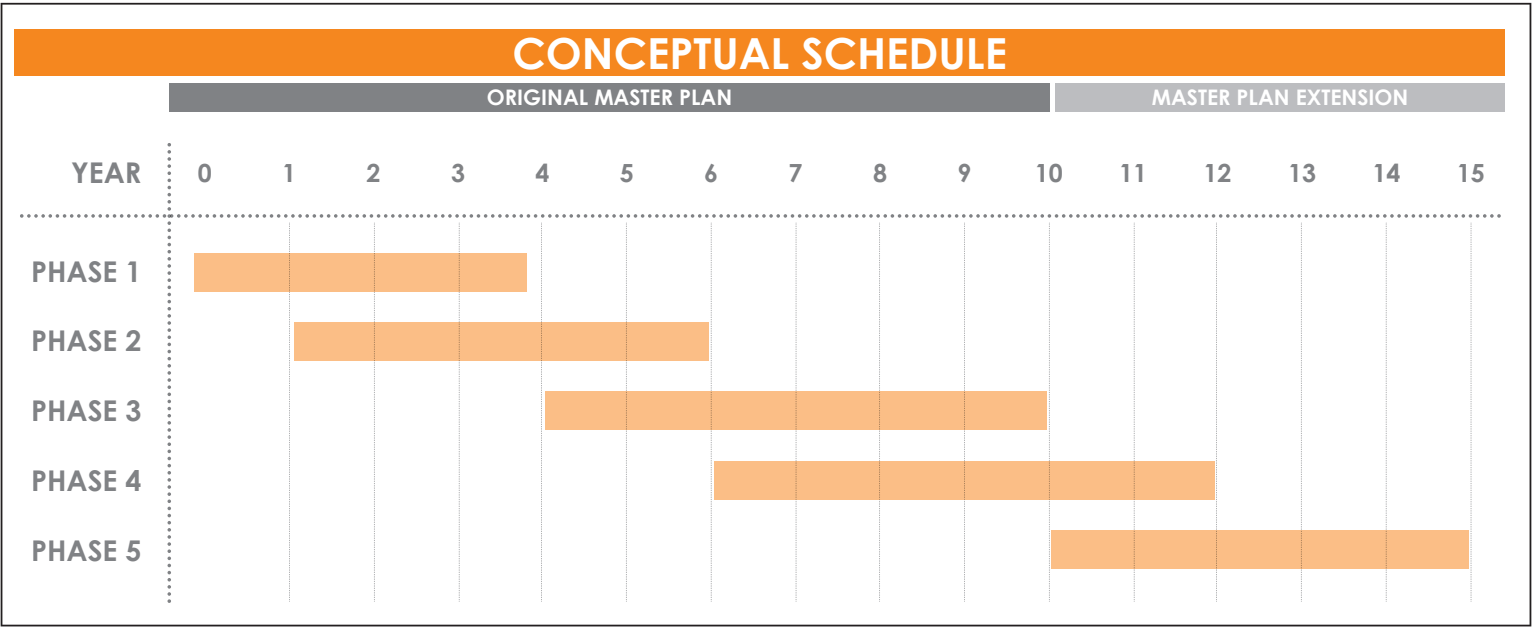
Con-way has publicly placed all of its vacant parcels up for sale (a sales website is available for reference at <http://21nwpx.com/>). Interest from prospective buyers has seen a dramatic increase in the past 12 months. Buyer interest will ultimately determine the pace for acquisition and development, but Con-way expects to sell its properties and enable completion of the overall development within 10-15 years. The graphic below shows the proposed schedule for development of the Master Plan.

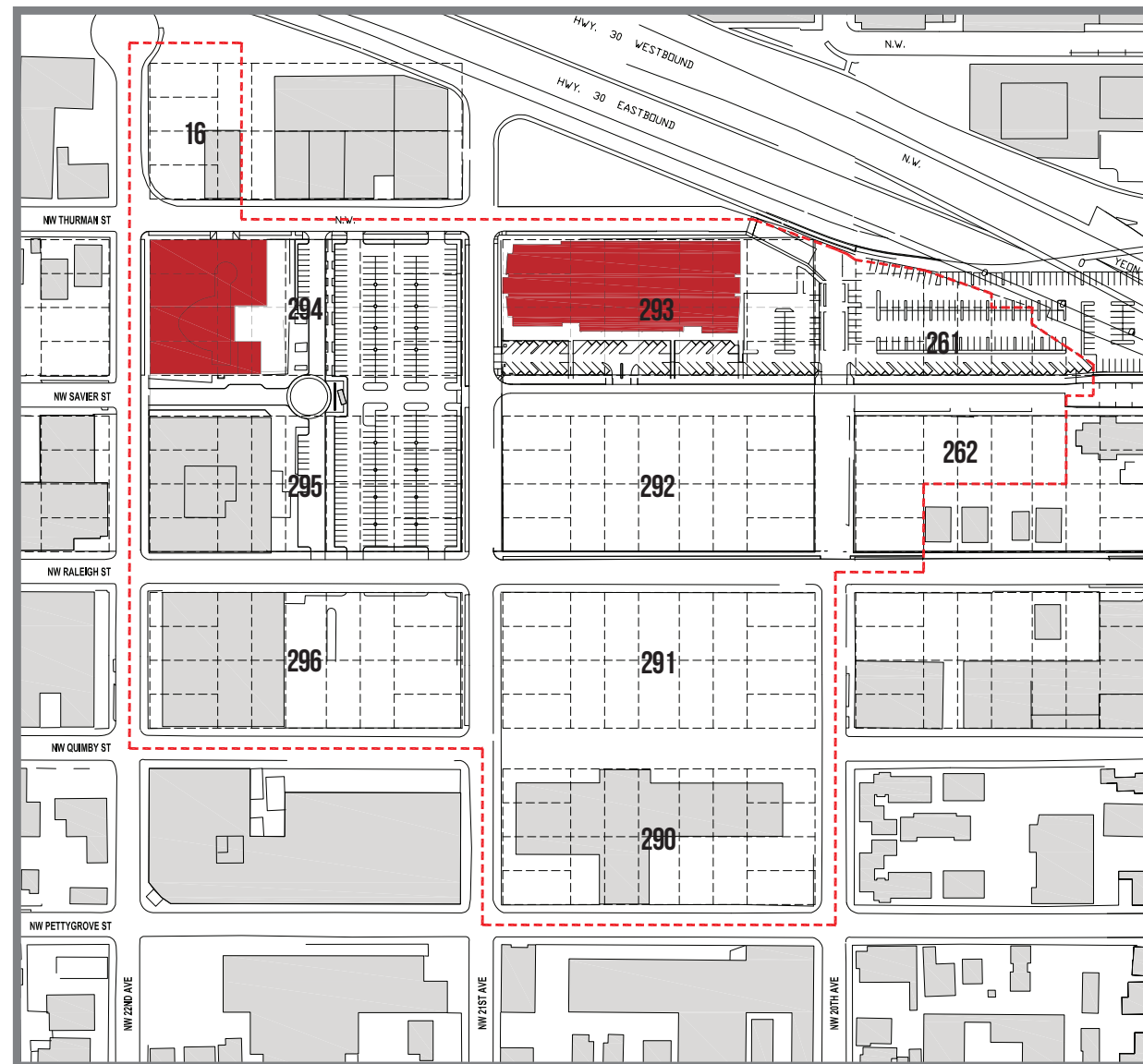
Phasing, while not absolute, is expected to occur over five phases as described on the following pages. The earliest phases of development will occur at or near the southern fringes of the plan boundary and progressively work inward and northward from the boundary. Replacement parking for Con-way drives this phasing strategy, saving those parcels that adjoin the current office facilities until last to accommodate replacement parking for Con-way.

INFRASTRUCTURE

There are several infrastructure pieces being addressed that will require continued discussion, analysis and design. These cannot be completed prior to the completion of the Master Plan application process. However, each of the following are addressed and described to the degree they are known within the Master Plan document:

- Streetcar extension
- Green streets
- On-street parking
- Ecodistrict strategies





EXISTING CONDITIONS

The majority of the land area within the plan boundaries consist of paved surface areas designed for parking and truck maneuvering. Six separate buildings exist on the property, two of which are occupied by Con-way.

BUILDINGS

Con-way will continue to own and occupy the existing office buildings (Adtech I and Adtech II) on parcels 293 and 294, thereby maintaining a large, 1,000-person office user within the heart of the mixed-use redevelopment program, during and beyond the development of the overall project. Parcel 293 accommodates a 250,000 sq. foot¹, five-story plus basement office building completed in 1999. Parcel 294 accommodates a 158,000 sq. foot¹, four-story plus basement office building completed in 1988.

Parcel 295 includes a 148,000 sq. foot¹, three-story plus basement building constructed in 1973. Con-way currently leases portions of the building to other tenants. The future of this building will be determined by the development entity that purchases the building. Con-way does not intend to re-develop the building for its own use or for lease purposes.

Parcel 296 includes a 36,000 sq. foot single-story warehouse building constructed in 1952. The site is currently under contract. The concept involves re-purposing this building as a component of a mixed-use project. Details of this project prospect are included in the Appendix.

Parcel 290 includes a 44,000 sq. foot structure, built in 1967, used as a truck service facility. Portions of the building offer interesting interior volumes, but it is unlikely that the building, or portions of it, can be adaptively re-used in a financially viable manner.

Parcel 16 includes a 9,400 sq. foot single-story commercial building constructed in 1948; the building will likely be demolished if, and when, this parcel gets re-developed.

STREETS

A parcel that would extend NW Quimby Street between NW 20th and 21st Avenues is currently private property, owned by Con-way. The parcel is designated as a local street in the Transportation Element of the City of Portland Comprehensive Plan. This parcel will be sold with adjoining parcels and developed as a special street devoted to bicycle and pedestrian use that also allows for service and emergency vehicles.

NW Saver Street between 21st and 22nd Avenues does not currently exist as a public street but rather as private property. This block will be restored as a public street/right-of-way (ROW) and re-dedicated to the City, in concert with the development of Blocks 294 and 295.

NW Saver Street between 19th and 21st Avenues exists as a public ROW, but is not fully built out in accordance with PBOT street standards. The blocks will be expanded to meet City street standards, in concert with the development of Blocks 261, 262 and 292. The easternmost portion of NW Saver to NW 19th is outside of the Master Plan boundary. ODOT owns the abutting property to the north, and St. Patrick's Church owns the abutting property to the south.

NW 20th between NW Raleigh and Thurman does not currently exist as a fully improved public street. The street will be improved as part of the preferred option to address traffic congestion at the NW 23rd and Vaughn intersection.

NW 20th between NW Upshur and NW Thurman does not currently exist as a public street but as ODOT ROW beneath the freeway ramps. This street is outside the Con-way Master Plan area. The street will be improved as part of one option to address traffic congestion at the NW 23rd and Vaughn intersection.

Con-way expects to burden the pro-rata share of cost for each street improvement to the properties that front or adjoin the ROW in question. They will require, as a part of sales agreements, that streets and related ROW improvements will be made on a block-by-block basis to coincide with each development project. Efforts to establish an LID for all necessary street improvements are being discussed with PBOT, including the segment of NW Saver to NW 19th outside the Master Plan boundary; however no commitments have been made at this time.

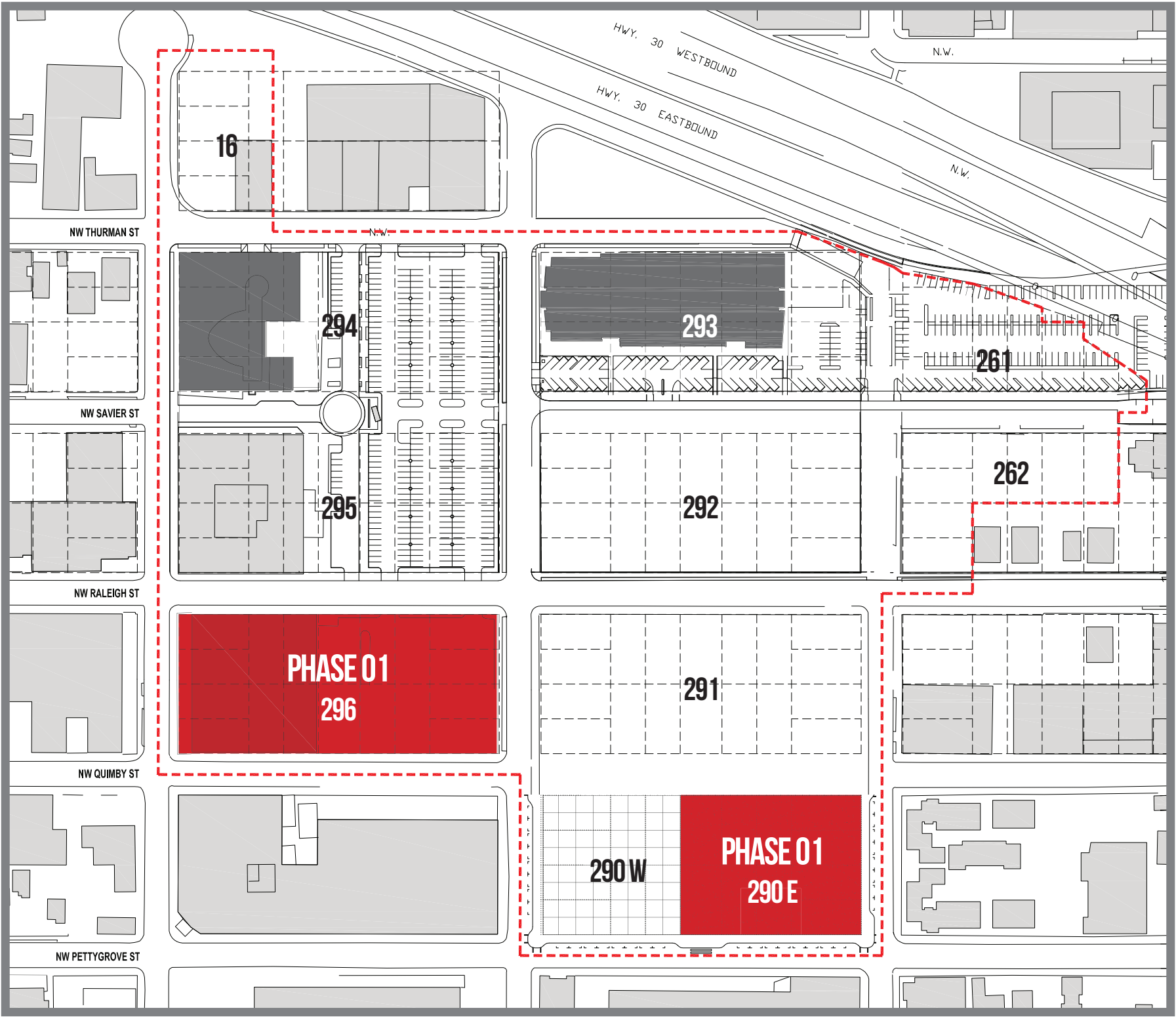
¹Floor areas include the gross square feet of a basement level. See Appendix for floor areas related to FAR.

PHASE 01

Con-way is under contract on the sale of Block 296 to a private development entity. The buyer is proposing to re-use the existing warehouse building and convert it to a grocery store as part of a multi-building mixed-use redevelopment of the parcel. They are poised to proceed with design and construction pending a lease commitment from a grocer and completion of the Master Plan process, thereby the reason for its status as Phase 1.

This Phase 1 project is seen by Con-way and neighbors to be highly catalytic. A much-needed grocer in this location will help stimulate development for retail, office and residential projects as well as help promote the proposed densities envisioned in this Master Plan.

Design and development of the Neighborhood Park on Block 290 will also happen as part of Phase 01.

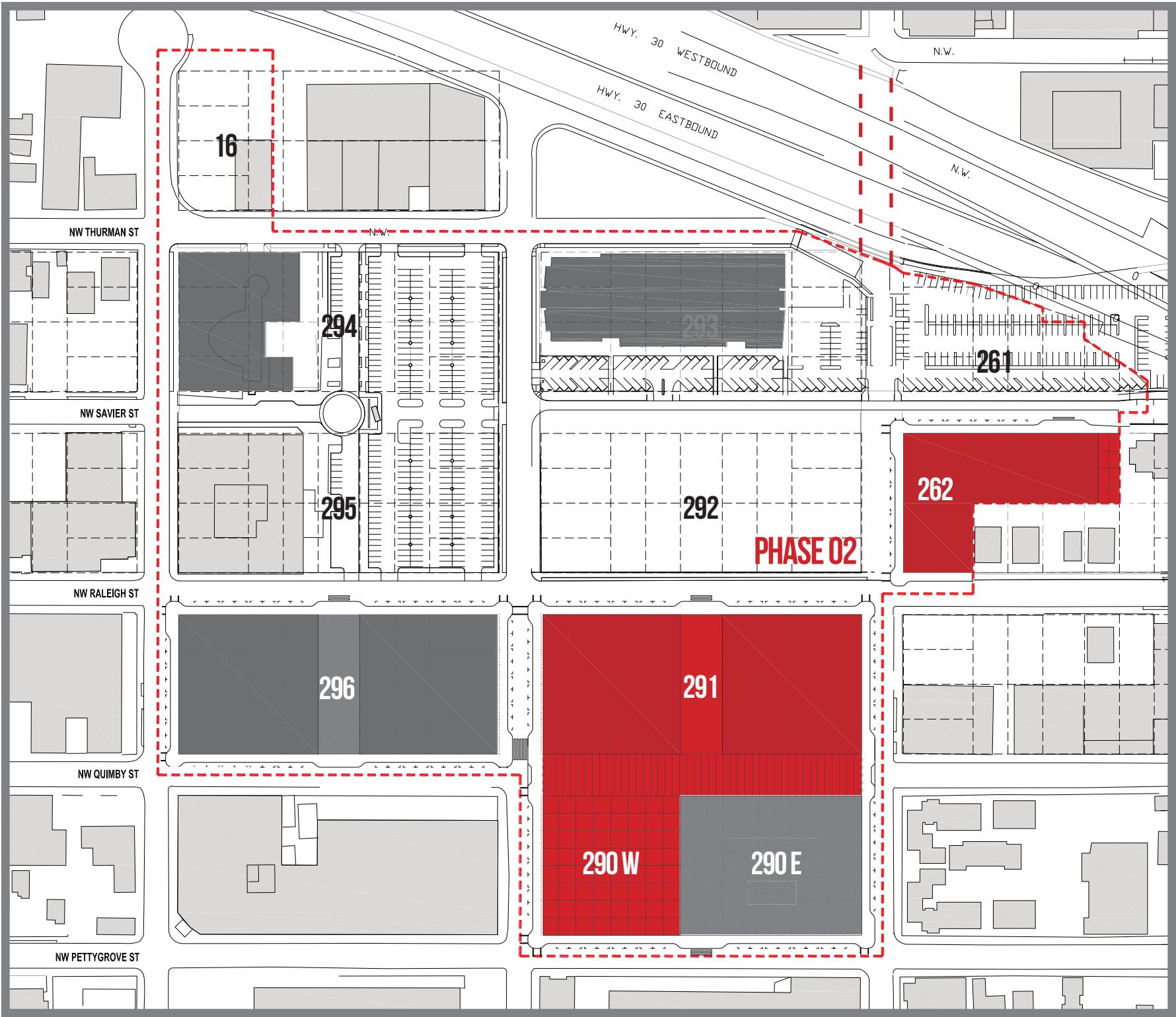


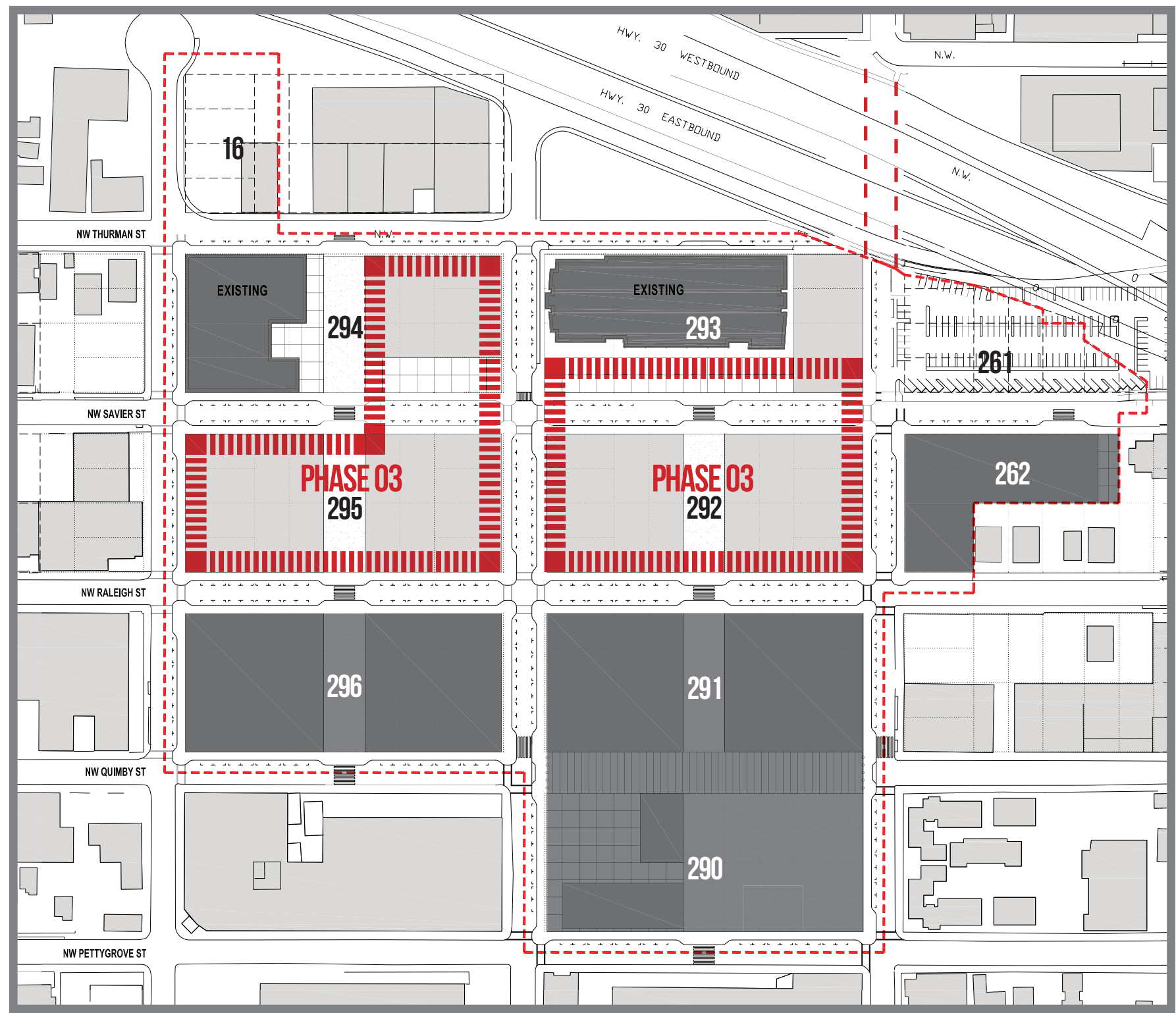
PHASE 02

Phase 02 is envisioned to include any or all of the parcels that are on the southern and eastern perimeter of the Master Plan area, including parcels 290, 291, and 262.

As described for Phase 01, all parties are working hard to enable the design and construction of the Neighborhood park/Open Space as soon as possible. Parcels that abut the Neighborhood park would ideally be designed in parallel with the park design effort, but market conditions will dictate whether this happens.

Interim phase neighborhood square development shall not be subject to Design Review.





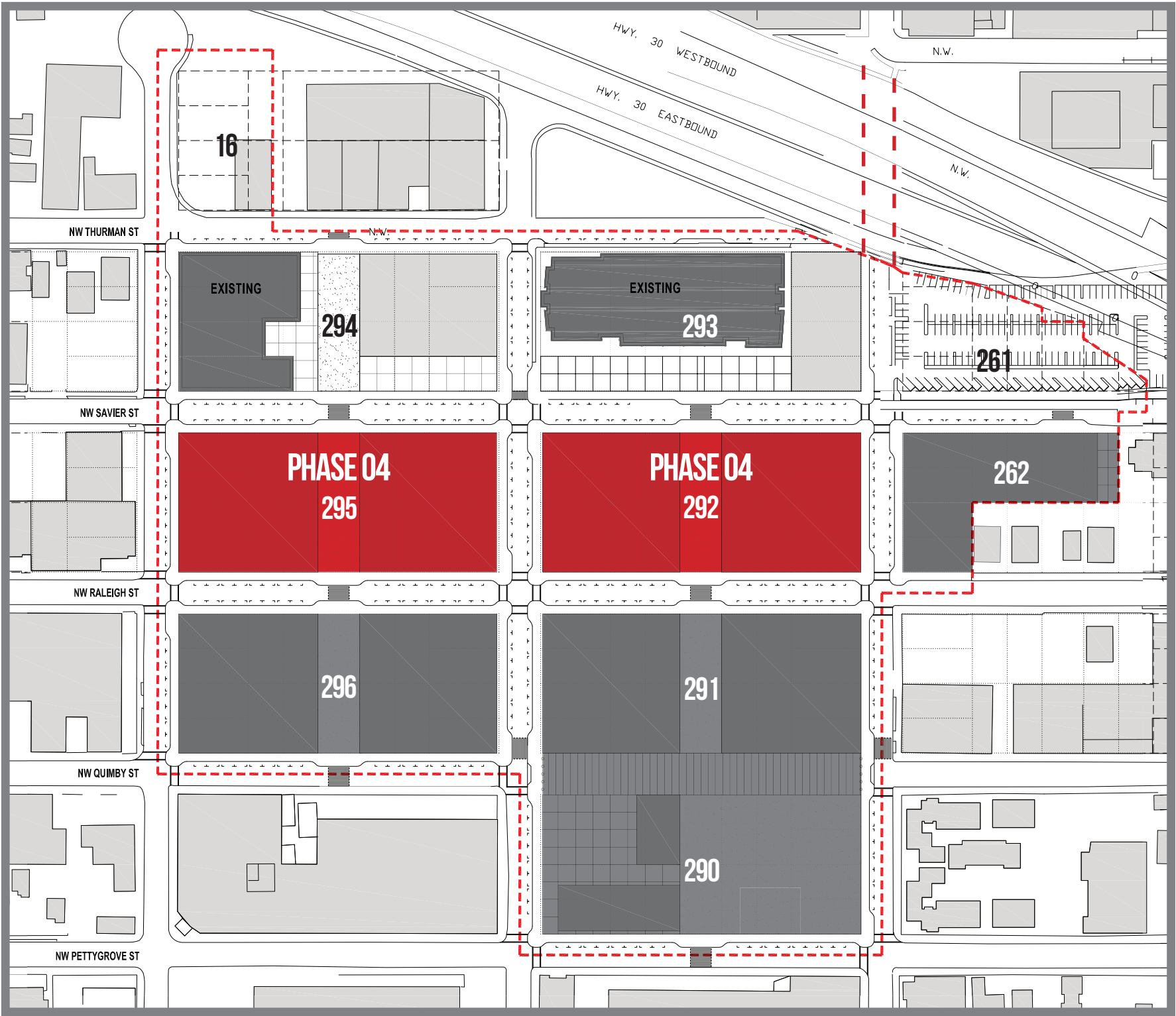
PHASE 03

Phase 3 is anticipated to be the underground parking structure(s) that will ultimately replace the surface parking that presently serves Con-way's staff. Parcels 292, 294 and 295 are all candidates for relocating Con-way's parking needs to underground parking structures. No vertical development can happen on these parcels until these underground structures are planned and designed. Ideally, the underground structures can be designed to underlay any immediate or future vertical development (Phase 4).

Con-way has options for where this parking might be located, including any or all of those lots shown within the red dashed lines as illustrated on the adjoining site plan diagram. These garages will need to be designed to accommodate both Con-way's parking needs plus the needs of the development that occurs above-grade on those same lots. Shared parking strategies will help reduce the overall parking inventory that ultimately needs to be constructed.

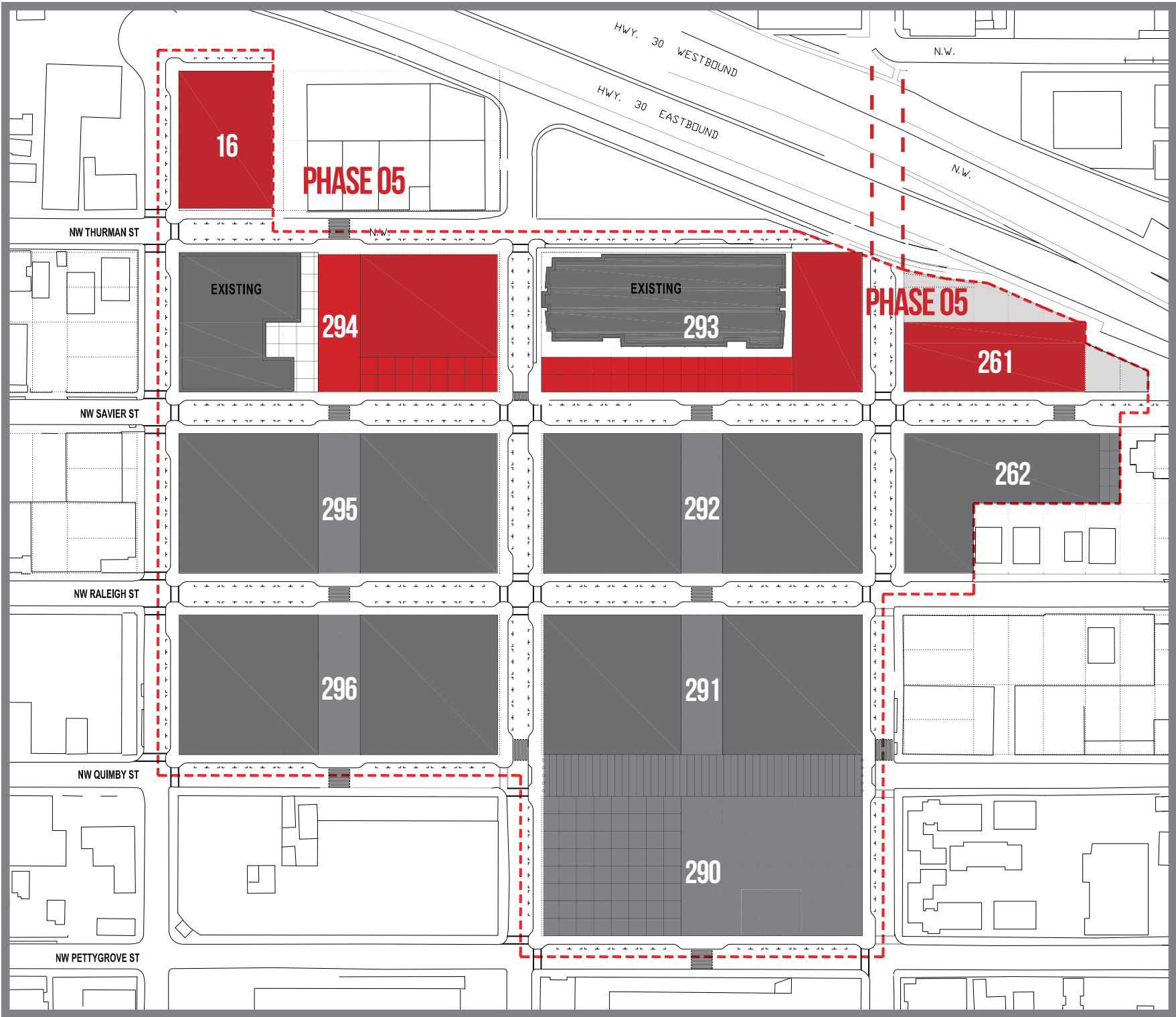
PHASE 04

Phase 4 is anticipated to include all of the structures that are developed above new (Phase 3) parking facilities. Market conditions may cause these buildings to be developed in close coordination with, and immediately following the parking, or deferred to some future date. Temporary provisions for finishing the at-grade "lids" of the parking structures will need to be made if vertical development above is postponed to some later date.



PHASE 05

Phase 5 is anticipated to include all of the infill development at the north end of the Master Plan boundary—properties that adjoin Con-way's existing office buildings. These developments are likely to be office/employment uses, due to the proximity of the freeway to the north. Current market conditions suggest that new office development is likely to occur during the latter stages of the overall development. However, Blocks 16 and 261 could accommodate vertical development at any time, given they are outside of the potential footprint for Con-way's underground parking structures.



SECTION 8

PROCESS

33.562.300.D

8. Process. The Northwest Master Plan must include:

A. *A proposed process and procedure for design review of development, including any development specifically called for in the Northwest Master Plan, if different than procedures for conventional design review; and*

B. *A proposed process and procedure for amendments to an adopted Northwest Master Plan, if different than in 33.562.300.F.*

SECTION 8
PROCESS

If conflicts occur between the NW Master Plan and the City of Portland Zoning Code requirements, the requirements of the NW Master Plan shall prevail.

- The alternative design review procedure provided in Chapter 33.218.015 shall not apply;
- Underlying base zone allowed uses for the property zoned IG1 on Block 16 shall be expanded to also include all allowed uses for the EXd zone;
- Parking standards of Chapter 33.266 shall apply except for instances wherein there is a conflict between Con-way parking rations provided in Section 5 of this Master Plan and Chapter 33.266; and in those cases, the Con-way parking ratios provided in Section 5 shall supersede Chapter 33.266 and shall control.
- Interim phase neighborhood square development shall not be subject to Design Review.
- The proposed process for Design Review in the Northwest Master Plan will be the Design Review process described in Title 33.
- Existing Con-way surface parking lots are not subject to non-conforming upgrades per Chapter 33.258.
- The process for Design Review of the Square will require compliance with the Neighborhood Contact requirements of Chapter 33.700.025
- Amendments to the Master Plan will be processed pursuant to Section 8 (see below).

AMENDMENT PROCESS

The master plan provides for an amendment process in Chapter 33.562.300.F that does not reflect the nature of the proposed master plan in that it requires an amendment for a variety of circumstances not anticipated as part of the Con-way Master Plan. Therefore, the master plan will replace Chapter 33.562.300.F with the following:

Amendments to the Con-way Master Plan.

Amendment Required and Review Procedures. The Con-way Master Plan is a market-driven master plan that provides for a flexible development framework that anticipates a broad variety of potential allowed outcomes. Because there is not an exact and specific development outcome required for the ultimate build-out of the Con-way Master Plan the amendment process will be required for only very significant deviations from the approved Master Plan.

Review Procedures. Amendments to the Con-way Master Plan are reviewed as follows:

1. The following amendments will be processed through a Type III procedure before the Hearings Officer:
 - a. A change that increases the overall density of the entire Con-way master plan area above a 3:1 floor area ratio;
 - b. Changes to the Master Plan boundary;
 - c. Increase in the overall maximum square footage of uses as allowed in Section 5, Standard 2.
2. The following amendments will be processed through a Type III procedure before the Design Commission:
 - a. Removal of dedicated open space; or
 - b. Changes to the Design Standards and Guidelines.
3. If amendments are proposed that include changes to 1 and 2 above, then the Design Commission will make a recommendation regarding any items under 2 above to the Hearings Officer who will make the final decision under a Type III procedure.

Approval Criteria. The approval criteria for an amendment to the Con-way master plan are as follows:

1. Overall. The amendment is consistent with the approved Con-way Master Plan's vision and purpose;
2. Design. The urban design elements provided in the purpose statements of the Design Standards and Guidelines of the approved Con-way master plan continue to be met after the amendment;
3. Transportation. The net new weekday p.m. peak hour trip generation of the overall Master Plan site with the amendment remains less than or equal to 1,535 trips. In the event that the Master Plan site net new weekday p.m. peak hour trip generation exceeds 1,535 trips, a transportation impact study will be required to demonstrate what mitigation measures (if any) will be required of the amendment to satisfy City of Portland and Oregon Department of Transportation operating standards at impacted intersections.

SECTION 9

APPROVAL CRITERIA

33.562.300.D

9. Written Statement. A written statement, describing how all approval criteria for the Northwest Master Plan are met.

SECTION 9:
APPROVAL CRITERIA

The following information responds to the applicable City of Portland Planning and Zoning Code (referred to herein by “Chapter” or “Code”) requirements and is organized by the relevant sections of the Code. If applicable, the Chapter section or portion of the Chapter section is copied in italicized bold and the Applicant’s response to each code section follows the code reference.

Chapter 33.562.300.G Approval Criteria for the Northwest Master Plan

Chapter 33.562.300.G.1 Overall.

The proposed Northwest Master Plan, and development allowed by it, will be consistent with the purpose of the plan district, and the purpose of this section, as well as other applicable zoning code provisions.

The Con-way Master Plan allows a development framework that is consistent with the purpose of the Northwest Plan District and the Northwest Master Plan Section as follows:

The “Purpose” of the Northwest Plan District comes directly from the Code as follows:

Chapter 33.562.010 Purpose. The Northwest plan district implements the Northwest District Plan, providing for an urban level of mixed-use development including commercial, office, housing, and employment. Objectives of the plan district include strengthening the area’s role as a commercial and residential center. The regulations of this chapter:

- **Promote housing and mixed-use development;**
- **Address the area’s parking scarcity while discouraging auto-oriented developments;**
- **Enhance the pedestrian experience;**
- **Encourage a mixed-use environment, with transit supportive levels of development and a concentration of commercial uses, along main streets and the streetcar alignment; and**
- **Minimize conflicts between the mixed-uses of the plan district and the industrial uses of the adjacent Guild’s Lake Industrial Sanctuary.**

The Con-way Master Plan satisfies each of the individual purpose statements and will create a mixed-use community that enhances the existing commercial and residential nature of the neighborhood. The Application satisfies each of the bulleted purpose statements of this Chapter as follows:

- **Promote housing and mixed-use development;**

The Master Plan provides for a mixed-use development consisting of residential housing, commercial office and retail, which are all permitted uses allowed in the retained ExD zoning. Specifically, the plan allows for approximately 1,200 to 1,500 units of residential housing, up to 450,000 square feet of new commercial office, and an additional 150,000 square feet of retail. This mix of development types will promote housing by capping the amount of commercial office and retail allowed – leaving a significant amount of residential to fill-in the remainder of available development opportunities. It further promotes a mix of uses by identifying retail corridors along NW 21st Avenue and NW Thurman Street where ground floor retail uses are mandated. The master plan recognizes and adopts the existing underlying zoning in respect to retail, which currently requires retail along main streets (identified as NW 21st and NW Thurman). This combination of regulations ensures the master plan promotes a mix of uses appropriate for the surrounding neighborhood.

- **Address the area’s parking scarcity while discouraging auto-oriented developments;**

The Applicant recognizes the issues surrounding parking in the greater Northwest neighborhood. To address parking in the Master Plan area, Con-way will utilize a variety of methods to mitigate the impact on parking. First, Con-way is discouraging auto trips (and auto-oriented development) through an aggressive transportation demand management program (“TDM”). The TDM is defined in more detail in Section 6 of the Master Plan. To summarize, the TDM implements a variety of methods, including a reduction in maximum parking ratios over the life of the master plan that will lead to less auto trips. Furthermore, the TDM sets targets for trip reductions that will be monitored and measured so that assurances are provided that the TDM will be successful in reducing automobile traffic in the neighborhood. This will directly impact the effect of the Master Plan development on parking in the neighborhood. Second, the Master Plan will be a mixed-use development that provides access to amenities within the neighborhood which will reduce both the need for an automobile as well as the use of an automobile for people living the plan area. Third, the Master Plan will implement an underground parking strategy as explained in Sections 6 & 7 of the Master Plan. The development and use of underground parking will alleviate the parking scarcity in the greater neighborhood while providing needed parking to the mixed-use Master Plan district. Since underground parking will meet the parking needs of development within the Master Plan, surface parking spaces, which are being increased through street improvements, will be available to visitors, shoppers and others in the vicinity. Finally, the Master Plan itself discourages auto-oriented development through its cap on uses. Most significantly, the caps work to prohibit any “destination” or

“regional” retail development (such as an IKEA or Wal-Mart destined to serve a regional market) through the plan’s cap on retail at 150,000 square feet total, with no single user over 40,000 square feet. Other than providing for a single use supermarket up to 40,000 square feet, the current standards limiting single uses to 20,000 square feet will be retained. Although a large employer could locate in the Master Plan Area, the TDM plan will alleviate the impact on auto usage through the elements of the TDM dealing with reduction in employment trips. With this mix of strategies, Con-way has successfully addressed the area’s parking scarcity and discouraged auto-oriented developments.

- **Enhance the pedestrian experience;**

The Con-way Master Plan puts a premium on an enhanced pedestrian experience through the use of generous sidewalks, high quality design, pedestrian scale development (with a focus on active uses along pedestrian corridors); and a network of parks and open spaces for a pedestrian friendly experience throughout the entire area. Please see Sections 5 and 6 of the Master Plan for imagery and additional standards and guidelines that promote the pedestrian experience. Through these standards and guidelines the Con-way Master Plan provides an enhanced pedestrian experience.

- **Encourage a mixed-use environment, with transit supportive levels of development and a concentration of commercial uses, along main streets and the streetcar alignment; and**

The Master Plan ensures a mixed-use environment through the use of caps on each type of development so that the neighborhood will have residential, commercial office and retail uses. The levels of development allowed are limited by an overall neighborhood floor area ratio (“FAR”) of 3:1. This limitation allows for an intensity of development that can be supported by transit through both Streetcar and Tri-met service. With about 25 percent of the Master Plan area devoted as parks and open spaces, the resulting development sites will achieve an average FAR of approximately 4:1. This level of density is also represented similarly through the larger plan district, which has an FAR cap of anywhere between 1:1 and 6:1 with bonus options. Again similar to the larger plan district, the Master Plan area provides for a concentration of commercial retail uses along NW 21st Avenue, which is a designated main street and transit corridor. Currently no Streetcar alignment exists in the Master Plan area. However, the Applicant is working closely with the Streetcar Board (and serves as a member on that board), and the future Streetcar alignment is anticipated to run along NW 21st Avenue, which will strengthen the main street designation and requirement for concentration of commercial uses along this corridor. Through the use of required mixed development types in combination with the FAR cap on density and required commercial corridor along NW 21st Avenue, the Applicant has satisfied the regulations related to this element of the Code. Please see Sections 5 and 6 of the Master Plan for additional information

on the mixed-uses, FAR cap and commercial corridor.

- **Minimize conflicts between the mixed-uses of the plan district and the industrial uses of the adjacent Guild’s Lake Industrial Sanctuary.**

The Con-way Master Plan area is naturally separated from the Guild’s Lake Industrial Sanctuary through the I-405 state highway, which literally separates the edge of the Con-way property from the edge of the Industrial Sanctuary. To additionally minimize conflicts between the plan district mixed-use style development and the Industrial Sanctuary, the Master Plan utilizes height opportunities along the edge of the Con-way Master Plan area that will further create a barrier between the two uses (see section 5 showing height limitations).

Additionally, the “Purpose” of the Northwest Master Plan comes directly from the Code as follows:

Chapter 33.562.300 Purpose. A Northwest Master Plan will ensure:

- **Pedestrian-oriented, transit-supportive development;**
- **Development that includes a variety of uses, but retains the EX zone focus on employment uses that need a central location;**
- **High-quality design appropriate to an urban setting;**
- **Active uses on the ground floor of buildings along designated transit streets and pedestrian routes;**
- **A street pattern that provides for frequent, convenient pedestrian and vehicle connections and emulates levels of connectivity similar to the adjacent block pattern;**
- **Transportation and parking demand management strategies that decrease reliance on the automobile;**
- **Development that is integrated into the broader urban fabric;**
- **Transitions to adjacent areas with different uses and intensities through use, height and massing of new development, considering historic resources, and the character of the area anticipated through the Northwest district Plan process; and**
- **Consideration of opportunities to provide a park, plaza, or other open space that can be used by those working and living in the neighborhood; and efficient use of land.**

A number of the purpose statements of the Northwest Master Plan are duplicative with the purpose statements of the Northwest Plan District. Because the language in the Northwest Master Plan section predates adoption of the Northwest Plan District many of the objectives are simply the same. However, the applicant will also respond to the purpose statement sections of the Northwest Master Plan purpose and demonstrate how each

purpose statement of the Northwest Master Plan is met as follows:

- ***Pedestrian-oriented, transit-supportive development;***

The master plan area focuses on pedestrian-oriented development that provides expansive pedestrian-accessways; generous sidewalks and enhanced pedestrian realm design guidelines and standards. Additionally, the master plan promotes a variety of multi-modal uses, such as streetcar, bus and bicycle that make the development transit-supportive and friendly to the pedestrian. Please also see Sections 5 & 6 of the master plan for further detailed explanations of these components of the master plan.

- ***Development that includes a variety of uses, but retains the EX zone focus on employment uses that need a central location;***

The uses allowed in the master plan area include commercial office, retail and residential. This mix is ensured through the use of development caps that ensure that it is not possible for only a single type of use to be developed in the master plan area. Additionally, the master plan area allows for almost one million square feet of office (including new office as well as Con-way's existing office use) to allow for substantial employment uses in a central location. The master plan retains the existing EX zoning, and allows for the wide range of permitted uses in the EX zone to apply to a parcel currently zoned ID??.

- ***High-quality design appropriate to an urban setting;***

Section 5 of the master plan explains in great detail a rigorous review and approval process wherein all development in the master plan area must adhere to a set of standards and guidelines that ensure high-quality design. Please see Section 5 as well as the recommended approval of the Design Commission relative to this criterion.

- ***Active uses on the ground floor of buildings along designated transit streets and pedestrian routes;***

Active uses on the ground floor of buildings along designated transit streets and pedestrian routes are required in Section 5 of the master plan and will be met throughout the master plan area.

- ***A street pattern that provides for frequent, convenient pedestrian and vehicle connections and emulates levels of connectivity similar to the adjacent block pattern;***

The adjacent block pattern (200 X 460 foot blocks) will be honored throughout the master plan area with the use of the existing street grid as well as the pedestrian accessways, which break up the north-south connection through the existing superblocks. The resulting pedestrian network (200 X 200 foot blocks) actually improves pedestrian connectivity compared to the adjacent block pattern. Section 6 of the master plan provides detailed analysis of the connectivity for both pedestrian and vehicular traffic provided within the master plan area.

- ***Transportation and parking demand management strategies that decrease reliance on the automobile;***

Again, Section 6 provides a robust transportation demand management plan to be deployed in the master plan area as a component of the master plan. Through these proven TDM strategies combined with multi-modal transit opportunities, the master plan will decrease reliance on the automobile in the master plan area.

- ***Development that is integrated into the broader urban fabric;***

Section 5 details how proposed development will be integrated into the broader urban fabric through the use of design standards and guidelines. The standards and guidelines provide a definitive framework that ensures all development will be integrated into the broader urban fabric through high quality material, superior design and thoughtful transitions between old and new. Please see Section 5 for a more detailed look at the standards and guidelines.

- ***Transitions to adjacent areas with different uses and intensities through use, height and massing of new development, considering historic resources, and the character of the area anticipated through the Northwest District Plan process; and***

Again, through the design review process provided in Section 5, the character of the master plan area is set up to meet these criteria. Specifically, the guidelines and standards ensure that new development will be done in a manner that provides transitions to adjacent areas, is of a height and massing that is in line with adjacent areas and integrates with the surrounding historic resources.

- ***Consideration of opportunities to provide a park, plaza, or other open space that can be used by those working and living in the neighborhood; and efficient use of land.***

Section 5 provides that a neighborhood park and square will be provided with the master plan, along with significant additional publically accessible open space through the use of pedestrian accessways. This open space grid provides an efficient use of land by protecting just over 25% of the Con-way property to publically accessible open space and transferring that density to more efficient urban scale projects available throughout the rest of the site. This is done while still retaining an overall 3:1 FAR which will help to make sure the scale and intensity of the overall development compliments the existing adjacent uses.

Chapter 33.562.300.G.2. Design.

- The urban design elements of the proposed overall scheme and site plan provide a framework for development that will result in an area with an urban development pattern that will be attractive, safe, and pleasant for pedestrians, and is integrated with historic resources, and the character of the nearby area anticipated through the Northwest District Plan process. The urban design elements of the proposed overall scheme and site plan must meet the design guidelines that are in effect for***

the site at the time of application.

The Design Standards and Guidelines proposed for the Master Plan create a robust set of urban design requirements that ensure the Master Plan area will be developed in an attractive, safe, and pleasant manner that provides for enhanced pedestrian opportunities. Additionally, the Design Standards and Guidelines provide for integration with the surrounding historic resources through the use of gracious transitions between the existing uses and new uses. For example, at St. Patrick's Church, the Master Plan allows for a pocket park between St. Patrick's and the new building envelope that will help to both honor the historic value of the church while integrating it into the new development. Additionally, through the use of materials, urban design elements, scale and massing the Design Guidelines develop a pattern of development that will seamlessly integrate into the surrounding neighborhood in a manner that compliments the character of the existing neighborhood. Please see section 5 of the Master Plan for a full discussion of the urban design elements of the Master Plan area. Additionally, the urban design elements of the proposed Master Plan greatly improve upon the existing Community Design Guidelines that are currently in effect for the site, by providing more specific design guidance tailored to the character of the area and unique to this specific place.

b. The proposed design guidelines, standards, and review procedures specified in the Northwest Master Plan must ensure that:

(1) An environment will be created which is attractive, safe, and pleasant for pedestrians, including consideration of such elements as the location and orientation of buildings and main entrances, the design and use of the ground floor of structures, and the location, design and landscaping of parking lots and structures;

Section 5 of the Master Plan demonstrates that each application for development within the Master Plan area will go through a robust design review process as provided by the City's type III review. This includes detailed review and input by neighborhood members, City staff and the Design Commission to ensure that each development satisfies the Design Standards and Guidelines. The Design Standards and Guidelines consider and safeguard the development of attractive, safe and pleasant pedestrian experiences through the use of a blend of standards and guidelines that create an urban design standard for the neighborhood. Section 5 of the master plan provides a detailed discussion of the location and orientation of buildings, main entrances, and design and use of ground floor structures. The the location, design and landscaping of parking lots and structures will be governed by existing City standards. Because the Master Plan allows for market driven development that occurs over time, the review process and Design Standards and Guidelines are the ultimate tool in ensuring that this criterion is met. By ensuring that each development goes through a thorough review process and complies with the Design Standards and Guidelines, the Master Plan provides that the ultimate environment created will satisfy this criterion.

(2) Scale and massing of the development addresses the broader context of the area, including historic resources, and the uses and development anticipated through the Northwest District Plan process, specifically at the edges of the Northwest Master Plan area; and

The scale and massing of the Master Plan is planned in a manner that honors the character of the surrounding neighborhood and stitches into the broader area by creating a network of public and private realm spaces that are both new and integrated into the existing neighborhood. Massing and commercial density is focused along the existing retail corridor on NW 21st, which extends the existing retail area into the Master Plan area. The requirement for active ground floor retail uses along NW 21st ensures that a lively pedestrian environment will carry through the Master Plan area from the existing neighborhood. Maximum height limits provide for height transitions from the NW 21st Main Street to the west, south and east edges of the Master Plan area to closely match the heights allowed on adjacent properties. Please see section 5 of the Master Plan, which provides greater detail on the use of scale and massing to achieve these goals. Additionally, Section 5 outlines a network of open spaces and parks that helps to integrate the broader neighborhood into Master Plan area through pedestrian accessways and public parks. In particular, the open space plan specifically connects public spaces to the east and west of the Master Plan area, creating and integrated network of public realm that helps to incorporate the edges of the Master Plan area into the greater district plan area.

(3) The approach to implementing design review, including the guidelines and standards, will ensure that the quality of design and public process is as good or better than that achieved through conventional design review. The guidelines and standards proposed, including existing guidelines and standards, must be appropriate for the Northwest Master Plan area and the type of development anticipated by the purpose statement of this section.

Design review for the Master Plan area will be implemented through the standard City type III design review process. This process is a well-known process that ensures the quality of design and the level of public process will be very high. The Design Standards and Guidelines that will inform the review process can be found in section 5 and have been created specifically for the Master Plan area. As explained above, these standards and guidelines provide for a well- planned urban design framework that is appropriate for the Master Plan area and meets the purpose of this Chapter.

Please note that the entire set of design guidelines and standards have been thoroughly reviewed through 5 separate public hearings before

the Portland Design Commission. The Design Commission recommends approval of this Section G.2 and finds it meets the design elements of the master plan criteria.

Chapter 33.562.300.G.3. Transportation.

- a. *The Northwest Master Plan must comply with the policies, street classifications, and street designations of the Transportation Element of the Portland Comprehensive Plan;***

Please see Section 6 of the Master Plan for a full review of how the Master Plan complies with the policies, street classifications and street designations of the Transportation Element of the Portland Comprehensive Plan.

- b. *The transportation system is capable of safely supporting the proposed uses in the plan district in addition to the existing and planned uses in the area. Evaluation factors include street capacity and level-of-service in the vicinity of the plan district, on-street parking impacts, access requirements and needs, impacts on transit operations and access to transit, impacts on adjacent streets and on neighborhood livability, and safety for all modes of travel, particularly pedestrians and bicyclists; and***

Kittelson and Associates, Inc. ("KAI") has prepared a Multi-Modal Transportation Impact Study ("TIS") that demonstrates the transportation system can support the proposed uses with mitigation. The TIS evaluates street capacity, level of service in the Master Plan vicinity and the greater plan district area, on-street parking impacts, access requirements and needs, impacts and access to transit, impacts on adjacent streets and neighborhood livability; and multi-modal safety. Please see the complete TIS attached as Appendix E and E.1 as well as Section 6 of the Master Plan for a full analysis of the transportation system and the Master Plan.

- c. *The proposed street plan must provide multi-modal street connections to match the surrounding street grid pattern where feasible.***

The street plan enhances the existing grid pattern in the Master Plan area by restoring multi-modal connections along NW Quimby and NW Savier as well as enhancing existing connections with new sidewalks and street furnishings. Additionally, through the use of mid-block pedestrian accessways the Master Plan also adds new multi-modal bike and pedestrian connections to the Master Plan area. This robust network of multi-modal connections satisfies this criterion. See Section 6 of the Master Plan.

Chapter 33.562.300.G.4. Retail Sales And Service Uses.

Where the Northwest Master Plan proposes Retail Sales and Service uses that are larger than 10,000 square feet per use, the following approval criteria must be met:

- a. *The proposed uses and development will primarily serve those who live and work in the immediate vicinity;***

The Master Plan proposes a maximum of 150,000 square feet of Retail Sales and Service uses to be spread throughout the Master Plan area. This limited amount of Retail Sales and Service and the type of uses that occur in a size less than 20,000 square feet (as generally required in the master plan area) ensures that the proposed uses will primarily serve those who live and work in the immediate vicinity in the following ways:

(i) The new Retail Sales and Service Uses are in a currently underserved area. The north end of the Northwest Plan District is currently a mix of warehouse, limited residential, office and limited retail. It is an underserved retail area for the day-to-day needs of residents and employees. Although the district has significant retail offerings, the type of destination retail that the district is known for does not serve the needs of its residents or employees. Attached as Appendix D is a thorough analysis and review by E.D. Hovee & Company, LLC ("Hovee Memo"), local experts in economic and development services, that demonstrates thoroughly that the existing area surrounding the master plan is underserved. Furthermore, the Hovee Memo identifies that grocery services are a specifically nonexistent retail offering in the northern portion of NW Portland. The memo concludes that the proposed single 40,000 square foot grocery user will serve the immediate vicinity, which is currently in a grocery deficient situation. Additionally, the Hovee Memo examines the type of uses that come about in a size between 10,000 and 20,000 square feet per use. The types of uses identified, such as a pharmacy or hardware store at the type of day-to-day offerings that neighbors and employees in the area are currently lacking. The proposed 150,000 of maximum retail will result in a mix of medium sized retailers that will serve the day-to-day needs of both the new residents and employees in the master plan area, as well as the underserved existing neighbors surrounding the master plan area (see the Hovee Memo for a more detailed explanation)

(ii) The new Retail Sales and Service Uses will serve the new Master Plan development. The Master Plan provides a cap on each allowed category of uses to ensure that a mixed-use district will be developed. That mixed-use district will bring approximately 1,200 to 1,500 new housing units into the Master Plan area and up to 450,000 square feet of new office use. The influx of new housing and employment combined with the existing employment and housing in the district plan area creates a service area for Retail Sales and Service that can support between 129,000 to 201,200 square feet of Retail Sales and Service (see page 4 of the Hovee Memo). This support comes from just the new development expected in the master plan area and does not include the currently underserved surrounding development. The combination of the planned mixed-use district (ensured by the use of development caps) along with the surrounding underserved area more than demonstrates that the proposed Retail Sales and Service Uses will serve the immediate vicinity.

(iii) The size and type of proposed Retail Sales and Service will serve those who live and work in the area. The breakdown of Retail Sales and Service is anticipated to be smaller retail shops along NW 21st Avenue, a small format grocery store not to exceed 40,000 square feet at Block 296

and some limited ancillary retail elsewhere if additional square footage is available within the retail cap. The largest single retail use is capped at 40,000 square feet and a grocery store. The existing standard capping single uses to a maximum of 20,000 square feet is retained. This mix of sizes of Retail Sales and Service uses will serve those that live and work in the immediate vicinity by providing for more conventional day-to-day retail offerings such as a hardware store where a neighbor can pick up a hammer or a can of paint, as explained in the Hovee Memo. Additionally, the cap of 40,000 square feet for a single user ensures that no destination or regional retail will exist in the Master Plan area. Through the use of the caps on Retail, Residential and Office combined with the limited single retail user of 40,000 square feet, the proposed uses and development will primarily serve those who live and work in the immediate vicinity. (See the Hovee Memo).

b. The transportation system is capable of safely supporting the proposed uses and development;

Please see Section 6 of the Master Plan and the KAI TIS in Appendix E and E.1. The TIS specifically analyses the proposed 150,000 square feet of Retail Sales and Service uses and concludes that the transportation system is capable of safely supporting the proposed uses and development.

c. The proposed uses and development will not have significant adverse effects on the area;

The proposed uses will have a positive effect on the area. By ensuring a mixed-use development, the district plan area will be protected from any large concentration of a single use that would become a regional destination. Instead, the limited Retail Sales and Service uses will serve new residents and employees to the Master Plan area, as well as existing employees and residents. Specifically the retail size development allowed in the master plan area speaks directly to an underserved market of day-to-day commercial offerings for the average neighbor or employee who works in the area. The new services will be available within walking distance to many residents and employees will help to alleviate traffic congestion, enhance quality of life and promote a sense of community that without the proposed retail uses would not be achievable.

Additionally, it should be noted that the underlying zoning currently allows single users up to 20,000 square feet; although the master plan criteria requires the applicant to address the effects of single users above 10,000 square feet. The reason for this discrepancy is that the master plan criteria language predates the updated underlying zoning. Therefore, the underlying zoning itself demonstrates that the larger single users of 20,000 inherently do not have a significant adverse effect on the area as they are now anticipated and allowed outright in the plan area. This application focuses on the impact of the 40,000 square foot single use grocery store that is proposed; as well as providing an explanation of the type of user to be anticipated between 10,000 and 20,000 square feet. A grocery use is one of the single most significant types of users that can create a vibrant 20-minute neighborhood. The master plan is strictly limited to one single grocery user up to 40,000 square feet. When reviewed in total – the retail plan for the master plan is limited and specific in nature so that it ensures the proposed retail uses will

not have significant adverse effects on the area. Additionally, page 6 of the Hovee Memo further explains how the retail plan will not have significant adverse effects on the area.

d. The scale and intensity of the proposed use and development is consistent with historic resources, and the character of the area anticipated as a result of the Northwest District Plan process; and

Please see Section 5 above for a full discussion of the scale and intensity of the Master Plan. Specific design standards address maximum building heights, maximum and minimum floor area ratios, limitations on total retail sales and services and office uses, FAR transfer capabilities, neighborhood facilities, required building lines and ground floor retail requirements along NW 21st and the square, active ground floor use standards, parking standards and development standards for the square. All of these standards have been tailored to the Master Plan area in order to achieve the neighborhood character envisioned in the Northwest District Plan process.

Additionally, the Design Guidelines have been crafted to augment the Community Design Guidelines by providing more specificity and addressing specific urban design challenges unique to this area and appropriate for higher density development. The Design Guidelines address human scale at the pedestrian level, building variety and interest, weather protection, building scale and diversity of building form, transitions between public and private realms, and high quality materials and details. Design Guidelines are also included for parks and open spaces so that these spaces are well integrated with adjacent development, are accessible to the public and provide distinctive neighborhood identity.

e. A proposed Retail Sales And Service use or development of larger scale or intensity equally or better meets the purpose of this section.

The over-arching purpose of this section is to create a vibrant mixed-use neighborhood. It is imperative to include high quality retail sales and service uses within the Master Plan area in order to achieve the type of vibrant mixed-use neighborhood aspired to that allows residents and employees to walk to retail offerings to meet their everyday needs. The scale and intensity planned for a single 40,000 square foot grocery user, a potential 2 – 3 users between 10,000-20,000 square feet and a handful of smaller users is specifically designed to meet the needs of a 20-minute neighborhood. Envision a neighborhood with a grocery (40,000); a pharmacy (15,000) a hardware store (20,000); 2-3 restaurants (30,000) and a handful of smaller users such as a dry cleaner, UPS store, gift shop and coffee shop (10,000) and it becomes easier to see how the 150,000 cap allows for a specific type of neighborhood retail that better meets the purpose of this section. The use of 150,000 square feet of retail strategically placed within the Master Plan area better meets the purpose of this section.

APPENDIX

121	A. PROPOSED PROGRAM AREA DETAILED MODEL
122	B. PUBLIC INVOLVEMENT SUMMARY
123	C. ECODISTRICT SUMMARY & STRATEGY MATRIX
124	D. RETAIL NEEDS MEMORANDUM
128	E. MULTI-MODAL TRANSPORTATION IMPACT STUDY (INCLUDED AS A SEPARATE DOCUMENT)
129	E.1 RESPONSE TO TRANSPORTATION REVIEW COMMENTS

APPENDIX A. PROPOSED PROGRAM AREA DETAILED MODEL

AREAS	St. Patrick's Plaza Park Adtech 2 Adtech 1 Grocery																		
	16	261	262	290		291		292		293		294		295		296		TOTAL	
				W	E	W ¹	E	W ¹	E	W ⁵	E ⁵	W ⁴	E	W	E ¹	W	E ¹		
Site Area	26,830	41,735	41,000	50,296	50,296	46,000	46,000	46,000	46,000	72,000	19,981	46,000	46,000	46,000	46,000	46,000	46,000	762,138	
BASE FAR	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	3.00	2.89	3.11	2.66	3.34	3.00	3.00	3.00	3.00	BASE FAR	2.99
BASE AREA	80,490	125,205	123,000	150,887	150,887	138,000	138,000	138,000	138,000	208,300	62,080	122,550	153,450	138,000	138,000	138,000	138,000	BASE AREA	2,280,850
Height Limit	150	150	65	65	65	87 / 75	65	87 / 75	65	-	150	-	150 / 100	65 / 75	75 / 87	65 / 75	75 / 87		
Retail Area	-	-	-	10,000		15,000		25,000	-	-	-	-	-	-	25,000	46,000	25,000	Total Retail	146,000
Housing Area	-	-	123,000	75,000	-	158,444	103,000	188,444	138,000	-	-	-	-	138,000	188,444	92,000	188,444	Total Housing	1,392,775
Office Area	80,490	125,205	-			-	-	-	-	208,300	62,080	122,550	153,450	-	-	-	-	Total Office	752,075
TOTAL AREA	80,490	125,205	123,000	85,000	-	173,444	103,000	213,444	138,000	208,300	62,080	122,550	153,450	138,000	213,444	138,000	213,444	TOTAL AREA	2,290,850

PARKING	PHASE 5		PHASE 2					PHASE 3/4		PHASE 5		PHASE 3/4			PHASE 1			
Housing Units	-	-	112	68	-	144	94	171	125	-	-	-	-	125	171	84	171	Total Units1,266
Retail Parking	-	-	-	25	-	38	-	63	-	-	-	-	-	-	63	115	63	Total Retail Pkg365
Housing Parking	-	-	112	68	-	144	94	171	125	-	-	-	-	125	171	84	171	Total Housing Pkg1,266
Office Parking	121	188	-	-	-	-	-	-	93	-	-	-	230	-	-	-	-	Total Office Pkg632
Conway Parking								208	208					245				Total Conway Pkg662
TOTAL PARKING	121	188	112	93	-	182	94	442	427	-	-	-	230	371	234	199	234	TOTAL PARKING2,924

Assumptions

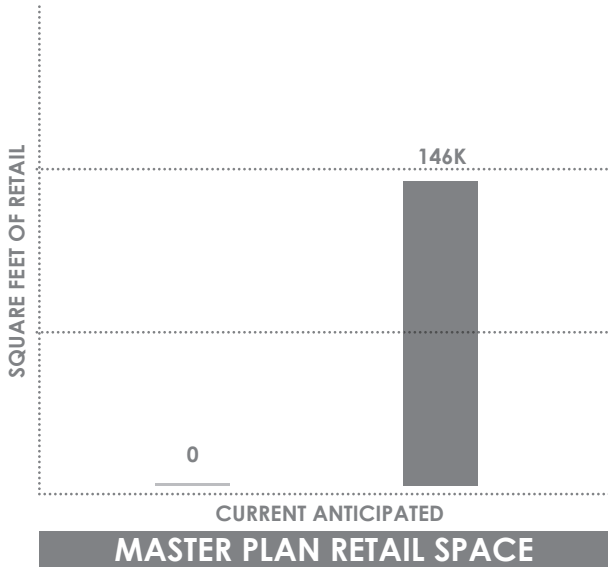
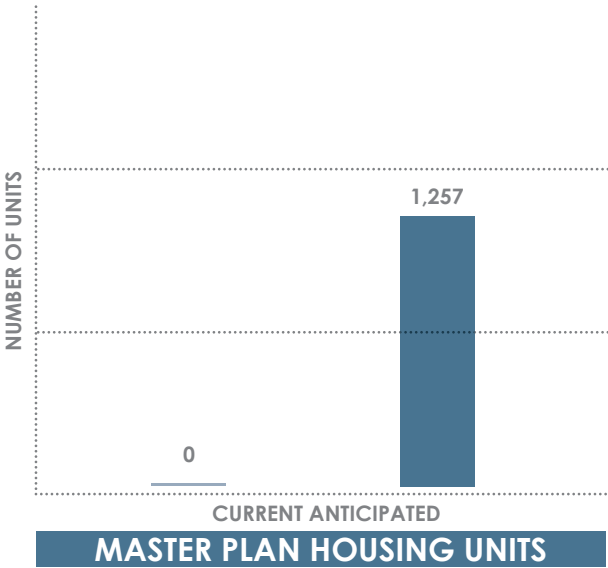
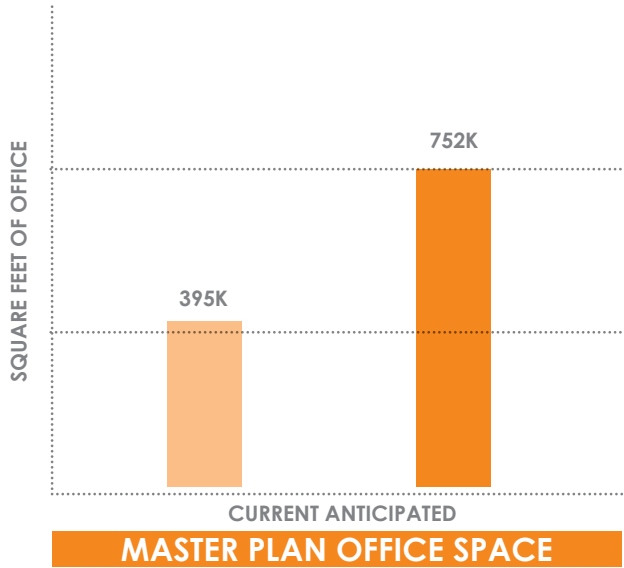
Housing units @ 1100 SF/unit

Phase 1 and 2 @ Phase 1 parking standards

Phase 3-Con-way replacement parking at Phase 2 parking standards

Phase 4 and 5 at Phase 3 parking standards

- Notes**
1. This block abuts a main street (NW 21st Ave) and has required ground-floor retail. 3' is dedicated to the right-of-way.
2. Block 290 areas borrow from the Quimby ROW, such that the total development area does not exceed 2,280,850.
3. Real Base FAR is not quite 3 to1 because a cap is placed on total base area for transportation analysis.
4. This number reflects actual square footage of existing building that is above-grade and counts as FAR. This building has a basement level that does not count as FAR per Title 33 definitions.
5. This number reflects actual square footage of the existing building per Note 4 above. The floor area indicated for Block 293E represents a possible new structure on the available land area directly east of the existing building.





Con-way Master Plan Open House
Occurred at Legacy Good Samaritan Hospital on the evening of March 13, 2012

APPENDIX B.
NW MASTER PLAN PUBLIC INVOLVEMENT

At the outset of the master planning process in March 2011, Con-way invited the Northwest District Association (NWDA) to appoint a leadership team to advise Con-way on all aspects of the master plan of interest to NWDA. The NWDA Board of Directors appointed members of the board who are also members of the Planning and Transportation committees to the leadership team, and that team and Con-way have met virtually weekly for the entire year. The NWDA leadership team also participated in meetings and work sessions with City Bureaus and Commissions, and other interested stakeholders. The Con-way consulting team participated in updates to the NWDA Board and its committees, and also sponsored a public open house prior to the finalization of the master plan application. Two public hearings were held with the Portland Design Commission on the Proposed Master Plan that included extensive public notice and press coverage. Dates for the primary meetings, work sessions and events are summarized below.

CON-WAY/NWDA LEADERSHIP TEAM WORKSESSIONS - 2011

March 17; March 31; April 7; May 11; May 19; June 2; June 16; June 23; July 7; July 21; July 28; August 4; August 11; September 1; September 22; October 4; October 11; October 18; October 25; November 10; November 17; December 8; December 15

CON-WAY/NWDA LEADERSHIP TEAM WORKSESSIONS - 2012

January 5; January 26; February 9; February 21; April 5; April 12

CON-WAY/NWDA LEADERSHIP TEAM WITH CITY BUREAUS & COMMISSIONS

City Pre-Application Conference: August 25, 2011

Portland Bureau of Transportation: September 29, 2011; March 21, 2012; May 21, 2012; June 29, 2012

Portland Bureau of Parks & Recreation: September 21, 2011; January 11, 2012; February 21, 2012; March 8, 2012; June 28, 2012

Portland Design Commission-Design Advice Request: December 1, 2011; February 16, 2012; March 1, 2012

CON-WAY/NWDA LEADERSHIP TEAM WITH INTERESTED PARTIES

St. Patrick's Church officials: November 2, 2011

Portland Public Schools officials: February 10, 2012

AIA Urban Design Panel: April 10, 2012

Legacy Health Services: May 23, 2012

Pearl District Planning Committee: June 19, 2012

Nob Hill Business Association: June 20, 2012

BRIEFINGS FOR NWDA OFFICIALS

NWDA Board of Directors Meetings: September 17, 2011; October 17, 2011; February 27, 2012; March 19, 2012; June 18, 2012; July 16, 2012; August 20, 2012

NWDA Transportation Committee Meeting: March 7, 2012

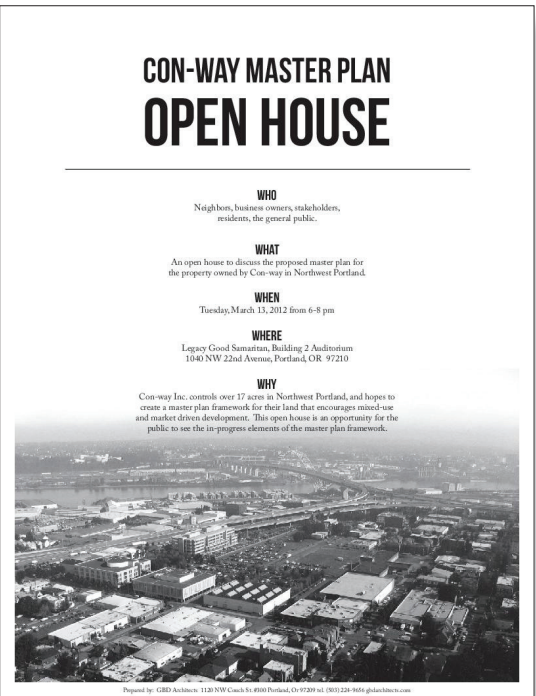
NWDA Planning Committee Meetings: April 5, 2012; April 12, 2012; August 2, 2012; August 16, 2012; August 23, 2012

PUBLIC OPEN HOUSE

March 13, 2012

PORTLAND DESIGN COMMISSION

Public Hearings on the Master Plan: December 1, 2011; January 19, 2012; March 1, 2012; July 19, 2012; August 2, 2012



Open House Invitation

Occurred at Legacy Good Samaritan Hospital on the evening of March 13, 2012

APPENDIX C. ECO DISTRICT SUMMARY & STRATEGIES

CON-WAY MASTER PLAN

ECO DISTRICT STRATEGY MATRIX

UPDATED: SEP 22, 2011

GBD Architects, Incorporated

MP Master Plan

SALE Purchase + Sale Agreements

DRMA District Resource Mgmt Assoc.

? Not sure

In 2009, the City of Portland partnered with Portland Sustainability Institute to create and test the concepts of ecodistricts; or neighborhoods committed to sustainability with the components of empowered people, green buildings and smart infrastructure. The re-development of the Con-way area will use the ecodistrict framework to advance and showcase Portland's efforts reduce carbon emissions and create healthy, connected neighborhoods.

Projects within the 16-acre development area will carefully align and coordinate district stakeholders, private developers, public agencies and utilities to:

- Design and construct high-performance buildings that meet the fossil-fuel free standards and timeline outlined in the Architecture 2030 Challenge.
- Optimize building orientations to increase the use of natural systems to passively heat and cool buildings.
- Create healthy and vibrant open spaces that incorporate or mimic natural systems.
- Incorporate the use of on-site renewable and alternative energy solutions including, but not limited to solar photovoltaic, solar thermal, waste heat recovery, co-generation, and biomass systems.
- Utilize a district-scale heating and cooling system that reduces carbon emissions and saves money over the life of the equipment.
- Support transportation modes that reduce GHG emissions and improve air quality.
- Divert materials from the waste stream through prevention, recycling and reuse throughout the entire design, construction and occupancy phases.
- Manage stormwater in a manner that allows for on-site infiltration, retention, treatment and utilization of rainwater on-site and within buildings where applicable and appropriate.
- Form a neighborhood governing entity that partners with district residents and businesses to manage sustainability initiatives (and other duties as identified).
- Monitor, improve and refine goals over time to understand the full range of social, economic and environmental impacts of the development.

SITE LOCATION + CONTEXT	NOTES
Smart location	Infill site / previously developed
Imperiled species / ecological community	Consult with agencies about existing species
Wetland Conservation	Site has no wetlands
Agricultural land Conservation	Protected soils not impacted
Floodplain avoidance	Confirm that site is not in 100-year flood plain
Preferred Location	Previously developed site
Brownfield redevelopment	Confirm whether site is classified as brownfield
Reduced automobile dependence	Access to transit options (streetcar, bus stops)
Bicycle network	Bike parking, bike boulevard streets, in-building bike storage requirements
Housing and jobs	Create a diverse mix of housing and jobs (office)
Restore habitat or wetlands	Create a new habitat or wetlands for species for 10% of site
Conservation management	Create a 10-year management plan for onsite habitats (by qualified ecologist)

DISTRICT PATTERN + DESIGN	NOTES
Walkable streets	Well designed street sections
Compact development	Medium- to high-density, FAR of 3to1
Connected and open community	Open to the public, no private open space or streets
Diversity of uses	Diverse uses inside and directly adjacent to the district
Mixed-income and diverse communities	15% of units at <50% AMI // 30% of units at <80% AMI
Reduced parking footprint	Limited surface parking throughout district
Street network	Limited dead-ends. Minimal bike and ped access through all dead-end streets
Transit facilities	Well designed transit shelters
Transportation demand management	Establish a district-wide transportation demand management program
Access to public spaces	Large, rectangular parks with good street access
Access to active public spaces	Access to recreation or high-activity open spaces
Universal accessibility	20% of units comply with FHAA requirements
Community outreach and involvement	Work hand-in-hand with community and neighborhood assoc.
Local food production	Allow or require food growth on roofs, side yards, etc. OR buy into CSA
Tree-lined and shaded streets	Meets Portland's street design guidelines
Neighborhood schools	Access to local schools

GREEN BUILDINGS	NOTES
Certified green buildings	Minimum certification standards district-wide
Minimum building energy efficiency	Requirements for minimum energy performance
Minimum building water efficiency	Requirements for minimum water use performance
Construction activity pollution prevention	Erosion and sedimentation control plan
Water efficient landscaping	Require water efficient landscaping throughout district
Existing building re-use	Create bonuses or incentives for the re-purposing of existing buildings
Minimize site disturbance	Various measures, including preservation of existing trees on-site
Stormwater management	Infiltrate all water on-site
Heat island reduction	Eco-roofs, open-grid pavers, shaded hardscape, etc.
Solar orientation	E-W axis of blocks, good for southern orientation of buildings
On-site renewable energy (per building)	Minimum requirements for buildings to generate their own renewable energy

DISTRICT UTILITIES + INFRASTRUCTURE	NOTES
District heating and cooling	Provide district heating and cooling and third-party ownership
Infrastructure energy efficiency	Energy efficient street lights, waste water pumps, traffic lights, etc.
Wastewater management	District wastewater management for reuse
Recycled content in infrastructure	For roads, parking lots, sidewalks, curbs, etc.
Waste management infrastructure	Hazardous waste, district-recycling, home food composting
Light pollution reduction	Of exterior public areas such as parks, streets, and plazas
Green street network	Network of green streets for stormwater management
Bulk purchasing	Bulk purchasing of standardized systems
Smart grid	Digital monitoring throughout district, adaptable to change

FEASIBILITY			IMPLEMENTATION			
EASY	?	HARD	MP	SALE	DRMA	?

x			x			
	x					x
x						
x			x			
		x				x
x			x			
	x		x			
		x	x		x	
	x				x	

FEASIBILITY			IMPLEMENTATION			
EASY	?	HARD	MP	SALE	DRMA	?

x			x			
x			x			
x			x		x	
x	x		x	x	x	
		x				x
x	x		x		x	
x			x			
	x				x	
x			x			
		x				x

FEASIBILITY			IMPLEMENTATION			
EASY	?	HARD	MP	SALE	DRMA	?

x			x	x	x	
x			x	x	x	
x						x
				x	x	x
	x		x			x
	x		x			x
	x				x	x
	x				x	x
		x				x
x				x	x	

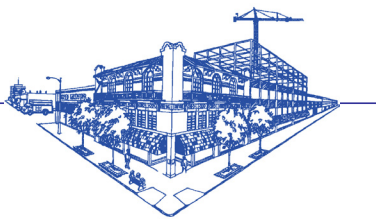
FEASIBILITY			IMPLEMENTATION			
EASY	?	HARD	MP	SALE	DRMA	?

		x	x	x	x	
	x					x
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x						x
	x					x
x			x			
	x					x
	x					x

APPENDIX D. RETAIL NEEDS MEMORANDUM

E. D. Hovee
& Company, LLC

Economic and Development Services



MEMORANDUM

To:	John Spencer, AICP Spencer Consultants / Spencer & Kupper
From:	Eric Hovee & Andrea Logue
Subject:	NW District Local Serving Retail & Store Size Needs
Date:	August 14, 2012

Portland’s Northwest District (NWD) is recognized as the city’s most intensely developed urban neighborhood – a place of diverse housing options, substantial employment, and regionally recognized destination retail. However, the NWD community has been underserved with day-to-day convenience retail – a market *gap* that may widen with added housing development on the Con-way property, in conjunction with housing development occurring elsewhere in the district.

This memorandum is aimed to: a) address the need for added neighborhood serving retail; and b) consider store sizes required to attract competitive retail consistent with urban retail standards.¹ Key topics covered in summary fashion by this memo report are:

NWD Demographics
Neighborhood Retail / Services Needs
Store Size Requirements

NWD DEMOGRAPHICS

Context for this analysis of local serving retail and store size needs is provided by a brief review of NWD demographics. This includes discussion of characteristics of the resident population, district employment, future development potential and implications for local serving retail.

Resident Population. As of 2009, population of the NW District was estimated at close to 9,400 residents. Preliminary estimates for 2010 indicate a current residential population of just over 9,500. Of added note, available data indicates that:²

- The typical NWD household has 1.4 residents (on average) – well below average household size experienced elsewhere in Portland or throughout the metro region.
- While the district is known for a large supply of high value vintage older homes, nearly 90% of residential units district-wide (including apartments) are renter-occupied.

- Median age of population is relatively young at 33.4 years of age, compared with a city-wide median age of over 37 years.
- Education rates are relatively high, and higher proportions of NWD residents walk, bike or take transit to work than is the case citywide or regionally.
- However, at less than \$41,000 per year, median household incomes are only 75% of the citywide and 65% of metro-wide median incomes.

NWD Employment. Even with relatively high residential densities, NWD has an even larger base of employment, with over 13,200 in-district jobs as of 2008. In effect, for every person who lives in the district, there are about 1.4 people who work in the NWD.

Prior to the economic recession, NWD’s job base had increased by 11% over the five-year period extending from 2003-08. With 3,730 jobs, education and health services account for the largest share of total employment, followed by business management / administration, accommodations and food services, and then retail trade.

Development Potential. With a mix of land uses located side-by-side in a compact geographic area close to Downtown, the residential character of the NWD is in many respects distinctive among Portland neighborhoods. Through the recent recession, close-in neighborhoods like the NWD generally have fared better than suburban counterparts not only in Portland but in urban areas throughout the U.S.

Both nationally and in Portland, the “new normal” favors places that appeal to young creatives, foster smaller scale and organic change, green development, and work-live integration consistent with “20-minute neighborhoods.”³ With early phases of economic recovery now being experienced, NWD activity is picking up relatively rapidly as evidenced by five identified residential developments totaling 355 rental units within about one block of the area covered by the Con-way master plan.⁴ Three of the developments have frontage on NW 23rd Avenue, including ground floor retail space.

Properties directly within the boundary covered directly by the Con-way / Proposed Northwest Master Plan envisions future development of 1,200-1,500 housing units together with 450,000 square feet of new office commercial and 150,000 square feet of retail oriented to NW 21st Avenue (including an approximate 30-40,000 square foot small format grocery).

Implications for Local Serving Retail & Service Commercial Needs. Important considerations of district demographics for the Con-way proposed Northwest Master Plan can be summarized as including:

- Need for NWD to become better known as a place where locally generated retail and service commercial needs can be served alongside the destination activity for which the district is currently recognized.
- Recognition of locally generated NWD demand as encompassing both resident and employee needs.
- Opportunity to further the NWD ethic of less auto reliance by offering a full range of services locally, avoiding the need to travel elsewhere for day-to-day convenience needs.

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E-mail: edhovee@edhovee.com

E.D. Hovee & Company, LLC for Spencer Consultants / Spencer & Kupper:
NW District Local Serving Retail & Store Size Needs

NEIGHBORHOOD RETAIL / SERVICE NEEDS

The ability to better meet local resident and employee needs depends on better understanding NWD functions for local and destination retail. This is followed by more specific analysis of the local market demand to support 150,000 square feet of added commercial retail space as proposed with the Con-way Master Plan process.

NWD District Local & Destination Retail. Recent analysis conducted by our firm for the City of Portland has documented the extent to which NWD serves local and non-local retail interests. As noted, locally generated needs can be defined as including those both of the districts resident population and those who work in the district. Non-local supported retail activity is consistent with the district’s reputation for destination retailing – drawing customers from throughout the metro region and beyond due to the reputation of the district, as well as individual stores.

Both in 2009 and 2010, the volume of sales activity experienced by retail and dining businesses in the district was 50% more than what the local resident population alone conceivably could support on its own. Specialty stores generate nearly seven times more volume that what could be supported by the residential population alone. Home furnishings retailers experience volume that is more than four times what could be supported by NWD residents; apparel and dining business volume is nearly double resident-based purchasing power.

Despite these categories of clear destination draw, there are merchandise categories where sales volumes are below what resident-based purchasing power should be able to support. Under-retailed categories include groceries, health / personal care, hardware / garden / building supplies, and auto-related functions ranging from new car dealers to parts supply.

Rather than emphasizing continued NWD destination appeal, the Con-way proposed Northwest Master Plan process is aimed to better serve the needs of local residents and employees. While primarily serving the needs associated with new on-site residential and office space, retail uses could also serve as yet under-retailed needs of the larger NWD residential and employment base.

Local Market for 150,000 Square Feet of Con-way Site Retail Development. Consistent with demographics of the Northwest and Pearl District neighborhoods, it would take between 2,000-2,100 households to support 150,000 square feet of commercial space. At current household sizes of 1.40-1.45 per unit, this equates to residential population of 2,800-3,050.

The number of households to support the planned commercial space is more than the 1,200-1,500 residential units currently anticipated with the Con-way master plan. However, the full complement of locally generated market support can be expected to comprise not only demand from new on-site residents but also Con-way site employment coupled with the ability to better address existing neighborhood retail sales leakage.

Both low and high scenario estimates of potential locally generated market support are estimated as is illustrated by the chart on the following page. This range of estimate is consistent with possible variations in the mix of residential and employment activity that may occur on-site.

Locally Supportable Commercial Retail / Service Space with Con-way Site

Component of Commercial Retail Space	Range of Estimate		
	Low	High	
Added Household Demand			
# of Households	1,200	1,500	Range with Con-way master plan
Supportable Retail/Service Space per Household	70.0	75.0	Varies with extent of economic recovery
Subtotal Retail Space Demand (SF)	84,000	112,500	
Added Office Worker Demand			
Office Space Developed	450,000	450,000	Per Con-way master plan proposal
Building Space per Employee (SF)	400	250	Depends on type & mix of office use
Added Office Employees	1,130	1,800	
Retail Space Developed	150,000	150,000	Per Con-way master plan proposal
Building Space per Employee (SF)	550	400	Depends on mix of retail use
Added Retail Employees	270	380	
Added Office + Retail Employees	1,400	2,180	Direct employment on Con-way site
Supportable Retail/Service Space per Employee	20.7	24.6	Per ICSC Office Space Survey, 2012
Subtotal Retail Space Demand (SF)	29,000	53,600	
Minimum Sales Leakage Recapture			
Subtotal Retail Space Demand (SF)	16,000	35,100	Range with NWD and/or Pearl Districts
Household + Office Worker Demand			
Total Retail Space Supported (SF)	129,000	201,200	
Retail Space Development (SF)	150,000	150,000	
% Potentially Supported by Local Demand Only	86%	134%	

Sources: E. D. Hovee & Company using customized retail spending data from ESRI and ICSC.⁵

As indicated by this chart, on-site residential and employment activity coupled with addressing net sales leakage would support between 129,000 to more than 201,000 square feet of retail commercial space. This represents a range of about 86%-134% of the 150,000 square feet of commercial space currently envisioned with the Master Plan process.

While there is some retail associated with several of the residential projects (currently estimated at 355 residential units) in proximity to the Con-way Master Plan area, there will be added demand generated by these projects not directly met by their developments. Some of this demand undoubtedly will also be served by the Con-way area development, though this potential is not included within the calculations noted above. To the extent that these or other vicinity area projects generate added local retail demand, the above noted estimates are a conservative representation of local-serving retail that Con-way site development could support.

STORE SIZE REQUIREMENTS

This analysis is also specifically aimed to address City of Portland Planning and Zoning Code (PZC) provisions related to criteria for **Retail Sales and Service Uses**. The Northwest Master Plan for the Con-way site proposes that some retail sales and services uses exceed a 10,000 square feet building size threshold, subject to five approval criteria. Three of these criteria (a, c, and e) are of most relevance to this market analysis. Each is considered in turn.

a. The proposed uses and development will primarily serve those who live and work in the immediate vicinity. Retail and related service businesses in the Northwest District currently serve a combination of destination and local customer purposes. While overall retail activity clearly supports more than just needs of the local population, the need for more day-to-day convenience retail is clearly indicated by sales leakage for such resident-based needs as grocery, pharmacy and hardware / garden supply. And stores associated with these uses all typically exceed 10,000 square feet of building area.⁶

A principal use identified by the Master Plan as requiring a store size in excess of 10,000 square feet is what has been described as a **small format grocery** limited to a maximum of 40,000 square feet. Nationally, supermarkets are routinely developed in the range of 45,000 to over 70,000 square feet. Even larger store sizes are common for discount groceries or hyper-market chains such as Fred Meyer and Walmart that combine grocery with other general merchandise departments.

By comparison, smaller format grocery (including specialty grocers) is accustomed to smaller store sizes, generally in the range of 20-40,000 square feet. Store sizes for well-known Portland area based grocers such as New Seasons and Zupans average about 25,000 square feet – but with site-based variations. As examples, the New Seasons at Interstate Avenue and Rosa Parks Way is 32,000 square feet; conversely, Zupans recently opened a 21,000 square foot market in Lake Oswego’s Lake Grove Village Plaza.

QFC has built stores in the 30,000 square foot range. While the typical Whole Foods has been over 50,000 square feet, some smaller format stores are in the 30,000 square foot size range.

While a small format grocery represents the most significant lead anchor anticipated for Con-way site development, other important neighborhood-serving retailers also often require store sizes in excess of 10,000 square feet. Of particular importance for day-to-day convenience needs are pharmacy and hardware stores.

The median store size of the major nationally recognized **pharmacy** operators (such as Walgreens, Rite Aid and CVS) is between 14-15,000 square feet. While some stores are smaller and some are larger, the size range typically reported is from just under 11,000 square feet to 19,000 square feet (though a Walgreens on Cesar Chavez is even larger, at nearly 22,000 square feet). Nationally and in the Portland area, pharmacy businesses have become more consolidated in the hands of these national firms or as part of grocery stores. Virtually all of these operators with stores outside of grocery locations are above the current 10,000 square foot size standard.

A notable countercurrent is represented by Pharmaca which had 23 stores in the western U.S. as of 2011 including one NWD pharmacy – its only store to date in Portland. Store sizes range up to just over 7,000 square feet.⁷ As yet, this non-traditional pharmacy remains the exception to what has been the more dominant trend nationally.

While often independently owned, **hardware stores** are also increasingly associated with either True Value or Ace as providing merchandise purchasing power not available to the individual store owner. The typical size range for an Ace Hardware is 10-20,000 square feet; True Value stores are somewhat smaller, at up to about 15,000 square feet.

In summary, the ability to effectively accommodate the full range of neighborhood-serving retail will require some uses of 10,000 square feet or more. While there are many day-to-day convenience functions that can be met with smaller stores (as with restaurants / cafes and many personal service businesses), critical local-serving businesses of grocery, pharmacy and hardware can be expected to require stores of more than 10,000 square feet. In the case of grocery, the size range for a competitive small format grocery could range up to as much as 40,000 square feet.

As proposed, the Con-way Master Plan is expected to have the effect of discouraging destination oriented or regional retail by placing a 150,000 square foot cap on the total amount of added retail. No single retail use of over 20,000 square feet will be allowed except for a single grocery user of up to 40,000 square feet. Retail and service business tenants will be targeted toward day-to-day convenience services for persons who live and/or work in the immediate vicinity.

At 150,000 square feet, the scale of development is consistent with market supported needs of locally generated resident demand. Auto oriented development as might occur with destination-oriented customers will be further discouraged by reducing maximum parking ratios.

c. The proposed uses and development will not have significant adverse effects on the area. With the limitations on project size and tenant mix as noted, adverse effects are minimized from several perspectives:

- Commercial retail and service businesses will be sized and targeted to primarily serve those who live and work in the immediate vicinity.
- By accommodating resident and employee retail and service needs on-site, travel to other retail areas with resulting traffic impacts will be minimized.
- By not oversizing the amount of on-site commercial space to more than what is clearly supported, there is less likelihood of needing to attract tenants that would unduly compete with existing neighborhood- or destination-oriented businesses.

e. A proposed Retail Sales and Service use or development of larger scale or intensity equally or better meets the purpose of this section. As described, the scale of retail service and sales activity on the site of the Con-way master plan is intended to match to the needs for such services generated on-site and from the immediate vicinity area. A smaller scale development likely would not meet the full range of needs generated by on-site resident and employment-related development. A larger scale development would result in the need for destination-oriented retail activity which is not the purpose of this proposed Master Plan.

E. D. Hovee & Company, LLC appreciates the opportunity to provide this NW District Local Serving Retail & Store Size Needs analysis on behalf of Spencer Consultants and the proposed Con-way Master Plan. We would be happy to respond to questions or comments regarding any aspect of this report.

ENDNOTES

- ¹ Information for this report has been obtained from sources generally deemed to be reliable. However, accuracy of information from third party sources is not guaranteed and subject to change without notice. The observations and findings provided by this report are those of the authors and should not be construed as representing the opinion of any other party prior to their express approval, whether in whole or part.
- ² The NWD study area for this assessment comprises geography generally bounded by Burnside Street on the south, the I-405 freeway on the east, NW Vaughn Street on the north, and NW 25th Avenue / NW Westover Road on the west. Population / demographic information is from ESRI, a national GIS and data firm providing estimates specific to custom neighborhood geographies. Employment information is from geo-coded data as provided by the Oregon Employment Department (OED).
- ³ This analysis is based on research conducted as part of the E. D. Hovee prepared memorandum on *Northwest Parking Plan & District Vitality – Summary Report* dated November 15, 2011.
- ⁴ Five apartment projects are identified as under construction or in the permitting process on sites within a block of Con-way but outside the master plan boundary: 23rd & Lovejoy - 92 units; 23rd & Raleigh -24 units; 23rd & Savier -179 units; on Pettygrove - 40 units; 20th & Quimby - 20 units. Information is as provided by Spencer Consultants, August 6, 2012.
- ⁵ ESRI is a national GIS and demographic data firm. Customized demographic and retail data sets specific to NWD and the Pearl District have been run in conjunction with this analysis. ICSC analysis is that of the International Council of Shopping centers with a 2012 report, *Office-Worker Retail Spending in a Digital Age*.
- ⁶ Information for this store size analysis is drawn from several sources including the Urban Land Institute (ULI) publication *Dollars & Cents of Shopping Centers*, the *Retail Trade Directory* published by Trade Dimensions, and proprietary research of E. D. Hovee & Company, LLC.
- ⁷ Information is as cited by *Chain Drug Review* with the article “Merchant of the Year is Pharmaca’s Panzer,” January 4, 2011.

This *NW District Local Serving Retail & Store Size Needs* assessment has been conducted for Spencer Consultants and Con-way Enterprise Services by the economic and development consulting firm **E. D. Hovee & Company, LLC**. Since 1984, E. D. Hovee has provided market feasibility, economic impact and economic planning services on behalf of private, non-profit and public agency clients – both in the Pacific Northwest and nationally.

On behalf of Con-way Enterprise Services, the firm has previously conducted an analysis of *The Case for Density & Mixed Use* of the subject NW Portland site. The firm has provided economic and development assessments for other mixed use master planned developments in both the Pearl and South Waterfront districts. On behalf of the City of Portland, E. D. Hovee has conducted an *Economic Opportunities Analysis* (EOA) in conjunction with update of the Portland Plan and has recently completed an assessment of *Northwest District Parking Plan & District Vitality*.

APPENDIX E.
MULTI-MODAL TRANSPORTATION IMPACT STUDY
[Included as a separate document.]

APPENDIX E.1. RESPONSE TO TRANSPORTATION REVIEW COMMENTS



MEMORANDUM

Date:	August 8, 2012	Project #: 8424.02
To:	Kurt Krueger, P.E. Portland Bureau of Transportation 1120 SW 5 th Avenue, Room 800 Portland, OR 97204	
Cc:	Philip Selinger, Northwest District Association Avi Tayar, P.E., Oregon Department of Transportation Region 1 Robert Haley, Portland Bureau of Transportation Jill Long, Lane Powell PC John Spencer, Spencer Consultants	
From:	Chris Brehmer, P.E.	
Project:	Con-way Master Plan Multi-Modal Transportation Impact Analysis	
Subject:	Response to July 17, 2012 Transportation Review Comments	

Representatives of Kittelson & Associates, Inc., the Portland Bureau of Transportation (PBOT), the Oregon Department of Transportation (ODOT), and Northwest District Association (NWDA) have met to discuss transportation improvement options to support the proposed Con-Way master plan. These meetings and related discussions have helped to inform and formulate the transportation review process associated with the Con-way property. We appreciate the time and effort each of the respective parties has invested to move the transportation issues forward in a collaborative manner.

This memorandum responds to transportation-related issues identified in the NWDA’s July 17, 2012 letter to the City of Portland’s Bureau of Development Services. Subsequent to the July 17, 2012 NWDA letter, we prepared and submitted documentation of a NW 20th Avenue Extension Alternative Mitigation Strategy dated August 8, 2012. Given the substantial documentation presented in the Alternative Mitigation Strategy Memorandum, the following memorandum only responds to key transportation issues not otherwise addressed in the April 2012 *Con-way Master Plan Multi-Modal Transportation Impact Study* or the August 8, 2012 memorandum.

Comments raised in the July 17, 2012 NWDA letter are summarized below, followed by our response.

General Comment 1) “Trip Generation Assumptions which are consistent with the City’s adopted growth projections, but which the NWDA has demonstrated are significantly lower than actual and planned growth in the study area for 2005 to 2013. This is of particular concern given the already over-burdened operation Level of Service F of the NW 23rd/Vaughn intersection. The proposed

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improvements to 23rd & Vaughn are only a temporary solution and a long-term solution must be defined and funded sooner than the transportation analysis suggests.”

Response: While we appreciate the NWDA’s comment, we respectfully submit that our analysis methodology is consistent with industry practice and City of Portland planning models. We offer the following considerations:

- The April 2012 *Con-way Master Plan Multi-Modal Transportation Impact Study* documents the assumptions used to develop the background growth rate for the study. We validated that one percent annual background growth assumption using the City of Portland’s version of the Metro travel forecasting model.
- The proposed 20th Avenue Extension mitigation strategy adds substantial capacity to the NW 23rd Avenue/NW Vaughn Street intersection, as documented in the August 8, 2012 Mitigation Strategy Memorandum (weekday a.m. peak hour intersection volume-to-capacity ratio of 0.78 and weekday p.m. peak hour volume-to-capacity ratio of 0.81 at site build-out). As a result, the new mitigation package offers additional capacity for other future development and longer-term viability.
- The Con-way Master Plan growth is forecast to add 605 weekday p.m. peak hour trips to the NW 23rd Avenue/NW Vaughn Street intersection in the year 2022 whereas the total entering volume at the intersection is estimated at 3,935 trips. From a proportionality perspective, the Con-way Master Plan growth represents approximately 15.4 percent of the total weekday p.m. peak hour volume projected to be using the intersection (605/3,935 weekday p.m. peak hour trips). Assuming additional background growth at the intersection as suggested in the NWDA comments would increase total entering volumes at the intersection, *reducing* the proportional traffic impact (and thus mitigation responsibility) of the Con-way property.

NW 23RD/VAUGHN INTERSECTION

Comment 1) “We are concerned that the NW 20th option would be underutilized and that Con-way traffic would instead use NW 23rd and create additional congestion as vehicles attempt to turn left across NW 23rd northbound traffic. Signage would have to be well-placed and stated to encourage the preferred out-of-direction route via NW Wilson Street. The parking garage(s) serving the Conway properties might be best oriented to make a convenient connection via NW 20th.”

Response: We concur that provision of clear wayfinding signage is an important element of the mitigation strategy. Success of the proposed NW 20th Avenue mitigation package (and the Conway Master Plan) depends in part on effectively routing site traffic away from making westbound left-turns at the NW 23rd Avenue/NW Vaughn Street intersection. The location and design of required signage will be addressed during the design stage of the project.

Comment 2) “With the opening of NW 20th there is a concern that traffic – especially truck traffic – could unduly infiltrate the residential neighborhood on NW 20th south of Pettygrove. We suggest that some form of traffic diverters be considered for that intersection.”

Response: NW 20th Avenue will provide local truck access to the Con-way property and is expected to carry truck traffic between NW Wilson Street and roughly NW Raleigh Street to serve properties within the Con-Way Master Plan. There is no intent to facilitate additional truck traffic to the area to the south of the Con-way Master Plan area via NW 20th Avenue. Con-way proposes that all intersections along NW 20th Avenue within the Master Plan area include curb extensions for pedestrian crossing ease and traffic management, providing a disincentive for any through truck traffic. No additional traffic diverters are proposed.

From the perspective of a truck driver who might be destined south of NW Pettygrove, the NW 18th Avenue/NW 19th Avenue couplet would offer a faster and more convenient travel path compared to NW 20th Avenue. For example, with the new NW 20th Avenue Extension, southbound truck traffic will be able circulate through the jughandle, turn left at NW Thurman Street Eastbound, travel one block east (on NW Thurman Street Eastbound), and then turn right onto NW 19th Street. Travel north/south on NW 19th Street will then be faster due to fewer north-south stop controlled intersections as compared to NW 20th Avenue.

Comment 3) “While use of the eastbound “frontage” road between NW 23rd and NW 19th would be reduced with the diversion of former U-turns, we are concerned that vacating the frontage road between NW 21st and NW 20th might create undue traffic congestion at NW 21st and Thurman Street. We would like the excavation of that section of frontage road to be considered to address the grade inconsistency at the revised NW 20th/Thurman intersection.”

Response: The August 8, 2012 Alternative Mitigation Strategy Memorandum presents two options regarding the eastbound “frontage” road, one of which is the scenario of lowering and retaining the existing roadway as suggested in the NWDA comments. We believe the NW 21st Street/NW Thurman Street intersection would operate acceptably under either alternative, though retaining the existing NW Thurman Street Eastbound (frontage road) would result in fewer southbound left-turn movements at the NW 21st Avenue/NW Thurman Street intersection. Con-way is prepared to support either option and our understanding is that PBOT is leaning towards retaining use of the existing “frontage” road alignment through implementation of a revised grade.

Comment 4) “The double right turn from northbound NW 23rd to the freeway ramp poses a potential hazard for pedestrians using that crossing. Right turn on red would need to be clearly prohibited to address that hazard. The existing right turn on red prohibition is constantly violated.”

Response: Subject to ODOT and PBOT approval, the proposed intersection improvements could readily accommodate additional signing to prohibit right-turns on red as a new traffic signal mast arm and other related signing, striping, and equipment will be provided.

NW 23RD/RALEIGH INTERSECTION

Comment 1) “The reconfiguration (removal of an existing curb extension) at the NW 23rd/Raleigh intersection and the addition of a traffic signal make this a key portal for the Con-way development. The Multi-Modal Transportation Impact Study calls out some specific provisions for this intersection. The NWDA would like to see a conceptual layout of that intersection to be assured that the various needs and modes of travel can be accommodated – including pedestrians, bikes and streetcar. We are reluctant to see active modes and the quality of the streetscape compromised by the dedicated right turn lane proposed for westbound Raleigh at that intersection. We support the cited measures to block the infiltration of traffic from the Con-way master plan area into the residential neighborhood via NW Raleigh at the intersection.”

Response: The April 2012 *Con-way Master Plan Multi-Modal Transportation Impact Study* recommends signalization of the NW 23rd Avenue/NW Raleigh Street intersection in conjunction with provision of a westbound right-turn lane. The right-turn lane was recommended to minimize queuing on the westbound intersection approach but would require removal of on-street parking and loss of the existing curb extension in the northeast quadrant of the intersection.

After further discussion and review, we concur that preserving the existing pedestrian curb extension and on-street parking exceed the benefits of constructing a westbound right-turn lane. From an analysis perspective, Appendix I of the April 2012 *Con-way Master Plan Multi-Modal Transportation Impact Study* contains the Year 2022 Mitigated Total Traffic Operations Worksheets. The worksheets include an operational analysis of the NW 23rd Avenue/NW Raleigh Street intersection assuming signalization with the existing intersection approach geometry (e.g., without a westbound right-turn lane). The analysis demonstrates that the intersection operates at an acceptable Level of Service “B” during the weekday a.m. peak hour and Level of Service “C” during the weekday p.m. peak hour with a single lane approach westbound.

TRIGGERS

Comment 1) “The Master Plan does not identify triggers for any of the traffic mitigation strategies – most notably the NW 23rd and Vaughn intersection needs and reconfiguration of the NW 23rd/Raleigh intersection. Individual developers are not going to assume this responsibility. Triggers should be expressed in housing units constructed and square feet of new office and retail space. Financing sources, such as LID and SDC funds, need to be identified for these changes and tied to the adopted triggers. Given the poor Level of Service F performance of the intersection today, the proposed mitigation of NW 23rd & Vaughn should be complete before any new buildings are occupied in the masterplan area.

Response: Mitigation triggers are being drafted by PBOT staff and will be included in the staff recommendations.

TRANSIT

Comment) Transit Service: The Conway Master Plan Multi-Modal Transportation Impact Study is out of touch with transit plans for the neighborhood. Line 17th service on NW 21st might better be referred to generically, as it will be re-designated as Line 77 in September. The neighborhood has lobbied for transit service on the NW 18th/19th couplet, which is not addressed in the report. The benefit and limitation of the streetcar extension towards a mode-split shift away from single occupant cars is not addressed.

Response: We accept the clarification provided by NWDA related to the Line 17 changes in the Tri-Met service to the area.

The April 2012 *Con-way Master Plan Multi-Modal Transportation Impact Study* and subsequent analysis have made no attempt to assume further transit service to the area, though such service should reduce single occupant vehicle trips to the Master Plan site and thus would be of benefit to the project and the area.

Con-way supports efforts to provide additional transit service to the area and has developed the transportation plan in a manner that does not preclude a potential future streetcar extension.

NEXT STEPS

We trust this memorandum adequately responds to the NWDA comments listed. The remainder of the comments raised in their July 17, 2012 letter will be addressed separately by Con-way. If you have any questions, please contact us at 503-228-5230.