



100 NW 20TH PLACE, PORTLAND, OR 97209

DESIGN REVIEW APPLICATION - OCTOBER 18TH, 2012

PROJECT SUMMARY

APPLICANT: KROGER NORTHWEST

CONTACT: ADAM SCHATZ, PROJECT MANAGER
3800 SE 22ND AVENUE
PORTLAND, OR 97202

APPLICANTS REPRESENTATIVE: GROUP MACKENZIE

CONTACT: RYAN SCHERA, PLANNER
1515 SE WATER AVENUE, SUITE 100
PORTLAND, OR 97214

PROJECT TEAM: GROUP MACKENZIE
TERRY KRAUSE – ARCHITECT
RYAN SCHERA – PLANNER
BOB FRENTRESS – CIVIL ENGINEER
BRENT AHREND – TRAFFIC ENGINEER
ROBIN LAUGHLIN – LANDSCAPE ARCHITECT

SITE ADDRESS: 100 NW 20TH PLACE
PORTLAND, OR 97209

PROPERTY ID: R198660
SITE SIZE: 99,009 SF (2.27 ACRES)
ZONING: (CXd) CENTRAL COMMERCIAL, DESIGN OVERLAY
PLAN DISTRICT: CENTRAL CITY (GOOSE HOLLOW SUBDISTRICT)

NEIGHBORHOOD: NORTHWEST DISTRICT NEIGHBORHOOD ASSOCIATION

EARLY ASSISTENCE MEETING: NOVEMBER 24, 2010, EA 10-192008

PRE-APPLICATION CONFERENCE: May 10, 2011, EA 11-112104 PC

DESIGN ADVICE REQUEST: JULY 7, 2011 & AUGUST 4, 2011, EA 11-112110 DA

NEIGHBORHOOD MEETINGS: NORTHWEST DISTRICT & GOOSE HOLLOW NEIGHBORHOOD ASSOCIATIONS
FEBRUARY 3 2011, FEBRUARY 10 2011, MAY 10 2012, MAY 17 2012, JUNE 6 2012



APPLICATION REQUEST SUMMARY

The applicant requests Type III Design Review approval for a major renovation and expansion to the existing Fred Meyer store. The project consists of the demolition of structured parking (two levels) and the construction of a 2-4 level store expansion comprised of tenant space on the ground level and community rooms/offices on the upper levels. In addition, the applicant also requests modification approval to the following standards:

Modification 1: 33.510.220.B and 33.130.230.B.2 and 3 - Ground Floor Windows
The windows along the east elevation are proposed to be less than 50 percent of the length and 25 percent of the wall area at the ground level.

Modification 2: 33.510.215.D.2 - Required Building Lines
The colonnade design element along West Burnside is proposed to project into the special building line.

Modification 3: 32.32.020.A - General Standards and Sign Features
To further the thematic style of the signage for the store, each of the blade signs are proposed to be larger than 30 SF.

Modification 4: 33.266.220.2.A.b - Short-term Bicycle Parking Location
Due to the store being built up to the property line it is not possible to provide all of the required short-term bicycle parking on site and in a way that complies with all of the bicycle location standards. Therefore, the short-term bicycle parking is proposed to be located off site and within the pedestrian corridor. However, bicycle parking is proposed in the right-of-way along NW 20th Ave, W Burnside and NW 20th Place near the store entrances.



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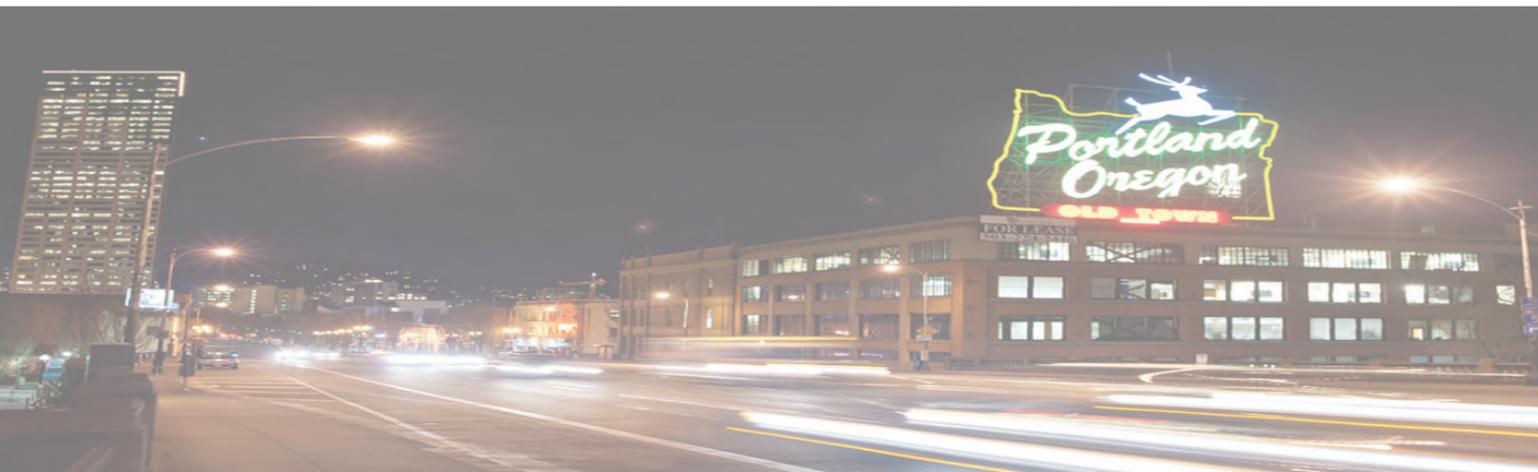
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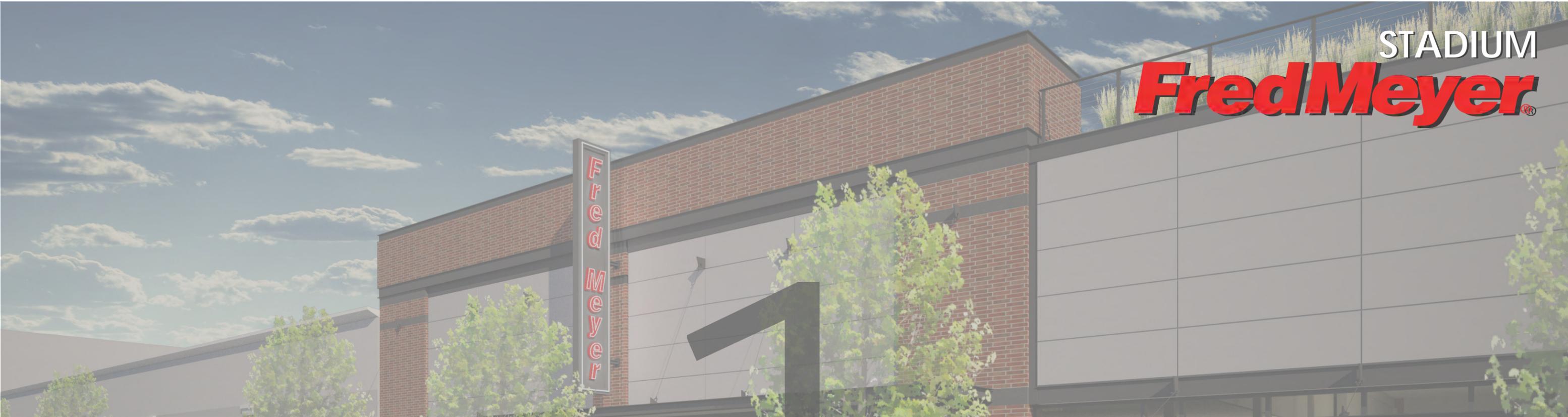
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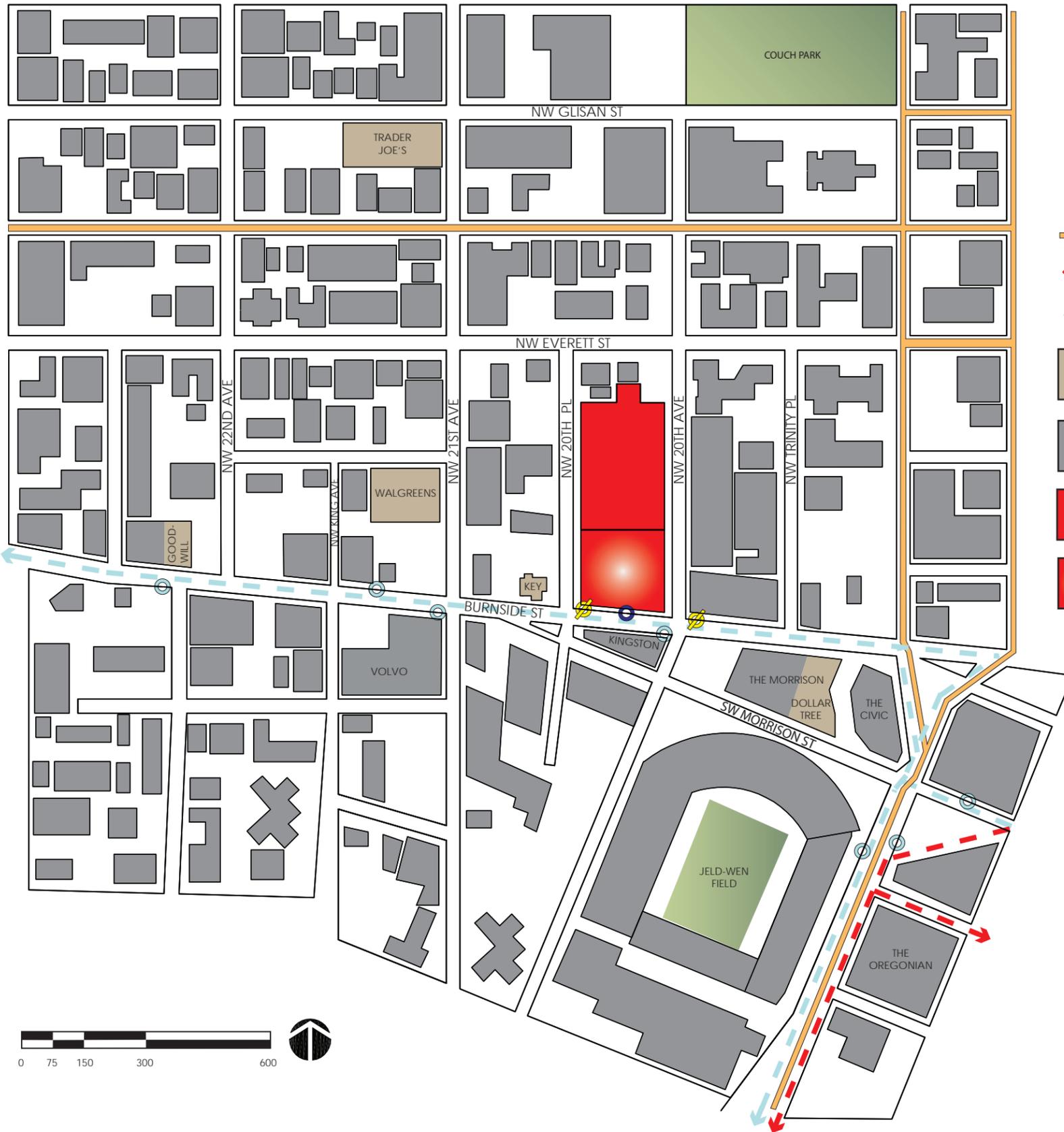
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PROJECT OVERVIEW

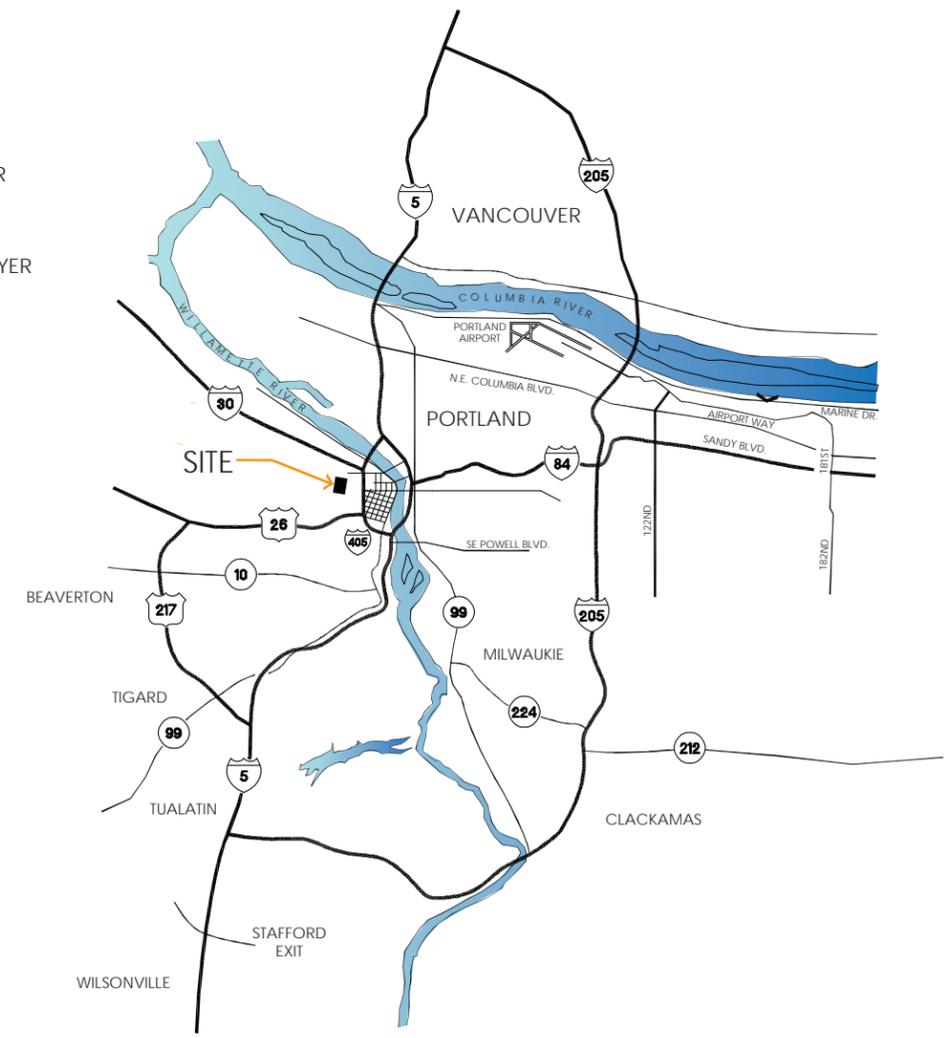


- EXISTING BUS STOP
- BUS STOP TO BE RELOCATED
- NEW CONSOLIDATED BUS STOP
- BICYCLE ROUTE
- LIGHT RAIL
- BUS ROUTE
- RETAIL FOR ADJACENT NEIGHBORHOODS
- EXISTING BUILDINGS
- EXISTING FRED MEYER
- PROPOSED FRED MEYER EXPANSION

SITE DESCRIPTION

The subject site is the existing Fred Meyer store located at the southwest corner of NW 20th Place and West Burnside Street. The site is zoned Central Commercial with a design review overlay (CXd – Goose Hollow Subdistrict), and is located within the Central City Plan District. The site is located in the Northwest District neighborhood and is bounded by residential properties to the north, West Burnside Street to the south, NW 20th Avenue to the east, and NW 20th Place to the west.

Surrounding Uses:
 North: Apartments/Multiplexes
 East: Apartments/Warehouse/Auto Repair
 South: Restaurants/Apartments/Jeld Wen Field
 West: Bank/Pharmacy/Apartments/Parking Lots



PROPOSAL

Fred Meyer is proposing a major renovation and expansion of its existing retail store just north of West Burnside Street between NW 20th Avenue and NW 20th Place. The planned expansion will occur to the south of the existing store towards West Burnside Street. This will involve demolition of the existing sloped plate structured parking decks and the vacant Hollywood Video building on the southwest corner of the site, as well as elimination of the site access to parking from West Burnside Street and NW 20th Place. The below-grade parking will be expanded under the new store expansion, with the existing two access/egress driveways remaining along NW 20th Avenue. On the ground level, the expansion area will consist of the existing store entry along NW 20th Place relocating to a mid-block location, a new store entry adjacent to the corner of West Burnside Street and NW 20th Place, and new tenant spaces - suitable for a variety of uses including office or retail, at the SE corner of West Burnside and NW 20th Avenue.

Upper levels are planned primarily for employee functions and offices as well as a multi-use community room. The roof of the new expansion will be a living green roof, a model of sustainable design that will serve many environmental functions including lowering urban heat gain, stormwater management, overall reduction in carbon footprint, and a visual oasis from adjacent structures. The second floor community room will have windows and terraces looking out to the green roof. In addition to the green roof on the new expansion, skylights will be incorporated into the new and existing roof to the extent feasible to maximize day lighting, further reducing the stores on-going energy needs.

Along West Burnside the store expansion consists of a continuous colonnade with balconies and canopies that protrude over a portion of the pedestrian space. Beneath the colonnade, a terrace consisting of ramps and stairways circumvent the grade change along West Burnside Street and provides access to the punctuated store and tenant entrances, as well as outdoor amenities, such as a new mid-block bus stop and outdoor seating area. These design elements also provide views of downtown, Jeld Wen Field, the West Hills and the West Burnside corridor from multiple vantage points. A variety of materials are proposed, including brick, metal panel systems and wire guardrails, steel canopies, aluminum storefront window systems, and scored concrete walkways. These materials are to be integrated into the existing store's façades in order to create a cohesive relationship between the old and new segments.



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DEVELOPMENT STANDARDS AND DESIGN GUIDELINES

Standard	CX Zone	CC Plan District	Proposed
Primary Uses			
Retail Sales and Service Use	Allowed Outright (33.130.100)	---	Existing Retail Bldg: 7,777 SF Store: 61,840 SF Total: 69,617 SF
Office			Proposed Lower Level: 11,169 SF Sales Floor: 92,448 SF Mezzanine: 5,740 SF Office Floor: 8,594 SF Total: 117,951 SF Net Change: 48,334 SF
Lot Size	No required minimum lot dimension (33.130.200)	---	99,009 SF with greater than 10' front lot line (existing)
Maximum FAR	4 to 1 (33.130.205)	4 to 1 (without bonuses)	1.15 to 1
Maximum Height	75 feet (33.130.210)	75 feet	55'-3" (top of parapet).
Minimum Building Setbacks	None (33.130.215)	10 feet along W. Burnside Special Building Line (33.510.215.C)	Building North, Rear: 6 feet West, Side: 0 feet East, Side: 0 feet South, Front: 10 feet Colonnade South, Front: 5 inches
Maximum Building Setbacks	10 feet along a Transit Street or Pedestrian District (33.130.215)	---	Building South, Front: 10 feet
Building Coverage	No limit (33.130.220)	---	98.0%
Minimum Landscaping Area	None (33.130.225)	---	Existing: 1,550 SF Planters: 705 SF Total: 2,255 SF

Standard	CX Zone	CC Plan District	Proposed
Ground Windows	Windows must be at least 50% of length and 25% of ground level wall area. Include all exterior wall areas up to 9' above finished grade. (33.130.230)	Windows must be at least 50% of length and 25% of ground level wall area. Include all exterior wall areas up to 9' above finished grade. (33.130.230)	Window Area South: 823 SF (56%) West: 2,110 SF (54%) East: 204 SF (4%) Window Length South: 136 LF (68%) West: 226 LF (50%) East: 28.5 LF (6%)
Screening Garbage and Recycling Collection Areas	Exterior areas must be screened from street and any adjacent properties to L2 or F2 Standards. (33.130.235.B)	---	Garbage area will be located interior of the building on the lower floor level.
Screening Mechanical Equipment	Equipment on the ground must be screened from the street and any abutting residential zones, complying with L3 or F2 Standards. Equipment on roofs must be screened if the equipment is within 50' of an R Zone. (33.130.235.C)	---	Mechanical equipment will be located on the rooftop and inside the building. None of the rooftop mechanical units are located within 50' of an R Zone.
Pedestrian Standards Connection – Site with more than one street frontage	Straight line connection between one main entrance of each building and the adjacent street. Additional connection required between each of the other streets and a pedestrian entrance, if 50% of a street facing façade is within 10' – no connection is required to that street. (33.130.240)	---	Pedestrians will access the main entrance to the building from the sidewalk of NW 20 th Place or the walkway connection through the lower level from NW 20 th Avenue. Pedestrians can also access the store via the entrance at the corner of NW 20 th Place & W. Burnside.
Transit Street Main Entrance	For portions of a building within the maximum building setback, at least one main entrance for each tenant space must be within 25 feet of the transit street. (33.130.242)	---	Entrance Setback W. Burnside Street Store: 18 feet Tenant Spaces: 10 feet

Standard	CX Zone	CC Plan District	Proposed
Exterior Display, Storage, and Work Activities	Not allowed (33.130.245)	---	No exterior display, storage or work activity is proposed.
Trucks and Equipment	Does not apply (33.130.255)	---	No regularly parked trucks or equipment are proposed. Only intermittent and short term pick-up and delivery activities are proposed.
Drive Through Facilities	Does not apply (33.130.260)	---	No drive-through facility is proposed.
Residential Development	Does not apply (33.140.265)	---	Residential Development is not proposed.
Detached Accessory Structure	Does not apply (33.130.265)	---	Detached Accessory structure not proposed.
Fences	Does not apply (33.130.270)	---	No fencing is proposed.
Demolitions	Must comply with 33.445, Historic Resources Protection Zone (33.130.275)	---	Demolition requirements will be met.
Nonconforming development	33.130.285	---	The existing store, surface parking lot and proposed expansion meet current standards.
Parking and Loading	Comply with Chapter 33.266 33.130.290	---	See response below
Signs	Comply with Title 32 33.130.295	---	All signage attached to the building complies with 33.32.020
Street Trees	See Chapter 30.293 (33.130.305)	---	Will be provided as required by City Forester.
Superblock Requirement	33.140.310	---	Not applicable

Standard	CX Zone	CC Plan District	Proposed
Recycling Areas	33.130.310 See Section 17.102.180 - All Businesses within the City shall recycle their recyclable materials in compliance with Administrative Rules established by the Office of Sustainable Development.	---	Included in garbage area, inside of building. Recycling will comply with the Office of Sustainable Development Administrative Rules.
Parking Lot Landscaping	Interior Parking Lot Landscaping: 45 SF per stall Perimeter Parking Lot Landscaping: 5 ft of L2	--	Interior Landscaping-Surface Parking Lot Spaces: 36 total Existing: 1,770 SF Perimeter Landscaping-Surface Parking Lot Existing: 5 ft of L2
Minimum Automobile Parking	None (33.266.110)	--	Surface Parking Lot Existing: 36 spaces Proposed: No Change Parking Garage Existing: 253 spaces Proposed: 187 spaces Total: 228 spaces Net Change: -66 spaces
Maximum Automobile Parking	Office: 1/294 SF Retail Sales and Service: 1/196 SF	Office: 1/1000 SF of net building area Retail: 1/1000 SF	Maximum 292 Spaces without CCPR.
Loading Facility	2 loading spaces required for buildings with more than 50,000 SF floor area	---	2 existing loading docks are located along east elevation (NW 20 th Avenue). Two Loading Zones: 44 LF and 121.5 LF (NW 20 th Avenue).
Bicycle Parking Long Term Spaces	Retail Sales and Service: 1 per 12,000 SF building area Office: 2, or 1 per 10,000 SF building area	---	Retail Sales and Service: 18 Office: 2 Total: 20
Bicycle Parking Short Term Spaces	Retail Sales and Service: 1 per 5,000 SF building area Office: 2, or 1 per 40,000 SF building area	---	Retail Sales and Service: 22 Office: 2 Total: 24

CENTRAL CITY DESIGN GUIDELINES

A2	Emphasize Portland Themes	(SHEET 38)
A3	Respect the Portland Block Structure	(SHEET 32)
A4	Use unifying elements	(SHEET 32)
A5	Enhance, embellish, and identify areas	(SHEET 32)
A7	Establish and maintain a sense of urban enclosure	(SHEET 32)
A8	Contribute to a vibrant streetscape	(SHEET 32)
B1	Reinforce and enhance the pedestrian system	(SHEET 33)
B2	Protect the pedestrian	(SHEET 33)
B3	Bridge pedestrian obstacles	(SHEET 23)
B4	Provide stopping and viewing places	(SHEET 33)
B5	Make plazas, parks, and open spaces successful	(SHEET 33)
B6	Develop Weather protection	(SHEET 34)
B7	Integrate Barrier free design	(SHEET 23)
C1	Enhance view opportunities	(SHEET 33)
C2	Promote quality and permanence in development	(SHEET 26)
C3	Respect Architectural Integrity	(SHEET 26)
C4	Complement the Context of existing buildings	(SHEET 34)
C5	Design for Coherency	(SHEET 34)
C6	Develop transitions between buildings and public spaces	(SHEET 33)
C7	Design corners that build active intersections	(SHEET 34)
C8	Differentiate the sidewalk level of buildings	(SHEET 27)
C9	Develop flexible sidewalk level spaces	(SHEET 22)
C10	Integrate Encroachments	(SHEET 24)
C11	Integrate Roofs and use rooftops	(SHEET 38)
C12	Integrate Exterior Lighting	(SHEET 33)
C13	Integrate signs	(SHEET 33)

GOOSE HOLLOW DESIGN GUIDELINES

A5.3	<i>Enhance West Burnside Street by extending and improving its boulevard treatment and its environment west of the Park Blocks.</i>	(SHEET 32)
A5.5	<i>Incorporate water features or water themes which enhance the quality, character, and image of the Goose Hollow district.</i>	(SHEET 36)
A5.6	<i>Incorporate works of art or other special design features that increase which enhance the public enjoyment of the district.</i>	(SHEET 36)
B1.1	<i>Provide human scale and interest to buildings along sidewalks</i>	(SHEET 24)
B1.2	<i>Orient primary entries at pedestrian circulation points which conveniently connect to transit services</i>	(SHEET 24)
C1.1	<i>Design parking garage exteriors to integrate with surroundings</i>	(SHEET 20)
C1.2	<i>Integrate signs and awnings to be complementary to buildings architecture</i>	(SHEET 34)
C3.1	<i>Locate buildings to provide for future infill on surface parking lots</i>	(SHEET 13)
C7.1	<i>Reduce the impact on pedestrians from cars entering & exiting garage access</i>	(SHEET 20)

*GOOSE HOLLOW GUIDELINES SHOWN ITALICIZED

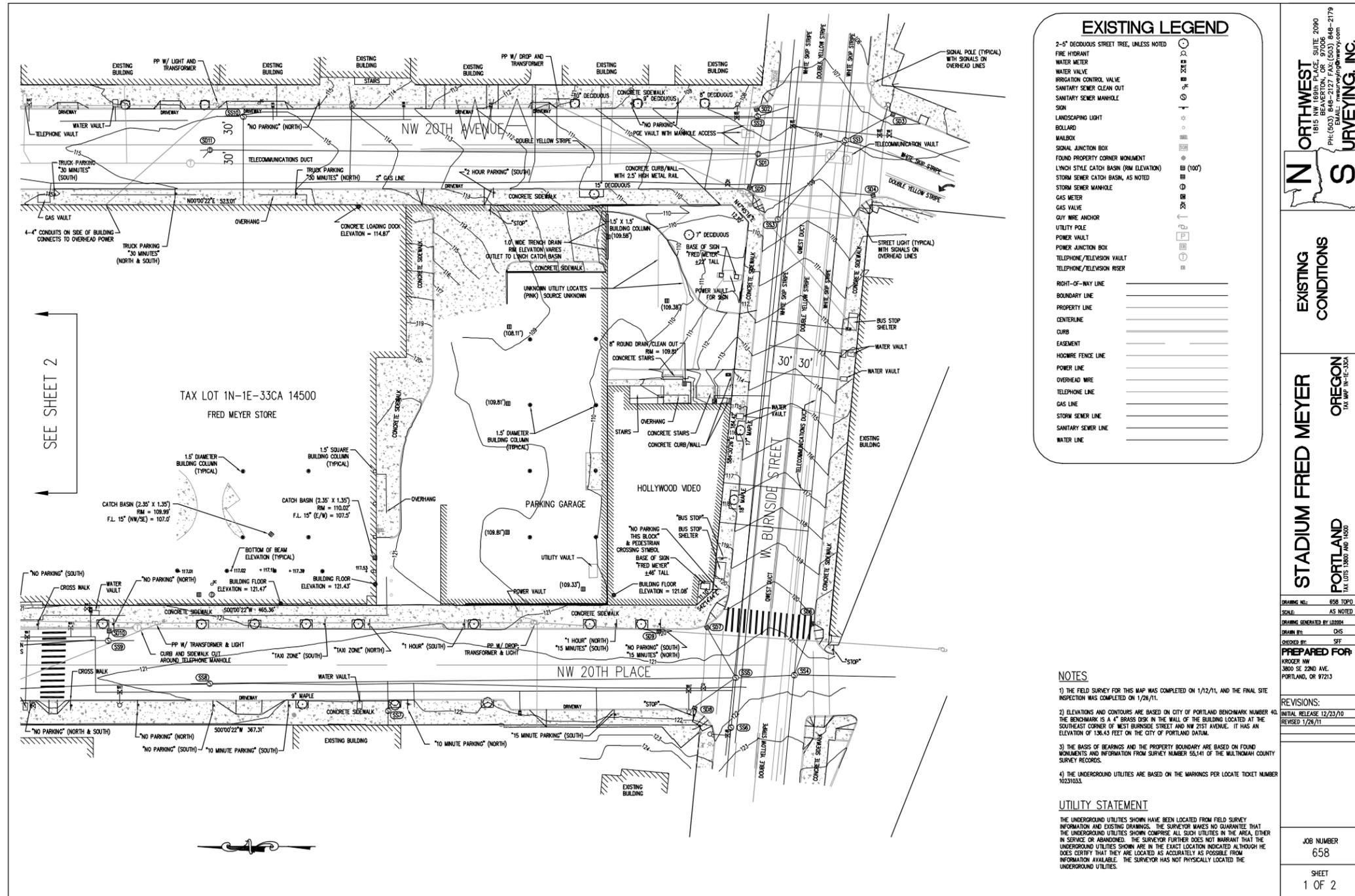
STADIUM

Fred Meyer



3

PLANS



EXISTING LEGEND

2-5" DECIDUOUS STREET TREE, UNLESS NOTED	⊙
FIRE HYDRANT	⊕
WATER METER	⊖
WATER VALVE	⊗
IRRIGATION CONTROL VALVE	⊘
SANITARY SEWER CLEAN OUT	⊙
SANITARY SEWER MANHOLE	⊕
SIGN	⊙
LANDSCAPING LIGHT	⊙
BOLLARD	⊙
MAILBOX	⊙
SIGNAL JUNCTION BOX	⊙
FOUND PROPERTY CORNER MONUMENT	⊙
LINCH STYLE CATCH BASIN (RM ELEVATION)	⊙
STORM SEWER CATCH BASIN, AS NOTED	⊙
STORM SEWER MANHOLE	⊙
GAS METER	⊙
GAS VALVE	⊙
CITY WIRE ANCHOR	⊙
UTILITY POLE	⊙
POMER VAULT	⊙
POWER JUNCTION BOX	⊙
TELEPHONE/TELEVISION VAULT	⊙
TELEPHONE/TELEVISION RISER	⊙
RIGHT-OF-WAY LINE	---
BOUNDARY LINE	---
PROPERTY LINE	---
CENTERLINE	---
CURB	---
EASEMENT	---
HOCKWIRE FENCE LINE	---
POWER LINE	---
OVERHEAD WIRE	---
TELEPHONE LINE	---
GAS LINE	---
STORM SEWER LINE	---
SANITARY SEWER LINE	---
WATER LINE	---

NOTES

- 1) THE FIELD SURVEY FOR THIS MAP WAS COMPLETED ON 1/12/11, AND THE FINAL SITE INSPECTION WAS COMPLETED ON 1/28/11.
- 2) ELEVATIONS AND CONTOURS ARE BASED ON CITY OF PORTLAND BENCHMARK NUMBER 40. THE BENCHMARK IS A 4" BRASS DISK IN THE WALL OF THE BUILDING LOCATED AT THE SOUTHEAST CORNER OF WEST BURNSIDE STREET AND NW 21ST AVENUE. IT HAS AN ELEVATION OF 136.43 FEET ON THE CITY OF PORTLAND DATUM.
- 3) THE BASIS OF BEARINGS AND THE PROPERTY BOUNDARY ARE BASED ON FOUND MONUMENTS AND INFORMATION FROM SURVEY NUMBER 55,141 OF THE MULTNOMAH COUNTY SURVEY RECORDS.
- 4) THE UNDERGROUND UTILITIES ARE BASED ON THE MARKINGS PER LOCATE TICKET NUMBER 10231033.

UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.

ORTHWEST SURVEYING, INC.
1815 NE 15TH PL. SUITE 2000
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EMAIL: nrsurvey@nwsurvey.com

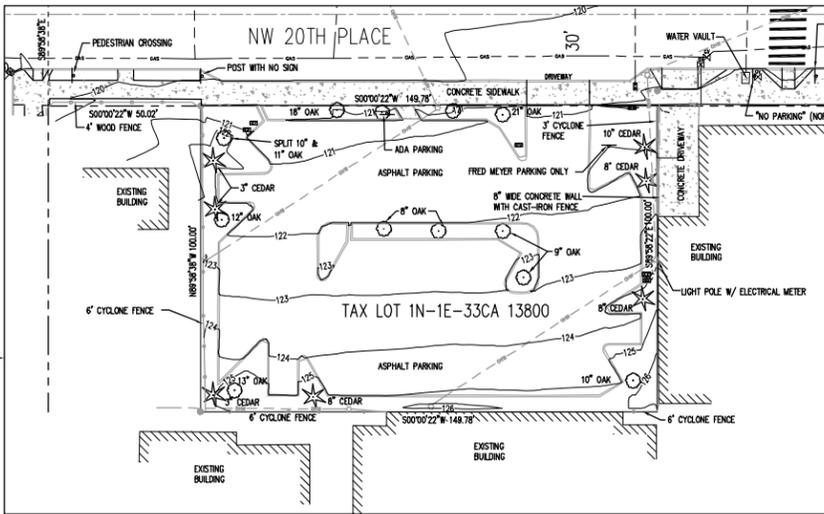
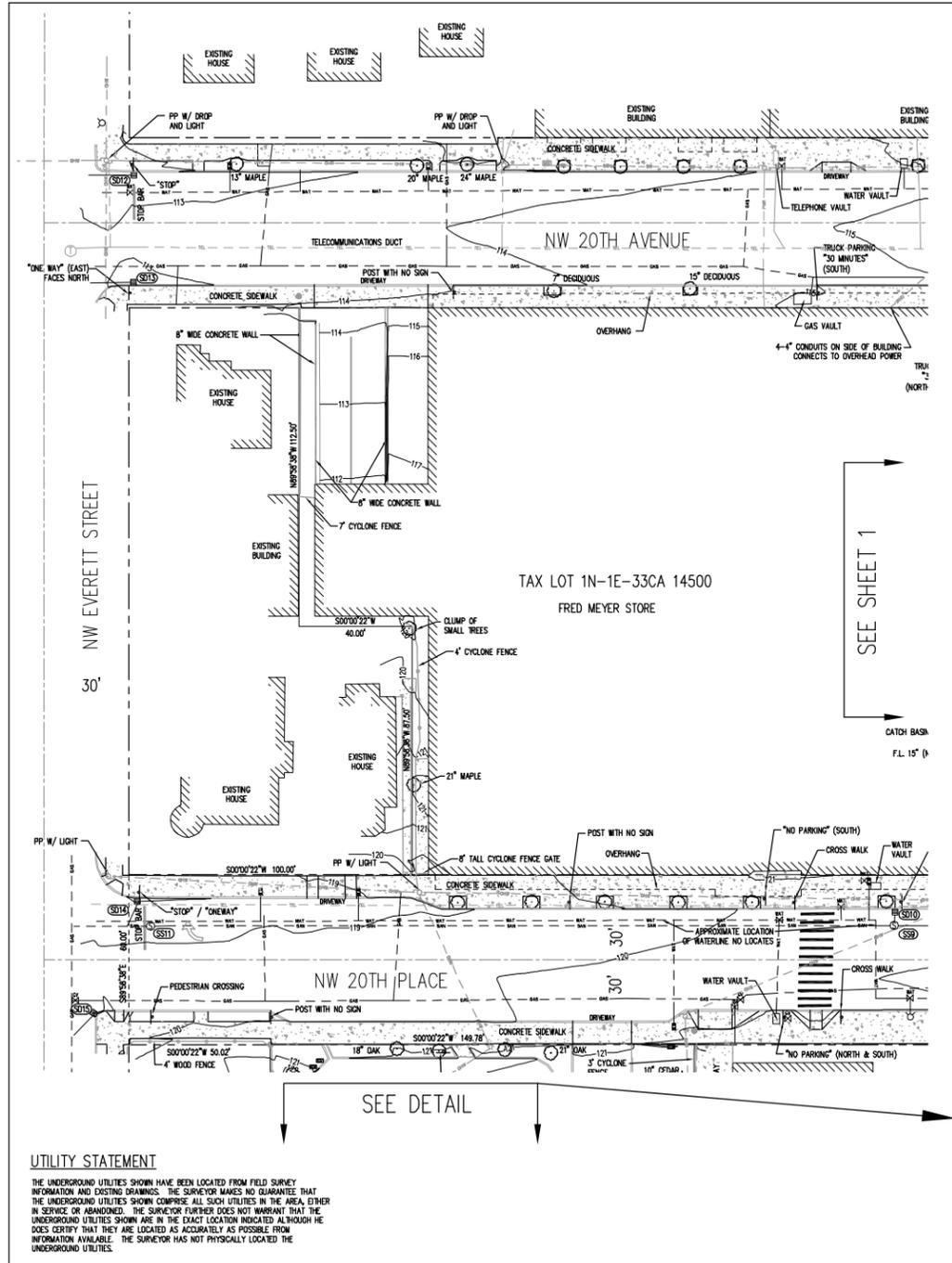
STADIUM FRED MEYER
PORTLAND
11A, LOTS 13807 AND 13808

EXISTING CONDITIONS

DRAWING NO.: 658 TOPO
SCALE: AS NOTED
DRAWING GENERATED BY: L20004
DRAWN BY: CHS
CHECKED BY: SFF
PREPARED FOR:
KROGER NW
3800 SE 22ND AVE.
PORTLAND, OR 97213

REVISIONS:
INITIAL RELEASE 12/23/10
REVISED 1/28/11

JOB NUMBER: 658
SHEET: 1 OF 2



- STORM DRAINAGE NOTES**
- S271 MANHOLE
RM = 108.46'
I.E. 12" CONC. IN (N) = 97.7'
I.E. 12" CONC. IN (N) = 88.1'
I.E. 12" CONC. OUT (S) = 87.8'
 - S272 CATCH BASIN
RM = 107.15'
I.E. 8" DP (W) = 105.8'
PIPE PARTIALLY FULL OF SILT
 - S273 CATCH BASIN
RM = 106.52'
I.E. 12" CONC. (NW) = 104.1'
PIPE PARTIALLY FULL OF SILT
 - S274 CATCH BASIN
RM = 108.17'
I.E. 10" CONC. (N) = 106.5'
 - S275 CATCH BASIN
RM = 108.30'
SILT NO PIPE VISIBLE
 - S276 CATCH BASIN
RM = 119.97'
I.E. 12" CONC. (NW) = 118.0'
 - S277 CATCH BASIN
RM = 120.06'
I.E. 8" CONC. (SW) = 117.9'
 - S278 CATCH BASIN
RM = 120.99'
I.E. 8" CONC. (SE) = 118.6'
 - S279 CATCH BASIN
RM = 119.85'
FULL OF SILT NO VISIBLE PIPE
NOT SHOWN ON PORTLANDMAPS.COM
 - S280 CATCH BASIN
RM = 120.13'
I.E. 10" CONC. OUT (W) = 116.0'
 - S281 MANHOLE
RM = 115.73'
F.L. 12" CONC. IN (S) = 103.7'
IN (N AND NW) OUT (S)
 - S282 CATCH BASIN
RM = 112.32'
I.E. 9" VSP OUT (N) = 110.5'
 - S283 CATCH BASIN
RM = 112.75'
I.E. 8" DP OUT (N) = 110.8'
 - S284 CATCH BASIN
RM = 117.92'
I.E. 8" CONC. OUT (W) = 115.8'
 - S285 CATCH BASIN
RM = 118.93'
I.E. 8" DP OUT (NW) = 116.5'

- SANITARY SEWER NOTES**
- S286 COMBINED SEWER MANHOLE
RM = 107.89'
F.L. = 84.8' + 24" VSP IN (W) 30" VSP OUT (E)
I.E. 10" VSP IN (NW) = 86.3' +
I.E. 12" CONC. IN (SW) = 86.3' +
I.E. 12" CONC. IN (N) = 86.5' +
NO ELEVATION PROVIDED FOR THE PIPE FROM THE CATCH BASIN TO THE SOUTHEAST
 - S287 COMBINED SEWER MANHOLE
RM = 107.46'
I.E. 8" DP (E) = 105.5'
F.L. 9" VSP (N TO S) = 100.3'
 - S288 COMBINED SEWER FLOW CONTROL MANHOLE
RM = 109.59'
F.L. 10" VSP = 101.1' +
IN (W) OUT (SE & E)
 - S289 COMBINED SEWER MANHOLE
RM = 121.67'
I.E. 9" VSP IN (N) = 99.3' +
I.E. 12" VSP IN (S) = 100.0' +
F.L. 24" VSP (E TO W) = 98.0' +
* NO ELEVATION PROVIDED FOR THE PIPE FROM THE CATCH BASIN TO THE SOUTHEAST
 - S290 COMBINED SEWER FLOW CONTROL MANHOLE
RM = 121.52'
9" VSP IN (N & W) OUT (S & E)
 - S291 COMBINED SEWER MANHOLE
RM = 122.20'
IN (W) OUT (E)
 - S292 SANITARY SEWER MANHOLE
RM = 122.25'
I.E. 8" CONC. IN (W) = 117.7'
I.E. 6" CONC. OUT (E) = 117.5'
 - S293 COMBINED SEWER MANHOLE
RM = 124.40'
F.L. 9" VSP (N TO S) = 114.0'
 - S294 COMBINED SEWER MANHOLE
RM = 120.32'
I.E. 10" CONC. IN (E) = 115.1'
F.L. 9" CONC. (S TO N) = 111.9'
 - S295 COMBINED SEWER MANHOLE
RM = 115.03'
F.L. 9" VSP = 104.3' +
IN (N & W) OUT (S)
 - S296 COMBINED SEWER MANHOLE
RM = 118.63'
F.L. 9" VSP (S TO N) = 110.2'

UTILITY STATEMENT
THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.

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EMAIL: nrsurveying@verizon.com

EXISTING CONDITIONS

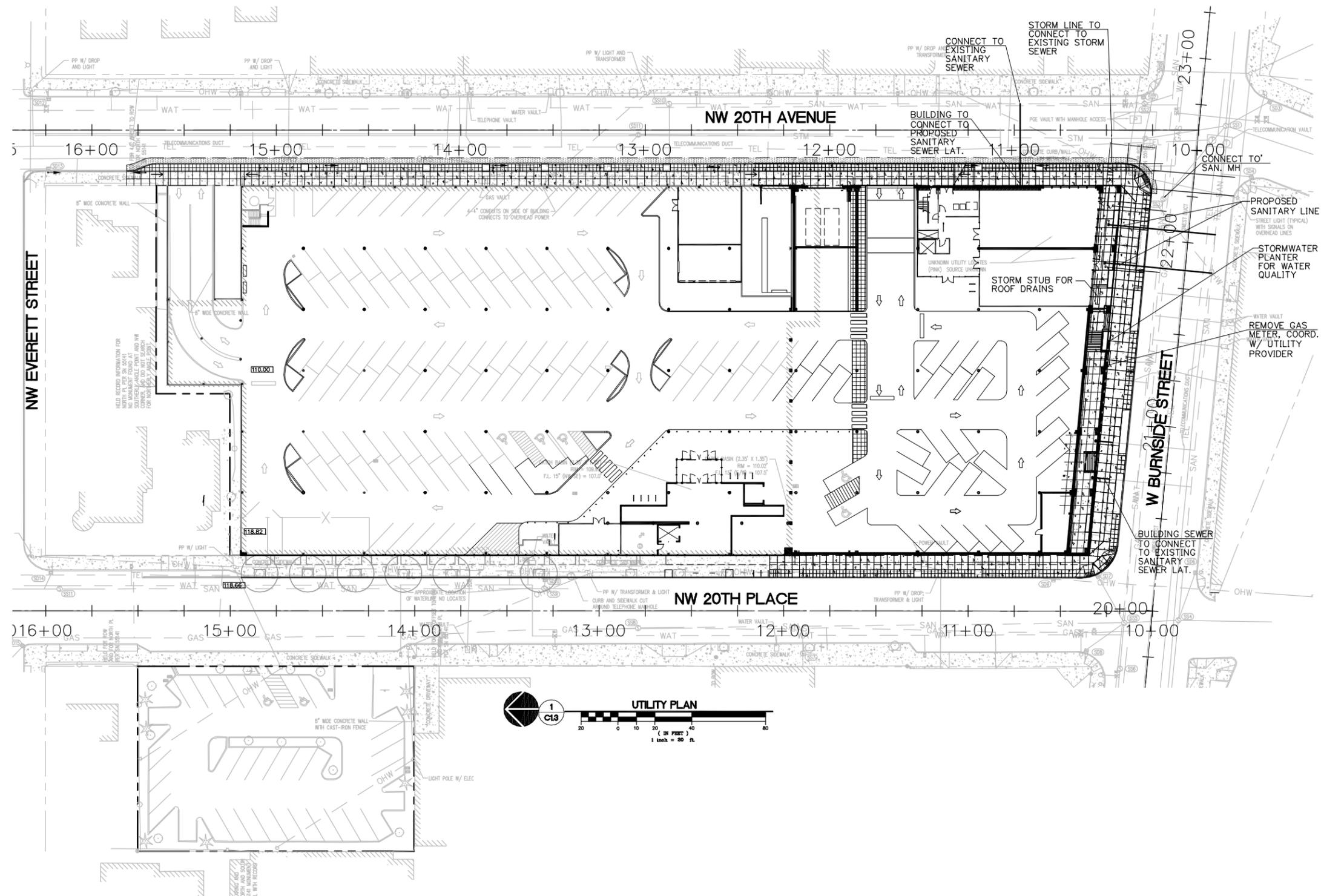
STADIUM FRED MEYER
PORTLAND
TAX LOTS 13800 AND 14500

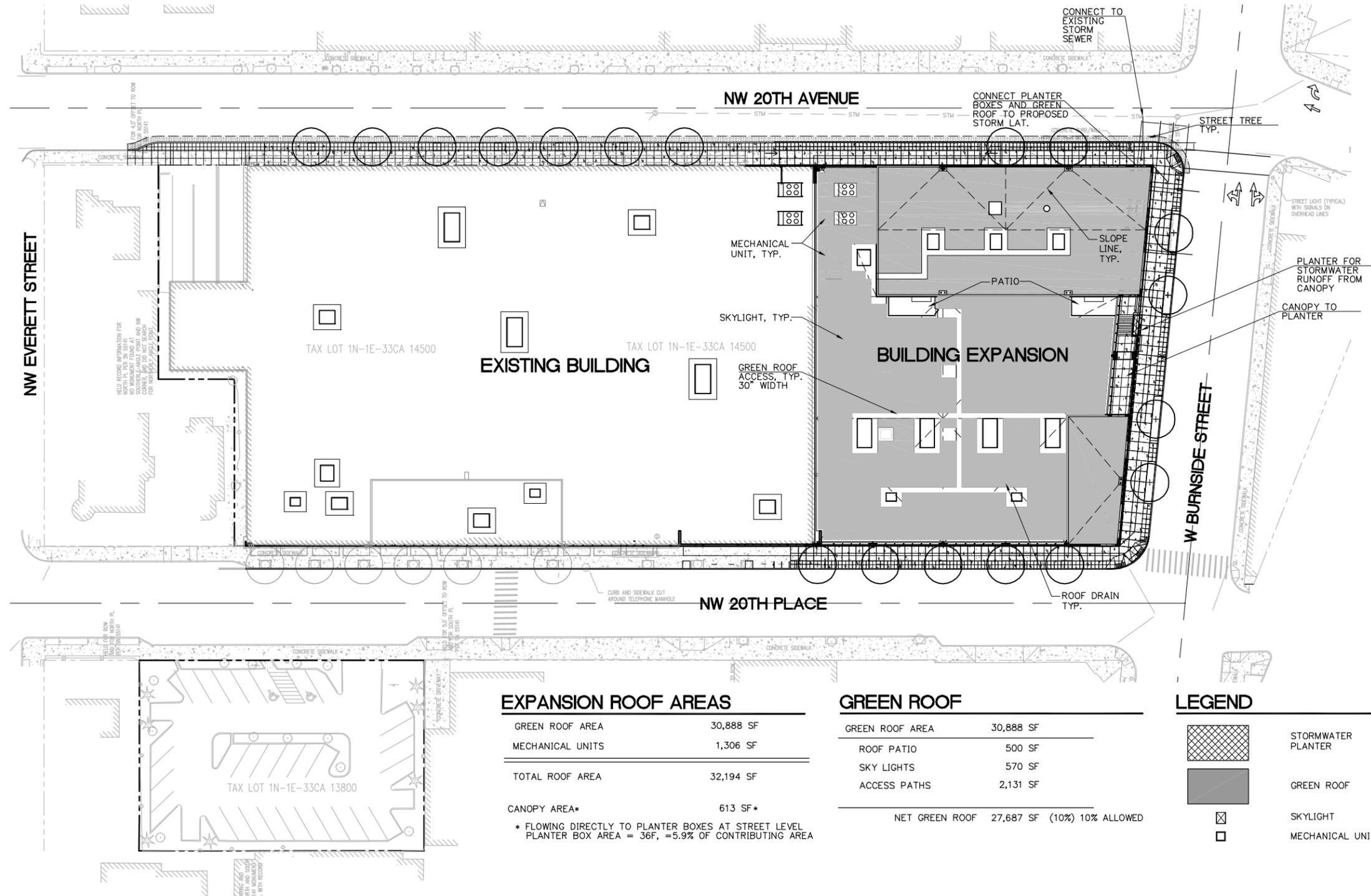
DRAWING NO.: 658 TOPO
SCALE: AS NOTED
DRAWING GENERATED BY: LORRAH
DRAWN BY: CHS
CHECKED BY: SFJ
PREPARED FOR:
KROGER NW
3800 SE 22ND AVE.
PORTLAND, OR 97213

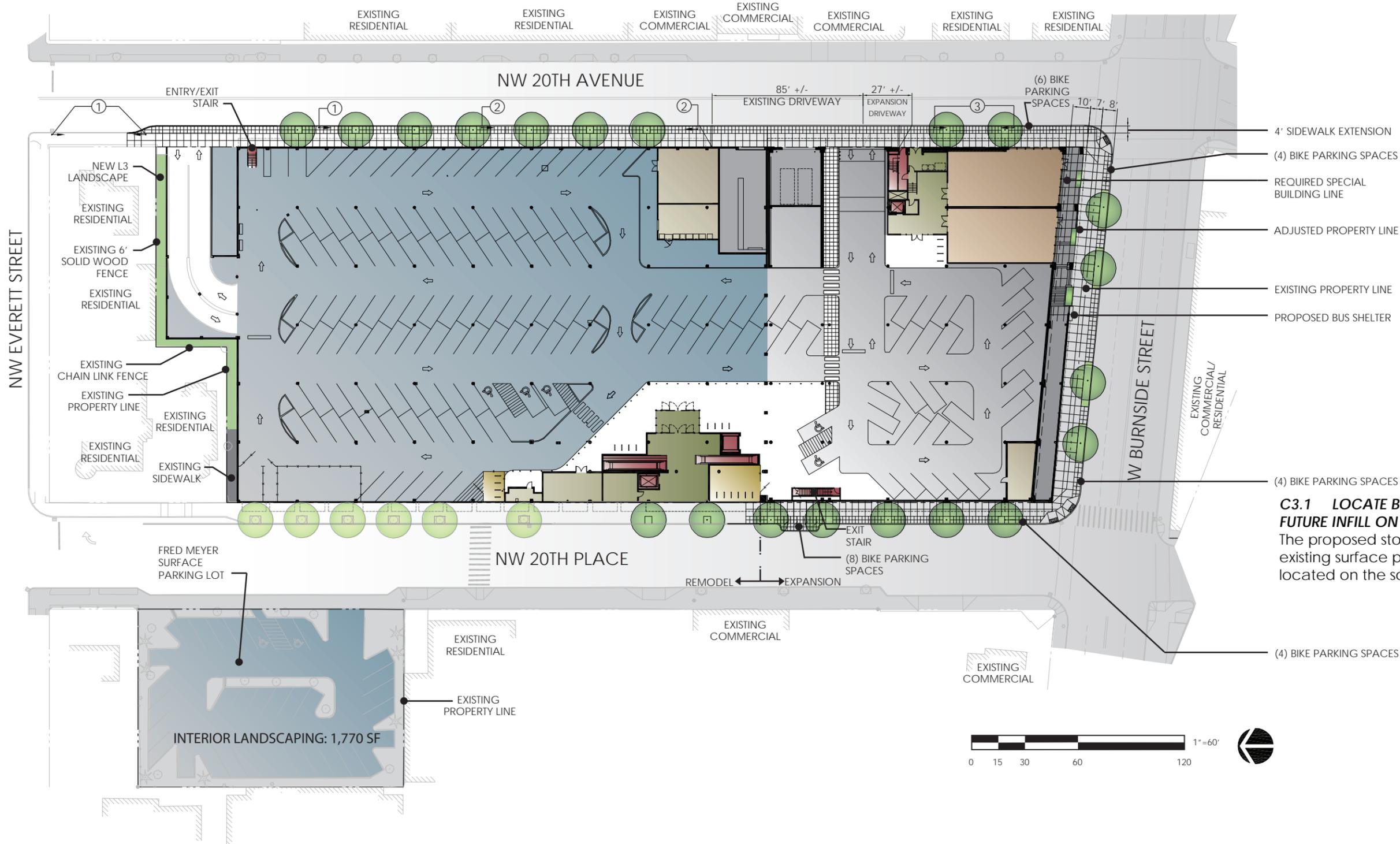
REVISIONS:
INITIAL RELEASE 12/23/10
REVISED 1/26/11

JOB NUMBER
658

SHEET
2 OF 2







- 4' SIDEWALK EXTENSION
- (4) BIKE PARKING SPACES
- REQUIRED SPECIAL BUILDING LINE
- ADJUSTED PROPERTY LINE
- EXISTING PROPERTY LINE
- PROPOSED BUS SHELTER
- (4) BIKE PARKING SPACES
- (4) BIKE PARKING SPACES

C3.1 LOCATE BUILDINGS TO PROVIDE FOR FUTURE INFILL ON SURFACE PARKING LOTS
The proposed store expansion is replacing the existing surface parking and structured parking located on the south portion of the site.



PARKING

<u>EXISTING CONDITION:</u>	
EXISTING SURFACE PARKING:	36 (2 HC)
EXISTING LOWER LEVEL PARKING:	253 (3 HC)
<u>PROPOSED:</u>	
PROPOSED SURFACE PARKING:	36 (NO CHANGE)
PROPOSED LOWER LEVEL PARKING:	187 (2 HC)
TOTAL:	223 (7HC) (-66)

BUILDING

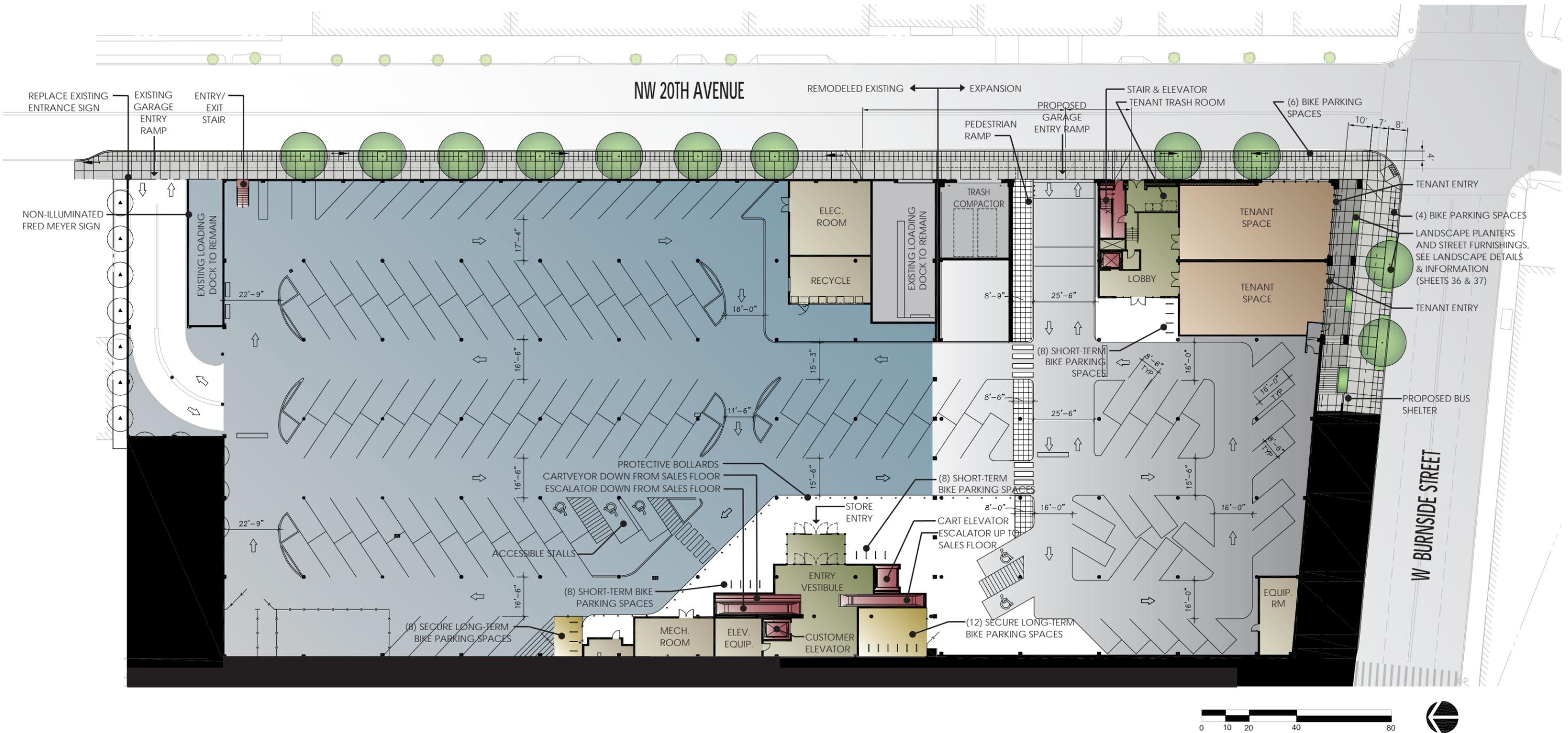
<u>BUILDING AREA - NON-PARKING</u>	
<u>LOWER LEVEL FLOOR:</u>	
ENTRY VESTIBULE:	3,312 SF
TENANT & LOBBY:	5,580 SF
MISCELLANEOUS:	2,277 SF
SALES FLOOR:	92,448 SF
MEZZANINE FLOOR:	5,740 SF
OFFICE FLOOR:	8,594 SF
TOTAL:	117,951 SF

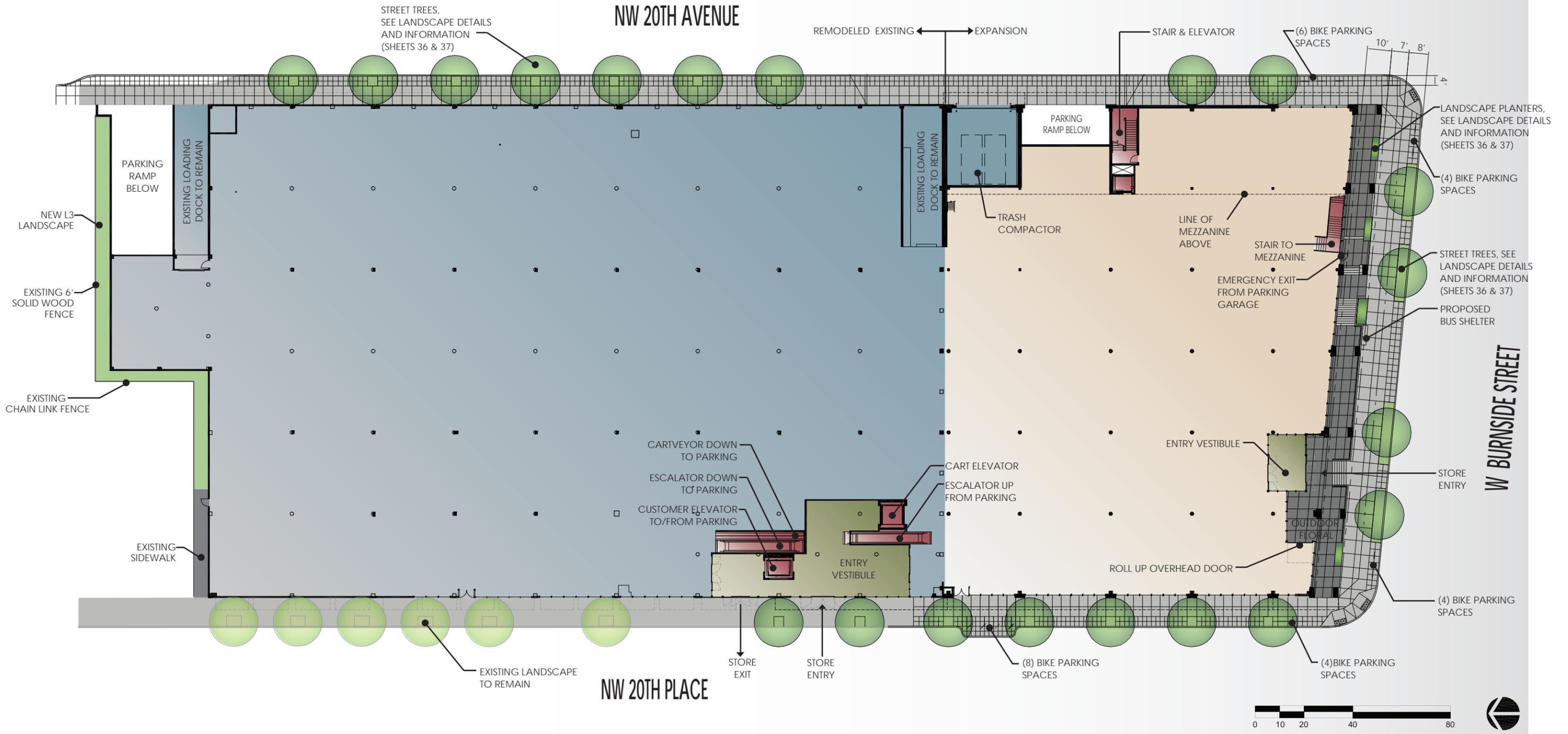
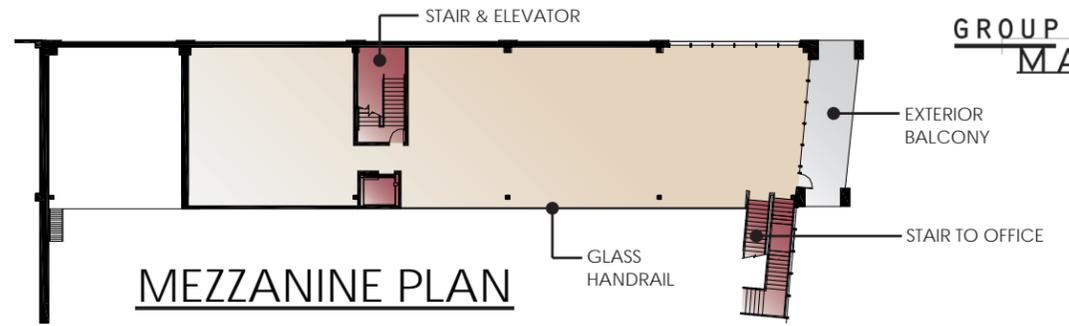
KEYNOTES AND LEGEND

- PARKING & LOADING SIGNAGE
- ① 90 MIN. VISITOR PARKING EXCEPT BY ZONE L PERMIT
 - ② NO PARKING
30 MIN. LOADING ZONE
7-11AM MON-FRI
 - ③ NO PARKING
30 MIN. LOADING ZONE
7-11AM MON-FRI

LEGEND

- PROPOSED STREET TREE
- EXISTING STREET TREE





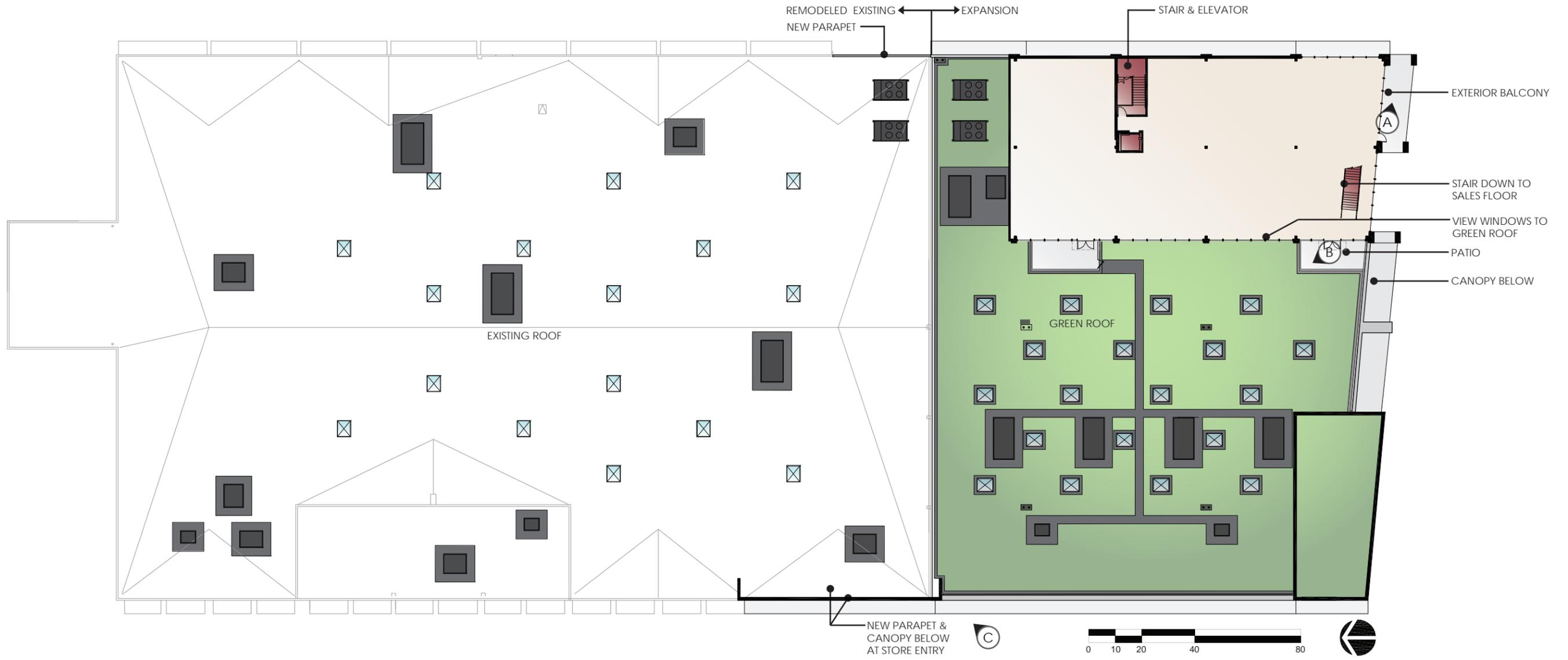


SEE SHEET 44 FOR RAILING DETAILS



SEE SHEET 44 FOR RAILING DETAILS

SEE SHEET 44 FOR CANOPY DETAILS





SEE SHEET 44 FOR RAILING DETAILS

ROOF TOP PATIO



CONCEPT - GREEN ROOF



SEE SHEET 44 FOR RAILING DETAILS

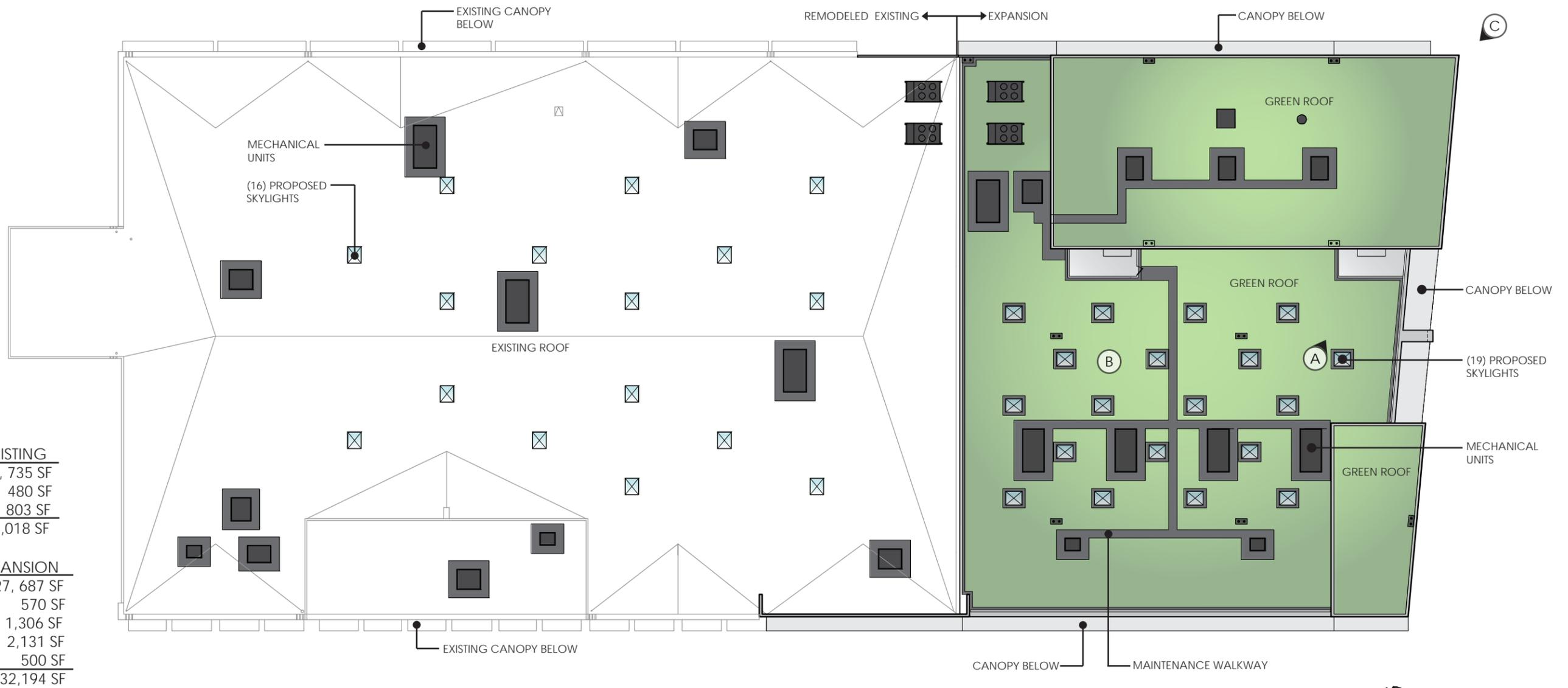
SOUTHEAST CORNER AT BALCONIES

ROOF AREA DATA - EXISTING

EXISTING ROOF:	60,735 SF
(16) SKYLIGHTS:	480 SF
MECH. UNITS:	803 SF
TOTAL	62,018 SF

ROOF AREA DATA - EXPANSION

GREEN ROOF:	27,687 SF
SKYLIGHTS:	570 SF
MECH. UNITS:	1,306 SF
ACCESS WALKWAY:	2,131 SF
ROOF PATIO:	500 SF
TOTAL	32,194 SF

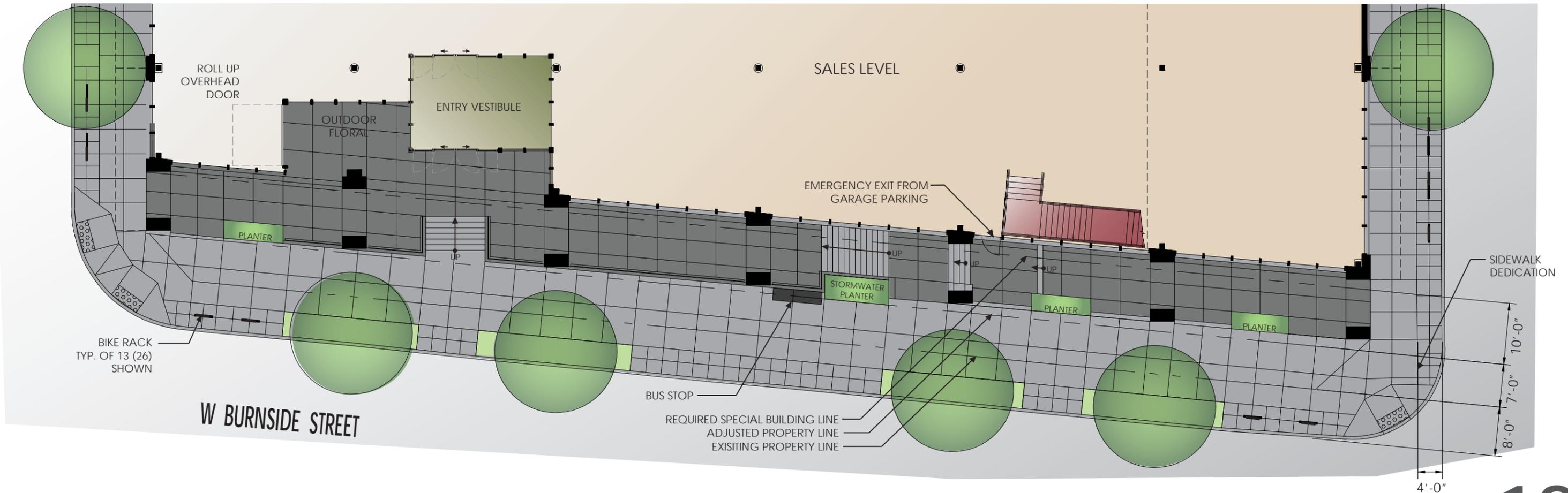




1 PARTIAL PERSPECTIVE VIEW OF BURNSIDE FRONTAGE



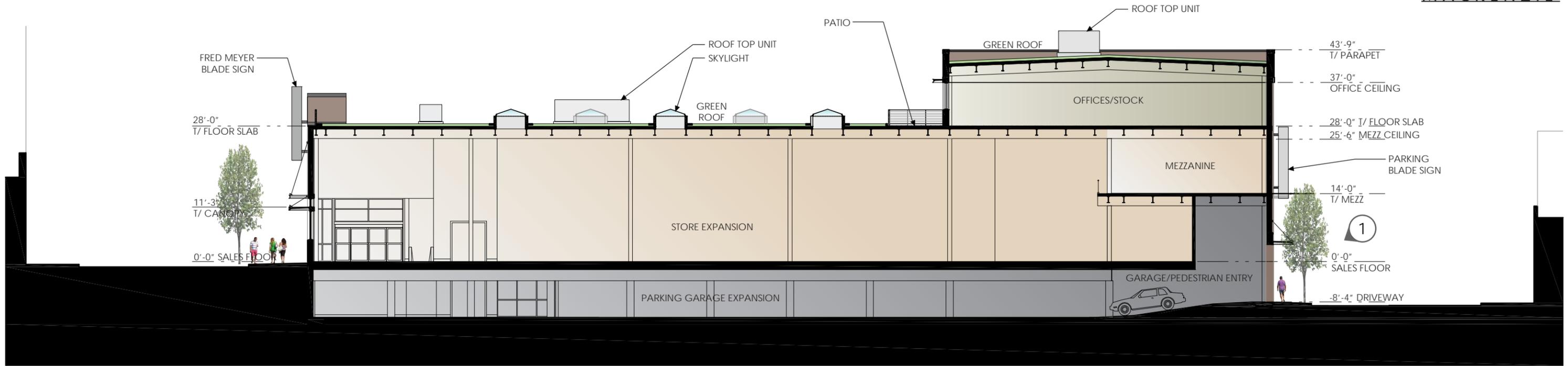
2 PARTIAL PERSPECTIVE VIEW OF BURNSIDE FRONTAGE





4

SECTIONS AND ELEVATIONS



PARKING GARAGE ENTRANCE

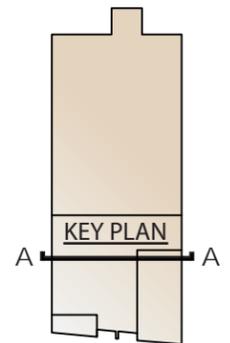
ETCHED STAINLESS STEEL PANEL, SEE SHEET 30



PARKING GARAGE PEDESTRIAN ENTRANCE



NON-ILLUMINATED (1'-10"x7'-10") "WELCOME" AND BIKE PARKING SIGN AT PEDESTRIAN ENTRANCE.

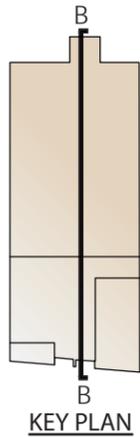
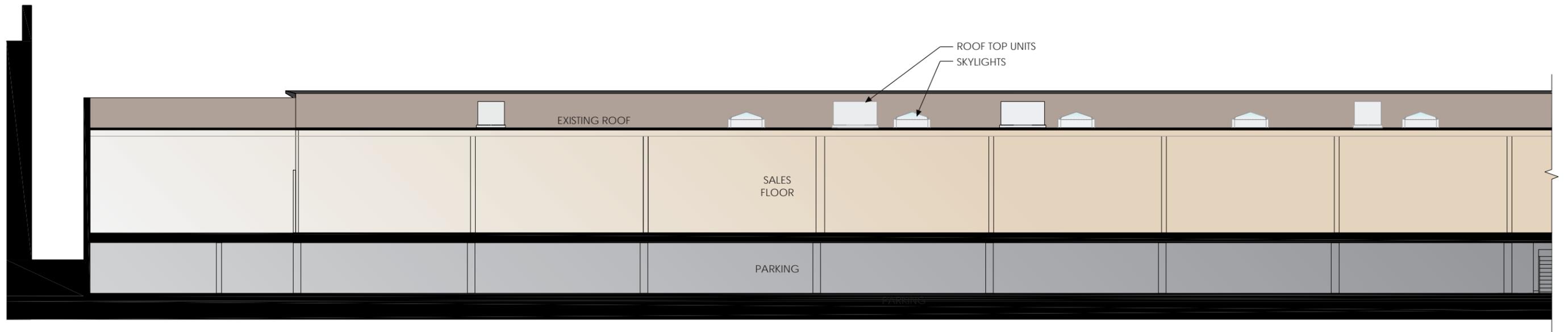
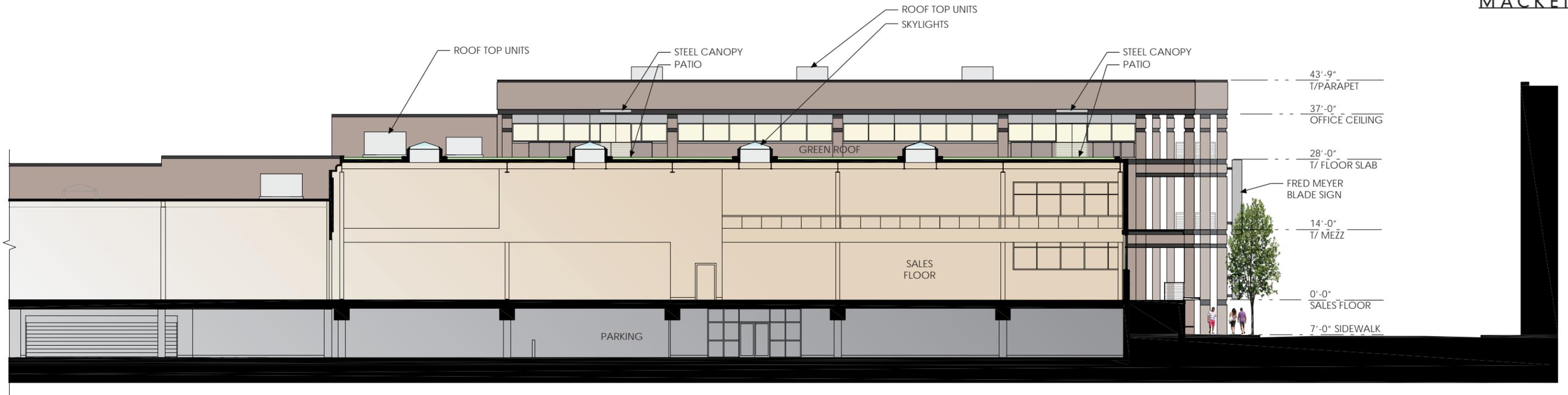


C1.1 DESIGN PARKING GARAGE EXTERIORS TO INTEGRATE WITH SURROUNDINGS

The upper levels of the existing parking garage are proposed to be eliminated with the store expansion. The remaining ground level parking garage will get a new exterior treatment that will be cohesive with the proposed store expansion.

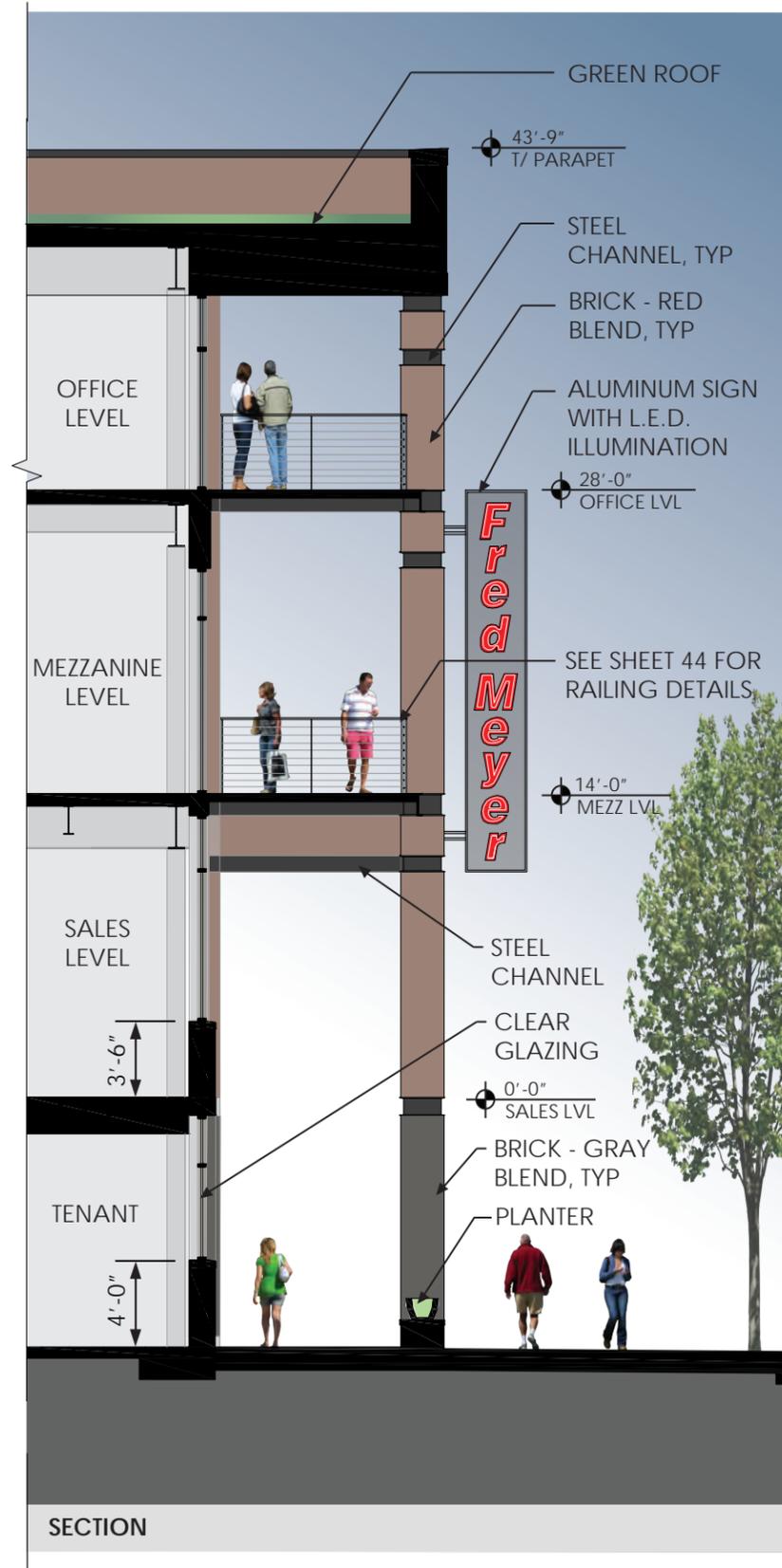
C7.1 REDUCE THE IMPACT ON PEDESTRIANS FROM CARS ENTERING AND EXITING GARAGE ACCESS

With the expansion of the store, the existing curb cuts that provide access to the existing parking garage along NW 20th Place and West Burnside are being eliminated, significantly reducing conflicts between pedestrians and vehicles in the area. The remaining parking garage entries/exits along NW 20th Avenue will employ alarm buzzers as vehicles exit the garage to alert pedestrians and traffic to exercise caution. This method is similar to other systems currently utilized in the Central City.



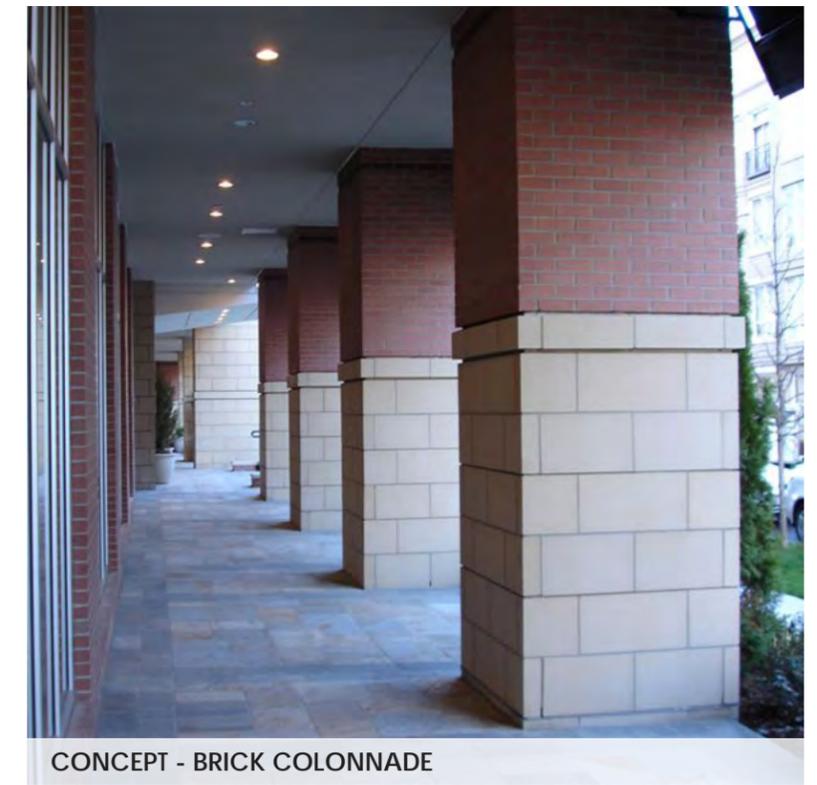


PERSPECTIVE VIEW



SECTION

C9 DEVELOP FLEXIBLE SIDEWALK LEVEL SPACES
The sidewalk level along the perimeter of the building has been designed to accommodate a variety of uses to fuel an active pedestrian environment. Elements such as the bus stop, outdoor seating area and tenant spaces provide a range of amenities and functional spaces along the streetscape.



CONCEPT - BRICK COLONNADE



PERSPECTIVE VIEW



SECTION

B3 BRIDGE PEDESTRIAN OBSTACLES

As part of the store expansion, and in order to circumvent the large change in grade along West Burnside Street, three store entries are proposed allowing pedestrians to access the store via any of the street frontages without having to walk up or down West Burnside Street. In addition, the two existing curb cuts along NW 20th Place and West Burnside will be eliminated. By reducing the number of curb cuts along the perimeter of the store, conflicts between pedestrians and vehicles will be reduced.

B7 INTEGRATE BARRIER FREE DESIGN

West Burnside Street features a large grade change. In an effort provide accessibility to all patrons, a series of ramps and stairways are proposed to help facilitate movement for people entering and exiting the store along West Burnside Street. Additionally, a separate entrance into the store is provided in the parking garage. The parking garage entrance allows patrons coming from NW 20th Avenue the ability to access the sales floor via ramp and escalator thereby avoiding having to scale West Burnside Street.



CONCEPT - RAISED FRONTAGE



PERSPECTIVE VIEW



SECTION

B1.1 PROVIDE HUMAN SCALE AND INTEREST TO BUILDINGS ALONG SIDEWALKS

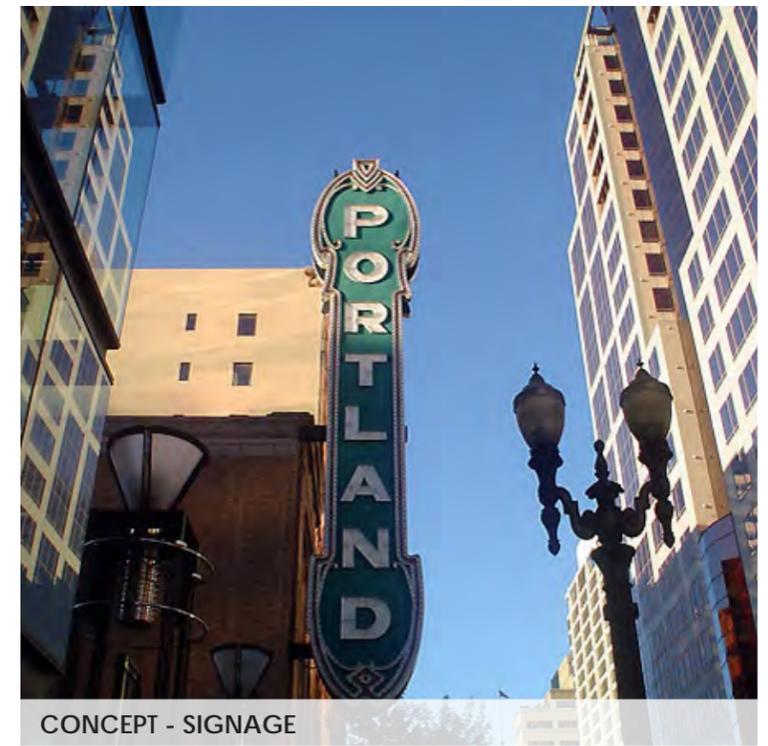
Large storefront glazing, canopies, and directional lighting have been incorporated along the building facades at the ground level. Above the ground level, balconies, signage and rooftop patios contribute to the pedestrian visual interest and enjoyment.

B1.2 ORIENT PRIMARY ENTRIES AT PEDESTRIAN CIRCULATION POINTS WHICH CONVENIENTLY CONNECT TO TRANSIT SERVICES

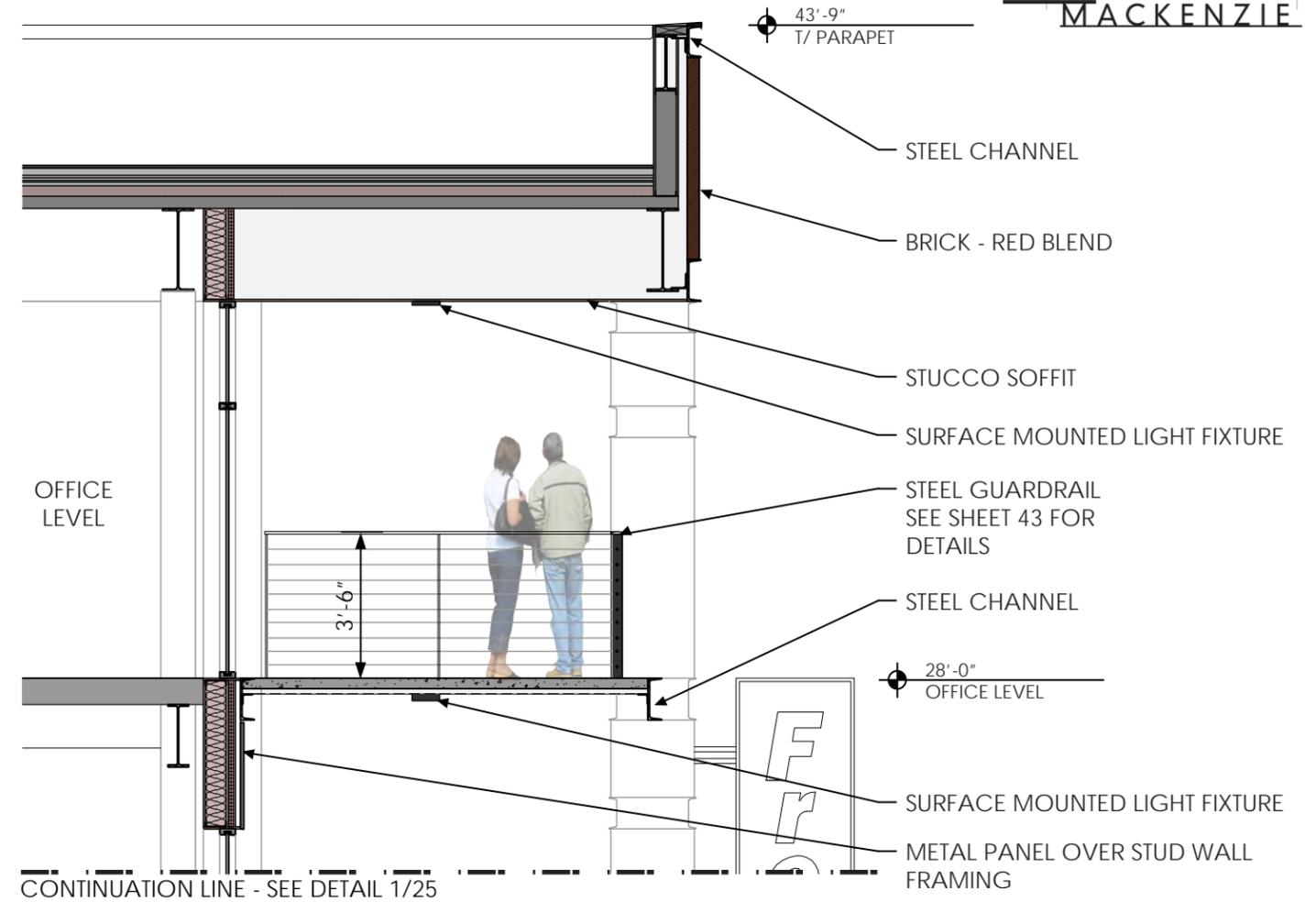
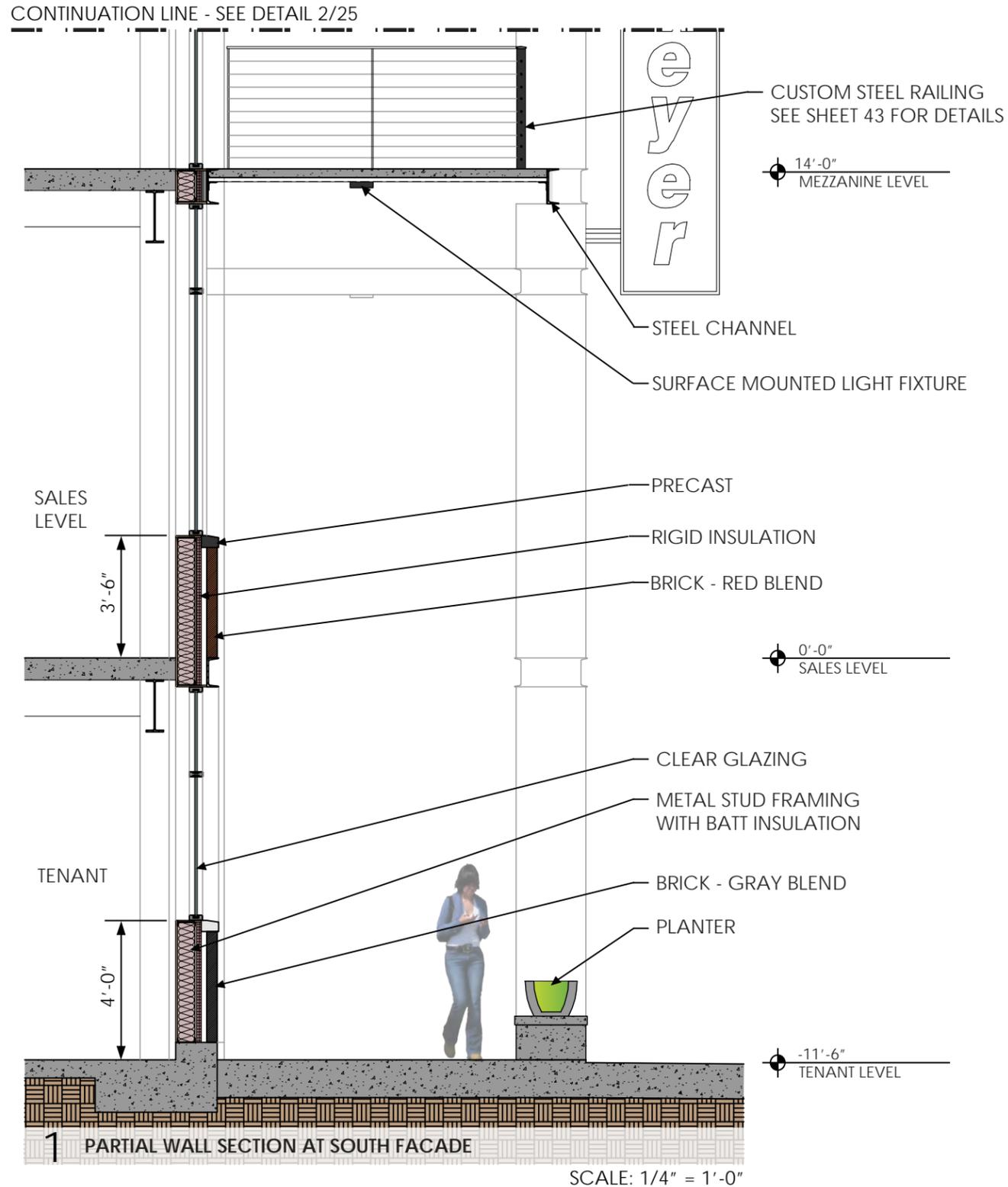
As part of this application the two existing bus stops located at the corners of NW 20th Place & West Burnside and NW 20th Avenue & West Burnside are proposed to be consolidated and incorporated into the terrace wall, providing a new mid-block location. A mid-block location provides riders and pedestrians with a convenient and central locale to any of the three primary store entries. One primary entry is located along NW 20th Place, another is located at the intersection of NW 20th place and West Burnside Street and the other is located via the parking garage entry along NW 20th Avenue.

C10 INTEGRATE ENCROACHMENTS

Beyond the blade signage, projections into the public right-of-way are limited to pedestrian amenities—the mid-block bus shelter and building canopies.

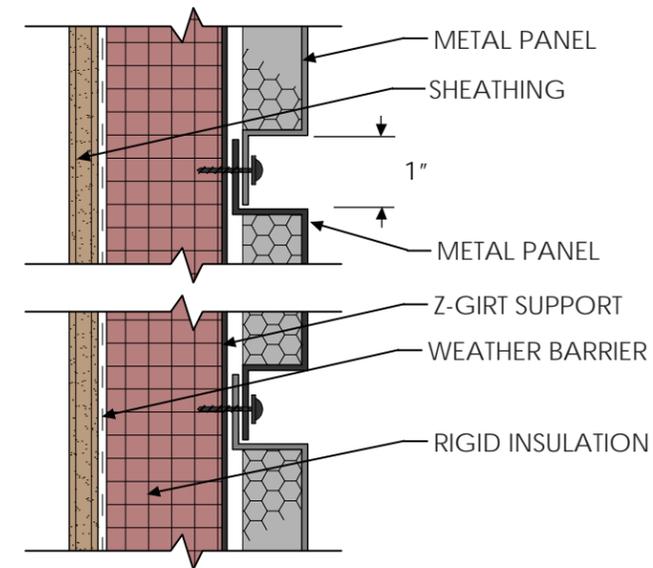


CONCEPT - SIGNAGE



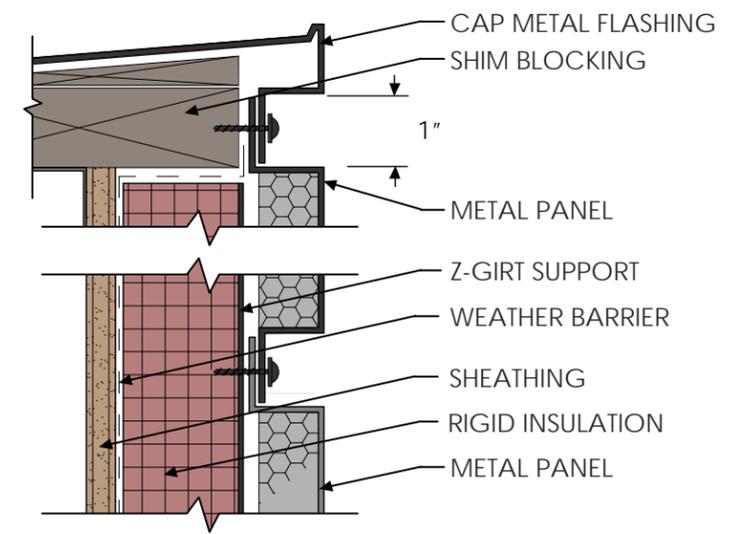
2 PARTIAL WALL SECTION AT SOUTH FACADE

SCALE: 1/4" = 1'-0"



3 METAL PANEL/ARRICAST BAND DETAIL

SCALE: 3" = 1'-0"



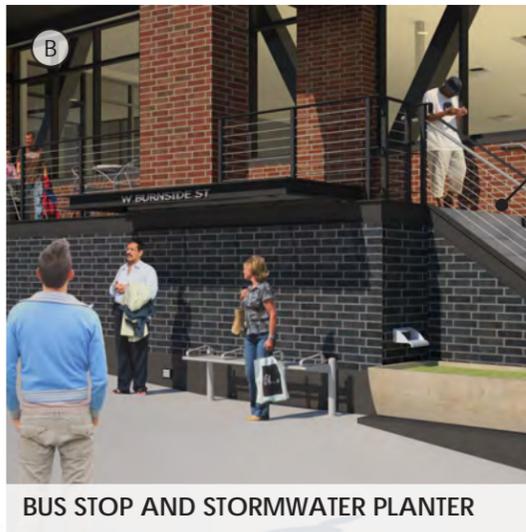
4 METAL PANEL/ARRICAST PARAPET DETAIL

SCALE: 3" = 1'-0"



STEPS AT SOUTHWEST ENTRY

SEE SHEET 44 FOR RAILING DETAILS

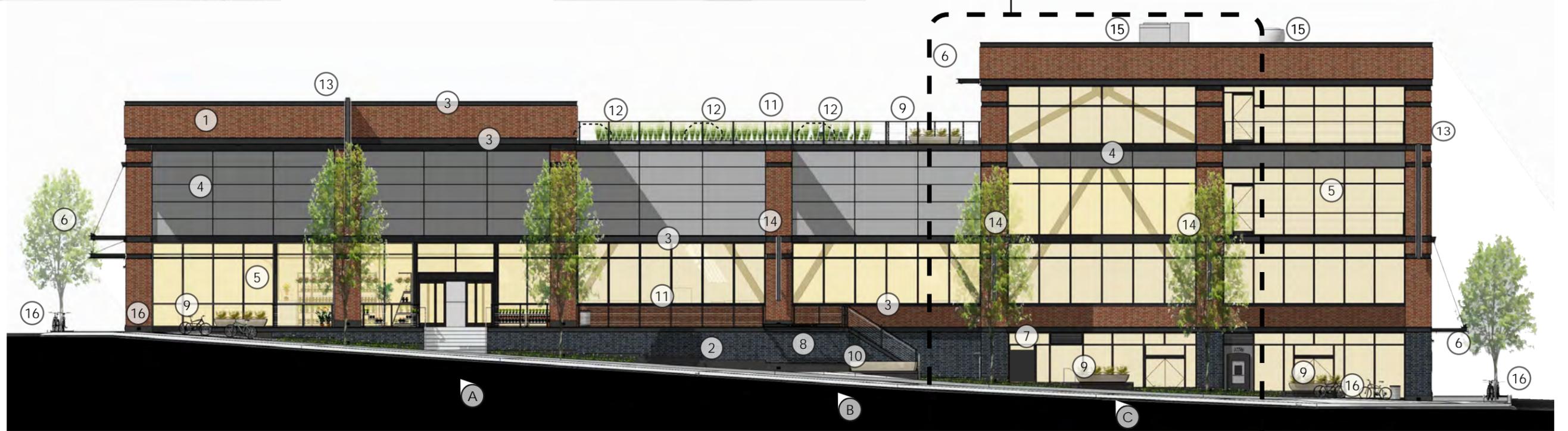


BUS STOP AND STORMWATER PLANTER

SEE SHEET 44 FOR RAILING DETAILS



STANDARD PLANTER AT COLONNADE



SOUTH ELEVATION

*NOTE: NO RTUs VISIBLE FROM THIS ELEVATION

SCALE: 1/16" = 1'-0"

MATERIALS KEYNOTES

- 1 - BRICK (NORMAN RED BLEND)
- 2 - BRICK (NORMAN GREY BLEND)
- 3 - STEEL CHANNEL
- 4 - METAL PANEL SYSTEM
- 5 - ALUM. STOREFRONT GLAZING SYSTEM
- 6 - STEEL CANOPY
- 7 - METAL LOUVERS
- 8 - BUS STOP CANOPY/BENCH
- 9 - PLANTER
- 10 - STORMWATER PLANTER
- 11 - CABLE GUARDRAIL
- 12 - SKYLIGHT (BEYOND)
- 13 - FM SIGNAGE
- 14 - TENANT SIGNAGE
- 15 - ROOF TOP MECHANICAL EQUIPMENT
- 16 - BICYCLE PARKING

GROUND FLOOR WINDOW CALCULATIONS

- SOUTH
- GROUND LEVEL WALL AREA: 1,475 SF
- GROUND LEVEL WINDOW AREA: 842 SF (368 SF REQUIRED)
- BUILDING LENGTH: 200'-0"
- WINDOW LENGTH: 136'-0" (100'-0" REQUIRED)
- WEST
- GROUND LEVEL WALL AREA: 4,048 SF
- GROUND LEVEL WINDOW AREA: 2,185 SF (1,012 SF REQUIRED)
- BUILDING LENGTH: 449-10"
- WINDOW LENGTH: 226'-0" (225'-0" REQUIRED)
- EAST
- GROUND LEVEL WALL AREA: 4,185 SF
- GROUND LEVEL WINDOW AREA: 204 SF (1,046 SF REQUIRED)
- BUILDING LENGTH: 465'-0"
- WINDOW LENGTH: 28'-6" (232'-6" REQUIRED)

C2 PROMOTE QUALITY AND PERMANENCE IN DEVELOPMENT

The store expansion consists of a variety of exterior materials, including brick and cast stone masonry, metal panel systems and wire guardrails, steel canopies, aluminum storefront window systems, and scored concrete walkways, to create building facades that provide a range of visual experiences and promote a sense of permanence. These materials are proposed to be integrated into the existing store façades in order to create a cohesive relationship between the old and new segments.

C3 RESPECT ARCHITECTURAL INTEGRITY

The existing store does not feature façade elements desired to be extended to West Burnside Street. In response, the design for the expansion incorporates elements and materials prevalent within the surrounding area, integrating these into both the old and new segments of the store to encourage an architectural integrity respectful of the neighborhood.

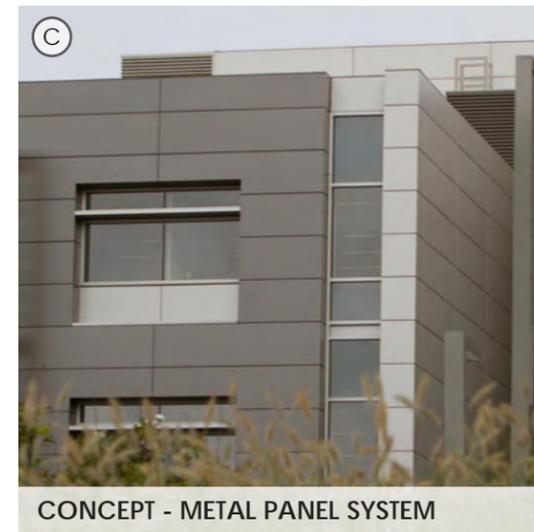


SIDEWALK AT SOUTHEAST CORNER

ETCHED STAINLESS STEEL PANEL, SEE SHEET 30



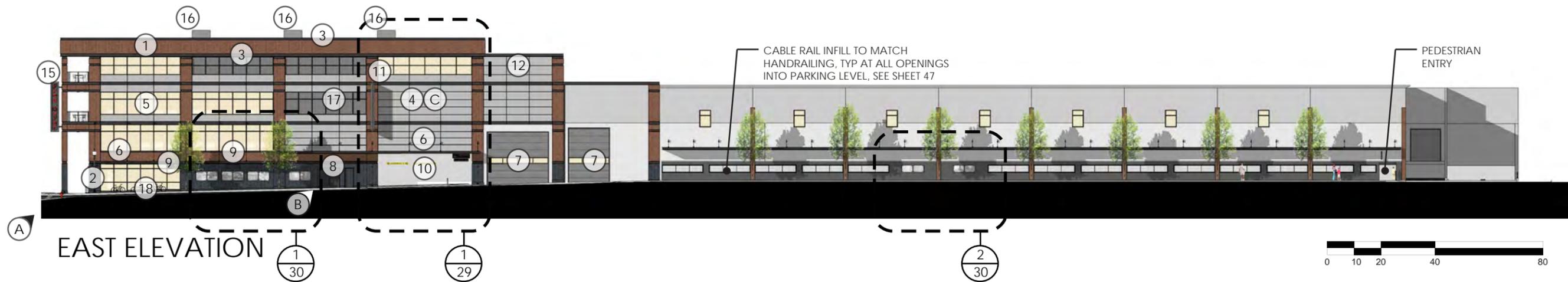
SIGN AT PARKING LEVEL ENTRANCE



CONCEPT - METAL PANEL SYSTEM



WEST ELEVATION



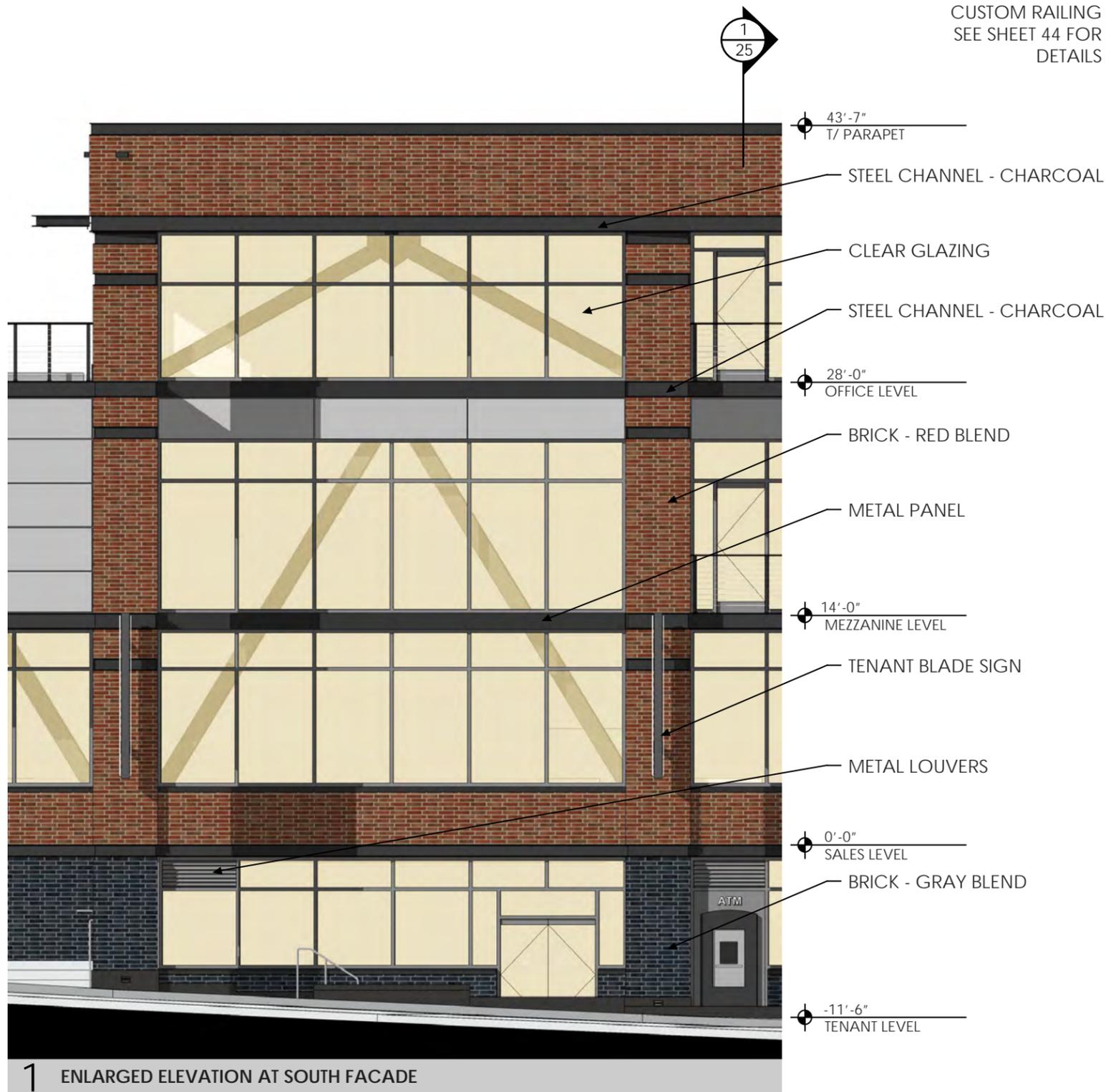
EAST ELEVATION

MATERIALS KEYNOTES

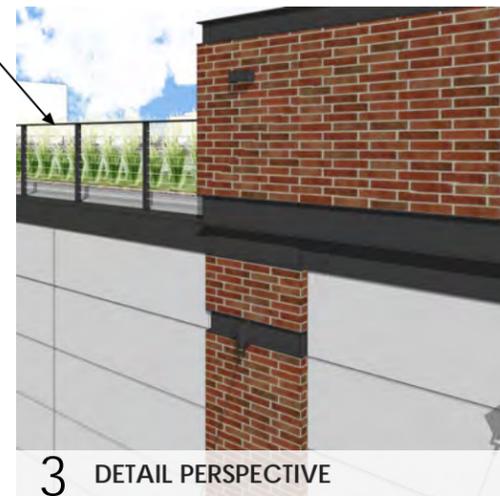
- | | | |
|-------------------------------------|------------------------------------|------------------------------|
| 1 - BRICK (NORMAN RED BLEND) | 9 - METAL LOUVERS | 17 - SPANDREL GLAZING SYSTEM |
| 2 - BRICK (NORMAN GREY BLEND) | 10 - PARKING LEVEL ENTRANCE | 18 - BICYCLE PARKING |
| 3 - STEEL CHANNEL | 11 - PARKING SIGNAGE | |
| 4 - METAL PANEL SYSTEM | 12 - NEW PARAPET | |
| 5 - ALUM. STOREFRONT GLAZING SYSTEM | 13 - CABLE GUARDRAIL | |
| 6 - STEEL CANOPY | 14 - SKYLIGHTS (BEYOND) | |
| 7 - METAL COILING DOOR W/ WINDOWS | 15 - FM SIGNAGE | |
| 8 - HM DOOR | 16 - ROOF TOP MECHANICAL EQUIPMENT | |
| | | |
| | | |

C8 DIFFERENTIATE THE SIDEWALK LEVEL OF BUILDINGS

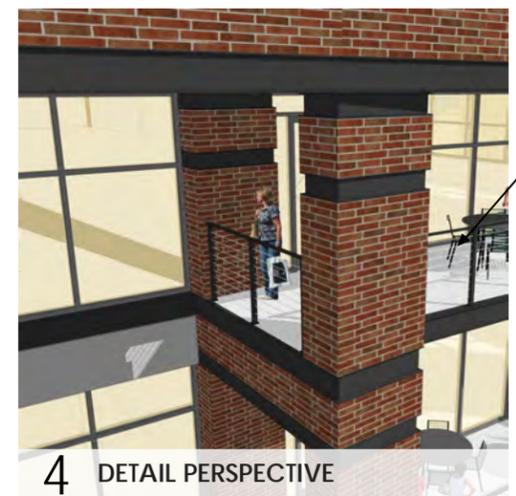
Differentiation in building materials and elements in the design helps create a human scale at the sidewalk level. For instance, the bays between the structural columns are infilled with large expanses of glass at the sidewalk level to provide pedestrians a visual connection to the active interior spaces. In contrast, the upper levels of the expansion consist of metal paneling and the upper levels of the existing store consist of painted concrete.



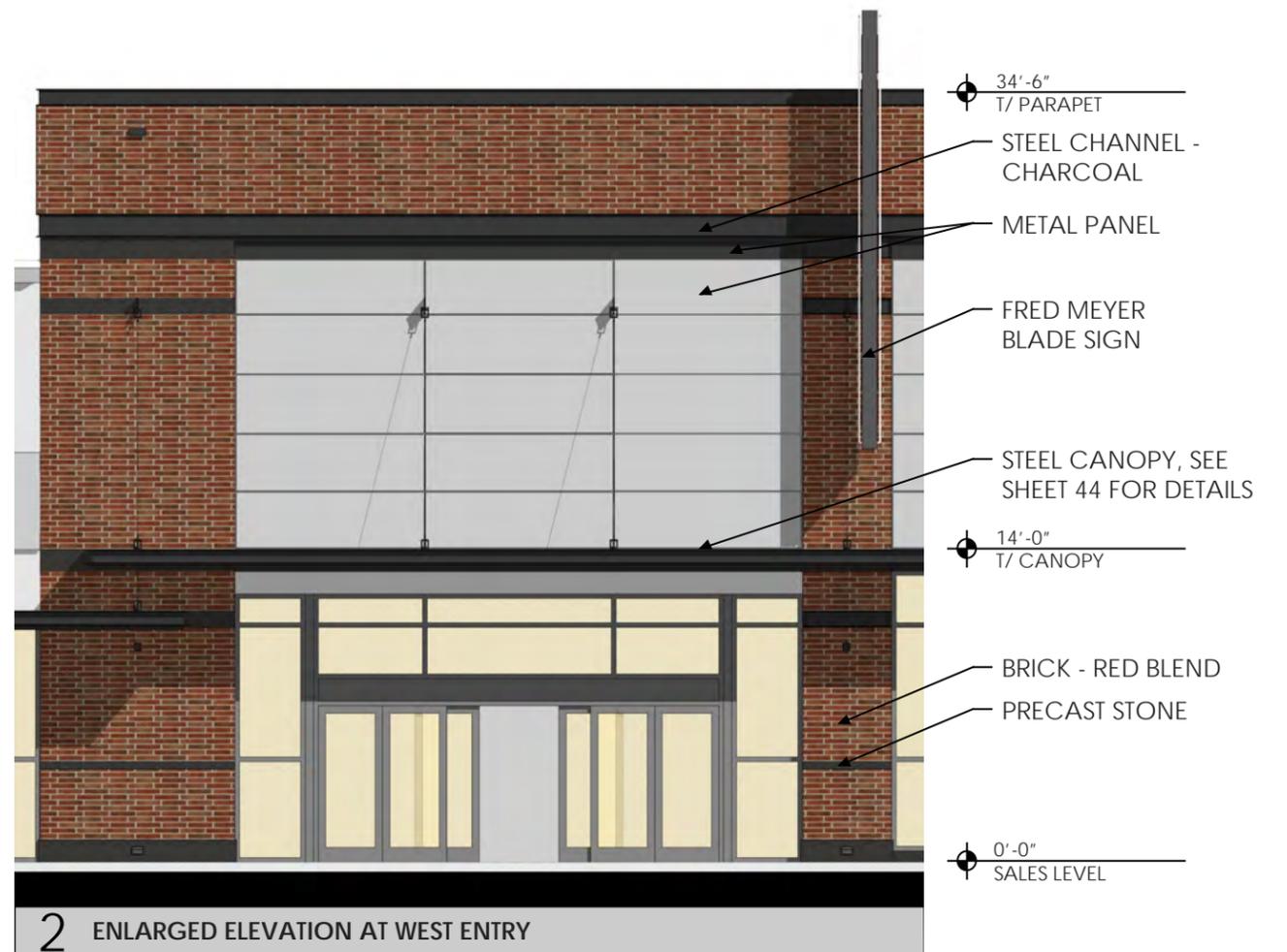
SCALE: 1/8" = 1'-0"



3 DETAIL PERSPECTIVE



4 DETAIL PERSPECTIVE



SCALE: 1/8" = 1'-0"

1 ENLARGED ELEVATION AT SOUTH FACADE

2 ENLARGED ELEVATION AT WEST ENTRY

CUSTOM RAILING, SEE SHEET 44 FOR DETAILS

43'-9"
T/ PARAPET

STEEL CHANNEL - CHARCOAL

METAL PANEL

28'-0"
OFFICE LEVEL

PARKING BLADE SIGN

METAL PANEL

BRICK - RED BLEND

14'-0"
MEZZANINE LEVEL

STEEL CANOPY, SEE SHEET 44 FOR DETAILS

"WELCOME TO FRED MEYER" SIGN (1'-10"x7'-10")

BIKE PARKING SIGN

0'-0"
SALES LEVEL

BRICK - GRAY BLEND

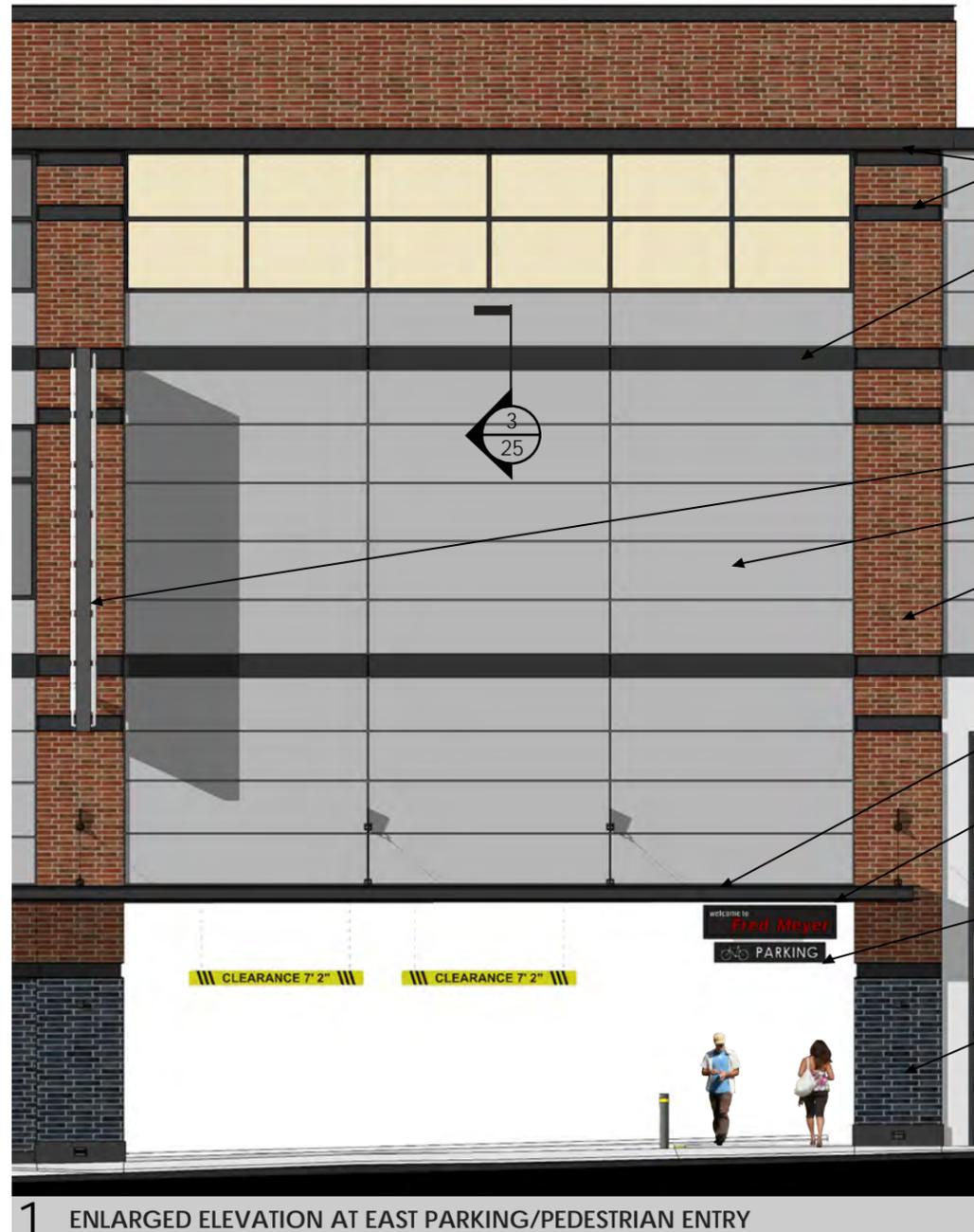
-11'-6"
TENANT LEVEL



3 DETAIL PERSPECTIVE

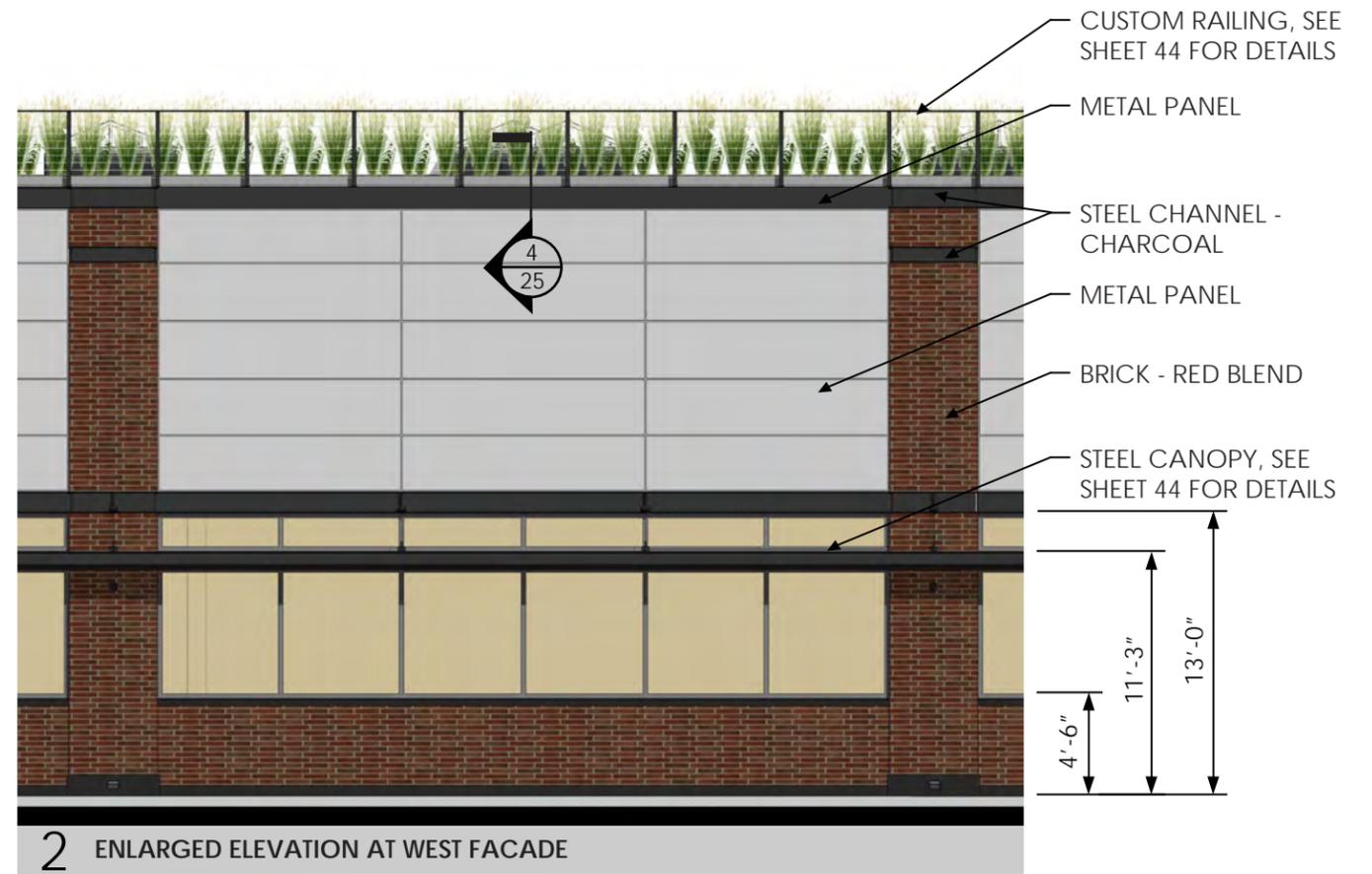


4 DETAIL PERSPECTIVE



1 ENLARGED ELEVATION AT EAST PARKING/PEDESTRIAN ENTRY

SCALE: 1/8" = 1'-0"



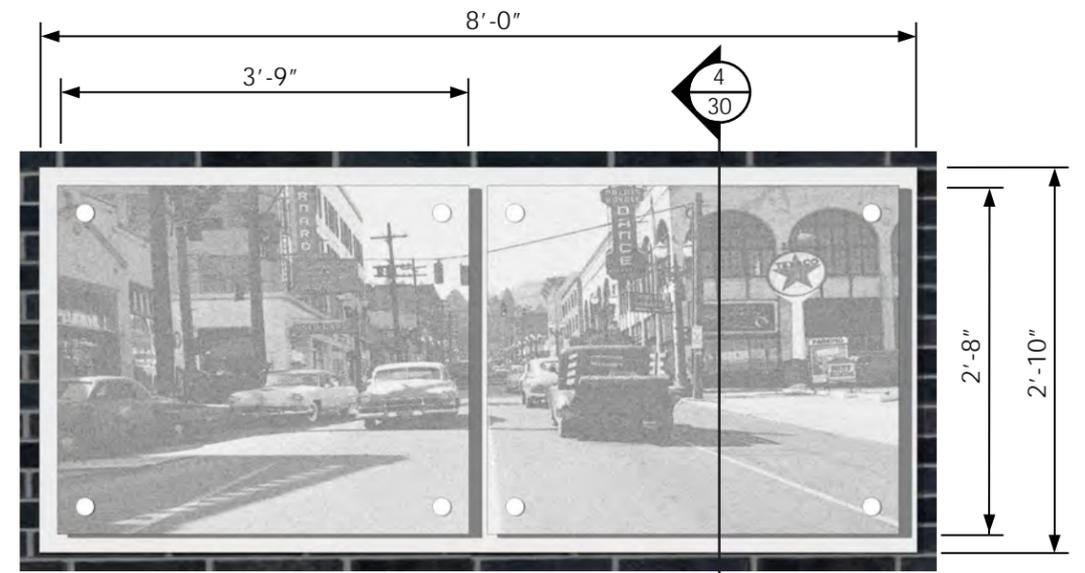
2 ENLARGED ELEVATION AT WEST FACADE

SCALE: 1/8" = 1'-0"

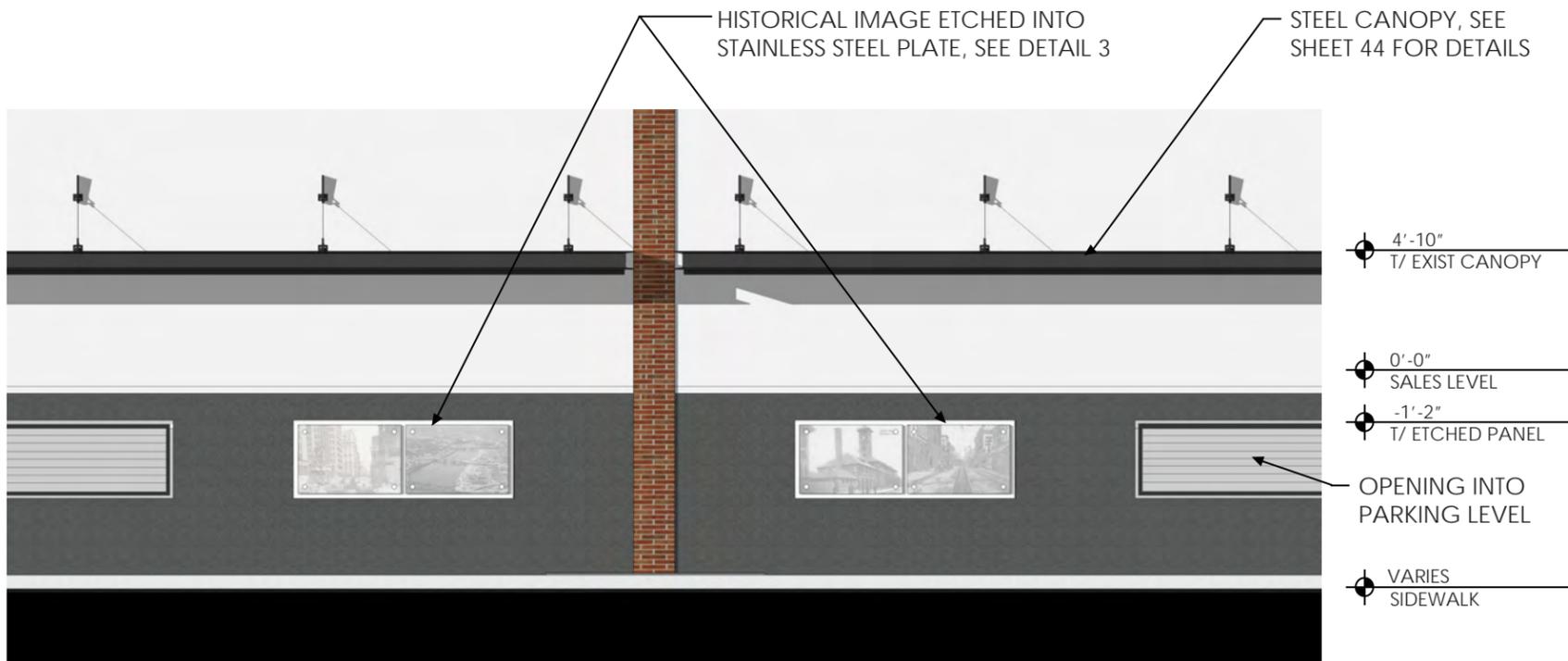


1 PARTIAL EAST ELEVATION
NTS

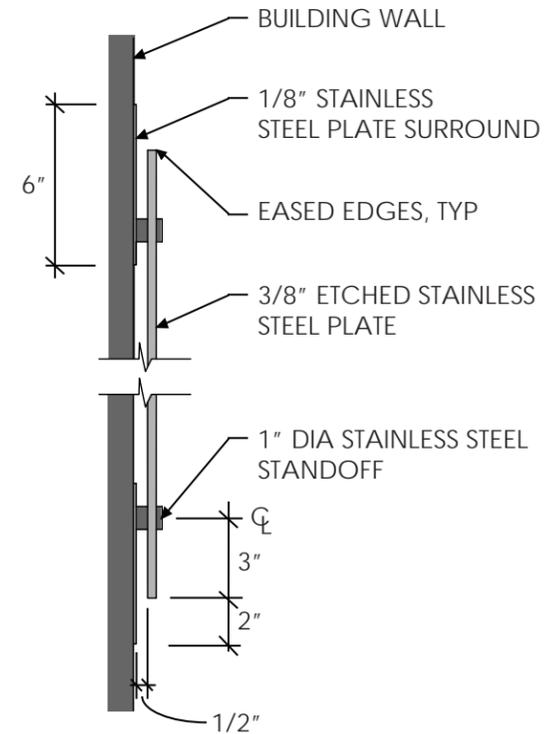
HISTORICAL IMAGE ETCHED INTO STAINLESS STEEL PLATE, SEE DETAIL 3



3 DETAIL ELEVATION OF ETCHED PANEL
NTS



2 PARTIAL EAST ELEVATION
NTS



4 SECTION AT ETCHED PANEL
NTS

DESCRIPTION:
ON THE EAST ELEVATION ARTWORK IS PROPOSED AT THE PEDESTRIAN SCALE TO MITIGATE FOR THE DEFICIENCY OF GROUND FLOOR WINDOWS. CONCEPTUALLY THE ARTWORK WILL CONSIST OF HISTORICAL IMAGES, ETCHED INTO 3/8" STAINLESS STEEL PLATE WITH EASED EDGES. THE ARTWORK WILL BE MOUNTED TO THE BUILDING WALL AND BACKED BY A 1/8" STAINLESS STEEL PLATE. THE GRAPHICS SHOWN ARE EXAMPLES OF THE TYPES OF IMAGES WHICH WOULD BE ETCHED INTO THE STEEL PLATES. FRED MEYER INTENDS TO SOLICIT SUGGESTIONS FROM THE NEIGHBORHOOD ASSOCIATION FOR POTENTIAL IMAGES PRIOR TO FRED MEYER MAKING A FINAL SELECTION.



STADIUM
Fred Meyer

5

PERSPECTIVES



DAY PERSPECTIVE



NIGHT PERSPECTIVE

A3 RESPECT THE PORTLAND BLOCK STRUCTURE

Variations in street and block configurations, natural features, building types and architecture contribute to the distinct character of the neighborhood and the Central City. By redeveloping and extending the building towards West Burnside the project will meet community objectives by enhancing the public realm and the urban form by encouraging a more active and multifunctional streetscape and block pattern. The massing of the project in conjunction with the adjacent buildings creates volume within the block and continues the pattern of development.

A4 USE UNIFYING ELEMENTS

The project utilizes materials and elements consistent within the Central City, the surrounding neighborhood and along West Burnside Street. The building is proposed to be constructed of brick masonry—classic materials that provide a contextual relationship between the proposed store expansion and the historic character of the neighborhood. Other elements, such as metal panels, aluminum storefront window systems, and steel canopies, respond to and complement recent developments along West Burnside.

A5 ENHANCE, EMBELLISH, AND IDENTIFY AREAS

A5.3 ENHANCE WEST BURNSIDE STREET BY EXTENDING AND IMPROVING ITS BOULEVARD TREATMENT AND ITS ENVIRONMENT WEST OF THE PARK BLOCKS.

The store expansion has been designed with the goal of creating a structure that "reaches out to" and "embraces" the public realm both through overall physical form as well as through small-scale details. The facade along West Burnside consists of a continuous colonnade with balconies and canopies that protrude over a portion of the pedestrian space. The colonnade design creates a linear focus along West Burnside that increases opportunities for safe and pedestrian-friendly activities within a widened corridor. The canopies and balconies give the public realm a pedestrian scale in addition to providing ample weather protection such as rain cover and shade. Beneath the colonnade, a terrace consisting of ramps and stairways circumvent the grade change along West Burnside Street and provides access to the punctuating store and tenant entrances as well as outdoor amenities, such as the bus stop and outdoor seating area. The pedestrian experience and boulevard treatment is further enhanced by the integration of landscape plantings and street furniture. Raised planters located along the terrace provide pedestrians the ability to sit and enjoy the space. A widened sidewalk, plantings and street trees within the furnished zone provide increased separation and buffer pedestrians from the heavy vehicular traffic of West Burnside Street.

A7 ESTABLISH AND MAINTAIN A SENSE OF URBAN ENCLOSURE

The design of the store expansion is an urban response within an urban context. The addition is built out to the minimum setbacks on each street frontage providing a strong built edge that is common along West Burnside Street. The scale and proportion of the building creates a balanced street volume. The colonnade and terrace design provides an urban edge to West Burnside Street and reinforces the urban character by incorporating a variety of projecting and recessed elements (balconies, canopies, raised planters and signage) and integrating urban elements in response to site characteristics (change in grade, low seating walls and planting strips).

A8 CONTRIBUTE TO A VIBRANT STREETScape

The streetscape is designed to foster human activity and interaction. The colonnade and terrace design creates protected and flexible places at the sidewalk level that encourage pedestrian activity, provide attractive visual access for pedestrians and complement the urban environment.



DAY PERSPECTIVE



NIGHT PERSPECTIVE

B1 REINFORCE AND ENHANCE THE PEDESTRIAN SYSTEM

A variety of experiences are incorporated into the pedestrian realm. An outdoor seating area, bus stop and raised landscape planters have been incorporated into the design of the terrace and the sidewalk corridor. These elements provide opportunities for stopping, resting or simply enjoying the sidewalk experience.

B2 PROTECT THE PEDESTRIAN

The pedestrian corridor consists of a widened sidewalk, planting strip and street trees within the furnishing zone. In addition, the colonnade, raised planters and terrace provide separation and buffer pedestrians from the heavy vehicular traffic of West Burnside Street. Building signage and both pedestrian and building lighting will direct pedestrians safely and conveniently to building entries and outdoor amenities.

B4 PROVIDE STOPPING AND VIEWING PLACES

The outdoor seating area located on the terrace and the raised planters provide opportunities for pedestrians to stop, view, socialize and rest. These seating areas are set back from the sidewalk and sheltered from the weather by the building's overhang.

B5 MAKE PLAZAS, PARKS, AND OPEN SPACES SUCCESSFUL

Focal points such as the terrace and building entries are oriented toward West Burnside Street and add character to the public realm.

C1 ENHANCE VIEW OPPORTUNITES

Several opportunities for views have been integrated into the design of the store expansion. Views of downtown, Jeld Wen Field, the West Hills and the West Burnside corridor are accessible from multiple vantage points and from both indoor and outdoor locations. Elements such as the terrace along the street level, balconies on the upper floor levels and the rooftop patios attract human activity and present viewing opportunities. The expansive amount of glazing proposed along the exterior of the sales floor and upper floor levels offers patrons and employees opportunities to these views from inside the store.

C6 DEVELOP TRANSITIONS BETWEEN BUILDINGS AND PUBLIC SPACES

By stepping the building back and integrating a continuous colonnade with balconies and canopies over the public realm, the design creates a human-scaled transition for pedestrians moving from the movement zone to other areas, such as the terrace and raised planters, which foster opportunities for gathering and seating.

C12 INTEGRATE EXTERIOR LIGHTING

Exterior lighting is integrated into the building façade and along the street level. Lighting fixtures are oriented to emphasize the rhythm of the structural columns. Consistent lighting at the sidewalk level improves the pedestrian environment, enhancing safety.

C13 INTEGRATE SIGNS

The signage and graphics consist of blade signs, building/tenant signs and directional signs that feature a common theme, fonts and colors. Main blade signs ("Fred Meyer") are located at upper elevations and signify store entrances. A parking entrance sign ("Parking") sign is located near the parking garage to signify the entrance to the parking garage. Building and tenant signs are located at lower elevations to provide human-scale to the pedestrian environment as well as signify entries. All signage is proposed to be internally illuminated and placed so that they complement the skyline by day and night.



VIEW FROM ACROSS BURNSIDE



VIEW OF SOUTHEAST CORNER FROM ACROSS 20TH PLACE

B6 DEVELOP WEATHER PROTECTION

A continuous colonnade with balconies and canopies protrude over portions of the pedestrian space. The canopies and balconies give the public realm a pedestrian scale in addition to providing ample weather protection such as rain cover and shade.

C1.2 INTEGRATE SIGNS AND AWNINGS TO BE COMPLEMENTARY TO BUILDINGS ARCHITECTURE

A compilation of new blade signage and building signage have been designed to be complementary and in character with the building. Future tenant signage will incorporate a consistent design.

C4 COMPLEMENT THE CONTEXT OF EXISTING BUILDINGS

The proposed store expansion incorporates design characteristics reflected in both the surrounding area as well as other areas within the Central City. The overall building proportion, scale and exterior style are complementary of the urban fabric established along West Burnside Street. The uses of the colonnade and terrace elements not only provide an urban response to urban challenges but are also reminiscent of similar approaches within the Central City.

C5 DESIGN FOR COHERENCY

The proposed store expansion uses an integrated system of materials that includes brick and cast stone masonry, aluminum storefront window systems, metal panel systems and steel canopies. These materials are proposed to be integrated into the existing store façades in order to create a cohesive relationship between the old and new segments of the building. The materials are cohesively integrated into the varied building volumes in order to create a common building form at both the street level and the upper floor levels.

C7 DESIGN CORNERS THAT BUILD ACTIVE INTERSECTIONS

Both intersections along West Burnside Street are designed to activate their respective corners. The corner at NW 20th Avenue and West Burnside Street consists of two retail tenant spaces that provide direct entry into those spaces from the adjacent sidewalk. Similarly, the corner at NW 20th Place and West Burnside Street consists of a store entrance that provides a direct connection to pedestrians. Signage, pedestrian entrances and canopies are used to highlight these building corners and active connections.



6

LANDSCAPE



A5.5 INCORPORATE WATER FEATURES OR WATER THEMES WHICH ENHANCE THE QUALITY, CHARACTER, AND IMAGE OF THE GOOSE HOLLOW DISTRICT

A5.6 INCORPORATE WORKS OF ART OR OTHER SPECIAL DESIGN FEATURES THAT INCREASE WHICH ENHANCE THE PUBLIC ENJOYMENT OF THE DISTRICT

Located between the stairs and the public sidewalk, a raised stormwater planter is incorporated into the pedestrian corridor. A metal scupper/spout fixture on the side of terrace wall will provide pedestrians an opportunity to view rainwater from the canopies being discharged into the landscape planter.



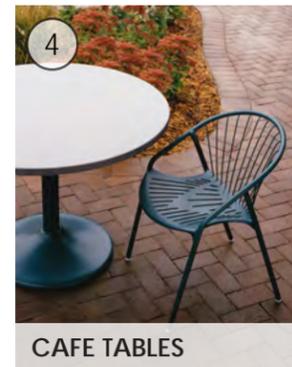
1
LINEAR PLANTERS



2
STORMWATER



3
BENCHES



4
CAFE TABLES



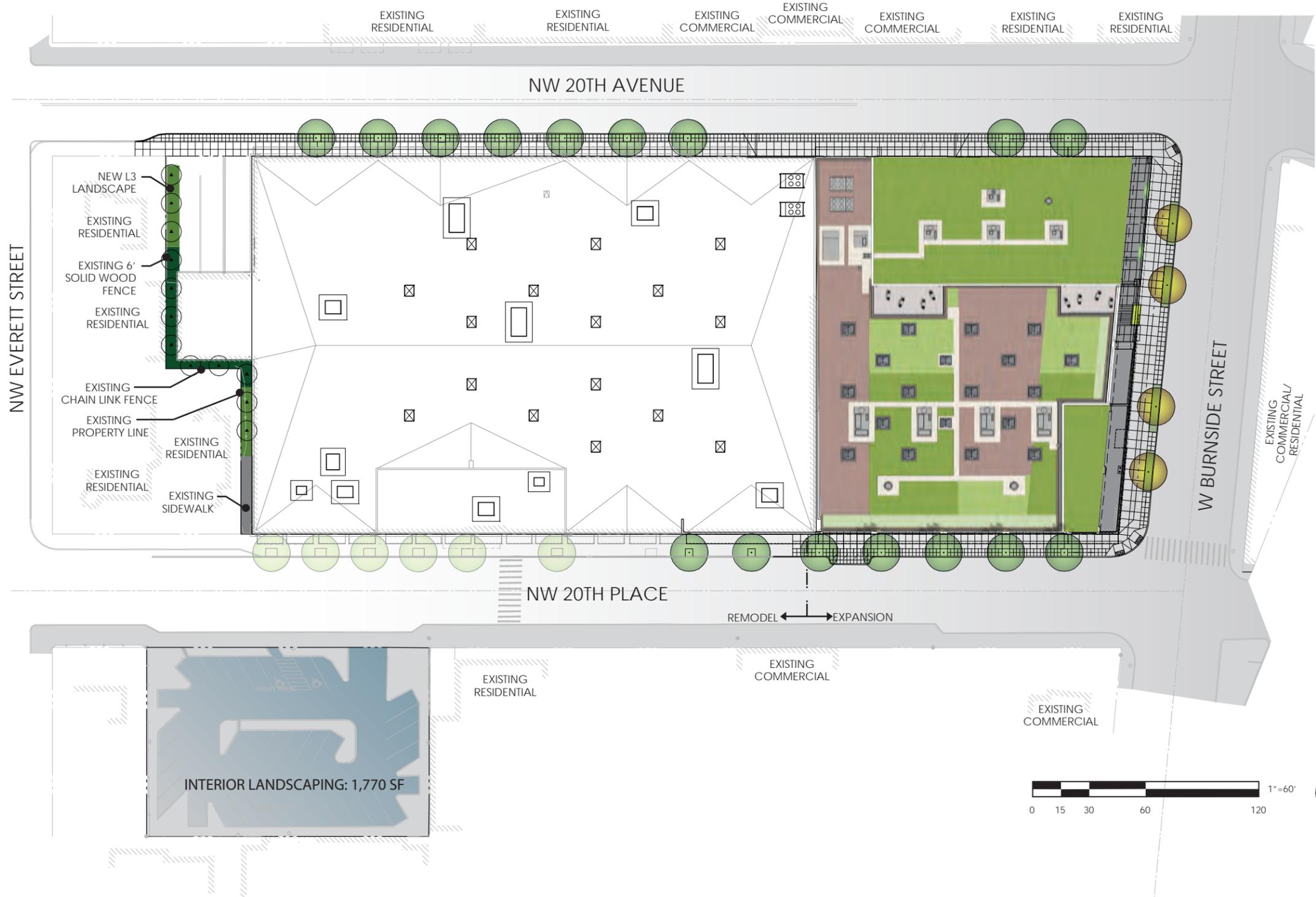
5
CITY APPROVED TREE GRATE



6
CITY APPROVED STREET TREE



7
GREEN ROOF - SEE SHEET 38



LEGEND

- STREET TREES**
- ACER GRANDIDENTATUM 'SCHMIDT'
 - ROCKY MOUNTAIN GLOW MAPLE

- GINKGO BILOBA 'SARATOGA'
- SARATOGA GINKGO

- EXISTING STREET TREE

L-3 PLANTINGS

- ACER GRISEUM
- PAPERBARK MAPLE

- MYRICA CALIFORNICA
- PACIFIC WAX MYRTLE

- LIGUSTRUM JAPONICUM 'TEXANUM'
- JAPANESE PRIVET

STORMWATER PLANTER

- JUNCUS PATENS
- SPREADING RUSH

BURNSIDE PLANTER STRIPS

- ILEX CRENATA
- JAPANESE HOLLY
- ARCTOSTAPHYLUS UVA URSI
- KINNIKICK

LINEAR PLANTERS

- BUXUX SEPERVIRENS 'SUFFRUTICOSA'
- EDGING BOXWOOD
- PHORMIUM TENAX 'DARK DELIGHT'
- NEW ZEALAND FLAX
- AJUGA REPTANS
- CARPET BUGLE
- VINCA MINOR
- DWARF PERIWINKLE

GREEN ROOF

The green roof system is a design-build element of the project. Working with a green roof specialist, the design team will guide plant selection and layout to implement the roof garden vision. The roof garden concept is organized by large color blocks that echo the building façade. The brick red columns lay horizontally through the garden with select varieties of sedums and succulents that provide an overall uniform blended red color. Alternating block geometries of grays and greens echo the metal panels of the new façade. Accents of taller grasses provide edge definition and a sense of scale. Sun angle, heat reflection and shading opportunities will drive final variety selection.

PROPOSED GREEN ROOF PLANTINGS

- SEDUM ACRE
- SEDUM ALBUM
- SEDUM KAMTSCHATICUM
- SEDUM RUPESTRE
- SEDUM SPURIUM
- SEDUM MIDDENDORFFIANUM DIFFUSUM
- SEDUM STEFCO
- SPOROBOLUS HETEROLEPIS
- CALAMAGROSTIS ACUTIFLORA
- KOELERIA GLAUCA
- FESTUCA IDAHOENSIS
- DESCHAMPSIA FLEXUOSA

A2 EMPHASIZE PORTLAND THEMES

Portland is at the forefront of the green building movement and has established its own eco-character through the ambitions and support of its citizens and policy makers. The project celebrates this by incorporating a large roof garden (eco-roof) consisting of an extensive system of colorful sedums and tall-grasses to be viewed and enjoyed from two rooftop patios. The eco-roof will offer much needed habitat for birds and insects, improve stormwater management as well as provide greenspace within an urban context.

C11 INTEGRATE ROOFS AND USE ROOFTOPS

The rooftop area accommodates a variety of uses, both those necessary from a functional standpoint those that provide enjoyment for users. Two rooftop terraces, accessible from the upper floor level, offer views of downtown, the West Hills and the rooftop garden. Mechanical equipment and the associated access walkways have been sized and placed to not impede on views or be seen from the adjacent street levels.



SEDUMS



PERENNIALS/GRASSES



ROOF TOP PATIO

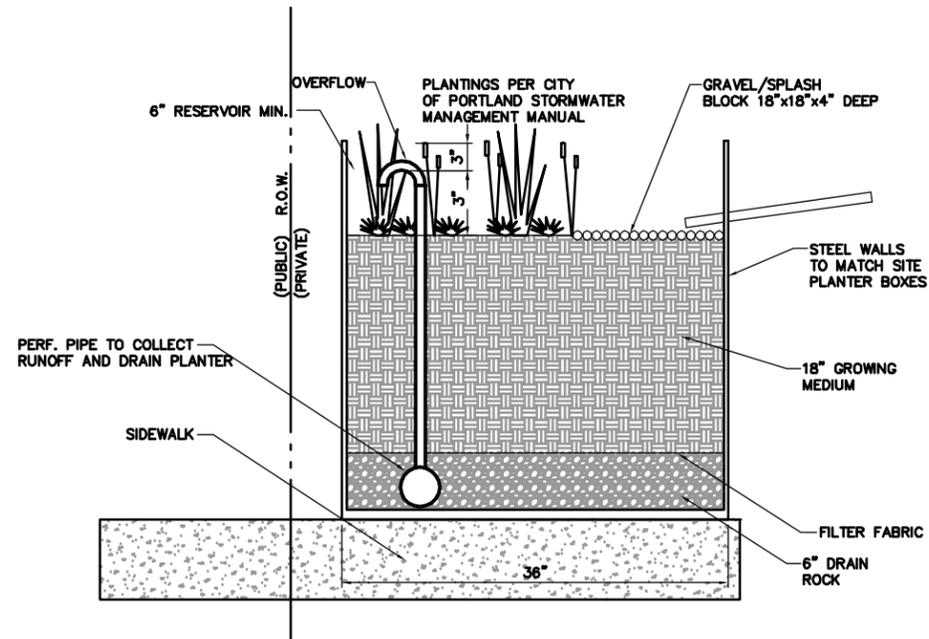
NW 20TH PL

NW 20TH AVE

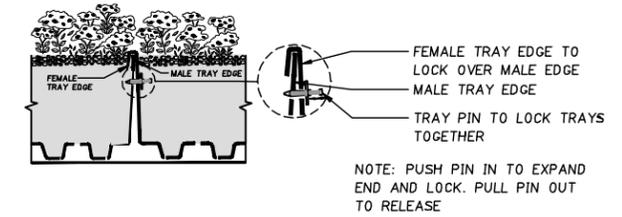


W BURNSIDE ST

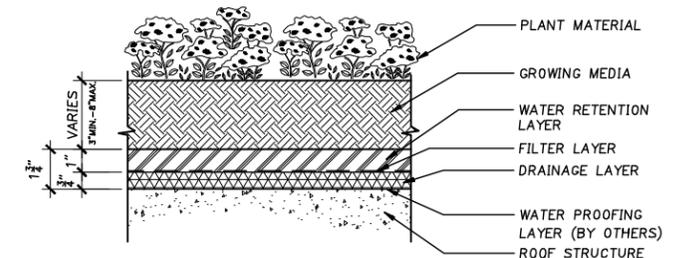
*NOTE: SEE SHEET 39 FOR STORMWATER DETAILS



1 STORMWATER PLANTER BOX
N.T.S.

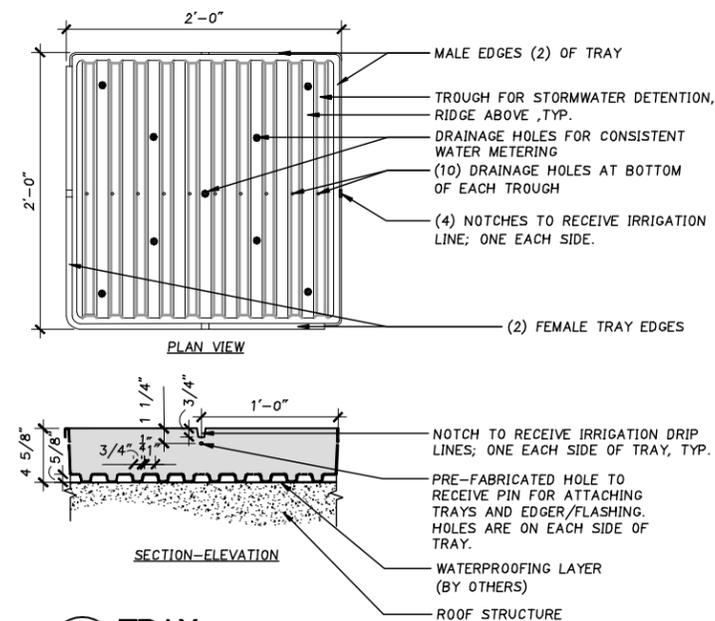


3 TRAY CONNECTION
DETAIL 1 1/2"=1'-0"

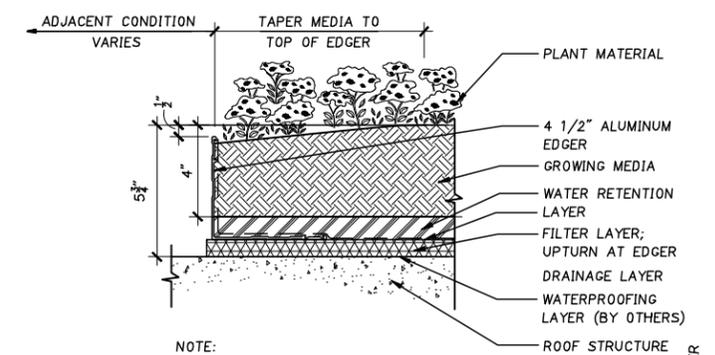


NOTE:
1. SYSTEM COMPONENTS ARE SHOWN AT NOMINAL SIZE, COMPRESSION WILL OCCUR AT RETENTION LAYER BASED ON AMOUNT (HEIGHT/VOLUME) OF GROWING MEDIA PLACED ON SYSTEM. MAXIMUM COMPRESSION 1/4" ±.

STANDARD MULTILAYER
DETAIL 1 1/2"=1'-0"



2 TRAY
DETAIL 3/4"=1'-0"



NOTE:
1. SYSTEM COMPONENTS ARE SHOWN AT NOMINAL SIZE, COMPRESSION WILL OCCUR AT RETENTION LAYER BASED ON AMOUNT (HEIGHT/VOLUME) OF GROWING MEDIA PLACED ON SYSTEM. MAXIMUM COMPRESSION 1/4" ±.

5 4.5" ALUM. EDGER AT MULTILAYER
DETAIL 1 1/2"=1'-0"



STADIUM
Fred Meyer

APPENDIX

--- LINE AT 9'-0" ABOVE FINISH GRADE (AFG)
 - - - - MAXIMUM SILL HEIGHT OF GROUND FLOOR WINDOWS
 (4'-0" AFG)

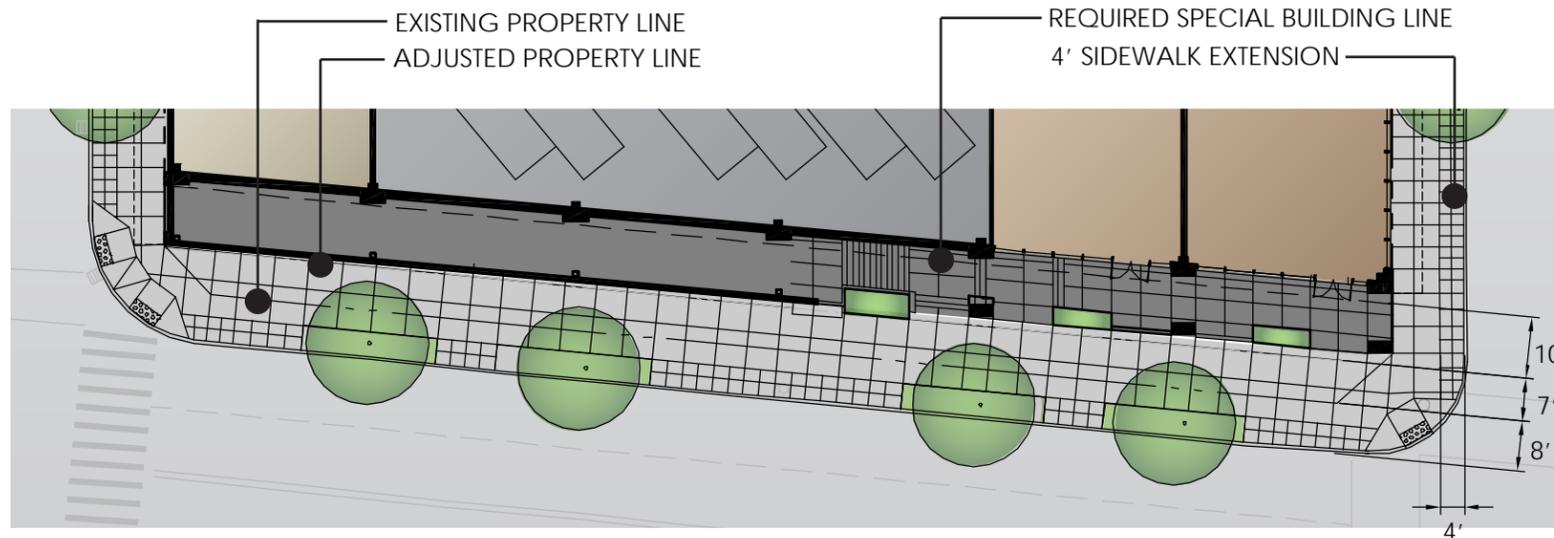
EAST ELEVATION
 GROUND LEVEL WALL AREA: 4,185 SF
 GROUND LEVEL WINDOW AREA: 204 SF (1,046 SF REQUIRED)
 BUILDING LENGTH: 465'-0"
 WINDOW LENGTH: 28'-6" (232'-6" REQUIRED)



1

1 Modification 1: 33.510.220.B and 33.130.230.B.2 and 3 - Ground Floor Windows

Due to the challenges presented by the existing slopes and trying to preserve the daylight basement level parking, compliance with the ground floor window requirement both in length and area will be difficult to meet along the east elevation. According to code section 33.510.220.C., projects having more than 50% of their ground level space in uses that are not conducive to windows, such as parking, may request a modification to the ground floor window requirement through design review. Because 56% of the ground floor is devoted to the parking garage, a modification to the ground floor window requirement is requested.



2 Modification 2: 33.510.215.D.2 - Required Building Lines

The colonnade and terrace design along West Burnside is proposed to project into the special building line. According to comments provided at the Design Advice Request hearings on July 7, 2011 and August 4, 2011, the Design Commission is supportive of projections into the special building line that benefit the public realm. The primary goal of the proposed colonnade design is to provide a safe and convenient route to in order to circumvent the large change in grade along West Burnside Street and to enhance the overall pedestrian experience. The colonnade and terrace design offers a variety of elements such as balconies, canopies, and raised planters that encourage gathering and seating in addition to fostering an active streetscape.

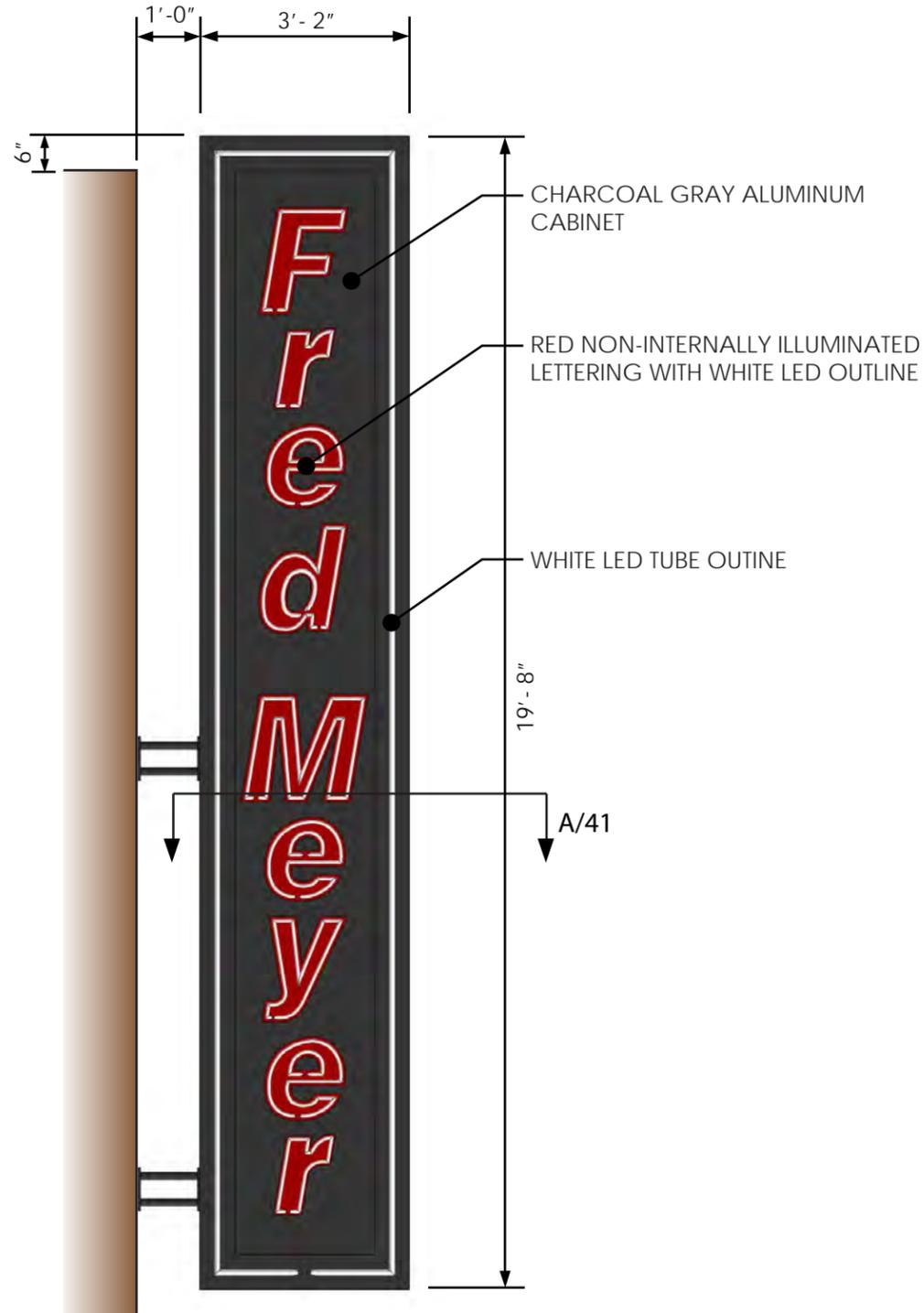
2

Modification 3: 32.32.020.A - General Standards and Sign Features

To further the thematic style of the signage for the store, each of the blade signs are proposed to be larger than 30 SF. See Sheet 42 for signage information.

Modification 4: 33.266.220.2.A.b - Short-term Bicycle Parking Location

Due to the store being built up to the property line it is not possible to provide all of the required short-term bicycle parking on site and in a way that complies with all of the bicycle location standards. Therefore, the short-term bicycle parking is proposed to be located off site and within the pedestrian corridor.



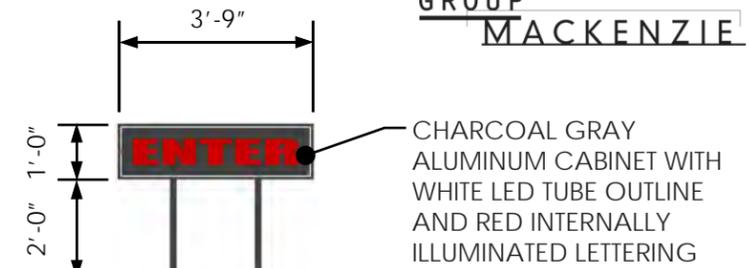
MAIN BLADE
SIGN (62 SF)

*MATCHING BLADE SIGN AT WEST ENTRY (62 SF)

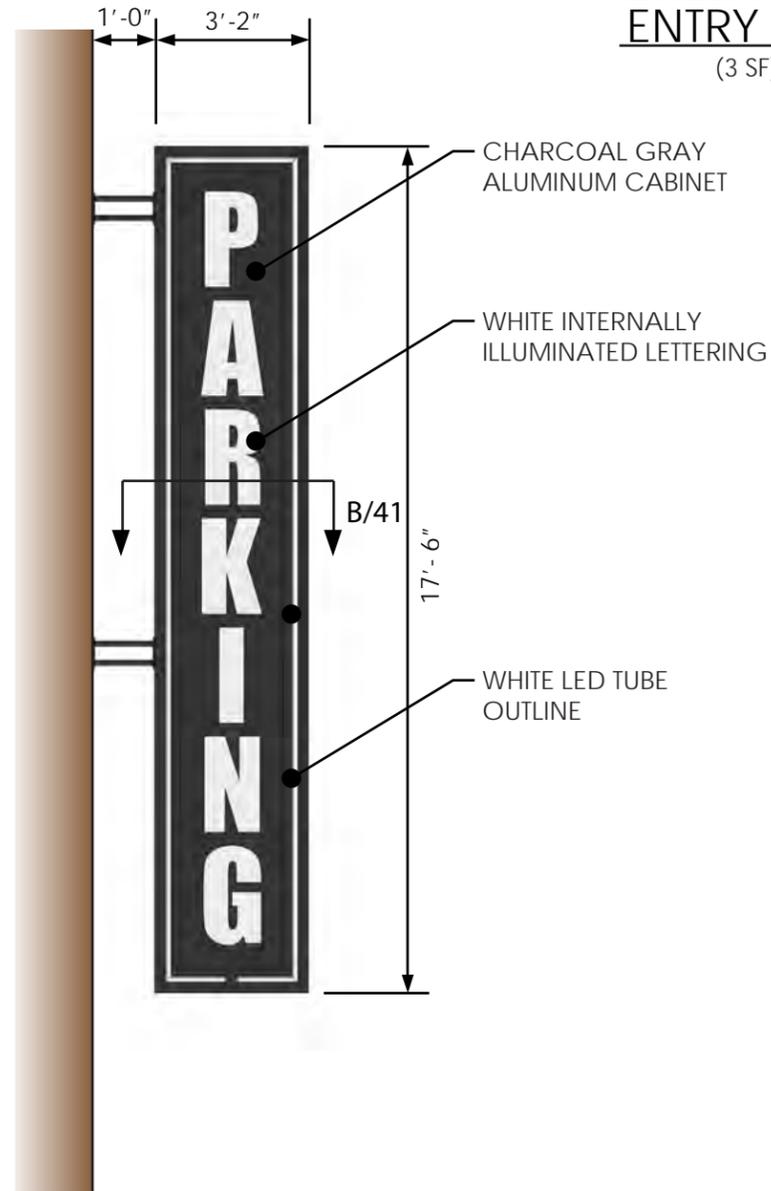
*MATCHING BLADE SIGN AT SE CORNER = 2'-10" WIDE x 17'-6" TALL (49.5 SF)



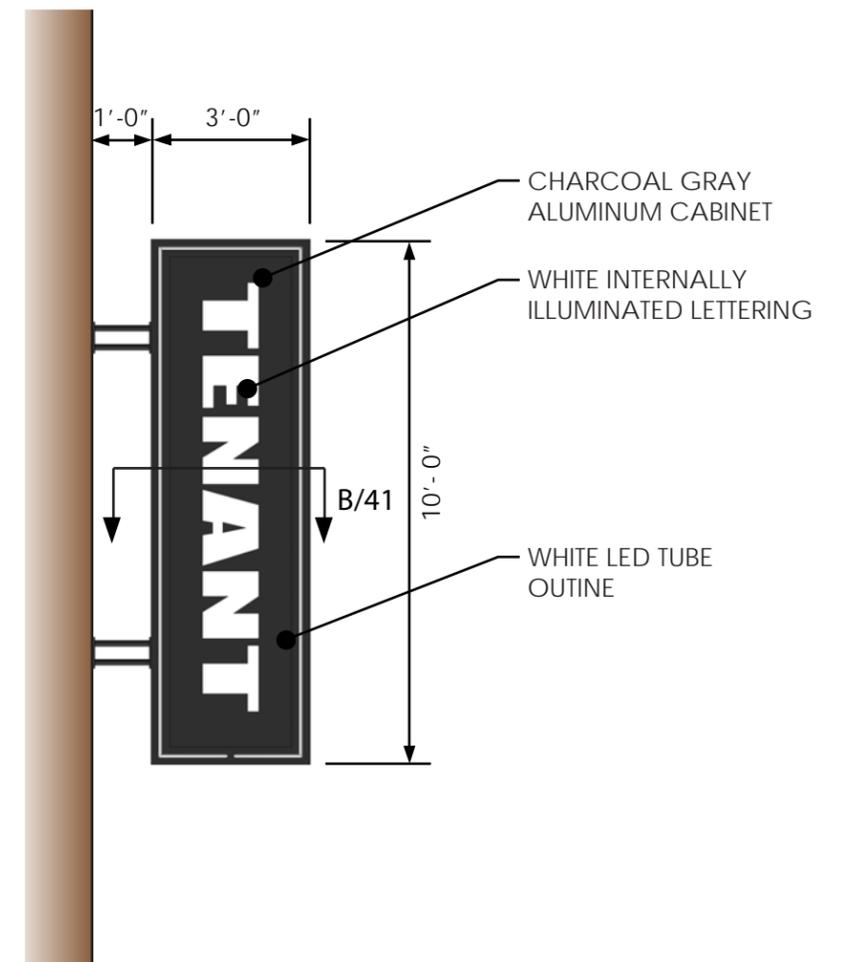
NORTH PEDESTRIAN
ENTRY SIGN
(3 SF)



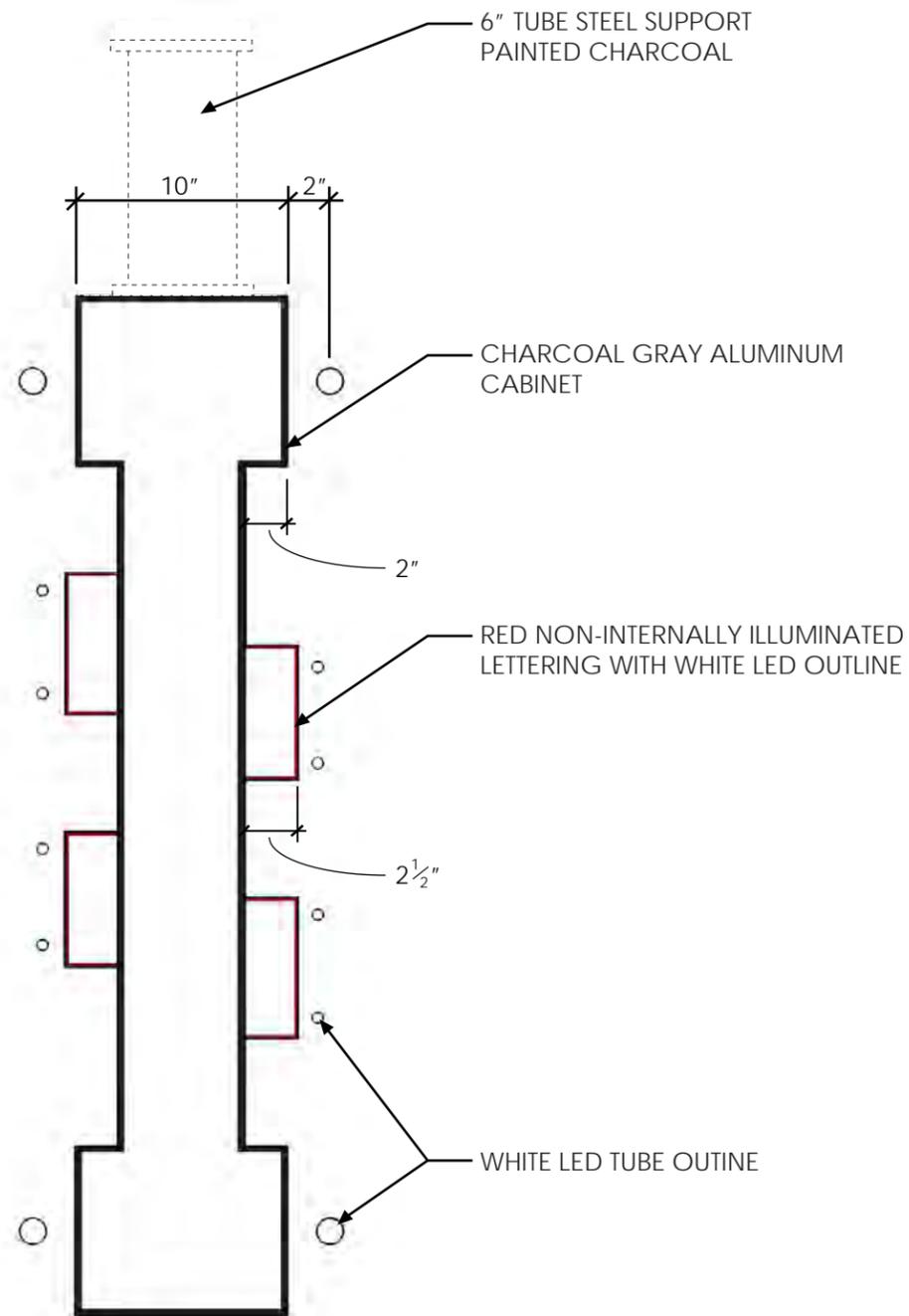
NORTH PARKING
SIGN
(4 SF)



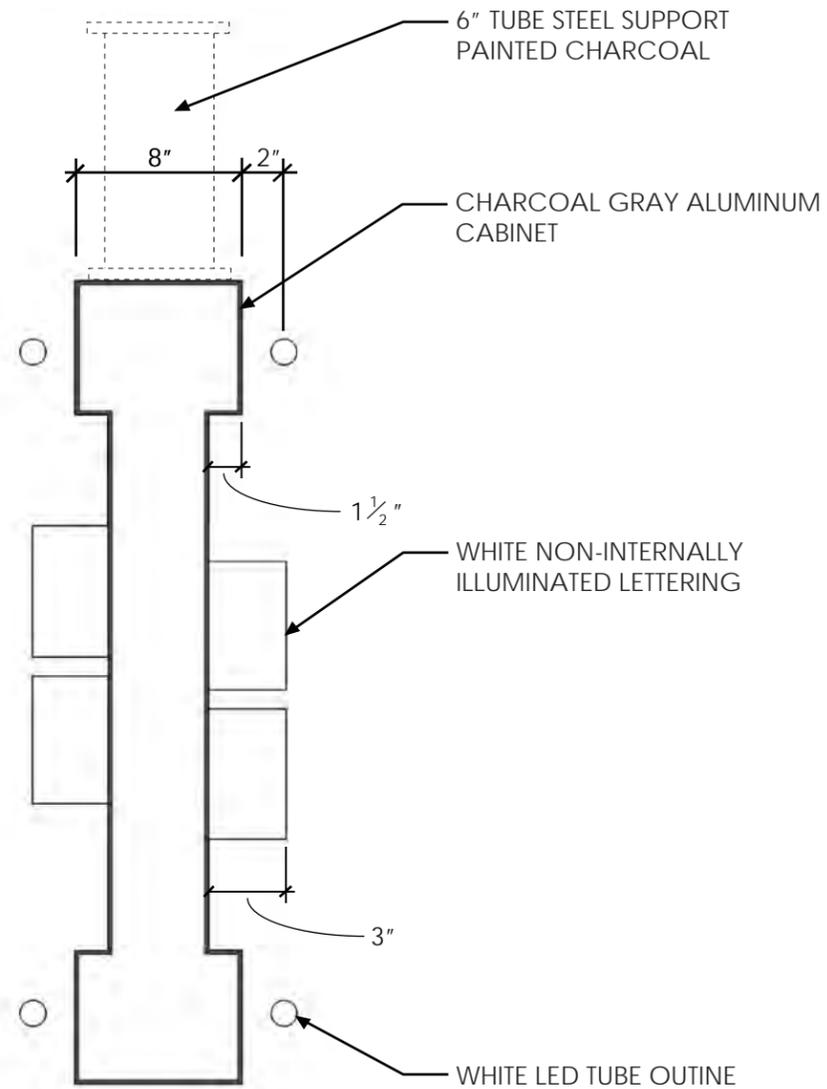
PARKING
ENTRANCE
SIGN
(56 SF)



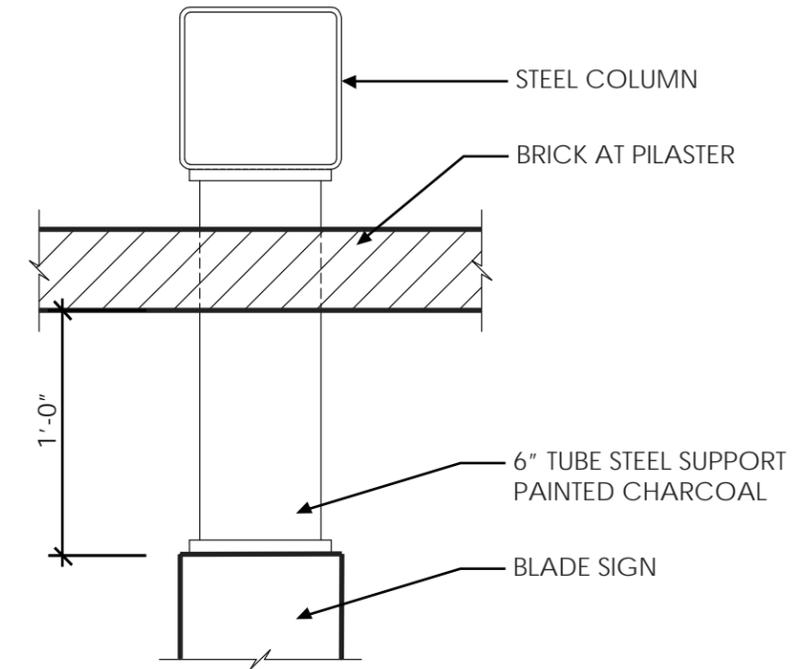
TENANT
BLADE
SIGN
(30 SF)



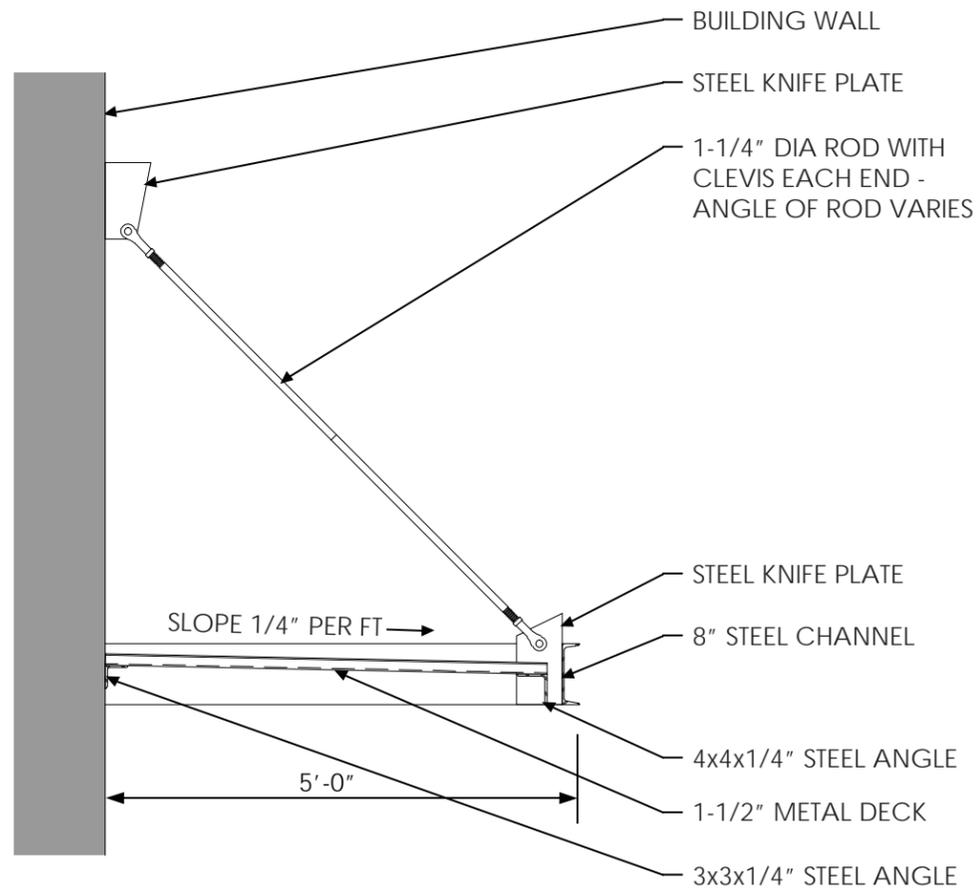
A TYPICAL BLADE
SIGN SECTION



B PARKING/TENANT
BLADE SIGN SECTION



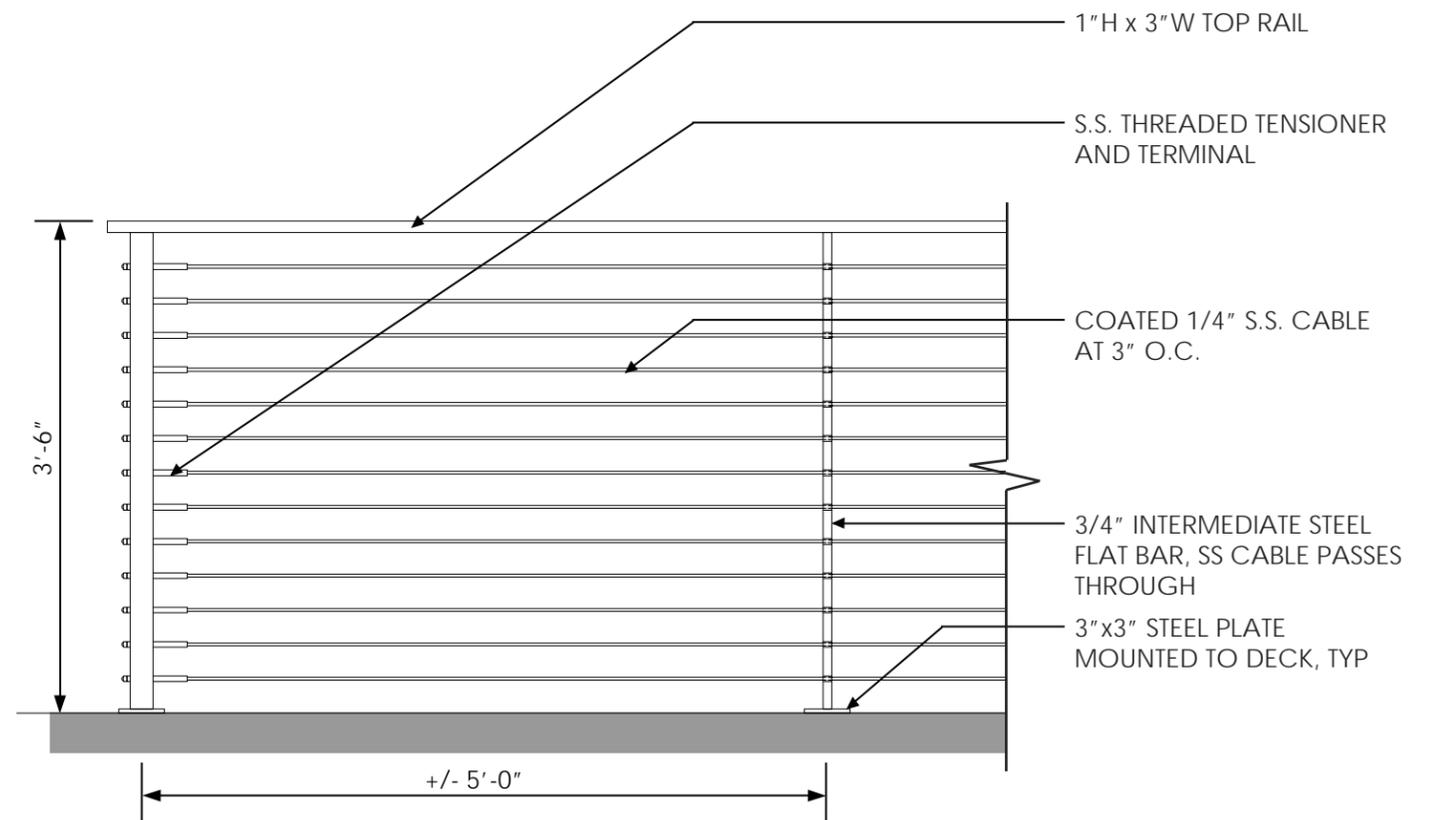
C BLADE SIGN SUPPORT
AT BUILDING PILASTER



1 TYPICAL CANOPY SECTION

SCALE: 1/2" = 1'-0"

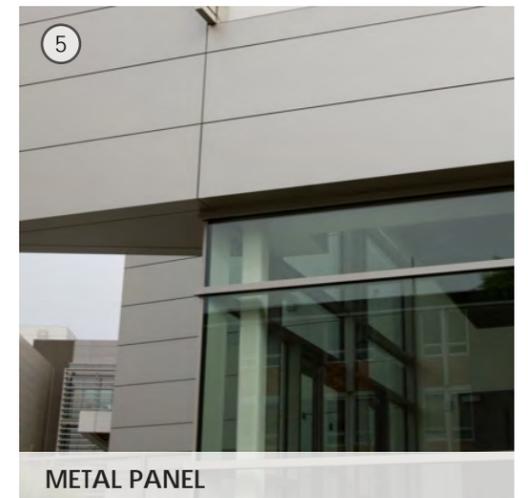
NOTE: ALL CANOPIES TO MATCH EXISTING. ALL EXPOSED CANOPY STEEL TO BE POWDER-COATED "CHARCOAL". ALL EXISTING CANOPIES TO BE REPAINTED "CHARCOAL".



2 TYPICAL RAILING SECTION

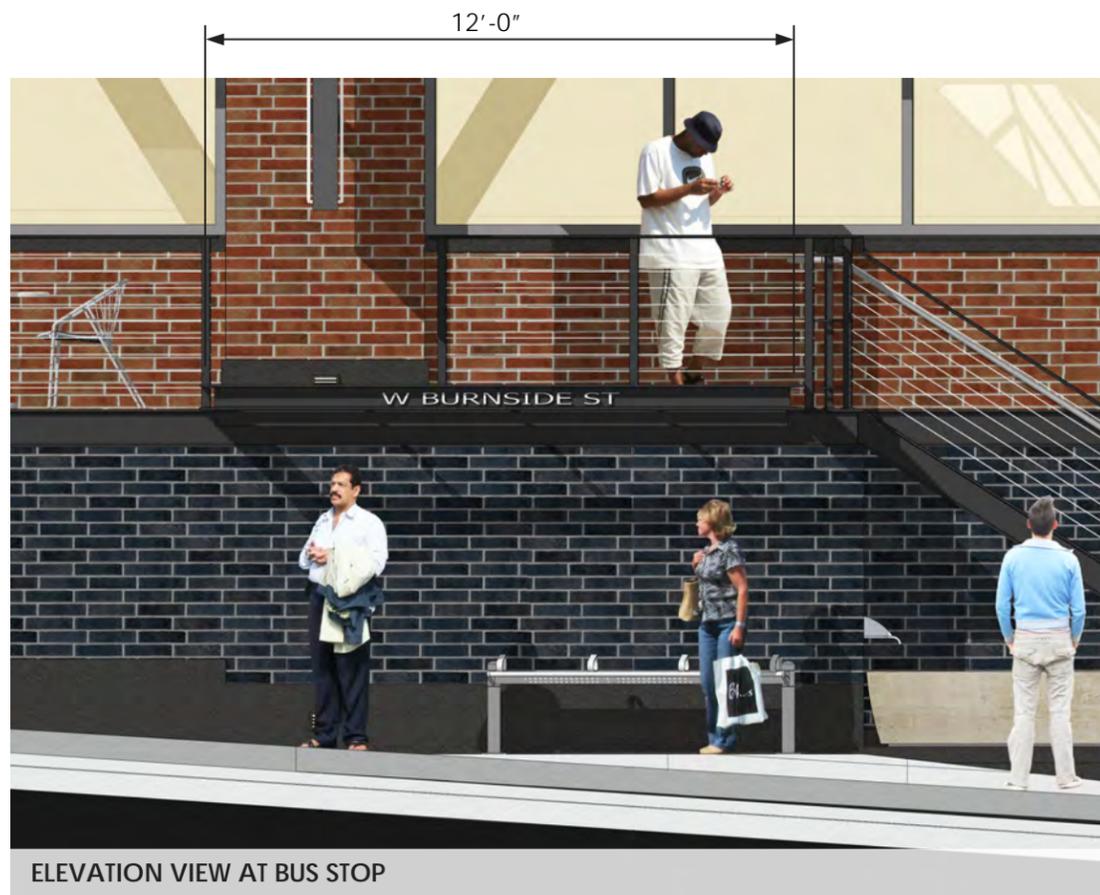
SCALE: 3/4" = 1'-0"

NOTE: ALL EXPOSED RAILING STEEL TO BE POWDER-COATED "CHARCOAL"

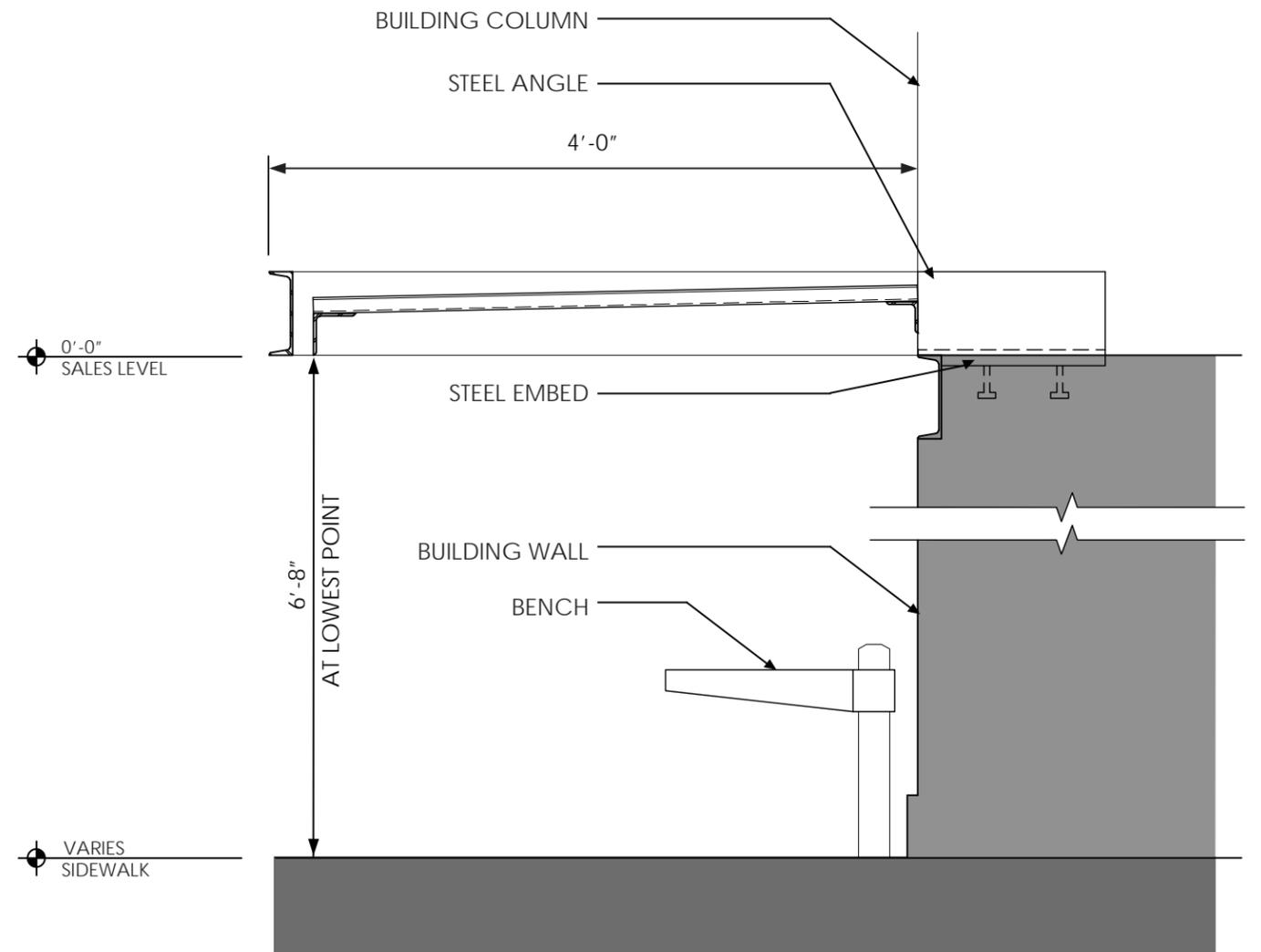




PERSPECTIVE VIEW AT BUS STOP



ELEVATION VIEW AT BUS STOP



1 BUS CANOPY SECTION

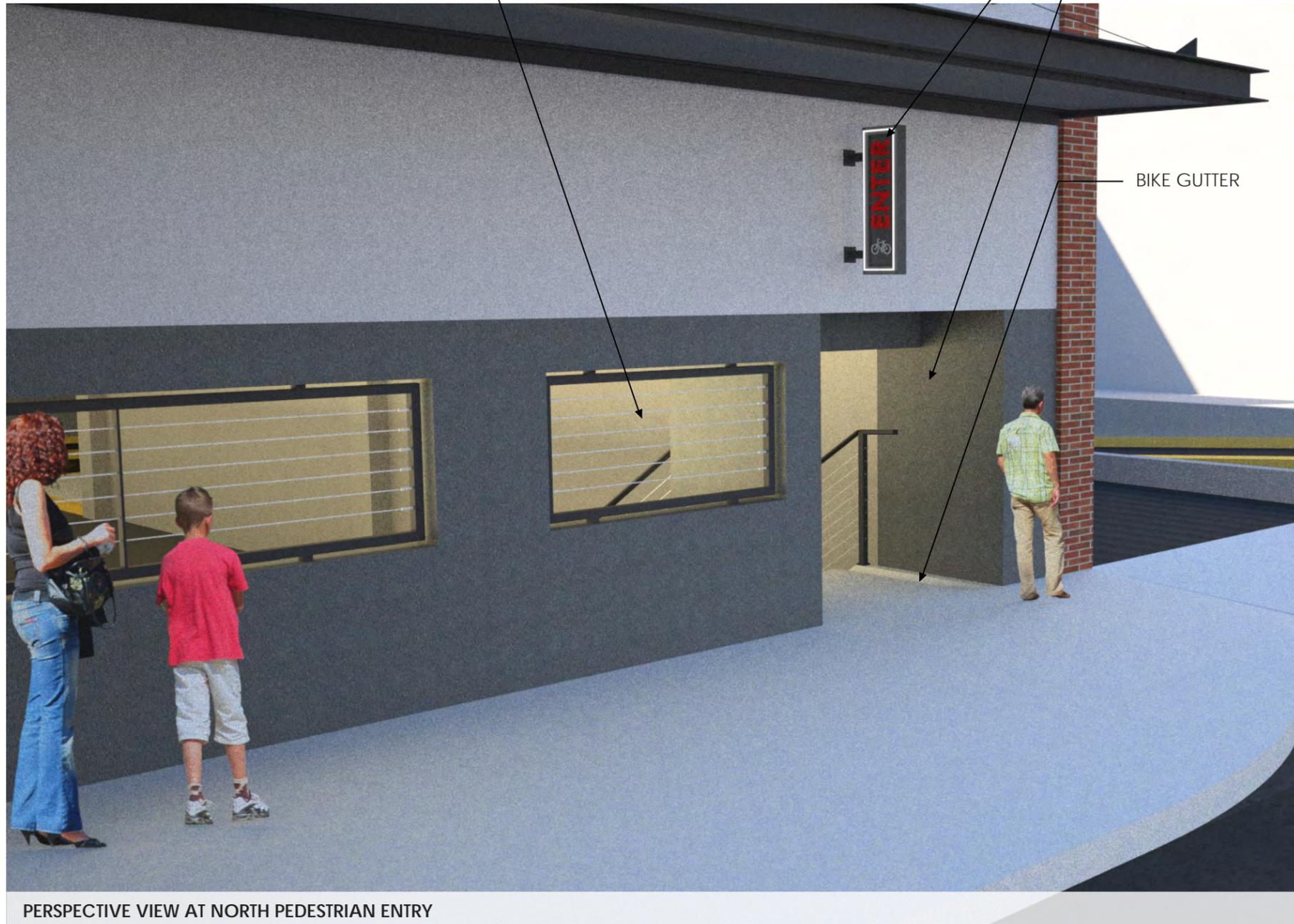
SCALE: 3/4" = 1'-0"

DECORATIVE SCREENING - CABLE RAIL
INFILL TO MATCH HANDRAILING, TYP AT
ALL OPENINGS INTO PARKING LEVEL

ENTRY SIGNAGE (3 SF)

PEDESTRIAN ENTRY

BIKE GUTTER



PERSPECTIVE VIEW AT NORTH PEDESTRIAN ENTRY



ENLARGED EAST ELEVATION

SCALE: NTS

6'-0"