



City of Portland Design Commission

Design Advice Request

SUMMARY MEMO

Date: 10/22/2024
To: Garrett Helm | Bora Architecture & Interiors
From: Tanya Paglia, Design & Historic Review Team
503-865-6518 | Tanya.Paglia@portandoregon.gov
Re: EA 24-067517 – Ida B. Wells Modernization
Design Advice Request Commission Summary Memo – September 19, 2024

Thank you for taking advantage of the opportunity to hold a Design Advice Request regarding your project. I hope you find it informative and valuable as you continue with your project development. Following, is a summary of the comments provided by the Design Commission at the September 19, 2024 Design Advice Request. This summary was generated from notes taken at the public meeting and a subsequent review of the public meeting recordings. To review those recordings, please visit: <https://efiles.portlandoregon.gov/Record/16990898/>.

These Design Commission comments are intended to guide you in further design exploration of your project. These comments may also inform City staff when giving guidance over the course of future related land use reviews. It should be understood that these comments address the project as presented on September 19, 2024. As the project design evolves, the comments, too, may evolve or may no longer be pertinent.

Design Advice Requests are not intended to substitute for other Code-required land use or legislative procedures. Please keep in mind that the formal Type III land use review process [which includes a land use review application, public notification and a Final Decision] must be followed once the Design Advice Request meetings are complete, if formal approval for specific elements of your project is desired.

Please continue to coordinate with me as you prepare your Type III Land Use Review Application.

Encl:
Summary Memo

Cc: Design Commission
Respondents

Executive Summary.

- General support for the project as an improvement to the school campus experience.
- Rework the north campus approach to create a sense of place and safely channel pedestrians and cyclists to the main building entrance at the south side of the site via the north-south pedestrian connection.
- A prominent arrival plaza on the north end of the site will create a more visible and welcoming entry sequence for the campus.
- The building massing and placement are on track.
- Unanimous support for the Modification allowing the building to be set back more than 10' from SW Vermont St and SW Burlingame Ave.
- Majority support for locating main building entrance on south side of campus along SW Vermont.
- Support for locating grandstand on the west side of the track and field.
- Reconfigure the pool parking lot driveway to improve bike/ped safety at the north end of campus and to avoid breaking up the north-south pedestrian plaza/walkway.
- Prioritize bicycle lanes and crossings for cyclists entering campus from the north and improve all shared bike/ped facilities throughout the campus with clear delineations of which is bike vs ped via material changes, and adequately size paths for all users.
- Provide data on campus travel patterns in the design review submittal.
- Outdoor gathering spaces are heading in the right direction, although more information is needed in the design review submittal.
- Based on the limited information provided, it appears that the building composition is evolving in a positive direction.

Commissioners Present. Chair McCarter, Vice-Chair Robinson, Commissioner Bue, Commissioner Rodriguez, Commissioner Santner, Commissioner Swank, and Commissioner Vaz.

Summary of Comments. Following is a general summary of Commission comments by design tenet.

CONTEXT

- **Northern campus approach**
 - The pedestrian and bicycle experience of entering the campus from the north is important and needs to be improved.
 - The north campus approach needs to create a sense of place and also channel pedestrians to the main building entrance at the south side of the site.
 - A prominent arrival plaza on the north end of the site will create a more visible and welcoming entry sequence for the campus. The current arrival plaza feels like an afterthought.

- **Placement and Massing of Building**

- The building massing and placement are on track.
- Unanimous support for the Modification to allow the building to be set back more than 10' from SW Vermont St and SW Burlingame Ave. It allows spill out space to activate the front of the building and also helps with accessibility related to the large grade change up to the building.
- Majority support for locating main building entrance on south side of campus along SW Vermont.
- However, one commissioner expressed concerns with the entrance location, noting that vehicle congestion along SW Vermont is likely, especially with the elementary school buses for Rieke dropping off along Vermont.

- **Grandstand location**

- Support for locating grandstand on the west side of the track and field with commissioners noting that this location prioritizes the pedestrian experience along the central pedestrian plaza/walkway by not looming over it, crowding it, blocking western viewsheds, and casting a shadow over it. It also better avoids glare for spectators in the grandstand.
- It was noted that the grandstand built into the hillside on the east side would be nice, but since building into the hillside is not viable, west side is preferred.

PUBLIC REALM

- **Site circulation**

Central north-south pedestrian connection

- With many students coming to campus from the north, and the main entrance located at the south end of the site, it is important that the pedestrian route makes it comfortable to move through the site from the north.
- Refine the transportation system at the north end of site and its connection to the north-south pedestrian spine.
- Pedestrians should be better separated from vehicles and the north campus approach should be made less vehicle centric.

Pool parking lot vehicle connection

- The drive connecting the loop road with pool parking lot is a pedestrian obstacle and disrupts the north-south spine formed by the campus' central pedestrian plaza/walkway that connects pedestrians coming to campus from the north with the main entrance.
- Explore ways to reconfigure the tennis courts and the pool parking lot driveway and include studies in the design review submittal.

- Some reconfiguration options include moving the driveway to the east side of the tennis courts, shifting both the tennis courts and the driveway to the east, or placing the parking lot right off the loop drive to avoid the need for an access driveway that splices up pedestrian areas (this option might involve incorporating a landscaped area with the special tree into the parking lot).

Bicycle circulation

- Prioritize bicycle lanes and crossings for cyclists entering campus from the north.
- Entering the site by different modes from the north is confusing – there needs to be a much better hierarchy of road design up there.
- Improve all shared bike/ped facilities throughout the campus with clear delineations of which is bike vs ped via material changes.
- Many of the paths through the site look overly skinny – best practice for most multi-use pathways is to make them 12-14' wide. If these trails are going to be shared with bikes and peds, make sure they are adequately sized for all users.

Additional information desired for design review

- Provide data on campus travel patterns including:
 - How people approach the site – which direction they are coming from, and on which side they enter the campus.
 - Mode split.
 - Analysis of car, bus, and van drop-off and pick-up patterns/locations.
 - Diagram of the site plan showing all pedestrian movement towards the site, including from bus stops.
 - Diagram of key destinations such as pool and field entry locations.

• **Outdoor gathering spaces**

- Outdoor gathering spaces are heading in the right direction, including the central north-south pedestrian plaza, although refinement is needed at the northern end.
- Detailed plans on outdoor gathering spaces including landscape and hardscape plans, etc. would be helpful as would a diagram of the entrances and egresses of the building and their relationship to the pedestrian plazas and connections.

QUALITY & PERMANENCE

• **Overall composition**

- Based on the limited information provided, it appears that the building composition is evolving in a positive direction.

- Placement of the bars is working well, and façade studies look on track. The colonnade at the front is a plus.
- Plan/diagram of building looks great – commons at the center creates views out and makes the building readable to people inside it. The four equal quadrants are good.

Exhibit List

- A. Applicant's Submittals
 - 1. Original project summary, team information, zoning summary, and design information
 - 2. Updated project summary, team information, zoning summary, and design information
 - 3. Project addendum
- B. Zoning Map
- C. Drawings
 - 1. Site Plan (attached)
 - 2. Project Evolution and Design Drivers
 - 3. Aerial Massing Study (attached)
 - 4. Representative Image of Project (Perspective)
 - 5. Representative Image of Project (Perspective) (attached)
 - 6. Zoning Summary
 - 7. Addendum Index
 - 8. Addendum: Existing Site Topography
 - 9. Addendum: Driving Factors for Planning and Design
 - 10. Addendum: Planning Phase: Key Design Drivers
 - 11. Addendum: Planning Phase: Early Site Plan Studies
 - 12. Addendum: Existing Site Plan with Key Design Drivers
 - 13. Addendum: Planning Phase: Buildable Area & Arrival, Scheme 1
 - 14. Addendum: Planning Phase: Buildable Area & Arrival, Scheme 2
 - 15. Addendum: Planning Phase: Options
 - 16. Addendum: Building Massing Diagram with Annotations
- D. Notification
 - 1. Mailing list
 - 2. Mailed notice
 - 3. Posting instructions sent to applicant
 - 4. Posting notice as sent to applicant
 - 5. Applicant's statement certifying posting
- E. Service Bureau Comments
 - 1. PBOT
- F. Public Testimony
 - 1. Jeanne Roy, jeanneroy62@gmail.com
- G. Other
 - 1. Application form
 - 2. Pre-Application Conference Summary Notes (EA 23-105044 APPT), held 1/23/2023
 - 3. Staff memo to Design Commission 9/12/2024
 - 4. Attendee testifier sheet
 - 5. Staff presentation, 9/19/2024
 - 6. Applicant presentation, 9/19/2024

BORA

07/29/2024

Ida B. Wells Modernization – Design Advice Request #1 Application

Team Information

- Owner: *Portland Public Schools*
- Architect: *Bora Architecture & Interiors*
- Civil Engineer: *KPFF Consulting Engineers & Rivero Design*
- Landscape Architect: *Walker Macy*
- Structural Engineer: *KPFF Consulting Engineers & NBZ Consulting*
- M/E/P/FP: *PAE & Burman Design*
- Theatrical & A/V: *The Shalleck Collaborative*
- Code Consultant: *Code Bird Consulting*
- Land Use Consultant: *The Bookin Group*
- Community Engagement Consultant: *After Bruce*

Summary of Development Program

Approx. 320,000 sf comprehensive high school to accommodate 1,700 students. Building program includes classrooms, science labs, 2D and 3D art classrooms, CTE classrooms (including a wood and metal shop), commons and commercial kitchen, gymnasiums, 500-seat theater, performing arts spaces including band, choir, and black box theater, and administrative offices. Site program includes development (to varying levels) across ~35-acre site, including track and field with grandstand and support buildings, baseball and softball fields with associated site structures, and pool support building to serve existing-to-remain PP&R public pool.

Sheet Index [NOTE: Drawing Set included is in DRAFT and will be finalized 14 days prior to meeting]

Page 3 – Visual Zoning Summary

Page 4 – Project Evolution and Design Drivers

Page 5 – Proposed Site Plan

Page 6 – Aerial Massing Study

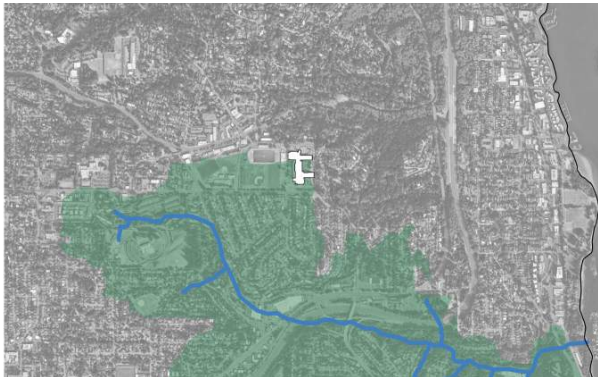
Page 7 – Representative Image of Project (Perspective)

Anticipated Modifications / Adjustments

1. Long-term bicycle parking at Ida B. Wells HS
2. Long-term bicycle parking at Rieke ES (proposed to be constructed with required non-conforming upgrades)
3. Maximum building setback in an IR zone



Attendance Area



Stephen's Creek

ZONING

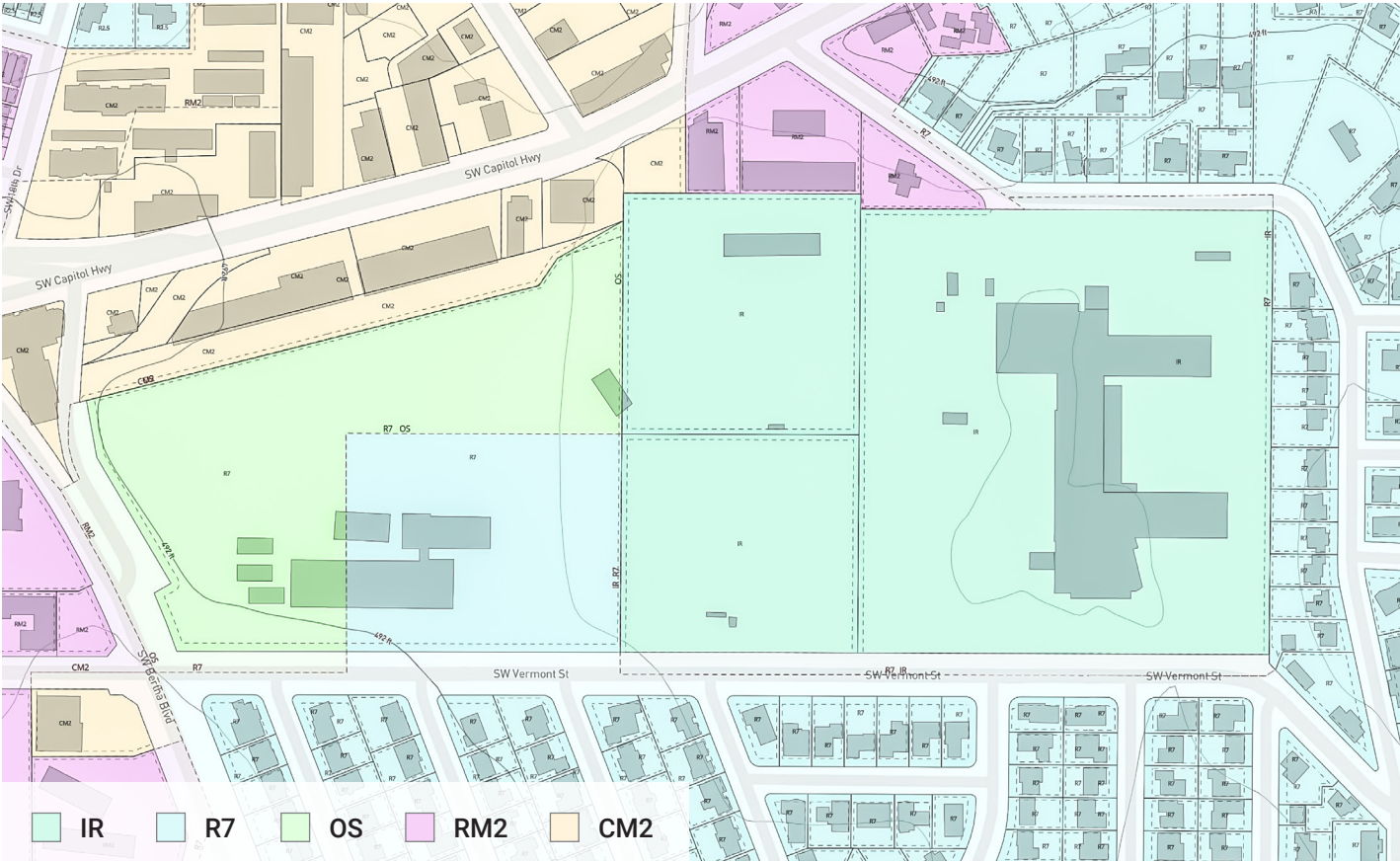
The Ida B. Wells site is made up of 4 separate tax lots (including Rieke Elementary) located between SW Capitol Highway and SW Burlingame Avenue to the north, SW Burlingame Avenue to the East, SW Vermont Street to the South, and SW Bertha to the West. Zoning designations on the site include IR – Institutional Residential, R7 – Single Dwelling Residential, OS – Open Space, with a Design Overlay on the IR portions only. The site falls within the Hillsdale Plan District. School uses are a Conditional Use in the OS and R7 zones.

In the IR zone, High Schools are a Limited use that can be approved either through a Conditional Use or through either a Conditional Use Master Plan or an Impact Mitigation Plan (for a large campus over 5 acres in size). Applicant may choose which process. All streets surrounding the site are local service transit streets (i.e. not transit streets). But the entire site is located in a Pedestrian district. The private street that connects SW Capital Highway to SW Vermont is classified as a Neighborhood Walkway.

The following table highlights the main zoning requirements for the site. Of note in the zoning table is the height limit at 75' and the 50' setback regulations for sports fields and courts where the site abuts a residential property.

STANDARD	REQUIREMENT
FAR	
Max FAR	2:1
Min Density/FAR	No requirement
Bonus Density	N/A
Height	
Max Height	75 ft
Setbacks	
Lot line abutting or across the street from OS, RF-R2.5	1 foot for every 2 feet of building height, but not less than 10 feet
Lot line abutting or across the street from RM1-RMP, IR zone	
Lot line abutting or across the street from C, CI, E or zone	
Max Building Setback	10 ft
Maximum Building Coverage	70% of site area
Minimum Landscape Area	20% of site area
Maximum Building Length	None
Landscaping Abutting an R-zoned lot	10 feet to the L3
Landscaping Across the street from an R-zoned lot	10 feet to the L1
Building Facade Articulation required	No
Ground Floor Windows required	Yes
Transit Street Main Entrance	No
Title 11 Trees	Applies – 1/3 of trees over 12" and all trees over 20" should be preserved

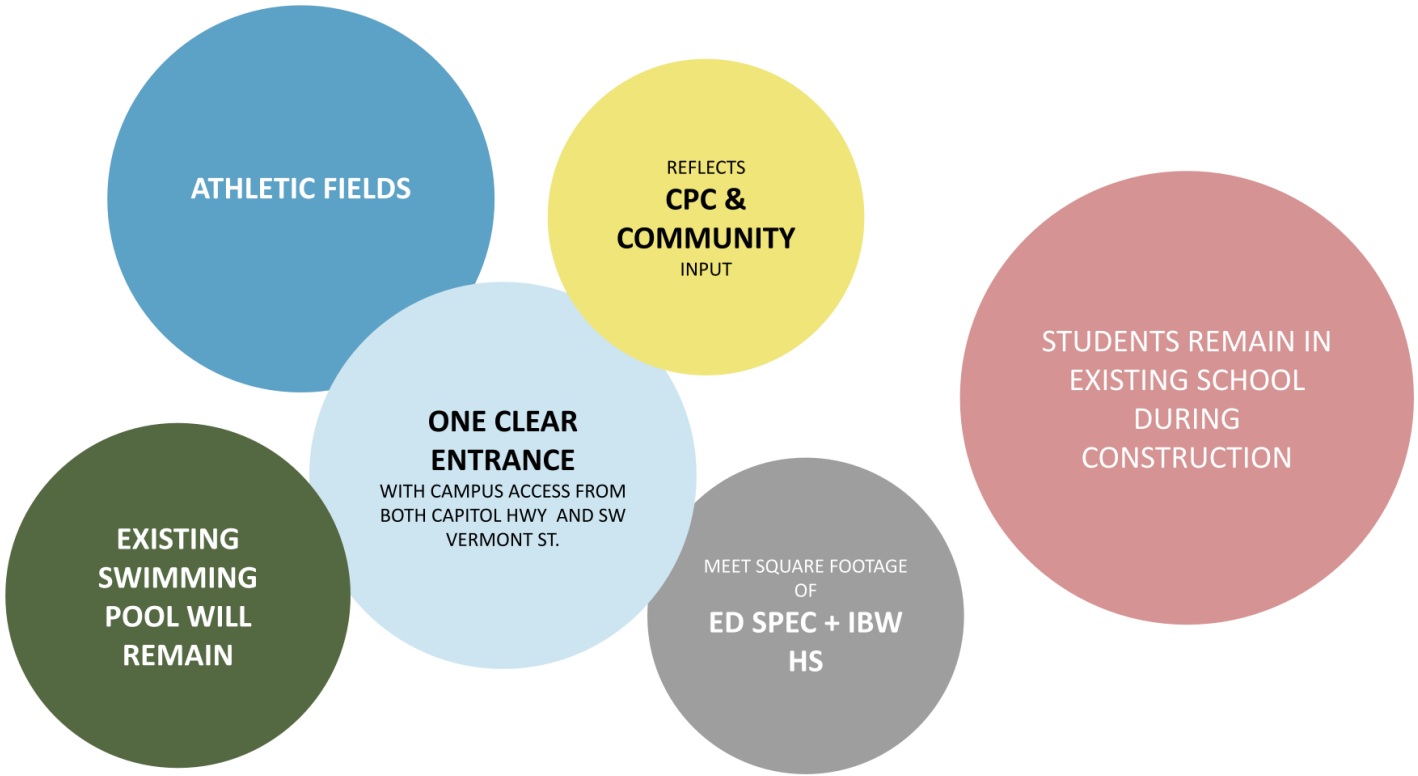
These additional standards from the multi-dwelling zones would apply in the IR zone if the site is developed as an "Institutional Campus" as approved though a CUMP or IMP:	
Street-Facing Facades (33.120.232)	- Windows must cover 40% of ground floor wall area for walls 5 feet or closer to the street lot line. - Windows must cover at least 25% of ground floor wall for walls over 5 feet from street lot line.
Required Outdoor and Common Area (33.120.240)	- At least 10% of the total site must be provided as common area – either as outdoor courtyards or play areas
Pedestrian Standards (33.120.255)	5 foot wide hard-surfaced pedestrian paths must connect the building to the street and connect all to all other areas of the site.
R7 and OS Development Standards	
Maximum FAR	0.5 to 1
Maximum Height	50 ft
Minimum Setback R7	1 foot for every 2 feet of building height but in no case less than 15 ft.
Minimum Setback OS (33.100.200.B.2)	1 foot for each foot of building height
Maximum Setback	20 ft
Maximum Building Coverage	50%
Minimum Landscaped Area	25%
Buffering from Across the street from a Residential Zone	15 feet to the L1
Outdoor activity facilities – min setbacks	50 feet from abutting residential properties
Pedestrian Standards	5 foot wide hard-surfaced pedestrian paths must connect the building to the street and connect all to all other areas of the site.
Hillsdale Plan District Requirements	
Drive-Through Facilities	Are prohibited



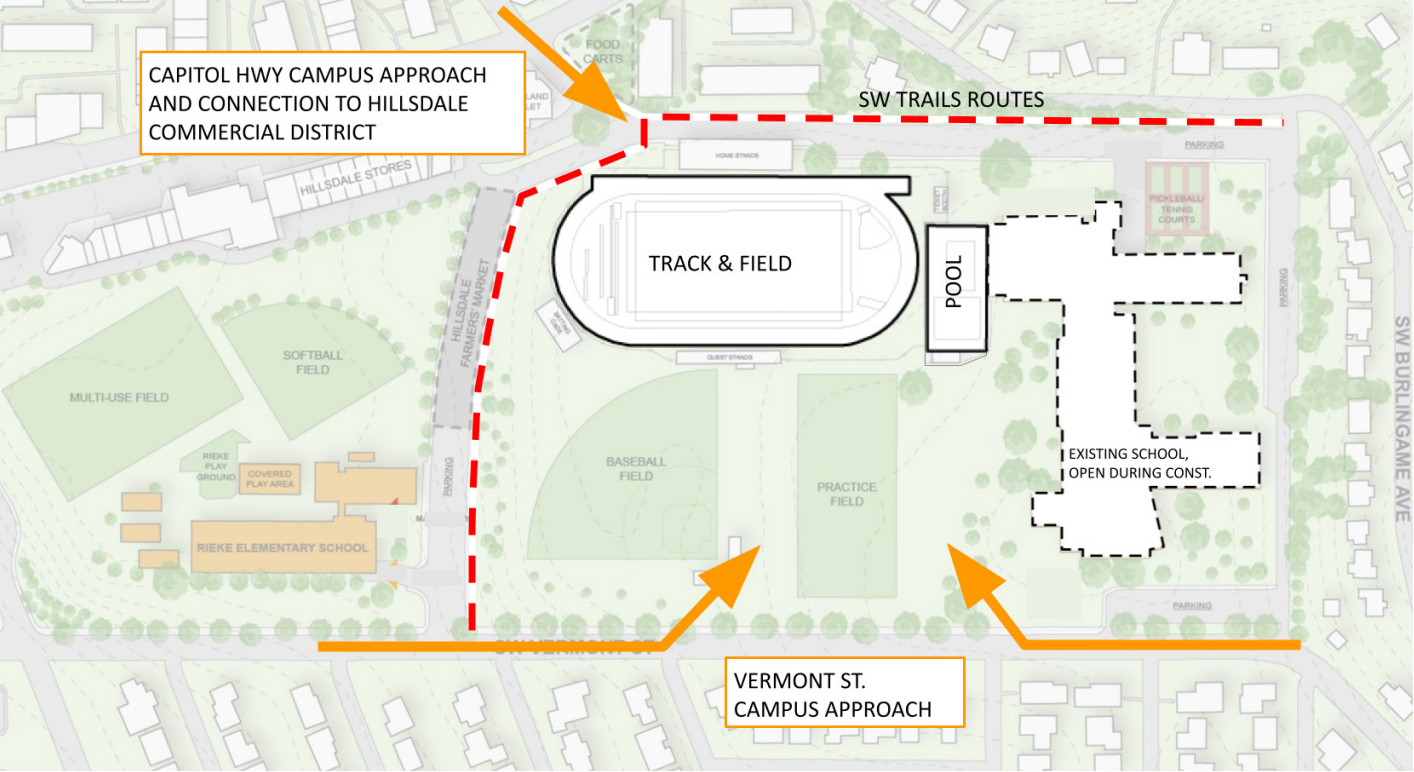
DRIVING FACTORS FOR PLANNING

The project team looked at the Ida B. Wells site comprehensively to develop a range of site planning possibilities. Early on in this analysis, major site design drivers were identified, both physical components of the site (sports fields, public swimming pool, the location of the existing school), as well as parameters and guidelines provided by PPS including the education specification requirements for the building itself.

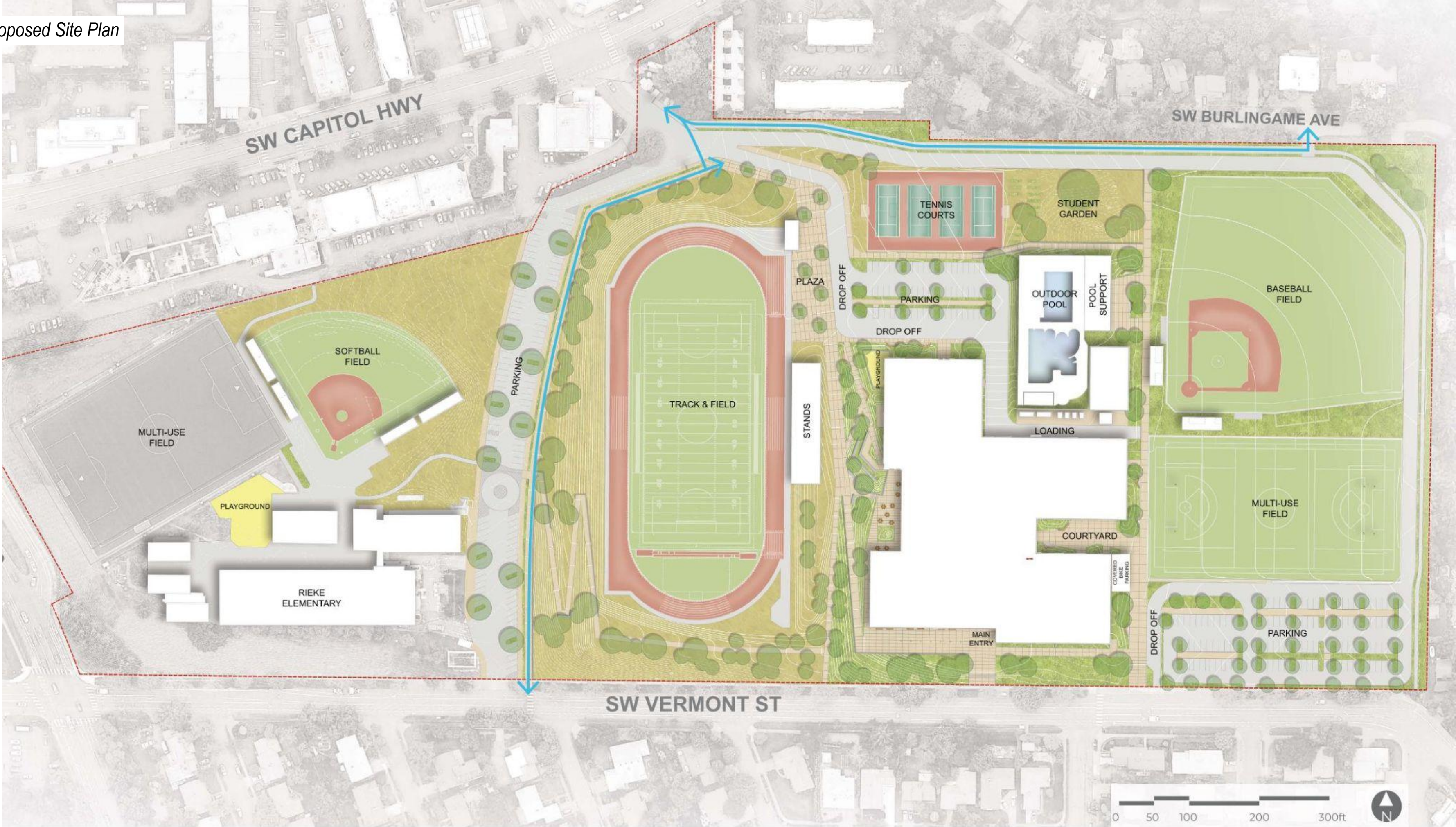
The design team also analyzed solar orientation of the building massing, the site topography, and site zoning requirements. Additionally, the design team studied the approaches to the campus and opportunities for the building entry to connect to the northwest approach from SW Capitol Highway, and/or the southern approach from SW Vermont Street.



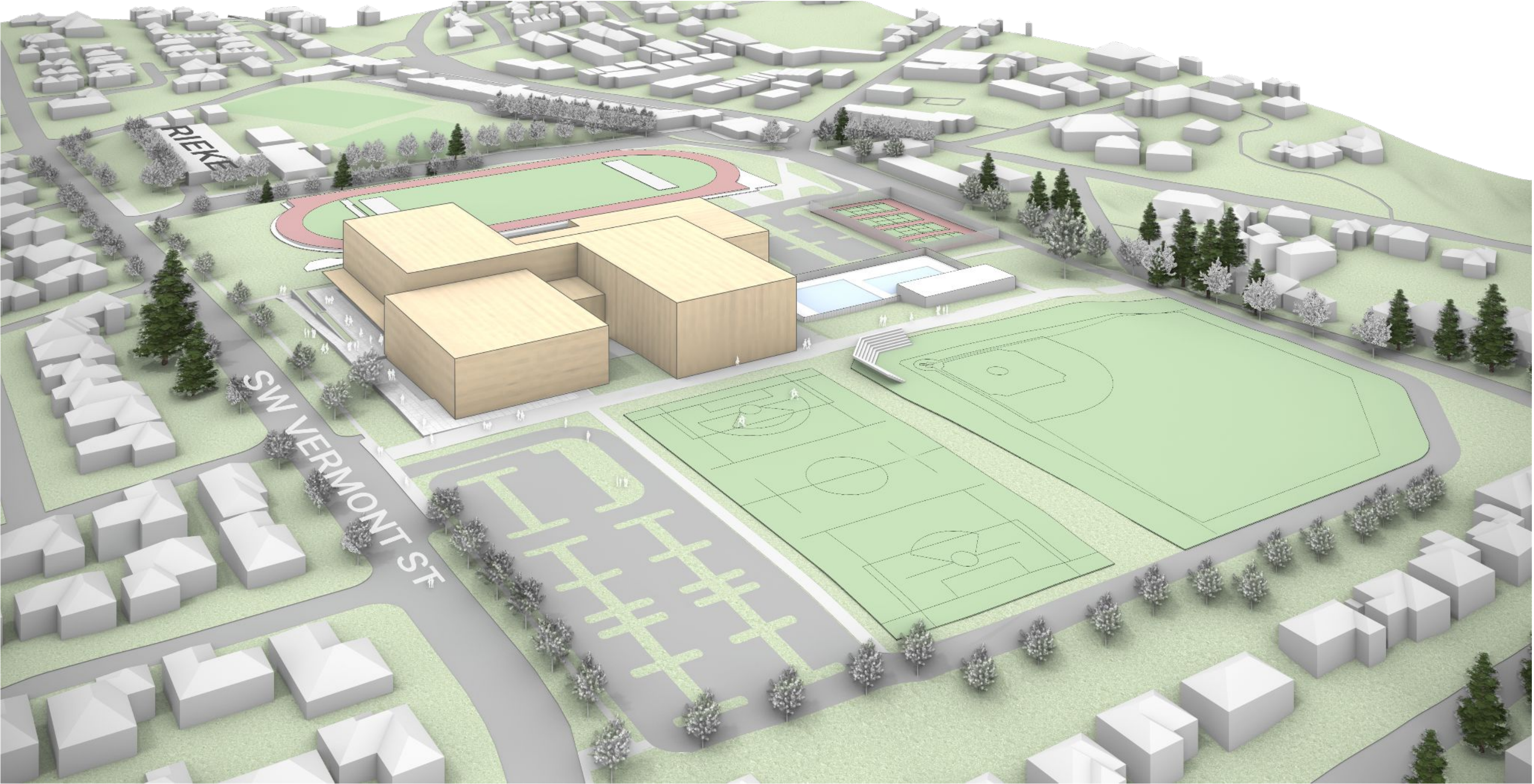
Site Diagram of Driving Factors



Proposed Site Plan



- TRAIL
- PROPERTY LINE



Representative Image of Project (Perspective)



BORA

09/05/2024

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Attendance Area



Stephen's Creek

ZONING

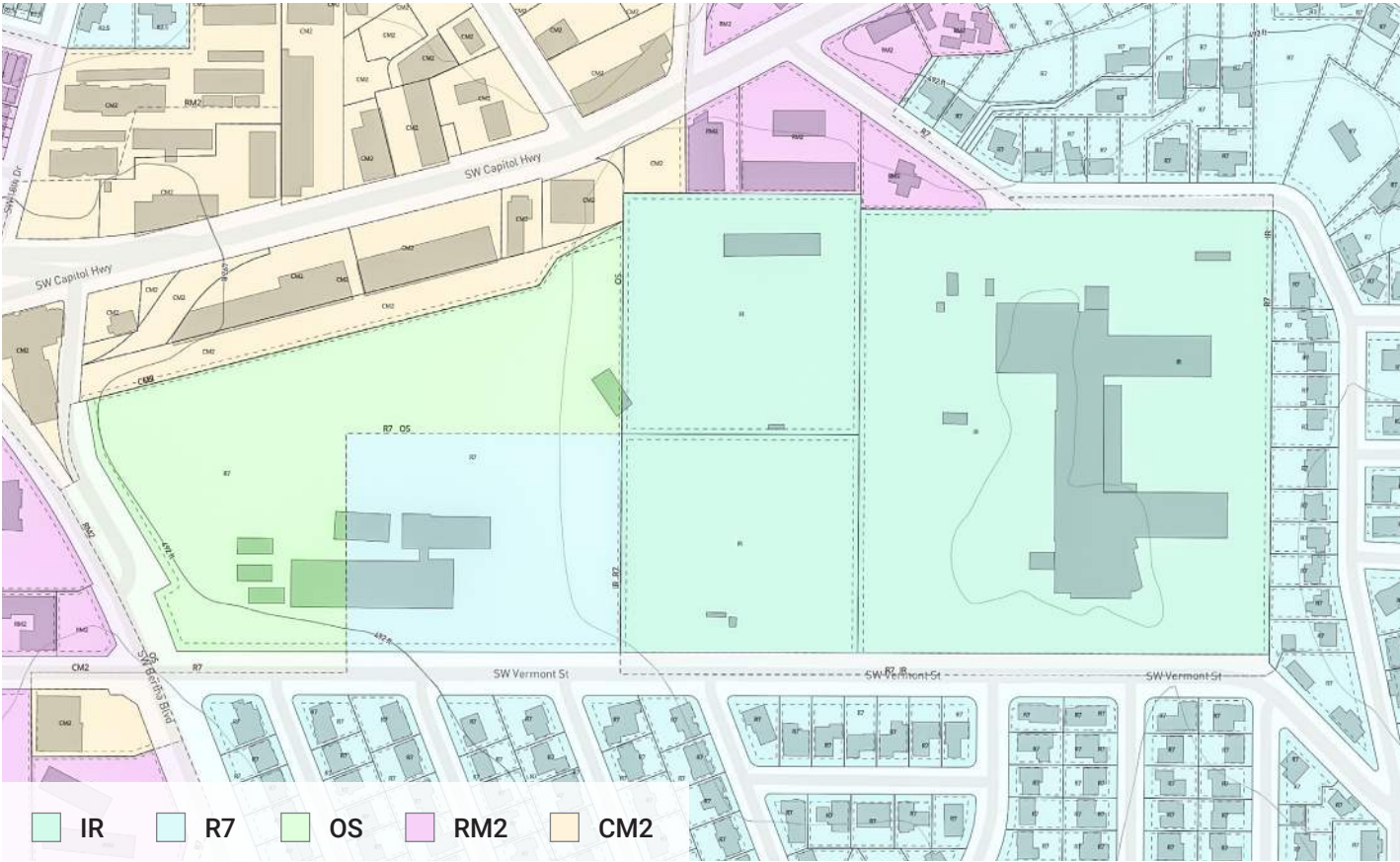
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The following table highlights the main zoning requirements for the site. Of note in the zoning table is the height limit at 75' and the 50' setback regulations for sports fields and courts where the site abuts a residential property.

STANDARD	REQUIREMENT
FAR	
Max FAR	2:1
Min Density/FAR	No requirement
Bonus Density	N/A
Height	
Max Height	75 ft
Setbacks	
Lot line abutting or across the street from OS, RF-R2.5	1 foot for every 2 feet of building height, but not less than 10 feet
Lot line abutting or across the street from RM1-RMP, IR zone	
Lot line abutting or across the street from C, CI, E or zone	
Max Building Setback	10 ft
Maximum Building Coverage	70% of site area
Minimum Landscape Area	20% of site area
Maximum Building Length	None
Landscaping Abutting an R-zoned lot	10 feet to the L3
Landscaping Across the street from an R-zoned lot	10 feet to the L1
Building Facade Articulation required	No
Ground Floor Windows required	Yes
Transit Street Main Entrance	No
Title 11 Trees	Applies – 1/3 of trees over 12" and all trees over 20" should be preserved

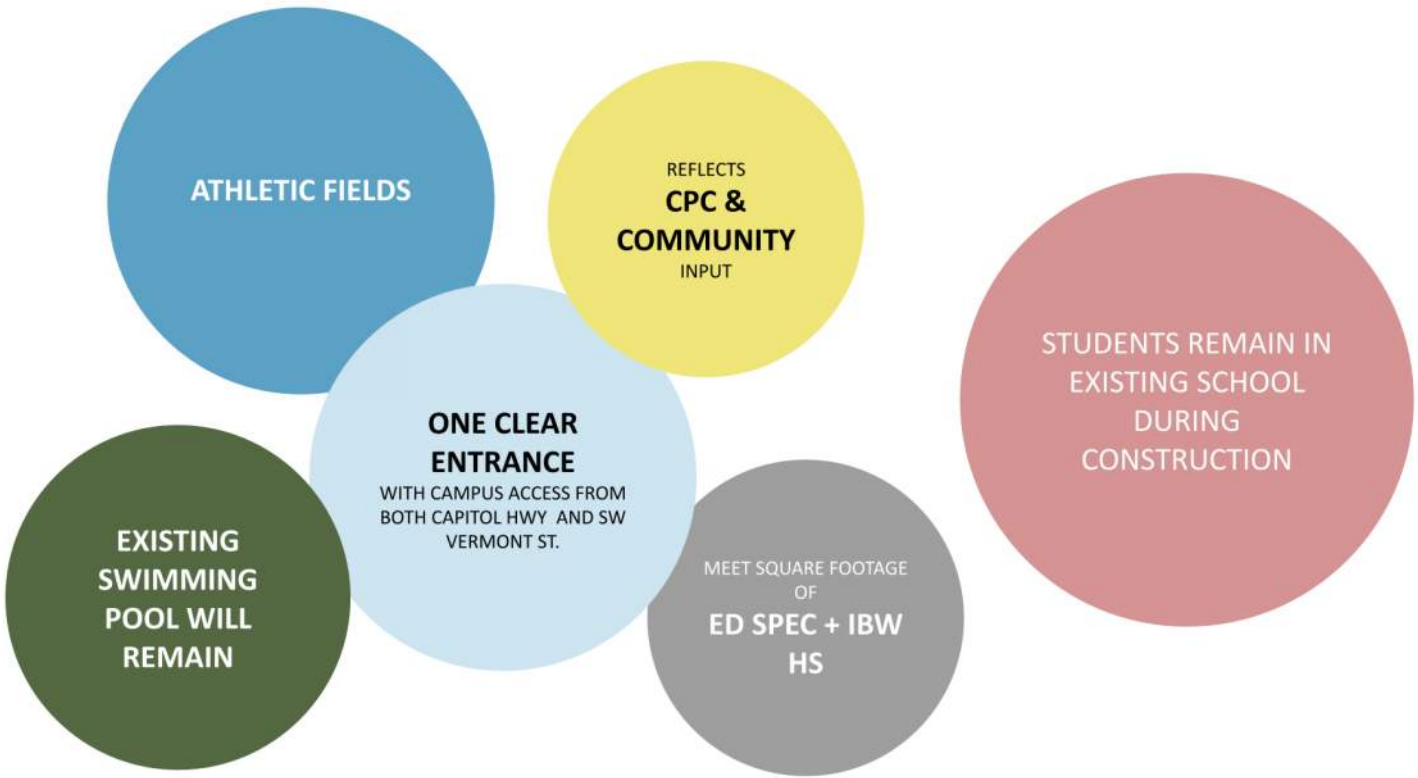
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Drive-Through Facilities	Are prohibited



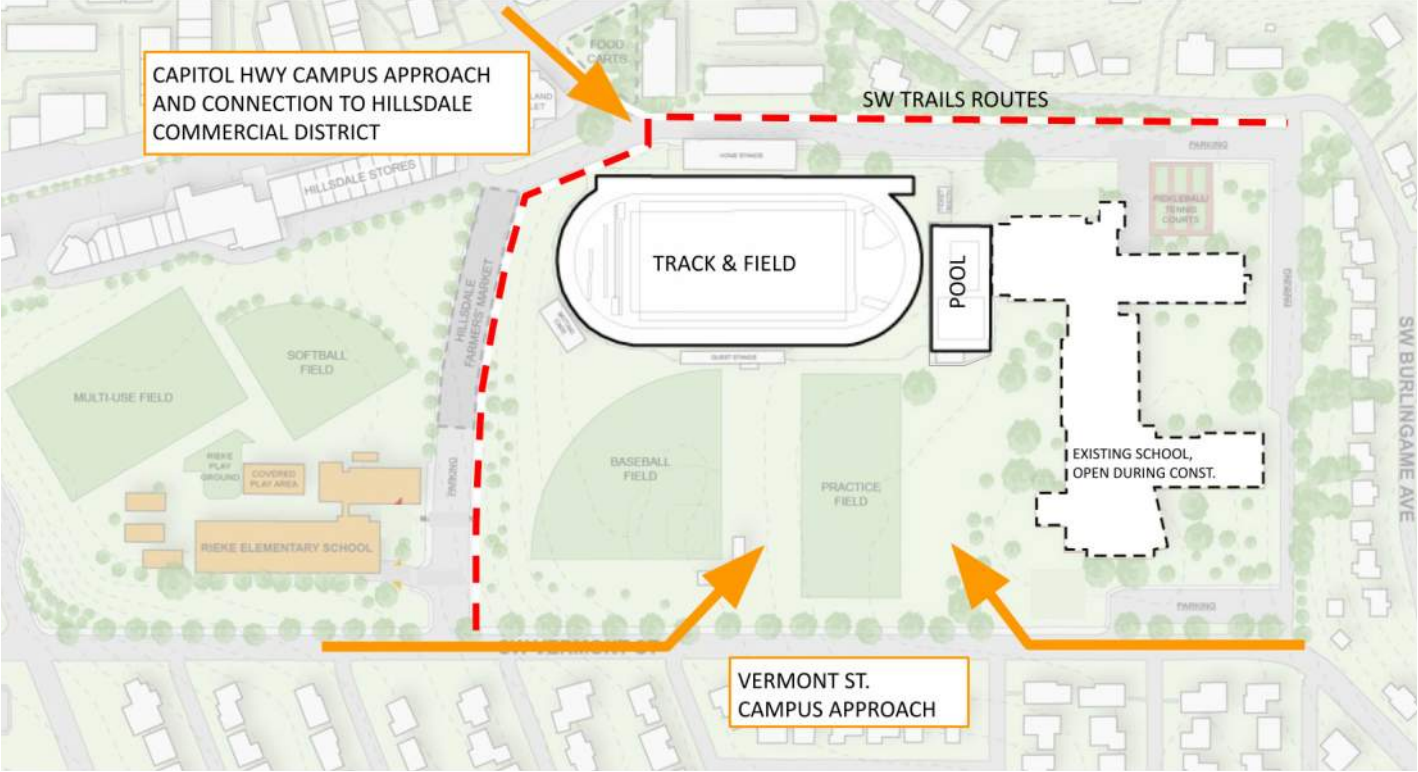
DRIVING FACTORS FOR PLANNING

The project team looked at the Ida B. Wells site comprehensively to develop a range of site planning possibilities. Early on in this analysis, major site design drivers were identified, both physical components of the site (sports fields, public swimming pool, the location of the existing school), as well as parameters and guidelines provided by PPS including the education specification requirements for the building itself.

The design team also analyzed solar orientation of the building massing, the site topography, and site zoning requirements. Additionally, the design team studied the approaches to the campus and opportunities for the building entry to connect to the northwest approach from SW Capitol Highway, and/or the southern approach from SW Vermont Street.



Site Diagram of Driving Factors



View of main entrance from Southwest Vermont St.



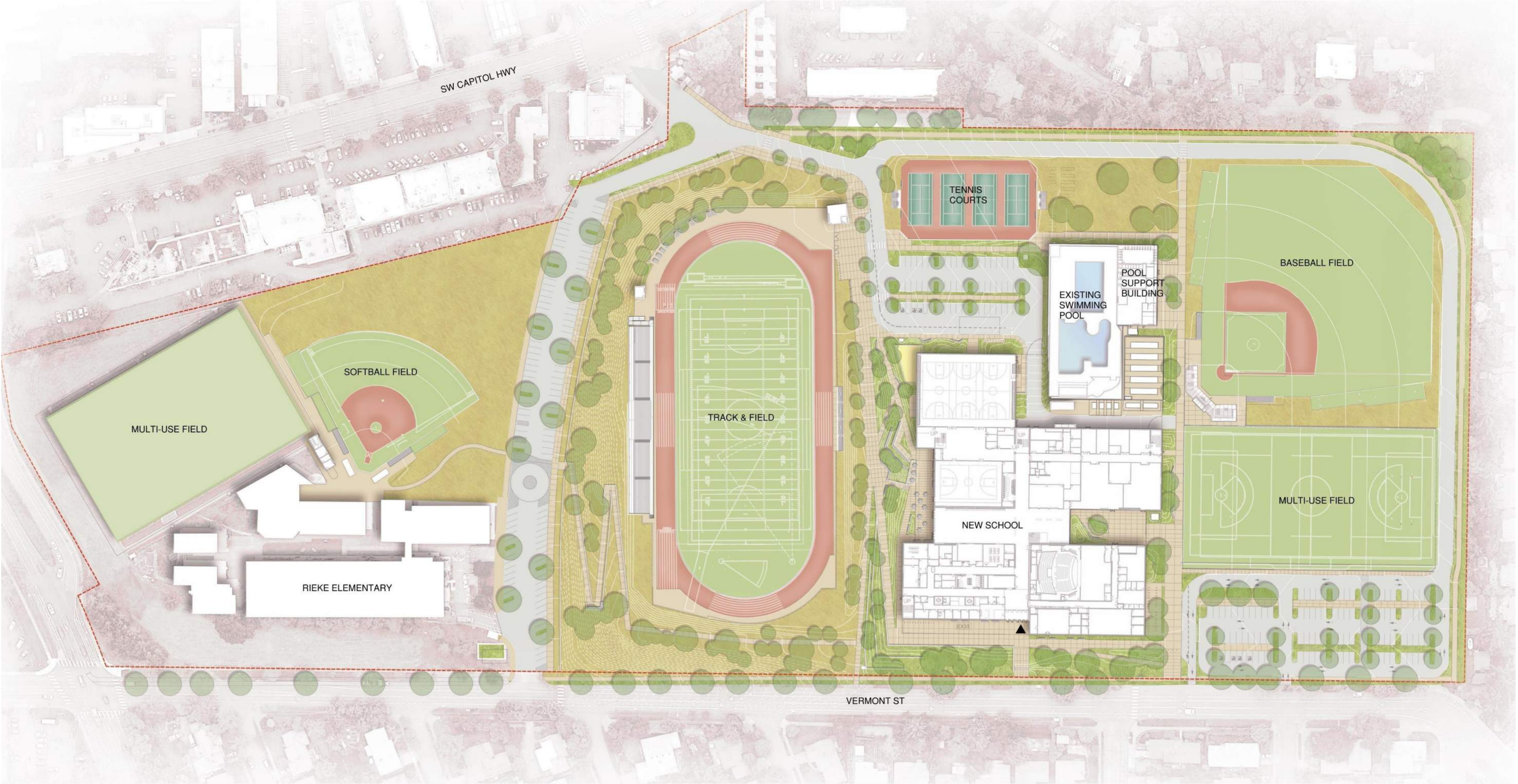
Track and field grandstand



Views to the west and existing baseball field



Ida B. Wells pool





Representative Image of Project (Perspective)



West Facade



View from SW Vermont St.

BORA

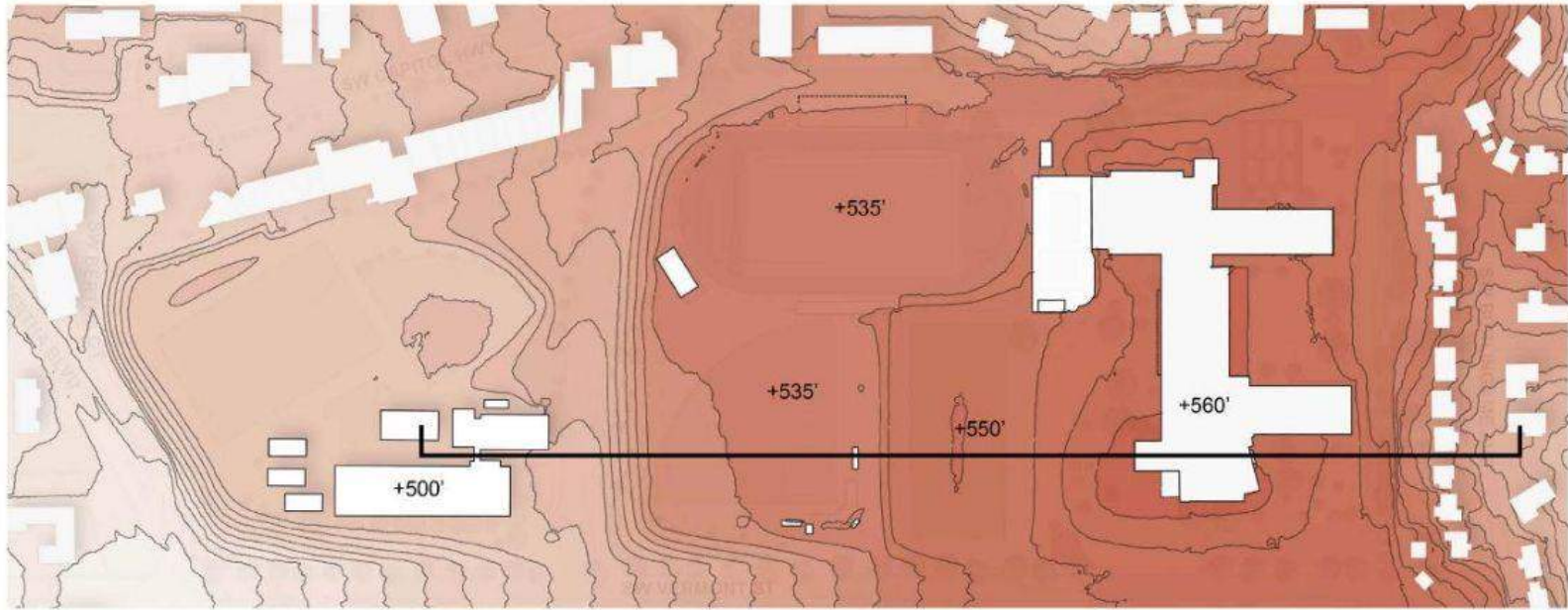
09/12/2024

Ida B. Wells Modernization – Design Advice Request #1 Application

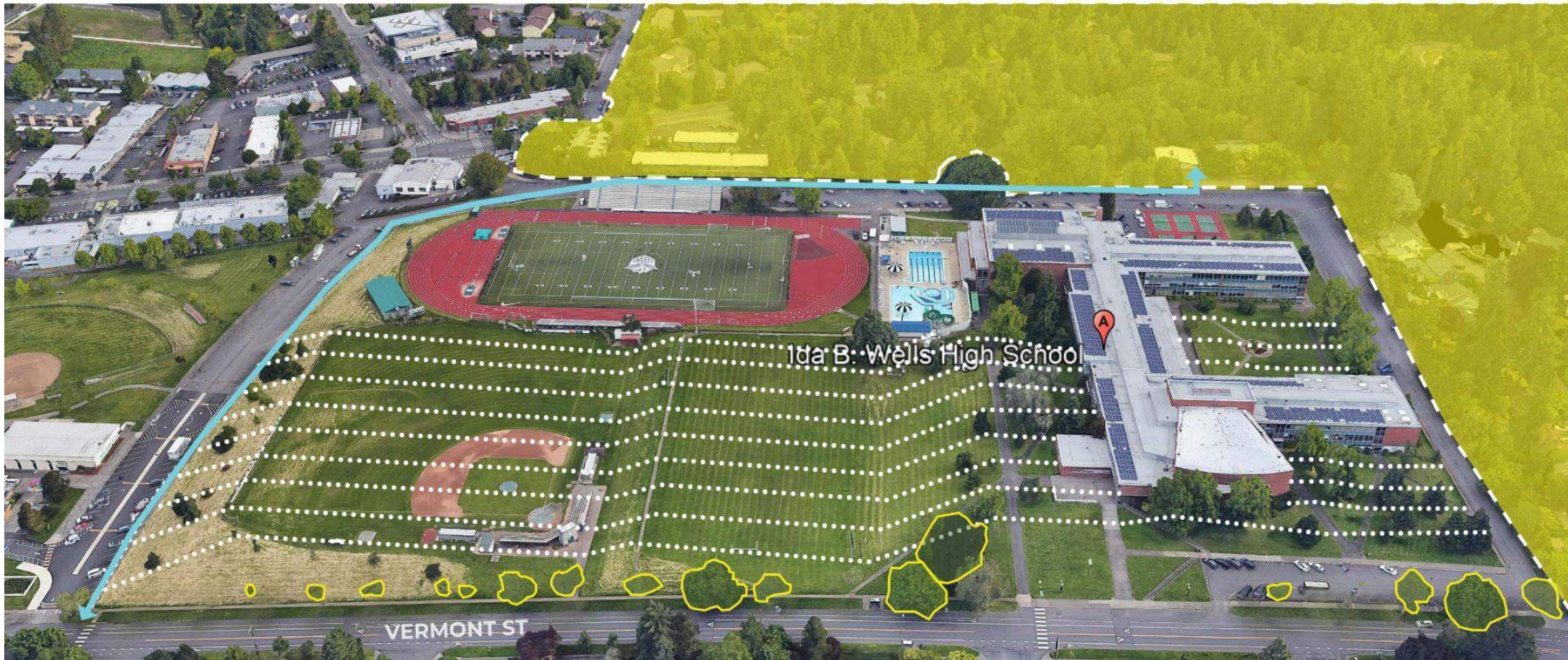
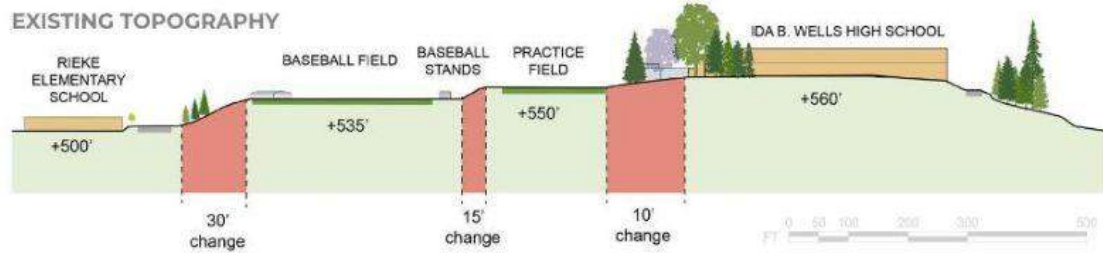
ADDENDUM

Materials from public meetings during the comprehensive planning phase:

- Existing Site Topography
- Driving Factors for Planning and Design
- Planning Phase: Key Design Drivers
- Planning Phase: Early Site Plan Studies
- Existing Site Plan with Key Design Drivers
- Planning Phase: Buildable Area & Arrival, Scheme 1
- Planning Phase: Buildable Area & Arrival, Scheme 2
- Planning Phase: Options
- Building Massing Diagram with Annotations



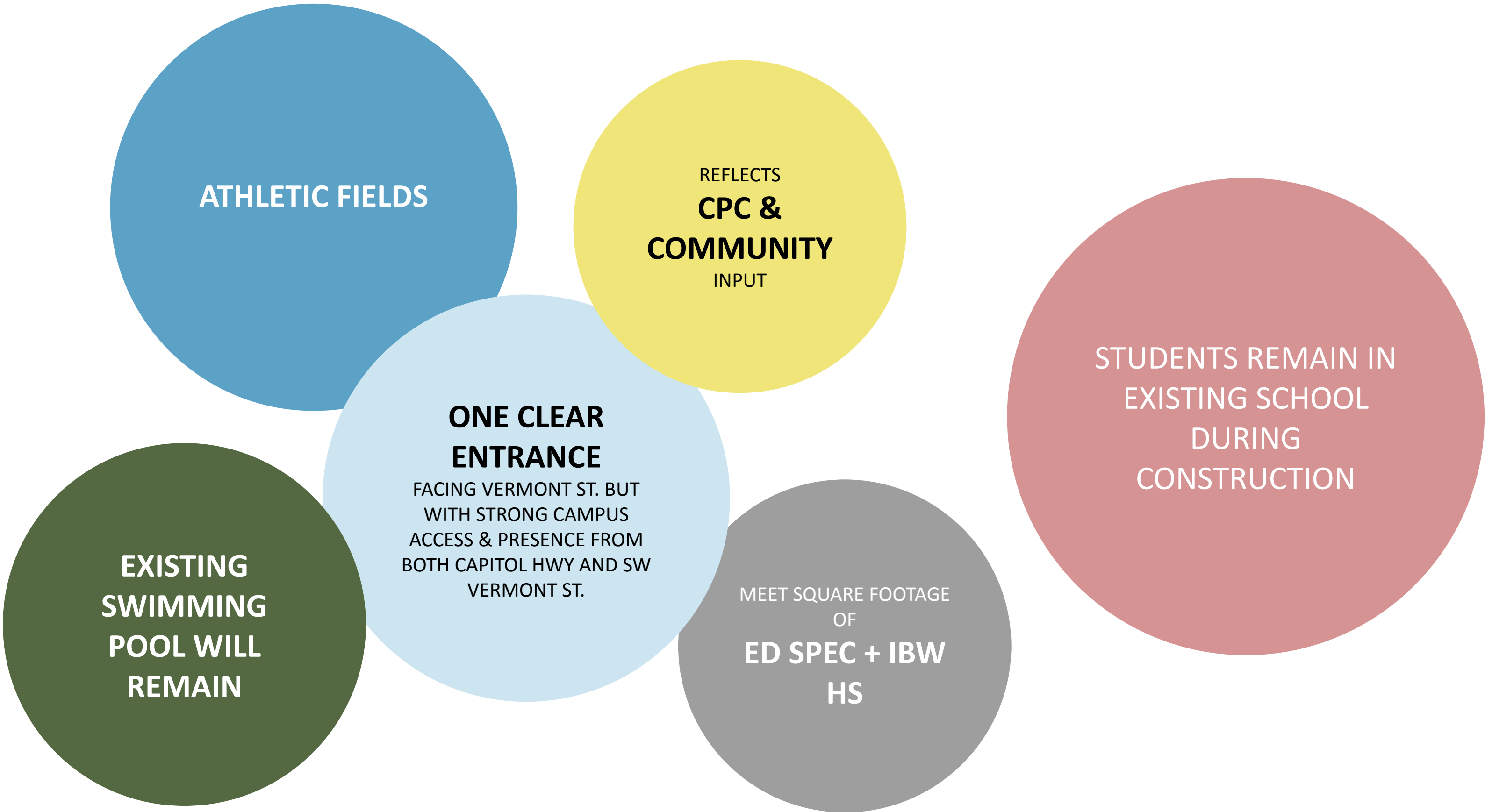
DESIGN DRIVERS:
TOPOGRAPHY
ENHANCE THE ECOLOGY OF THE SITE
CONNECTIVITY



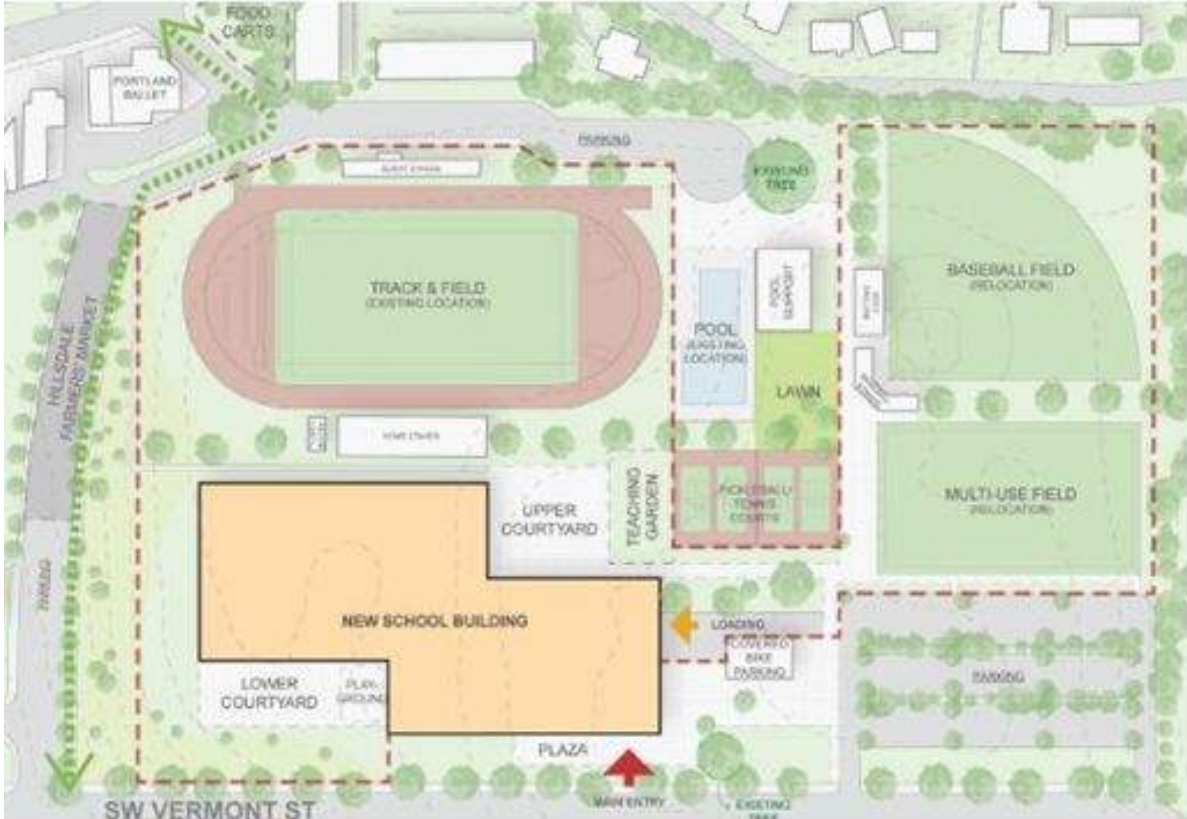
DRIVING FACTORS FOR PLANNING AND DESIGN



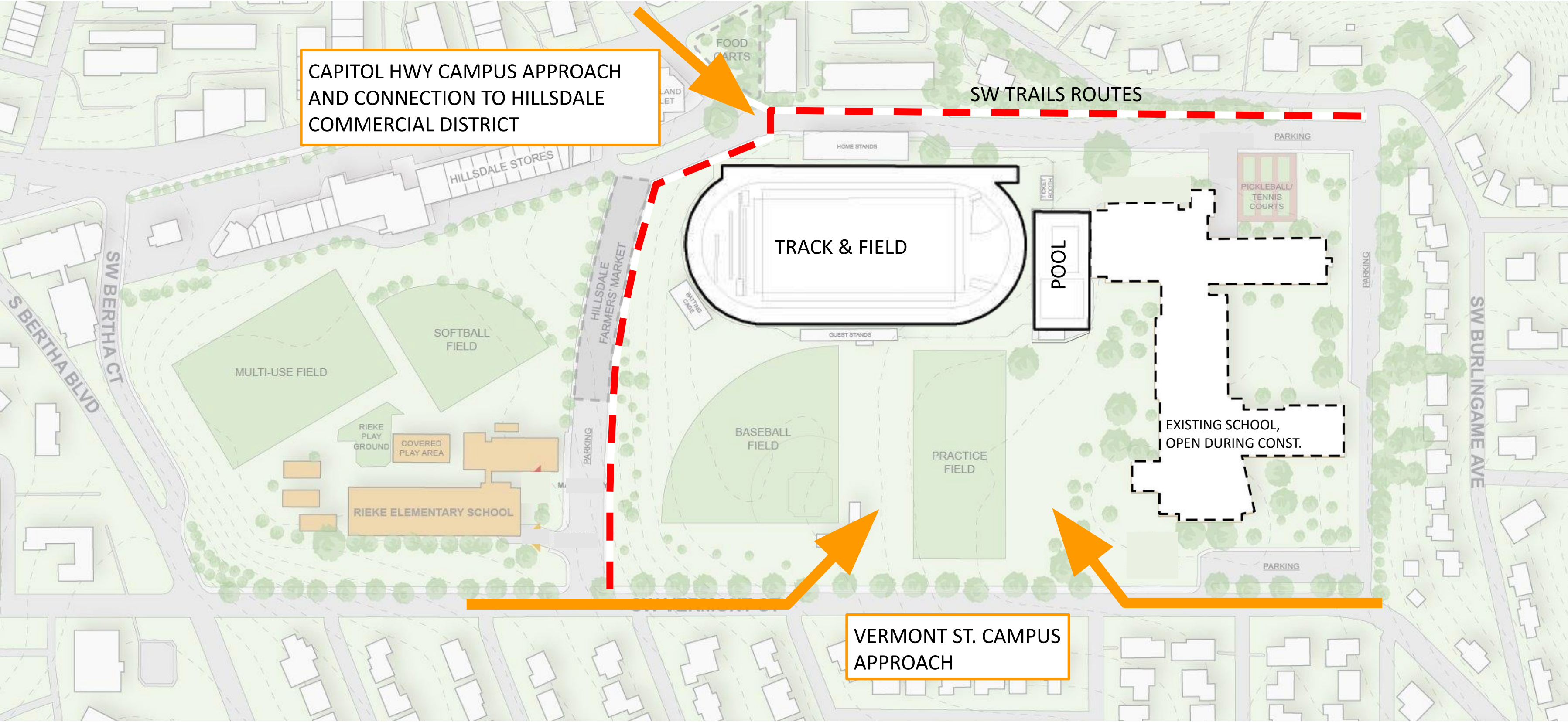
PLANNING PHASE - KEY DESIGN DRIVERS



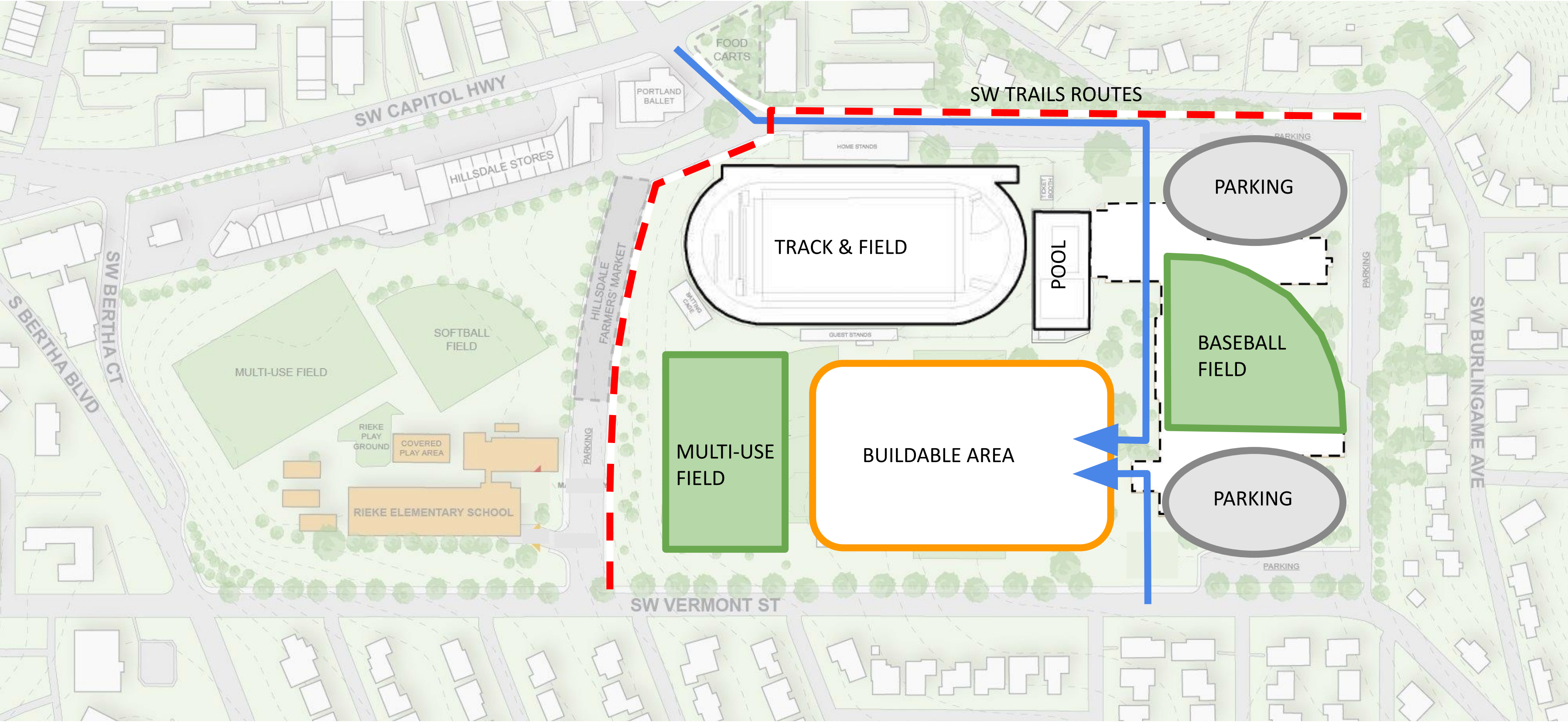
PLANNING PHASE - EARLY SITE PLAN STUDIES



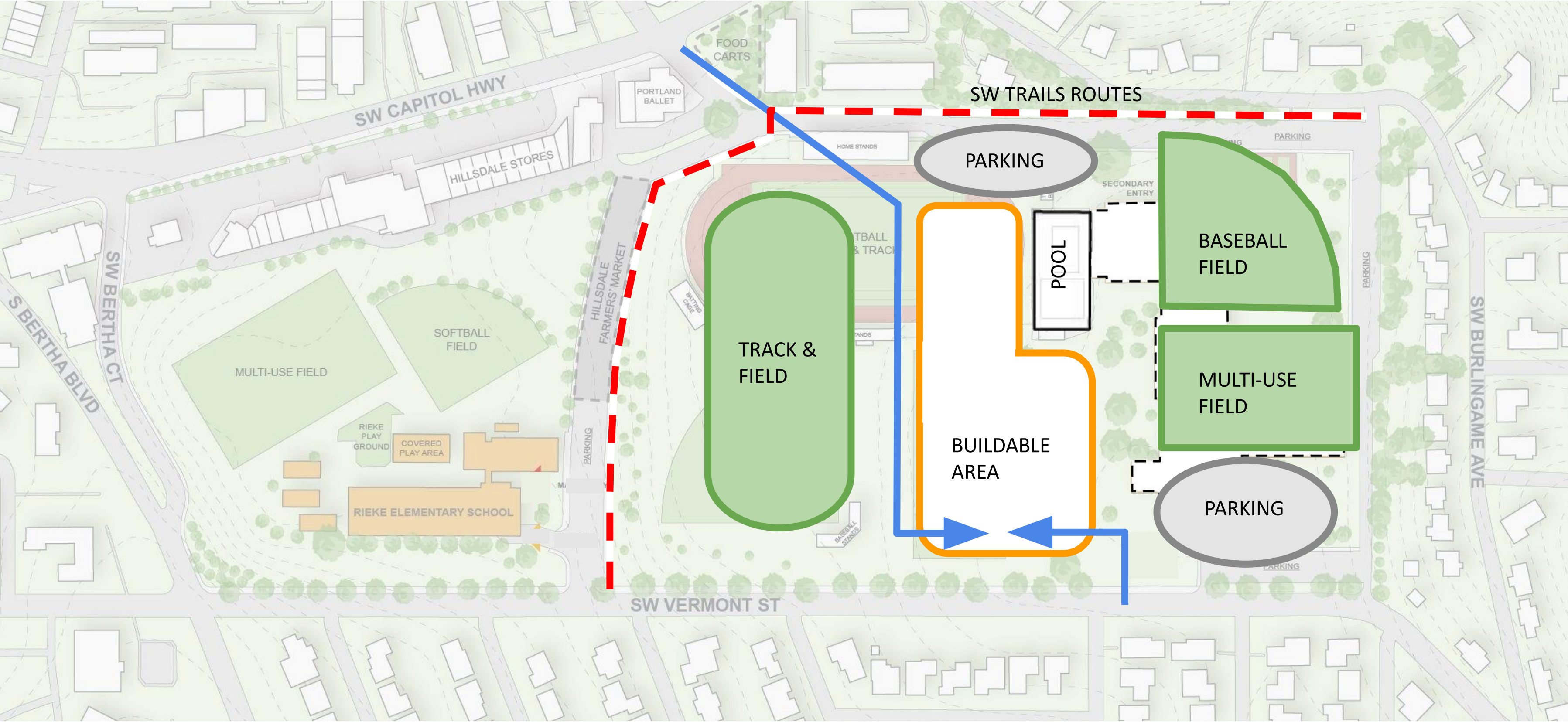
EXISTING SITE - KEY DESIGN DRIVERS



PLANNING PHASE - BUILDABLE AREA & ARRIVAL, SCHEME 1

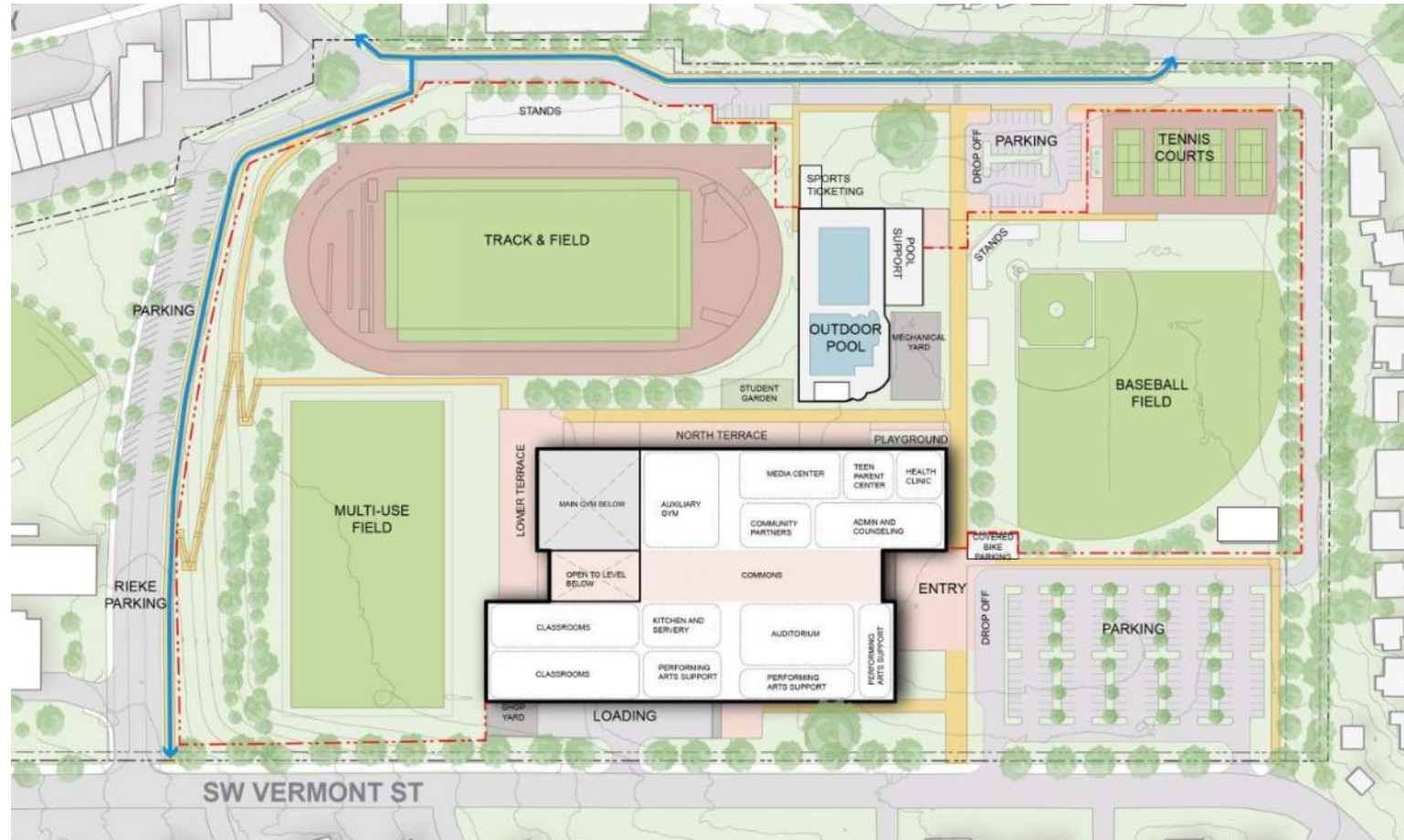


PLANNING PHASE - BUILDABLE AREA & ARRIVAL, SCHEME 2



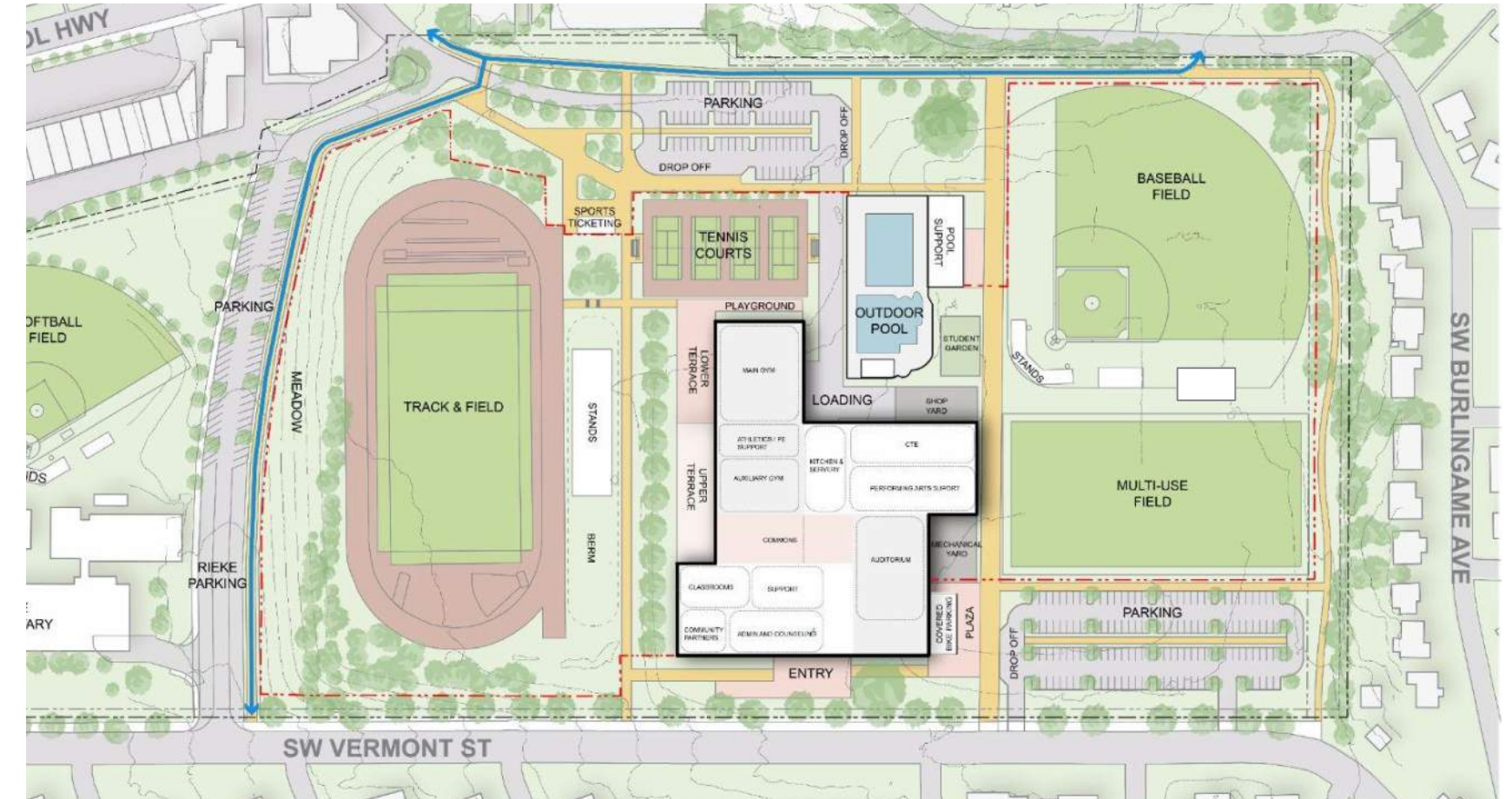
PLANNING PHASE - FINAL OPTIONS

SCHEME 1

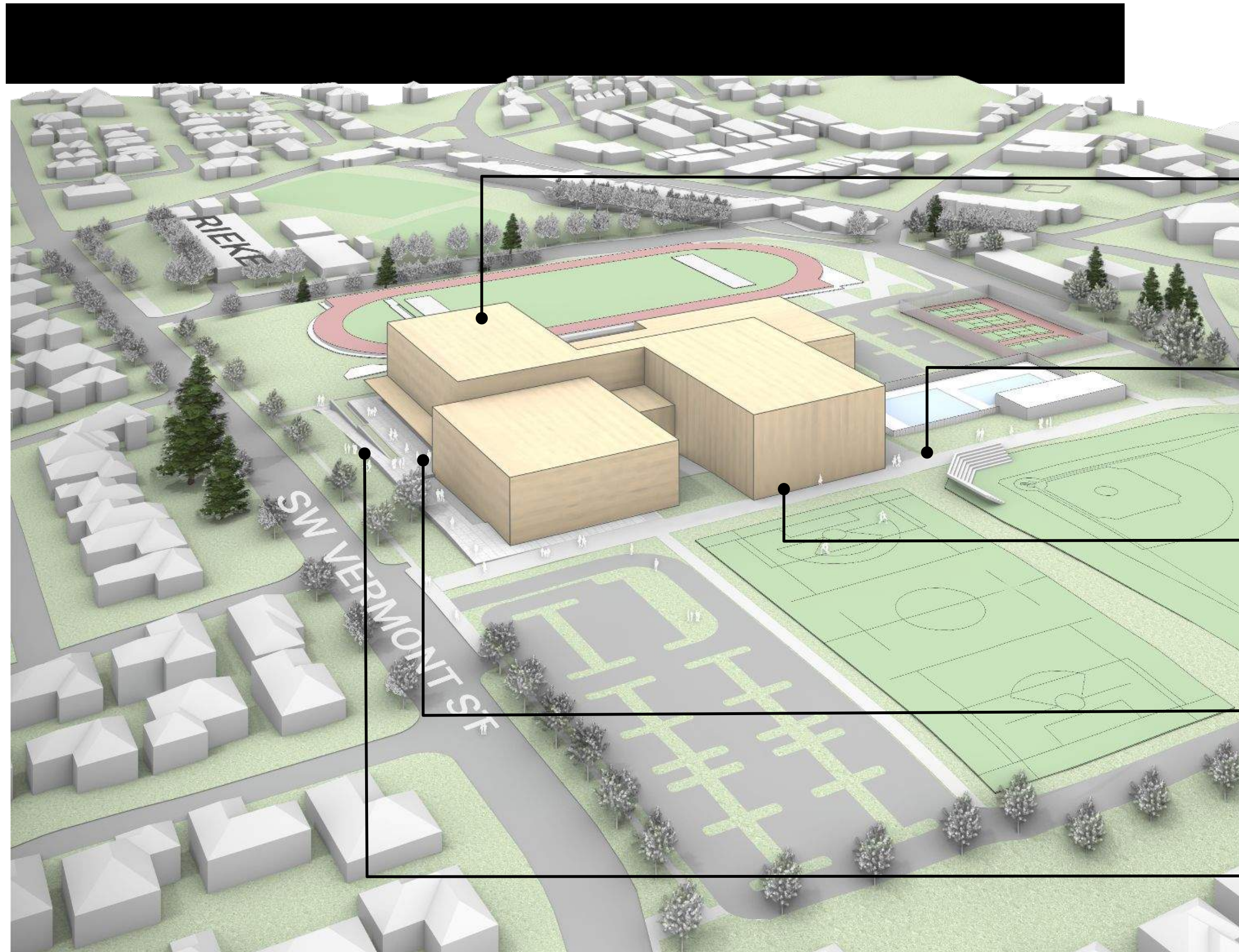


- Track & Field remains in place with improvements
- Minimal Track & Field disruption during construction
- Pathway connection to Rieke parking and fields
- Main parking lot and drop-off near building entry
- Vehicular connection between the two parking lots
- Separation between school and pool offers flexibility with building footprint
- 3-story building height

SCHEME 2



- Track & Field in optimal N-S orientation
- More space for construction staging
- Direct pedestrian connection to Capitol Highway
- Parking lots and drop-offs near Capitol Hwy and Vermont St.
- Vehicular access at opposite ends of building
- Less flexibility with building footprint due to pool and track locations
- 4-story building height



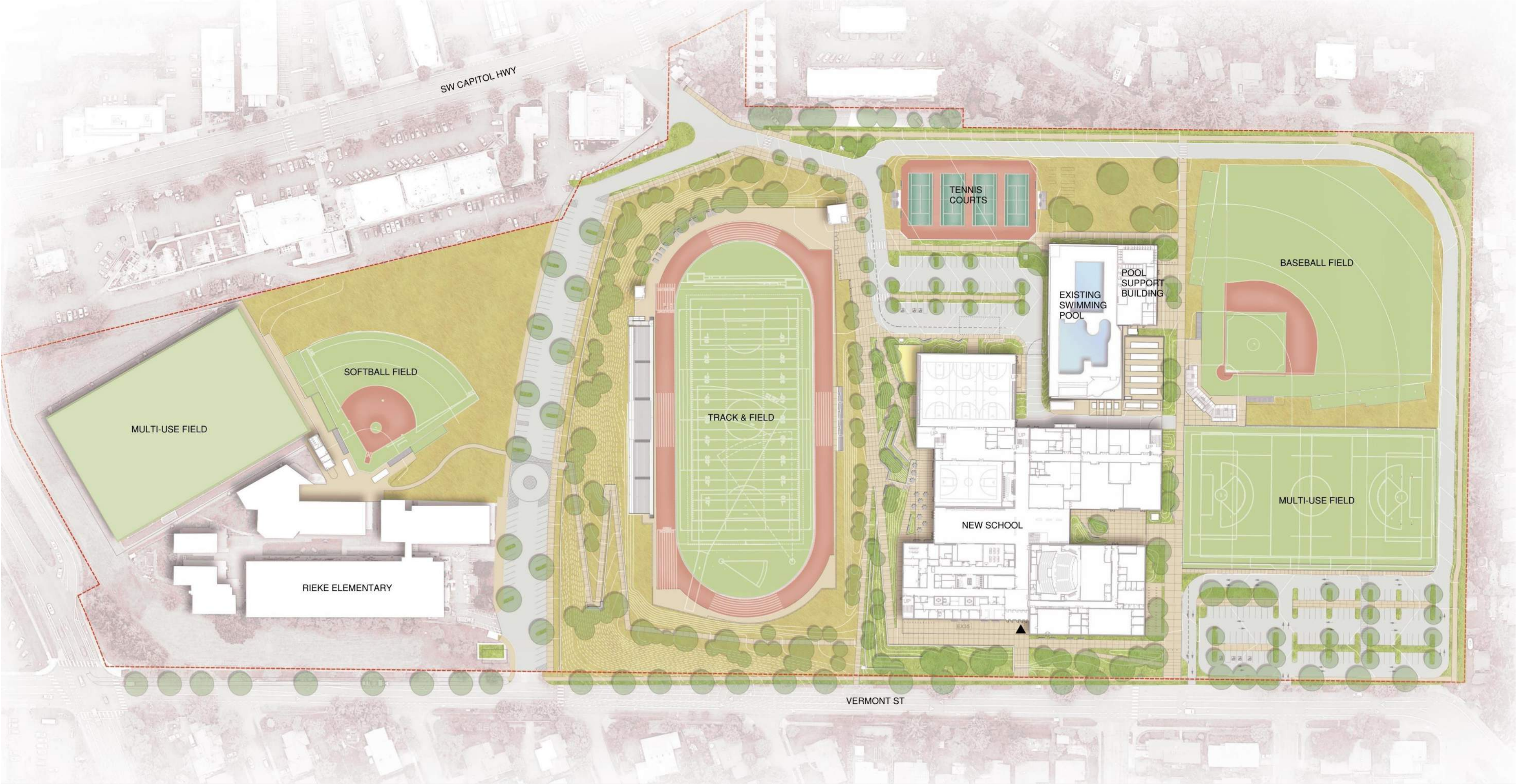
Building orientation and massing allows for **maximum daylighting and minimizes western sun exposure**, reducing building energy consumption and eliminating solar discomfort

Pathways through campus **connect to the SW Trails network and the broader business and residential district**, making the school a beacon of activity in SW Portland

Building massing is optimal for a timber structure, allowing for **reduced embodied carbon emissions, improved indoor air quality, and biophilic design opportunities**.

Main entrance faces Vermont St. with direct pathways **connecting it to both campus approaches and parking lots**

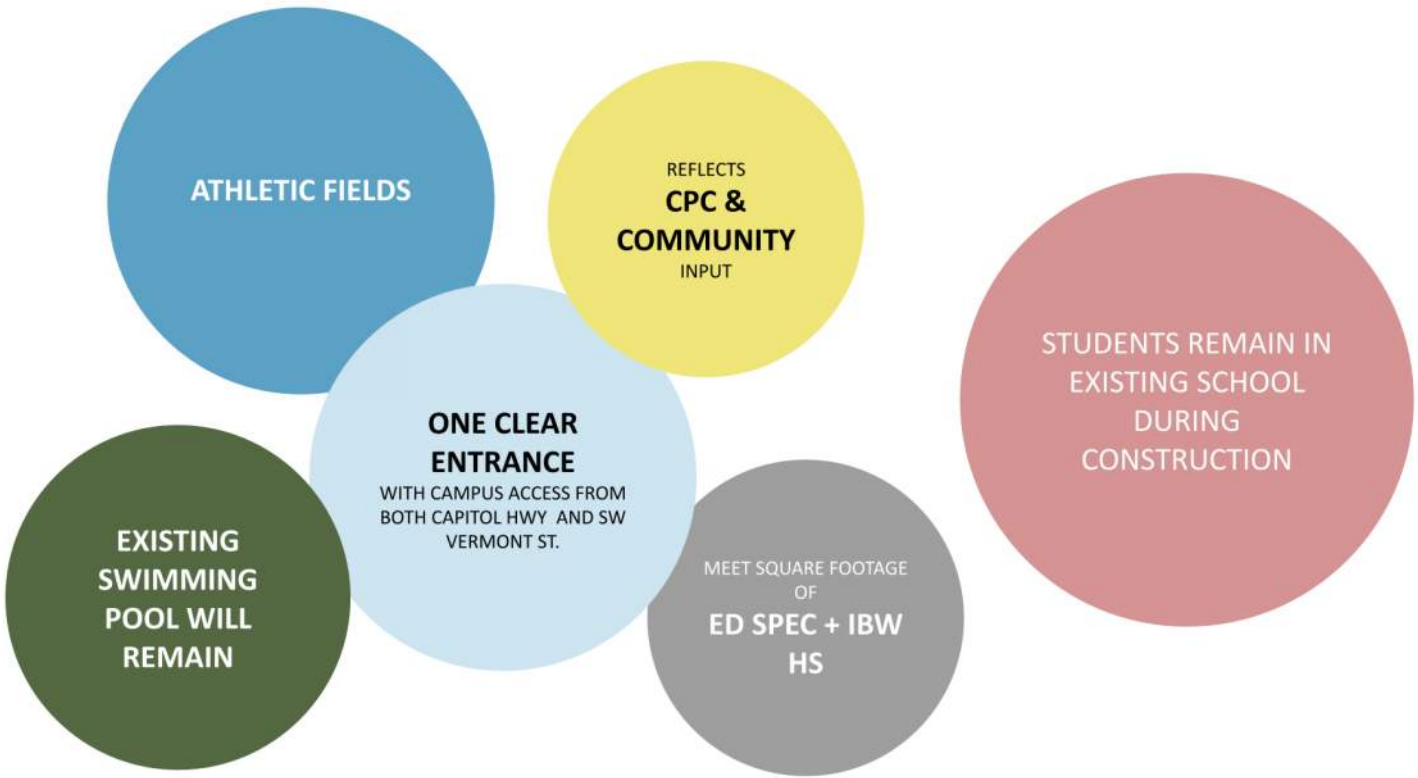
Site configuration allows for universal accessibility, going beyond code to create a **physical place of inclusion** at every scale



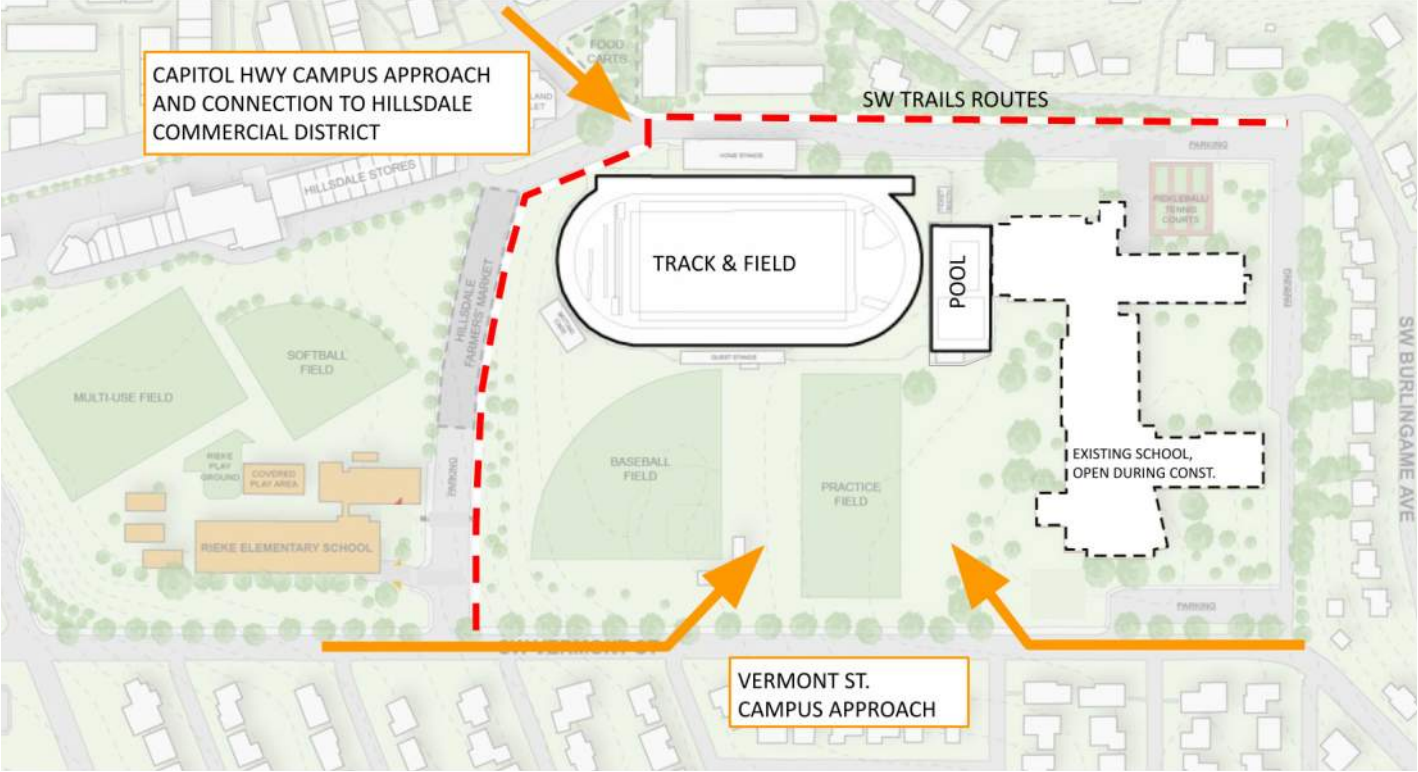
DRIVING FACTORS FOR PLANNING

The project team looked at the Ida B. Wells site comprehensively to develop a range of site planning possibilities. Early on in this analysis, major site design drivers were identified, both physical components of the site (sports fields, public swimming pool, the location of the existing school), as well as parameters and guidelines provided by PPS including the education specification requirements for the building itself.

The design team also analyzed solar orientation of the building massing, the site topography, and site zoning requirements. Additionally, the design team studied the approaches to the campus and opportunities for the building entry to connect to the northwest approach from SW Capitol Highway, and/or the southern approach from SW Vermont Street.



Site Diagram of Driving Factors





Representative Image of Project (Perspective)



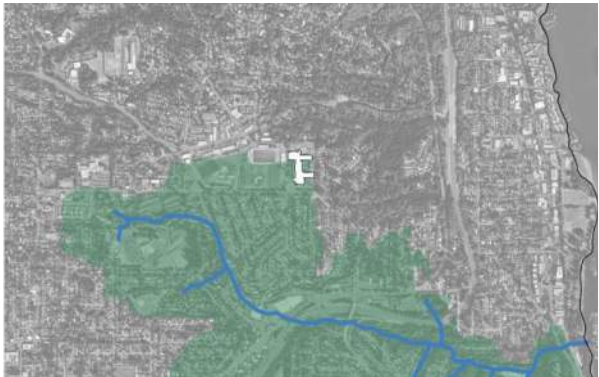
West Facade



View from SW Vermont St.



Attendance Area

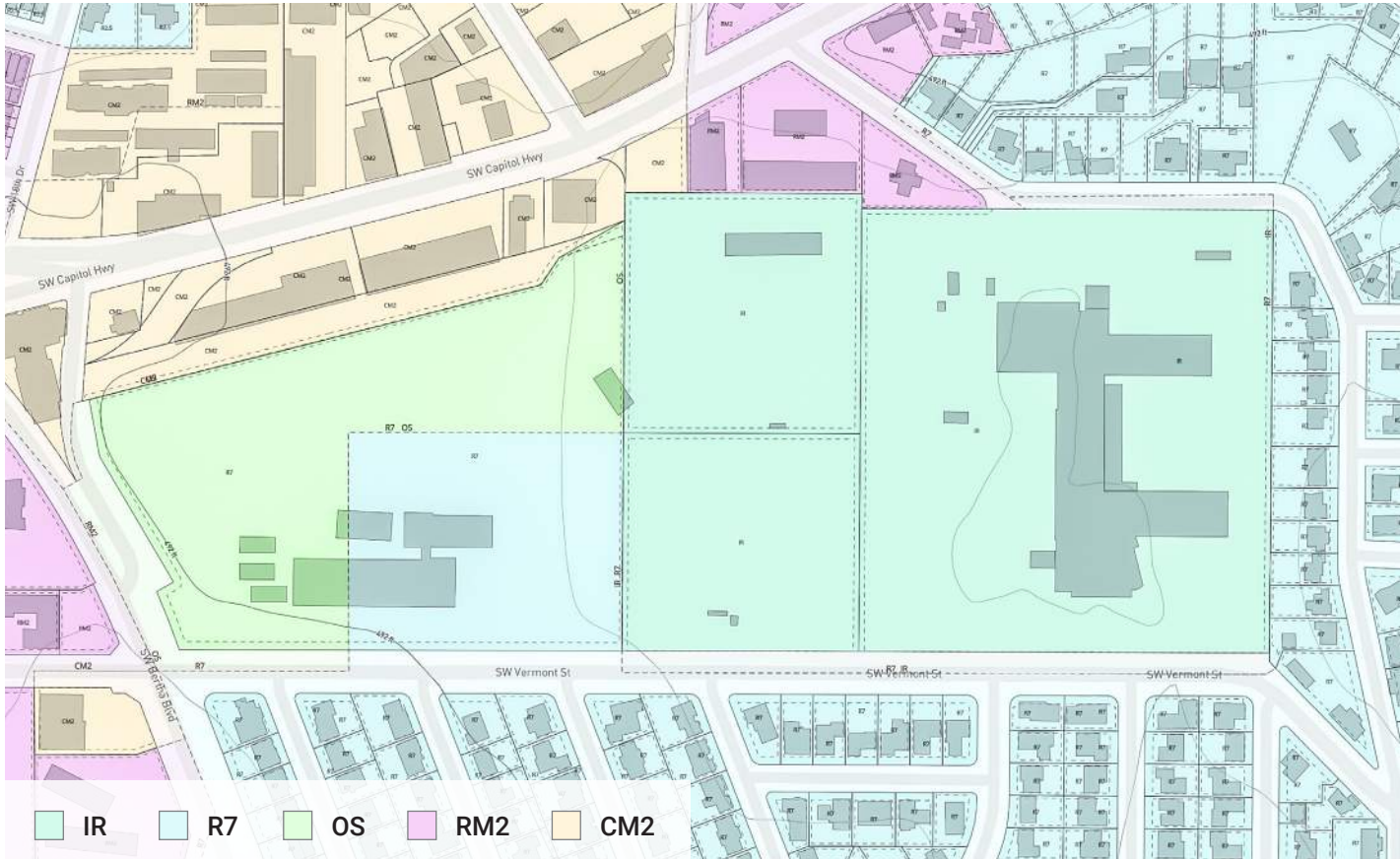


Stephen's Creek

ZONING

The Ida B. Wells site is made up of 4 separate tax lots (including Rieke Elementary) located between SW Capitol Highway and SW Burlingame Avenue to the north, SW Burlingame Avenue to the East, SW Vermont Street to the South, and SW Bertha to the West. Zoning designations on the site include IR – Institutional Residential, R7 – Single Dwelling Residential, OS – Open Space, with a Design Overlay on the IR portions only. The site falls within the Hillsdale Plan District. School uses are a Conditional Use in the OS and R7 zones.

In the IR zone, High Schools are a Limited use that can be approved either through a Conditional Use or through either a Conditional Use Master Plan or an Impact Mitigation Plan (for a large campus over 5 acres in size). Applicant may choose which process. All streets surrounding the site are local service transit streets (i.e. not transit streets). But the entire site is located in a Pedestrian district. The private street that connects SW Capital Highway to SW Vermont is classified as a Neighborhood Walkway.



The following table highlights the main zoning requirements for the site. Of note in the zoning table is the height limit at 75' and the 50' setback regulations for sports fields and courts where the site abuts a residential property.

STANDARD	REQUIREMENT
FAR	
Max FAR	2:1
Min Density/FAR	No requirement
Bonus Density	N/A
Height	
Max Height	75 ft
Setbacks	
Lot line abutting or across the street from OS, RF-R2.5	1 foot for every 2 feet of building height, but not less than 10 feet
Lot line abutting or across the street from RM1-RMP, IR zone	
Lot line abutting or across the street from C, CI, E or zone	
Max Building Setback	10 ft
Maximum Building Coverage	70% of site area
Minimum Landscape Area	20% of site area
Maximum Building Length	None
Landscaping Abutting an R-zoned lot	10 feet to the L3
Landscaping Across the street from an R-zoned lot	10 feet to the L1
Building Facade Articulation required	No
Ground Floor Windows required	Yes
Transit Street Main Entrance	No
Title 11 Trees	Applies – 1/3 of trees over 12" and all trees over 20" should be preserved

These additional standards from the multi-dwelling zones would apply in the IR zone if the site is developed as an "Institutional Campus" as approved though a CUMP or IMP:

Street-Facing Facades (33.120.232)	- Windows must cover 40% of ground floor wall area for walls 5 feet or closer to the street lot line. - Windows must cover at least 25% of ground floor wall for walls over 5 feet from street lot line.
Required Outdoor and Common Area (33.120.240)	- At least 10% of the total site must be provided as common area – either as outdoor courtyards or play areas
Pedestrian Standards (33.120.255)	5 foot wide hard-surfaced pedestrian paths must connect the building to the street and connect all to all other areas of the site.

R7 and OS Development Standards

Maximum FAR	0.5 to 1
Maximum Height	50 ft
Minimum Setback R7	1 foot for every 2 feet of building height but in no case less than 15 ft.
Minimum Setback OS (33.100.200.B.2)	1 foot for each foot of building height
Maximum Setback	20 ft
Maximum Building Coverage	50%
Minimum Landscaped Area	25%
Buffering from Across the street from a Residential Zone	15 feet to the L1
Outdoor activity facilities – min setbacks	50 feet from abutting residential properties
Pedestrian Standards	5 foot wide hard-surfaced pedestrian paths must connect the building to the street and connect all to all other areas of the site.

Hillsdale Plan District Requirements

Drive-Through Facilities	Are prohibited
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BORA

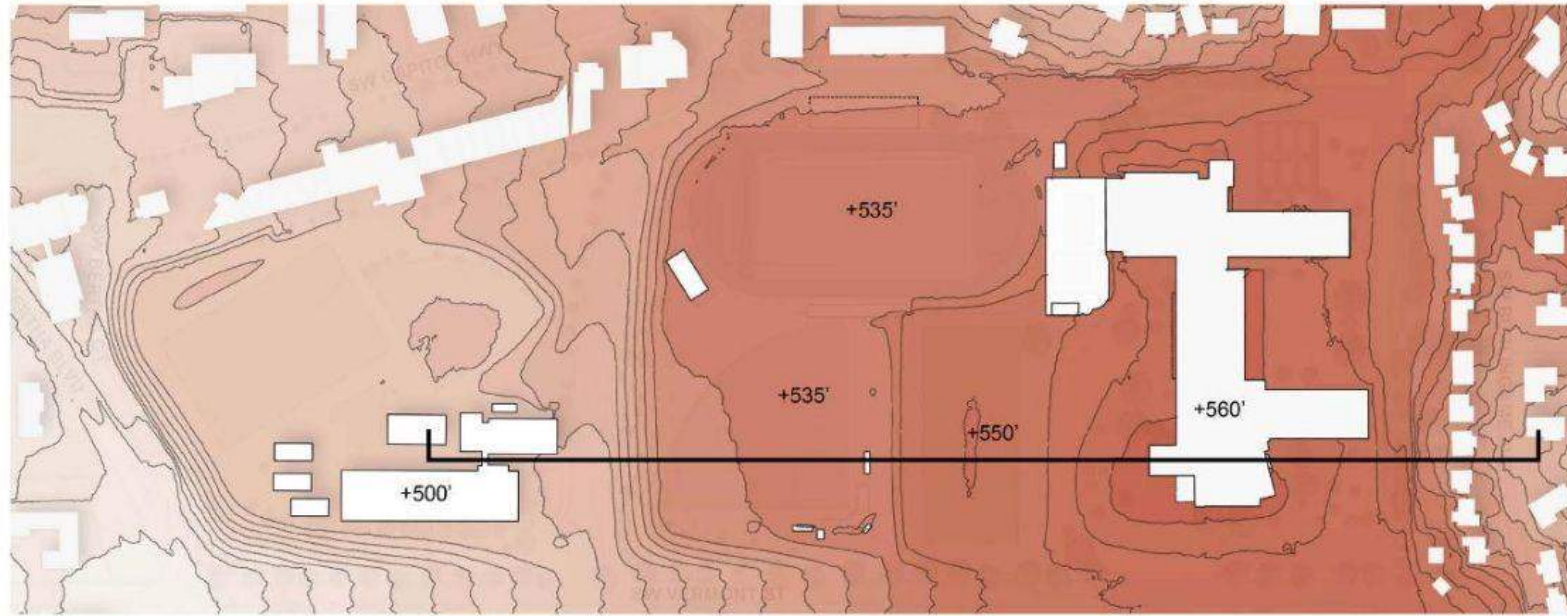
09/12/2024

Ida B. Wells Modernization – Design Advice Request #1 Application

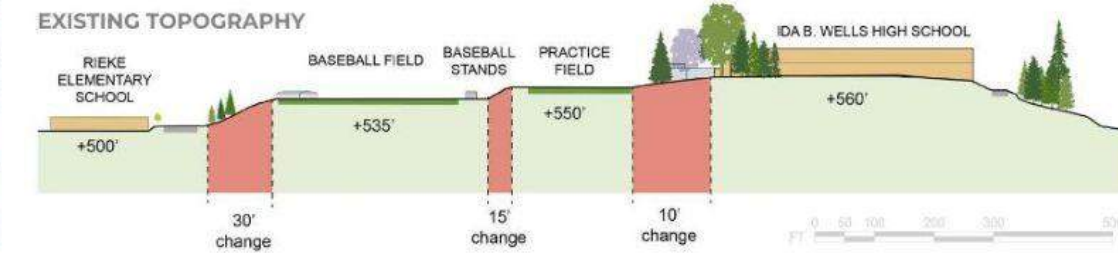
ADDENDUM

Materials from public meetings during the comprehensive planning phase:

- Existing Site Topography
- Driving Factors for Planning and Design
- Planning Phase: Key Design Drivers
- Planning Phase: Early Site Plan Studies
- Existing Site Plan with Key Design Drivers
- Planning Phase: Buildable Area & Arrival, Scheme 1
- Planning Phase: Buildable Area & Arrival, Scheme 2
- Planning Phase: Options
- Building Massing Diagram with Annotations



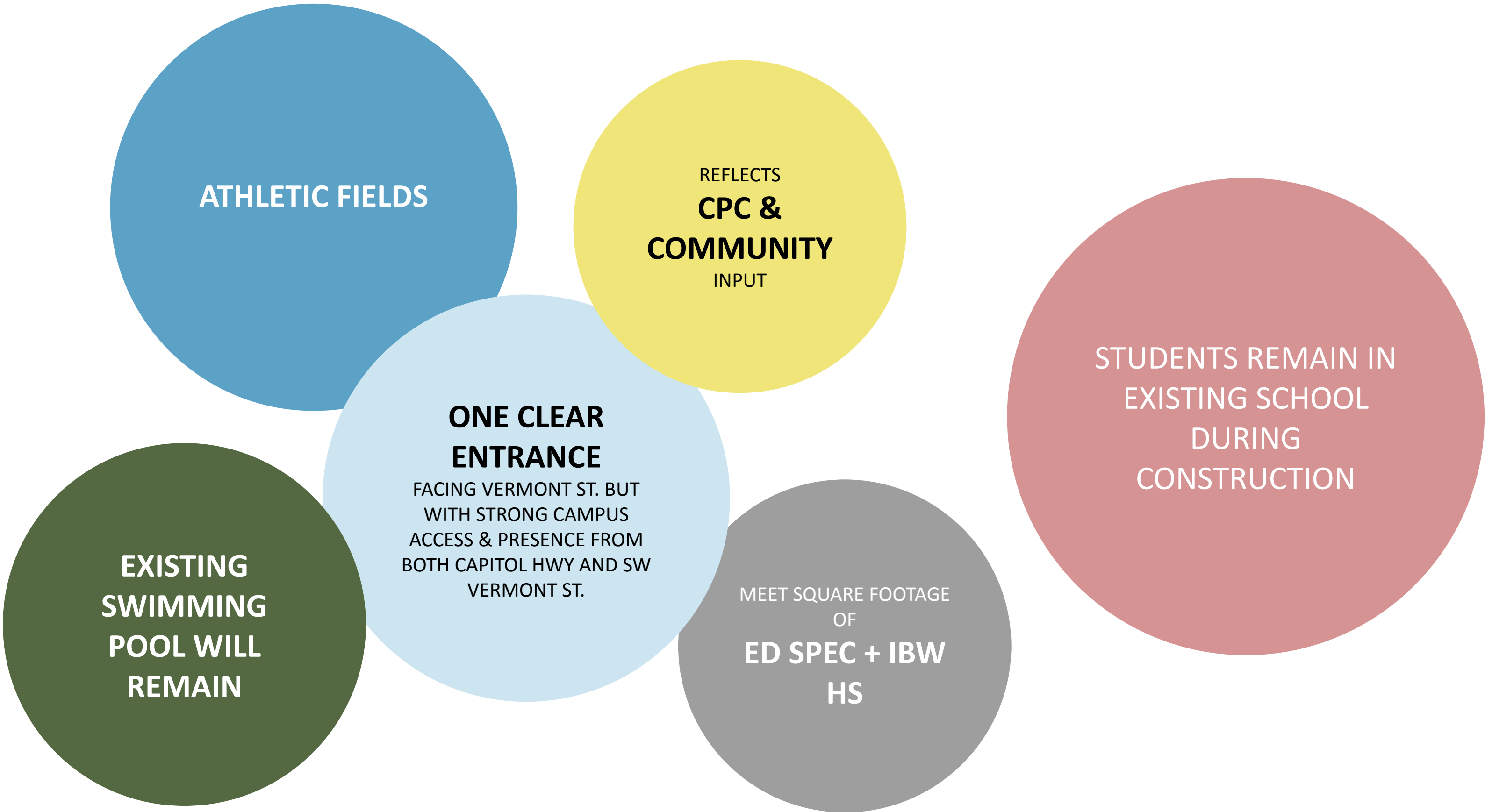
DESIGN DRIVERS:
TOPOGRAPHY
ENHANCE THE ECOLOGY OF THE SITE
CONNECTIVITY



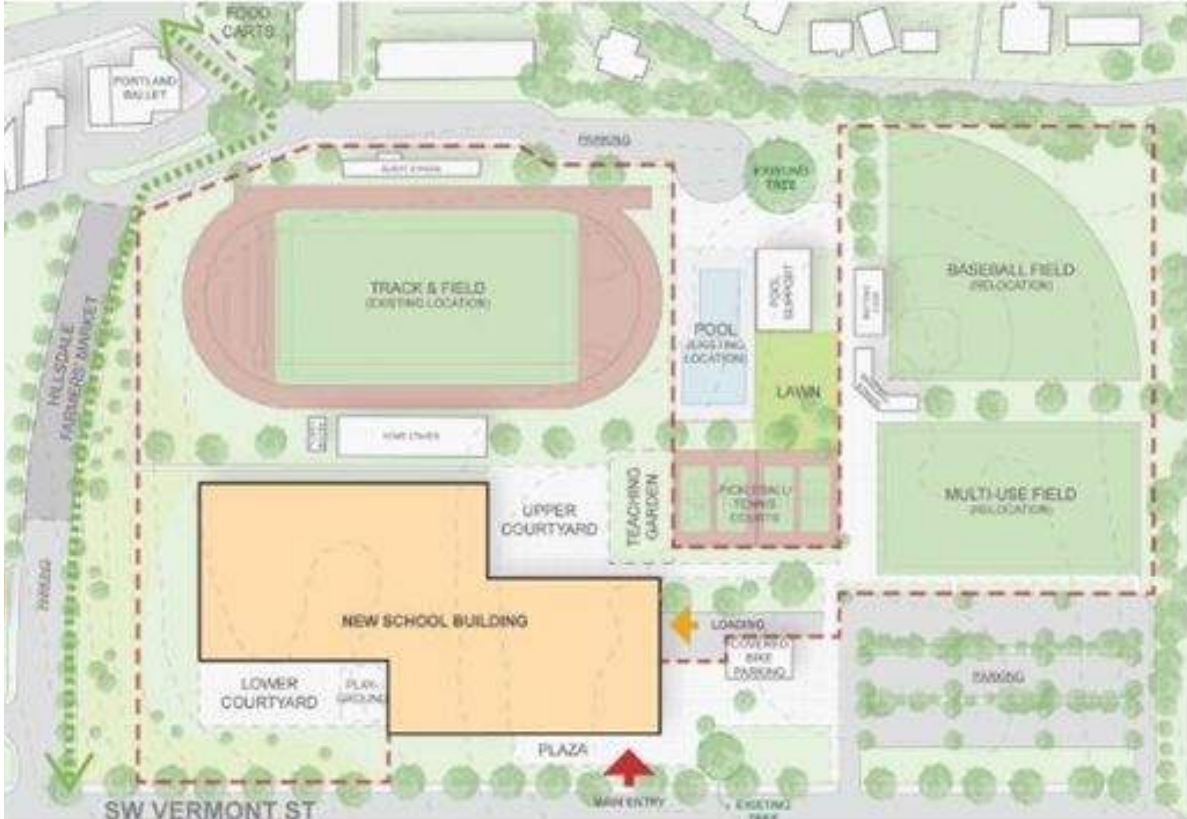
DRIVING FACTORS FOR PLANNING AND DESIGN



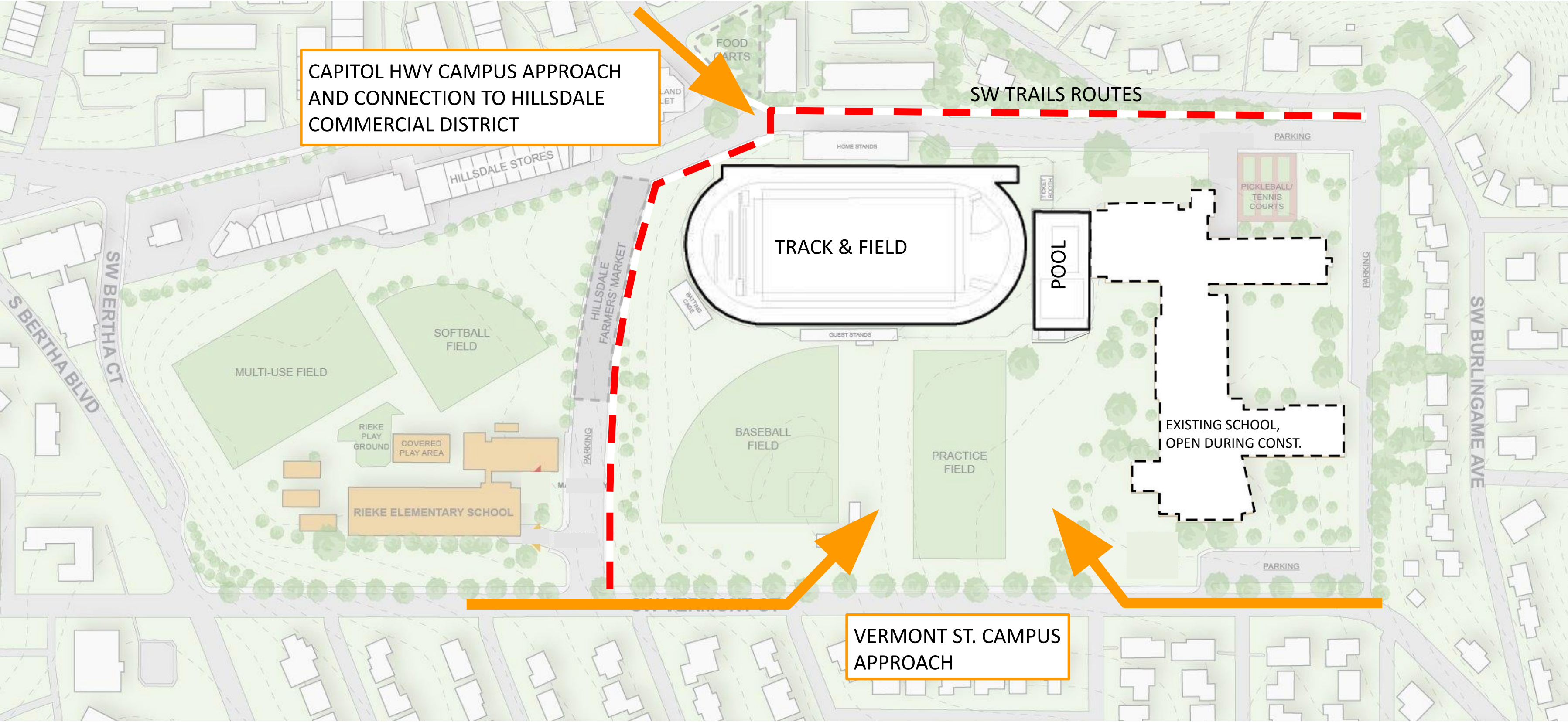
PLANNING PHASE - KEY DESIGN DRIVERS



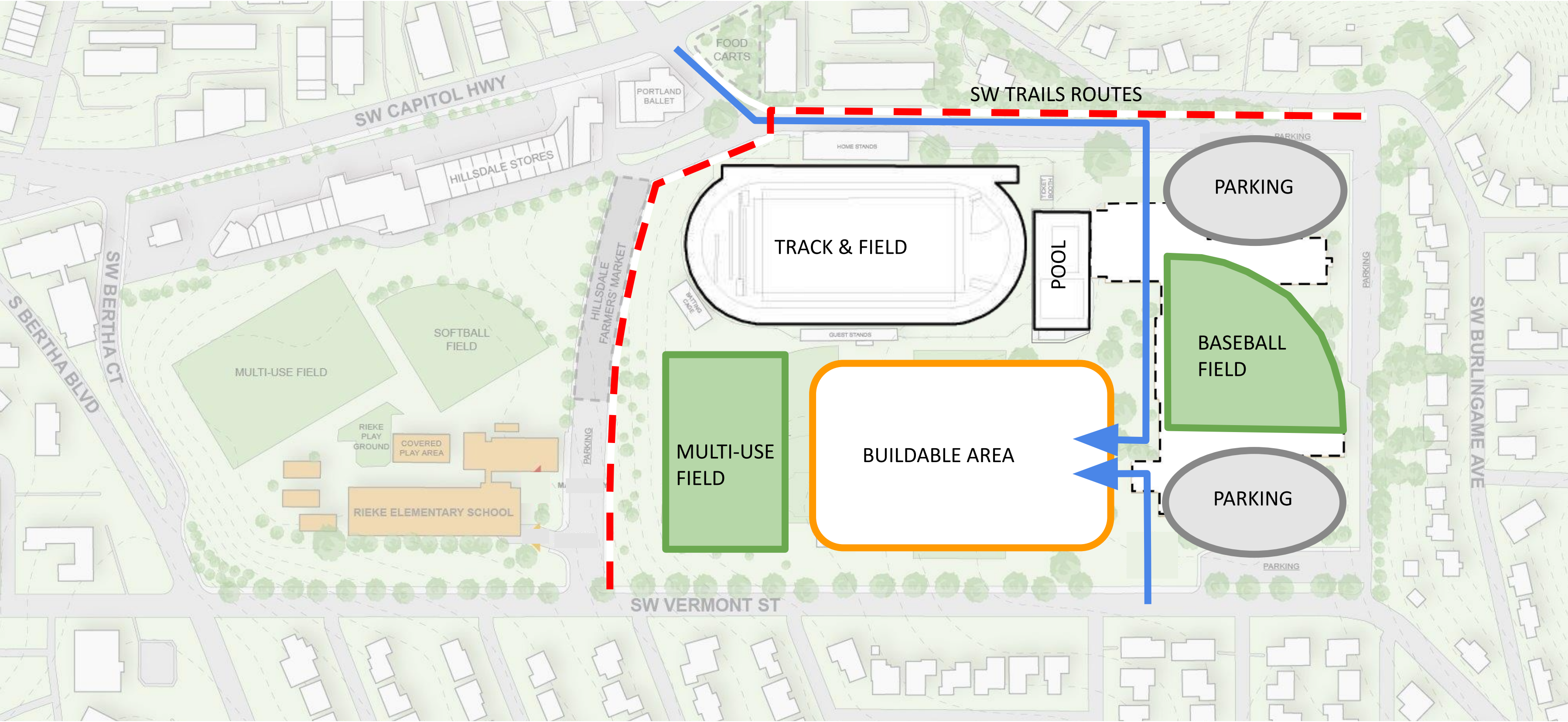
PLANNING PHASE - EARLY SITE PLAN STUDIES



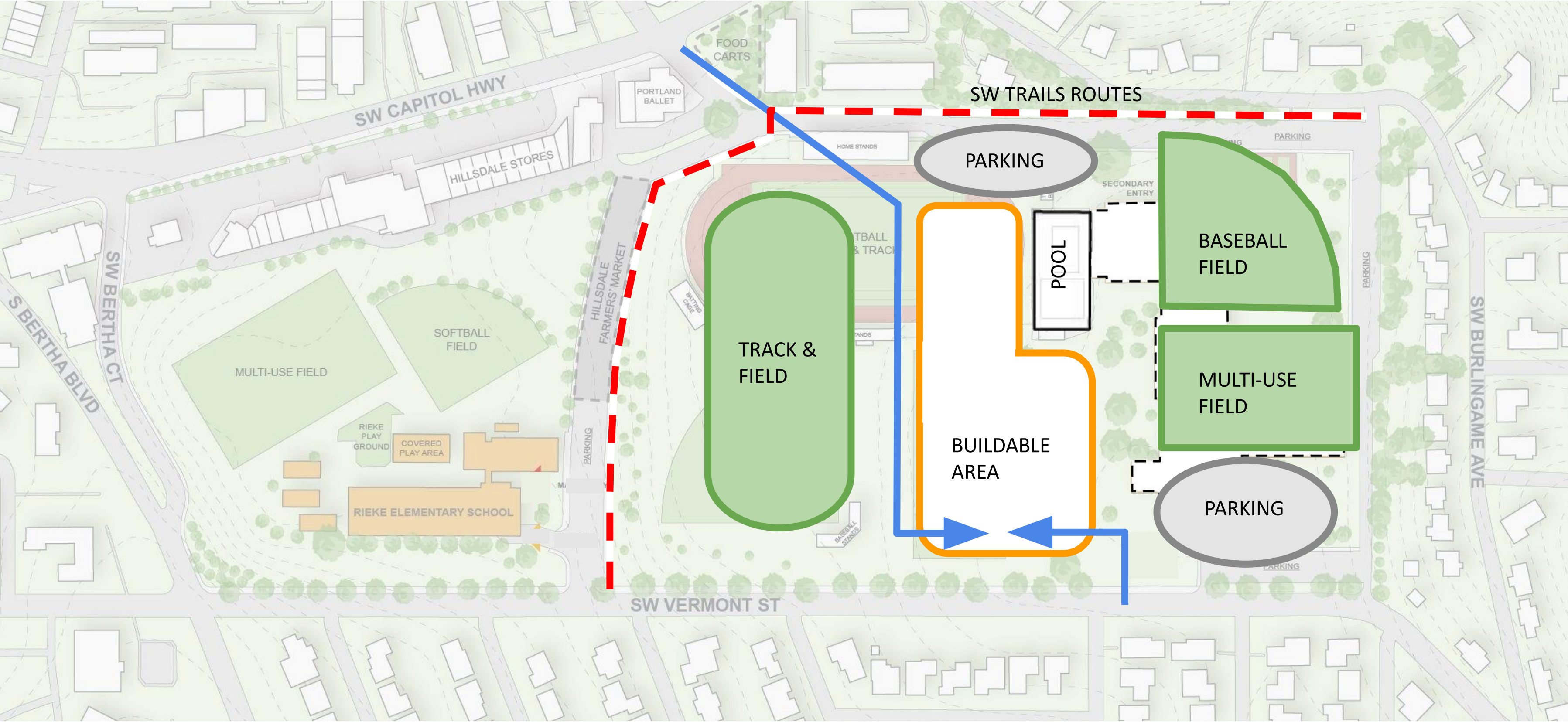
EXISTING SITE - KEY DESIGN DRIVERS



PLANNING PHASE - BUILDABLE AREA & ARRIVAL, SCHEME 1

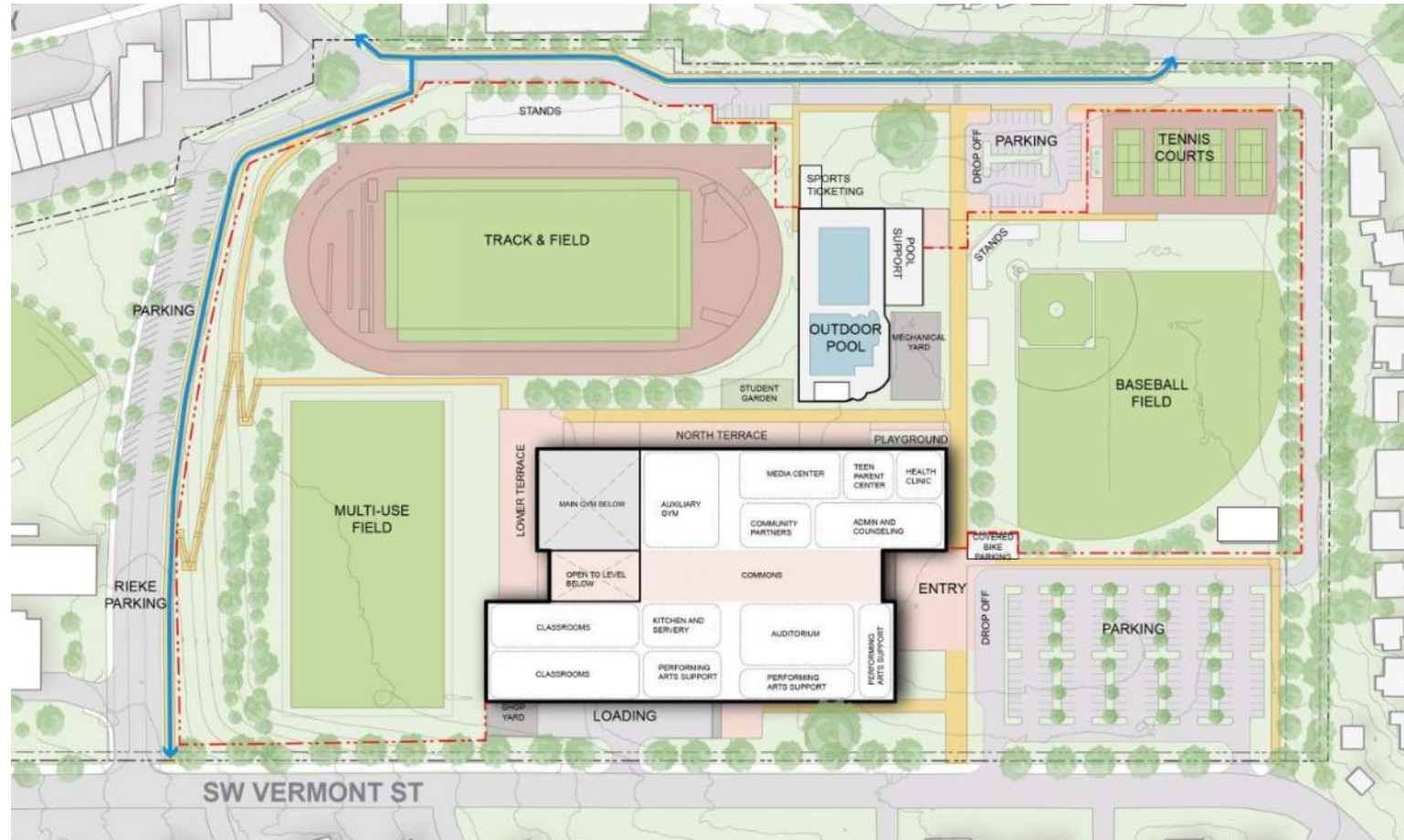


PLANNING PHASE - BUILDABLE AREA & ARRIVAL, SCHEME 2



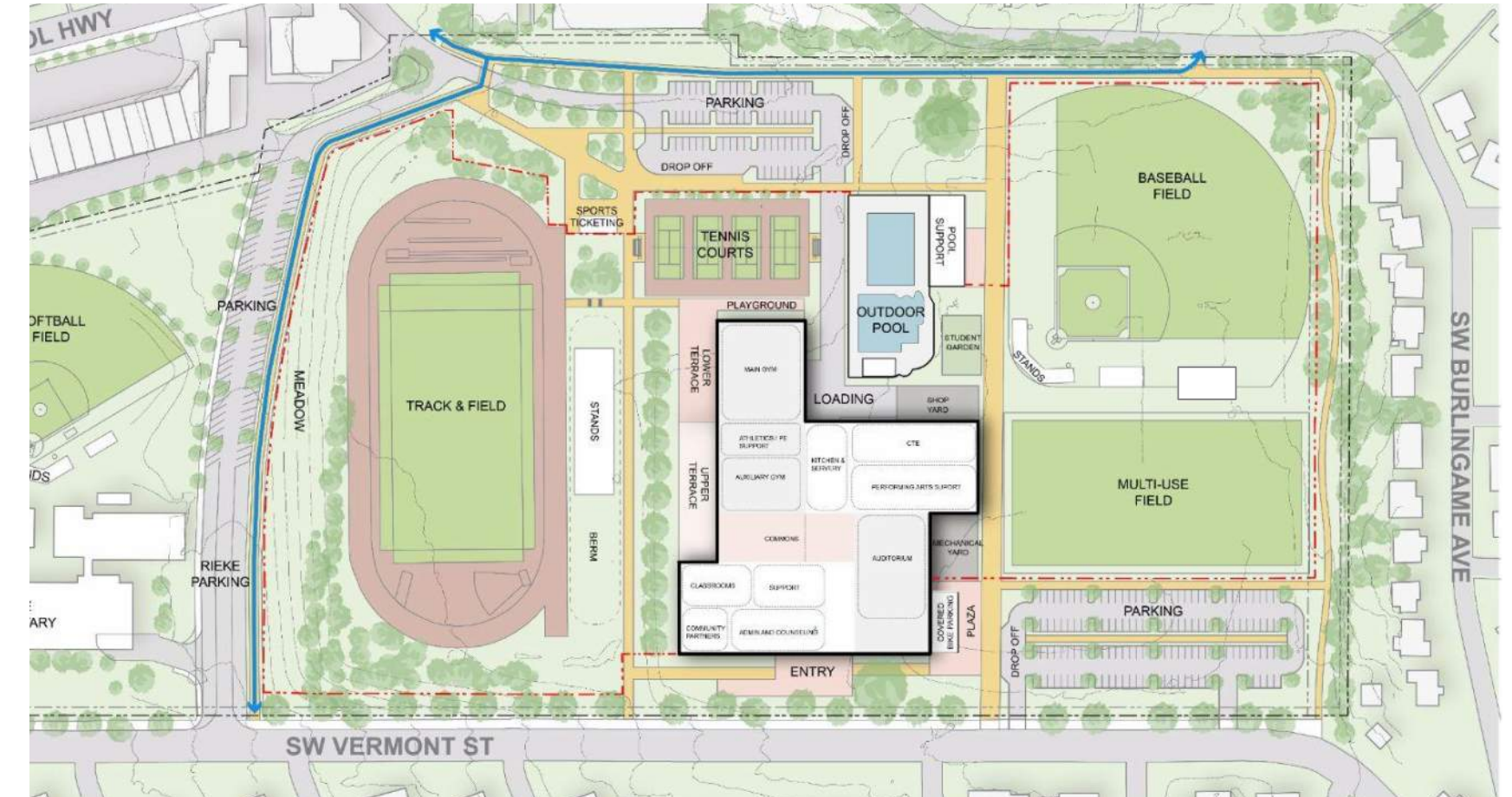
PLANNING PHASE - FINAL OPTIONS

SCHEME 1

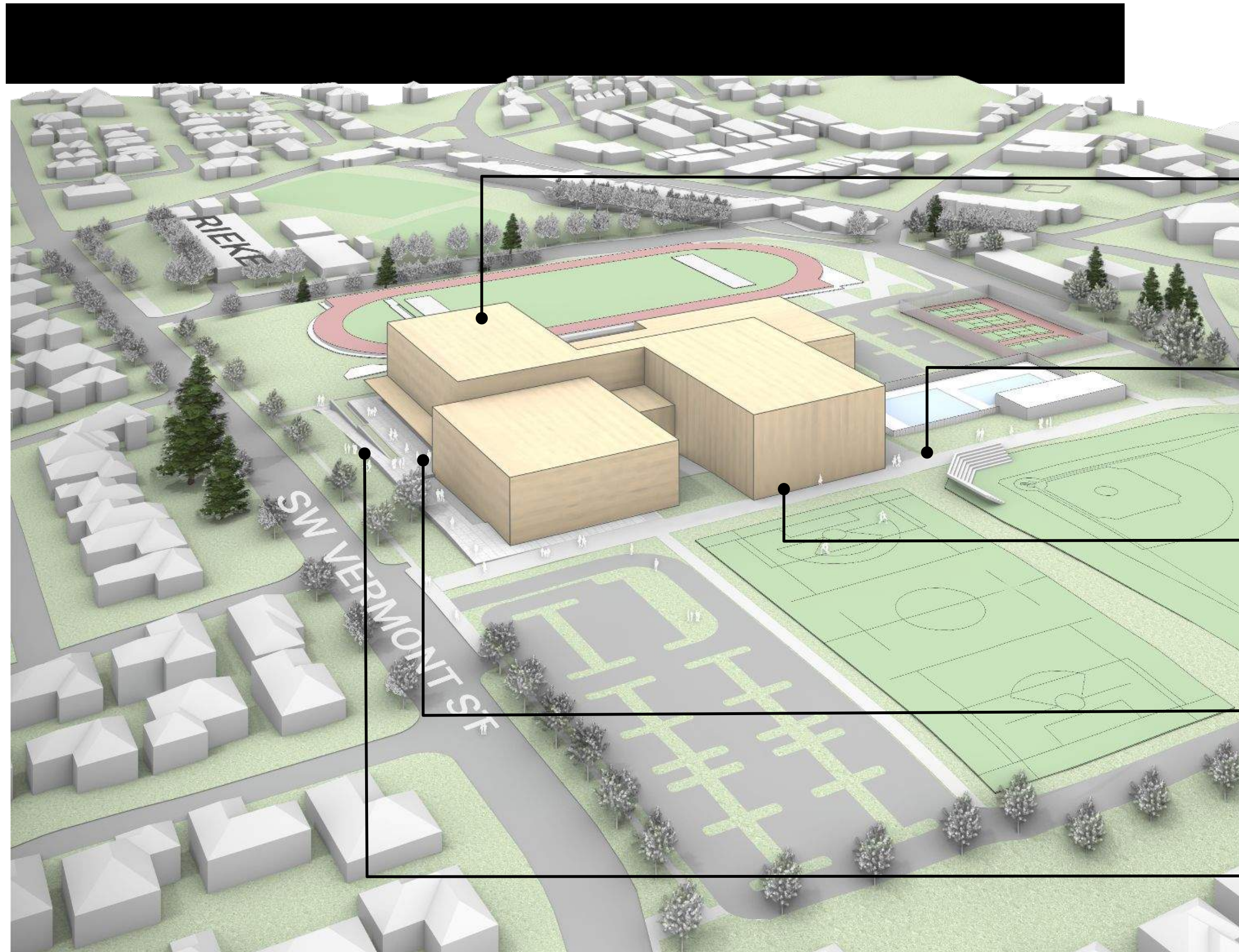


- Track & Field remains in place with improvements
- Minimal Track & Field disruption during construction
- Pathway connection to Rieke parking and fields
- Main parking lot and drop-off near building entry
- Vehicular connection between the two parking lots
- Separation between school and pool offers flexibility with building footprint
- 3-story building height

SCHEME 2



- Track & Field in optimal N-S orientation
- More space for construction staging
- Direct pedestrian connection to Capitol Highway
- Parking lots and drop-offs near Capitol Hwy and Vermont St.
- Vehicular access at opposite ends of building
- Less flexibility with building footprint due to pool and track locations
- 4-story building height



Building orientation and massing allows for **maximum daylighting and minimizes western sun exposure**, reducing building energy consumption and eliminating solar discomfort

Pathways through campus **connect to the SW Trails network and the broader business and residential district**, making the school a beacon of activity in SW Portland

Building massing is optimal for a timber structure, allowing for **reduced embodied carbon emissions, improved indoor air quality, and biophilic design opportunities**.

Main entrance faces Vermont St. with direct pathways **connecting it to both campus approaches and parking lots**

Site configuration allows for universal accessibility, going beyond code to create a **physical place of inclusion** at every scale

	A	B	C	D	E	F
1	ENDORSEMENT	INFO1	INFO2	NAME	ADDRESS/IO ADDRESS	CITYSTATEZIP/ADDRESSEE
2	RETURN SERVICE REQUESTED			MORDEN DALE T & MORDEN JOAN C	6403 SW BURLINGAME PL	PORTLAND OR 97239-2685
3	RETURN SERVICE REQUESTED			QUEEN GRAEME & JENNIFER STEVENS	6411 SW BURLINGAME PL	PORTLAND OR 97239
4	RETURN SERVICE REQUESTED			JOANNE R MACNAUGHTON TR	6485 SW BURLINGAME PL	PORTLAND OR 97239
5	RETURN SERVICE REQUESTED			HUESTER BRYAN & HUESTER CHERI	6487 SW BURLINGAME PL	PORTLAND OR 97239
6	RETURN SERVICE REQUESTED			BAACK DONALD & BAACK V GAILE	6495 SW BURLINGAME PL	PORTLAND OR 97239-7001
7	RETURN SERVICE REQUESTED			EAGLE N WOLF LLC	1517 SW SUNSET BLVD	PORTLAND OR 97239
8	RETURN SERVICE REQUESTED			LEVY SHAWN A	6515 SW 19TH AVE	PORTLAND OR 97239
9	RETURN SERVICE REQUESTED			WORLD OF FLAVORS INC	1523 SW SUNSET BLVD	PORTLAND OR 97239
10	RETURN SERVICE REQUESTED			USHER ERIC J & USHER STACEY A	6220 SW CAPITOL HWY #3	PORTLAND OR 97239-2677
11	RETURN SERVICE REQUESTED			SEWARD MICHELLE TR	6521 SW 19TH AVE	PORTLAND OR 97239-1978
12	RETURN SERVICE REQUESTED			O'GARA MOLLY M	6547 SW 19TH AVE	PORTLAND OR 97239-1978
13	RETURN SERVICE REQUESTED			NYSTROM CLAIRE	6220 SW CAPITOL HWY #6	PORTLAND OR 97239
14	RETURN SERVICE REQUESTED			ASHLEY RHODA A	6551 SW 19TH AVE	PORTLAND OR 97239
15	RETURN SERVICE REQUESTED			LEE DAVID S	6220 SW CAPITOL HWY #9	PORTLAND OR 97239
16	RETURN SERVICE REQUESTED			BEZODIS FAMILY TR	6234 SW BURLINGAME AVE	PORTLAND OR 97239
17	RETURN SERVICE REQUESTED			STRATER MAX S	6555 SW 19TH AVE	PORTLAND OR 97239
18	RETURN SERVICE REQUESTED			SU JANE POYIN	6559 SW 19TH AVE	PORTLAND OR 97239
19	RETURN SERVICE REQUESTED			HART TERESA	6191 SW CAPITOL HWY	PORTLAND OR 97239
20	RETURN SERVICE REQUESTED			NGUYEN ANGELA	1513 SW SUNSET BLVD	PORTLAND OR 97239
21	RETURN SERVICE REQUESTED			ROHIT &SHARON MATHEW LIV TR	6180 SW CAPITOL HWY	PORTLAND OR 97239-2650
22	RETURN SERVICE REQUESTED			KROHA ANDREW P	6182 SW CAPITOL HWY	PORTLAND OR 97239
23	RETURN SERVICE REQUESTED			CARR MARY E	6184 SW CAPITOL HWY	PORTLAND OR 97239-7012
24	RETURN SERVICE REQUESTED			DOERR TIMOTHY S	6190 SW CAPITOL HWY	PORTLAND OR 97239
25	RETURN SERVICE REQUESTED			MCDUFFEE DEBORAH M	6192 SW CAPITOL HWY	PORTLAND OR 97239-7012
26	RETURN SERVICE REQUESTED			ESPARZA FAMILY TR	6735 SW CAPITOL HILL RD #4	PORTLAND OR 97219
27	RETURN SERVICE REQUESTED			TAN PATRICK	6735 SW CAPITOL HILL RD #6	PORTLAND OR 97219
28	RETURN SERVICE REQUESTED			BRUMM JAMES M & BRUMM APRIL M	6815 SW CAPITOL HILL RD #10	PORTLAND OR 97219
29	RETURN SERVICE REQUESTED			DEWITZ PATTI D	6815 SW CAPITOL HILL RD #13	PORTLAND OR 97219
30	RETURN SERVICE REQUESTED			CRARY GEORGE M & DANNER WILLIAM C	6815 SW CAPITOL HILL RD #14	PORTLAND OR 97219-2665
31	RETURN SERVICE REQUESTED			VERGHESE SANTHOSH C & JOSE ANNU	6815 SW CAPITOL HILL RD #16	PORTLAND OR 97219
32	RETURN SERVICE REQUESTED			SECHARMIDAL MCARTHUR M	6815 SW CAPITOL HILL RD #18	PORTLAND OR 97219
33	RETURN SERVICE REQUESTED			JOSE ANNU & VERGHESE SANTHOSH C	6815 SW CAPITOL HILL RD #9	PORTLAND OR 97219
34	RETURN SERVICE REQUESTED			BERRIS JODI E	6825 SW CAPITOL HILL RD #21	PORTLAND OR 97219
35	RETURN SERVICE REQUESTED			COX JENNIFER	6825 SW CAPITOL HILL RD #23	PORTLAND OR 97219
36	RETURN SERVICE REQUESTED			CRISP RICHARD W	6825 SW CAPITOL HILL RD #24	PORTLAND OR 97219
37	RETURN SERVICE REQUESTED			WARREN JULIA A	6825 SW CAPITOL HILL RD #25	PORTLAND OR 97219-2978
38	RETURN SERVICE REQUESTED			BRANDT CALA T	6825 SW CAPITOL HILL RD #26	PORTLAND OR 97219
39	RETURN SERVICE REQUESTED			HOFFMAN TRACY L	6825 SW CAPITOL HILL RD #27	PORTLAND OR 97219
40	RETURN SERVICE REQUESTED			CATHERY BRUCE H	6835 SW CAPITOL HILL RD #30	PORTLAND OR 97219
41	RETURN SERVICE REQUESTED			LARSON EMILY A	6835 SW CAPITOL HILL RD #32	PORTLAND OR 97219
42	RETURN SERVICE REQUESTED			HICKSON-BLYTHE TR	6835 SW CAPITOL HILL RD #35	PORTLAND OR 97219
43	RETURN SERVICE REQUESTED			MARTIN CANDICE	6835 SW CAPITOL HILL RD #36	PORTLAND OR 97219
44	RETURN SERVICE REQUESTED			ROTH TIMOTHY	6835 SW CAPITOL HILL RD #37	PORTLAND OR 97219
45	RETURN SERVICE REQUESTED			WENDE BREANNA & WENDE CRAIG M	6714 SW BURLINGAME AVE	PORTLAND OR 97219
46	RETURN SERVICE REQUESTED			PRODUCTS CORP OF NORTH AMERICA	6726 SW BURLINGAME AVE	PORTLAND OR 97219
47	RETURN SERVICE REQUESTED			WILSON WILLIAM K	815 SW CHESTNUT ST	PORTLAND OR 97219-2143
48	RETURN SERVICE REQUESTED			ADP LLC	1 ADP BLVD MS #B401	ROSELAND NJ 07068
49	RETURN SERVICE REQUESTED			HILLSDALE INVESTORS LLC	10524 SW 11TH DR	PORTLAND OR 97219
50	RETURN SERVICE REQUESTED			YOO CHUL MIN	11350 NE HALSEY ST	PORTLAND OR 97220
51	RETURN SERVICE REQUESTED			ROZYCKI MARTIN T & LEVITT ARIANA S	1155 SW CHESTNUT DR	PORTLAND OR 97219
52	RETURN SERVICE REQUESTED			PIZZICATO INC	121 S BANCROFT ST	PORTLAND OR 97239-4051
53	RETURN SERVICE REQUESTED			ROSE CITY PIZZA LLC	12514 NE 36TH AVE	VANCOUVER WA 98686
54	RETURN SERVICE REQUESTED			DR DHAI BARR LLC	12707 NE HALSEY ST	PORTLAND OR 97230
55	RETURN SERVICE REQUESTED			GOETZ JEANNE M TR	12758 SW 114TH TER	PORTLAND OR 97223
56	RETURN SERVICE REQUESTED			2305 TR	1336 SW TEXAS ST	PORTLAND OR 97219
57	RETURN SERVICE REQUESTED			HOUSING AUTHORITY OF PORTLAND	135 SW ASH ST	PORTLAND OR 97204-3598
58	RETURN SERVICE REQUESTED			SIMPSON RONALD E	13730 NE GLISAN ST APT 1	PORTLAND OR 97230-2556
59	RETURN SERVICE REQUESTED			HILLSDALE OFFICE SUITES LLC	1509 SW SUNSET BLVD #100	PORTLAND OR 97239
60	RETURN SERVICE REQUESTED			LEWIS JAMES E	15655 TWIN FIR RD	LAKE OSWEGO OR 97035
61	RETURN SERVICE REQUESTED		6225-6235 SW BURLINGAME AVE	PARTNERS LP	16133 VENTURA BLVD #630	ENCINO CA 91436
62	RETURN SERVICE REQUESTED			SCHNOLL KAREN T	1754 SW SUNSET BLVD	PORTLAND OR 97239
63	RETURN SERVICE REQUESTED			ABBOTT JULINA	1822 SW MOSS ST	PORTLAND OR 97219
64	RETURN SERVICE REQUESTED			PARADISE FALLS LLC	18555 SW TETON AVE	TUALATIN OR 97062
65	RETURN SERVICE REQUESTED			JOHNSTON RICHARD & JOHNSTON JEAN	1931 SAINT ANDREWS CIR	GILROY CA 95020
66	RETURN SERVICE REQUESTED			SHADOW HILLS-220 LLC	2040 SW VERMONT ST	PORTLAND OR 97219

	A	B	C	D	E	F
67	RETURN SERVICE REQUESTED			MISSION VETERINARY PARTNERS	20450 CIVIC CENTER DR	SOUTHFIELD MI 48076
68	RETURN SERVICE REQUESTED			MOVING MOXIE PILATES LLC	2110 SW KANAN ST	PORTLAND OR 97239-2183
69	RETURN SERVICE REQUESTED			SRJ LLC	21100 NE SANDY BLVD SP 139	FAIRVIEW OR 97024
70	RETURN SERVICE REQUESTED			MORSE PETER J & MORSE MOLLY A	2175 BOBCAT AVE SW	ALBANY OR 97321-4872
71	RETURN SERVICE REQUESTED			RUDD KORSCHGEN LLC	22018 S CENTRAL POINT RD	CANBY OR 97013
72	RETURN SERVICE REQUESTED			EHLERS JOSEPH F & EHLERS SATOKO A	2211 SW MARTHA ST	PORTLAND OR 97239-2048
73	RETURN SERVICE REQUESTED			MILLER RICHARD & MILLER MICHAELA	22500 SW JOHNSON RD	WEST LINN OR 97068
74	RETURN SERVICE REQUESTED			GREATER PORTLAND BIBLE CHURCH	2374 SW VERMONT ST	PORTLAND OR 97219-9440
75	RETURN SERVICE REQUESTED			MCCREADY KRISTIN M	2417 ST ELIAS DR	ANCHORAGE AK 99517
76	RETURN SERVICE REQUESTED			DAVA BEAD & TRADE INC	2470 NE SANDY BLVD	PORTLAND OR 97232-2340
77	RETURN SERVICE REQUESTED			GUINEE CECELIA E	2510 SE SALMON ST	PORTLAND OR 97214-3948
78	RETURN SERVICE REQUESTED			STRIKE FORCE DELTA INC	2555 SW 81ST AVE	PORTLAND OR 97225
79	RETURN SERVICE REQUESTED			STRAUB ALTON T &DEVOE JENNIFER E	2620 SW TEXAS ST	PORTLAND OR 97219
80	RETURN SERVICE REQUESTED			HOWE ADA & HOWE BRADLEY W	275 NW SILVERADO DR	BEAVERTON OR 97006-6362
81	RETURN SERVICE REQUESTED			NORTHWEST DEALERCO HOLDINGS LLC	29501 CANWOOD ST #200	AGOURA HILLS CA 91301
82	RETURN SERVICE REQUESTED			DIANA'S EXTENDED FAMILY LLC	3 MONROE PKWY #P PMB 230	LAKE OSWEGO OR 97035
83	RETURN SERVICE REQUESTED			GRAY ROBERT G & GRAY SALLY	3116 W LAKE ST #521	MINNEAPOLIS MN 55416
84	RETURN SERVICE REQUESTED			EVANS YVONNE & EVANS MICHAEL	3305 ZOE ST	JOHNS ISLAND SC 29455
85	RETURN SERVICE REQUESTED			1932 SW NEBRASKA LLC	33408 N DESERT SKY CIR	CAVE CREKK AZ 85331
86	RETURN SERVICE REQUESTED			1936 SW NEBRASKA LLC	33408 N DESERT SKY CIR	CAVE CREEK AZ 85331
87	RETURN SERVICE REQUESTED			HOUSE SHARI F	3505 SW DAKOTA ST	PORTLAND OR 97221
88	RETURN SERVICE REQUESTED			KELLAN BETH P &KELLAN PETER W	3553 SW CARSON ST	PORTLAND OR 97219
89	RETURN SERVICE REQUESTED			BERTHA M FERRAN REV TR	3761 SW 58TH DR	PORTLAND OR 97221-1253
90	RETURN SERVICE REQUESTED			MC MENAMINS INC	430 N KILLINGSWORTH ST	PORTLAND OR 97217
91	RETURN SERVICE REQUESTED			6202 PARTNERS LLC	4908A NE 48TH AVE	VANCOUVER WA 98661-2658
92	RETURN SERVICE REQUESTED			NI HAO & HE JUN	4986 MULHOLLAND DR	LAKE OSWEGO OR 97035-4394
93	RETURN SERVICE REQUESTED			WHITTEN/GETTLESON FAM TR	5164 SW BOUNDARY ST	PORTLAND OR 97221
94	RETURN SERVICE REQUESTED			SEID JULIE YUK KWUN YAN TR	5210 SE 33RD PL	PORTLAND OR 97202
95	RETURN SERVICE REQUESTED			ZAGONE FAMILY TR	5312 CHILDS RD	LAKE OSWEGO OR 97035
96	RETURN SERVICE REQUESTED			HILLSDALE HEIGHTS LLC	6120 SW 18TH DR #39	PORTLAND OR 97239
97	RETURN SERVICE REQUESTED			WILLY ROY &WILLY THERESA	61310 AUGUST LN	BEND OR 97702
98	RETURN SERVICE REQUESTED			PAVLYK VOLODYMYR & PAVLYK CHRISTINE	6164 SW CAPITOL HWY	PORTLAND OR 97239-2650
99	RETURN SERVICE REQUESTED			FRIENDS OF HOPEWELL HOUSE	6171 SW CAPITOL HWY	PORTLAND OR 97239-2649
100	RETURN SERVICE REQUESTED			MISKO DAWN R	6220 SW CAPITOL HWY UN 2	PORTLAND OR 97239
101	RETURN SERVICE REQUESTED			MCMAMARA LEO	6220 SW CAPITOL HWY UN 4	PORTLAND OR 97239
102	RETURN SERVICE REQUESTED			SCHULER KATHRYN	6220 SW CAPITOL HWY UNIT 8	PORTLAND OR 97239
103	RETURN SERVICE REQUESTED			J SQUARE ONE LLC	6225-6245 SW CAPITOL HWY #6245	PORTLAND OR 97239
104	RETURN SERVICE REQUESTED			BOWMAN HEATHER A & BOWMAN JAMES S	6250 SW BURLINGAME AVE	PORTLAND OR 97239
105	RETURN SERVICE REQUESTED			INDY-HILL LLC	6312 SW CAPITOL HWY	PORTLAND OR 97239
106	RETURN SERVICE REQUESTED			AVISE YOLO JOINT TR	6390 SW BURLINGAME AVE	PORTLAND OR 97239
107	RETURN SERVICE REQUESTED			THE STERN FAMILY LP	6600 SW 105TH AVE #175	BEAVERTON OR 97008
108	RETURN SERVICE REQUESTED			O'DAY SEAN E & THOMAS KATHERINE C	6601 SW BURLINGAME AVE	PORTLAND OR 97239
109	RETURN SERVICE REQUESTED			SCHUFF KATHRYN G	6711 SW 12TH AVE	PORTLAND OR 97219
110	RETURN SERVICE REQUESTED			SCHLIMGEN JEFFREY & DIETZ ANNETTE	6712 SW 14TH AVE	PORTLAND OR 97219
111	RETURN SERVICE REQUESTED			NASLUND BRIGITTE M	6715 SW 15TH AVE	PORTLAND OR 97219
112	RETURN SERVICE REQUESTED			MARTINEZ ASHLIE & MARTINEZ DAVID	6733 SW 10TH AVE	PORTLAND OR 97219
113	RETURN SERVICE REQUESTED			CROOK KRISTIN	6735 SW CAPITOL HILL RD UNIT 1	PORTLAND OR 97219
114	RETURN SERVICE REQUESTED			DAOUD SAYED S ET AL	6735 SW CAPITOL HILL RD UNIT 3	PORTLAND OR 97219
115	RETURN SERVICE REQUESTED			CHAVEZ GEORGE & CHAVEZ VICTORIA	681 HAMLIN DR	CANYONVILLE OR 97417
116	RETURN SERVICE REQUESTED			WINTERS TRACY L	6814 SW 13TH AVE	PORTLAND OR 97219
117	RETURN SERVICE REQUESTED			DEAN ANGELA	6815 SW CAPITOL HILL RD APT 15	PORTLAND OR 97219-2665
118	RETURN SERVICE REQUESTED			HORENSTEIN MICHAEL T	6815 SW CAPITOL HILL RD APT 17	PORTLAND OR 97219
119	RETURN SERVICE REQUESTED			WARR LYNNE P	6825 SW CAPITOL HILL RD UN 28	PORTLAND OR 97219
120	RETURN SERVICE REQUESTED			LEE DANIEL & DURLAUF MALLORY	6828 SW 14TH AVE	PORTLAND OR 97219
121	RETURN SERVICE REQUESTED			HICKMAN MARY M & HICKMAN MARK A	6835 SW CAPITOL HILL RD UNIT 29	PORTLAND OR 97219
122	RETURN SERVICE REQUESTED			PIRABAN LLC	685 1ST ST APT 21	LAKE OSWEGO OR 97034-2375
123	RETURN SERVICE REQUESTED			E GARY SERVAIS LIVING TRUST	690 WINDING WAY SE	SALEM OR 97302
124	RETURN SERVICE REQUESTED			BARAGLI MARIA B & RIOSECO ZURINA	6901 SW 3RD AVE	PORTLAND OR 97219-2211
125	RETURN SERVICE REQUESTED			ROBERTS CHARLES E & MILLER TAMARA R	7001 SEAVIEW AVE NW #160 PMB 814	SEATTLE WA 98117-6021
126	RETURN SERVICE REQUESTED			CSG ENTERPRISES LLC	7620 SW 37TH AVE	PORTLAND OR 97219
127	RETURN SERVICE REQUESTED			INDIGO TRADERS LLC	7814 SW CAPITOL HWY	PORTLAND OR 97219
128	RETURN SERVICE REQUESTED			ANTONYAN LILIK TR	807 E WINDSOR RD	GLENDALE CA 91205-2407
129	RETURN SERVICE REQUESTED			FINCH HOWARD S & FINCH SHERRIE L	851 TRAEGER AVE #100	SAN BRUNO CA 94066-3091
130	RETURN SERVICE REQUESTED			CLOSE PROPERTIES	8615 SW 8TH AVE	PORTLAND OR 97219
131	RETURN SERVICE REQUESTED			ALVAREZ CESAR	862 SW TROY ST	PORTLAND OR 97219
132	RETURN SERVICE REQUESTED			DALTON HEIGHTS LLC	8733 SE DIVISION ST #201	PORTLAND OR 97266-1470

	A	B	C	D	E	F
133	RETURN SERVICE REQUESTED			SUBWAY HILLSDALE INC	8950 SW BARBUR BLVD	PORTLAND OR 97219
134	RETURN SERVICE REQUESTED			CHINOOK INVESTMENT CO	901 NE GLISAN ST	PORTLAND OR 97232-2730
135	RETURN SERVICE REQUESTED			RAMSAY SIGNS INC	9160 SE 74TH AVE	PORTLAND OR 97206
136	RETURN SERVICE REQUESTED			MILWAI LLC	9400 SW BARNES RD #400	PORTLAND OR 97225-6660
137	RETURN SERVICE REQUESTED			SHADOW HILLS-220 LLC	9500 SW BARBUR BLVD STE 300	PORTLAND OR 97219
138	RETURN SERVICE REQUESTED			ACUNA CARLOS	9746 SW 34TH PL APT 5	PORTLAND OR 97219-6281
139	RETURN SERVICE REQUESTED			BERTHA SENIOR HOUSING LP	P O BOX 23206	TIGARD OR 97281-3206
140	RETURN SERVICE REQUESTED			VERMONT SPRINGS APARTMENT LLC	P O BOX 8459	PORTLAND OR 97207-8459
141	RETURN SERVICE REQUESTED			DEBRA EBERWEIN LAZZARETTI TR	PO BOX 1037	LAKE OSWEGO OR 97035
142	RETURN SERVICE REQUESTED			HILLSHOOF LLC ET AL	PO BOX 1800	CORVALLIS OR 97339
143	RETURN SERVICE REQUESTED			J R ZUKIN CORP	PO BOX 1810	LOS GATOS CA 95031
144	RETURN SERVICE REQUESTED	OWNERS ASSOC OF		SIXTY TWO TWENTY CONDO	PO BOX 2171	TACOMA WA 98401
145	RETURN SERVICE REQUESTED			KEYBANK OF OREGON INC	PO BOX 22055	ALBANY NY 12201-2055
146	RETURN SERVICE REQUESTED			WATERSHED CONDOMINIUM ASSOCIATION	PO BOX 23206	TIGARD OR 97281-3206
147	RETURN SERVICE REQUESTED			HSIAO ALBERT K	PO BOX 270756	VADNAIS HTS MN 55127-0756
148	RETURN SERVICE REQUESTED			BANK OF AMERICA NA	PO BOX 32727	CHARLOTTE NC 28232
149	RETURN SERVICE REQUESTED			ONPOINT COMMUNITY CREDIT UNION	PO BOX 3750	PORTLAND OR 97208
150	RETURN SERVICE REQUESTED			STARBUCKS CORPORATION	PO BOX 460849 DEPT 114	HOUSTON TX 77056
151	RETURN SERVICE REQUESTED			EDWARD D JONES & CO LP	PO BOX 66528	ST LOUIS MO 63166-6528
152	RETURN SERVICE REQUESTED			DAMM RENEE S & DAMM MARINUS R	PO BOX 748	CENTRAL CITY CO 80427
153	RETURN SERVICE REQUESTED			EINSTEIN & NOAH CORP	PO BOX 802206	DALLAS TX 75380-2206
154	RETURN SERVICE REQUESTED			ROBERSON JONATHAN BARRETT	PO BOX 80573	PORTLAND OR 97280
155	RETURN SERVICE REQUESTED			WARDIN INVESTMENT CO LLC	PO BOX 80885	PORTLAND OR 97280-1885
156	RETURN SERVICE REQUESTED			OSTER SANDRA J	PO BOX 80957	PORTLAND OR 97280
157	RETURN SERVICE REQUESTED			HILLS 1 PDX LLC	PO BOX 96068	PORTLAND OR 97296-6002
158	RETURN SERVICE REQUESTED			JARVIS MICHELLE K	6194 SW CAPITOL HWY	PORTLAND OR 97239-7012
159	RETURN SERVICE REQUESTED			HYETT ILONA	6196 SW CAPITOL HWY	PORTLAND OR 97239-7012
160	RETURN SERVICE REQUESTED			LEONARD KATHERINE M	6198 SW CAPITOL HWY	PORTLAND OR 97239-7012
161	RETURN SERVICE REQUESTED			REISER JANET L	6220 SW CAPITOL HWY #10	PORTLAND OR 97239
162	RETURN SERVICE REQUESTED			HEDIGER IRENE M & HEDIGER ROGER F	6238 SW BURLINGAME AVE	PORTLAND OR 97239-2633
163	RETURN SERVICE REQUESTED			TACKETT-NELSON MATTHEW & AMY	6242 SW BURLINGAME AVE	PORTLAND OR 97239
164	RETURN SERVICE REQUESTED			ROSAS DEBORAH L	6244 SW BURLINGAME AVE	PORTLAND OR 97239-2633
165	RETURN SERVICE REQUESTED			SPECTOR MAX I & KLARICH SYDNEY K	6252 SW BURLINGAME AVE	PORTLAND OR 97239
166	RETURN SERVICE REQUESTED			OLIVARES CHRISTOPHER & LISA	6254 SW BURLINGAME AVE	PORTLAND OR 97239
167	RETURN SERVICE REQUESTED			RAVER DALE E & RAVEN DIANE L	6256 SW BURLINGAME AVE	PORTLAND OR 97239-2633
168	RETURN SERVICE REQUESTED			TODD GERALD A TR & TODD EVELYN J TR	6258 SW BURLINGAME AVE	PORTLAND OR 97239-2633
169	RETURN SERVICE REQUESTED			ROMANO THOMAS J & FOOTE JESSICA G	6259 SW BURLINGAME AVE	PORTLAND OR 97239
170	RETURN SERVICE REQUESTED			DYKHOUSE JANS T & WRIGHT JENNIFER	6262 SW BURLINGAME AVE	PORTLAND OR 97239-2633
171	RETURN SERVICE REQUESTED			BAKER AND SPICE LLC	6330 SW CAPITOL HWY	PORTLAND OR 97239-1938
172	RETURN SERVICE REQUESTED			HEIN GREENE TR	6310 SW BURLINGAME AVE	PORTLAND OR 97239
173	RETURN SERVICE REQUESTED			PARK CHRIS BIN	6311 SW CAPITOL HWY	PORTLAND OR 97239
174	RETURN SERVICE REQUESTED			TRACY NGUYEN	6362 SW CAPITOL HWY	PORTLAND OR 97239
175	RETURN SERVICE REQUESTED			MJA HEALTH SERVICES INC	6339 SW CAPITOL HWY	PORTLAND OR 97239
176	RETURN SERVICE REQUESTED			AGALICO LLC	6341 SW CAPITOL HWY	PORTLAND OR 97239
177	RETURN SERVICE REQUESTED			HILLSDALE NAIL & SPA LLC	6343 SW CAPITOL HWY	PORTLAND OR 97239
178	RETURN SERVICE REQUESTED			ROACH D MICHAEL	6316 SW CAPITOL HWY	PORTLAND OR 97239
179	RETURN SERVICE REQUESTED			GAUFRE GOURMET INC	6320 SW CAPITOL HWY	PORTLAND OR 97239
180	RETURN SERVICE REQUESTED			SAELOR LAI	6323 SW CAPITOL HWY	PORTLAND OR 97239
181	RETURN SERVICE REQUESTED			KIM JOSEPH M & JOAN M	6324 SW CAPITOL HWY	PORTLAND OR 97239
182	RETURN SERVICE REQUESTED			YAN YUK K	6325 SW CAPITOL HWY	PORTLAND OR 97239
183	RETURN SERVICE REQUESTED			DREISIN ROBERT & KEELING PAMELA	6350 SW BURLINGAME AVE	PORTLAND OR 97239-2635
184	RETURN SERVICE REQUESTED			OTHER WORLDS LLC	6350 SW CAPITOL HWY	PORTLAND OR 97239
185	RETURN SERVICE REQUESTED			BRADY BRIAN P & MUNCH JACQUELINE L	6395 SW BURLINGAME AVE	PORTLAND OR 97239
186	RETURN SERVICE REQUESTED			MARY ROBERTS TR	6394 SW BURLINGAME AVE	PORTLAND OR 97239
187	RETURN SERVICE REQUESTED			MT HOOD LUMBER TRADING INC	6405 SW BURLINGAME AVE	PORTLAND OR 97239-2636
188	RETURN SERVICE REQUESTED			MELINDA & JEB BINKLEY	6408 SW BURLINGAME PL	PORTLAND OR 97239
189	RETURN SERVICE REQUESTED			JAMOND MICHAEL T & ARNOLD JANICE M	6412 SW BURLINGAME PL	PORTLAND OR 97239-2684
190	RETURN SERVICE REQUESTED			DUDLEY JOHN	6414 SW BURLINGAME PL	PORTLAND OR 97239-2684
191	RETURN SERVICE REQUESTED			DRISCOLL ASHLEY O & DRISCOLL JOESPH	6415 SW BURLINGAME AVE	PORTLAND OR 97239
192	RETURN SERVICE REQUESTED			PHILIP CHARR LIV TR	6418 SW BURLINGAME PL	PORTLAND OR 97239
193	RETURN SERVICE REQUESTED			FLEMING MOLLYANNE & PATRICK J II	6425 SW BURLINGAME AVE	PORTLAND OR 97239-2636
194	RETURN SERVICE REQUESTED			BRONNER KENNETH & BRONNER NATALIA Y	6435 SW BURLINGAME AVE	PORTLAND OR 97239
195	RETURN SERVICE REQUESTED			THAYER & HEYBACH FAMILY TR	6484 SW BURLINGAME PL	PORTLAND OR 97239
196	RETURN SERVICE REQUESTED			WIEBER KELLY E & WIEBER STEPHANIE K	6488 SW BURLINGAME PL	PORTLAND OR 97239-2691
197	RETURN SERVICE REQUESTED			PLANCK STEPHEN R & PLANCK GAIL L	6502 SW BURLINGAME AVE	PORTLAND OR 97239-2638
198	RETURN SERVICE REQUESTED			MARLA KAY JEREMIAH REV LIV TR	6505 SW BURLINGAME AVE	PORTLAND OR 97239

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199	RETURN SERVICE REQUESTED			BAKER IVAN &BAKER KELSEY	6515 SW BURLINGAME AVE	PORTLAND OR 97239
200	RETURN SERVICE REQUESTED			FREER KATHERINE & MOYER DAVID	6525 SW BURLINGAME AVE	PORTLAND OR 97239-2637
201	RETURN SERVICE REQUESTED			ZAHLER BYRON G & ZAHLER JOAN M	6611 SW BURLINGAME AVE	PORTLAND OR 97239
202	RETURN SERVICE REQUESTED			MATSON JEFFREY & MATSON JENNIFER	6625 SW BURLINGAME AVE	PORTLAND OR 97239
203	RETURN SERVICE REQUESTED			SZAFRANSKI LILLI	801 SW TERWILLIGER PL	PORTLAND OR 97239
204	RETURN SERVICE REQUESTED			JORDAN HANNA & JAITLA JASDEEP	807 SW TERWILLIGER PL	PORTLAND OR 97239
205	RETURN SERVICE REQUESTED			FLEENER BRIAN & FLEENER KATHERINE	818 SW VINCENT PL	PORTLAND OR 97239-2670
206	RETURN SERVICE REQUESTED			IWAI MIKI	821 SW TERWILLIGER PL	PORTLAND OR 97239
207	RETURN SERVICE REQUESTED		FILIPOVIC OMAR	& TURBIC-FILIPOVIC SELMA	822 SW TERWILLIGER PL	PORTLAND OR 97239-2666
208	RETURN SERVICE REQUESTED			FENTON RONALD K & CROTTEAU JENNIFER	824 SW VINCENT PL	PORTLAND OR 97239
209	RETURN SERVICE REQUESTED			BERNASEK JENNIFER & TIMOTHY	825 SW BURLINGAME TER	PORTLAND OR 97239
210	RETURN SERVICE REQUESTED			DENNIS ANTHONY W & DENNIS LISA K	830 SW VINCENT PL	PORTLAND OR 97239
211	RETURN SERVICE REQUESTED			BRECKER JENNIFER & BRECKER JARVIS	855 SW BURLINGAME TER	PORTLAND OR 97239
212	RETURN SERVICE REQUESTED			CANGELOSE CAMERON	917 SW CHESTNUT ST	PORTLAND OR 97219
213	RETURN SERVICE REQUESTED			SWAN DANIEL C & SWAN JANET C	1104 SW VERMONT ST	PORTLAND OR 97219
214	RETURN SERVICE REQUESTED			GARTLAND JAKE & GARTLAND ASHLEY	1111 SW FLORIDA ST	PORTLAND OR 97219
215	RETURN SERVICE REQUESTED			SCHMIDT RANDALL A & SCHMIDT LYN K	1120 SW VERMONT ST	PORTLAND OR 97219-2164
216	RETURN SERVICE REQUESTED			BERWIN MELANIE M	1124 SW CHESTNUT DR	PORTLAND OR 97219
217	RETURN SERVICE REQUESTED		JORDAN ETHAN	& GRIFFITH-JORDAN SANDIE M	1133 SW CHESTNUT DR	PORTLAND OR 97219
218	RETURN SERVICE REQUESTED			CARLSON BRIAN & CARLSON MOLLY	1135 SW FLORIDA ST	PORTLAND OR 97219
219	RETURN SERVICE REQUESTED			VILLAREAL RENE & VILLAREAL TRACY	1140 SW CHESTNUT DR	PORTLAND OR 97219
220	RETURN SERVICE REQUESTED			ASPLUND TROY D & ASPLUND CARLA K	1144 SW FLORIDA ST	PORTLAND OR 97219-2054
221	RETURN SERVICE REQUESTED			PULL BRETT L & SAKANISHI MIO	1147 SW FLORIDA ST	PORTLAND OR 97219-2053
222	RETURN SERVICE REQUESTED			YOKOYAMA AKINOBU & KAWAHARA COLLEEN	1151 SW CHESTNUT DR	PORTLAND OR 97219-2166
223	RETURN SERVICE REQUESTED			PULLEN MICHAEL A & PULLEN SHANNON H	1157 SW FLORIDA ST	PORTLAND OR 97219-2053
224	RETURN SERVICE REQUESTED		SHULTS EMILY	& DE LARQUIER JEANNE-SARAH	1160 SW FLORIDA ST	PORTLAND OR 97219
225	RETURN SERVICE REQUESTED			HAMILTON TIMOTHY & HAMILTON JOAN	1163 SW FLORIDA ST	PORTLAND OR 97219-2053
226	RETURN SERVICE REQUESTED			MCCORMICK TERESA R	1166 SW FLORIDA ST	PORTLAND OR 97219
227	RETURN SERVICE REQUESTED			TALYANSKY VITALY & KOTLYAR ROZA	1167 SW CHESTNUT DR	PORTLAND OR 97219
228	RETURN SERVICE REQUESTED			GELLERT LAURENCE & GELLERT LESLIE	1175 SW FLORIDA ST	PORTLAND OR 97219-2053
229	RETURN SERVICE REQUESTED			VAUGHAN-DIETER LYNDSEY ET AL	6703 SW 13TH AVE	PORTLAND OR 97219
230	RETURN SERVICE REQUESTED			BACON-SHONE ROBIN	6708 SW 12TH AVE	PORTLAND OR 97219-2002
231	RETURN SERVICE REQUESTED			RICKLES JEROME & RICKLES MICHELLE	6710 SW 11TH DR	PORTLAND OR 97219-2148
232	RETURN SERVICE REQUESTED			OLSON MAIKO & OLSON RYAN	6710 SW 13TH AVE	PORTLAND OR 97219
233	RETURN SERVICE REQUESTED			PADDACK RYCHEN M III	6711 SW 10TH AVE	PORTLAND OR 97219
234	RETURN SERVICE REQUESTED			HARRIS HOWARD R	6715 SW 11TH DR	PORTLAND OR 97219-2147
235	RETURN SERVICE REQUESTED			TOTTEN CHRISTINE & TOTTEN JEFFREY	6715 SW 13TH AVE	PORTLAND OR 97219
236	RETURN SERVICE REQUESTED			STEWART CASEY O	6718 SW 14TH AVE	PORTLAND OR 97219-2018
237	RETURN SERVICE REQUESTED			STEVENSON RICHARD & BARBARA	6720 SW 11TH DR	PORTLAND OR 97219-2148
238	RETURN SERVICE REQUESTED			ENGELMAN STEVEN & ENGELMAN JENNIFER	6721 SW 10TH AVE	PORTLAND OR 97219
239	RETURN SERVICE REQUESTED			SCHEURER DARIN & SCHEURER SHARON	6723 SW 12TH AVE	PORTLAND OR 97219-2001
240	RETURN SERVICE REQUESTED			FORTGANG ANDREW & FORTGANG LAUREN	6724 SW 13TH AVE	PORTLAND OR 97219-2010
241	RETURN SERVICE REQUESTED			SIMON NEIL	6727 SW 13TH AVE	PORTLAND OR 97219
242	RETURN SERVICE REQUESTED			RUDISILL MATHIAS	6728 SW 14TH AVE	PORTLAND OR 97219
243	RETURN SERVICE REQUESTED			ABEL JOHN R	6729 SW 12TH AVE	PORTLAND OR 97219
244	RETURN SERVICE REQUESTED			MOSTELLER TROY & MOSTELLER SERENA	6730 SW 11TH DR	PORTLAND OR 97219
245	RETURN SERVICE REQUESTED			BERNSTEIN ANDREW W	6732 SW 13TH AVE	PORTLAND OR 97219
246	RETURN SERVICE REQUESTED			FUKUNAGA RAY R & FUKUNAGA LORI	6733 SW 13TH AVE	PORTLAND OR 97219
247	RETURN SERVICE REQUESTED			LA GRAND LINDA M	6735 SW 12TH AVE	PORTLAND OR 97219-2001
248	RETURN SERVICE REQUESTED			RELAMPAGOS ISMAEL & ANDREA	6736 SW 14TH AVE	PORTLAND OR 97219-2018
249	RETURN SERVICE REQUESTED			LORBER EILEEN M	6740 SW 11TH DR	PORTLAND OR 97219-2148
250	RETURN SERVICE REQUESTED			VIERs MATTHEW D & VIERs SHARON F	6740 SW 12TH AVE	PORTLAND OR 97219
251	RETURN SERVICE REQUESTED			BAKER JUDITH	6740 SW 13TH AVE	PORTLAND OR 97219
252	RETURN SERVICE REQUESTED			BELL VALLISTYNE	6745 SW 11TH DR	PORTLAND OR 97219
253	RETURN SERVICE REQUESTED			ALTERMATT RUSSELL N & SHIRLEY A	6745 SW 13TH AVE	PORTLAND OR 97219-2009
254	RETURN SERVICE REQUESTED			GREGORY RHONDA M	6747 SW 10TH AVE	PORTLAND OR 97219
255	RETURN SERVICE REQUESTED			TUNSTALL RONALD & TUNSTALL KAREN	6800 SW 11TH DR	PORTLAND OR 97219
256	RETURN SERVICE REQUESTED			ALDER MICHAEL E	6801 SW BURLINGAME AVE	PORTLAND OR 97219-2127
257	RETURN SERVICE REQUESTED			BROHMAN WILLIAM & BROHMAN AMANDA	6803 SW 11TH DR	PORTLAND OR 97219
258	RETURN SERVICE REQUESTED			ROY RICHARD & ROY JEANNE	6805 SW 12TH AVE	PORTLAND OR 97219-2003
259	RETURN SERVICE REQUESTED			BALSHAM MARTHA	6810 SW 11TH DR	PORTLAND OR 97219
260	RETURN SERVICE REQUESTED		COMMERFORD CRAIG G	& TRUAX COMMERFORD JENNIFER L	6812 SW 10TH AVE	PORTLAND OR 97219
261	RETURN SERVICE REQUESTED			AGUAYO DANIEL	6815 SW 10TH AVE	PORTLAND OR 97219-2118
262	RETURN SERVICE REQUESTED			MACODRUM DONALD & MACODRUM MALEA	6815 SW 11TH DR	PORTLAND OR 97219
263	RETURN SERVICE REQUESTED			SOLOMIN JOSHUA & SOLOMIN RACHEL	6818 SW 12TH AVE	PORTLAND OR 97219
264	RETURN SERVICE REQUESTED			HUGHART FAMILY TR	6825 SW 10TH AVE	PORTLAND OR 97219

	A	B	C	D	E	F
265	RETURN SERVICE REQUESTED			PARASU PAVAN & COSTELLO BRIANNA	6826 SW 10TH AVE	PORTLAND OR 97219
266	RETURN SERVICE REQUESTED			GREEN DAVID ALEXANDER & ALICIA B	906 SW CHESTNUT ST	PORTLAND OR 97219
267	RETURN SERVICE REQUESTED			LAUER FAMILY TR	940 SW CHESTNUT ST	PORTLAND OR 97219
268	RETURN SERVICE REQUESTED			MOON MICHAEL R & MOON DEBRA S	6705 SW 15TH AVE	PORTLAND OR 97219
269	RETURN SERVICE REQUESTED			TAKEO HIROSHI & TAKEO ALISON K	6707 SW 14TH AVE	PORTLAND OR 97219
270	RETURN SERVICE REQUESTED			ELEY ERIC & ITELD LILY	6709 SW 15TH AVE	PORTLAND OR 97219
271	RETURN SERVICE REQUESTED			LESCH STEPHEN E JR & LESCH ALETHA D	6710 SW 15TH AVE	PORTLAND OR 97219
272	RETURN SERVICE REQUESTED			WESTOVER THOMAS M & DEMARIE MOLLY C	6720 SW 15TH AVE	PORTLAND OR 97219
273	RETURN SERVICE REQUESTED			LANDER EDITH A	6721 SW 14TH AVE	PORTLAND OR 97219-2017
274	RETURN SERVICE REQUESTED			HRISTIC ANA	6723 SW 15TH AVE	PORTLAND OR 97219
275	RETURN SERVICE REQUESTED			BERG PAUL W	6735 SW 14TH AVE	PORTLAND OR 97219
276	RETURN SERVICE REQUESTED			NEIMAN SUZANNE & KIZHAPPALI RAMESH	6803 SW 15TH AVE	PORTLAND OR 97219
277	RETURN SERVICE REQUESTED			GINSBERG KYLO	6810 SW 15TH AVE	PORTLAND OR 97219
278	RETURN SERVICE REQUESTED			RUSSILL KARI S	6815 SW 14TH AVE	PORTLAND OR 97219
279	RETURN SERVICE REQUESTED			WANG LIN & NIE XIAO JING	6827 SW 15TH AVE	PORTLAND OR 97219-2028
280	RETURN SERVICE REQUESTED			ALLEN JAMES A & ALLEN JOELLE C	6830 SW 15TH AVE	PORTLAND OR 97219
281	RETURN SERVICE REQUESTED			LUCERO APRIL & LUCERO STEVEN J	6840 SW 15TH AVE	PORTLAND OR 97219-2029
282	RETURN SERVICE REQUESTED			THOMPSON LINDA T	6735 SW CAPITOL HILL RD #2	PORTLAND OR 97219
283	RETURN SERVICE REQUESTED			ACHESON EMMA M C & JOHNSON BRIAN M	6503 SW 19TH AVE	PORTLAND OR 97239
284	RETURN SERVICE REQUESTED			MCCLEARY SEAN & MCCLEARY BRYN	6636 SW BURLINGAME AVE	PORTLAND OR 97239
285	RETURN SERVICE REQUESTED			PIGG MATTHEW J & BREMNER REBECCA A	6640 SW BURLINGAME AVE	PORTLAND OR 97239-2640
286	RETURN SERVICE REQUESTED			MILLER KENNETH & MILLER ANNE	820 SW BURLINGAME TER	PORTLAND OR 97239-2648
287	RETURN SERVICE REQUESTED			STEVENS AARON B & JENNIFER ESP	830 SW BURLINGAME TER	PORTLAND OR 97239-2648
288	RETURN SERVICE REQUESTED			REESE WILLIAM J & REESE SAMINA A	868 SW BURLINGAME TER	PORTLAND OR 97239-2648
289				CURRENT RESIDENT	1112 SW CHESTNUT DR	PORTLAND OR 97219
290				CURRENT RESIDENT	1130 SW CHESTNUT DR	PORTLAND OR 97219
291				CURRENT RESIDENT	1146 SW CHESTNUT DR	PORTLAND OR 97219
292				CURRENT RESIDENT	1151 SW VERMONT ST	PORTLAND OR 97219
293				CURRENT RESIDENT	1308 SW CHELTENHAM ST	PORTLAND OR 97239
294				CURRENT RESIDENT	1308 SW CHELTENHAM ST #5	PORTLAND OR 97239
295				CURRENT RESIDENT	1308 SW CHELTENHAM ST #6	PORTLAND OR 97239
296				CURRENT RESIDENT	1308 SW CHELTENHAM ST #7	PORTLAND OR 97239
297				CURRENT RESIDENT	1308 SW CHELTENHAM ST #8	PORTLAND OR 97239
298				CURRENT RESIDENT	1320 SW CHELTENHAM ST #1	PORTLAND OR 97239
299				CURRENT RESIDENT	1320 SW CHELTENHAM ST #2	PORTLAND OR 97239
300				CURRENT RESIDENT	1320 SW CHELTENHAM ST #3	PORTLAND OR 97239
301				CURRENT RESIDENT	1320 SW CHELTENHAM ST #4	PORTLAND OR 97239
302				CURRENT RESIDENT	1332 SW CHELTENHAM ST #10	PORTLAND OR 97239
303				CURRENT RESIDENT	1332 SW CHELTENHAM ST #11	PORTLAND OR 97239
304				CURRENT RESIDENT	1332 SW CHELTENHAM ST #12	PORTLAND OR 97239
305				CURRENT RESIDENT	1332 SW CHELTENHAM ST #13	PORTLAND OR 97239
306				CURRENT RESIDENT	1332 SW CHELTENHAM ST #14	PORTLAND OR 97239
307				CURRENT RESIDENT	1332 SW CHELTENHAM ST #15	PORTLAND OR 97239
308				CURRENT RESIDENT	1332 SW CHELTENHAM ST #16	PORTLAND OR 97239
309				CURRENT RESIDENT	1332 SW CHELTENHAM ST #17	PORTLAND OR 97239
310				CURRENT RESIDENT	1332 SW CHELTENHAM ST #18	PORTLAND OR 97239
311				CURRENT RESIDENT	1332 SW CHELTENHAM ST #19	PORTLAND OR 97239
312				CURRENT RESIDENT	1332 SW CHELTENHAM ST #20	PORTLAND OR 97239
313				CURRENT RESIDENT	1332 SW CHELTENHAM ST #9	PORTLAND OR 97239
314				CURRENT RESIDENT	1405 SW VERMONT ST	PORTLAND OR 97219
315				CURRENT RESIDENT	1501 SW SUNSET BLVD	PORTLAND OR 97239
316				CURRENT RESIDENT	1505 SW SUNSET BLVD	PORTLAND OR 97239
317				CURRENT RESIDENT	1509 SW DEWITT ST #2F	PORTLAND OR 97239
318				CURRENT RESIDENT	1509 SW SUNSET BLVD	PORTLAND OR 97239
319				CURRENT RESIDENT	1509 SW SUNSET BLVD #1010	PORTLAND OR 97239
320				CURRENT RESIDENT	1509 SW SUNSET BLVD #1A	PORTLAND OR 97239
321				CURRENT RESIDENT	1509 SW SUNSET BLVD #1B	PORTLAND OR 97239
322				CURRENT RESIDENT	1509 SW SUNSET BLVD #1C	PORTLAND OR 97239
323				CURRENT RESIDENT	1509 SW SUNSET BLVD #1D	PORTLAND OR 97239
324				CURRENT RESIDENT	1509 SW SUNSET BLVD #1E	PORTLAND OR 97239
325				CURRENT RESIDENT	1509 SW SUNSET BLVD #1F	PORTLAND OR 97239
326				CURRENT RESIDENT	1509 SW SUNSET BLVD #1G	PORTLAND OR 97239
327				CURRENT RESIDENT	1509 SW SUNSET BLVD #1I	PORTLAND OR 97239
328				CURRENT RESIDENT	1509 SW SUNSET BLVD #1K	PORTLAND OR 97239
329				CURRENT RESIDENT	1509 SW SUNSET BLVD #1L	PORTLAND OR 97239
330				CURRENT RESIDENT	1509 SW SUNSET BLVD #200	PORTLAND OR 97239

	A	B	C	D	E	F
331				CURRENT RESIDENT	1509 SW SUNSET BLVD #2A	PORTLAND OR 97239
332				CURRENT RESIDENT	1509 SW SUNSET BLVD #2B	PORTLAND OR 97239
333				CURRENT RESIDENT	1509 SW SUNSET BLVD #2C	PORTLAND OR 97239
334				CURRENT RESIDENT	1509 SW SUNSET BLVD #2D	PORTLAND OR 97239
335				CURRENT RESIDENT	1509 SW SUNSET BLVD #2E	PORTLAND OR 97239
336				CURRENT RESIDENT	1509 SW SUNSET BLVD #2F	PORTLAND OR 97239
337				CURRENT RESIDENT	1509 SW SUNSET BLVD #2H	PORTLAND OR 97239
338				CURRENT RESIDENT	1509 SW SUNSET BLVD #B1	PORTLAND OR 97239
339				CURRENT RESIDENT	1515 SW SUNSET BLVD	PORTLAND OR 97239
340				CURRENT RESIDENT	1519 SW SUNSET BLVD	PORTLAND OR 97239
341				CURRENT RESIDENT	1522 SW SUNSET BLVD	PORTLAND OR 97239
342				CURRENT RESIDENT	1606 SW SUNSET BLVD	PORTLAND OR 97239
343				CURRENT RESIDENT	1818 SW CAROLINA ST	PORTLAND OR 97239
344				CURRENT RESIDENT	1818 SW CAROLINA ST #1	PORTLAND OR 97239
345				CURRENT RESIDENT	1818 SW CAROLINA ST #2	PORTLAND OR 97239
346				CURRENT RESIDENT	1818 SW CAROLINA ST #3	PORTLAND OR 97239
347				CURRENT RESIDENT	1818 SW CAROLINA ST #4	PORTLAND OR 97239
348				CURRENT RESIDENT	1820 SW VERMONT ST	PORTLAND OR 97219
349				CURRENT RESIDENT	1820 SW VERMONT ST #A	PORTLAND OR 97219
350				CURRENT RESIDENT	1820 SW VERMONT ST #B	PORTLAND OR 97219
351				CURRENT RESIDENT	1820 SW VERMONT ST #C	PORTLAND OR 97219
352				CURRENT RESIDENT	1820 SW VERMONT ST #C/D	PORTLAND OR 97219
353				CURRENT RESIDENT	1820 SW VERMONT ST #D	PORTLAND OR 97219
354				CURRENT RESIDENT	1820 SW VERMONT ST #E	PORTLAND OR 97219
355				CURRENT RESIDENT	1820 SW VERMONT ST #F	PORTLAND OR 97219
356				CURRENT RESIDENT	1820 SW VERMONT ST #G	PORTLAND OR 97219
357				CURRENT RESIDENT	1820 SW VERMONT ST #H	PORTLAND OR 97219
358				CURRENT RESIDENT	1820 SW VERMONT ST #I	PORTLAND OR 97219
359				CURRENT RESIDENT	1820 SW VERMONT ST #J	PORTLAND OR 97219
360				CURRENT RESIDENT	1820 SW VERMONT ST #K	PORTLAND OR 97219
361				CURRENT RESIDENT	1820 SW VERMONT ST #L	PORTLAND OR 97219
362				CURRENT RESIDENT	1820 SW VERMONT ST #M	PORTLAND OR 97219
363				CURRENT RESIDENT	1820 SW VERMONT ST #O	PORTLAND OR 97219
364				CURRENT RESIDENT	1825 SW VERMONT ST	PORTLAND OR 97219
365				CURRENT RESIDENT	1825 SW VERMONT ST #1	PORTLAND OR 97219
366				CURRENT RESIDENT	1825 SW VERMONT ST #10	PORTLAND OR 97219
367				CURRENT RESIDENT	1825 SW VERMONT ST #11	PORTLAND OR 97219
368				CURRENT RESIDENT	1825 SW VERMONT ST #12	PORTLAND OR 97219
369				CURRENT RESIDENT	1825 SW VERMONT ST #14	PORTLAND OR 97219
370				CURRENT RESIDENT	1825 SW VERMONT ST #15	PORTLAND OR 97219
371				CURRENT RESIDENT	1825 SW VERMONT ST #2	PORTLAND OR 97219
372				CURRENT RESIDENT	1825 SW VERMONT ST #3	PORTLAND OR 97219
373				CURRENT RESIDENT	1825 SW VERMONT ST #4	PORTLAND OR 97219
374				CURRENT RESIDENT	1825 SW VERMONT ST #5	PORTLAND OR 97219
375				CURRENT RESIDENT	1825 SW VERMONT ST #6	PORTLAND OR 97219
376				CURRENT RESIDENT	1825 SW VERMONT ST #7	PORTLAND OR 97219
377				CURRENT RESIDENT	1825 SW VERMONT ST #8	PORTLAND OR 97219
378				CURRENT RESIDENT	1825 SW VERMONT ST #9	PORTLAND OR 97219
379				CURRENT RESIDENT	1900 SW VERMONT ST	PORTLAND OR 97219
380				CURRENT RESIDENT	1902 SW VERMONT ST	PORTLAND OR 97219
381				CURRENT RESIDENT	1904 SW IDAHO ST	PORTLAND OR 97239
382				CURRENT RESIDENT	1904 SW VERMONT ST	PORTLAND OR 97219
383				CURRENT RESIDENT	1906 SW VERMONT ST	PORTLAND OR 97219
384				CURRENT RESIDENT	1908 SW VERMONT ST	PORTLAND OR 97219
385				CURRENT RESIDENT	1909 SW VERMONT ST	PORTLAND OR 97219
386				CURRENT RESIDENT	1910 SW VERMONT ST	PORTLAND OR 97219
387				CURRENT RESIDENT	1912 SW IDAHO ST	PORTLAND OR 97239
388				CURRENT RESIDENT	1912 SW VERMONT ST	PORTLAND OR 97219
389				CURRENT RESIDENT	1913 SW VERMONT ST	PORTLAND OR 97219
390				CURRENT RESIDENT	1914 SW VERMONT ST	PORTLAND OR 97219
391				CURRENT RESIDENT	1915 SW VERMONT ST	PORTLAND OR 97219
392				CURRENT RESIDENT	1916 SW IDAHO ST	PORTLAND OR 97239
393				CURRENT RESIDENT	1916 SW VERMONT ST	PORTLAND OR 97219
394				CURRENT RESIDENT	1918 SW VERMONT ST	PORTLAND OR 97219
395				CURRENT RESIDENT	1919 SW NEBRASKA ST	PORTLAND OR 97239
396				CURRENT RESIDENT	1920 SW IDAHO ST	PORTLAND OR 97239

	A	B	C	D	E	F
397				CURRENT RESIDENT	1920 SW VERMONT ST	PORTLAND OR 97219
398				CURRENT RESIDENT	1921 SW VERMONT ST	PORTLAND OR 97219
399				CURRENT RESIDENT	1922 SW IDAHO ST	PORTLAND OR 97239
400				CURRENT RESIDENT	1922 SW VERMONT ST	PORTLAND OR 97219
401				CURRENT RESIDENT	1923 SW VERMONT ST	PORTLAND OR 97219
402				CURRENT RESIDENT	1924 SW VERMONT ST	PORTLAND OR 97219
403				CURRENT RESIDENT	1926 SW VERMONT ST	PORTLAND OR 97219
404				CURRENT RESIDENT	1928 SW VERMONT ST	PORTLAND OR 97219
405				CURRENT RESIDENT	1930 SW VERMONT ST	PORTLAND OR 97219
406				CURRENT RESIDENT	1932 SW NEBRASKA ST	PORTLAND OR 97239
407				CURRENT RESIDENT	1932 SW VERMONT ST	PORTLAND OR 97219
408				CURRENT RESIDENT	1934 SW NEBRASKA ST	PORTLAND OR 97239
409				CURRENT RESIDENT	1934 SW VERMONT ST	PORTLAND OR 97219
410				CURRENT RESIDENT	1936 SW NEBRASKA ST	PORTLAND OR 97239
411				CURRENT RESIDENT	1936 SW VERMONT ST	PORTLAND OR 97219
412				CURRENT RESIDENT	1938 SW NEBRASKA ST	PORTLAND OR 97239
413				CURRENT RESIDENT	1938 SW VERMONT ST	PORTLAND OR 97219
414				CURRENT RESIDENT	1940 SW VERMONT ST	PORTLAND OR 97219
415				CURRENT RESIDENT	1942 SW VERMONT ST	PORTLAND OR 97219
416				CURRENT RESIDENT	1944 SW VERMONT ST	PORTLAND OR 97219
417				CURRENT RESIDENT	1946 SW VERMONT ST	PORTLAND OR 97219
418				CURRENT RESIDENT	1948 SW VERMONT ST	PORTLAND OR 97219
419				CURRENT RESIDENT	1950 SW VERMONT ST	PORTLAND OR 97219
420				CURRENT RESIDENT	1952 SW VERMONT ST	PORTLAND OR 97219
421				CURRENT RESIDENT	1954 SW VERMONT ST	PORTLAND OR 97219
422				CURRENT RESIDENT	1956 SW VERMONT ST	PORTLAND OR 97219
423				CURRENT RESIDENT	1958 SW VERMONT ST	PORTLAND OR 97219
424				CURRENT RESIDENT	1960 SW VERMONT ST	PORTLAND OR 97219
425				CURRENT RESIDENT	1962 SW VERMONT ST	PORTLAND OR 97219
426				CURRENT RESIDENT	1964 SW VERMONT ST	PORTLAND OR 97219
427				CURRENT RESIDENT	1966 SW VERMONT ST	PORTLAND OR 97219
428				CURRENT RESIDENT	1968 SW VERMONT ST	PORTLAND OR 97219
429				CURRENT RESIDENT	1970 SW VERMONT ST	PORTLAND OR 97219
430				CURRENT RESIDENT	2000 SW VERMONT ST	PORTLAND OR 97219
431				CURRENT RESIDENT	2002 SW VERMONT ST	PORTLAND OR 97219
432				CURRENT RESIDENT	2004 SW VERMONT ST	PORTLAND OR 97219
433				CURRENT RESIDENT	2006 SW VERMONT ST	PORTLAND OR 97219
434				CURRENT RESIDENT	2008 SW VERMONT ST	PORTLAND OR 97219
435				CURRENT RESIDENT	2010 SW VERMONT ST	PORTLAND OR 97219
436				CURRENT RESIDENT	2012 SW VERMONT ST	PORTLAND OR 97219
437				CURRENT RESIDENT	2014 SW VERMONT ST	PORTLAND OR 97219
438				CURRENT RESIDENT	2016 SW VERMONT ST	PORTLAND OR 97219
439				CURRENT RESIDENT	2018 SW VERMONT ST	PORTLAND OR 97219
440				CURRENT RESIDENT	2020 SW VERMONT ST	PORTLAND OR 97219
441				CURRENT RESIDENT	2020 SW VERMONT ST #I & I	PORTLAND OR 97219
442				CURRENT RESIDENT	2022 SW VERMONT ST	PORTLAND OR 97219
443				CURRENT RESIDENT	2024 SW VERMONT ST	PORTLAND OR 97219
444				CURRENT RESIDENT	2026 SW VERMONT ST	PORTLAND OR 97219
445				CURRENT RESIDENT	2028 SW VERMONT ST	PORTLAND OR 97219
446				CURRENT RESIDENT	2028 SW VERMONT ST #28	PORTLAND OR 97219
447				CURRENT RESIDENT	2030 SW VERMONT ST	PORTLAND OR 97219
448				CURRENT RESIDENT	2032 SW VERMONT ST	PORTLAND OR 97219
449				CURRENT RESIDENT	2034 SW VERMONT ST	PORTLAND OR 97219
450				CURRENT RESIDENT	2036 SW VERMONT ST	PORTLAND OR 97219
451				CURRENT RESIDENT	2038 SW VERMONT ST	PORTLAND OR 97219
452				CURRENT RESIDENT	2050 SW VERMONT ST	PORTLAND OR 97219
453				CURRENT RESIDENT	2052 SW VERMONT ST	PORTLAND OR 97219
454				CURRENT RESIDENT	2054 SW VERMONT ST	PORTLAND OR 97219
455				CURRENT RESIDENT	2056 SW VERMONT ST	PORTLAND OR 97219
456				CURRENT RESIDENT	2058 SW VERMONT ST	PORTLAND OR 97219
457				CURRENT RESIDENT	2060 SW VERMONT ST	PORTLAND OR 97219
458				CURRENT RESIDENT	2062 SW VERMONT ST	PORTLAND OR 97219
459				CURRENT RESIDENT	2064 SW VERMONT ST	PORTLAND OR 97219
460				CURRENT RESIDENT	2070 SW VERMONT ST	PORTLAND OR 97219
461				CURRENT RESIDENT	2072 SW VERMONT ST	PORTLAND OR 97219
462				CURRENT RESIDENT	2074 SW VERMONT ST	PORTLAND OR 97219

[illegible]

[illegible]

	A	B	C	D	E	F
595				CURRENT RESIDENT	6120 SW 18TH DR #40	PORTLAND OR 97239
596				CURRENT RESIDENT	6134 SW 18TH DR	PORTLAND OR 97239
597				CURRENT RESIDENT	6134 SW 18TH DR #41	PORTLAND OR 97239
598				CURRENT RESIDENT	6134 SW 18TH DR #42	PORTLAND OR 97239
599				CURRENT RESIDENT	6134 SW 18TH DR #43	PORTLAND OR 97239
600				CURRENT RESIDENT	6134 SW 18TH DR #44	PORTLAND OR 97239
601				CURRENT RESIDENT	6134 SW 18TH DR #45	PORTLAND OR 97239
602				CURRENT RESIDENT	6134 SW 18TH DR #46	PORTLAND OR 97239
603				CURRENT RESIDENT	6134 SW 18TH DR #47	PORTLAND OR 97239
604				CURRENT RESIDENT	6134 SW 18TH DR #48	PORTLAND OR 97239
605				CURRENT RESIDENT	6134 SW 18TH DR #49	PORTLAND OR 97239
606				CURRENT RESIDENT	6134 SW 18TH DR #50	PORTLAND OR 97239
607				CURRENT RESIDENT	6134 SW 18TH DR #52	PORTLAND OR 97239
608				CURRENT RESIDENT	6134 SW 18TH DR #53	PORTLAND OR 97239
609				CURRENT RESIDENT	6134 SW 18TH DR #54	PORTLAND OR 97239
610				CURRENT RESIDENT	6134 SW 18TH DR #55	PORTLAND OR 97239
611				CURRENT RESIDENT	6134 SW 18TH DR #56	PORTLAND OR 97239
612				CURRENT RESIDENT	6134 SW 18TH DR #57	PORTLAND OR 97239
613				CURRENT RESIDENT	6146 SW 18TH DR	PORTLAND OR 97239
614				CURRENT RESIDENT	6146 SW 18TH DR #59	PORTLAND OR 97239
615				CURRENT RESIDENT	6146 SW 18TH DR #60	PORTLAND OR 97239
616				CURRENT RESIDENT	6146 SW 18TH DR #61	PORTLAND OR 97239
617				CURRENT RESIDENT	6146 SW 18TH DR #62	PORTLAND OR 97239
618				CURRENT RESIDENT	6146 SW 18TH DR #63	PORTLAND OR 97239
619				CURRENT RESIDENT	6146 SW 18TH DR #64	PORTLAND OR 97239
620				CURRENT RESIDENT	6146 SW 18TH DR #65	PORTLAND OR 97239
621				CURRENT RESIDENT	6146 SW 18TH DR #66	PORTLAND OR 97239
622				CURRENT RESIDENT	6146 SW 18TH DR #67	PORTLAND OR 97239
623				CURRENT RESIDENT	6146 SW 18TH DR #68	PORTLAND OR 97239
624				CURRENT RESIDENT	6146 SW 18TH DR #69	PORTLAND OR 97239
625				CURRENT RESIDENT	6146 SW 18TH DR #70	PORTLAND OR 97239
626				CURRENT RESIDENT	6164 SW CAPITOL HWY #A	PORTLAND OR 97239
627				CURRENT RESIDENT	6164 SW CAPITOL HWY #B	PORTLAND OR 97239
628				CURRENT RESIDENT	6170 SW CAPITOL HWY	PORTLAND OR 97239
629				CURRENT RESIDENT	6186 SW CAPITOL HWY	PORTLAND OR 97239
630				CURRENT RESIDENT	6188 SW CAPITOL HWY	PORTLAND OR 97239
631				CURRENT RESIDENT	6202 SW BURLINGAME AVE	PORTLAND OR 97239
632				CURRENT RESIDENT	6202 SW BURLINGAME AVE #1	PORTLAND OR 97239
633				CURRENT RESIDENT	6202 SW BURLINGAME AVE #2	PORTLAND OR 97239
634				CURRENT RESIDENT	6202 SW BURLINGAME AVE #3	PORTLAND OR 97239
635				CURRENT RESIDENT	6202 SW BURLINGAME AVE #4	PORTLAND OR 97239
636				CURRENT RESIDENT	6202 SW BURLINGAME AVE #5	PORTLAND OR 97239
637				CURRENT RESIDENT	6202 SW BURLINGAME AVE #6	PORTLAND OR 97239
638				CURRENT RESIDENT	6202 SW BURLINGAME AVE #7	PORTLAND OR 97239
639				CURRENT RESIDENT	6202 SW BURLINGAME AVE #8	PORTLAND OR 97239
640				CURRENT RESIDENT	6220 SW CAPITOL HWY	PORTLAND OR 97239
641				CURRENT RESIDENT	6220 SW CAPITOL HWY #1	PORTLAND OR 97239
642				CURRENT RESIDENT	6220 SW CAPITOL HWY #11	PORTLAND OR 97239
643				CURRENT RESIDENT	6220 SW CAPITOL HWY #2	PORTLAND OR 97239
644				CURRENT RESIDENT	6220 SW CAPITOL HWY #4	PORTLAND OR 97239
645				CURRENT RESIDENT	6220 SW CAPITOL HWY #5	PORTLAND OR 97239
646				CURRENT RESIDENT	6220 SW CAPITOL HWY #7	PORTLAND OR 97239
647				CURRENT RESIDENT	6220 SW CAPITOL HWY #8	PORTLAND OR 97239
648				CURRENT RESIDENT	6225 SW BURLINGAME AVE	PORTLAND OR 97239
649				CURRENT RESIDENT	6225 SW CAPITOL HWY	PORTLAND OR 97239
650				CURRENT RESIDENT	6235 SW BURLINGAME AVE	PORTLAND OR 97239
651				CURRENT RESIDENT	6235 SW BURLINGAME AVE #101	PORTLAND OR 97239
652				CURRENT RESIDENT	6235 SW BURLINGAME AVE #102	PORTLAND OR 97239
653				CURRENT RESIDENT	6235 SW BURLINGAME AVE #103	PORTLAND OR 97239
654				CURRENT RESIDENT	6235 SW BURLINGAME AVE #104	PORTLAND OR 97239
655				CURRENT RESIDENT	6235 SW BURLINGAME AVE #105	PORTLAND OR 97239
656				CURRENT RESIDENT	6235 SW BURLINGAME AVE #106	PORTLAND OR 97239
657				CURRENT RESIDENT	6235 SW BURLINGAME AVE #107	PORTLAND OR 97239
658				CURRENT RESIDENT	6235 SW BURLINGAME AVE #108	PORTLAND OR 97239
659				CURRENT RESIDENT	6235 SW BURLINGAME AVE #109	PORTLAND OR 97239
660				CURRENT RESIDENT	6235 SW BURLINGAME AVE #110	PORTLAND OR 97239

	A	B	C	D	E	F
661				CURRENT RESIDENT	6235 SW BURLINGAME AVE #111	PORTLAND OR 97239
662				CURRENT RESIDENT	6235 SW BURLINGAME AVE #112	PORTLAND OR 97239
663				CURRENT RESIDENT	6235 SW BURLINGAME AVE #113	PORTLAND OR 97239
664				CURRENT RESIDENT	6235 SW BURLINGAME AVE #114	PORTLAND OR 97239
665				CURRENT RESIDENT	6235 SW BURLINGAME AVE #201	PORTLAND OR 97239
666				CURRENT RESIDENT	6235 SW BURLINGAME AVE #202	PORTLAND OR 97239
667				CURRENT RESIDENT	6235 SW BURLINGAME AVE #203	PORTLAND OR 97239
668				CURRENT RESIDENT	6235 SW BURLINGAME AVE #204	PORTLAND OR 97239
669				CURRENT RESIDENT	6235 SW BURLINGAME AVE #205	PORTLAND OR 97239
670				CURRENT RESIDENT	6235 SW BURLINGAME AVE #206	PORTLAND OR 97239
671				CURRENT RESIDENT	6235 SW BURLINGAME AVE #207	PORTLAND OR 97239
672				CURRENT RESIDENT	6235 SW BURLINGAME AVE #208	PORTLAND OR 97239
673				CURRENT RESIDENT	6235 SW BURLINGAME AVE #209	PORTLAND OR 97239
674				CURRENT RESIDENT	6235 SW BURLINGAME AVE #210	PORTLAND OR 97239
675				CURRENT RESIDENT	6235 SW BURLINGAME AVE #211	PORTLAND OR 97239
676				CURRENT RESIDENT	6235 SW BURLINGAME AVE #212	PORTLAND OR 97239
677				CURRENT RESIDENT	6235 SW BURLINGAME AVE #213	PORTLAND OR 97239
678				CURRENT RESIDENT	6235 SW BURLINGAME AVE #214	PORTLAND OR 97239
679				CURRENT RESIDENT	6235 SW BURLINGAME AVE #215	PORTLAND OR 97239
680				CURRENT RESIDENT	6238 SW CAPITOL HWY	PORTLAND OR 97239
681				CURRENT RESIDENT	6240 SW BURLINGAME AVE #A	PORTLAND OR 97239
682				CURRENT RESIDENT	6245 SW CAPITOL HWY	PORTLAND OR 97239
683				CURRENT RESIDENT	6246 SW BURLINGAME AVE	PORTLAND OR 97239
684				CURRENT RESIDENT	6250 SW CAPITOL HWY	PORTLAND OR 97239
685				CURRENT RESIDENT	6254 SW BURLINGAME AVE #A	PORTLAND OR 97239
686				CURRENT RESIDENT	6256 SW CAPITOL HWY	PORTLAND OR 97239
687				CURRENT RESIDENT	6257 SW CAPITOL HWY	PORTLAND OR 97239
688				CURRENT RESIDENT	6274 SW CAPITOL HWY	PORTLAND OR 97239
689				CURRENT RESIDENT	6274 SW CAPITOL HWY #2	PORTLAND OR 97239
690				CURRENT RESIDENT	6302 SW CAPITOL HWY	PORTLAND OR 97239
691				CURRENT RESIDENT	6303 SW CAPITOL HWY	PORTLAND OR 97239
692				CURRENT RESIDENT	6306 SW CAPITOL HWY	PORTLAND OR 97239
693				CURRENT RESIDENT	6307 SW CAPITOL HWY	PORTLAND OR 97239
694				CURRENT RESIDENT	6308 SW CAPITOL HWY	PORTLAND OR 97239
695				CURRENT RESIDENT	6309 SW CAPITOL HWY	PORTLAND OR 97239
696				CURRENT RESIDENT	6312 SW CAPITOL HWY #428	PORTLAND OR 97239
697				CURRENT RESIDENT	6315 SW CAPITOL HWY	PORTLAND OR 97239
698				CURRENT RESIDENT	6319 SW CAPITOL HWY	PORTLAND OR 97239
699				CURRENT RESIDENT	6325 SW CAPITOL HWY #1	PORTLAND OR 97239
700				CURRENT RESIDENT	6325 SW CAPITOL HWY #2	PORTLAND OR 97239
701				CURRENT RESIDENT	6325 SW CAPITOL HWY #A	PORTLAND OR 97239
702				CURRENT RESIDENT	6326 SW CAPITOL HWY	PORTLAND OR 97239
703				CURRENT RESIDENT	6327 SW CAPITOL HWY	PORTLAND OR 97239
704				CURRENT RESIDENT	6327 SW CAPITOL HWY #A	PORTLAND OR 97239
705				CURRENT RESIDENT	6327 SW CAPITOL HWY #B	PORTLAND OR 97239
706				CURRENT RESIDENT	6327 SW CAPITOL HWY #B-1	PORTLAND OR 97239
707				CURRENT RESIDENT	6327 SW CAPITOL HWY #B2	PORTLAND OR 97239
708				CURRENT RESIDENT	6327 SW CAPITOL HWY #C	PORTLAND OR 97239
709				CURRENT RESIDENT	6327 SW CAPITOL HWY #C PMB 12	PORTLAND OR 97239
710				CURRENT RESIDENT	6327 SW CAPITOL HWY #C PMB 250	PORTLAND OR 97239
711				CURRENT RESIDENT	6327 SW CAPITOL HWY #C PMB 280	PORTLAND OR 97239
712				CURRENT RESIDENT	6327 SW CAPITOL HWY #D	PORTLAND OR 97239
713				CURRENT RESIDENT	6335 SW CAPITOL HWY	PORTLAND OR 97239
714				CURRENT RESIDENT	6335 SW CAPITOL HWY #A	PORTLAND OR 97239
715				CURRENT RESIDENT	6335 SW CAPITOL HWY #B	PORTLAND OR 97239
716				CURRENT RESIDENT	6337 SW CAPITOL HWY	PORTLAND OR 97239
717				CURRENT RESIDENT	6343 SW CAPITOL HWY #A	PORTLAND OR 97239
718				CURRENT RESIDENT	6343 SW CAPITOL HWY #B	PORTLAND OR 97239
719				CURRENT RESIDENT	6344 SW CAPITOL HWY	PORTLAND OR 97239
720				CURRENT RESIDENT	6345 SW CAPITOL HWY	PORTLAND OR 97239
721				CURRENT RESIDENT	6349 SW CAPITOL HWY	PORTLAND OR 97239
722				CURRENT RESIDENT	6351 SW CAPITOL HWY	PORTLAND OR 97239
723				CURRENT RESIDENT	6352 SW CAPITOL HWY	PORTLAND OR 97239
724				CURRENT RESIDENT	6354 SW CAPITOL HWY	PORTLAND OR 97239
725				CURRENT RESIDENT	6355 SW CAPITOL HWY	PORTLAND OR 97239
726				CURRENT RESIDENT	6356 SW CAPITOL HWY	PORTLAND OR 97239

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	A	B	C	D	E	F
793				CURRENT RESIDENT	6388 SW CAPITOL HWY #A	PORTLAND OR 97239
794				CURRENT RESIDENT	6388 SW CAPITOL HWY #B	PORTLAND OR 97239
795				CURRENT RESIDENT	6394 SW CAPITOL HWY	PORTLAND OR 97239
796				CURRENT RESIDENT	6415 SW BURLINGAME PL	PORTLAND OR 97239
797				CURRENT RESIDENT	6508 SW 19TH AVE	PORTLAND OR 97239
798				CURRENT RESIDENT	6509 SW 19TH AVE	PORTLAND OR 97239
799				CURRENT RESIDENT	6540 SW 19TH AVE	PORTLAND OR 97239
800				CURRENT RESIDENT	6552 SW 19TH AVE	PORTLAND OR 97239
801				CURRENT RESIDENT	6552 SW 19TH AVE #101	PORTLAND OR 97239
802				CURRENT RESIDENT	6552 SW 19TH AVE #102	PORTLAND OR 97239
803				CURRENT RESIDENT	6552 SW 19TH AVE #103	PORTLAND OR 97239
804				CURRENT RESIDENT	6552 SW 19TH AVE #104	PORTLAND OR 97239
805				CURRENT RESIDENT	6552 SW 19TH AVE #105	PORTLAND OR 97239
806				CURRENT RESIDENT	6552 SW 19TH AVE #106	PORTLAND OR 97239
807				CURRENT RESIDENT	6552 SW 19TH AVE #107	PORTLAND OR 97239
808				CURRENT RESIDENT	6552 SW 19TH AVE #108	PORTLAND OR 97239
809				CURRENT RESIDENT	6552 SW 19TH AVE #109	PORTLAND OR 97239
810				CURRENT RESIDENT	6552 SW 19TH AVE #110	PORTLAND OR 97239
811				CURRENT RESIDENT	6552 SW 19TH AVE #111	PORTLAND OR 97239
812				CURRENT RESIDENT	6552 SW 19TH AVE #112	PORTLAND OR 97239
813				CURRENT RESIDENT	6552 SW 19TH AVE #201	PORTLAND OR 97239
814				CURRENT RESIDENT	6552 SW 19TH AVE #202	PORTLAND OR 97239
815				CURRENT RESIDENT	6552 SW 19TH AVE #203	PORTLAND OR 97239
816				CURRENT RESIDENT	6552 SW 19TH AVE #204	PORTLAND OR 97239
817				CURRENT RESIDENT	6552 SW 19TH AVE #205	PORTLAND OR 97239
818				CURRENT RESIDENT	6552 SW 19TH AVE #206	PORTLAND OR 97239
819				CURRENT RESIDENT	6552 SW 19TH AVE #207	PORTLAND OR 97239
820				CURRENT RESIDENT	6552 SW 19TH AVE #208	PORTLAND OR 97239
821				CURRENT RESIDENT	6552 SW 19TH AVE #209	PORTLAND OR 97239
822				CURRENT RESIDENT	6552 SW 19TH AVE #210	PORTLAND OR 97239
823				CURRENT RESIDENT	6552 SW 19TH AVE #211	PORTLAND OR 97239
824				CURRENT RESIDENT	6552 SW 19TH AVE #212	PORTLAND OR 97239
825				CURRENT RESIDENT	6552 SW 19TH AVE #213	PORTLAND OR 97239
826				CURRENT RESIDENT	6554 SW 19TH AVE	PORTLAND OR 97239
827				CURRENT RESIDENT	6629 SW 19TH AVE	PORTLAND OR 97239
828				CURRENT RESIDENT	6640 SW BURLINGAME AVE	PORTLAND OR 97219
829				CURRENT RESIDENT	6641 SW 19TH AVE	PORTLAND OR 97239
830				CURRENT RESIDENT	6704 SW 15TH AVE	PORTLAND OR 97219
831				CURRENT RESIDENT	6711 SW 14TH AVE	PORTLAND OR 97219
832				CURRENT RESIDENT	6712 SW 14TH AVE #A	PORTLAND OR 97219
833				CURRENT RESIDENT	6712 SW 14TH AVE #B	PORTLAND OR 97219
834				CURRENT RESIDENT	6731 SW 11TH DR	PORTLAND OR 97219
835				CURRENT RESIDENT	6735 SW CAPITOL HILL RD #1	PORTLAND OR 97219
836				CURRENT RESIDENT	6735 SW CAPITOL HILL RD #3	PORTLAND OR 97219
837				CURRENT RESIDENT	6735 SW CAPITOL HILL RD #5	PORTLAND OR 97219
838				CURRENT RESIDENT	6735 SW CAPITOL HILL RD #7	PORTLAND OR 97219
839				CURRENT RESIDENT	6735 SW CAPITOL HILL RD #8	PORTLAND OR 97219
840				CURRENT RESIDENT	6810 SW 14TH AVE	PORTLAND OR 97219
841				CURRENT RESIDENT	6815 SW CAPITOL HILL RD #11	PORTLAND OR 97219
842				CURRENT RESIDENT	6815 SW CAPITOL HILL RD #12	PORTLAND OR 97219
843				CURRENT RESIDENT	6815 SW CAPITOL HILL RD #15	PORTLAND OR 97219
844				CURRENT RESIDENT	6815 SW CAPITOL HILL RD #17	PORTLAND OR 97219
845				CURRENT RESIDENT	6825 SW CAPITOL HILL RD #20	PORTLAND OR 97219
846				CURRENT RESIDENT	6825 SW CAPITOL HILL RD #22	PORTLAND OR 97219
847				CURRENT RESIDENT	6825 SW CAPITOL HILL RD #28	PORTLAND OR 97219
848				CURRENT RESIDENT	6835 SW CAPITOL HILL RD #29	PORTLAND OR 97219
849				CURRENT RESIDENT	6835 SW CAPITOL HILL RD #31	PORTLAND OR 97219
850				CURRENT RESIDENT	6835 SW CAPITOL HILL RD #33	PORTLAND OR 97219
851				CURRENT RESIDENT	6835 SW CAPITOL HILL RD #34	PORTLAND OR 97219
852				CURRENT RESIDENT	6835 SW CAPITOL HILL RD #38	PORTLAND OR 97219
853				CURRENT RESIDENT	817 SW TERWILLIGER PL	PORTLAND OR 97239
854				CURRENT RESIDENT	833 SW TERWILLIGER PL	PORTLAND OR 97239
855	RETURN SERVICE REQUESTED		OWNER	SCHOOL DISTRICT NO 1	PO BOX 3107	PORTLAND OR 97208-3107
856	RETURN SERVICE REQUESTED	APPLICANT	BORA ARCHITECTURE & INTERIORS	HELM GARRETT	1705 SE 3RD AVE	PORTLAND OR 97214
857	RETURN SERVICE REQUESTED	OWNERS AGENT	PORTLAND PUBLIC SCHOOLS	BEZIO DONNA	501 N DIXON ST	PORTLAND OR 97227
858	RETURN SERVICE REQUESTED	PARTY OF INTEREST	THE BOOKIN GROUP	CLEEK DEBBIE	1020 SW TAYLOR ST #555	PORTLAND OR 97205

	A	B	C	D	E	F
859	RETURN SERVICE REQUESTED		DISTRICT 4 NEIGHBORHOOD COALITION	DARLENE URBAN GARRETT	434 NW 6TH AVE SUITE 202	PORTLAND OR 97209
860	RETURN SERVICE REQUESTED		HILLSDALE BUS ASSOC	BUREAU MEREDITH	PO BOX 80885	PORTLAND OR 97280
861	RETURN SERVICE REQUESTED		LAND USE CONTACT	HILLSDALE NEIGHBORHOOD ASSOCIATION	PO BOX 19902 7805 SW 40TH AVENUE	PORTLAND OR 97280
862	RETURN SERVICE REQUESTED		LAND USE CONTACT	FRIENDS OF TERWILLIGER	430 SW HAMILTON ST	PORTLAND OR 97239
863	RETURN SERVICE REQUESTED			DOUG KLOTZ	1908 SE 35TH PLACE	PORTLAND OR 97214
864	RETURN SERVICE REQUESTED		PORTLAND METRO REGIONAL SOLUTIONS	C/O DLCD REGIONAL REP	1600 SW FOURTH AVE #109	PORTLAND OR 97201
865	RETURN SERVICE REQUESTED		LAND USE CONTACT	TRANSIT DEVELOPMENT	1800 SW FIRST AVE SUITE 300	PORTLAND OR 97201
866	RETURN SERVICE REQUESTED		LAND USE CONTACT	STATE HISTORIC PRESERVATION OFFICE	725 SUMMER NE #C	SALEM OR 97301
867	RETURN SERVICE REQUESTED		LAND USE CONTACT	AIA URBAN DESIGN COMMITTEE	403 NW 11TH	PORTLAND OR 97209
868				PORTLAND PARK TRAIL	TATE WHITE	B106/R1302
869					BRANDON SPENCER-HARTLE	B299/R7000
870					PROSPER PORTLAND	129/PROSPER
871					RICH CASSIDY	TRAFFIC

Design Advice Request

Ida B. Wells Modernization

CASE FILE	EA 24-067517 DA		
WHEN	Thursday, 9/19/2024 @ 1:30 PM <i>(This is the hearing start time - see Commission agenda for estimated project start time)</i>		
WHERE	ONLINE: Meeting link will be listed on the agenda available at www.portland.gov/ppd/design-commission		
HOW	TO COMMENT: Follow instructions on the Design Commission agenda <u>or</u> email the planner at tanya.paglia@portlandoregon.gov		
REVIEW BY	Design Commission		
PROCESS	A Design Advice Request is a voluntary review process that allows the Commission to provide early feedback on a development proposal, prior to the required land use review		
PROPOSAL	Design Advice Request (DAR) meeting for a proposal to replace the existing Ida B. Wells High School located in the Hillsdale Plan District with a new four-story building to accommodate ~1,700 students. Redevelopment of the site will include pedestrian/bicycle pathways, parking areas, athletic fields, and field support structures. An existing swimming pool, operated by PP&R, will remain along with the existing Rieke ES buildings. The new school building will offer a full range of high school facilities and will also accommodate a childcare center and a health clinic.		
REVIEW APPROVAL CRITERIA	Portland Citywide Design Guidelines		
SITE ADDRESS	1151 SW Vermont St		
ZONING/ DESIGNATION	CM2 – Commercial/Mixed Use 2; IR – Institutional Residential; OS – Open Space; R7 – Residential 7,000 base zones with Historic Resource Overlay Hillsdale Plan District		
APPLICANT(S)	Garrett Helm, Bora Architecture & Interiors	OWNER(S)	Donna Bezio, Portland Public Schools, School District No 1
QUESTIONS? BDS CONTACT	Tanya Paglia, City Planner (503) 865-6518 / tanya.paglia@PortlandOregon.gov Portland Permitting & Development, 1900 SW 4 th Ave, Suite 5000, Portland, OR 97201		

Traducción e interpretación | Chuyển Ngữ hoặc Phiên Dịch | 翻译或传译 | Turjumida ama Fasiraadda | 翻訳または通訳 | ການແປພາສາ ຫຼື ການອະທິບາຍ
Письменный или устный перевод | Traducere sau Interpretare | 번역및통역 | الترجمة التحريرية أو الشفوية | Письмовий або усний переклад

 503-823-7300  BDS@PortlandOregon.gov  www.PortlandOregon.gov/bds/translated TTY: 503-823-6868
Relay Service: 711



City of Portland, Oregon

Portland Permitting & Development

Land Use Services

David Kuhnhausen, Interim Director
Phone: (503) 823-7300
TTY: 711
www.portland.gov/ppd

DESIGN ADVICE REQUEST PROCESS OVERVIEW

Purpose

Design Advice Requests (DARs) are a form of early assistance intended to provide a public forum for the preliminary discussion and exchange of information between the applicant, PP&D staff, the public, and the representative Commission. The feedback that results from a DAR is advisory and preliminary in nature. The DAR is not a land use review and decisions are not made in the DAR process.

An applicant may request advice from the Design Commission or Historic Landmarks Commission prior to submitting a land use review. In some cases, a DAR may be required by a provision of the Zoning Code. These requests do not substitute for required prescribed regulatory or legislative processes.

Public Participation

The public meeting with the Commission will provide an opportunity for parties to submit oral and written comment. The Commission relies on Portlanders to bring their perspective on their community. Portland has a strong design legacy that continues through this process. The public's early input on significant projects helps to make sure we get this right. Continued participation through the land use review is necessary for public comments to be part of the land use review record.

Meeting Order

The order of appearance for those attending the meeting is as follows:

- PP&D Staff Introduction
- Applicant Presentation
- City Staff Discussion Topics
- Public Comments
- Commission and Applicant Discussion

Guiding Criteria

Design Guidelines are used to guide the conversation during the DAR because they are the approval criteria used in the subsequent land use review. All feedback should relate to the concept's response to the Guidelines. Copies of the Design Guidelines are available online at portlandoregon.gov/designguidelines.

Outside DAR Scope

The Commissions only have the authority to influence elements of a project that relate to the approval criteria. For example, guidelines do not address private views. Here other resources for questions on issues that the Commissions cannot address:

Bureau of Planning and Sustainability (BPS)
503-823-7700 | portlandoregon.gov/bps

- Off-Street Parking Requirements
- Upcoming Legislative Projects on Zoning
- Zoning Allowances

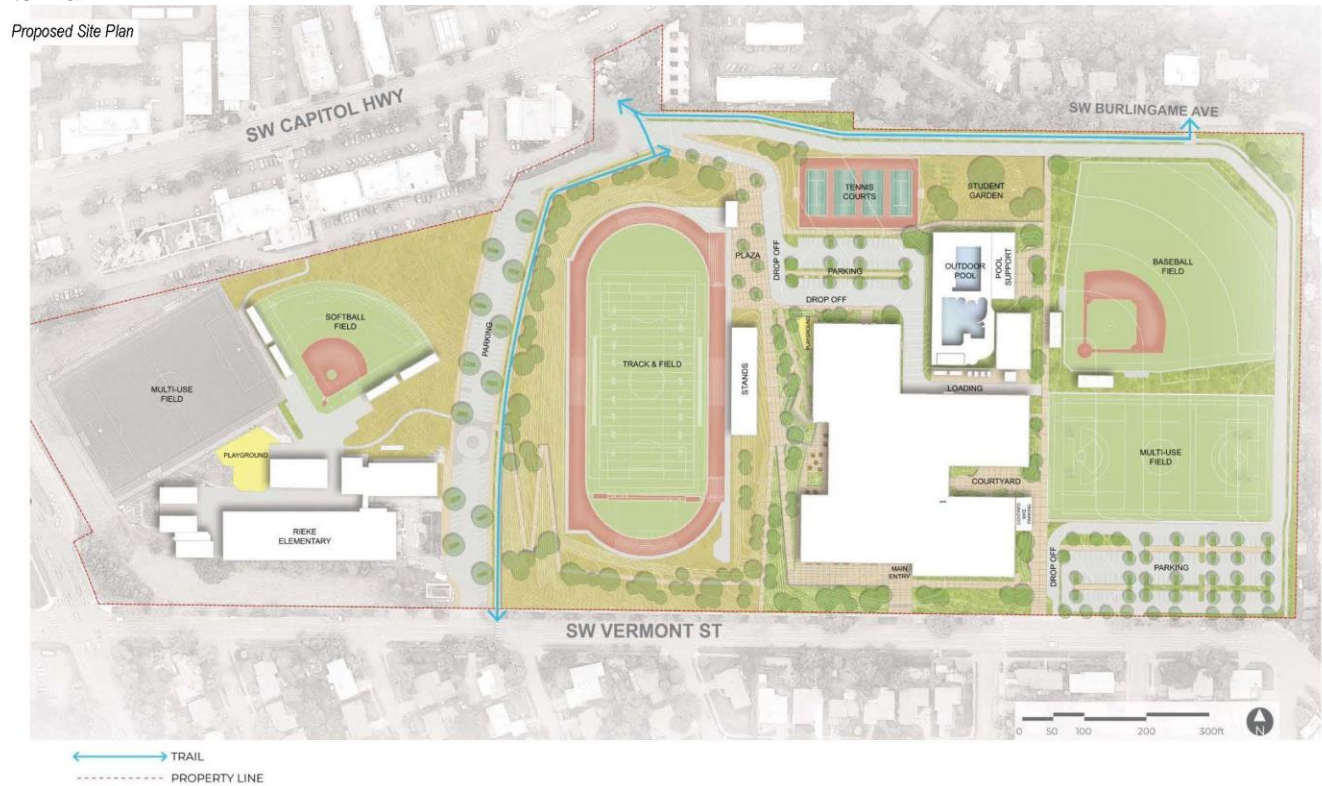
Bureau of Transportation (PBOT) | 503-823-5185 | portlandoregon.gov/transportation

- On-Street Parking
- Construction Impacts on Streets & Sidewalks

Office of Community & Civic Life
503-823-4519 | portlandoregon.gov/civic

- Neighborhood Association Information
- Crime Prevention
- Noise Control Program
- Neighborhood Mediation

Site Plan



Rendering West Elevation

Representative Image of Project (Perspective)



Observing or Testifying at the Portland Design Commission, Historic Landmarks Commission, or Adjustment Committee Webinar Hearings

Thank you for your interest in attending a land use public hearing. All hearings are currently held virtually, via Zoom. The information below will help you get connected.

***If you do not have access to the internet from a home computer or mobile phone, please see the end of this document for instructions on how to participate from a City building at 1900 SW 4th Avenue in downtown Portland.

Preparing for the Hearing:

1. To access the Zoom Webinar, please go to the online hearing Agenda, and click the link under the hearing date you are interested in participating: <https://www.portlandoregon.gov/bds/42441>
2. In advance of the hearing, please review documents and drawings in the project link within the Online Agenda.
 - Please also provide comments to the planner assigned in advance of the hearing.

Getting into the Hearing [Registering in Zoom to observe or participate in Hearing]:

1. In order to observe or testify in the hearing, please be sure to Register for the Webinar as soon as possible.
 - The Webinar Link is posted to the Online Agenda typically one week prior to the hearing date.
2. Once you register you will receive an email notification of how to log-in or access the Webinar.
3. You can enter the Webinar no sooner than ten minutes before the start of the hearing.
4. You will be held in the Zoom waiting room until the Webinar begins. (Please note each individual agenda item has an estimated start time.)
5. If using a smartphone or tablet, download the Zoom app for easy entry into the Webinar.

Public participation in the Hearing:

1. After Staff and Applicant presentations, the Chair will announce public testimony is open, and will ask if anyone else would like to testify.
2. You can provide public comment in this Webinar in several ways:
 - If during registration you indicated you would like to testify, we will put your name in order of request. Once in the hearing, testifiers will be renamed "Testifier 1 – (Your Name)"
 - Members of the public will be automatically muted except for when they are called by the Hearings Clerk for their public comment. During the Webinar, the Hearings Clerk will promote participants to "Panelists" in the order of Webinar Registrations received. When it is your turn to provide testimony, please accept the Clerk's invitation to be promoted to Panelist.
 - If you indicated in your registration that you did not want to testify but later changed your mind, when testimony is open:
 - Click the "raise your hand" function in ZOOM, and the Hearings Clerk will add you to the list of testifiers.
 - If you will be participating by call-in, raise your hand by pressing *9 - the Webinar host will see this notification.
 - When you are moved to Panelist position for your testimony, your name will be announced by the Chair or Hearings Clerk. Please be prepared to provide testimony.
 - Each testifier is allotted 2 minutes of testimony unless the Chair grants additional time.
 - Please manage your time when testifying. As a courtesy to other testifiers and our volunteer Commissions, please do not attempt to exceed the allotted amount of time.
3. We will enable screen sharing of presentations only for Design and Landmarks Commission members, project teams, and staff participating in the Webinar.
4. Testifiers who engage in inappropriate behavior or language will be promptly removed from the hearing.

Follow-up:

1. The Webinar will be recorded and uploaded to the City of Portland Auditors website, under the Case File Number, here: <https://efiles.portlandoregon.gov/Search>.

If you do not have access to the internet from a home computer or mobile phone, we can provide accommodations for you to view a live video display of the hearing from 1900 SW 4th Avenue in downtown Portland. This option for participation requires you to travel to a City building where we will provide a computer for viewing the hearing. City staff will not be present at this viewing location. If you require such accommodation, please contact the BDS Hearings Clerk at 503-865-6525 before 8 AM on the day of the hearing.



City of Portland, Oregon
Portland Permitting & Development
Land Use Services

David Kuhnhausen, Interim Director
Phone: (503) 823-7300
TTY: 711
www.portland.gov/ppd

Date: 8/19/2024

To: Garrett Helm, Bora Architecture & Interiors

From: Tanya Paglia, Land Use Services, Tanya.Paglia@portlandoregon.gov

RE: Design Advice Request posting for EA 24-067517 DA – Ida B. Wells Modernization

Dear Garrett Helm:

I have received your application for a Design Advice Request (DA) at 1151 SW Vermont St. Your case number is given above. The first meeting with the Design Commission is scheduled for **Thursday, September 19, 2024**. I am the planner handling your case, and can answer any questions you might have during the process.

You are required to post notice on the site of your proposal 20 days before the hearing. The information below will help you do this. I am also enclosing instructions for making the required posting boards and the notice that should be placed on the signs.

- A. You must post one of these signs adjacent to each street frontage on the site.
- B. These signs must be placed within 10 feet of the street frontage line, and must be visible to pedestrians and motorists. You may not post in the public right-of-way.
- C. Because the meeting with the Design Commission for your case is scheduled for **9/19/2024** you must post the notice by **8/30/2024**, 20 days before the hearing.
- D. A certification statement is enclosed, which you must sign and return. The statement affirms that you posted the site. It also confirms your understanding that if you do not post the notice by the date above, your hearing will be automatically postponed. You must return this statement to us by **9/5/2024**, 14 days before the hearing.
- E. You should not remove the notice before the meeting, but it must be taken down within two weeks after the meeting. You may want to save the posting boards to use for the required site posting during the Type III land use review.

Encls: Posting Notice
Statement Certifying Posting
Additional Instructions for Posting Notice Signs

cc: Application Case File

Garrett Helm, Bora Architecture & Interiors
1705 SE 3rd Avenue, Portland, OR 97214

DATE: _____

TO: Tanya Paglia | Tanya.Paglia@portlandoregon.gov
Bureau of Development Services | 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201

APPLICANT'S STATEMENT CERTIFYING DESIGN ADVICE REQUEST POSTING

Case File EA 24-067517 DA

This certifies that I have posted notice on my site. I understand that the meeting with the Design Commission is scheduled for **9/19/2024** at 1:30PM, and that I was required to post the property at least 20 days before the hearing.

The required number of poster boards, with the notices attached, were set up on _____(date). These were placed adjacent to each street frontage so that they were visible to pedestrians and motorists.

I understand that this form must be returned to Portland Permitting & Development no later than 9/5/2024, 14 days before the scheduled meeting. I also understand that if I do not post the notices by 8/30/2024, or return this form by 9/5/2024, my meeting will automatically be postponed.

In addition, I understand that I may not remove the notices before the meeting, but am required to remove them within two weeks of the meeting.

Signature

Print Name

Address

City/State/Zip Code

Design Advice Request

Ida B. Wells Modernization

CASE FILE	EA 24-067517 DA		
WHEN	Thursday, 9/19/2024 @ 1:30 PM <i>(This is the hearing start time - see Commission agenda for estimated project start time)</i>		
WHERE	ONLINE: Meeting link will be listed on the agenda available at www.portland.gov/ppd/design-commission		
HOW	TO COMMENT: Follow instructions on the Design Commission agenda <u>or</u> email the planner at tanya.paglia@portlandoregon.gov		
REVIEW BY	Design Commission		
PROCESS	A Design Advice Request is a voluntary review process that allows the Commission to provide early feedback on a development proposal, prior to the required land use review		
PROPOSAL	Design Advice Request (DAR) meeting for a proposal to replace the existing Ida B. Wells High School located in the Hillsdale Plan District with a new four-story building to accommodate ~1,700 students. Redevelopment of the site will include pedestrian/bicycle pathways, parking areas, athletic fields, and field support structures. An existing swimming pool, operated by PP&R, will remain along with the existing Rieke ES buildings. The new school building will offer a full range of high school facilities and will also accommodate a childcare center and a health clinic.		
REVIEW APPROVAL CRITERIA	Portland Citywide Design Guidelines		
SITE ADDRESS	1151 SW Vermont St		
ZONING/ DESIGNATION	CM2 – Commercial/Mixed Use 2; IR – Institutional Residential; OS – Open Space; R7 – Residential 7,000 base zones with Historic Resource Overlay Hillsdale Plan District		
APPLICANT(S)	Garrett Helm, Bora Architecture & Interiors	OWNER(S)	Donna Bezio, Portland Public Schools
QUESTIONS? BDS CONTACT	Tanya Paglia, City Planner (503) 865-6518 / tanya.paglia@PortlandOregon.gov Portland Permitting & Development, 1900 SW 4 th Ave, Suite 5000, Portland, OR 97201		

Traducción e interpretación | Chuyển Ngữ hoặc Phiên Dịch | 翻译或传译 | Turjumida ama Fasiraadda | 翻訳または通訳 | ຫາກວາມສາມາດ ຫາກວາມອະທິບາຍ
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 503-823-7300  BDS@PortlandOregon.gov  www.PortlandOregon.gov/bds/translated TTY: 503-823-6868
Relay Service: 711

Garrett Helm, Bora Architecture & Interiors
1705 SE 3rd Avenue, Portland, OR 97214

DATE: 09/03/2024

TO: Tanya Paglia | Tanya.Paglia@portlandoregon.gov
Bureau of Development Services | 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201

APPLICANT'S STATEMENT CERTIFYING DESIGN ADVICE REQUEST POSTING

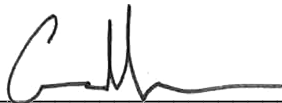
Case File EA 24-067517 DA

This certifies that I have posted notice on my site. I understand that the meeting with the Design Commission is scheduled for **9/19/2024** at 1:30PM, and that I was required to post the property at least 20 days before the hearing.

The required number of poster boards, with the notices attached, were set up on 08/30/2024 (date). These were placed adjacent to each street frontage so that they were visible to pedestrians and motorists.

I understand that this form must be returned to Portland Permitting & Development no later than 9/5/2024, 14 days before the scheduled meeting. I also understand that if I do not post the notices by 8/30/2024, or return this form by 9/5/2024, my meeting will automatically be postponed.

In addition, I understand that I may not remove the notices before the meeting, but am required to remove them within two weeks of the meeting.



Signature

Garrett Helm, Bora Architecture & Interiors

Print Name

1705 SE 3rd Ave

Address

Portland, Oregon 97214

City/State/Zip Code



City of Portland, Oregon
Portland Permitting & Development
Public Infrastructure Permit Review -
Transportation

David Kuhnhausen, Interim Director
Phone: (503) 823-7300
TTY: 711
www.portland.gov/ppd

Transportation Design Advice Request Response

Date: September 4, 2024
To: Tanya Paglia, BDS Land Use Services
503-865-6518, Tanya.Paglia@portlandoregon.gov
From: Tammy Boren-King, PBOT Development Review
503-823-2948, Tammy.Boren-King@portlandoregon.gov
Case File: EA 24-067517
Location: 1151 SW VERMONT ST
R#: R328555, R328556, R328614, R328618, R328614, R328614
Proposal: DZ HEARING - Replacement of the existing high school with a new four-story building to accommodate ~1,700 students. Redevelopment of the site will include pedestrian/bicycle pathways, parking areas, athletic fields, and field support structures. An existing swimming pool, operated by PP&R, will remain along with the existing Rieke ES buildings. The new school will offer a full range of high school facilities, including a theater, band and choir rooms, a cafeteria and commons, and a media center as well as classrooms and science labs, art rooms, shop spaces and a variety of specialized learning environments. The school will also accommodate a childcare center and a health clinic.

Please see the pre-application conference notes in 23-105044-EA for a full list of the applicable transportation related standards and required improvements. The high school rebuild will trigger a conditional use review with substantial transportation related approval criteria.

The applicant team is in contact with transportation staff outside of the design review process. The plans submitted to the DAR are highly conceptual. As such, the transportation comments are also rather conceptual. Staff would like to reiterate the following points which may impact the design:

-Connectivity standards apply. Transportation staff support a proposal such as the submitted plan which will provide an east/west connection and two north/south connections through the site. The conditional use review process will determine which routes will require a public access easement. Staff is supportive of the comprehensive on-site pedestrian circulation system, most of which will not have a public access easement over it. Staff recognizes the need to balance public access for through movements with the reality of this being a public school site that contains both a high school and an elementary school.

- The on-site pedestrian crossing at the north edge of the site where multiple driveways converge is a high priority for improvement. It appears the design is considering this location though the design is still schematic.

-SW Burlingame Ave. does not have a curb. This means public improvement requirements apply and staff have to require concept approval of a public works permit prior to land use

approval if SW Burlingame will be improved. Another option is for the applicant to secure a public works alternative review to alter the improvement requirements to the degree that both transportation and environmental services staff agree the conditional use approval criteria can be met without the need for concept approval of a public works permit. The applicant team has been in contact. Staff anticipate an alternative review will be submitted once the on-site design is further along in the design process. Depending on the outcome of the alternative review, public works concept approval may be deferred to the building permit stage.

-The amount of on-site vehicle parking needed will be determined through the conditional use review. Data will be needed to determine the appropriate amount of vehicle parking.

-Two on-site loading spaces meeting the Type A standard are required and appear to be shown on the site plan.

-PBOT Active Transportation will review the request to adjust the long-term bicycle parking requirements in consultation with PP&D land use staff. The applicant team has been in contact regarding what information should be submitted for this review.

From: jeanneroy62@gmail.com <jeanneroy62@gmail.com>

Sent: Wednesday, September 11, 2024 11:22 AM

To: Paglia, Tanya <Tanya.Paglia@portlandoregon.gov>

Subject: Comments for 9/19 hearing about Ida B Wells Modernization

Hello Tanya,

I would like to request that the plan for modernization include sheltered bike parking like Grant has. I live close to the school, and every day SUVs are parked on our street so that it's difficult to get in and out. I don't know why students are not biking to school, but I believe that sheltered bike parking will make it more attractive.

Jeanne Roy
6805 SW 12th Ave.
Portland 97219
503-244-0026



City of Portland, Oregon - Portland Permitting & Development

1900 SW Fourth Avenue • Portland, Oregon 97201 | 503-823-7300 | www.portland.gov/ppd

Early Assistance Application

FOR INTAKE, STAFF USE ONLY

Date Rec _____ by _____

LU Reviews Expected _____

Related cases _____

☐ Y ☐ N Unincorporated MC

☐ Y ☐ N Potential Landslide Hazard Area (LD & PD only)

☐ Y ☐ N Combined Flood Hazard Area

☐ Y ☐ N DOGAMI (high)

File Number: _____

Appt Date/Time: _____

Qtr Sec Map(s) _____ Zoning _____

Plan District _____

Historic and/or Design District _____

Neighborhood _____

District Coalition _____

Business Assoc _____

Neighborhood within 400/1000 ft _____

APPLICANT: Complete all sections below. Email this application and supporting documents (listed on page 2) to: LandUseIntake@portlandoregon.gov.
Once the application is received, staff will contact you regarding payment and scheduling a date and time for your meeting, if you're requesting a meeting.

Site Address _____ Site Size/Area _____

Property ID(s) R _____ R _____ R _____ R _____ R _____ R _____ R _____

Short Project Description:

Design & Historic Review New development: list project valuation; Renovations: list exterior alteration value

\$

Check the box that indicates which Early Assistance Type you are applying for and if you'd like a meeting or to receive written notes only.

Please choose one EA type per application.

Early Assistance Type	City Reviewers	On-line MS Teams meeting & written notes provided	No meeting, written notes provided
<input type="checkbox"/> Pre-application Conference This option is only for proposals that need a Type III or IV land use review. Check here if the proposal includes a street vacation <input type="checkbox"/>	Land Use Services, Transportation, Environmental Services, Water, Parks, others as needed		
<input type="checkbox"/> Design Advice Request Public Zoom meeting with Design Commission or Historic Landmarks Commission	Land Use Services and Design Commission or Historic Landmarks Commission		
<input type="checkbox"/> Zoning and Infrastructure Bureaus Check here if the proposal includes a street vacation <input type="checkbox"/>	Land Use Services, Transportation, Environmental Services, Water, Parks		
<input type="checkbox"/> Zoning Only	Land Use Services		
<input type="checkbox"/> Residential Infill & Middle Housing Land Divisions Discussion of options-no plans will be reviewed. The fee is \$776.	Land Use Services		
Pre-Permit Zoning Plan Check	Land Use Services		
<input type="checkbox"/> Public Works Inquiry for 1-2 housing units This option is only for proposals for 1-2 units that don't require a land use review or property line adjustment	Transportation, Environmental Services, Water		

Contact Information of all Interested Parties (Include a separate sheet for additional parties, if needed.)

APPLICANT INFORMATION *Please check this box if the Applicant is also the Owner ☐

Name _____ Company _____

Mailing Address _____

City _____ State _____ Zip Code _____

Day Phone _____ Email _____

Check all that apply: ☐ Owner ☐ Owner's Representative ☐ Other (please list party's role) _____

Name _____ Company _____

Mailing Address _____

City _____ State _____ Zip Code _____

Day Phone _____ Email _____

Check all that apply: ☐ Owner ☐ Owner's Representative ☐ Other (please list party's role) _____

Name _____ Company _____

Mailing Address _____

City _____ State _____ Zip Code _____

Day Phone _____ Email _____

Please submit the following materials with your application to LandUseIntake@portlandoregon.gov

- ☐ Written project description, including proposed stormwater disposal system and additional property IDs, if not included on Page 1.
- ☐ List of questions to be discussed.
- ☐ If you've selected the Residential Infill / Middle Housing Land Division option, we will only accept this completed application & your list of questions (please don't submit anything else). If you want review of site plans for a residential infill option or middle housing land division, please be select either the Zoning Only or the Zoning and Infrastructure Bureaus option.
- ☐ Site plans & elevations, drawn to a measurable scale (with scale and scale bar identified). If the site is in a design overlay and you intend to meet community design standards, full-sized drawings are needed to confirm the standards are met.
- ☐ If the site is in a design overlay and you're planning to meet design standards, completed scorecards are required. Scorecards are available at <https://www.portland.gov/ppd/land-use-review-fees-and-types/design-standards>.

Note the following:

1. See the Land Use Services fee schedule for detailed fee information: www.portland.gov/ppd/current-fee-schedules.
2. Public notice (email and internet posting) is provided for Pre-application conferences and Design Advice Requests.
3. Only materials that are submitted with the original Early Assistance application will be addressed by City staff; we are unable to address any additional or edited materials submitted after the application is received.
4. Estimates for System Development Charges (SDCs) are not provided at Early Assistance Meetings. Please refer to [SDC information on the PP&D website](#).
5. Plans examiners (building code specialists) do not participate in Early Assistance in any capacity. For life safety and building code questions, consult with a plans examiner by [scheduling a 15-minute appointment](#) or applying for a [Life Safety Preliminary Meeting](#)



City of Portland, Oregon
Bureau of Development Services
Land Use Services
FROM CONCEPT TO CONSTRUCTION

Carmen Rubio, Commissioner
David Kuhnhausen, Interim Director
Phone: (503) 823-7300
TTY: 711
www.portland.gov/bds

BDS – Conference Facilitator Summary Memo

Pre-Application Conference

Date: January 23, 2024

To: Donna Bezio, Portland Public Schools, dbezio@pps.net
Becca Cavell, Project Manager, BORA Architects & Interiors, cavell@bora.co
Debbie Cleek, The Bookin Group, cleek@bookingroup.com

From: Matt Wickstrom, Conference Facilitator
Matt.Wickstrom@portlandoregon.gov, 503-865-6513

Case File: EA 23-105044

Location: 1151 SW VERMONT ST

Property ID: R328555, R328556, R328614, R328618, R328614, R328614

Proposal: A Pre-Application Conference to discuss redevelopment of the existing high school site with a new 3-4 story building to accommodate 1,700 students. Redevelopment of the site will also include practice fields, parking lots and a potential swimming pool. The school will offer a full range of high school facilities, including a theater, band and choir rooms, a cafeteria and commons, and a media center as well as classrooms and science labs, art rooms, shop spaces and a variety of specialized learning environments. The school may also accommodate a small childcare center and a health clinic.

This conference summary report identifies the participants at the conference, provides an initial response to the issues and requirements for the proposed project with separate response from key bureau representatives. This memo identifies current land use review fee information and provides related information that may be helpful as the project moves from concept to completion.

Pre-Application Conferences are required for all major (Type III and IV procedure) land use reviews. The purpose of the conference is to inform the applicant of the substantive and procedural requirements of the Land Use Review, to identify the submittal requirements and documents this information for the applicant and all interested persons.

The information provided at the conference and included in this summary is based on the information that was submitted prior to and at the meeting and reflects regulations in effect at the time of the conference. The meeting is intended to convey information. It is not a public hearing and no land use decision is rendered at the conference. Interested persons may attend the conference and obtain copies of all the written information that is submitted and prepared for it.

You must submit your Land Use Review application within two-years of the Conference.

Conference date: December 21, 2023

Expiration of Conference: December 20, 2025

A. Comments from Bureau Representatives: The chart below identifies the staff who participated in the conference and/or who submitted written comments:

Response attached	Bureau	Responsibilities	Contact
Yes	BDS Land Use Services	Review of land use review	Andy Gulizia 503-865-6724 Grace Jeffreys 503-865-6521
Yes	PBOT	Public Streets	Tammy Boren-King 503-823-2948
Yes	BES	Sanitary sewer service, public and private stormwater management, drainageways and drainage reserves, source controls	Abigail Cermak 503-823-7577
Yes	BDS Site Development	Septic systems, private rights-of-way, geotechnical requirements, erosion control	Jeff Duquette 503-823-5075
Yes	Water Bureau	Connections to public water	Bryan Vittori 971-917-5458
Yes	Fire Bureau	Access grades, fire hydrants, turnarounds	Zach Parrish 503-991-2764
Yes	Urban Forestry	Street trees	Amil Jakupovic 503-823-4491
Yes	Portland Parks & Recreation	Trail requirements	Marty Stockton 503-201-7199
No	devTeam Portland, BDS	Further information on devTeam Portland can be accessed at: Process Management - Major Projects Group Portland.gov	Alice Nielsen 503-823-3448

Please refer to the memo from Andy Gulizia and Grace Jeffreys, BDS Land Use Services for the list of application submittal requirements for the required land use review(s). Also, the attached responses from the City bureaus identify additional requirements that are pertinent to the land use review or a later Building Permit submittal.

If you have questions about comments included in this Pre-Application Summary Report, please contact the representative identified in the respective memo. Please note that staff comments are based on the information submitted at the time of application. If you have questions regarding the proposal beyond those covered in this summary report, or if your proposal changes in scope or configuration, a new Pre-Application Conference may be required or an additional Early Assistance application may be needed to provide responses to your follow-up questions.

B. Fees

Below is an estimate of land use fees that may apply to your proposal. Fees charged will be those in effect when the Land Use Review application is submitted. When more than one Land Use Review is requested, full fees are charged for each additional review. The fees below are current; however, fees will increase effective July 1, 2024. You may view the current [Land Use Review](#)

[fees](#) at the following link: [Fee Schedules - Building Permit Costs, Trade Permit Costs and Other BDS Fees | Portland.gov](#)

Land Use Review Type	Estimated Fee
Type III Conditional Use Review	\$18,473 + \$5,503 (for each Adjustment Review)

During the building permit process, Permit Fees will be charged for review of your permits and Systems Development Charges (SDCs) may be assessed for new development. An online fee estimator is available on the BDS website at the following link: [Estimate the Cost of Your Building Permit | Portland.gov](#).

C. Other Information

1. Preliminary Life Safety Plan Review Consultation: For Building Code/Life Safety information, you may sign up for a Preliminary Life Safety meeting. Information can be found at the following link: [Life Safety Preliminary Meeting Request Packet | Portland.gov](#)

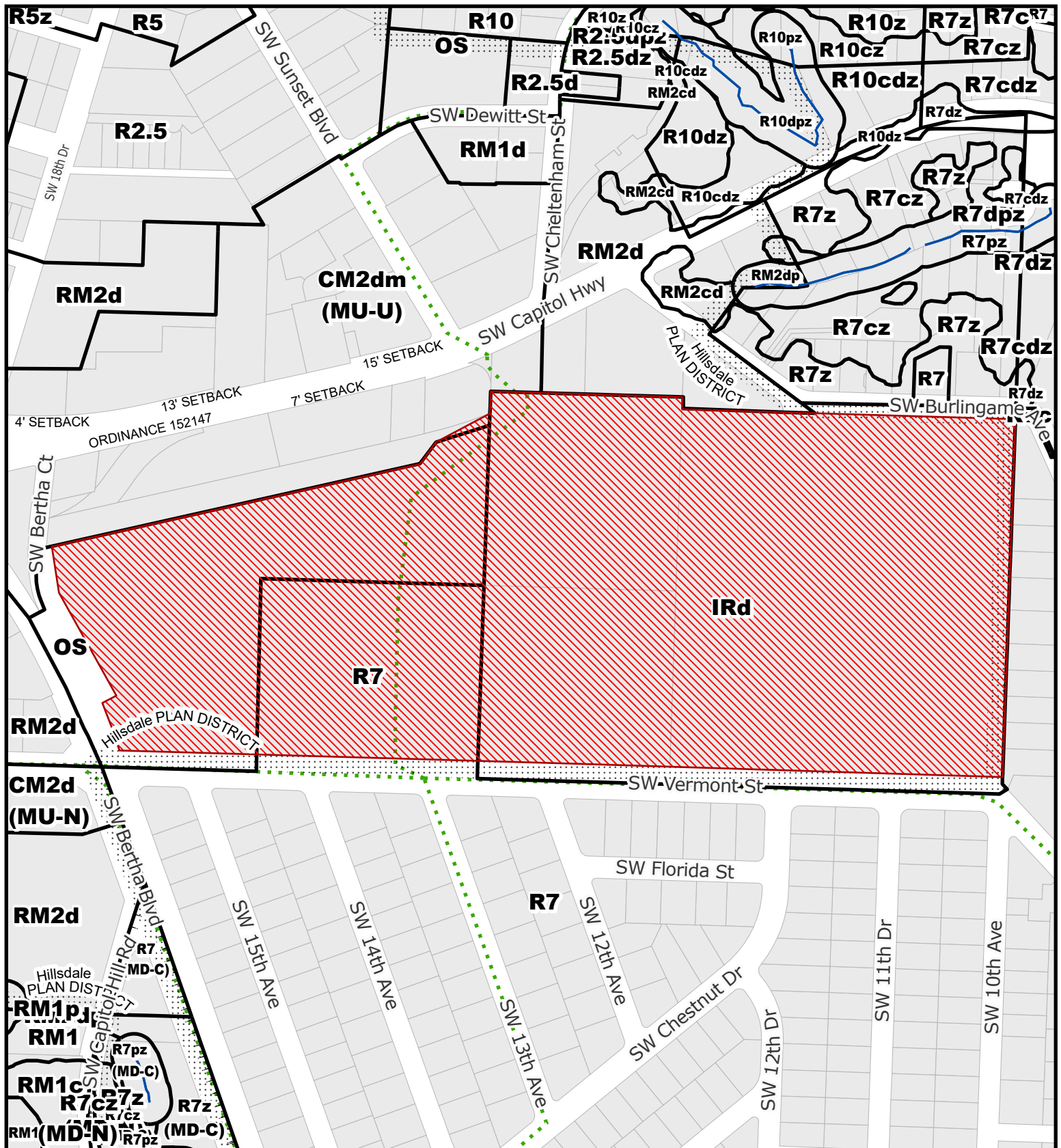
2. Electric Service Requirements. Information on electric service requirements for properties served by PGE can be found at the following link: <https://www.portlandgeneral.com/construction/electric-service-requirements>; and information on electric service requirements for properties served by Pacific Power can be found at the following link: <https://www.pacificpower.net/working-with-us/builders-contractors/electric-service-requirements.html>

Please note that the service requirements included in these links may not cover all requirements associated with your project. Applicants should contact the PGE Service Coordinator at 503-736-5450 or the Pacific Power Business Center at 888-221-7070 to identify issues that are specific to your project and to coordinate electric service requirements.

3. PGE requires minimum clearances from electric wires, conductors and cables. Before building, please be aware of these clearances by calling PGE at 503-736-5450. For more information on the [PGE Minimum Clearance Requirements](#) use the following link: [Why You Should Respect Portland General Electric's Power Line Clearances | Portland.gov](#).
4. Portland Housing Bureau. The Housing Bureau provides financial assistance for various development phases to both nonprofit and for-profit affordable housing developers (typically multi-dwelling rental housing). For more information, go to the following link: <https://www.portland.gov/phb>.
5. Bureau of Planning and Sustainability. The Bureau of Planning and Sustainability delivers policy and programs related to green buildings, energy efficiency, renewable resources, waste reduction, and recycling. For more information on [Sustainability Programs and Services](#) in the Bureau of Planning and Sustainability go to the following link: <https://www.portlandoregon.gov/bps/67121>.
6. Energy Trust of Oregon. The [Energy Trust of Oregon](#) provides technical assistance and cash incentives for energy efficient design. For more information, go to the following link: <https://www.energytrust.org/>
7. Oregon Department of Energy. The [Oregon Department of Energy](#) Conservation Division provides information on a variety of programs to encourage energy conservation, including tax rebates and low-interest energy loans. For more information, go to the following link: <http://www.oregon.gov/energy/Pages/index.aspx>

Attachments:

Zoning Map
Scheme 01: Lift
Scheme 03: Gather
BDS Land Use Services Response
PBOT Response
BES Response
BDS Site Development Response
Water Bureau Response
Fire Bureau Response
Urban Forestry Response
Portland Parks & Recreation Response
Sign-in Sheet



For Zoning Code in Effect Post October 1, 2022

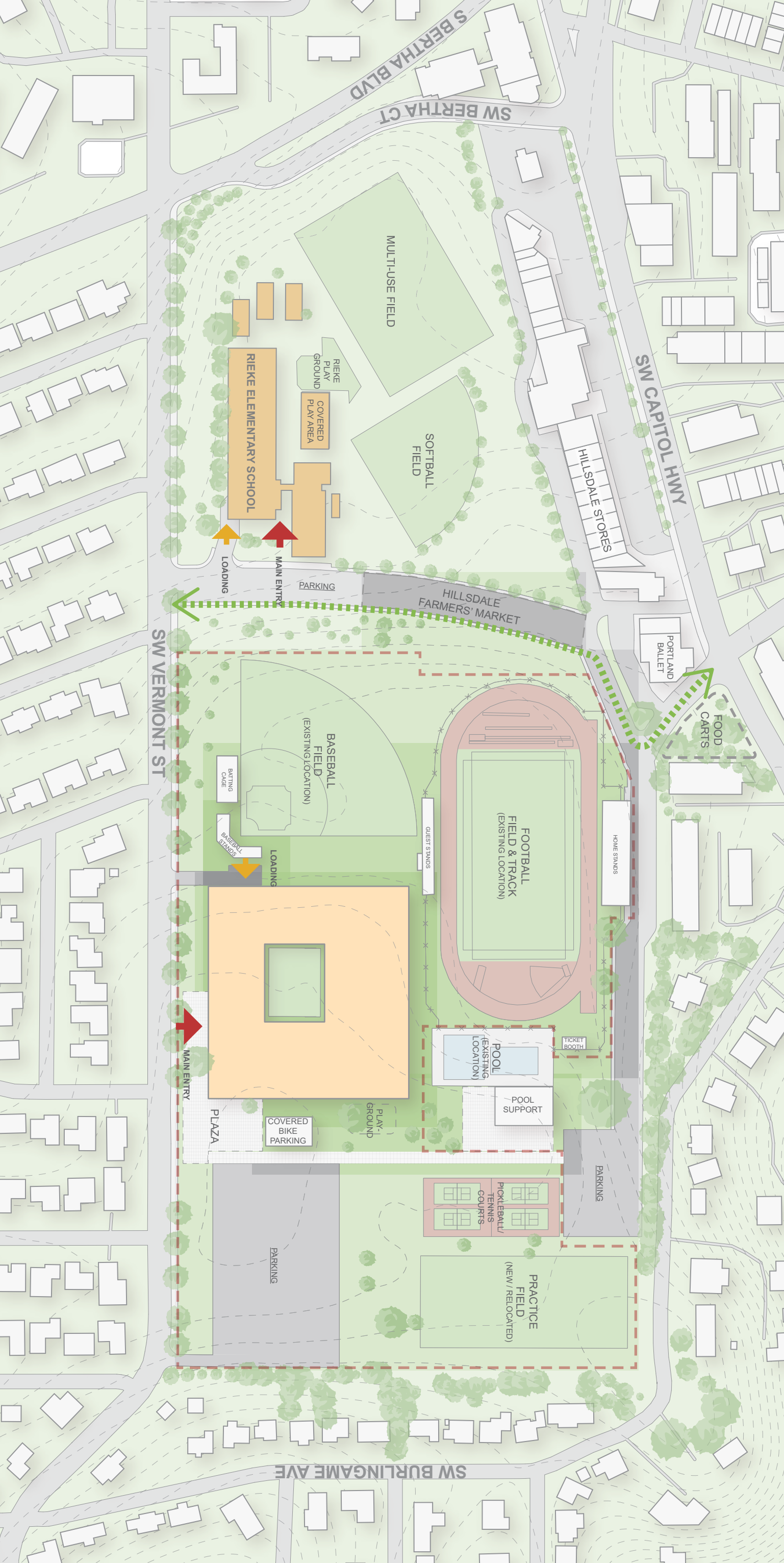
ZONING

THIS SITE LIES WITHIN THE:
HILLSDALE PLAN DISTRICT

- Site
- Plan District
- Stream
- Recreational Trails

File No.	EA 23 - 105044 PC
1/4 Section	3627,3628
Scale	1 inch =300 feet
State ID	1S1E16DC 3700
Exhibit	B Nov 28, 2023

IDA B. WELLS HIGH SCHOOL MODERNIZATION



SCHEME 01: LIFT



KEY:

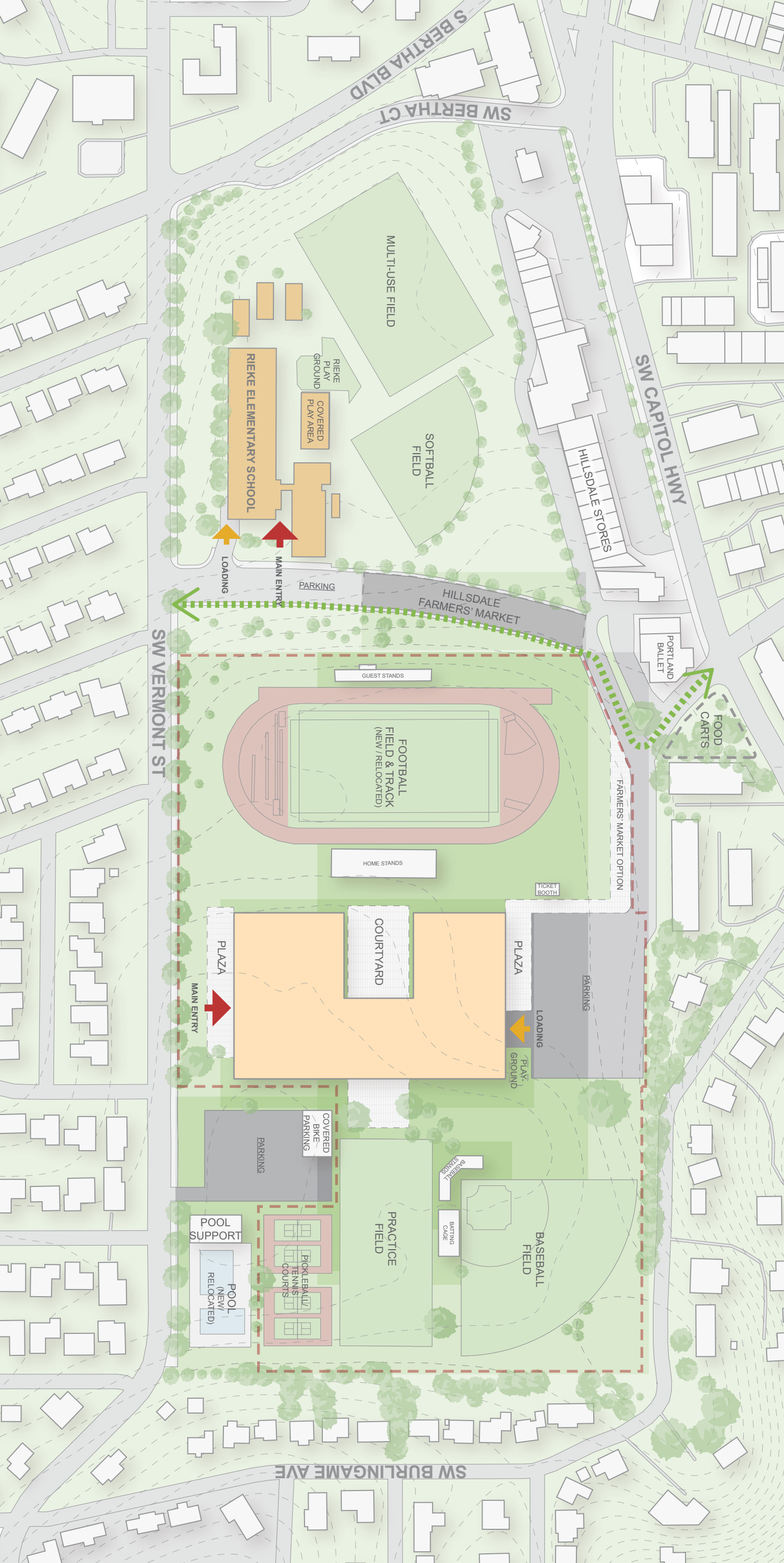


Security Perimeter



SW Trail

IDA B. WELLS HIGH SCHOOL MODERNIZATION



KEY:

Security Perimeter

SW Trail

SCHEME 03: GATHER





City of Portland, Oregon
Bureau of Development Services
Land Use Services
FROM CONCEPT TO CONSTRUCTION

Carmen Rubio, Commissioner
David Kuhnhausen, Interim Director
Phone: (503) 823-7300
TTY: 711
www.portland.gov/bds

BDS – Land Use Planner Response

Pre-Application Conference

Date: January 8, 2024

To: Donna Bezio
(503) 804-5059
dbezio@pps.net

From: Grace Jeffreys
503-865-6521, Grace.Jeffreys@portlandoregon.gov
Andrew Gulizia
503-865-6714, Andrew.Gulizia@portlandoregon.gov

File No.: 23-105044

Location: 1151 SW Vermont St.

Tax Account: R328555, R328556, R328614, R328618

State ID Number: 1S1E16CD 03800, 1S1E16CD 03900, 1S1E16CD 02500, 1S1E16DC 03700

Zoning: IR(IC), OS, R7 & CM2(MU-U), d, Hillsdale

Proposal: A Pre-Application Conference to discuss redevelopment of the existing high school site with a new 3-4 story building to accommodate 1,700 students. Redevelopment of the site will also include practice fields, parking lots and a potential swimming pool. The school will offer a full range of high school facilities, including a theater, band and choir rooms, a cafeteria and commons, and a media center as well as classrooms and science labs, art rooms, shop spaces and a variety of specialized learning environments. The school may also accommodate a small childcare center and a health clinic.

Please contact us with questions regarding this memo, or if we can be of further assistance as you move forward with your proposal.

The information provided at the conference and included in this response is based on the information you provided prior to and at the conference and reflects regulations in effect at the time of the conference. This response provides information and guidance only. It is preliminary in nature and based on the information the applicant provided to BDS staff. It is neither a land use review nor a final decision regarding this project. References are to the Portland Zoning Code available online at www.portland.gov/code/33.

Zoning and Street Classifications of “Site”:

- **Base:** IR, OS, R7 & CM2
- **Overlay(s):** Design (d)
- **Plan District:** Hillsdale
- **Other:** Recreational Trail designation through the site

- **Pattern Area:** Western
- **Pedestrian District:** Hillsdale
- **Streets:** Adjacent streets are classified as follows:
 - SW Burlingame Avenue: City Bikeway, Neighborhood Walkway, Local Service Transit and Traffic.
 - SW Vermont Street: City Bikeway, Neighborhood Walkway, Local Service Transit and Traffic.
 - SW Bertha Court: City Bikeway, Neighborhood Walkway, Local Service Transit and Traffic.
 - SW Bertha Blvd.: Major City Walkway, City Bikeway, Transit Access and District Collector Traffic.
- **Corridors:** SW Bertha Blvd. is a Neighborhood Corridor
- **Area Plans:** Hilldale Town Center Plan, Southwest Community Plan
- **Centers:** Hillsdale Town Center
- **Historic Designation:** The existing high school building is on the city's Historic Resources Inventory (HRI)

A. Key Issues – Conditional Use Review:

The following issues and requirements have been summarized for the applicant to pay special attention to as they may impact the proposed project.

1. Type III Conditional Use Review Required. A Type III Conditional Use Review is required for this proposal per 33.281.050.C. The Type III land use review process is governed by 33.730.030 and is summarized here: <https://www.portland.gov/bds/documents/type-iii-land-use-procedure/download>. A Hearings Officer would issue the decision on the Conditional Use Review, and the Hearings Officer's decision could be appealed to the City Council.
2. Neighborhood Contact Requirement. Per 33.420.030, for proposals in the Design overlay zone that will add at least 10,000 square feet of net building area to a site, the neighborhood contact steps of 33.705.020.C., Neighborhood Contact III, are required. This requirement must be completed 14 days before submitting a land use review application. Additional information on the requirements of Neighborhood Contact III requirements at [portland.gov/bds/neighborhood-contact](https://www.portland.gov/bds/neighborhood-contact). A timeline of the process is available at <https://www.portland.gov/sites/default/files/2022/Neighborhood%20Contact%203%20Timeline%202022.04.pdf>
3. Fees. Current fees for land use reviews can be found in the Land Use Services Fee Schedule online here: <https://www.portland.gov/bds/zoning-land-use/land-use-review-fees-and-types#toc-land-use-review-fees>.
4. Conditional Use Review Approval Criteria. Because this site includes multiple base zones, two sets of Conditional Use approval criteria must be addressed: 33.815.100 (Uses in the Open Space Zone) and 33.815.105 (Institutional and Other Uses in Residential and Campus Institutional Zones). Please note the following as you prepare responses to these approval criteria:
 - a. To address the OS zone character and impacts (33.815.100.A), please refer to the purpose statement for the OS zone in 33.100.010.

- b. To address the proportion of Household Living (residential) uses in the residential area (33.815.105.A), please identify and discuss any other non-residential uses on residentially zoned property within an approximately 400-foot radius of the site. Please also detail the intensity and scale of the existing and proposed uses on the site.
 - c. To address physical compatibility (33.815.105.B), please provide details about how the design of proposed improvements will maintain compatibility with nearby residential areas. Mitigation measures such as setback distances and landscape screening should be discussed. For 33.815.105.B.1, you can state there are no City-designated scenic resources on or near this site.
 - d. To address public services (33.815.100.B and 33.815.105.D), please refer to the responses from the Portland Bureau of Transportation, the Bureau of Environmental Services, and the Portland Water Bureau.
 - e. To address livability impacts (33.815.100.C and 33.815.105.C), please discuss each of the listed factors individually. A photometric diagram should demonstrate how any exterior lights (including field lights) will not exceed the maximum glare standard in 33.262.080.A.
 - f. To address area plans (33.815.100.D and 33.815.105.E), please identify and discuss relevant statements from the Hillsdale Town Center Plan (<https://www.portland.gov/bps/planning/comp-plan-2035/documents/hillsdale-town-center-1997/download>) and the Southwest Community Plan (<https://www.portland.gov/bps/planning/comp-plan-2035/documents/southwest-community-plan-vision-policies-and-objectives-2000/download>).
5. Adjustments. You can include Adjustment requests with your Conditional Use Review application for Zoning Code standards that will not be met outright, subject to the limitations in 33.805.030. Please address the approval criteria in 33.805.040.A-F for each Adjustment request. For approval criterion A, please specifically address the purpose statement in the Zoning Code for each regulation to be modified. For approval criterion C (applicable if more than one Adjustment is requested), please refer to the purpose of the IR zone described in 33.150.010 and 33.150.030.C. For Adjustment requests that affect the part of the site zoned OS, please also address 33.100.010. For Adjustment requests that affect the part of the site zone R7, please also address 33.110.010. If you will apply for Design Review (see Section B, below), please include requests to modify zoning standards with the Design Review application rather than with the Conditional Use Review.
6. Conditional Use Master Plan or Impact Mitigation Plan Option. Though not required, you can pursue a Conditional Use Master Plan or Impact Mitigation Plan for this site instead of a standard Conditional Use Review. These reviews are also subject to the Type III land use review process and are generally intended for master plans that involve multiple phases of development. The requirements and approval criteria for a Conditional Use Master Plan are in 33.820 and the requirements and approval criteria for an Impact Mitigation Plan are in 33.848. Please also review the answer to your question about this in Section G, below.

B. Key Issues – Design Review:

The following issues and requirements have been summarized for the applicant to pay special attention to as they may impact the proposed project.

1. Design Review Options:

- a. Design Standards Track. For work proposed in the part of the site which has a design overlay and is IR zoned, since there is no Impact Mitigation Plan or Conditional Use Master Plan in effect for this site, the proposal is eligible to use the non-discretionary design standards track in Section 33.420.050. The Design Standards of Section 33.420.050, Table 420-2 will apply if this track is chosen. The table features required standards, all of which must be met, as well as optional standards, which are assigned points. The number of points that must be earned varies with the building height, site area and type of development. Note, development standards listed in Section 33.420.050 are separate from those in the base zone and cannot be adjusted or modified. If they cannot be met, a Design Review is required.
- b. Design Review Track. If not using design standards, a Type III Design Review will be required per Table 825-1 of Section 33.825.025. Additional information about Design Review can be found in the sections below.
- c. Neighborhood Contact Requirement (33.420.030). For proposals in the Design Overlay Zone that will add at least 10,000 square feet of net building area to a site, the neighborhood contact steps of 33.705.020.C., Neighborhood Contact III, are required. This requirement must be completed 14 days before submitting a Design Review application (or building permit for projects utilizing the Design Standards). Additional information on the requirements of Neighborhood Contact III requirements at portland.gov/bds/neighborhood-contact. A timeline of the process is available at <https://www.portland.gov/sites/default/files/2022/Neighborhood%20Contact%203%20Timeline%202022.04.pdf>

2. Design Review and Other Land Use Review Processes

- a. Design Review Approval Criteria. The applicable approval criteria are the Portland Citywide Design Guidelines and can be found at www.portlandoregon.gov/designguidelines. A matrix of the guidelines for your use has been attached.
- b. Modifications and Adjustments. The development standards of the Zoning Code are expected to be met. However, if a standard cannot be met, a Modification or Adjustment review can be requested alongside the Design Review. Please note, approvals are not guaranteed. To be approved, requests for Modifications or Adjustments must mitigate and better meet the purpose of the standard and applicable Design Guidelines.
 1. A Modification Review may be requested for site-related standards (such as setbacks, size of loading spaces) that are not met. Approval Criteria are found under Section 33.825.040.
 2. An Adjustment Review may be requested as part of the Design Review for use-related development standards (such as building height, number of loading spaces, number of parking spaces) that are not met. Approval Criteria are found under Section 33.805.040.

All additional land use reviews should be listed on the land use application, the respective fees paid, and a response provided that addresses the additional approval criteria listed in the relevant Portland Zoning Code Chapters.

- c. Fee(s). Current fee(s) for land use reviews can be found in the Land Use Services Fee Schedule online here: <https://www.portland.gov/bds/zoning-land-use/land-use-review-fees-and-types#toc-land-use-review-fees>.
- d. 2022 Guide to Design Review. You are encouraged to review the Guide to the Design Review Process prepared by the Design Commission, which provides guidance and expectations of the Design Review process. This guide was adopted by the Design Commission in January 2022. The guide can be found at https://www.portland.gov/sites/default/files/2022/2022-dz-guide-to-design-review_2.pdf.
- e. Certificate of Compliance. Approval of a Design Review allows for the proposed work to be built. The expectation is that the building permit will reflect the project (including the details) that was approved. To ensure this, a Certificate of Compliance will be required at the time of building permit as indicated in a condition of approval. The Certificate of Compliance form can be found at [portland.gov/bds/documents/certificate-compliance-design-and-historic-resource-review-approvals](https://www.portland.gov/bds/documents/certificate-compliance-design-and-historic-resource-review-approvals).
- f. Design Review Submittal.

GENERAL

- Digital submittal required.
- For final drawings (C Exhibits) and Appendix set (APP Exhibits):
 - Use 11"x17" format.
 - Leave a 1.5"x5" blank space at the bottom right corner for Staff to add the case number, exhibit number and stamp.
- Conduct a thorough review before submitting your drawing packet.
- Review all color quality in submittal to ensure it accurately represent the colors intended.
- When returning for a 2nd hearing, revisions to the prior submittals should be illustrated and clearly marked in a side-by-side comparison.

PROJECT INFORMATION & NARRATIVE

- Land Use Review application form
- Project team and project cost
- Project description
- Zoning summary
- Response to guidelines/approval criteria (Word doc.)
- Modifications and Adjustments requests & approval criteria responses (Word doc.)
- Response to DAR (narrative)
- Technical Reports - Stormwater Loading Analysis, Queuing Study, etc.

DRAWINGS

"C" Exhibits should represent proposed development/alterations, be at an architectural or engineering scale and use 11"x17" format:

- Title Page
- Table of Contents
- Site Plan
- Floor and Roof Plans - roof plan should show all rooftop elements, including mechanical.
- Elevations - B/W and color, and without shade or shadows, include material key, street-facing elevations in their immediate context, including adjacent buildings
- Building Sections - Include some depicting relationships to adjacent buildings

Enlarged Details - windows/doors, storefronts, canopies, balconies, signage and their attachments, etc., control joints, seismic joints, and other visible construction details.

- Materials / Colors - clearly identify each building material by name.
- Landscape Plans
- Lighting Plans
- Civil Plans
- Cut Sheets - only pertinent product info like type, finish, color, dimensions.

SUPPORTING INFORMATION

Appendix ("APP" Exhibits) should include information that supports the drawings and use 11"x17" format:

- Renderings - day and night in context, must be simple and not enhanced marketing-type images, avoid dramatic lighting effects.
- Context - plan area, urban (3-block radius), site
- Sightlines - sightline drawings from relevant vantage points
- Material Photos & Examples
- Massing & Design Concept
- Miscellaneous Diagrams - FAR, ground floor windows, clear vs. spandrel panels, height, Modifications, Adjustments, etc.
- Responses to DAR (diagram)

g. Design Review Approval Criteria

This preliminary feedback is in response to the information in your Pre-Application Conference submittal and based on the approval criteria of the Portland Citywide Design Guidelines (PCDG).

1. CONTEXT (PCDG 01, 02 & 03) – Site Specific comments:

The site is in the Hillsdale Town Center and the Inner Neighborhoods Pattern area. For this location, note (from the Portland Citywide Design Guidelines):

*Within **Town Centers**, development should provide links to and amenities for the region's high-capacity transit system. Open spaces such as plazas created by new development should support business operations, social interaction, gathering, waiting, and augmenting large community-focused events and activities.*

Because of the site's town center location and the community-oriented use (a high school), consideration will be needed of relationships to this existing pedestrian-oriented context. From the three PCDG guidelines related to context:

a. PCDG 01. Build on the character, local identity, and aspiration of the place.

- Community – Show how the proposal relates to the local community's identity, history, and cultural values and places.
- Nature - Show how the proposal reflects and enhances local natural resources.

b. PCDG 02: Create Positive Relationship with surroundings.

- Building Massing - Show how the proposal develops effective placement and proportion of building massing toward adjacent lower-scale development and residential uses.

- Street Wall – Show how the proposal helps create a more vibrant relationship to the streets. Because it is such a large site, consider how to use strategic locations to create stronger relationships to existing streets, especially the site corners and street terminus locations.

SW Vermont: When approaching the site from areas south and east of the site, the southeast corner of the SW Vermont frontage is the first part of the site one sees. Currently there is a surface parking area at that corner.

- **Scheme 1** increases the size of this surface parking, which creates an even less welcoming face to those approaching from this direction.
- **Scheme 3** locates the new pool in this corner of the site, which provides some potential for a more welcoming frontage at this corner.

Consider how to create stronger connections towards those approaching the site from the southeast.

SW Burlingame Ave: Along the length of northern frontage along SW Burlingame, there is surface parking with blocked off curb cuts. The existing building is set back but does contain an entrance facing north.

- **Both Schemes 1 & 3** have a main entrance facing south, and contain no entrance facing north, which results in turning their backs towards the north and the main town center. Consider how to create stronger connections towards the town center to the north.

- Connectivity - Show how the proposal creates visual and physical links to adjacent pedestrian pathways and neighboring open spaces. As noted above, both Schemes 1 & 3 need to create stronger connections to the north and the town center area. If a main entrance remains facing south, one way to do this may be to create stronger connections through the site from north to south.

Provide studies of existing and proposed pedestrian movement adjacent to, through and within the site, set within the wider context. Consider how the proposal will support and enhance near-by pedestrian-related amenities such as transit, gathering spaces such as the Farmers Market, close-by retail and community uses (library), and pedestrian recreation opportunities (trails).

c. PCDG 03: Integrate and enhance on-site features and opportunities to contribute to a location's uniqueness.

- Natural Resources - Show how the proposal integrates topography and natural resources found on-site.
- Viewpoints - Show how the proposal integrates views to community points of interest.
- Social and Cultural Significance - Show how the proposal incorporates the site's significant cultural or social history.

2. PUBLIC REALM - General Comments:

- a. Ground Floor Activation - Maximize the ground floor glazing and active uses behind it, especially on corners (PCDG 04, 05).
- b. Ground level weather protection - Pedestrian weather protection should be provided. This can be achieved with generous canopies that project out over the

sidewalk or ground level setbacks for at least a majority of all street frontages (PCDG 04).

- c. Utility Transformers - Please note new requirements in Section 5.a. below, Electric Utility Provider Coordination (PCDG 04, 05). If new utility transformers are needed, they should be located on-site as follows:
 - Within the building behind active uses or behind loading area, or
 - In underground parking garages, or
 - Underground below open space if the lid and associated no-build zone are integrated with the landscape design.
- d. Gas & Electric Meters - Locate gas and electric meters inside the building to minimize their impact. Gas regulators may be placed on the building's exterior and should be well integrated and, ideally, screened within the façade (PCDG 06).
- e. Signs - Signs should be sized appropriately for the building, the district and the pedestrian realm and should incorporate district-appropriate materials (PCDG 04).

3. QUALITY AND RESILIENCE - General Comments:

- a. Ground level materials - Materials at the ground floor should be durable enough for use adjacent to public sidewalks, such as brick and masonry (PCDG 08).
- b. Building materials - High quality, durable building materials (building skin, storefronts, windows, doors, canopies, signs, etc.), that respond to the context of the surrounding area or district, are expected. At ground level facades facing pedestrian areas, provide robust materials that ensure longevity (PCDG 08).
- c. Vents/Louvers & Mechanical - Vents through the roof are preferable. If wall-mounted on the facade, vents/louvers should be integrated into the window openings. The Code Guide specific to the screening of dryer vents can be found at portlandoregon.gov/bds/article/726141. Associated mechanical units should be organized and screened (PCDG 08).
- d. Exterior Lighting - Exterior lighting should be integrated into the building's overall concept and ensure a safe pedestrian condition along the adjacent sidewalk and within open spaces on the site. Exterior lighting can be used to highlight the building's architecture, however, should not impact the skyline at night (PCDG 04, 08).
- e. Landscaping - Plan for sufficient space to support large shade trees and layered landscaping. Consider locating trees to the south and west of buildings to maximize their shade benefits. Large shade trees are increasingly critical to the health and comfort of residents and occupants (PCDG 03, 07, 09)

C. Processing the Reviews

1. If you apply for Design Review (rather than using the Design Standards track in 33.420.050), please plan to apply for the Conditional Use Review at the same time, but in a separate application. Because the Conditional Use Review requires compatibility of the building with adjacent residential developments under 33.815.105.B, and the Design Review gets into the details of the building design, it is usually best for the Design Review to be deemed complete before the Conditional Use Review is deemed complete. This is to

ensure there is high confidence that changes associated with the Design Review, which is a more specific review, will not adversely affect the Conditional Use review and findings, which are more general. If the Conditional Use application is deemed complete prior to the Design Review and the Design Review proposal gets modified during its separate review, the Conditional Use may need to be updated or redone to be consistent and comply with the Design Review.

2. Appeals of a Hearings Officer's decision for a Type III Conditional Use Review and appeals of Design Commission's Decision of a Type III Design Review both go before the City Council.
3. To have a *De Novo* appeal hearing before City Council that allows new information to be considered rather than an on-the-record hearing with only existing evidence, the applicant must submit the request for a *De Novo*/evidentiary hearing that also provides a full extension of the statutory 120-day clock. The request must be made within 21 days of submission of the original land use application. That form can be found online here: <https://www.portland.gov/bds/documents/request-evidentiary-hearing-and-waiver-right-decision-within-120-days/download>

D. Applicable Development Standards

Please note, meeting the minimum Title 33 standards does not ensure that a project will meet the Conditional Use Review or Design Review approval criteria and receive approval. It is recommended you design to meet/exceed the approval criteria, and not begin with just meeting the minimum Title 33 standards.

1. Development standards that will apply to the project. These include, but are not necessarily limited to, those from the following chapters in the Zoning Code (Title 33) and other City codes available online at www.portland.gov/code.
 - 33.100 Open Space Zone
 - 33.110 Single-Dwelling Residential Zones
 - 33.150 Campus Institutional Zones
 - 33.248 Landscaping and Screening
 - 33.258 Nonconforming Situations
 - 33.262 Off-Site Impacts
 - 33.266 Parking, Loading, and Transportation and Parking Demand Management
 - 33.272 Public Recreational Trails
 - 33.279 Recreational Fields for Organized Sports
 - 33.281 Schools and School Sites
 - 33.420 Design Overlay Zone
 - 33.445 Historic Resource Overlay Zone
 - 33.534 Hillsdale Plan District
 - Title 11 Tree Code
 - Title 32 Sign Code
2. Specific development standards to note for this project. This list is not intended to be comprehensive. Always reference the code for all applicable development standards and the exact language of each standard. Also, please review Section G, below, for zoning regulations you asked about in your letter.

- The existing high school building on the site is noted as a Significant Resource in the City's Historic Resource Inventory (HRI). Demolition or major alterations constituting a demolition of Significant Resources on the City's HRI is subject to the 120-day delay as specified in 33.445.330 and 33.445.340. Per 33.445.340, the 120-day delay allows time for consideration of alternatives to demolition, such as rehabilitation, reuse, relocation, or architectural salvage. The 120-day delay is a nondiscretionary administrative process with public notice but no hearing. An application for a 120-day Demolition Delay can be found at portlandoregon.gov/bds/article/154486
- New or modified motor vehicle parking areas must comply with all applicable requirements in 33.266.130. As you review these regulations, please keep in mind this site is in a pedestrian district.
- Bike parking regulations are in 33.266.200 and 33.266.210. For 33.266.200, this site is in the "Standard B" bike parking area.
- The new high school building will require two "Standard A" truck loading spaces. Please review 33.266.310.
- Please carefully review the nonconforming upgrades requirement in 33.258.070.D.2. The nonconforming upgrades requirement will apply to the entire site, including both the elementary school and high school campuses (33.258.070.D.2.c.1). Please note the tree density requirement for the site (33.258.070.D.2.b.1, 11.50.050) can be based on the area inside the development impact area (11.50.030), though the required trees can be anywhere on the site.
- A landscaping plan is required that complies with all applicable requirements in 33.248. This document may be helpful in selecting plant varieties: <https://www.portland.gov/sites/default/files/2020/landscaping-manual-5.pdf>. Note that because this is a school site, 33.281.140 generally allows lower shrubs to substitute for taller shrubs that may be required in other Zoning Code chapters.
- Signs larger than 32 square feet within the Design ("d") overlay zone are subject to Design Review.
- A tree plan is required that complies with the following Tree Code Sections: 11.50.040 (tree preservation), 11.50.050 (tree density), and 11.60.030 (tree protection specifications). The development impact area option in 11.50.030 is also relevant.

E. Coordination with Other Agencies

Refer to individual responses from all participating bureaus, review groups and agencies for more detailed information.

1. Electric Utility Provider Coordination

- Utility Transformers – Plan for Power (new change for 7/1/22). Utility transformers should be located on-site, behind active use street frontage, behind at-grade loading docks, or in underground parking garages. Please consult with the Case Planner assigned to this Early Assistance appointment and coordinate with the utility providing electrical service to determine power needs, transformer sizing and transformer location. Please note this document [Guidance to Siting Electrical Transformers](#) and the below [Path to Siting Electrical Transformers](#).

If an agreement is made with the Land Use Case Planner, and a written acknowledgement from the Utility Provider is provided per the steps below, a

transformer can be located in the ROW. Please note, [PBOT has a Design Exception Request per TRN 8.13](#) process for locating private use utility vaults in the public right-of-way, known as Utility Vault Exclusive Use for Development (UVE). Additional information can be found at portland.gov/transportation/development/utility-vault-permitting-and-leasing-development.

Path to Siting Electric Transformers

1. Planner communicates the following to applicant:
 - a. Plan for power on-site: contact your power company to understand your power needs (see contacts further below). Specifically, the size of the utility provider's transformer and electrical room.
 - b. Identify your ground floor activation requirements (Zoning Code/land use approval criteria)
 2. If planner finds no Code or Design/Historic Resource Review issues with the proposed location for a transformer on site, transformer is located on site.
 3. If planner finds Code or Design/Historic Resource Review regulations not met with the proposed location[s] for a transformer on site:
 - a. Applicant works with power provider to identify other suitable locations on private property. If unsuccessful, applicant follows 'initial submittal' guidelines in TRN 8.13, co-signed by utility, to request locating transformer in a vault in the ROW (and city staff and applicant can get started on locating the vault to maximize trees and allow for utility needs); or
 - b. Applicant can pursue further discussion to study design solutions in a Design Advice Request or additional Early Assistance meetings to allow transformer on site.
 - If a solution is found, transformer is located on site.
 - If no solutions, follow step 3.a above.
- For properties served by PGE - portlandgeneral.com/construction/electric-service-requirements
 - For properties served by Pacific Power - pacificpower.net/working-with-us/builders-contractors/electric-service-requirements.html
 - Note that the service requirements included in these links may not cover all requirements associated with your project. Applicants should contact the PGE Service Coordinator at 503-736-5450 or the Pacific Power Business Center at 888-221-7070 to identify issues that are specific to your project and to coordinate electric service requirements.
 - PGE requires minimum clearances from electric wires, conductors and cables. Please be aware of these clearances by calling PGE at 503-736-5450. More information on PGE minimum clearance can be found at portland.gov/bds/documents/why-you-should-respect-portland-general-electrics-power-line-clearances
2. Transportation (PBOT)
 - Garage entrance setback from property line. A "Driveway Design Exception" (DDE) by PBOT is required for garage entry doors to be located less than 20' from the property line to ensure that queuing in the ROW is minimized. The Design Review process supports garage doors to be located no more than 5' from the building façade.

A DDE application is required to be submitted to PBOT with or in advance of the Land Use Review because the DDE must be completed prior to issuance of a Land Use Review decision. The application and information can be found at portlandoregon.gov/transportation/77521

- Oriel Windows. Projecting bays (aka oriel windows) are subject to the Window Projections into Public ROW standards. Note standards A through D must be met. However, Exceptions to standards E through G can be requested as part of the Design Review. The Code Guide for these standards can be found at portlandoregon.gov/bds/article/68600. A Modification fee is required for staff to consider an Exception.
- Loading. If Modifications or Adjustments to Title 33 loading standards are contemplated, you must reach out early and coordinate with both PBOT and BDS Design Review staff ahead of the Land Use Review submittal. PBOT may require a Traffic Scope Approval and Loading Demand Study, which both need to be submitted before the Land Use Review submittal. More information can be found at portland.gov/transportation/development/traffic-impact-studies.
- Public Works Permit – If PBOT requires a 30% Public Works Permit approval before their response to a Design Review, it should be noted that such approvals can often take longer than the Land Use Review process. Therefore, you are highly encouraged to initiate the Public Works Permit process before you submit the Conditional Use Review and Design Review.

3. Environmental Services (BES)

- Stormwater Management. Stormwater management information, including infiltration tests, utility plans, stormwater facility designs, and site landscaping, must be submitted with the Design Review application. BES needs to review these elements early to ensure there are no issues that could affect the building size, location, or site design.

4. Fire / Life Safety

- Preliminary Life Safety Meeting. A separate, preliminary life safety meeting is very beneficial to identify critical life safety and building code issues early in the process and allow the Design Review proposal to address those issues. This meeting is encouraged prior to submitting the Design Review. More information and the application can be found at portland.gov/bds/documents/life-safety-preliminary-meeting-request-packet.
- Glazing and Energy Calculations. If large amounts of glazing are proposed, it is highly encouraged that energy calculations are done prior to submitting the Design Review, in case the building design is affected by energy code requirements.

5. Urban Forestry – Projects that require street tree preservation and planting should reach out to Urban Forestry early in the concept design phase to understand the requirements and process.
6. Parks (PP&R) – Projects adjacent to, or part of, a public open space, greenway, or public trail should reach out and coordinate with Parks early in the concept design phase. List specific contact if possible.

F. Previous Land use Reviews

Following are land use review approvals for this site since 1981. (Conditions of approval from land use reviews applied for prior to 1981 would not apply per 33.700.110.A.)

- LU 16-191377 AD: 2016 Adjustment Review approval for a scoreboard. No relevant conditions of approval.
- LU 14-173012 ZE: 2014 Zoning Map Error Correction approval. No conditions of approval.
- LU 10-115344 CU: 2010 Conditional Use Review approval for a building addition and to increase enrollment at Rieke Elementary School. No relevant conditions of approval.
- LU 09-111701 CU: 2009 Conditional Use Review approval for a portable classroom building and to increase enrollment at Rieke Elementary School. No relevant conditions of approval.
- LU 07-119231 CU AD: 2007 Conditional Use Review and Adjustment Review approval for a portable classroom building at Rieke Elementary School. No relevant conditions of approval.
- LUR 96-00933 CU: 1997 Conditional Use Review approval for expansion of the swimming pool and for a new concession stand. No relevant conditions of approval.
- LUR 92-00168 CU: 1992 Conditional Use Review approval for a concessions building and a batting cage. No relevant conditions of approval.
- CU 111-86: 1986 Conditional Use Review approval for filming booths at the Wilson High School stadium. No conditions of approval.
- CU 074-86: 1986 Conditional Use Review approval for storage and an announcing booth at the Woodrow Wilson High School stadium. No relevant conditions of approval.
- CU 049-85: 1985 Conditional Use Review approval for a sign. No relevant conditions of approval.

G. Zoning Code and Tree Code Questions from Your Letter

1. *The IBW and Rieke Elementary sites share parking area with ±368 total parking stalls between the two schools. How will the sites be evaluated in terms of showing compliance with parking maximums and adequacy of parking in the CU approval criteria.*

Response: The entire, contiguous school district property is considered a single site (33.910, definitions of “site” and “ownership”). The maximum parking for this site is established through the land use review (Table 266-1, footnote 2). Please refer to the Portland Bureau of Transportation memo for information about establishing adequacy of parking for the approval criteria.

2. *The entire site is located in a pedestrian district. Table 150-2 indicates that the IR zoned portions of the site have a 10 foot maximum setback, but there are no standards in Section 33.150.215 that describe if or how this standard would be applied to a through lot with both streets the same classification. (Note that 33.150.215.C only applies to sites zoned CI2). Additionally, will the project also need to meet the maximum setback standard from Bertha Court west of Rieke Elementary?*

Response: The 10-foot maximum building setback in Table 150-2 applies to both of the streets which abut the IR-zoned portion of this site (SW Vermont St. and SW Burlingame Ave.) For this requirement to be met outright, the south façade of any new building must be within 10 feet of the SW Vermont St. lot line and the north façade must be within 10 feet of the SW Burlingame Ave. lot line. Since that is not practical in this case, you will need to request an exception to this standard. This can be an Adjustment request alongside your Conditional Use Review or, if you also apply for Design Review, it can be a Modification request with the Design Review. The maximum setback standard in Table 150-2 does not apply to SW Bertha Ct. because Table 150-2 only applies in the

IR-zoned portion of the site (33.700.070.F), and SW Bertha Ct. does not abut this part of the site. SW Bertha Ct. abuts the OS-zoned portion of the site, and while a maximum setback would apply to Conditional Use development in the OS zone, your proposal does not include any new building area within the OS zone.

3. *Table 150-2 indicates that ground floor windows standards apply in the IR zone, but Section 33.150.250 only provides standards for the CI2 zone. Do the ground floor window standards apply to this site?*

Response: With the ambiguity in the current code, BDS is not currently applying the ground floor windows standard to development in the IR zone. However, a package of code amendments (RICAP 10) expected to become effective in October 2024 will clarify that the ground floor windows requirement applies in the IR zone. See <https://www.portland.gov/bps/planning/ricap10>. The zoning regulations applicable to your land use application will be those in effect at the time of submittal, as long as the application is made complete within 180 days (33.700.080.A).

4. *Please describe any advantages to getting approval either a Conditional Use Master Plan or an Impact Mitigation Plan to have the site recognized as an “Institutional Campus” vs just getting approval of a Conditional Use? As an institutional campus is it possible to write our own design standards for the site, eliminating the need for future design reviews (see Section 33.420.050.B.4).*

Response: For a standard Conditional Use Review, all building permits for the approved work must be issued within 3 years of the Conditional Use Review approval (33.730.130.B.1). The Housing Regulatory Relief Project (<https://www.portland.gov/bps/planning/housing-regulatory-relief/documents>) will extend this deadline from 3 years to 5 years for all projects if adopted as proposed in early 2024. The RICAP 10 project mentioned in #3, above, includes a recommended provision to allow 2-year extensions to the 5-year period through a Type Ix land use review. (Your application would need to be submitted after new regulations take effect in order to be subject to them.) Conditional Use Master Plans and Impact Mitigation Plans are generally intended for master plans that involve multiple phases of development, and while the application fees for these reviews are higher than for a standard Conditional Use Review (see <https://www.portland.gov/bds/documents/land-use-services-fee-schedule-city-portland-effective-july-1-2023/download>), they can remain in effect for longer than a standard Conditional Use Review. A Conditional Use Master Plan can remain valid for up to 10 years (33.820.060) and an Impact Mitigation Plan can remain in effect until all approved work is completed (33.848.060). Obtaining a Conditional Use Master Plan or Impact Mitigation Plan would not allow you to propose alternative design standards for the site. 33.420.050.B.4 refers to when the design standards in 33.420.050.C may not be used as an alternative to Design Review, but this code section only applies in residential zones. Though residential use is allowed in the IR zone, the IR zone is classified as a campus institutional zone rather than a residential zone (33.150.020).

5. *If the site is not recognized as an “Institutional Campus” though a CUMS or IMP do the standards of 33.150.267.C.3. apply to this site at all? And if so, please describe how 33.120.240 – Required Outdoor Areas – gets applied to this site since this is a residential standard applied to a non-residential site?*

Response: If there is no Conditional Use Master Plan or Impact Mitigation Plan, the site would not be considered an “institutional campus” for purposes of 33.150.267.C, so these requirements would not apply. If the site was made an “institutional campus” through a Conditional Use Master

Plan or Impact Mitigation Plan, the requirement in 33.120.240 would still not apply because this requirement is only triggered for dwelling units, and no dwelling units are proposed.

6. *Are the outdoor tennis courts subject to the standards of Chapter 33.279 – Recreational Fields for Organized Sports?*

Response: Tennis courts are not subject to the standards in 33.279. Per 33.279.020.A, these standards only apply to fields used for “organized sports.” The definition of “organized sports” in 33.910 specifically excludes hard-surfaced courts.

7. *How will the standards for the Major Public Trail be applied to this site, considering that only a small part of the starred zoning line is located on the IBW tax lots and the vast majority of the line is on the Rieke Elementary tax lot? If the trail requirement is triggered with this proposal can the City elaborate on the requirements for the trail (i.e. flexibility of location through the site, width and surfacing requirements, are the hours it must be open to the public (5am – 10 pm) flexible?*

Response: Based on the requirements in 33.272 and the relevant administrative rule (https://www.portland.gov/sites/default/files/2021/permanent-rule-determination-of-rough-proportionality-for-major-public-trail-requirements_final_signed_071018.pdf), construction of a publicly accessible trail through this site will be required. For purposes of this requirement, the entire property including the Rieke Elementary School campus is treated as a single site. Please continue to discuss the trail requirements and hours with Marty Stockton at Portland Parks and Recreation (marty.stockton@portlandoregon.gov or 503-201-7199).

8. *What level of review (Type II, Type III) is anticipated for the Conditional Use Review and the Design Review?*

Response: The Conditional Use Review would be a Type III land use review (33.281.050.C). A Design Review for a new building larger than 80,000 square feet would also be a Type III land use review (Table 825-1).

9. *Based on the need to keep the existing school building in place during construction of the new building, it is likely that a significant majority of the trees on the site will need to be removed. Please discuss how mitigation for the removal of the trees will be calculated in terms of cost and planning of additional trees?*

Response: Under 11.50.040.C.1, a mitigation payment to the Tree Planting and Preservation Fund is required to remove more than 2/3 of non-exempt trees between 12 and 20 inches in trunk diameter and to remove any non-exempt trees 20 inches or larger in trunk diameter. You can choose to base tree preservation standards on either the whole site or a smaller development impact area (11.50.030). Under the current fee schedule (<https://www.portland.gov/trees/trees-development/documents/fy-24-fee-schedule-urban-forestry/download>), the mitigation payment to remove more than 2/3 of non-exempt trees between 12 and 20 inches in diameter is \$1,800 for each additional tree removed. The mitigation payment to remove non-exempt trees 20 inches or larger is \$450 per inch (for example, \$9,000 for a 20-inch tree). The mitigation payment would be collected with the building permit fees due at building permit issuance.

10. *The current Title 11 code language states “The regulations for Private Trees in Subsection 11.50.040 C.1. sunset after December 31, 2024. After December 31, 2024, the regulations*

in effect will be those in effect on January 1, 2015.” If the land use review was submitted in 2024 and the building permit was submitted in 2025 or 2026 which standards would be applied to the site? Could the city please provide a copy of the 2015 standards that will go into effect in 2025?

Response: In 2024, the City intends to seek approval of an ordinance that will amend this section to keep current standards applicable after December 31, 2024. However, this is the language of 11.50.040.C.1 that applied on January 1, 2015:

C. Tree Preservation Requirement. Any trees preserved shall be protected in accordance with the specifications in Section 11.60.030.

1. Private Trees.

- a. Retention. An applicant shall preserve and protect at least 1/3 of the trees 12 inches and larger in diameter located completely or partially on the development site. Retaining trees at least 6 and less than 12 inches in diameter that are documented in a report prepared by an arborist or landscape professional to be Garry Oak (*Quercus garryana*), Pacific Madrone (*Arbutus menziesii*), Pacific Yew (*Taxus brevifolia*), Ponderosa Pine (*Pinus ponderosa*), or Western Flowering Dogwood (*Cornus nuttallii*) species are not included in the total count of trees on the site but may be used toward meeting the preservation standard.
- b. Mitigation. For each tree removed below the 1/3 requirement, payment to the Tree Planting and Preservation Fund is required equivalent to the cost of two trees. See Section 11.15.010.

H. Neighborhood Notification

When you apply for a Type III land use review, all neighbors within 400 feet and all neighborhood associations and recognized organizations within 1,000 feet of your site will receive notification of your proposal. Contact information for neighborhood associations, neighborhood district coalitions, and business associations is available at <https://www.portlandoregon.gov/civic/search/>.

Design Guidelines Matrix

CITYWIDE DESIGN GUIDELINES (2021)		PROJECT NAME	CASE NUMBER
DATE		PROJECT ARCHITECT	PROJECT VALUE \$
CONTEXT	COMMENTS		
01: Build on the character, local identity, and aspiration of the place. Possible design approaches may include, but are not limited to: - Community - Architecture - Nature			
02: Create Positive Relationship with surroundings. Possible design approaches may include, but are not limited to: - Building Massing - Street Wall - Connectivity - Adjacent Historic Landmarks			
03: Integrate and enhance on-site features and opportunities to contribute to a location's uniqueness. Possible design approaches may include, but are not limited to: - Natural Resources - Viewpoints - On-site Older Buildings and Historic Resources - Social and Cultural Significance			
PUBLIC REALM	COMMENTS		
04: Design the sidewalk level of buildings to be active and human-scaled. Possible design approaches may include, but are not limited to: - Ground Floor Heights - Multiple Entries and Windows - Weather Protection - Lighting - Residential Setbacks			
05: Provide opportunities to pause, sit, and interact. Possible design approaches may include, but are not limited to: - Seating - Integrate Bicycles - Art/Water - Enclosure - Trees and Landscaping			
06: Integrate and minimize the impact of parking and building services. Possible design approaches may include, but are not limited to: - Vehicles and Parking - Utilities, Trash and Recycling - Vaults - Stormwater Planters - Long-Term Bicycle Parking			
QUALITY & PERMANENCE	COMMENTS		
07: Support the comfort, safety, and dignity of residents, workers, and visitors through thoughtful site and building design. Possible design approaches may include, but are not limited to: - Internal Open Spaces - Internal Connections - Vehicles Areas - Solar Access - Windows and Entries			

<p>08: Design for quality, using enduring materials and strategies with a coherent approach. Possible design approaches may include, but are not limited to:</p> <ul style="list-style-type: none">- Unity- Articulation- Application of Exterior Materials- Quality of Materials- Building Openings	
<p>09: Design for resilience, health, and stewardship of the environment, ensuring adaptability to climate change and the evolving needs of the city. Possible design approaches may include, but are not limited to:</p> <ul style="list-style-type: none">- Adaptable Buildings- Resource Conservation- Native Landscaping- Ecoroofs- Bird-Safe- Daylight and Air- On-Site Stormwater	



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PBOT – Development Review

Pre-Application Conference Response

Date: December 22, 2023
To: Matt Wickstrom, Conference Facilitator
503-865-6513, Matt.Wickstrom@portlandoregon.gov
From: Tammy Boren-King, PBOT Development Review
503-823-2948, Tammy.Boren-King@portlandoregon.gov
Jacob Wiser, PBOT Permit Engineering
Jacob.wiser@portlandoregon.gov
Case File: EA 23-105044
Location: 1151 SW VERMONT ST
R#: R328555, R328556, R328614, R328618, R328614, R328614
Proposal: A Pre-Application Conference to discuss redevelopment of the existing high school site with a new 3-4 story building to accommodate 1,700 students. Redevelopment of the site will also include practice fields, parking lots and a potential swimming pool. The school will offer a full range of high school facilities, including a theater, band and choir rooms, a cafeteria and commons, and a media center as well as classrooms and science labs, art rooms, shop spaces and a variety of specialized learning environments. The school may also accommodate a small childcare center and a health clinic.

materials to identify potential issues and requirements. The response is presented in the following order:

- A. Key Issues and Submittal Requirements**
- B. PBOT Permit Engineering Review Comments**
- C. Responses to Applicant Questions**
- D. Approval Criteria**
- E. Required Public Improvements, Dedication, and Driveways**
- F. Public Works Permit Process**
- G. Building Permit Information**
- H. Appeals (Public Works Alternative Review Information)**

A. Key Issues and Requirements

1. The project will require a conditional use review. The transportation related zoning code criteria are found in 33.815.105.D. A transportation impact study (TIS) prepared by an Oregon licensed traffic engineer is required to address these criteria. Please see the attachment at the end of this document for additional information about the TIS scoping process. Please have the project's traffic engineer submit the scope for approval by PBOT prior to conducting the data collection. What data will need to be collected will be determined during the scoping process. Should the traffic consultant have questions, please have them reach out to me or PBOTDevRevTrafficScopes@portlandoregon.gov.

Please note the Housing Regulatory Relief code update project is projected to be heard at City Council in December and put into effect in early 2024.

<https://www.portland.gov/bps/planning/housing-regulatory-relief>. This project will remove on-street parking impacts from the approval criteria in 33.815.105.D.2.a. If the application is submitted after on-street parking impacts are removed from the approval criteria, PBOT will not require the applicant to document on street parking availability. A discussion of vehicle parking is still needed as part of the application since the amount of parking to allow is a discretionary decision per Per Table 266-1, footnote 1. Uses subject to a conditional use review may establish parking requirements through the review. The amount of vehicle parking needed to serve the various uses on the site will need to be documented.

2. The following information is typically included in the necessary TIS for this type of Land Use review:

- Provisions for pedestrian and bicycle access to and through the site as well as an estimate of pedestrian/bicycle demand for the users of the site. Given the trail requirement through the site, it is anticipated this will have a strong impact on the review.

- The daily, weekly, and monthly schedule capturing school operations, public pool operations, special events, and community uses of the site. This should discuss the day to day school operations as well as the times when multiple site users are overlapping- such as when athletics happen on the same day as the farmer's market.

- Vehicular pick-up/drop off operations. A detailed description of how both school buses and private vehicles pick up and drop off students will be needed. This should discuss both schools on the site and whether their arrival and departure times overlap as well as athletics. It is preferable for there to be diagrams as well for the on-site circulation involved.

- Parking demand. The amount of on-site required parking for a Conditional Use is determined during the Conditional Use review. Traditionally this has involved some form of parking study in the public right-of-way (ROW) in addition to a detailed description of the operation of any on-site parking. As noted above, on street parking impacts is being removed from the approval criteria so PBOT will not be requiring the applicant to address this topic with an on-street parking count. The applicant is encouraged to provide information on the anticipated amount of parking demand that will be generated and how much (if any) of that demand will not be accommodated on site. Please be sure to provide an analysis of activities beyond the school day such as athletics and community activities.

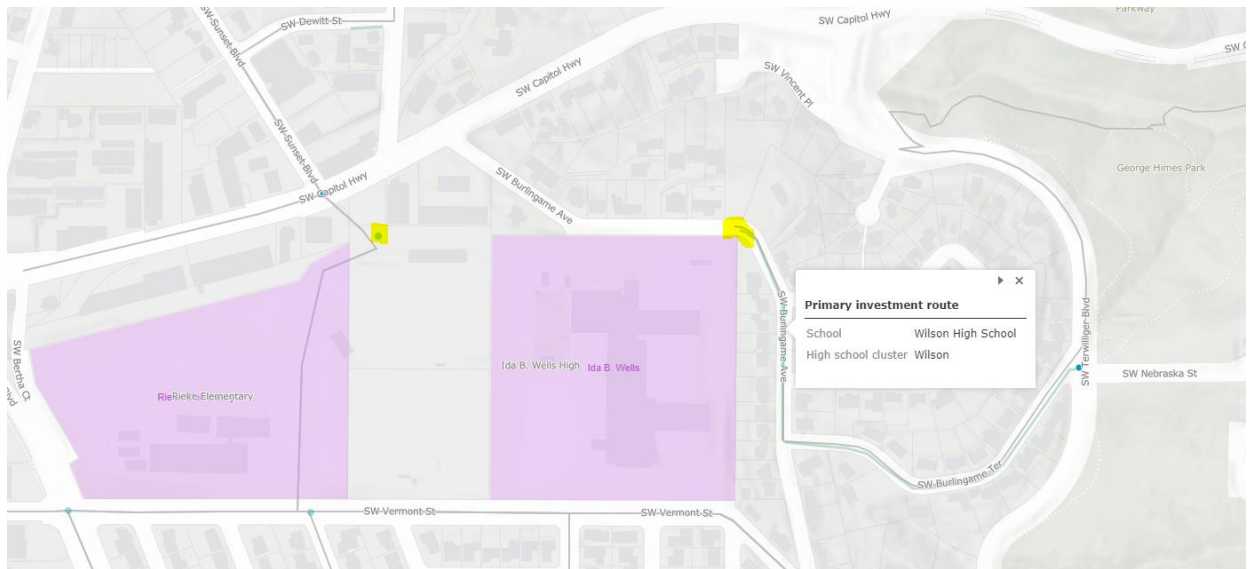
-It is typically also appropriate to study area intersections for level of service issues. Which intersections to study is decided as part of the TIS scoping process. At least some of this analysis will need to be informed by whether or not the school rebuild is anticipated to be an increase in trips over the existing school.

The final TIS should clearly, directly and adequately address all applicable transportation-related approval criteria and each evaluation factor associated with the subject Conditional Use review in 33.518.105.D.

3. One evaluation factor referenced above is related to connectivity. PCC 17.88.040 Through Streets establishes the City's spacing standards. Street connections should be no further apart than 530-ft. When full streets are not possible, pedestrian/bicycle connections should be no further than 330-ft apart. Connecting existing streets is encouraged. In this area, public through connections far exceed the spacing standard.

With that said, it is not typical for public rights-of-way to be extended through institutional uses, especially not through the site of a public elementary school. While PBOT may not be seeking a through street or pedestrian connection in right-of-way, the applicant should still be prepared to explain how the project will increase connectivity through the site to the surrounding rights-of-way and comply with the trail standard. This analysis should specifically state how users of the site will be able to access the site from the surrounding neighborhood. Information regarding on site improvements to pedestrian access should be included in any application for a public works alternative review.

4. The connectivity factors for this land use review should take into account how students access the site. While dedicated rights-of-way may not be needed through the site, pedestrian improvements are needed. The City's Safe Routes To School program has a project map available at <https://pdx.maps.arcgis.com/apps/MapSeries/index.html?appid=3e902fe9aa7f49eaaf57f9ce562ade57>. Please note that removal of the parking area from the north edge of the site will remove an important connection to SW Burlingame Ave. The proposed redevelopment should retain an on-site pedestrian connection to SW Burlingame Ave, which is the primary investment route to access the school site on foot from SW Terwilliger Blvd. The on-site pedestrian crossing at the north edge of the site where multiple driveways converge is also a high priority for improvement.



5. Two on-site loading spaces meeting the Type A standard are required (33.266.310.C.2.c). It is very unlikely this standard will be modified given the large amounts of vehicle area proposed.
6. Bicycle parking is required. While the submitted materials did not include a specific request to adjust the required amount of bicycle parking, all of the high school rebuilds processed to date have sought a bicycle parking adjustment. These types of adjustments are processed in consultation with PBOT's active transportation section, who provided a substantial response regarding submittal requirements. Please see the attachment at the end of this document.
7. Both of the submitted site plans show changing the parking area access from SW Vermont St. For the Gather scheme, it is preferred that the driveway for this parking area align with one of the T-intersections within the site frontage or be mid-block between the T-intersections if possible.
8. There are two triggers in Title 17 for when public ROW improvements are required. If a project is a "significant alteration" as defined in 17.88.010.C, then frontage improvements are required within the existing right-of-way. "Significant Alteration" is defined as changes to property that are 35 percent or greater than the assessed value of all improvements on the site. Mandatory improvements for fire, life safety, and accessibility do not count toward the threshold." The second trigger is an increase in trips. Per 17.88.020, alterations which increase the number of occupants of a site are required to provide a standard full width improvement, including sidewalks, and may require dedication to allow the full width improvement to be constructed. For PBOT purposes, occupancy is determined based on trips rates from the *Institute of Transportation Engineers Trips Generation Manual, 11th Edition*. Standard improvements are based on the requirements of [PBOT Development Review Manual to Creating Public Streets and Connections](#) and the [Portland Pedestrian Design Guide](#).

For the purpose of these notes, staff has provided full information in case the project will result in an increase in trips to the site. The review of the TIS based on the final design will determine if the proposal is an increase in trips to the site or not. If there is no increase in

trips, dedication is not required but improvements are required with the existing right-of-way. If there is an increase in trips, dedication and improvements are required. In either scenario, staff anticipates the applicant will wish to pursue a public works alternative review to modify the public improvement requirements.

The information in Section E goes over standard improvements for this site in more detail. They are substantial. The TIS data can be used to help craft an argument about the impact of the proposed development. Please feel free to continue to coordinate with me in advance of the submittal of the alternative review. While it seems probable that some relief to full standard improvements is likely, it also seems probable that PBOT would not be able to support a full waiver of all public improvements. The applicant should anticipate at least some public improvements and the need for a public works permit. Any public works alternative review to alter standard improvements must be approved prior to PBOT being able to support the land use review. Please feel free to call me to discuss this if there are any questions. 503-823-2948.

9. Standard Improvements:

SW Vermont St. has an overall sidewalk corridor width of 20-ft which exceeds the requirement for a 12-ft sidewalk corridor. The paved sidewalk is only 5-ft wide, while current code requires 6-ft. The standard is to reconstruct with a configuration of 0.5-ft curb, 12-ft furnishing zone, 6-ft sidewalk, and 1.5-ft frontage zone. Dual ADA ramps are required at the corner with SW Bertha Blvd. as well as ADA compliant receiving ramps at the following T-Intersections:

SW 10th Ave, SW 11th Dr, SW Chestnut Dr (which has a marked cross walk), SW 12th Ave, SW 13th Ave. (which has a marked cross walk), SW 14th Ave, SW 15th Ave.

Any existing ramps which meet current standards can be retained. Ramp reports are needed to determine if a ramp meets current standards.

SW Bertha Blvd. is mapped as having 30-ft of right-of-way from face of curb to property line. **SW Bertha Ct.** is mapped as having a variable width right-of-way but mostly 28-ft from face of curb to property line. Both segments are mapped with a 7.5-ft curb tight sidewalk. The standard is for a 15-ft corridor on SW Bertha Blvd. and a 12-ft corridor on SW Bertha Ct. While adequate right-of-way exists, substantial grading and retaining would be needed to achieve the standard.

SW Burlingame Ave. is mapped as paved street without curbs or sidewalks. Standard improvements include a minimum of 20-ft paved roadway with a 0.5-ft curb set 14-ft from centerline (footnote of Table 1 of TRN 1.09), 8-ft public stormwater facility, 6-ft sidewalk, and 1.5-ft frontage zone. It is estimated this will require 5-feet of dedication.

10. The anticipated improvements will trigger a Public Works Permit. A chart describing which situations qualify for Minor Improvement Permits and which situations require Public Works Permits is available at <https://www.portland.gov/publicworks/sidewalk-frontage->

[improvement-requirements.](#)

11. Since SW Burlingame Ave. does not have a curb. This means improvements in this section will trigger public stormwater management, which can deeply affect the improvement design and may result in the need for property dedication. If standard improvements are needed, then concept approval of a public works permit is needed to meet the conditional use review criteria.

This means one of two things must happen:

-Staff have to require concept approval of a public works permit prior to land use approval

OR

- The applicant must secure a public works alternative review to alter the improvement requirements to the degree that both BES and PBOT agree the conditional use approval criteria can be met without the need for concept approval of a public works permit.

Summary of Submittal Requirements

1. Narrative addressing all zoning code approval criteria
2. Plans showing required dedication and improvements
3. Transportation Impact Study. Please see Attachment A
4. Public Works Alternative review if seeking modifications to the public improvement requirements

B. PBOT Permit Engineering Review Comments

PBOT Public Works Permit review comments were not yet available at time of this response. When available, they will be forwarded via separate cover. The applicant is encouraged to contact the Public Works Project Manager for any questions on the Public Works submittal requirements. General information on the Public Works process can be found here: <https://www.portlandoregon.gov/index.cfm?&c=53147>.

The contact for PBOT specific questions about engineering standards for this project is Jacob Wiser, PBOT Permit Engineering Jacob.wiser@portlandoregon.gov

C. Responses to Questions Submitted by the Applicant

1. **The IBW and Rieke Elementary sites share parking area with ±368 total parking stalls between the two schools. How will the sites be evaluated in terms of showing compliance with parking maximums and adequacy of parking in the CU approval criteria.** The applicant's traffic consultant will need to propose a scope as

part of the TIS. As noted in Section A, there are code changes which will affect the approval criteria depending on the timing of when the application is submitted. If more parking is requested than currently exists on the site, this will need to be justified with parking generation data. That data should take into account all of the users of the site.

- 2. How will the standards for the Major Public Trail be applied to this site, considering that only a small part of the starred zoning line is located on the IBW tax lots and the vast majority of the line is on the Rieke Elementary tax lot? If the trail requirement is triggered with this proposal can the City elaborate on the requirements for the trail (i.e. flexibility of location through the site, width a**

PBOT staff note the on-site trail improvements are not within PBOT's jurisdiction. PBOT staff do have substantial expertise in reviewing pedestrian/vehicle crossings and would gladly assist in reviewing any designs for an improved cross walk at the north end of the major public trail. This crossing has been identified as a beneficial improvement by PBOT's Safe Routes to School program. Additionally, the on-site pedestrian connection to SW Burlingame Ave. is part of the Safe Routes to School primary investment route for both Ida B. Wells and Rieke Elementary. PBOT has a high interest in how the on site improvement will continue to connect to SW Burlingame Ave.

- 3. Currently school bus pick-up and drop-off occurs in the public right-of-way of SW Vermont Street. Will this be allowed to continue with the redevelopment?**

On-street parking signage is controlled by PBOT Parking Control, who are not typically part of land use reviews. There is no specific plan to remove the on-street bus loading. Land use staff assume this will continue. Staff notes Roosevelt, Franklin, Grant, Lincoln, and Benson were all approved to retain on street loading. When McDaniel was rebuilt, 82nd Ave was a state highway under ODOT's control, so on-street parking was not a PBOT decision at that time.

- 4. Will a Transportation Analysis will be required for the conditional use review? Will ODOT be reviewing this also because SW Capital Highway is also State Route 10?**

A transportation analysis is required as described in Section A. Please see the ODOT response for their determination on whether or not they wish to be involved in the review. Based on their comments in the meeting, they will only need to be involved if the school rebuild will result in an increase in trips to the site.

D. Approval Criteria and Land Use Submittal Requirements

The applicant shall submit a written narrative adequately addressing the applicable zoning code approval criteria listed below for the required reviews:

Title 33 Approval Criteria	Code Citation and Link	Staff Notes
Conditional Use	33.815.105	Transportation Impact Study required. See Attachment A

Title 33 Approval Criteria	Code Citation and Link	Staff Notes
33.266 Parking, Loading, And Transportation And Parking Demand Management	33.266	Includes vehicle parking, bicycle parking, loading, and transportation demand management standards.

Additional City Code Requirements	Code Citation and Link	Staff Notes
Creating Public Streets and Pedestrian Connections through the Land Use and Building Permit Process.	TRN-1.09 - Design Standards for Public Streets Portland.gov	Street width and sidewalk configuration standards
Pedestrian Design Guide (2022)	TRN 1.092	Standards for pedestrian facilities including sidewalk corridors.
Traffic Capacity Analysis	TRN 10.27	Describes requirements related to Transportation Impact Studies and required mitigation measures
Driveway Standards	17.28.110 and TRN 10.40	Standards are applied at the time of building permit
Street Lighting	https://www.portlandoregon.gov/transportation/article/714407	PBOT Lighting Level Guidelines
Central Location for PBOT Design Standards	https://www.portland.gov/transportation/development/commonly-referenced-transportation-code-and-administrative-rules	Links to multiple PBOT design guidelines here.

E. Required Public Improvements, Dedication, and Driveways

TSP Classifications: At this location, the City's Transportation System Plan (TSP) classifies the abutting street(s) as follows:

The site IS within a Pedestrian District.

Street Name	Pedestrian	Bicycle	Transit	Freight	Traffic	Design
SW Bertha Blvd	Major City Walkway	City Bikeway	Transit Access Street	Major Truck Street	District Collector	Neighborhood Main Street
SW Bertha Ct.	Neighborhood Walkway	City Bikeway	Transit Access Street	Local Service	Local Service	Local Street
SW Vermont St.	Neighborhood Walkway	City Bikeway	Local Service	Local Service	Local Service	Local Street
SW Burlingame Ave	Local Street	City Bikeway	Local Service	Local Service	Local Service	Local Street

Existing Improvements: At this location, according to the City's GIS data, the site's abutting rights-of-way are improved as follows:

Street Name	ROW Width*	Roadway Width*/Condition	Pedestrian Corridor Width*/Configuration			
			Curb	Furnishing	Sidewalk	Frontage
SW Bertha Blvd	Varies	Variable width pavement	0.5-ft	--	7.5-ft	Varies. Mostly more than 20-ft
SW Bertha Ct	Varies	31-ft paved	0.5-ft	--	7.5-ft	Varies. Mostly more than 20-ft
SW Vermont St.	80-ft	40-ft paved	0.5-ft	12.5-ft	5-ft	2-ft
SW Burlingame Ave	50-ft	18-ft paved	Unimproved			

** The applicant is advised the information contained herein is derived from City GIS and other databases typically used by city staff, as well as information from the applicant. It is the applicant's responsibility to provide a current survey to document the location of the abutting rights-of-way and to confirm or challenge any anticipated dedication amount.*

Standard ROW Improvements:

Street Name	Curb Location	Pedestrian Corridor Width	Pedestrian Corridor Configuration				Estimated Dedication
			Curb	Furnishing/ Stormwater	Sidewalk	Frontage	
SW Bertha Blvd	No change	15-ft	0.5-ft	4-ft	8-ft	2.5-ft	None
SW Bertha Ct	No change	12-ft with excess allocated per Figure B-9	0.5-ft	4-ft	6-ft	1.5-ft	None
SW Vermont St.	No change	12-ft with excess allocated per Figure B-9	0.5-ft	12-ft furnishing zone	6-ft	1.5-ft	None
SW Burlingame Ave	14-ft from C/L	16-ft	0.5-ft	8-ft storm-water	6-ft	1.5-ft	5-ft

The estimated dedication amount is based on the information available to City Staff. A site specific survey is necessary to determine the final dedication amount, which will be determined via the Public Works Permit process.

Excess Pedestrian Corridor Width Allocation: The amount of right-of-way available behind the curb exceeds the required pedestrian corridor width on SW Bertha Blvd, SW Bertha Ct, and SW Vermont St. SW Bertha Blvd. and SW Bertha Ct. have slope issues that will affect the site's ability to provide standard improvements. SW Vermont St. has the excess allocated differently than current standards required. Staff recommends the applicant pursue a public works alternative review for these frontages.

Furnishing zone treatments are set by Table B-4 and Section B.2.2 of the Pedestrian Design Guide (2022). For all frontages, a landscaped furnishing zone is required except where bus loading will happen. A hardscaped furnishing zone is allowed where bus loading will happen.

ADA Compliant Corner or Mid-Block Ramps: The Americans with Disabilities Act (ADA) requires projects which construct a new pedestrian facility (e.g. a new sidewalk or ped-push button), resurface a portion of the street, or otherwise alter the street to provide ADA compliant curb ramps along the route as part of the project. The corner is required to be reconstructed to meet current City and Federal ADA standards. Information regarding ADA curb ramp design and construction criteria can be found at <https://www.portlandoregon.gov/transportation/article/727351>. A public works permit will be triggered.

Curb Extension: None required.

Street Trees: The applicant is advised to contact the City's Urban Forester at 503-823-TREE (8733) for specific information on the quantity and type of street trees that will be required. For

additional information on street tree requirements, the applicant is advised to visit <https://www.portland.gov/trees/trees-development/documents/street-tree-requirements-public-works-permits/download>.

Street Lighting: The following comments were provided by Adam Moore, PE PBOT Signals and Street Lighting:

- If the pedestrian ramps at Bertha & Vermont are rebuilt, new pedestrian poles at ADA-compliant locations will be required. This is pretty unlikely since, like you mentioned, that intersection was just rebuilt (unsure how we missed the ped poles in that rebuild).
- Additional lighting may be needed at the midblock pedestrian crossings on Vermont. These would be wood cobrahead poles.
- The TIS should assess if there will be an increase in traffic at Capitol Hwy & Sunset Blvd. If so, there may be a need to determine if a protected northbound (and southbound) left turn should be added to that signal.
- Lighting on Burlingame is a rather nonstandard post top pole arrangement. Nothing will be required on this frontage unless the project substantially modifies traffic patterns on this street, like if there's a whole new high volume school entrance accessed from Burlingame. In that case, additional lighting may be needed.

Driveways: In accordance with TRN 10.40.D.1, no portion of a driveway shall be located closer than 25-ft from the corner of a lot where two streets intersect.

F. Public Works Permit Process

The Right-of-Way improvements will need to be designed by an Oregon licensed civil engineer and constructed under a Public Works Permit, which is separate from the Building Permit that will be necessary for construction of the proposed project.

Concept Development Phase plans must be submitted to Public Works Permitting in order to verify the type of PW Permit that is required and to determine the required performance guarantee amount.

PW Design Review will determine specific design elements including stormwater management, bus stop, curb-cuts, landscaping, location of signage, location of utility poles and street lights, as well as other design requirements.

The applicant is therefore encouraged to contact Public Works at (503) 823-1987 or at pwp@portlandoregon.gov to become familiar with the process and initiate the appropriate meetings/process. Additional information on the City's Public Works Permitting process can be found at the following link: <http://www.portlandoregon.gov/publicworks>. It is important for the applicant to understand the Public Works process and timeline to avoid any conflicts with the Land Use Review and Building Permit process.

Concept Development approval (30% design) including plans, fees, signed application for permit, and a performance guarantee for the estimated value of the improvement must be submitted prior to Building Permit approval. The performance guarantee may be in the form of

a surety bond, irrevocable letter of credit, set-aside account, or cash deposit. Applicant should contact Public Works at (503) 823-1987 for appropriate forms and additional information.

G. Building Permit Information

At the time of building permit review you should be aware of the following:

1. When proposed development will prohibit use of an area of within the public right-of-way, a separate Temporary Street Closure permit will be required. Additionally, closures that do not allow safe passage and unobstructed flow of normal public use in a partially open area or lane, will also require a City approved Traffic Control Plan. For information on obtaining a separate Temporary Street Closure permit, please contact:
<https://www.portland.gov/transportation/permitting/temporary-street-use-permitting-tsup> (503-823-7611).
2. System Development Charges (SDCs) may be assessed for this development. To receive an estimate of the SDC amount, the applicant is advised to leave a voicemail message to include the case file number, at (503) 823-7002, Option 2. Additional information about PBOT SDCs can be found at: <https://www.portland.gov/transportation/permitting/tsdc>.

H. Appeals (Public Works Alternative Review)

Per TRN 1.27, an applicant may request the Public Works Alternative Review Committee (ARC) consider alternatives to the requirements identified in this document. The applicant is encouraged to provide sufficient detail in the request to enable the Committee to make a qualified decision. The Committee may seek additional information from the applicant, provide advice to the applicant, seek additional information from other City Bureaus, or complete additional reviews before a final decision is rendered. Any proposed alternative frontage improvements must be approved through the alternative review process prior to PBOT making a recommendation on an associated land use review or building permit.

PWA decisions are valid for one year from the date of approval and are the first step in the Public Works Appeal Process. Additional information on the Public Works Alternative Review Process, fees, and application materials can be found at <https://www.portlandoregon.gov/article/481371>.

Transportation Impact Study Early Assistance Attachment A

A Transportation Impact Study (TIS) is required to demonstrate the applicable land use review approval criteria are met. A TIS may also be needed to address operational or safety issues. Please note that many intersections in the City have limited capacity and may not be able to accommodate area growth plus the proposed land use without mitigation. Under some circumstances, lack of adequate capacity (level-of-service) at an intersection can result in denial of a land use proposal. Intersections of higher classified streets are most likely to have capacity constraints but other intersections may as well. The applicant's traffic engineer should investigate this issue well in advance of application for the land use review. Please see TRN 10.27- Traffic Capacity Analysis for Land Use Review Cases, available at <https://www.portlandoregon.gov/citycode/article/41049>.

The TIS must be prepared by an Oregon licensed traffic engineer. TIS's are complex and almost always require multiple engineering reviews to ensure that all issues are fully addressed in a technically acceptable manner. Please allow for adequate time in your project timeline. The more complex the situation, the longer the review is likely to take.

The scope of work for the required TIS must be approved by PBOT prior to preparation of the study. The applicant's traffic engineer must propose a draft scope of work via the scope approval forms available at <https://www.portland.gov/transportation/development/traffic-impact-studies>. The "Traffic Scope Approval Form" should always be completed as well the other appropriate scoping form or forms based on the project. The additional forms currently include separate scope forms for:

- Traffic Impact Study Parameters
- Parking Impact Study,
- Loading Demand Study
- Transportation and Demand Management Plan
- Accessory Short Term Rental Traffic and Parking Study

Completed Scope Approval forms and any supporting documentation are to be emailed to PBOT at PBOTDevRevTrafficScopes@portlandoregon.gov. Review of scoping documents generally takes 3 weeks, though this can vary. Please direct additional questions regarding the required TIS or the Scope Approval Forms to PBOTDevRevTrafficScopes@portlandoregon.gov.

Timeline for land use or building permit reviews:

PBOT recommends the TIS be submitted at the time of land use application. The completed TIS must be submitted prior to PBOT considering the land use

application complete. The TIS must be reviewed and accepted by PBOT prior to PBOT supporting approval of the land use review. If there is no land use decision required, then PBOT recommends submittal of the completed TIS with the initial building permit submittal. TIS are complex technical documents that take time to review and often require ongoing coordination between PBOT and the applicant's traffic engineer. Plan on a minimum of 4 weeks for the initial review. Additional time may be needed for revisions or additional data collection. The more complex the situation, the longer the review is likely to take.

Bicycle Parking Adjustment Guidance

This following information is requested if the applicant chooses to pursue an adjustment to bicycle parking standards:

1. Map of 'Distance from Student Home to Campus' maps from previous five years enrollment.
 - a. To protect privacy this type of map is typically provided as a dot density map with corresponding information that calls out total number of students living within:
 - i. 0-1 mile,
 - ii. 1-2 miles,
 - iii. 2-3 miles,
 - iv. 3-5 miles,
 - v. 5+ miles.
2. Comparable maps for three closest PPS Neighborhood High Schools
3. A prepared map of the site and anticipated placement of all long-term and short-term bicycle parking spaces, access routes, and bicycle rack model(s). Required covered bike spaces should be called out with detail on the type of cover.
 1. Reminder that per 33.266.210.D.1.(5) – *For schools, long-term bicycle parking must be placed where the closest space is within 100 feet of a main entrance.* (but we are supportive of spreading out near various entrances, as long as there is justification and these are entrances that are actually used by students)
4. Demonstration of how proposed secure spaces will meet security requirement 33.266.210.D.2.(2) (10% must meet that).



CITY OF PORTLAND ENVIRONMENTAL SERVICES



1120 SW Fifth Ave, Suite 613, Portland, Oregon 97204 ■ Mingus Mapps, Commissioner ■ Dawn Uchiyama, Director

Pre-Application Conference Response

Date: January 8, 2024

To: Matt Wickstrom, Conference Facilitator
503-865-6513, Matt.Wickstrom@portlandoregon.gov

From: Abigail Cermak, BES Systems Development
503-823-7577, Abigail.Cermak@portlandoregon.gov

Case File: EA 23-105044

Location: 1151 SW VERMONT ST

R#: R328555, R328556, R328614, R328618, R328614, R328614

Proposal: A Pre-Application Conference to discuss redevelopment of the existing high school site with a new 3-4 story building to accommodate 1,700 students. Redevelopment of the site will also include practice fields, parking lots and a potential swimming pool. The school will offer a full range of high school facilities, including a theater, band and choir rooms, a cafeteria and commons, and a media center as well as classrooms and science labs, art rooms, shop spaces and a variety of specialized learning environments. The school may also accommodate a small childcare center and a health clinic.

The Bureau of Environmental Services (BES) has reviewed the submitted materials to identify potential issues and requirements and provide the following comments. Some references to Portland City Code (PCC) are included below; the applicant may refer to the Auditor's Office [Online Charter and Code](#).

A. KEY ISSUES AND REQUIREMENTS

Following is a brief summary of issues and requirements that may impact your proposed project or are submittal requirements that will require time to prepare prior to submittal of the application.

1. The applicant must submit a site plan showing the existing sanitary sewer connection location(s) and stormwater disposal system(s) for the structures to remain on this site as well as all proposed sanitary connections and stormwater systems.
2. The applicant must submit a stormwater report, including the results of infiltration testing, with the land use application.
3. Applicable Source Control Manual requirements (such as trash and recycling areas, swimming pools, etc.) may trigger stormwater management, changes to site design, or new sanitary connections. Therefore, BES encourages the applicant to account for these requirements through the land use application.
4. Required public right-of-way improvements will trigger stormwater improvements, which must be reviewed through a Public Works Permit. Some of the area is eligible to pay an Offsite Stormwater Management Fee in lieu of building a stormwater facility.
5. A storm sewer extension in SW Burlingame Ave may be required depending on public stormwater infrastructure requirements and offsite stormwater discharge needs. Extensions must be reviewed through a Public Works Permit.

B. SANITARY SERVICE

1. *Sanitary Infrastructure:* According to available GIS data, the following sewer infrastructure is located in the vicinity of the project site:

- a. Public 8-inch to 12-inch concrete (CSP) combined sewer in SW Burlingame (BES as-built #2522 and #2097). BES models predict that the 8-inch section of this sewer may surcharge during large storm event.
 - 1) According to City records, the existing school has two (2) 6-inch diameter lateral connections to the maintenance hole (asset ID #ACM800) of the combined sewer in SW Burlingame Ave.
 - 2) Historic records also indicate there may be a sewer connection to the sanitary sewer in SW Capitol Hwy and through private property west of 6220 SW Capitol Hwy. However, these records were not clear and BES is unable to verify this connection.
 - 3) BES recommends determining the current sanitary connection location(s). An existing sanitary connection can be used if the connection is conforming, in good condition, and appropriately sized.
2. *Sanitary Line for a Single Structure Covering Multiple Lots:* Separate sanitary connections are required for each individual lot. However, when a single structure crosses a property line, BES does not typically require separate connections for those lots because separate ownership of the lots cannot be easily established. The applicant should confirm consistency of shared lines over lot lines with BDS plumbing review staff.
3. *Water Main and Sewer Main Locations:* According to available GIS data, three (3) water mains are located between the proposed development site and the combined sewer main in SW Burlingame St. Any new connection(s) to the sewer main will cross the water main and will require a [Water Utility Protection Plan](#). Sanitary laterals must meet required separation distances according to the Water Bureau; the applicant should contact the assigned Water Bureau reviewer or the Water Bureau general email (devrev@portlandoregon.gov) with questions related to required separation distances.
4. *Upsizing Existing Sanitary Lateral:* The portion of the building's lateral that is located in public right-of-way cannot be smaller than the pipe on-property (administrative rule [ENB-4.17](#) Section 12), and therefore may require upsizing, depending on the existing lateral diameter and/or the expected sanitary flow rates from the proposed development. Upsizing an existing sanitary lateral in the public right-of-way requires a sewer connection permit; see <https://www.portland.gov/bes/ur-uc-permit-guide> for guidance about sewer lateral permitting in the ROW, and Section 5.6 of the [Sewer and Drainage Facilities Design Manual](#) for sewer lateral design criteria.
5. *Connection Requirements:* Connections to the City sewer system must meet the standards of the City of Portland's [Sewer and Drainage Facilities Design Manual](#), [PCC 17.32.090](#), administrative rules [ENB-4.07](#) and [ENB-4.17](#), and all other relevant City codes and rules. Sanitary sewage from private property must be separately conveyed to the property line and connected through individual laterals for discharge to the City separate sanitary or combined sewer. Per [ENB-4.07](#), sewer connection permits are required to make new connections to City mains and laterals, relocate or upsize existing laterals, and repair sewers in City right-of-way. The permittee is responsible for verifying the location, depth and size of an existing sewer lateral and for ensuring the lateral is clear of obstructions prior to connection.

C. STORMWATER MANAGEMENT

1. *Stormwater Infrastructure:* According to available GIS data, the following stormwater infrastructure is located in the vicinity of the project site:
 - a. Public 8-inch to 15-inch concrete (CSP) storm sewer in SW Vermont St (BES as-built #20004).
 - b. Currently, stormwater from the public right-of-way discharges to existing storm sewer infrastructure.

2. **Public Storm Main Extension May Be Required:** To provide an approvable stormwater disposal location for public improvements and/or private development, the applicant **may** be required to extend a public storm main in SW Burlingame Ave. Public storm main extensions must be reviewed by the City through a Public Works Permit (PWP) in accordance with PCC 17.32.100.
 - a. **Timing:** The applicant should note that requirements related to the PWP could impact desired development timelines if they are not adequately addressed in a timely manner:
 - 1) If a storm sewer extension is required, then prior to approval of the Conditional Use land use review application, BES must approve the applicant's PWP Concept Development (30% engineering design) plans.
 - b. **Technical Guidance:** At this time it is unclear how stormwater is managed and where it currently discharges for the existing development on private property. A storm sewer extension may be required to provide a discharge point for public stormwater facilities and for the on-site improvements depending on need and if stormwater is proposed to be discharged offsite (and not to the storm sewer in SW Vermont St). For BES to further evaluate the necessity for a storm sewer extension in SW Burlingame Ave, the applicant should provide a preliminary stormwater plan and information about onsite infiltration feasibility as soon as possible.
3. **General Stormwater Management Requirements:** Development and redevelopment sites that include any of the triggers listed in PCC 17.38.040 are subject to the policies and standards of PCC 17.38.035, Portland's [Stormwater Management Manual](#) (SWMM) and [Source Control Manual](#) (SCM). Projects must comply with the current adopted version of the SWMM as of the permit application date. A fundamental evaluation factor in the SWMM is the Stormwater Infiltration and Discharge Hierarchy (Section 1.3.3), which sets the framework that will be used to determine when a project's stormwater runoff must be infiltrated onsite and when offsite discharge will be permitted, and the parameters that must be met for either scenario. If tested infiltration rates on a property are greater than or equal to 2 inches per hour, onsite infiltration will be required unless the site qualifies for the ecoroof exception per Section 3.2.1 or infiltration is determined infeasible based on site conditions described in Chapter 2 of the SWMM. Note that maximum building coverage allowed by the zoning code, including below grade development, does not exempt the applicant from stormwater requirements. Pollution reduction and flow control requirements must be met using vegetated facilities to the maximum extent feasible, though roof runoff and some paved impervious surfaces are exempt when discharging directly to a UIC (refer to Sections 1.3.2, 1.3.4, 3.2.4 and 4.2.2 of the SWMM).
4. **Public Right-of-Way Stormwater Management:** Stormwater runoff from the public right-of-way must comply with all applicable standards of the SWMM and Sewer and Drainage Facilities Design Manual and be conveyed to a discharge point along a route of service approved by the BES Director or the Director's designee. Additional guidance on meeting the 2020 SWMM for projects in the public right-of-way is available at <https://www.portland.gov/bes/stormwater/swmm-and-right-way-projects>.
 - a. **SW Vermont St and SW Bertha Ct - Sidewalk Improvements:** Under the 2020 SWMM, sidewalk improvements behind an existing curb that create or replace 500 SF or more of impervious area are no longer exempt from stormwater management requirements unless they fall under a specific exemption described in Section 1.2.1.2 of the SWMM. According to this section, pavement maintenance or repair including repaving and resurfacing within the existing footprint of the paved surface is exempt, provided no soil (subgrade) is exposed and pavement is replaced in-kind. For pedestrian-only areas, unless base repair is explicitly specified, replacement in-kind is assumed to be maintenance. Note that tree credits can be applied in the public right-of-way as a potential stormwater management strategy for sidewalk and other triggered impervious areas (refer to Section 4.2.5 for additional information).

- 1) Based on the scope of PBOT requirements, it appears that the new and/or redeveloped impervious area behind the existing curb on SW Vermont St and SW Bertha Ct may trigger SWMM requirements. If triggered, this area is eligible to pay an Offsite Stormwater Management Fee in lieu of building a stormwater facility through the Staff Review Special Circumstances (SRSC) process.
 - 2) In this location, stormwater from the right-of-way discharges to an existing storm sewer inlet. Since there is no existing facility that meets pollution reduction and flow control requirements of the SWMM, the triggered impervious area must be managed through payment of the Offsite Management Fee or construction of facilities meeting SWMM requirements.
 - 3) If the applicant intends to pay the fee instead of constructing facilities, the SRSC application form must be submitted with the future Public Works Permit, or the applicant must submit documentation of intent to pay the fee with the application (documentation can include a statement in the land use narrative, an email to the reviewer identified above, or a statement on a set of plans). Refer to Section 1.8.1 of the SWMM for additional information. If the applicant elects to build stormwater facilities for these areas instead of paying the Offsite Management Fee, public facilities must be reviewed through the Public Works Permit process and additional property dedication may be necessary to accommodate them in the right-of-way. Refer to PBOT comments for additional dedication information. Be aware that if facilities are constructed, BES must approve the applicant's PWP Concept Development (30% engineering design) plans prior to approval of the preliminary land division application/environmental/design/conditional use review.
- b. ***Pedestrian Path Improvements:*** SWMM requirements must be met for the pedestrian path/connection required by PBOT if the improvements add or redevelop greater than 500 SF of impervious area. The applicant should consider whether or not stormwater runoff from the pedestrian path can discharge to filter strips per Section 4.2.6 of the SWMM. Alternatively, improvements that create, expand or replace pedestrian pathways qualify for payment of an Offsite Stormwater Management Fee in lieu of building a stormwater facility through the Staff Review Special Circumstances (SRSC) process. Refer to Section 1.8.1 of the SWMM for additional information.
- c. ***SW Burlingame Ave:*** Based on current requirements for public improvements and stormwater management triggers as described in the SWMM, it appears that public stormwater management will be required. Public stormwater management must be reviewed by the City through a Public Works Permit (PWP) in accordance with PCC 17.32.100.
- 1) ***Timing:*** The applicant should note that requirements related to public stormwater improvements and the associated PWP could impact desired development timelines if they are not adequately addressed in a timely manner:
 - (1) Prior to approval of the Conditional Use land use review application, BES must approve the applicant's PWP Concept Development (30% engineering design) plans.
 - 2) ***Stormwater Management Facilities:*** In order to meet SWMM requirements for the public right-of-way, the applicant must determine the appropriate stormwater system meeting the SWMM Stormwater Infiltration and Discharge Hierarchy (refer to Section 1.3.3). Public facilities may include vegetated stormwater facilities, or other facilities as approved by BES. In this location, BES does not anticipate infiltration of stormwater to be feasible within the right-of-way; therefore, applicable pollution reduction and flow control requirements must be met prior to discharge to the receiving system.

- 3) *Public Stormwater Report:* A Presumptive or Performance Approach stormwater report must be submitted with the PWP application. Refer to Section 4.3 of the SWMM for additional submittal requirements. Key elements of the PWP stormwater report include:
 - (1) For public vegetative stormwater facilities in the right-of-way, the stormwater report must include sizing calculations based on a clearly delineated drainage basin area. If the vegetated facility will be an infiltration facility, then site-specific infiltration testing in or near the right-of-way at the approximate depth of the proposed public facility is required.
5. *Private Property Stormwater Management:* Stormwater runoff from this project must comply with all applicable standards of the SWMM and SCM and be conveyed to a discharge point along a route of service approved by the BES Director or the Director's designee.
 - a. *Existing Development:* Plans submitted for the Conditional Use and Design reviews must show the stormwater disposal system for the existing paving and structures to remain including all downspouts, the type of private storm facility (if applicable), and the final stormwater disposal location. Any existing stormwater facilities must meet applicable setback requirements to new structures. If downspouts are routed to splashblocks, show that they meet setback and discharge requirements per Section 3.2.3.3 of the SWMM.
 - b. *Proposed Development - Stormwater Report:* The applicant must submit a Presumptive (SWMM Section 2.5.2) or Performance Approach (SWMM Section 2.5.3) stormwater report and a preliminary utility plan showing stormwater management facilities sized according to SWMM standards. The report must follow the outline included in Section 3.4.3 of the SWMM and be stamped by an Oregon registered engineer or other qualified design professional. Required elements of the report include:
 - 1) Results of infiltration tests, unless an Oregon-licensed engineer, certified engineering geologist, or registered geologist presents compelling evidence that site conditions make onsite infiltration of stormwater infeasible. The stormwater report narrative must incorporate this evidence in addressing how the project will meet the Stormwater Infiltration and Discharge Hierarchy. Infiltration testing may still be required at the discretion of BES. If testing is completed, the design team must test infiltration rates at depths that appear conducive to infiltration using field-based decisions and by referencing available soil information, geotechnical analyses and/or boring logs. To the extent feasible, proposed infiltration facilities must correspond with the location and depth of the completed infiltration testing. Indicate on a plan the approximate location of the test(s). Refer to Section 2.3.2 of the SWMM for complete infiltration testing requirements.
 - 2) Calculations prepared by an engineer using the [Presumptive Approach Calculator \(PAC\)](#). If using other software under the Performance Approach, the principles of Section 2.5.3 must be followed.
 - 3) If BES approves offsite discharge to the separate storm-only system in SW Vermont St, PCC 17.38 and the SWMM require stormwater discharge to meet pollution reduction and flow control requirements as described in Section 1.3.5 of the SWMM. The applicant must show through the Presumptive or Performance Approach stormwater report how pollution reduction and flow control standards that apply to the proposed discharge point will be met.
 - 4) Determination of the seasonal high depth to groundwater per Section 2.3.1 of the SWMM prepared by an Oregon registered professional engineer, certified engineering geologist or registered geologist.
 - 5) Note that for stormwater facilities in the public right-of-way, a separate stormwater report will be required as part of the public works permit. See comments below and

refer to [Portland's Public Works Permitting website](#) for more information about the permitting process.

- c. *Infiltration Facility Setback Requirements:* Required minimum setback distances from infiltration facilities to slopes, property lines, foundations and other features are described in Section 2.2.4 and Table 2-1 of the SWMM. Most infiltration facilities must be set back 5 feet from parcel property lines and 10 feet from foundations, as measured to the high water level at the edge of vegetated facilities, the middle of drywells, and the edge of soakage trenches. Note that no setbacks are required for property lines with the right-of-way. Additional specific setback requirements are described for rain gardens, filter strips, downspout extensions, and ponds (see Section 2.2.4 of the SWMM). BES will review all proposals to reduce required setbacks. Stormwater facilities with reduced setbacks may need to be designed under the Presumptive or Performance Approach and additional documentation and analysis (such as analysis from a geotechnical engineer, structural engineer, registered geologist or other environmental professional) may be required as described in Section 2.2.4 of the SWMM.
 - d. *Shared Private Stormwater Systems:* In order to comply with Oregon Plumbing Specialty Code, proposals to share stormwater systems across property lines must be approved through a [Plumbing Code Appeal](#). Additionally, a private easement or covenant for future easement must be shown over the shared facility. BES will review the easement to ensure that it covers an approvable stormwater system. BDS will review the easement document and the size of the proposed easement. The applicant is encouraged to contact BDS Plumbing directly with questions related to private easements and [plumbing code appeals](#): (503)-823-7388 for residential projects, and (503) 823-7302 for commercial projects.
6. *Clean River Rewards Program:* Clean River Rewards, Portland's stormwater discount program, offers discounts up to 100% of the City's onsite stormwater management charge to ratepayers who manage stormwater runoff on their property or retain large trees. Please note that this discount is not automatically applied; ratepayers must register their property and describe how stormwater is being managed to qualify. Be aware that some properties discharging to the Multnomah County Drainage Districts (within the Columbia Slough watershed) are not charged an onsite stormwater management fee by the City and are therefore ineligible to register for this program. See the [Clean River Rewards website](#) or call 503-823-1371 for more information.

D. GENERAL PUBLIC WORKS PERMIT INFORMATION

For questions related to the public improvements described throughout these notes, please contact Andre Duval at (503) 823-7214 or andre.duval@portlandoregon.gov or the BES Development Engineering hotline at (503) 823-7761, option 3.

1. *General Public Works Permit Information:* Information on the City's public works permit (PWP) process, including submittal requirements and review timelines, is available at www.portlandoregon.gov/publicworks. All submitted public works plans must meet the City's Sewer and Drainage Facilities Design Manual (SDFDM), SWMM, and public works permitting plan submittal requirements and drafting standards. Contact Public Works Permitting at (503) 823-1987 or pwpp@portlandoregon.gov with questions related to the general public works permit process.
2. *Hazardous Substances Code:* The City's Hazardous Substances Code (PCC 17.24.067) requires the excavation and removal of disturbed contaminated soils from right-of-way access areas and utility corridors. The soils must be replaced with clean fill at a minimum depth of 5 feet. A demarcation/contaminant barrier is also required when it has been determined the soils are contaminated at depth. Erosion control measures for contaminated soils (Section 8.2.1 of the SCM) must be met. Soil stockpiles must be covered and contained with a barrier on all four sides, with an impervious layer underneath the stockpile to inhibit contaminants from leaching back into the soil.

E. SUBMITTAL REQUIREMENTS FOR LAND USE

1. Full land use plan set, including preliminary utility plan showing all existing and proposed sanitary and storm facilities and connections.
2. A Presumptive/Performance approach stormwater report, including the results of infiltration testing, as described in this memo.
3. Documentation of intent to pay the SRSC Offsite Management Fee for qualifying improvements in the public right-of-way. Otherwise, the applicant would be required to initiate a Public Works Permit to construct public stormwater facilities and BES must approve the Concept Development (i.e. 30% design) PWP Plans prior to approval of the Conditional Use and Design review land use applications.
4. If a public sewer extension and right-of-way improvements will be required, those improvements must be reviewed by the City under a Public Works Permit (PWP). Prior to approval of the Conditional Use and Design review applications BES must approve the Concept Development (i.e. 30% design) PWP plans. Refer to the [City of Portland Public Works Permitting website](#) or contact Public Works Permitting (503-823-1987 or pwp@portlandoregon.gov) to learn how to begin the permit submittal process. There are established minimum review times for the PWP process; the applicant should take this into account with project scheduling.

F. PERMIT INFORMATION

At the time of permit review the applicant should be aware of the following:

1. *Connection Fees:* Sewage system connection fees and system development charges are assessed at the time of building plan review and change every fiscal year on July 1st. For additional information on these fees, navigate [here](#) or call the BES Development Review Team at 503-823-7761.
2. *Connection Requirements:* Connection to public sewers must meet the standards of the City of Portland's [Sewer and Drainage Facilities Design Manual](#).
3. *UIC Registration:* The Oregon Department of Environmental Quality (DEQ) regulates underground injection control (UIC) facilities to protect groundwater. Drywells and soakage trenches are examples of UICs. It is the applicant's responsibility to register all onsite UICs with DEQ, as appropriate. To learn more visit [DEQ's website](#) or contact the DEQ UIC Program at 503-229-5623. The SWMM also includes general UIC information.
4. *Source Control Requirements:* Source control requirements from the [Source Control Manual \(SCM\)](#), [Portland City Code \(PCC\) Title 17](#), and [BES Administrative Rules](#) that may be applicable to this project are listed below with the corresponding chapter, section, code, or rule. For specific questions on the following, please contact BES Source Control at 503-823-7122.
 - a. *Site Use and Activity-Based Source Control Requirements (SCM Chapter 6):* BES recommends the applicant review the following SCM sections to understand the structural, treatment, and operational BMP requirements that may impact the project design.
 - 1) *Waste and Recycling Storage (SCM Section 6.1)*
 - 2) *Food Cart Pods (SCM Section 6.2)*
 - 3) *Material Transfer and Loading Docks (SCM Section 6.4)*
 - 4) *Above-Ground Storage, Processing, or Transfer of Liquids (SCM Section 6.5)*
 - b. *BMP Design and O&M Requirements (SCM Chapter 7):* All projects that require an oil-water separator must meet the design requirements of Section 7.1.1 of the SCM. An Operations and Maintenance (O&M) Plan must be recorded with the County prior to

building permit approval. Additionally, projects that trigger signage and spill control, must meet the requirements of Section 7.1.2.

- c. *Site Dewatering Requirements* (SCM Chapter 9, [PCC 17.34](#), [PCC 17.36](#), [PCC 17.38](#), [PCC 17.39](#), [ENB 4.32](#)) BES evaluates requests for temporary and long-term stormwater and groundwater dewatering discharges into the city sewer system for approval or denial. See Appendix 1 of the SCM for the Construction Dewatering Discharge Application Form. If approved, a Discharge Permit for the storm or combined sewer may be required.
 - 1) Fees are assessed for temporary construction discharges to the public sewer system – navigate [HERE](#) for current rates and information about dewatering as it relates to [construction projects](#).
 - 2) Construction discharges to City UICs are prohibited.
 - 3) Construction discharges to private UICs (e.g., drywells or soakage trenches) must be authorized by DEQ's UIC Program.
- d. *Grease Management Program* ([PCC 17.34](#), [ENB 4.26](#)): The City requires grease management (GM) devices in all food service establishments and for any business/industry that may introduce fats, oils, or grease (FOG) into the public sewer. A monitoring access structure (MAS) may also be required. Please refer to the MAS discussion below regarding MAS requirements. For additional information, go to the [Cut Through the FOG](#) webpage.
- e. *Extra Strength Sewer Charge Program* ([PCC 17.34](#), [PCC 17.36](#), [ENB 4.25](#)): Food service establishments and some other industries incur a surcharge on their sewer bills. A monitoring access structure (MAS) may also be required. Please refer to the MAS discussion below regarding MAS requirements. Establishments that employ best management practices can apply for a rate reduction. Call 503-823-7093 for more information.
- f. *Monitoring Access Structures* ([PCC 17.34](#), [PCC 17.39](#), [ENB 4.03](#), [ENB 4.25](#), [ENB 4.26](#), [ENB 4.35](#)): Monitoring Access Structures (MAS) for sampling wastewater or stormwater discharges may be required for users that introduce, or are expected to introduce, pollutants into the public sewer from any non-domestic source. Please refer to the [MAS Administrative Rules](#) for additional information regarding MAS requirements.
- g. *Water Features* ([PCC 17.34](#), [PCC 17.39](#)): If a water feature, such as a fountain or swimming pool, uses treated water (e.g., chlorinated or biocides), the feature water and filter backwash must discharge to the sanitary waste line. If the feature water is not treated, under most circumstances the water can discharge to the storm sewer. If the source of the feed water is rainwater and required to discharge to the City sanitary sewer system or groundwater, a meter is required to assess sewer user fees; current rates are located [HERE](#).



City of Portland, Oregon
Bureau of Development Services
Land Use Services
FROM CONCEPT TO CONSTRUCTION

Carmen Rubio, Commissioner
David Kuhnhausen, Interim Director
Phone: (503) 823-7300
TTY: 711
www.portland.gov/bds

Site Development

Early Assistance Conference Response

Date: January 5, 2024

To: Matt Wickstrom, Conference Facilitator
503-865-6513, Matt.Wickstrom@portlandoregon.gov

From: Jeff Duquette, Jeff.Duquette@portlandoregon.gov

Case File: EA 23-105044

Location: 1151 SW VERMONT ST

R#: R328555, R328556, R328614, R328618, R328614, R328614

Proposal: A Pre-Application Conference to discuss redevelopment of the existing high school site with a new 3-4 story building to accommodate 1,700 students. Redevelopment of the site will also include practice fields, parking lots and a potential swimming pool. The school will offer a full range of high school facilities, including a theater, band and choir rooms, a cafeteria and commons, and a media center as well as classrooms and science labs, art rooms, shop spaces and a variety of specialized learning environments. The school may also accommodate a small childcare center and a health clinic.

The Site Development Section of the Bureau of Development Services (BDS) has reviewed the pre-application conference materials to identify potential issues and requirements.

A. Key Issues and Requirements

Following is a brief summary of issues and requirements that may impact your proposed project or are submittal requirements that will require time to prepare prior to submittal of the application.

1. **Floodplain Requirements:** The site is not within a FEMA defined special flood hazard area.
2. **Geotechnical Engineering:** Geotechnical will be a key aspect given the site's terrain, shallow ground water, surface seepage and/or spring activity. A Geotechnical Design Report will be required at the time of permit application. A landslide hazard study may be required as part of a land-use application review.
3. **Landslide Hazard Study:** Landslide hazard studies are a key issue due to the time and cost involved in obtaining the required study, and because the results may affect site layout and design. Landslide hazard studies are required for land use application reviews when land divisions or planned developments are being proposed. If a Landslide hazard is triggered by a land-use application review, please

contact your BDS Site Development Reviewer for additional information:
jeff.duquette@portlandoregon.gov

4. Erosion & Sediment Control: Erosion prevention and sediment control is a key issue because the project area meets Special Site criteria and thus is subject to additional requirements for erosion, sediment and pollution control. The building permit application must include an erosion and sediment control plan that meets the minimum City Charter requirements detailed in Title 10 as well as Oregon DEQ Requirements for DEQ 1200C permits.

B. Geotechnical Engineering Requirements

In order to evaluate whether the foundation design of the proposed structure complies with the Oregon Structural Specialty Code, a geotechnical engineering report will be required at the time of building permit application. In addition, The State of Oregon considers education facilities “special occupancy structures”. As such, the geotechnical report must include a site-specific seismic hazard study.

Geotechnical elements of proposed construction that may trigger additional permit submittal materials include significant excavation work either adjacent to the Public Right of Way, near adjoining private property, or if excavation work is to be conducted near on-site occupied structures. In the cited scenarios, the geotechnical report would need to include recommendations for temporary excavation shoring and/or structural underpinning (or etc), and the permit application would need to include plans and design calculations for each.

Other geotechnical information and/or analysis requirements will include the proposal to use new stormwater infiltration facilities within the northeastern corner of the campus. Please refer to the section below “Stormwater Disposal & Treatment” for additional information.

Summary of geotechnical engineering information that must be submitted to Site Development at the time of land use review: none. Questions regarding this requirement may be directed to Jeff Duquette, 503-823-5075.

C. Stormwater Disposal and Treatment

The Bureau of Environmental Services will review the project for conformance to the 2008 Stormwater Management Manual.

Site Development will require additional information at the time of either land-use review or at the time of building permit application if stormwater disposal via infiltration is being proposed near the northeastern corner of the campus. There is an offsite mapped landslide (down gradient from the school) located east and north of SW Burlingame Ave. Introduction of stormwater disposal via infiltration in the northeastern corner of the campus could impact stability of this existing slide. BDS Site Development would require a geotechnical assessment of slope stability for this off-site slide feature if infiltration is being proposed on the northeastern side of the Campus. Please contact your BDS Site Development Reviewer for additional information: jeff.duquette@portlandoregon.gov

D. Demolition

Removal of any structure that exceeds 200 square feet in area requires a [demolition permit](#).

E. Erosion Control

Erosion prevention and sediment control requirements found in **Title 10** apply to both site preparation work and development. Full compliance with the erosion control requirements of Title 10, as well as maintenance of the erosion control elements, such as silt fences on private property, storm drain inlet protection and bio bags in the public right-of-way, is the responsibility of the property owner, the developer, and the builders. An erosion control plan must be submitted at the time of plan review. The project area meets the criteria specified in City Code 10.30.030 as a Special Site with additional requirements for erosion, sediment and pollution control. An erosion control plan prepared by a Certified Professional in Erosion and Sediment Control (CPESC) or State of Oregon registered professional engineer, and special inspections by the CPESC or P.E. during construction, will be required at the time of building permit review.

Please refer to the City of Portland **Erosion and Sediment Control Manual** for additional information regarding erosion and sediment control requirements.

DEQ permit required: The applicant is advised that a 1200-C permit from the Oregon Department of Environmental Quality is required for construction activities including clearing, grading, excavation, and stockpiling that will disturb one or more acres and may discharge to surface waters or conveyance systems leading to surface waters of the state, in addition to City requirements.

Summary of erosion control information that must be submitted to Site Development at the time of land use review: none. Questions regarding this requirement may be directed to Jeff Duquette, 503-823-5075.

F. Permit Requirements

1. Demolition permit(s).
2. Commercial building permit.
3. Geotechnical Report & Site Specific Seismic Hazard Study.

G. Submittal Requirements for Land Use

1. Preliminary utility plan.
2. Site grading plan and engineering calculations of cut and fill volumes.
3. Geotechnical report/Landslide Hazard Study.

Water Bureau

Early Assistance Appointment Response

Date: November 30, 2023

To: Matt Wickstrom, Conference Facilitator
503-865-6513, Matt.Wickstrom@portlandoregon.gov

From: Bryan Vittori, 971-917-5458, Bryan.Vittori@portlandoregon.gov

Case File: EA 23-105044

Location: 1151 SW VERMONT ST

Property ID: R328555, R328556, R328614, R328618, R328614, R328614

Proposal: A Pre-Application Conference to discuss redevelopment of the existing high school site with a new 3-4 story building to accommodate 1,700 students. Redevelopment of the site will also include practice fields, parking lots and a potential swimming pool. The school will offer a full range of high school facilities, including a theater, band and choir rooms, a cafeteria and commons, and a media center as well as classrooms and science labs, art rooms, shop spaces and a variety of specialized learning environments. The school may also accommodate a small childcare center and a health clinic.

The Portland Water Bureau (PWB) has reviewed the Early Assistance materials to identify potential issues and requirements.

A. WATER AVAILABILITY

1. Water is available to this site from the 8" cast iron (CI) and 8" ductile iron (DI) water main in SW Burlingame Ave. The static water pressure is estimated as 72 - 90 psi at 542 feet in elevation. These lines are in the Bertha 750 Sunset pressure zone.
2. Water is available to this site from the 8" CI and 12" CI water main in SW Vermont St. The static water pressure is estimated as 32 - 40 psi at 551 feet in elevation. These lines are in the Burlingame 643 Tank pressure zone.
3. The site is currently served through a 6" domestic meter and 6" fire service. These services are both off the 8" CI Service in SW Burlington Ave.

Service line and meter sizes are determined by the total fixture units being served by that line and meter. If the services are found to be inadequate, they will be resized at the expense of the applicant. If an existing service is not used for the new development, it must be removed. All fees to remove services are the responsibility of the applicant.

Submission of the fixture count is required at permit application. Please refer to the [W3](#) and [W4](#) forms on our website for more information.

4. The site irrigation is provided by a 6-inch irrigation service off the 12" CI water main in SW Vermont St. and a 2" irrigation service of the 8" CI water main in SE Vermont St.

B. KEY ISSUES

1. There is a 10-ft wide Water Bureau easement (ORD 114505) over a 12" CI water main that runs along the eastern and northern perimeter parking lot of the campus. There are no connections from this 12" CI water main to the campus.

Preliminary reports from PWB Planning indicate that it is feasible to abandon the 12" CI main. Any cost associated with main abandonment will be at the applicants expense with all work being preformed by the PWB. Please note that main abandonment work will fall outside of our Rate Ordinance requiring a Site Specific estimate.

Once the main is abandoned PWB will quit claim the easement.

2. Main abandonment could be achieved through an independent request or as part of the commercial building permit.

For an independent request please contact me directly.

3. To install a new sanitary lateral to the property you will need to cross an existing water main in SW Burlingame Ave. PWB will require a Utility Protection Plan prior to construction. Refer to the [Portland Guidelines for Utility Protection](#) for more information.
4. To obtain fire flow information fill out a "Fire Flow Request Form" found at our website, <https://www.portland.gov/water/water-development-services/request-fire-flow-information> or by calling 503-823-1408.
5. The property has access to mains in two different pressure zones. Please be aware of the different pressures available when requesting fire flow and designing your water systems.

C. OTHER CATEGORY

1. Backflow prevention assemblies are required for all dedicated irrigation services, dedicated fire lines, mixed-use/multi-tenant developments, certain occupancies, and meters larger than 1.5". A backflow assembly may be required on smaller services based on the use of the property. Dedicated fire sprinkler water services are to be equipped with a state-approved detector style backflow prevention assembly.

Backflow prevention assemblies can significantly impact property frontage development. Water Bureau required backflow assembly installations are required on private property, at the public right of way line, centered on the city's water service. Some installations are required to be installed above finished grade in an approved insulated outdoor enclosure. Assemblies installed inside of buildings must be approved prior to installation. Water services to high rise buildings (measured 75-feet from lowest finished floor to top of structure) are required to be equipped with Reduced Pressure type backflow assemblies.

Please reference possible backflow assembly requirements for your project at <https://www.portland.gov/water/backflow-prevention/backflow-assembly-installation-requirements> or contact Water Quality Inspection at 503-823-7479 for more information.

2. If there is contamination in or near the ROW at the location of proposed water mains or services, PWB requires:
 - a. Verification of clean soils at the location of the installations; or

- b. Identification of the extent and degree of contamination such that appropriate remediation plans can be generated prior to any PWB construction. The remediation, disposal fees, and charges are the responsibility of the applicant.
3. The developer will be required to confirm through permit review that the existing and proposed PWB infrastructure will meet City standards relative to the proposed improvements. Water Bureau infrastructure may need to be adjusted or fully reconstructed at the expense of the developer if standards cannot be met. Please review the Portland Water Bureau Engineering and Technical Standards Administrative Rule that was instituted June 2022 at <https://www.portland.gov/water/water-development-services/engineering-admin-rule>

D. WATER CODE REQUIREMENTS

The Portland Water Bureau's guiding code, [Title 21](#) was updated in January 2021. .

- **Water services may not cross property lines.** Separate services and meters are required for **development on an individual lot, regardless of lot ownership**. An exception is when a single structure crosses a property line, in which case a single meter may serve the structure.

Topic	Code and Comments	Code Citation & Link
Title 21	City Water Code	Title 21 Water
Engineering and Technical Standards	Administrative Rule	Administrative Rule

E. PERMIT INFORMATION

At the time of permit review (following the land use review) you should be aware of the following:

1. Meters will be sized during the building permit process. Sizing is based on total count of all fixtures supplied by the identified service. Applicant will provide an SDC Form, W-3, or W-4 for each meter as part of the building permit submittal. There will be no reduction in meter size based on grey water usage or the installation of low-flow fixtures.
2. All new domestic service taps and upsized meters will be assessed a [System Development Charge](#) (SDC). Fee is based on meter size except when a header service is installed, in which case the fee is based on the shared meter equivalent. See PWB Code Guide for more information: <https://www.portland.gov/water/water-development-services/separate-meter/>.
3. SDC credit will be given for meters that are permanently removed. SDC credit is applied towards services within the same lot and is not transferrable.
4. Fire lines are excluded from Systems Development Charges.
5. The applicant can consider the use of a combination domestic and fire service for domestic services of 2" or less.

6. Service/Mains work may fall outside of our Rate Ordinance requiring a Site Specific estimate. Ordinance rates for service installations and water main extensions can be found in Exhibit A of the current Water Fee Schedule: <https://www.portland.gov/water/water-development-services/water-development-fees/>.

Fire Bureau

Pre-Application Conference Response

Date: December 18, 2023

To: Matt Wickstrom, Conference Facilitator

503-865-6513, Matt.Wickstrom@portlandoregon.gov

From: Zach Parrish, 503-991-2764

Zach.Parrish@portlandoregon.gov

Case File: EA 23-105044

Location: 1151 SW VERMONT ST

Property ID: R328555, R328556, R328614, R328618, R328614, R328614

Proposal: A Pre-Application Conference to discuss redevelopment of the existing high school site with a new 3-4 story building to accommodate 1,700 students. Redevelopment of the site will also include practice fields, parking lots and a potential swimming pool. The school will offer a full range of high school facilities, including a theater, band and choir rooms, a cafeteria and commons, and a media center as well as classrooms and science labs, art rooms, shop spaces and a variety of specialized learning environments. The school may also accommodate a small childcare center and a health clinic.

Portland Fire & Rescue has reviewed the pre-application conference materials to identify potential issues and requirements in regard to **fire access and water supply**. Final determination of compliance with applicable codes and standards will be made during plan review, after building permit application has been submitted.

Please refer to Portland Fire & Rescue's code guide entitled Fire & Life Safety Requirements for Fire Department Access and Water Supplies

[Fire & Life Safety Requirements for Fire Department Access and Water Supplies \(portland.gov\)](#)

<https://www.portland.gov/sites/default/files/2020-03/fire-life-safety-requirements-for-fire-department-access-and-water-supplies-3-17-15.pdf>

This document details PF&R's requirements for fire department access and water supply; applicants will be expected to meet these requirements.

Please note that the scope of this review is limited to fire access and water supply at the site and does not include requirements related to the building itself. Questions about requirements related to the building can be addressed in a Preliminary Life Safety meeting if needed.

Please contact **Zachariah Parrish** if you have any questions:

Zachariah Parrish
Portland Fire & Rescue
Fire Plans Review
(503)-991-2764
zach.parrish@portlandoregon.gov



Urban Forestry

Early Assistance Response

Date: December 20, 2023
From: Amil Jakupovic
503-823-4491, Amil.Jakupovic@portlandoregon.gov
Case File: EA 23-105044
Location: 1151 SW VERMONT ST
Proposal: A Pre-Application Conference to discuss redevelopment of the existing high school site with a new 3-4 story building to accommodate 1,700 students. Redevelopment of the site will also include practice fields, parking lots and a potential swimming pool. The school will offer a full range of high school facilities, including a theater, band and choir rooms, a cafeteria and commons, and a media center as well as classrooms and science labs, art rooms, shop spaces and a variety of specialized learning environments. The school may also accommodate a small childcare center and a health clinic.

Portland Parks, Urban Forestry staff has reviewed the Early Assistance materials to identify potential issues and requirements in accordance with Title 11, Trees. This response identifies potential issues and/or impacts on existing street and heritage trees, and trees on city-owned or managed sites, if applicable. Trees on private property are subject to development standards from the Bureau of Development Services. See planner requirements for private property trees.

Please note that there may be other applicable tree requirements in Title 33 Planning & Zoning.

A. Response Summary

The development will be subject to Urban Forestry standards and requirements during the permit review process as detailed below.

The existing trees must be preserved at all phases of construction. The applicant must preserve trees per 11.60.030 Tree Protection Specifications. If the applicant wants to pursue removal, they must adequately demonstrate why the tree cannot be preserved while developing the site to City standards.

B. Tree Plan (11.50.060)

A tree plan must be submitted with each phase of review including land use reviews, building permit applications, and public works permits. A tree plan was not submitted with the EA application. The plan must include the following information for street trees:

- a. The size and location of street trees adjacent to the subject property.
- b. Trees proposed to be preserved including tree protection specifications in accordance with 11.60.030.
- c. Tree(s) proposed for removal.
- d. Tree planting plan (tree species and location(s)).



C. Street Trees

1. Existing Street Conditions

- a. SW Vermont St: The site has approximately 2030 feet of street frontage. The right-of-way is improved with pavement, curbs, planting strip, and sidewalks. There are no overhead high-voltage power lines. There are 32 street trees.
 - i. London plane tree, 16 in diameter, in good condition
 - ii. London plane tree, 13 in diameter, in good condition
 - iii. London plane tree, 26 in diameter, in good condition
 - iv. London plane tree, 17 in diameter, in good condition
 - v. London plane tree, 11 in diameter, in good condition
 - vi. London plane tree, 10.5 in diameter, in good condition
 - vii. London plane tree, 3 in diameter, in good condition
 - viii. London plane tree, 17.3 in diameter, in fair condition
 - ix. London plane tree, 6.5 in diameter, in fair condition
 - x. London plane tree, 19.7 in diameter, in fair condition
 - xi. Callery pear, 13 in diameter, in fair condition
 - xii. Callery pear, 15.2 in diameter, in fair condition
 - xiii. London plane tree, 13 in diameter, in good condition
 - xiv. London plane tree, 21.5 in diameter, in fair condition
 - xv. London plane tree, 17.5 in diameter, in fair condition
 - xvi. London plane tree, 19 in diameter, in fair condition
 - xvii. London plane tree, 18 in diameter, in fair condition
 - xviii. London plane tree, 22 in diameter, in fair condition
 - xix. London plane tree, 20.7 in diameter, in fair condition
 - xx. London plane tree, 14.8 in diameter, in fair condition
 - xxi. London plane tree, 25 in diameter, in fair condition
 - xxii. London plane tree, 14 in diameter, in good condition
 - xxiii. Norway maple, 25 in diameter, in fair condition
 - xxiv. London plane tree, 24 in diameter, in fair condition
 - xxv. London plane tree, 20 in diameter, in fair condition
 - xxvi. Norway maple, 32 in diameter, in fair condition
 - xxvii. London plane tree, 24 in diameter, in good condition
 - xxviii. London plane tree, 26.5 in diameter, in fair condition
 - xxix. London plane tree, 27 in diameter, in fair condition
 - xxx. London plane tree, 25 in diameter, in fair condition
 - xxxi. Norway maple, 24.5 in diameter, in good condition
 - xxxii. London plane tree, 25 in diameter, in fair condition
- b. SW Burlingame Ave: The site has approximately 462 feet of street frontage. The right-of-way is unimproved with pavement, no curbs, planting strips, or sidewalks. There are no overhead high-voltage power lines. There are 44 street trees.
 - i. Bigleaf maple, 6 in diameter, in good condition
 - ii. Bigleaf maple, 4.5 in diameter, in good condition
 - iii. Bigleaf maple, 5.5 in diameter, in good condition



- iv. Bigleaf maple, 35 in diameter, in good condition
- v. Bigleaf maple, 18.5 in diameter, in fair condition
- vi. Bigleaf maple, 6.5 in diameter, in fair condition
- vii. Bigleaf maple, 15 in diameter, in fair condition
- viii. Bigleaf maple, 10.5 in diameter, in fair condition
- ix. Bigleaf maple, 4.5 in diameter, in good condition
- x. Bigleaf maple, 13 in diameter, in poor condition
- xi. Bigleaf maple, 15 in diameter, in fair condition
- xii. Bigleaf maple, 22 in diameter, in fair condition
- xiii. Bigleaf maple, 12 in diameter, in fair condition
- xiv. Western red cedar, 10 in diameter, in good condition
- xv. Bigleaf maple, 15 in diameter, in fair condition
- xvi. Bigleaf maple, 16 in diameter, in fair condition
- xvii. Bigleaf maple, 12 in diameter, in fair condition
- xviii. Douglas fir, 8 inches in diameter, in good condition
- xix. Douglas fir, 22 inches in diameter, in good condition
- xx. Douglas fir, 36 inches in diameter, in good condition
- xxi. Bigleaf maple, 8 in diameter, in fair condition
- xxii. Western red cedar, 34 in diameter, in good condition
- xxiii. Bigleaf maple, 9.5 in diameter, in fair condition
- xxiv. Douglas fir, 22 inches in diameter, in good condition
- xxv. Bigleaf maple, 14 in diameter, in fair condition
- xxvi. Bigleaf maple, 15 in diameter, in fair condition
- xxvii. Bigleaf maple, 17 in diameter, in fair condition
- xxviii. Bigleaf maple, 8 in diameter, in fair condition
- xxix. Bigleaf maple, 8.5 in diameter, in fair condition
- xxx. Cottonwood, 44 in diameter, in fair condition
- xxxi. Douglas fir, 12 inches in diameter, in good condition
- xxxii. Bigleaf maple x 6, 4-12 in diameter, in fair condition
- xxxiii. Douglas fir, 8 inches in diameter, in good condition
- xxxiv. Douglas fir, 22 inches in diameter, in good condition
- xxxv. Douglas fir, 14 inches in diameter, in good condition
- xxxvi. Western red cedar x 3, 6-8 in diameter, in good condition
- xxxvii. Douglas fir, 42 inches in diameter, in good condition

- c. SW Bertha Blvd: The site has approximately 500 feet of street frontage. The right-of-way is improved with pavement, curbs, and no planting strip or sidewalks. There are no overhead high-voltage power lines. There are 3 street trees.

- i. Douglas fir, 19 inches in diameter, in good condition
- ii. Douglas fir, 16.5 inches in diameter, in good condition
- iii. Douglas fir, 14 inches in diameter, in good condition

2. *Street Tree Preservation* (11.50.040)

Based on the proposed development it appears existing street trees may be impacted. Development proposals must be configured to avoid street trees.

All existing street trees must be preserved at all phases of construction. Urban trees are a critical part of public green infrastructure. If the applicant believes the tree must be removed to facilitate development,



the applicant must provide adequate technical analysis demonstrating why the tree cannot be preserved while developing the site to City standards.

3. *Street Tree Protection Specifications* (11.60.030)

Tree protection is required in accordance with Title 11 Trees, Protection Methods (11.60.030). Tree protection shall follow either the Prescriptive or Performance path. Protection methods must be shown on the tree plan. If using the Performance path, the alternate tree protection plan must be prepared by an arborist who has visited the site.

<https://www.portland.gov/trees/trees-development/construction-and-tree-preservation>

4. *Street Tree Planting* (11.50.060.C)

The applicant has not provided a conceptual street tree planting plan. One street tree must be planted or retained for each full increment of 25 linear feet (11.50.060.C.1). Street trees must be planted at a minimum of 2.5 caliper inches. Trees will be required to be planted through (Building permit).

- a. SW Vermont St: There are approximately 2030 feet of street frontage at this location, requiring up to 81 trees to be planted or preserved.
- b. SW Burlingame Ave: There are approximately 462 feet of street frontage at this location, requiring up to 18 trees to be planted or preserved.
- c. Street Name (Parcel): There are approximately 500 feet of street frontage at this location, requiring up to 20 trees to be planted or preserved.

Street tree planting may be exempt under 11.50.060.B when existing above or below-grade utilities prevent planting street trees or when the existing planting strip is less than 3-feet wide.

Due to the existing condition of the right-of-way, street trees may not be required unless PBOT requires frontage improvements.

D. Heritage Trees

1. *Heritage Trees* (11.20.060):

There is not a heritage tree located on or adjacent to the site that is on the City of Portland's Heritage Tree list.





URBAN FORESTRY TREE REQUIREMENTS

Early Assistance and Land Use Review

Portland Parks & Recreation Urban Forestry staff review Early Assistance and Land Use Review materials to identify potential issues and requirements in accordance with Title 11, Trees and Title 33, Zoning Code. The purpose of these reviews is to identify potential issues and/or impacts on existing street trees, heritage trees, and trees on City-owned or managed sites (if applicable), as well as to provide adequate areas for future street tree planting on existing and proposed public streets. Trees on private property are subject to development standards from the Bureau of Development Services. See planning requirements for private property trees or call the Zoning Hotline at 503-823-7526.

Tree Plan Submittal Requirements (11.50.070)

A tree plan must be submitted with each phase of review including land use reviews, building permit applications, and public works permits. The tree plan information may be combined with other relevant plan sheets. The tree plan submittal shall include the following information:

- ☐ existing improvements;
- ☐ proposed alterations;
- ☐ existing street trees $\geq 3"$ DBH including size and location;
- ☐ existing on-site trees $\geq 6"$ DBH within 15' of the limits of disturbance;
- ☐ trees proposed for removal;
- ☐ tree planting proposal, including tree size, species and location; and
- ☐ trees to be retained and proposed tree protection measures meeting the specification in Chapter 11.60.

Any changes to an approved Tree Plan, including amending tree species must be approved by the City Forester. Please note that the City Forester may not approve revised tree planting plans based on the lack of species availability. To facilitate species availability, it is recommended that tree procurement occur approximately 6 months prior to installation.

Tree Mitigation (11.50.040.C.2)

Healthy street trees $\geq 6"$ DBH that are approved for removal shall be replanted with two trees in addition to trees required to be planted to meet Street Tree Planting Standards, below. When street improvements are to partially or fully unimproved streets, healthy street trees $\geq 12"$ DBH approved for removal shall be replanted with two trees, with trees planted to meet Street Tree Planting Standards credited towards meeting this requirement. Tree replacement for trees removed shall occur in the street planter strip, on site, or in the same watershed either by planting or by paying a fee in lieu of planting in accordance with table 60-1, below.

On City-owned or managed sites, healthy, non-nuisance trees $\geq 6"$ DBH that are approved for removal shall be replanted per the Administrative Rule for tree replacement standards, below:



Tree Replacement for Development on City Owned or Managed Sites

Size of tree to be removed (inches in diameter)	Number of trees to be planted
6 and up to 12	Up to 2
More than 12 and up to 20	Up to 3
More than 20 and up to 25	Up to 5
More than 25	Up to 6

Street Tree Planting Standards (11.50.050)

One street tree shall be planted or retained for each full increment of 25 linear feet per side of street frontage. Planting is exempt when existing above or below grade utilities prevent planting of street trees, or if the existing design of the street will not accommodate street tree planting because the planting strip is less than 3 feet wide, there is not a planting strip, or there is insufficient space to add tree wells. Trees planted to meet street tree planting standards are credited toward mitigation requirements when street improvements are to partially or fully unimproved streets. When the required number of trees cannot be planted, a fee in lieu of planting will be required, in accordance with Table 60-1, below.

Table 60-1 Broadleaf Tree Size Requirements

Development Type	Tree Size	
	On Site	Street
One and Two Family Residential	1.5"	1.5"
Multi Dwelling Residential	1.5"	2"
All others	1.5"	2.5"

Tree Planting Specifications

If there are fewer than 8 required trees, they may all be the same species. If there are between 8 and 24 required trees, no more than 40 percent can be of one species. If there are more than 24 required trees, no more than 24 percent can be of one species. Street tree species shall conform to the appropriate "City of Portland Approved Street Tree Planting List." The City Forester may approve or require an alternate or unlisted species.

All required street trees shall be planted in-ground following Standard Drawing Number P-581 "Typical Street Tree installation," except when in raised planters that are used to meet Bureau of Environmental Services storm water management requirements. Please include the Standard



Drawing Number P-581 as part of the Public Works permit application. Plant materials shall be installed to current nursery industry standards and proper arboricultural practices [American National Standards Institute, *ANSI A300 Part 6: Tree, Shrub, and Other Woody Plant Maintenance-Standard Practices (Planting and Transplanting)* 2012, Tree Care Industry Association, Inc. Londonderry, NH]. Plant materials shall be properly supported to ensure survival.

All trees required or approved to be planted by Title 11 shall be planted or payment in lieu of planting made prior to the expiration of the permit or City's final acceptance of the project, as applicable. However, it is encouraged that planting occur during the wet months or as per City Forester recommendations. Street tree planting may be deferred between May 1 and September 30 upon filing a performance guarantee as provided in Section 11.10.060 or other assurance deemed acceptable by the City Forester or BDS Director as applicable.

Tree Protection Specifications (11.60.030)

Trees to be retained shall be protected in accordance with Title 11 Trees, Protection Specifications (11.60.030.C). Tree protection shall be shown on the tree plan and include the distance from the trunk of the tree to the fence. A standard root protection zone is established as follows; a minimum of 1 foot radius (measured horizontally away from the face of the tree trunk) for each inch of tree diameter. Protection fencing shall be a minimum 6-foot high metal chain link construction fence, secured with 8-foot metal posts established at the edge of the root protection zone and permissible encroachment area.



MEMORANDUM

EA 23-105444 PC Site: 1151 SW Vermont St (Ida B. Wells High School)

DATE: January 17, 2024

TO: Matt Wickstrom, Conference Facilitator, Bureau of Development Services (BDS),
Matt.Wickstrom@portlandoregon.gov, 503-865-6513

FROM: Marty Stockton, Senior Parks Planner, Westside Liaison, Portland Parks & Recreation, Marty.Stockton@portlandoregon.gov, 503-201-7199 (work cell)

CASE FILE: EA 23-10544PC

LOCATION: 1151 SW Vermont St

PROPOSAL: A Pre-Application Conference to discuss redevelopment of the existing high school site with a new 3-4 story building to accommodate 1,700 students. Redevelopment of the site will also include practice fields, parking lots, and a potential swimming pool. The school will offer a full range of high school facilities, including a theater, band and choir rooms, a cafeteria and commons, and a media center as well as classrooms and science labs, art rooms, shop spaces, and a variety of specialized learning environments. The school may also accommodate a small childcare center and a health clinic.

PURPOSE:

Portland Parks & Recreation (PP&R) staff have reviewed the application materials to identify potential issues and requirements. The response is included in the PP&R comments section below.

APPLICANT QUESTIONS FOR PP&R:

How will the standards for the Major Public Trail be applied to this site, considering that only a small part of the starred zoning line is located on the IBW tax lots and the vast majority of the line is on the Rieke Elementary tax lot? If the trail requirement is triggered with this proposal can the City elaborate on the requirements for the trail (i.e. flexibility of location through the site, width and Surfacing requirements, are the hours it must be open to the public (5 am – 10 pm) flexible?

PP&R COMMENTS:

Portland Parks & Recreation (PP&R) has reviewed the materials for the Pre-Application Conference for the proposal, to modernize the existing high school by redeveloping the site. The project will need to feature a proposed north-south regional trail improvement and connection to the east-west Red Electric Trail along the SW Vermont Street right-of-way.



PP&R is expecting that the north-south regional trail be built within the Portland Public Schools site (as defined by ownership), which includes both Reike Elementary School and Ida B Wells High School. While there is some flexibility on the north-south regional trail's alignment, it seems appropriate on one side, perhaps the east side, of the driveway that accesses the Reike Elementary portion of the site.

The segment of the east-west Red Electric Trail will met by the existing and/or improvements to the existing sidewalk and bike lanes within SW Vermont Street's public right-of-way.

The question of what improvements are needed at the intersection of the north-south regional trail and SW Vermont Street may need to be addressed through the TIS and the public works alternative review process. The north-south regional trail should enter the right-of-way as close to the existing crossings at either SW 13th Avenue and/or SW Chestnut Street.

The Red Electric Trail is an adopted and proposed regional trail along the route of the old Red Electric train line. When complete, the Red Electric will be a major east-west artery in the regional path network that will connect the existing Fanno Creek Trail in Beaverton to the Willamette Greenway Trail in Portland's South Waterfront neighborhood. The Red Electric Trail Planning Study, accepted by the City Council in May 2007, identifies an alignment for a roughly 16-mile route that follows the historic Red Electric train line which operated for 15 years before closing in 1929. The adopted Red Electric Trail alignment is represented by the "stars" on the Portland Zoning Map within the northern portion of the site. The Red Electric Trail Planning Study is online at: <https://www.portland.gov/transportation/planning/documents/red-electric-trail-planning-study>. Farther west of Ida B Wells High School, PP&R is currently leading a federally funded project for the design of the Red Electric Trail east of SW Shattuck Road that connects Pendleton Park and Hayhurst Elementary School. Lora Lillard, Capital Project Manager at PP&R, is the project manager of this effort.

PP&R supports the existing onsite urban trail routes as described in the [Southwest Urban Trails Plan](#) (2000), especially the [urban trail and Safe-Route-to School route](#) onsite that connect to SW Burlingame Avenue.

Key PP&R comments:

1. The Red Electric Trail needs to connect from west to east along SW Vermont Street. This segment of the Red Electric Trail will be met by the existing and/or improvements to the existing sidewalk and bike lanes within SW Vermont Street's public right-of-way.

2. PP&R is flexible on the north-south trail alignment, but supports BDS and PBOT in where the trail is located when it intersects with SW Vermont Street to the south, whether at SW 13th Avenue and/or SW Chestnut Street where there are existing pedestrian crossings.
3. The north-south trail, a regional walking and biking trail, within the PPS-owned site typical standard is 12-foot asphalt/concrete paved, with 2-foot-wide gravel shoulders on each paved edge. For additional information on walking and biking trails, the PP&R trail standards can be found in the [Trail Design Guidelines for Portland's Park System](#) (May 2009).
4. For the north-south trail, an easement of 25 feet wide is typically specified, but negotiable. PP&R generally prefers to manage/maintain only the 16-foot wide paved and gravel area, with maintenance access to the wider 25 feet allowed (but this is negotiable). PP&R generally prefers that any side buffer/landscaped edges be managed/ maintained by the property owner or another responsible entity.
5. On the subject school site for the north-south regional trail, signage in marker format should provide trail identity and information on trail direction, mileage, and/or elevation changes as appropriate. Directional information might indicate recreational facilities or other significant destinations.
6. Regional trails generally need to be open 24/7, although some can also have special hours -- generally when city parks are open from 5:00 a.m. to midnight or other hours as set by the Parks Director. Regional trails are for both recreation and transportation purposes.
7. PP&R supports existing urban trail routes and improvements onsite that connect to SW Burlingame Avenue. The proposed redevelopment should retain an on-site pedestrian connection to SW Burlingame Ave, which is the primary investment route to access the school site on foot from SW Terwilliger Blvd.

PP&R looks forward to continuing to work with Portland Public Schools and the design and development team to ensure that the north-south regional trail and Red Electric Trail connection concept goals are met and that the future trail access is well-defined and protected from other uses on the site. Additionally, PP&R supports ongoing involvement and coordination on the Ida B. Wells-Barnett Outdoor Pool.

23-105044 PC
Ida B. Wells High School
1151 SW Vermont
Sign-in Sheet
December 21, 2023

Matt Wickstrom, BDS Pre-Application Conference Coordinator, matt.wickstrom@portlandoregon.gov, 503-865-6513

Andy Gulizia, BDS Title 33 Team, andrew.gulizia@portlandoregon.gov

Ben Nielsen (for Grace Jeffreys), BDS Design/Historic Team, grace.jeffreys@portlandoregon.gov

Tammy Boren-King, PBOT Development Review, tammy.boren-king@portlandoregon.gov, 503-823-2948

Abigail Cermak, Bureau of Environmental Services, abigail.cermak@portlandoregon.gov, 503-823-7577

Bryan Vittori, Water Bureau, bryan.vittori@portlandoregon.gov, 971-917-5458

Amil Jakupovic, Urban Forestry, amil.jakupovic@portlandoregon.gov

Jeff Duquette, BDS Site Development, jeff.duquette@portlandoregon.gov, 503-823-8586

Marty Stockton, Portland Parks & Recreation, marty.stockton@portlandoregon.gov

Mary Hoffman, Bureau of Planning & Sustainability, mary.hoffman@portlandoregon.gov

Chandra Alvey, BDS Process Management, Chandra.alvey@portlandoregon.gov

Avi Tayar, ODOT DevRev Engineering Lead; abraham.tayar@odot.oregon.gov

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Jenna Bogert, DKS Associates Traffic, jenna.bogert@dksassociates,

Maria Rivero, Rivero Design, maria@riverodesign.com

Debbie Cleek, The Booking Group, cleek@bookinggroup.com, 503-789-3211

Hector Lopez, Portland Public Schools, hlopez@pps.net

Bonna Bezio, Portland Public Schools, Project Manager, dbezio@pps.net

Erik Gerdin, Portland Public Schools, egerding@pps.net

Alexi Brooks, KPFF Civil, alexi.brooks@kpff.com

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Don Baack, SW Trails, don.baack@swtrails.org



City of Portland, Oregon
Portland Permitting & Development
Land Use Services

David Kuhnhausen, Interim Director
Phone: (503) 823-7300
TTY: 711
www.portland.gov/ppd

Design Advice Request

DISCUSSION MEMO

Date: 9/12/2024
To: Portland Design Commission
From: Tanya Paglia, Design & Historic Review Team
503-865-6518 | Tanya.Paglia@portandoregon.gov
Re: EA 24-067517 – Ida B. Wells Modernization
Design Advice Request Memo – September 19, 2024

This memo is regarding the upcoming DAR on September 19, 2024 for the Ida B Wells High School Modernization project. The following supporting documents are available as follows:

- Drawings – accessed here: <https://efiles.portlandoregon.gov/Record/16990898/>
Note, Commissioners who requested hard copies will receive the drawing set by courier.

I. PROGRAM OVERVIEW

Design Advice Request (DAR) meeting for a proposal to replace the existing Ida B. Wells High School located in the Hillsdale Plan District with a new four-story building to accommodate approximately 1,700 students. The existing Rieke Elementary School buildings will remain on the site, as will an existing swimming pool, operated by Portland Parks & Recreation. Redevelopment across the approximately 35-acre site will include pedestrian/bicycle pathways, parking areas, a track and field with grandstand and support buildings, baseball and softball fields with associated site structures, and a pool support building to serve the existing public pool. The new building will be approximately 320,000 SF. It will offer a full range of high school facilities and will also accommodate a childcare center and a health clinic.

II. DEVELOPMENT TEAM BIO

Architect	Garrett Helm Bora Architecture & Interiors
Owner's Representative	Donna Bezio Portland Public Schools
Project Valuation	\$350,000,000

III. FUTURE DESIGN REVIEW APPROVAL CRITERIA:

- Portland Citywide Design Guidelines
- 33.825.040, Modifications that will better meet design review requirements

IV. POTENTIAL MODIFICATIONS

Subject to the following approval criteria:

- A. Better meets design guidelines. The resulting development will better meet the applicable design guidelines; and
- B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested; and
- C. Mitigation of impacts. Any impacts resulting from the modifications are mitigated to the extent practical.

Following Modifications may be requested:

1. Maximum Building Setbacks Street Lot Line (33.150.215.C): to allow an increase in the maximum setback to be greater than 10' along the SW Vermont St and SW Burlingame Ave lot lines. (this will be two Modifications – one for each frontage)
2. Vehicle Area Frontage Limitation (33.266.130.C.3.b): to allow more than 50% of the site's frontage adjacent to SW Burlingame Ave to be vehicle area.

Staff would like input from the commission on the proposed Modifications. Refer to the staff discussion related to Setbacks below under “Context – Placement and Massing of the Building,” and related to Vehicle Area Frontage Limitation under “Public Realm – Vehicle Area.”

V. STAFF ANALYSIS & RECOMMENDED DAR DISCUSSION TOPICS

Staff advise you consider the following among your discussion items on 9/19/2024:

CONTEXT

- **Northern campus approach - Sense of place.** The pedestrian experience for pedestrians/cyclists coming to the site from the SW Capitol Highway campus approach is too auto-oriented and lacks a sense of place and arrival. The northern campus approach is a key entry point to the site. In addition to welcoming students that live within walking/cycling distance arriving from this direction, it is where many students who ride the public bus will be coming from. It also connects the Hillsdale Commercial District to the site.

Arrival Plaza. The northern site entry should be improved to better great pedestrians/cyclists and ways to improve the sense of place at this key entry should be explored. The plaza that runs along the west side of the school is a step in the right direction, but should be widened at its northern end and extended northwards to better welcome pedestrians to the site. It should extend as close to the entry of the site as possible so that it is visible as students walk onto the campus and head south to the entry. The plaza would be stronger if not interrupted by the driveway leading to the swimming pool parking (see topic related to the north-south spine through site).

Crossings. Given the vehicle lane that runs through this area of the site, safe crossings are essential.

Pedestrian: The pedestrian crossing connecting to the arrival plaza should be wider than a standard striped crossing and include a change in materials and grade change to fully signal that it is a pedestrian area.

Cyclist: Because cyclists will likely be entering via the vehicle lane and not the sidewalk, adding a clearly marked bicycle crossing that takes cyclists from the north approach, across the auto area and to the arrival plaza should be explored.

Portland Citywide Design Guidelines: 01 – Build on the character, local identity, and aspiration of the place, 02 – Create Positive Relationship with surroundings, 03 – Integrate and enhance on-site features and opportunities to contribute to a location's uniqueness.

- **Placement and Massing of Building.** A number of site constraints are driving the placement of the new building including the old high school building (which must remain open while the new school is built), the existing Portland Parks and Recreation pool, and the site's topography. The new building is oriented southward towards SW Vermont St with the main entrance facing Vermont. Students arriving from all directions will need to make their way down to the south side of the building to enter.

The front of the building is set back from the street with hardscape directly in front of the building and a sloped grassy area connecting down to sidewalk. A Modification is required for the building to be set back more than 10' back from SW Vermont St (and also from SW Burlingame Ave). This added setback area allows for spill out space and an added buffer from the street. Staff has no concerns with the placement and 4-story massing of the building and would like commission feedback on placement, massing, and approvability of the setback Modification.

Portland Citywide Design Guidelines: 01 – Build on the character, local identity, and aspiration of the place, 02 – Create Positive Relationship with surroundings, 03 – Integrate and enhance on-site features and opportunities to contribute to a location's uniqueness.

- **Grandstand location.** The grandstand facing the track and field is shown to the west of the field on the site plan. However, the applicants are considering a placement to the east, between the field and the school. Staff believes the alternative eastern placement of the tall structure would create a visual barrier along the plaza and would like the commission provide feedback on the two options.

Portland Citywide Design Guidelines: 02 – Create Positive Relationship with surroundings, 03 – Integrate and enhance on-site features and opportunities to contribute to a location's uniqueness, 07 – Support the comfort, safety, and dignity of residents, workers, and visitors through thoughtful site and building design.

PUBLIC REALM

- **North-south pedestrian connection.** As a very large campus site that is regularly traversed by not just students, but by many from the surrounding area, great connections through the site are essential. In addition to local residents that may need to cut across the large site, many will be heading to on-site destinations such as the high school main entrance, the elementary school main entrance, the Portland Parks & Recreation public pool, the fields for events, the local farmer's market, etc.

While the proposed site plan provides a number of valuable new trails through the site, there is room to improve the site's central north-south pedestrian spine. This will be a key connector for various pedestrians, as well as a key access point to the high school main entrance for people arriving from the north (as noted in "Context – Northern Campus Approach"). In its present configuration, the spine is interrupted by the driveway to the pool parking lot. Were this driveway shifted east, it would strengthen this north-south connector.

Portland Citywide Design Guidelines: 01 – Build on the character, local identity, and aspiration of the place, 02 – Create Positive Relationship with surroundings, 03 – Integrate and enhance on-site features and opportunities to contribute to a location's uniqueness, 07 – Support the comfort, safety, and dignity of residents, workers, and visitors through thoughtful site and building design.

- **Connections through the site.** Beyond the north-south spine discussed above, staff would like commission feedback on the overall circulation provided throughout the site for all modes.

Portland Citywide Design Guidelines: 01 – Build on the character, local identity, and aspiration of the place, 02 – Create Positive Relationship with surroundings, 03 – Integrate and enhance on-site features and opportunities to contribute to a location's uniqueness, 07 – Support the comfort, safety, and dignity of residents, workers, and visitors through thoughtful site and building design.

- **Relationship with the Street.** While the building design is still in an early stage, staff would like early feedback on the relationship of the front of the building with the street. Staff would like feedback on whether the preliminary design with a colonnaded area along the ground floor of the front facade and a hardscape plaza behind a grassy knoll is on the right track.

Portland Citywide Design Guidelines: 01 – Build on the character, local identity, and aspiration of the place, 02 – Create Positive Relationship with surroundings, 03 – Integrate and enhance on-site features and opportunities to contribute to a location's uniqueness, 04 – Design the sidewalk level of buildings to be active and human-scaled, 05 – Provide opportunities to pause, sit, and interact.

- **Vehicle Area at Street Frontages.** The site has vehicle area along two street frontages:
 - SW Burlingame Ave to the north where there is a drive aisle that loops from the northern entry point of the site around the north and east perimeters of the site, and
 - SW Vermont St to the south where a parking area is located in the southeast corner of the site.

While the southeast parking location seems appropriate for the large campus setting, Staff would like commission feedback on how heavily the lot should be screened with landscaping to improve the pedestrian experience along SW Vermont St. Staff would also like commission feedback on the overall amount of vehicle area including whether there is support for the Modification request related to allowing more than 50% of the site's frontage adjacent to SW Burlingame Ave to be vehicle area.

Portland Citywide Design Guidelines: 02 – Create Positive Relationship with surroundings, 06 – Integrate and minimize the impact of parking and building services, 07 – Support the comfort, safety, and dignity of residents, workers, and visitors through thoughtful site and building design, 09 – Design for resilience, health, and stewardship of the environment, ensuring adaptability to climate change and the evolving needs of the city.

QUALITY & PERMANENCE

- **Materials.** While exterior materials are not yet indicated, it would be worthwhile for the commission to provide preliminary comments on the types of materials that would be most appropriate for the building.

Portland Citywide Design Guidelines: 01 – Build on the character, local identity, and aspiration of the place, 02 – Create Positive Relationship with surroundings, 08 – Design for quality, using enduring materials and strategies with a coherent approach, 09 – Design for resilience, health, and stewardship of the environment, ensuring adaptability to climate change and the evolving needs of the city.

- **Overall Composition.** While the design of the building is not the focus of this DAR as it is at a conceptual stage, the composition, massing and scale seem appropriate to the site's context. Early feedback from the commission would be helpful as the design continues to develop.

Portland Citywide Design Guidelines: 01 – Build on the character, local identity, and aspiration of the place, 02 – Create Positive Relationship with surroundings, 03 – Integrate and enhance on-site features and opportunities to contribute to a location's uniqueness, 04 – Design the sidewalk level of buildings to be active and human-scaled, 08 – Design for quality, using enduring materials and strategies with a coherent approach, 09 – Design for resilience, health, and stewardship of the environment, ensuring adaptability to climate change and the evolving needs of the city.

9/19/24 - DESIGN COMMISSION HEARING

LU 24-061104 DZM – 901 NE Lloyd Center Concert Venue

ATTENDEES - *NO TESTIMONY (subject to change)*

FIRST NAME	LAST NAME	EMAIL	ADDRESS	ZIP	WOULD YOU LIKE TO TESTIFY	ARE YOU FOR OR AGAINST
Christe	White	cwhite@radlerwhite.com	111 SW Columbia Street, Suite 700	97201	YES	FOR
Zeke	Pratchett	pratchett.zt@gmail.com	1107 NE 9th Ave, Apt 302	97232	NO	FOR
Krista	Bailey	kristab@urbanrengroup.com	720 SW Washington, Suite 640	97205	NO	FOR
Lajune	Thorson	lajune.thorson@gmail.com	255 SW Harrison St.	97201	NO	N/A
Joey	Shoemaker	joeys@urbanrengroup.com	720 SW Washington St., Suite 640	97205	NO	FOR
Thomas	Kilbane	tom@urbanrengroup.com	720 SW Washington St. Suite 640	97205	NO	FOR
Grayson	Schoenbine	grayson@worksarchitecture.net	811 SE Stark S210	97214	NO	FOR
KIM	JOHNSON	KIM@7OAKSENGINEERING.COM	345 WESTFIELD ST. #107	97381	NO, N/A	FOR
kyle	andersen	kyle@gbdachitects.com	1120 NW Couch Street, 300	97209	NO	N/A
Emily	Mandic	emandic@americanassets.com	700 NE Multnomah St	97232	NO	FOR
S	Rath	nongvini@fmailbox.com	537 N. Buffalo Street	97217	NO	N/A
Caroline	Cilek	carolinecilek@dwt.com	560 SW 10th Avenue, Suite 700	97205	NO	FOR
Joshua	Baker	Joshua@ecolloyd.org	2203 Lloyd Center	97232	NO	FOR

EA 24-067517 DA – Ida B. Wells Modernization

ATTENDEES - *NO TESTIMONY (subject to change)*

FIRST NAME	LAST NAME	EMAIL	ADDRESS	ZIP	WOULD YOU LIKE TO TESTIFY	ARE YOU FOR OR AGAINST
Lajune	Thorson	lajune.thorson@gmail.com	255 SW Harrison St.	97201	NO	FOR
Hilary	Dorsey	hdorsey@djcoregon.com	1618 SW First Ave., Suite 400	97201	NO	N/A
kyle	andersen	kyle@gbdachitects.com	1120 NW Couch Street, 300	97209	NO	N/A



City of Portland
Design Commission

Design Advice Request

EA 24-067517 DA

Ida B. Wells Modernization 1151 SW Vermont St

September 19, 2024

Staff Presentation

Staff Introduction

Applicant Presentation

Staff Discussion Topics

Public Comments

Commission Discussion



Base & Overlay Zones:

- IRd, Institutional Residential, Design Review Overlay

- R7, Residential 7,000
- OS, Open Space Zone
- CM2, Commercial/Mixed Use 2

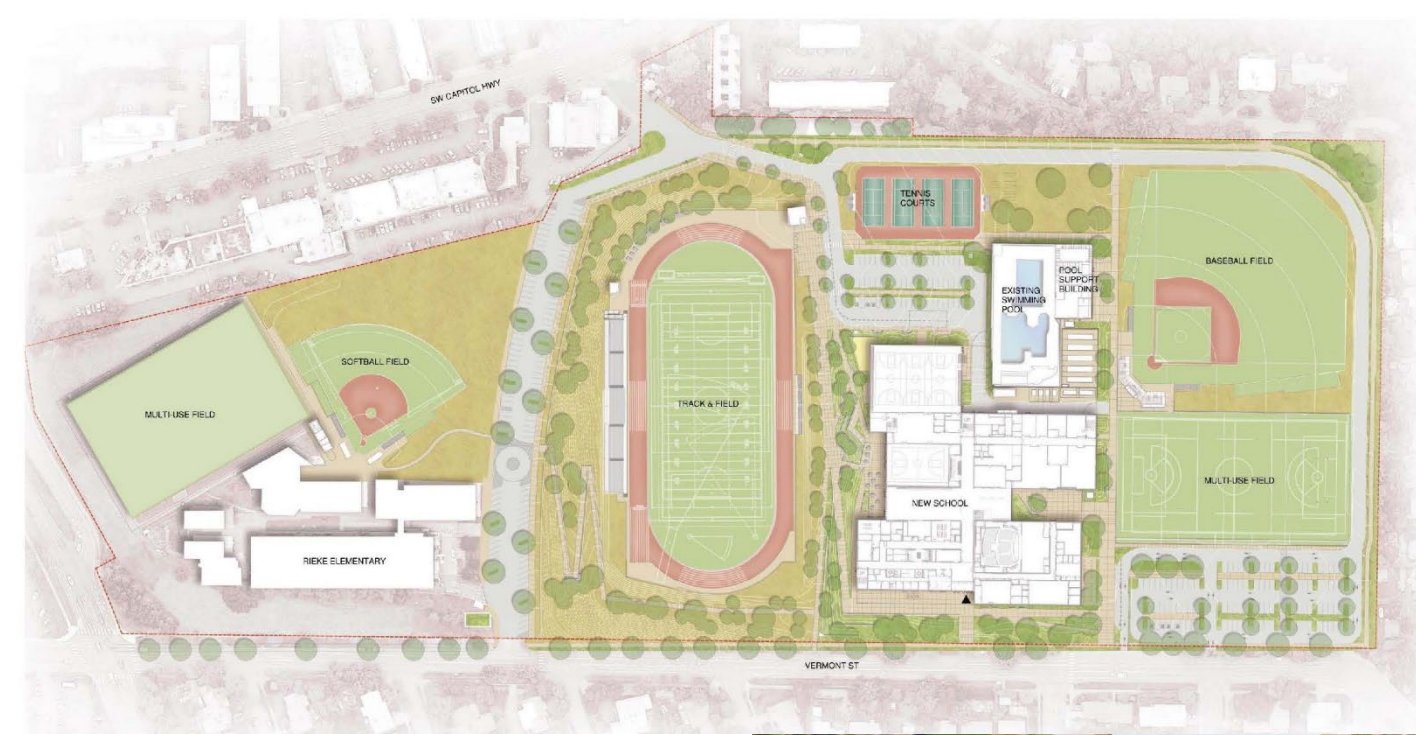
- Portland Citywide Design Guidelines
- PZC 33.825.040 – Modifications Considered During Design Review

- 75' max in IR zones

- 2:1 in IR zones

35.38 acres (1,541,152 SF)

Zoning



Modifications:

- Maximum Building Setbacks Street Lot Line (33.150.215.C): to allow an increase in the maximum setback to be greater than 10' along the SW Vermont St and SW Burlingame Ave lot lines. (this will be two Modifications – one for each frontage)
- Vehicle Area Frontage Limitation (33.266.130.C.3.b): to allow more than 50% of the site's frontage adjacent to SW Burlingame Ave to be vehicle area.

Zoning

Hillsdale Town Center Plan (1997)

“The Hillsdale community is located in Southwest Portland and centers around the commercial area along SW Capitol Highway. The commercial area of Hillsdale provides a mix of retail, service and office uses that serve both local and regional consumers.”



Context





View of site from the south from SW Vermont St looking north



View of site from the south from SW Vermont St looking northwest



View of site from the north from SW Sunset Blvd/SW Capitol Hwy looking south



View of site from the north from SW Burlingame looking southwest



View from the road through the center of the site looking east

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Context

- Northern campus approach – sense of place
- Placement and massing of the building
- Grandstand location

Public Realm

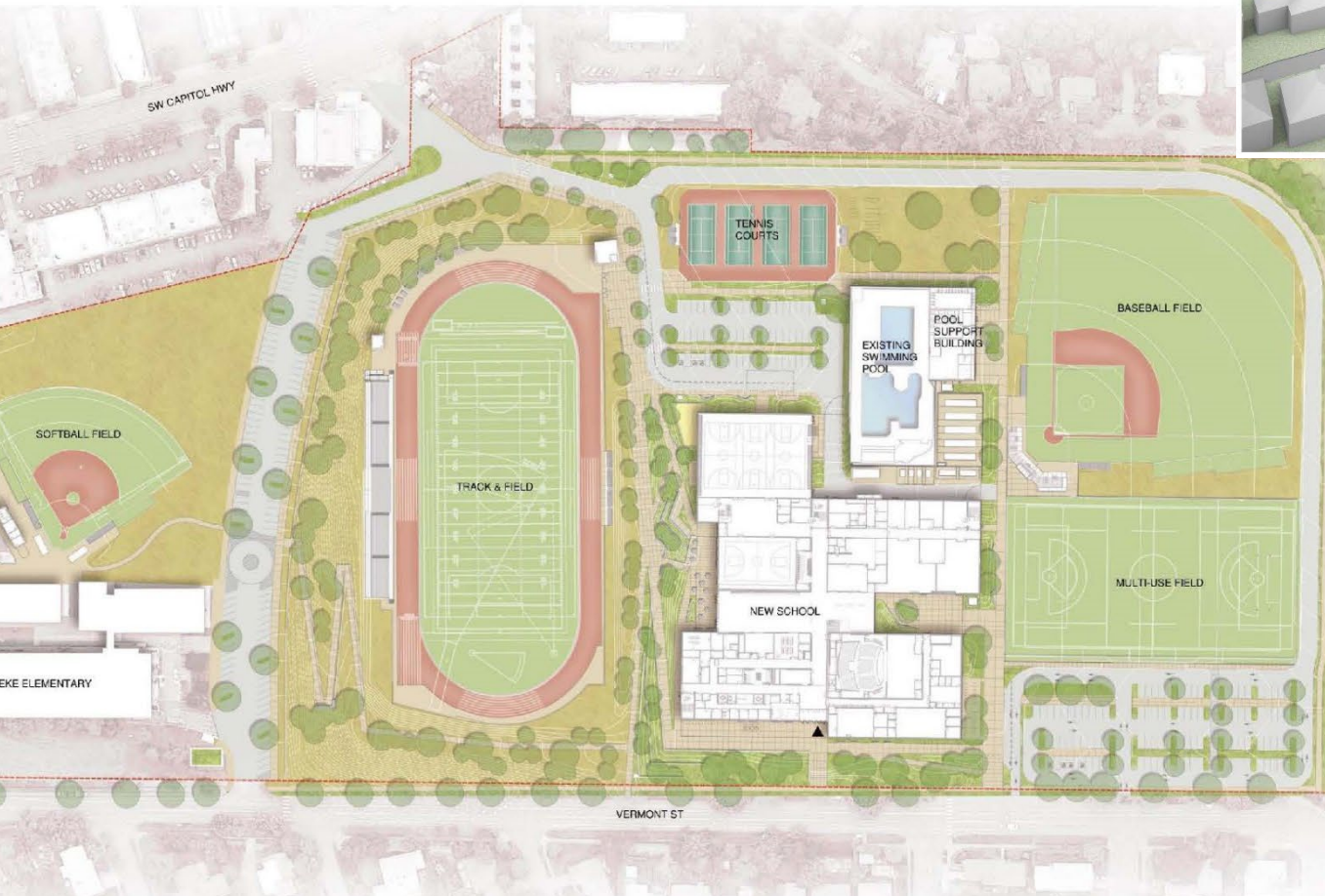
- North-south pedestrian connection
- Connections through the site
- Relationship with the street
- Vehicle area at street frontages

Quality & Permanence

- Materials
- Overall composition

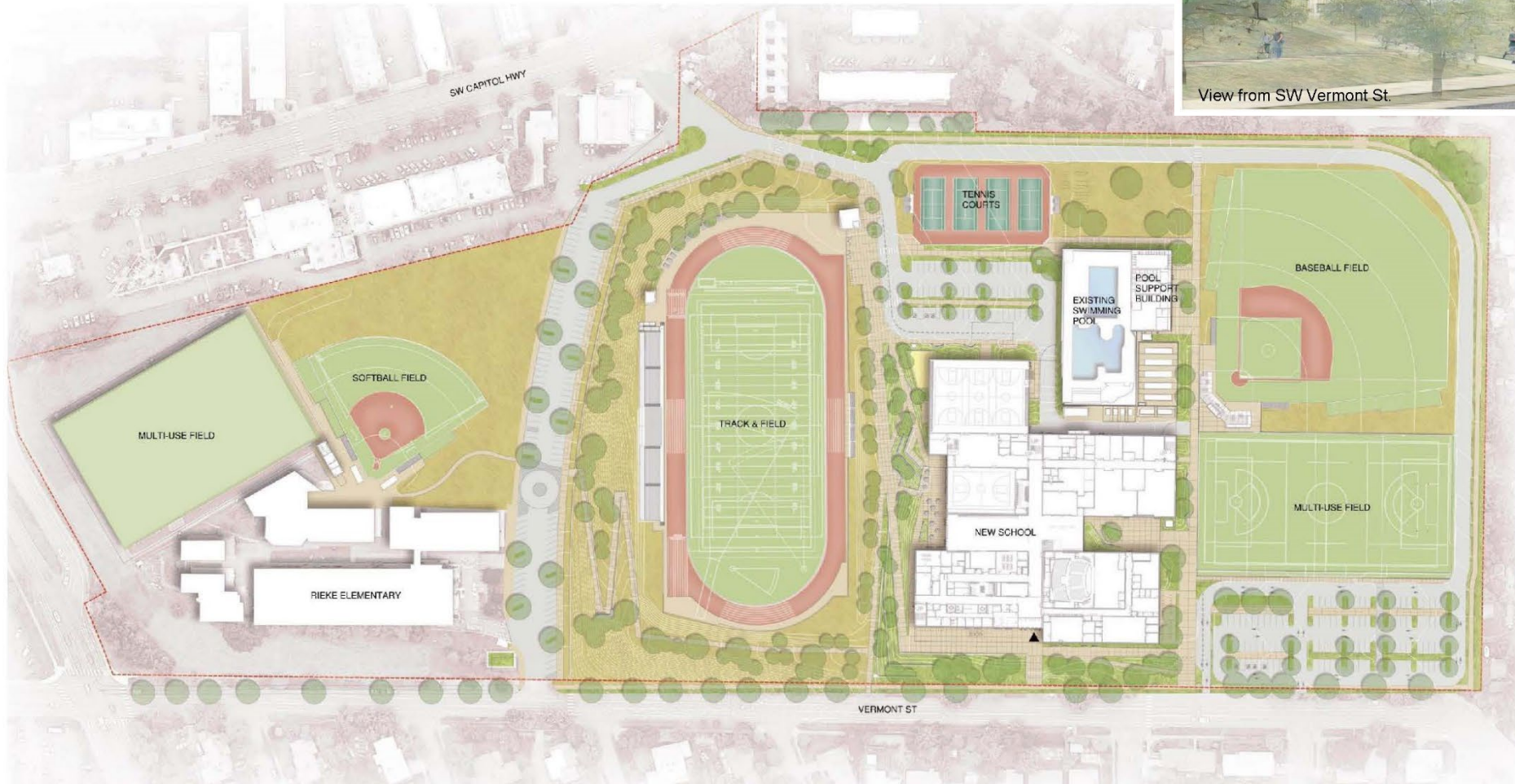
Context

- Northern campus approach – sense of place
- Placement and massing of the building
- Grandstand location



Public Realm

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Quality & Permanence

- Materials
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West Facade



View from SW Vermont St

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Portland Public Schools

Ida B. Wells High School Modernization

Design Advice Request Meeting #1
September 19, 2024



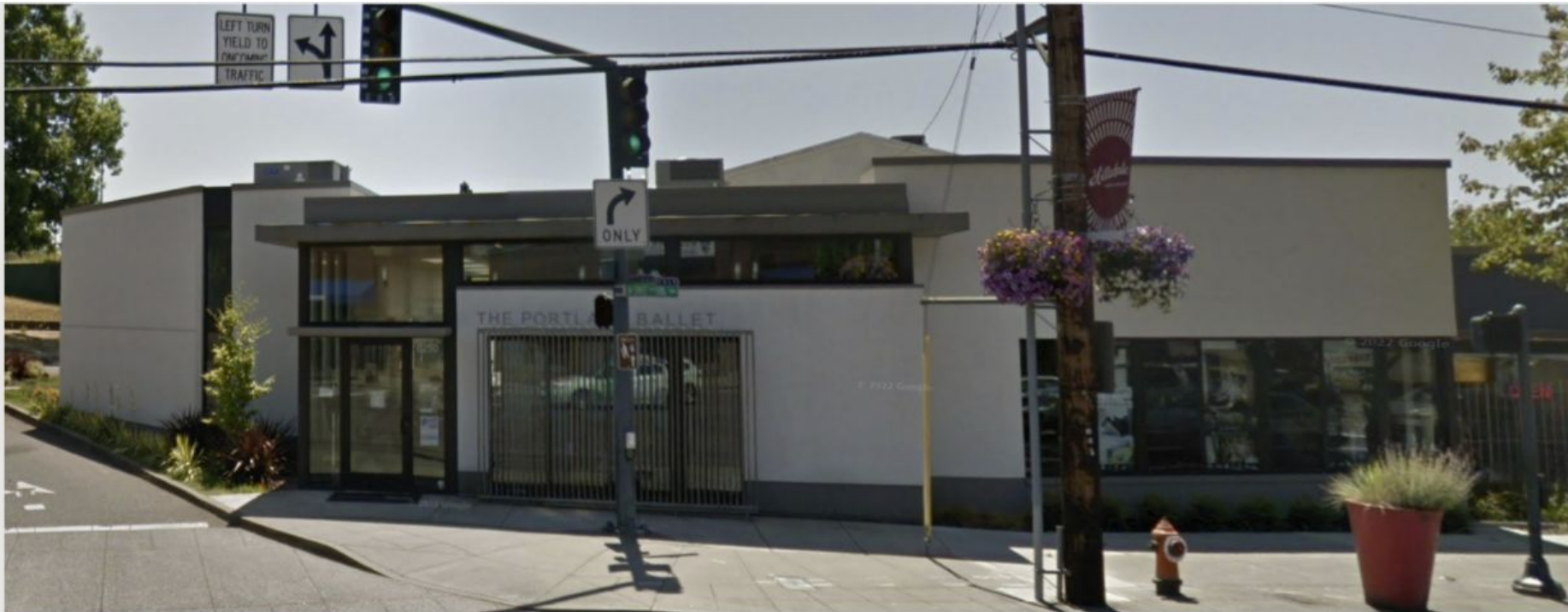
Site Overview



SITE



SITE - BUILT CONTEXT



SITE - BUILT CONTEXT



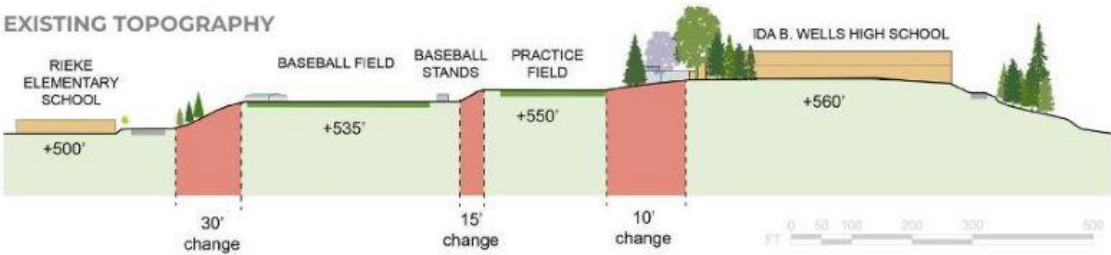
SITE - NATURAL CONTEXT



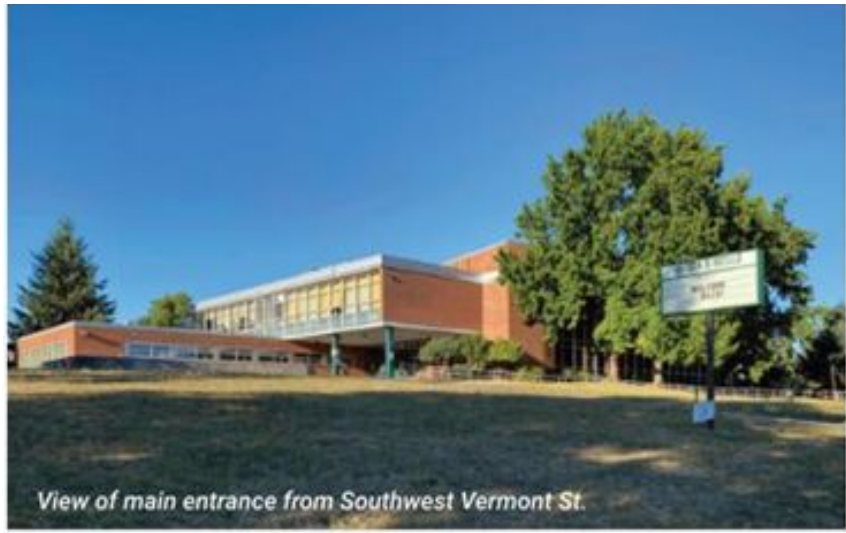
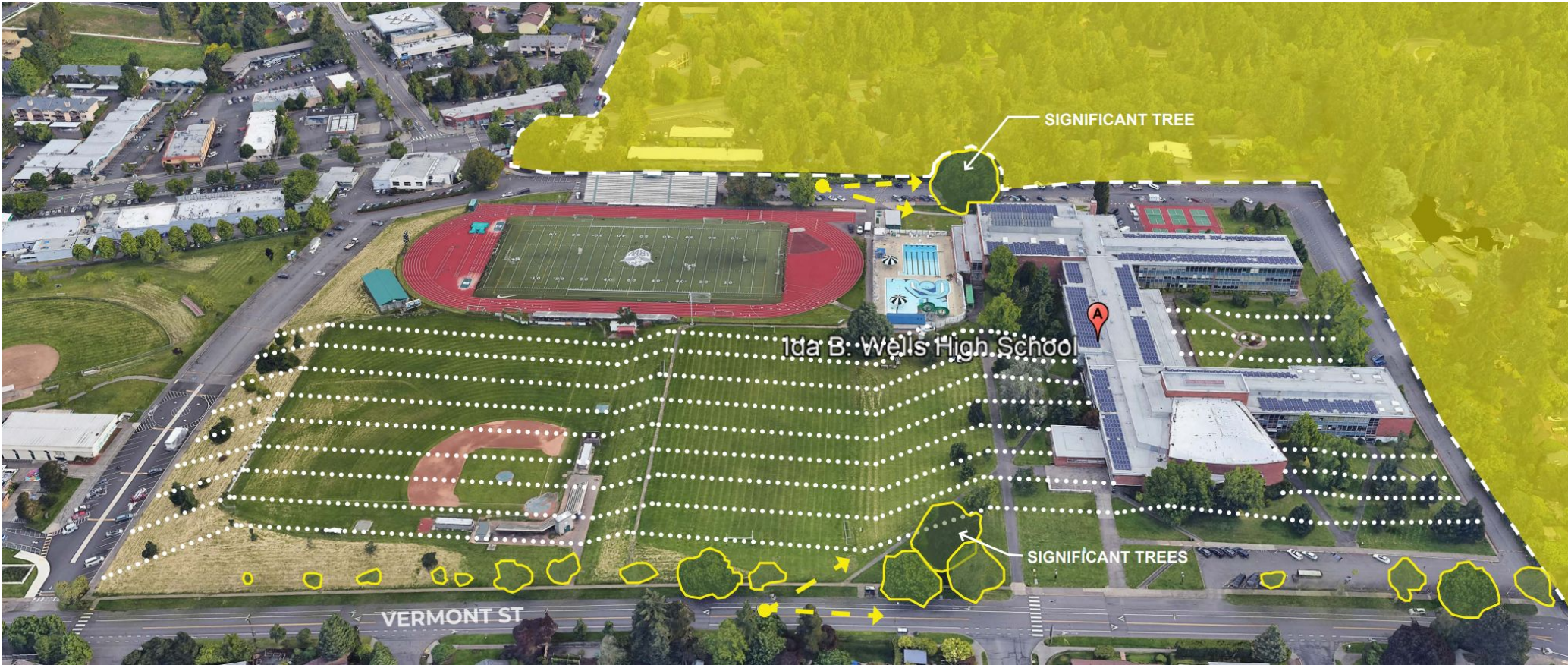
SITE - TOPOGRAPHY



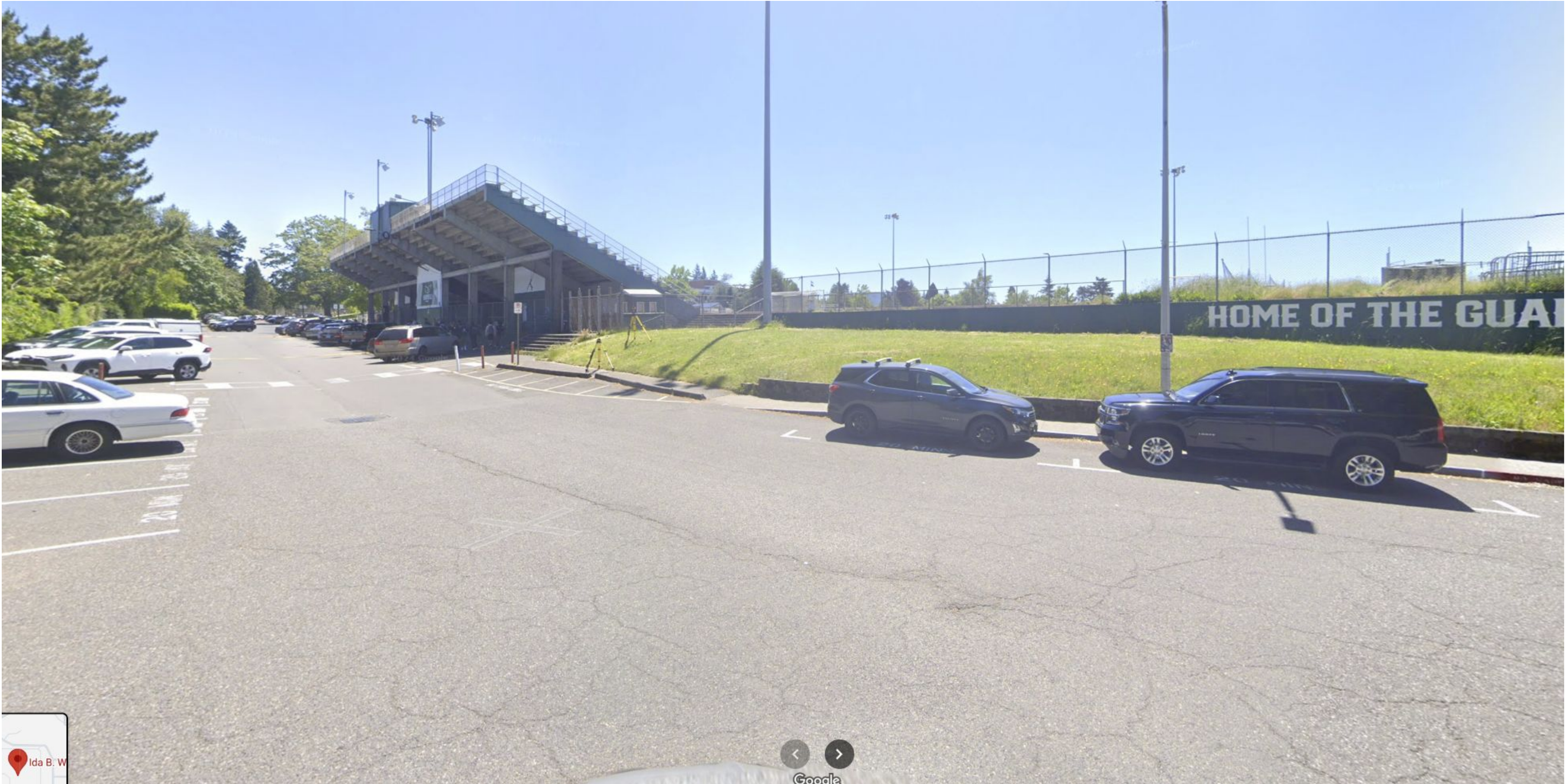
DESIGN DRIVERS:
TOPOGRAPHY
ENHANCE THE ECOLOGY OF THE SITE
CONNECTIVITY



DRIVING FACTORS FOR PLANNING AND DESIGN



SITE - EXISTING APPROACH FROM CAPITOL HIGHWAY



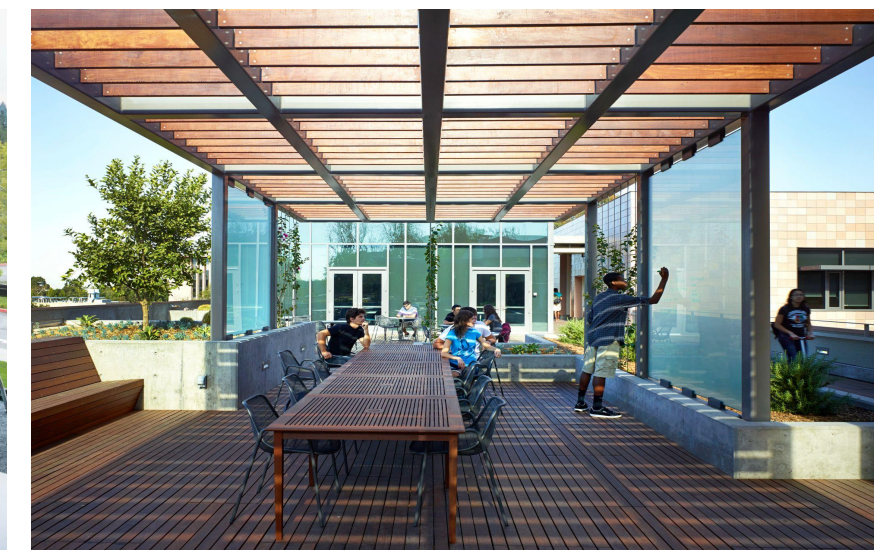
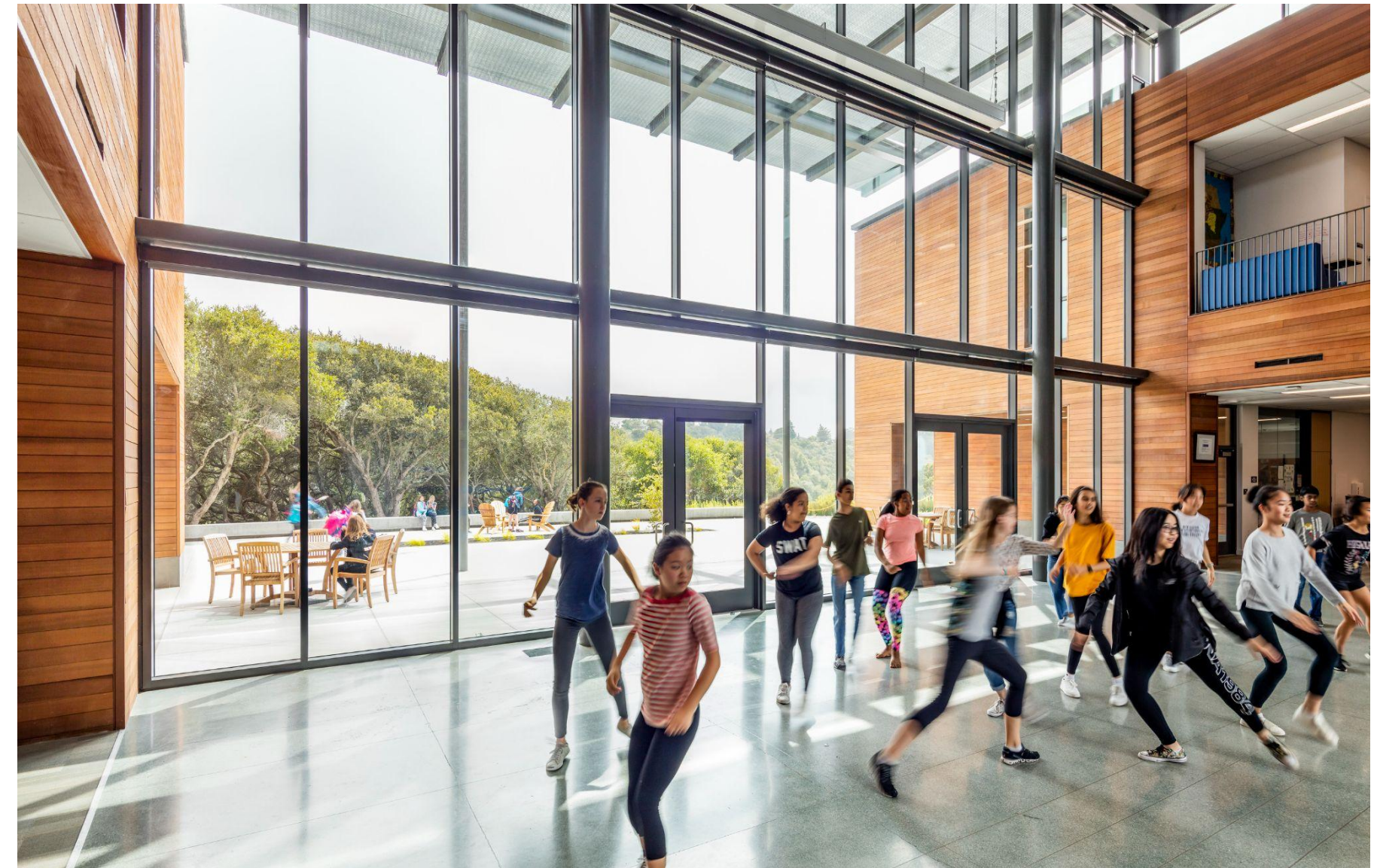
Comprehensive Planning Phase



GUIDING PRINCIPLES

1. STUDENT HEALTH (INTELLECTUAL, PHYSICAL + MENTAL)

- a. **SUPPORT** learning with great daylighting, healthy indoor air quality and excellent acoustics, borrowing the principles of biophilic design to achieve a welcoming environment.
- b. **CREATE** a bold, flexible teaching environment that will inspire and support a variety of learning styles well into the future.
- c. **DEVELOP** dynamic habitats for teenagers and teachers, supporting their social need to connect with one another as part of the path to teaching and learning success.
- d. **GATHER** students, faculty, and staff in a safe environment where they feel a sense of pride and belonging, coalescing the community within a central “heart” while creating a variety of flex spaces to offer choice.



GUIDING PRINCIPLES

2. EQUITY + INCLUSION

- a. **LIFT** the voices of a diverse student body, empowering and making visible the many cultures within the community through meaningful, equity-informed, impactful engagement.
- b. **SEEK** input from a broad set of voices throughout the process of design, respecting the truth of lived experience while elevating the most marginalized members of the community through transparency and trust-building activities.
- c. **CONNECT** to the broader business and residential district by making the new school a focal point and beacon of activity in SW Portland – supporting the financial health of nearby retailers while preserving security and safety for the student population.



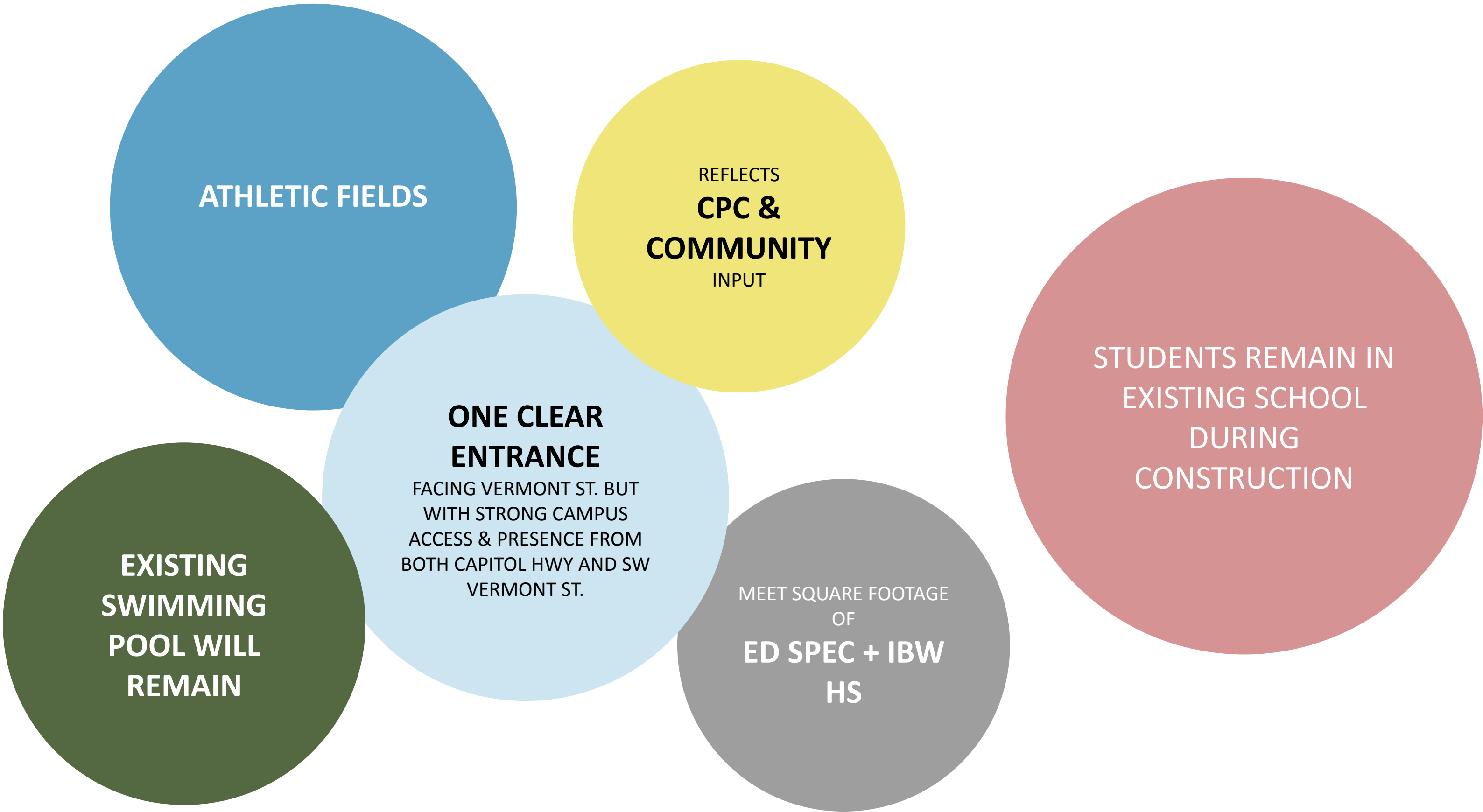
GUIDING PRINCIPLES

3. JUSTICE

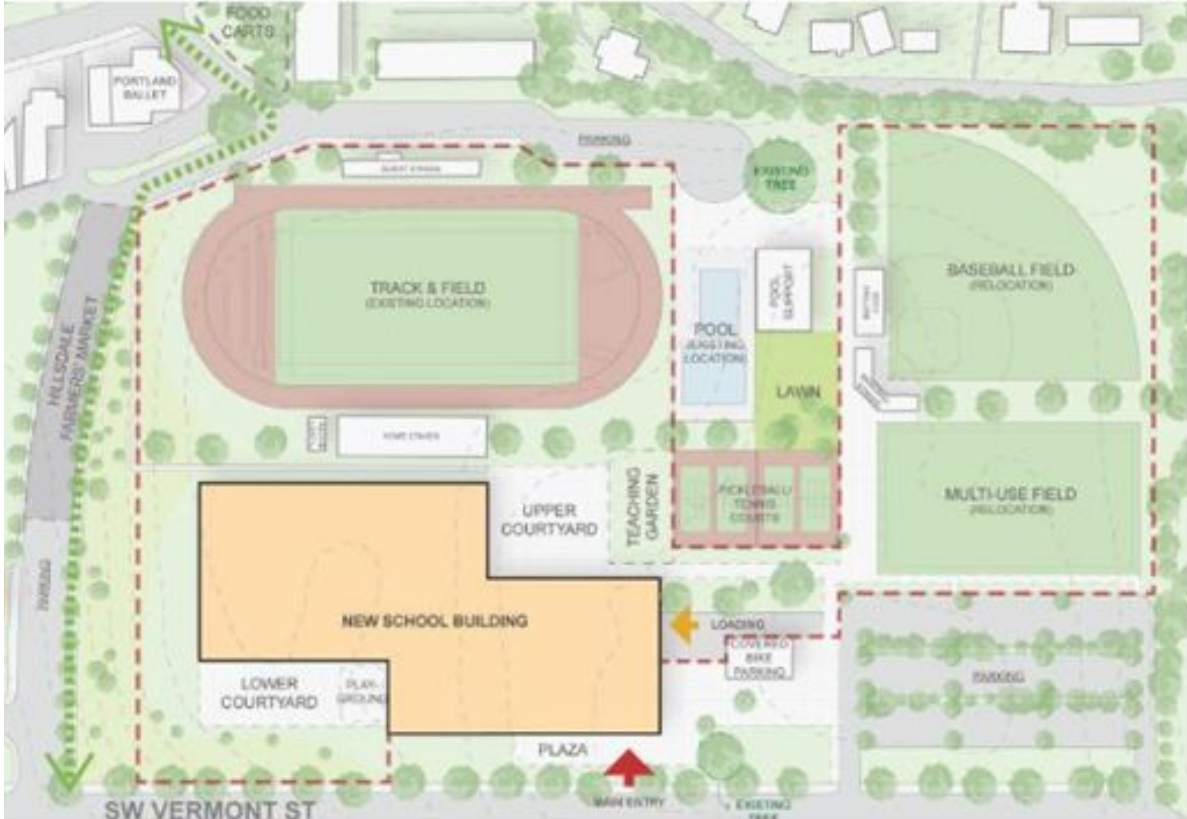
- a. **HONOR** the legacy of justice established by Ida B Wells through design, art and storytelling within the building and on the site.
- b. **LEAD** by example in creating one of the most sustainable schools in the country, fully compliant with the PPS Climate Crisis Response Policy while employing simple and easily maintained systems within enduring functional spaces and being stewards of taxpayer dollars.
- c. **EMBRACE** the lens of disability justice to create a school that is universally accessible, going beyond code to create a physical place of inclusion at the site and building scale.



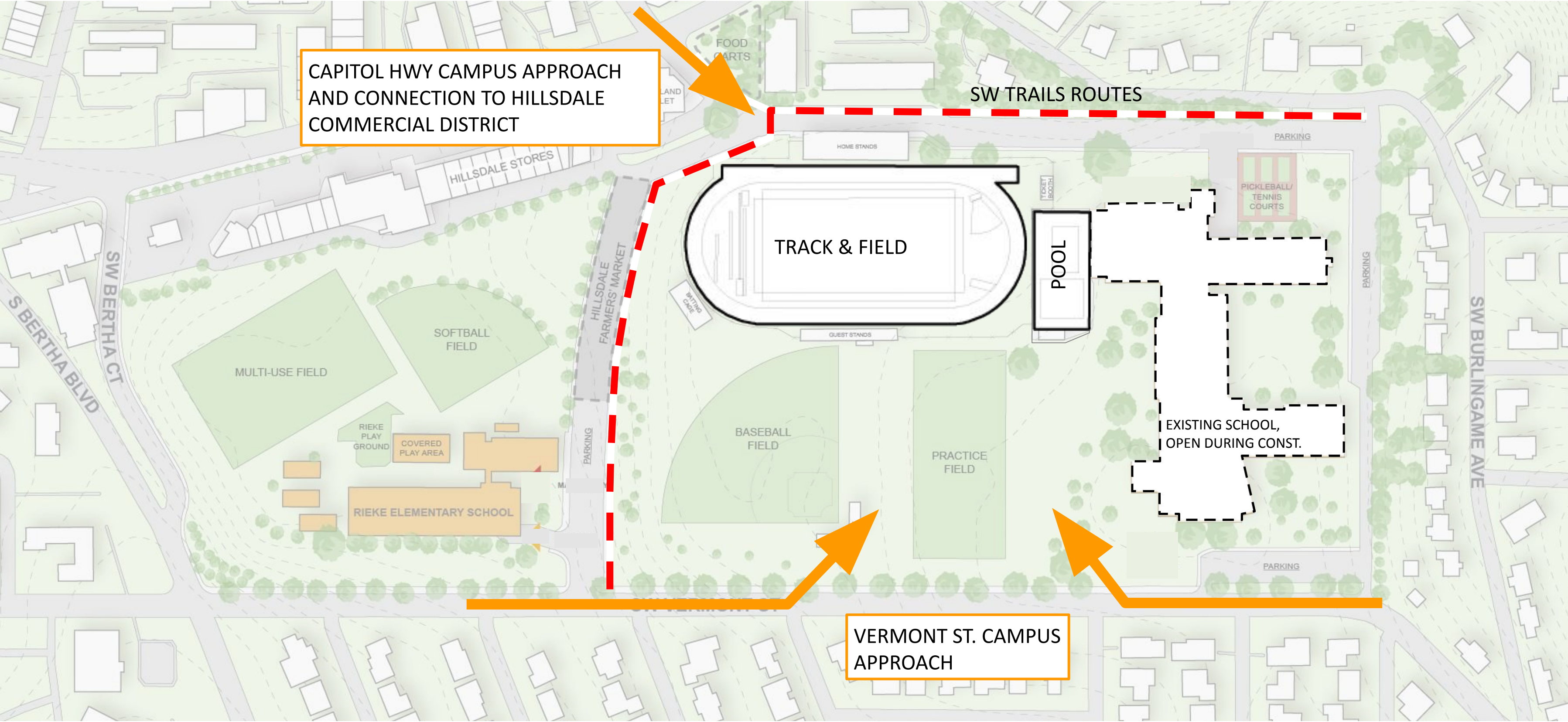
PLANNING PHASE - KEY DESIGN DRIVERS



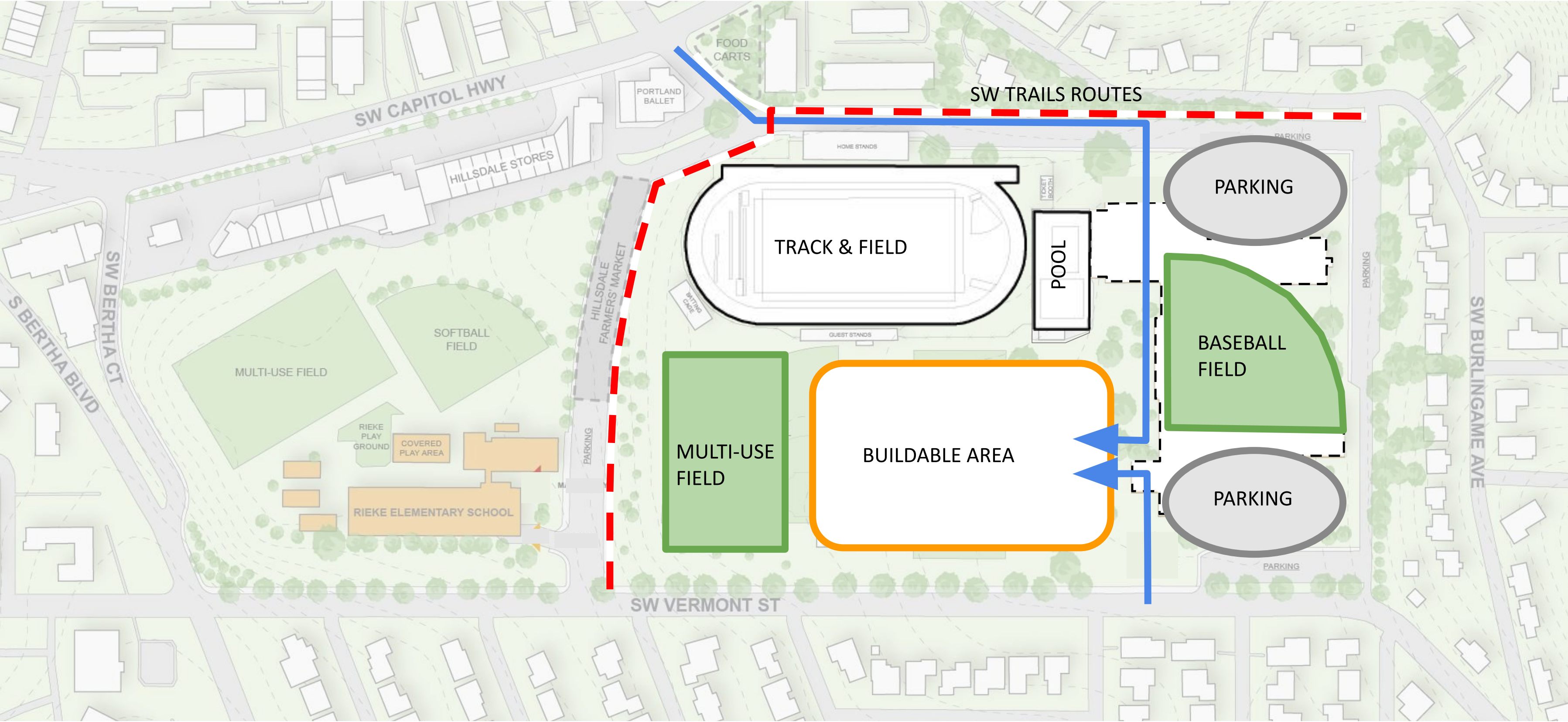
PLANNING PHASE - EARLY SITE PLAN STUDIES



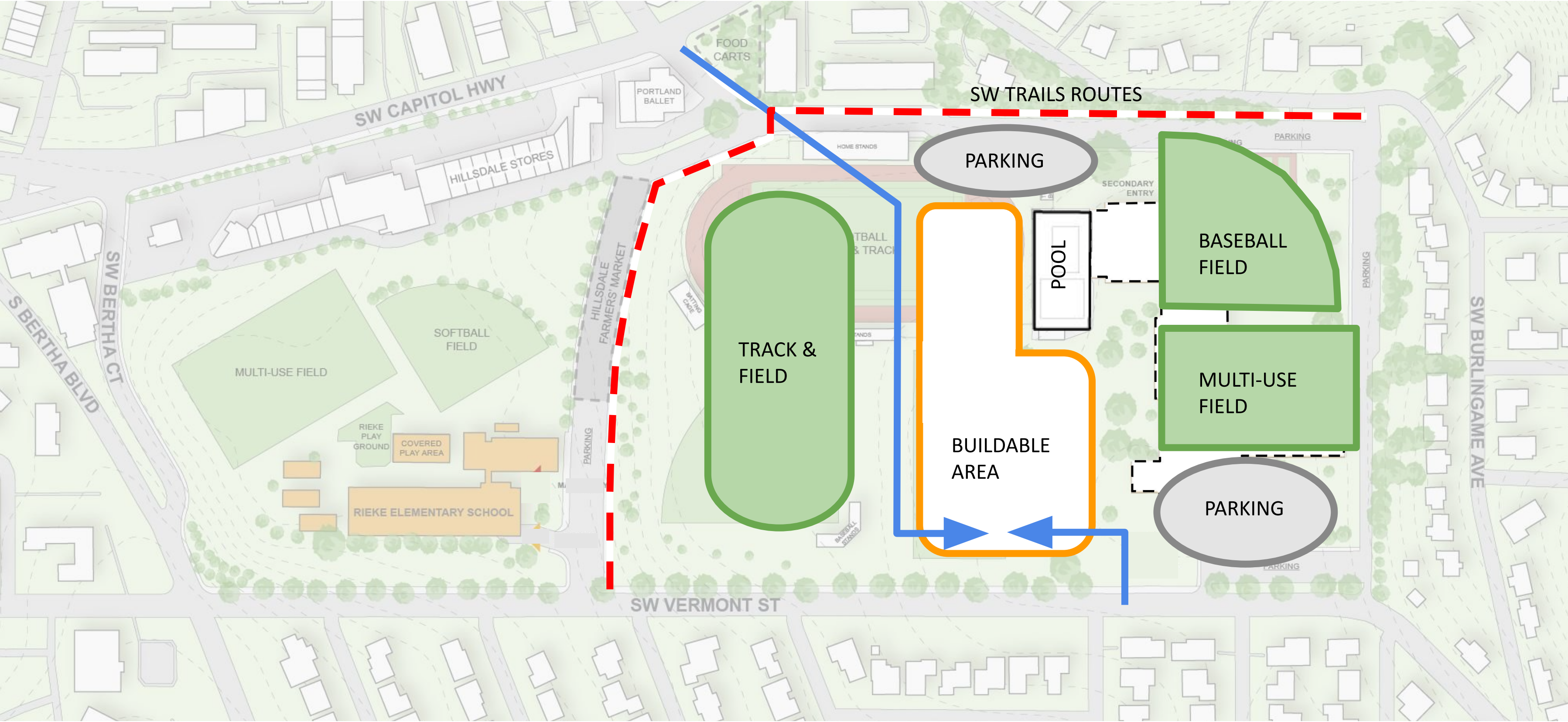
EXISTING SITE - KEY DESIGN DRIVERS



PLANNING PHASE - BUILDABLE AREA & ARRIVAL, SCHEME 1

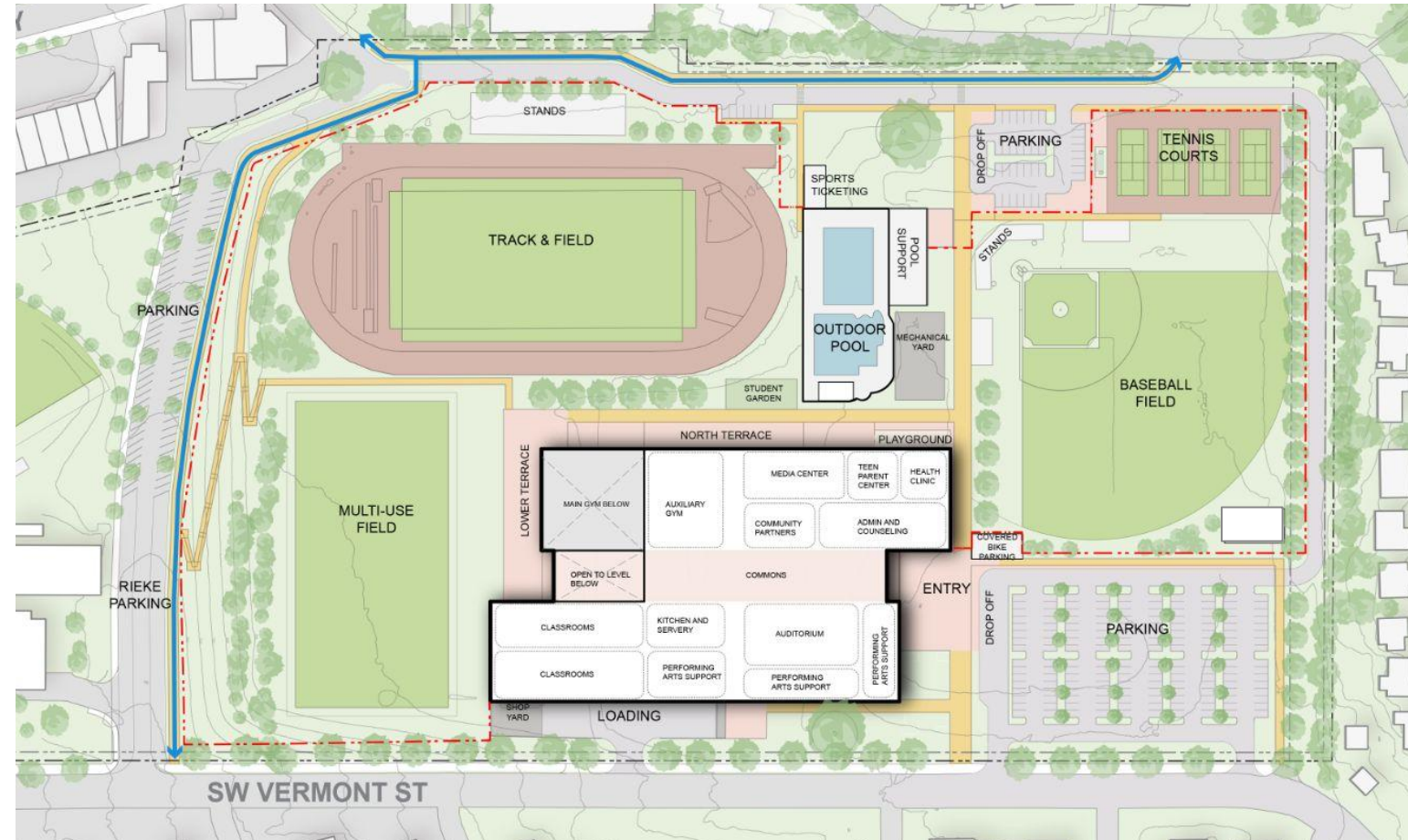


PLANNING PHASE - BUILDABLE AREA & ARRIVAL, SCHEME 2



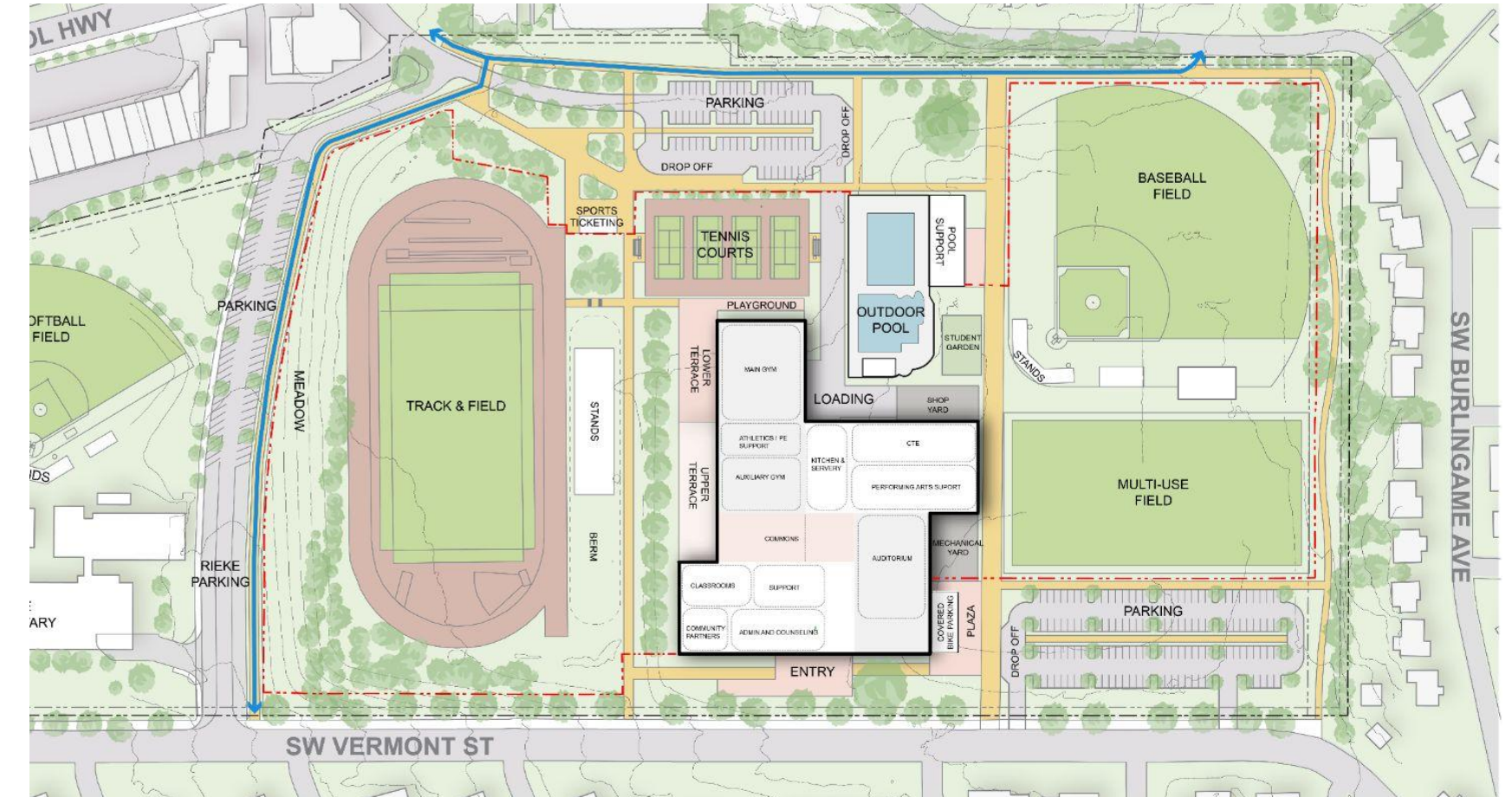
PLANNING PHASE - FINAL OPTIONS

SCHEME 1



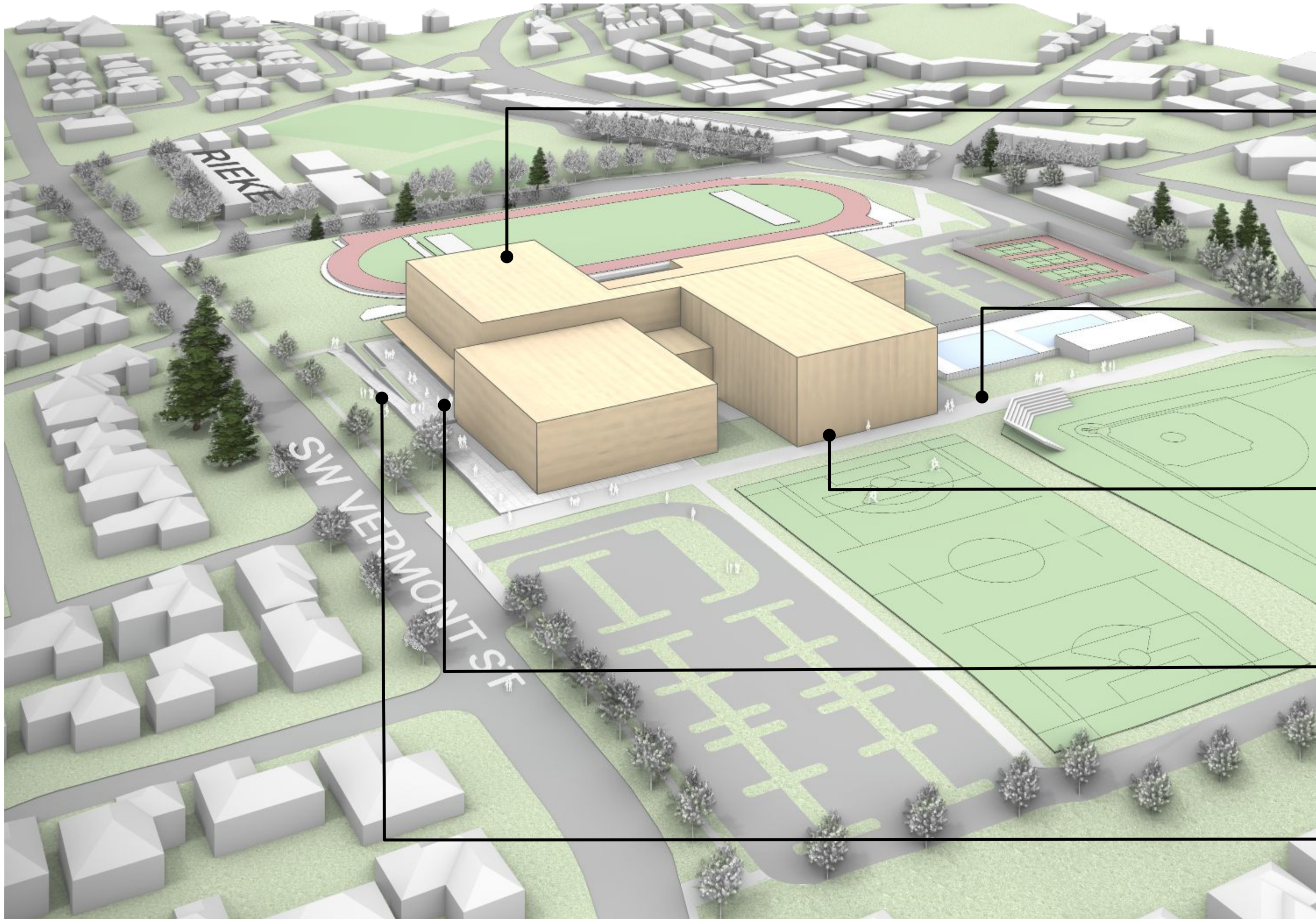
- Track & Field remains in place with improvements
- Minimal Track & Field disruption during construction
- Pathway connection to Rieke parking and fields
- Main parking lot and drop-off near building entry
- Vehicular connection between the two parking lots
- Separation between school and pool offers flexibility with building footprint
- 3-story building height

SCHEME 2



- Track & Field in optimal N-S orientation
- More space for construction staging
- Direct pedestrian connection to Capitol Highway
- Parking lots and drop-offs near Capitol Hwy and Vermont St.
- Vehicular access at opposite ends of building
- Less flexibility with building footprint due to pool and track locations
- 4-story building height

BUILDING MASSING - SCHEME 2



Building orientation and massing allows for **maximum daylighting and minimizes western sun exposure**, reducing building energy consumption and eliminating solar discomfort

Pathways through campus **connect to the SW Trails network and the broader business and residential district**, making the school a beacon of activity in SW Portland

Building massing is optimal for a timber structure, allowing for **reduced embodied carbon emissions, improved indoor air quality, and biophilic design** opportunities.

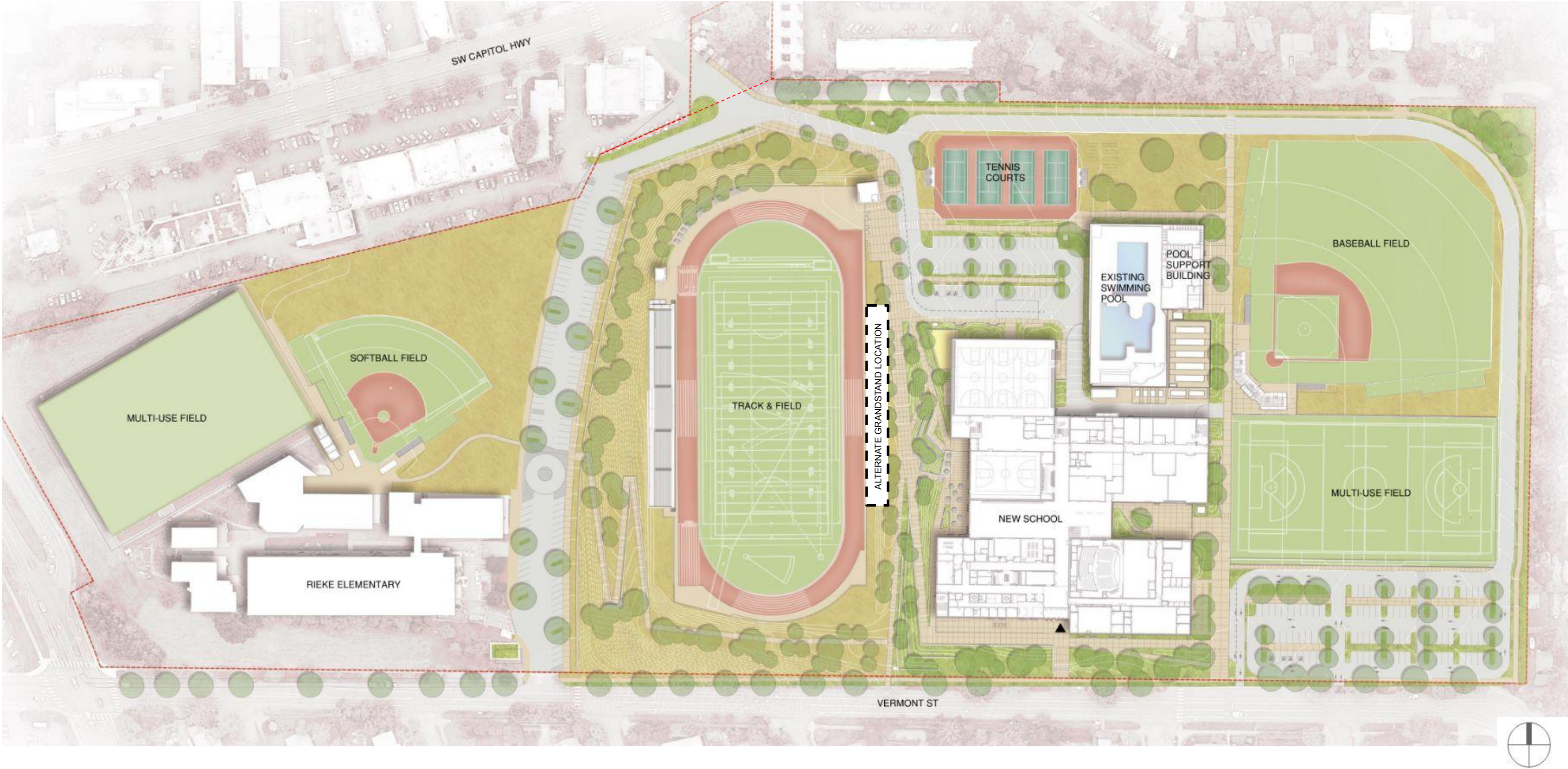
Main entrance faces Vermont St. with direct pathways **connecting it to both campus approaches** and parking lots

Site configuration allows for universal accessibility, going beyond code to create a **physical place of inclusion** at every scale

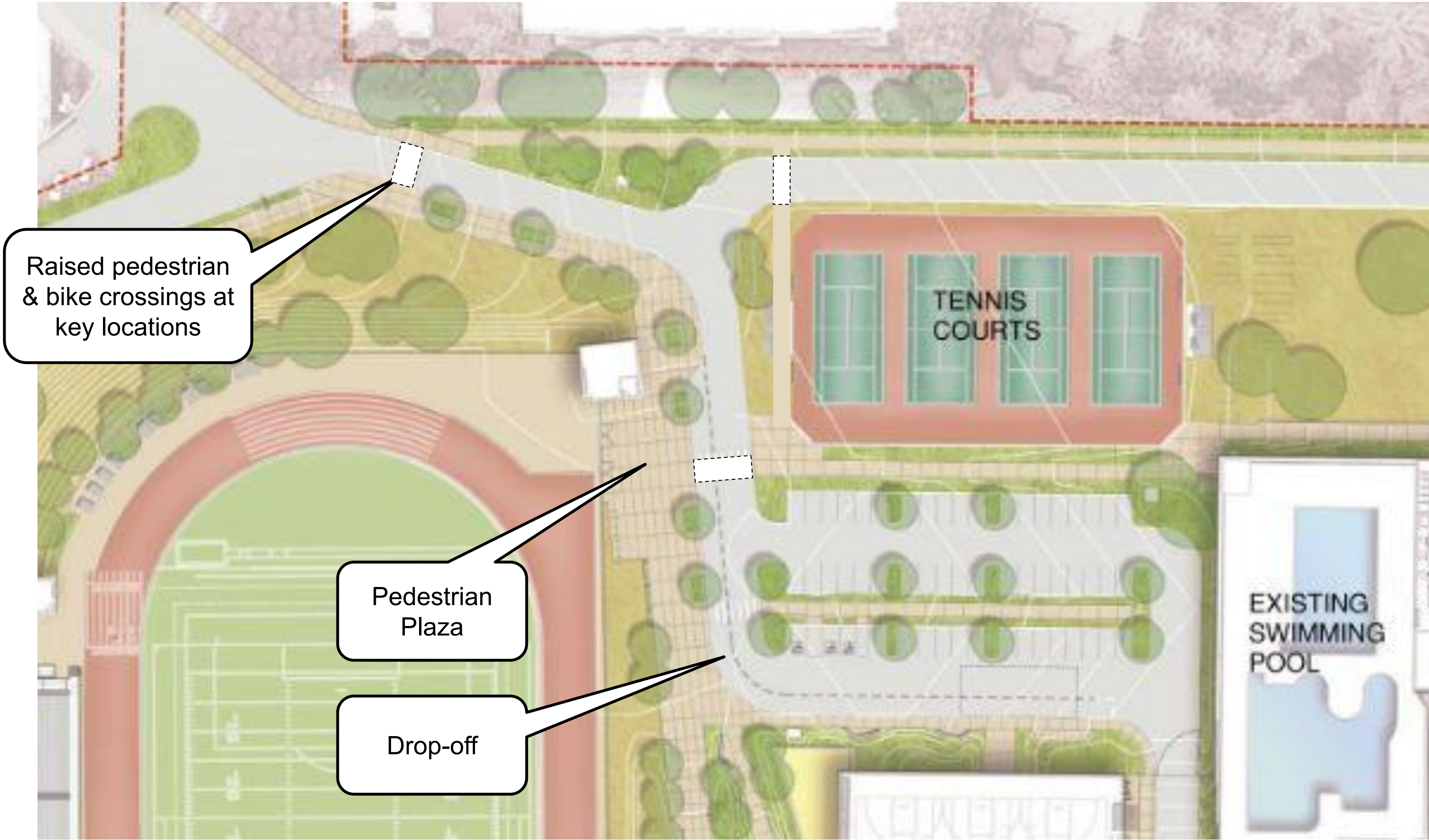
Current Design



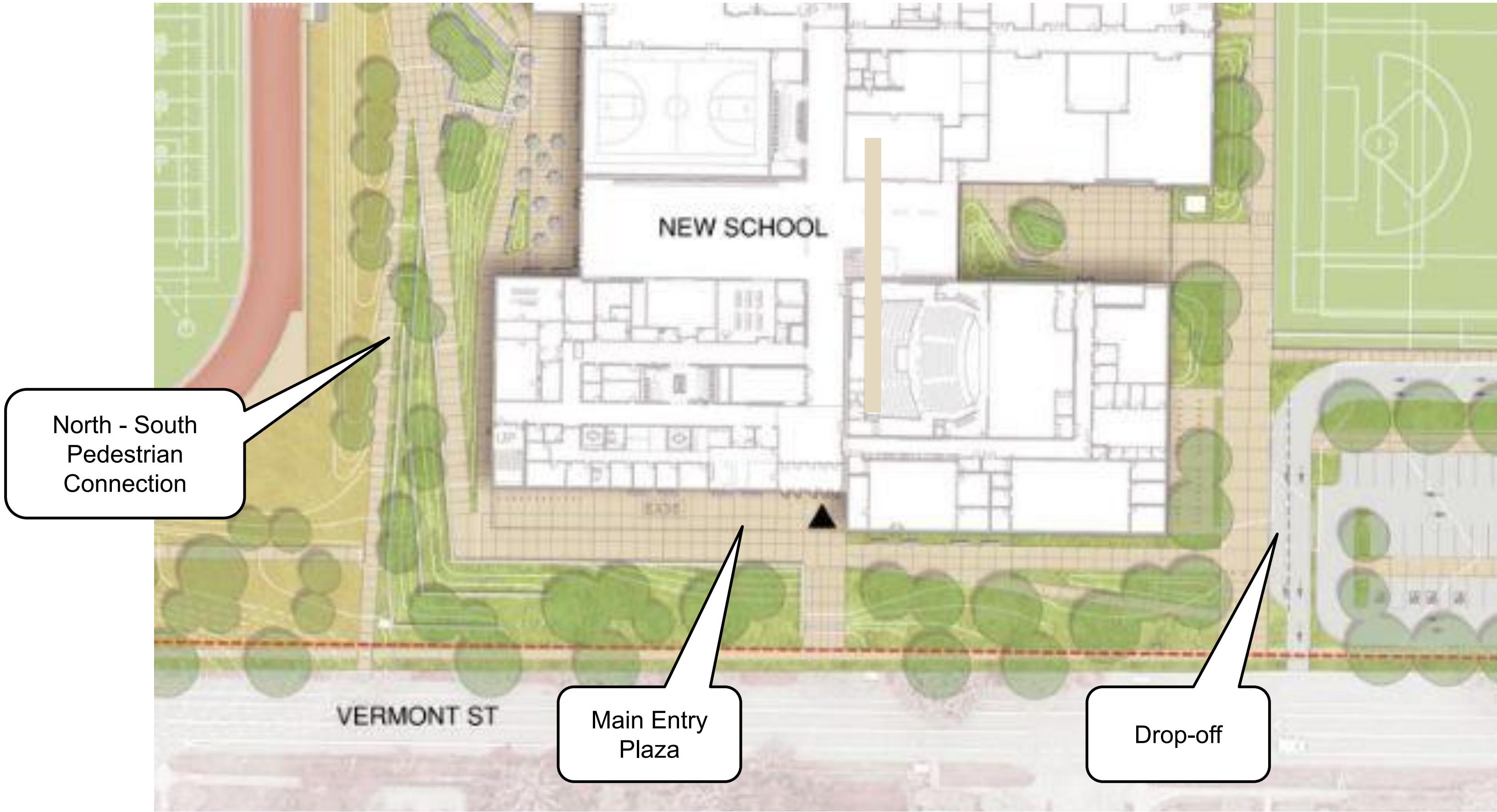
SITE PLAN - SCHEMATIC DESIGN



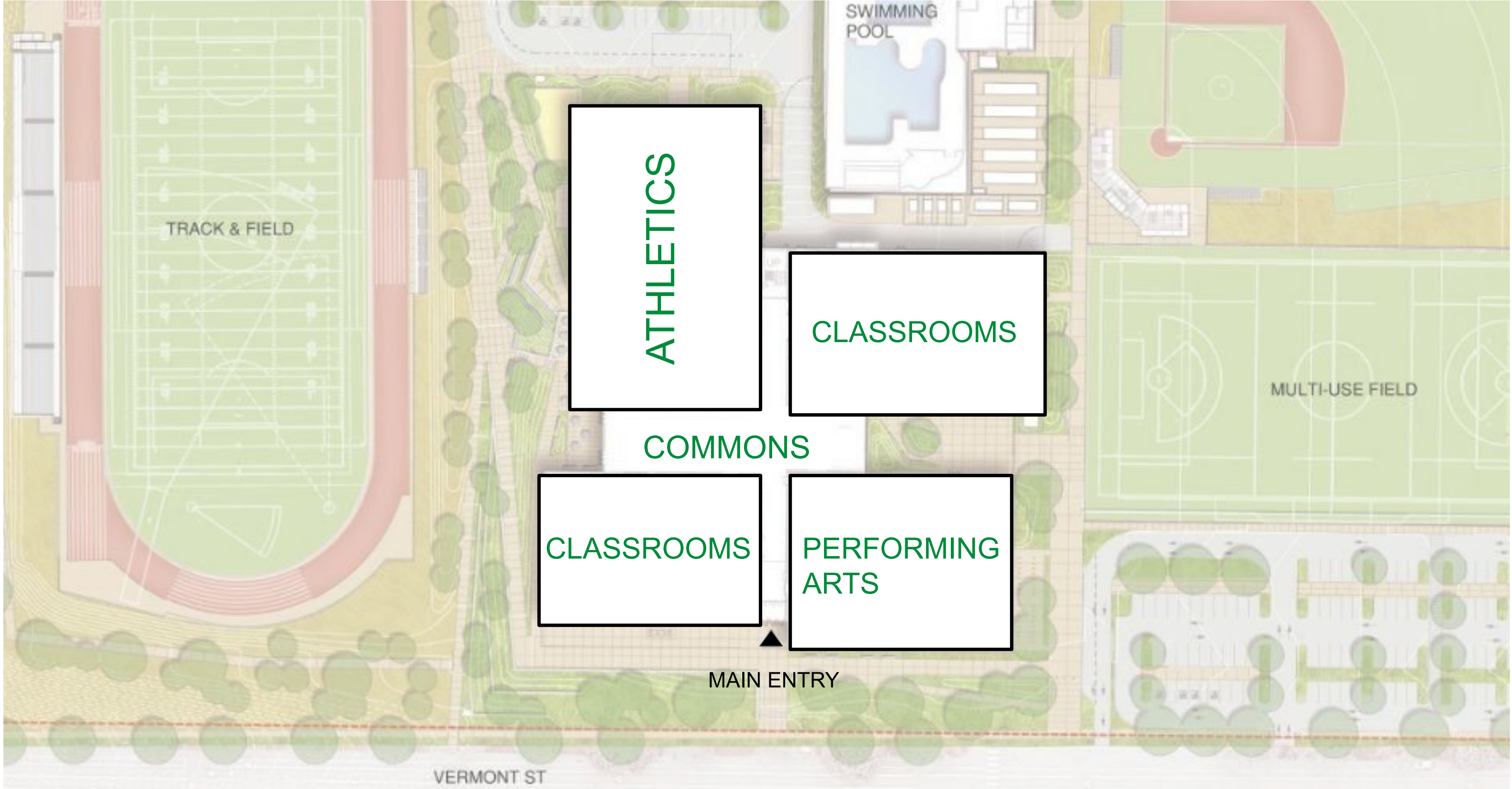
SITE PLAN - SCHEMATIC DESIGN



SITE PLAN - SCHEMATIC DESIGN



FLOOR PLAN ORGANIZATION



SW PERSPECTIVE RENDERING



WEST ELEVATION RENDERING



WEST ELEVATION RENDERING



PROJECT VISION



The new Ida B. Wells High School will embody the legacy of its namesake, shining a light on her commitment to truth, transparency, and justice.

PROJECT VISION



Bold
Innovative
Change
Truth
Lifting Up Voices
Persistence
Determination
Honor
Connection
Creative
Community
Support
Cultural Heritage
Knowledge sharing
Accessible to all
Gathering
Forums

Action
Justice
Advocacy
Pride
Feminism
Empathy
Courage
Inclusive
Empowerment
Breaking Barriers
Welcoming
Diversity
Kindness
Perseverance
Values
Free Speech

FAÇADE - MATERIAL AND COLOR



FAÇADE - COLOR STUDIES



FAÇADE - MATERIAL EXPLORATION



Metal

Cementitious Panel

Brick

Cost	TBD	TBD	TBD
Durability	MEDIUM	HIGH	HIGH
Maintenance	LOW	LOW	LOW
Lightweight	YES	YES	NO
Embodied Carbon	LOW	MEDIUM	HIGH
Supports Design Concept	YES	YES	MORE DIFFICULT TO ACHIEVE

Thank you!

