



**Casey Liles**

IBR Delivery Manager

**Shilpa Mallem**

IBR Design Manager

**Hayli Reff**

IBR Cultural Resources Manager

# Portland Historic Landmarks Commission

## IBR Draft SEIS Findings

October 28, 2024

# Overview of NEPA Process

# What is NEPA?

## National Environmental Policy Act of 1970

- ▶ Requires federal agencies to **assess and disclose environmental effects of proposed actions** prior to making decisions.
- ▶ Ensures agencies consider public comments as part of their decision making.
- ▶ The documentation of this process is known as an **Environmental Impact Statement (EIS)**.

### Environment

- Air quality, water quality, noise, vibration, ecosystems, climate etc.

### Historic/Cultural

- Historic and protected areas, archeological resources, Tribal consultation

### Community

- Residential and commercial displacement, environmental justice

# Purpose and Need



**Safety:** Narrow lanes, no shoulders, poor sight distances, bridge lifts, and short ramp distances for merging and diverging contribute to crashes.



**Earthquake vulnerability:**

In a major earthquake, the bridge would likely be significantly damaged, potentially beyond repair.



**Impaired freight movement:**

Congestion and bridge lifts slow down freight carrying goods along I-5, a critical economic trade route on the West Coast.



**Inadequate bike & pedestrian paths:**

Narrow shared use paths, low railing heights, and lack of dedicated pathways impede safe travel.



**Congestion:** Over 143,000 vehicles crossed the Interstate Bridge each weekday in 2019 with more than 10 hours of daily congestion.



**Limited public transportation:**

Limited transit options and existing bus service can be unreliable due to traffic congestion and bridge lifts.

# Technical Areas Evaluated



## Transportation

- Transportation
- Air Quality
- Aviation
- Energy
- Greenhouse Gas Emissions
- Navigation
- Noise and Vibration



## Community

- Cultural Resources
- Economics
- Electric and Magnetic Fields
- Environmental Justice
- Equity
- Hazardous Materials
- Land Use
- Neighborhoods
- Parks and Recreation
- Property Acquisitions
- Public Services
- Utilities



## Environment

- Climate Change
- Ecosystems
- Geology and Groundwater
- Visual Quality
- Water Quality and Hydrology
- Wetlands and Waters

# Modified LPA + Design Options



# Modified LPA Components



# What is Being Studied in the Draft SEIS?

The IBR Program is a continuation of the I-5 Columbia River Crossing Project. The IBR Program Draft SEIS is a supplemental environmental analysis document that builds on the 2008 Draft EIS, 2011 Final EIS and 2011 Record of Decision. The Modified Locally Preferred Alternative (LPA) similarly builds on the CRC LPA and includes modifications made to address changes in the physical environment, community priorities, and regulations.

## ▶ Modified Locally Preferred Alternative

- Improve active transportation facilities and connections
- Extend light rail transit (LRT) from Expo to Evergreen Blvd plus bus on shoulder
- Three new LRT stations
- Replace bridges over Columbia River and North Portland Harbor
- Modify seven interchanges on I-5
- Three through lanes and at least one auxiliary lane in each direction
- Variable-rate tolling for motorists using the river crossing as a demand-management and financing tool

## ▶ No-Build Alternative

- None of the improvements associated with Modified LPA would be implemented
- Other planned projects that are independent of the IBR Program would proceed

## ▶ Design Options being Studied

- Bridge configuration: Movable span, single-level, double-deck/stacked
- With or without C Street ramps to/from I-5
- Inclusion of one or two auxiliary lanes
- Possible park & rides at Waterfront and Evergreen Transit stations
- I-5 alignment: centered or shifted west between SR14 and Mill Plain Blvd

**The Draft SEIS evaluates the Modified LPA in comparison to the No-Build Alternative.**

**The analysis is conducted for the future condition, which is the year 2045 for this Draft SEIS.**



# Flythrough Visualizations of Program Investments



Videos showing flythroughs of program investments and design options being studied are available on the [IBR Program's YouTube channel](#) and at [www.interstatebridge.org/nextsteps](http://www.interstatebridge.org/nextsteps)



# Program Area Map

- ▶ Investments shown represent the areas being studied in the Draft SEIS and do not reflect a final decision about what will be built.

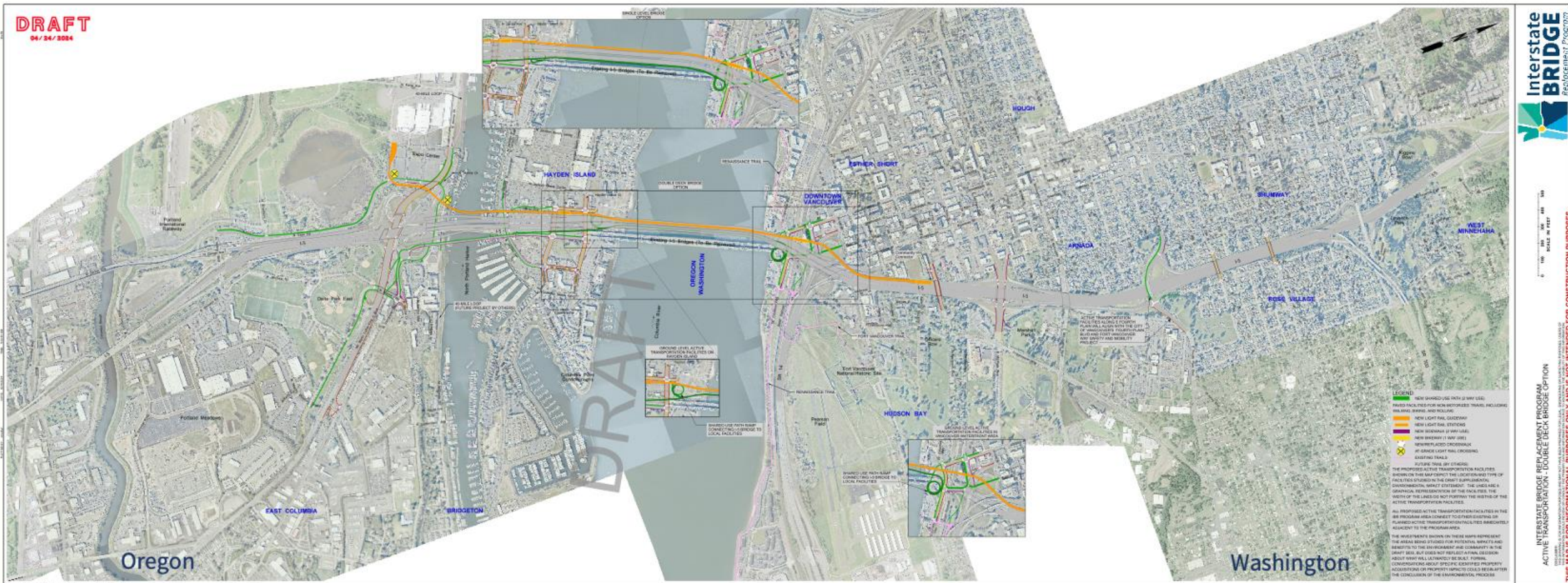


Program area map is available at: [www.interstatebridge.org/media/fnjho04j/mlpa-roll-plot-5-25-23.pdf](http://www.interstatebridge.org/media/fnjho04j/mlpa-roll-plot-5-25-23.pdf)



# Active Transportation Roll Map

- ▶ Investments shown represent the areas being studied in the Draft SEIS and do not reflect a final decision about what will be built.





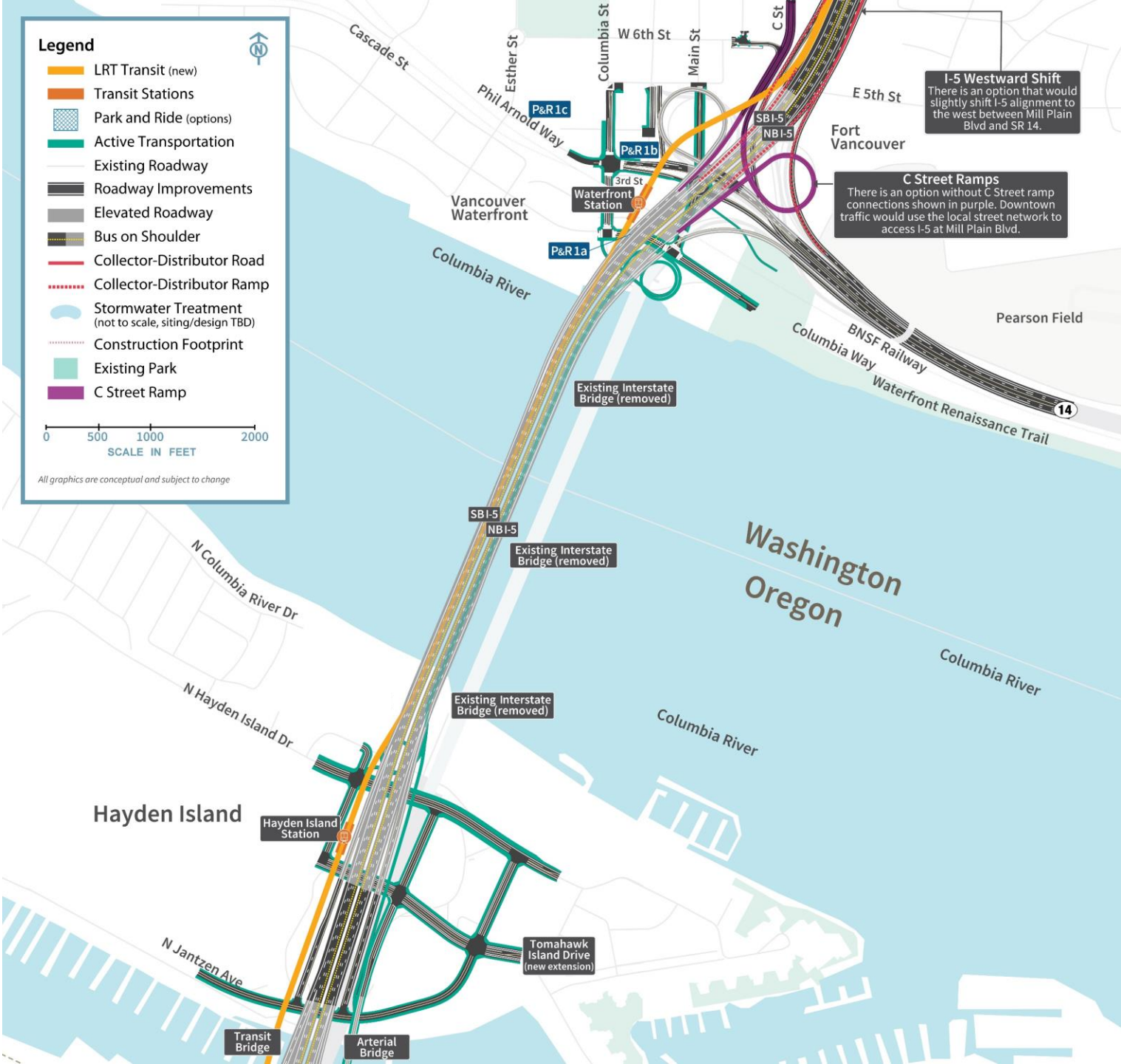
# Columbia River

**Legend**

-  LRT Transit (new)
-  Transit Stations
-  Park and Ride (options)
-  Active Transportation
-  Existing Roadway
-  Roadway Improvements
-  Elevated Roadway
-  Bus on Shoulder
-  Collector-Distributor Road
-  Collector-Distributor Ramp
-  Stormwater Treatment (not to scale, siting/design TBD)
-  Construction Footprint
-  Existing Park
-  C Street Ramp

0 500 1000 2000  
SCALE IN FEET

*All graphics are conceptual and subject to change*

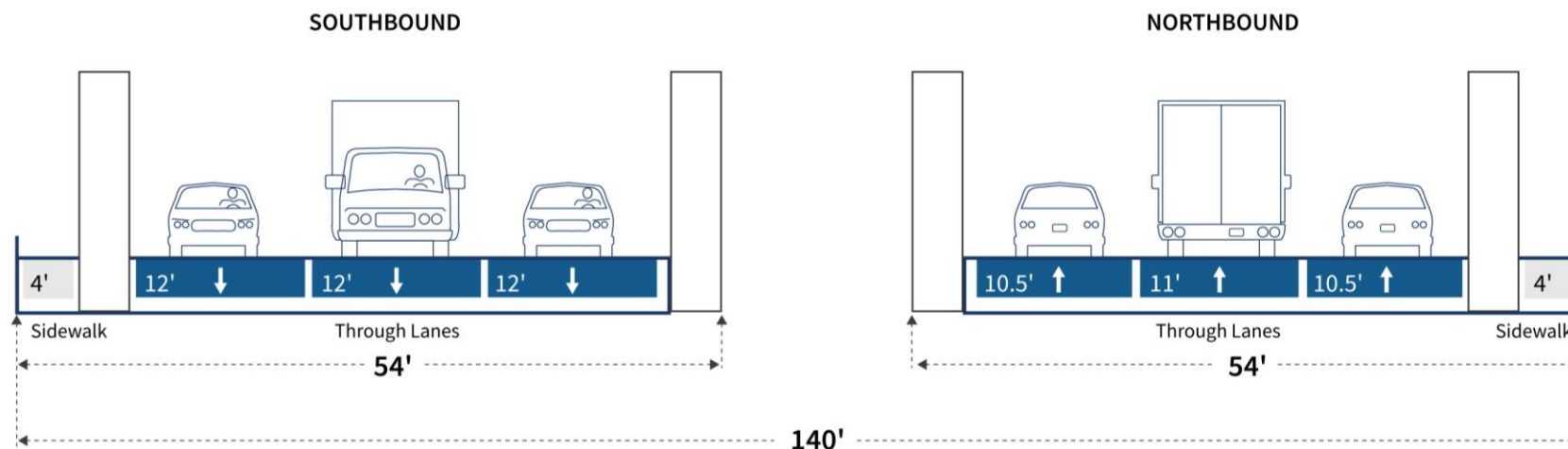


**I-5 Westward Shift**  
There is an option that would slightly shift I-5 alignment to the west between Mill Plain Blvd and SR 14.

**C Street Ramps**  
There is an option without C Street ramp connections shown in purple. Downtown traffic would use the local street network to access I-5 at Mill Plain Blvd.



# Existing Interstate Bridge

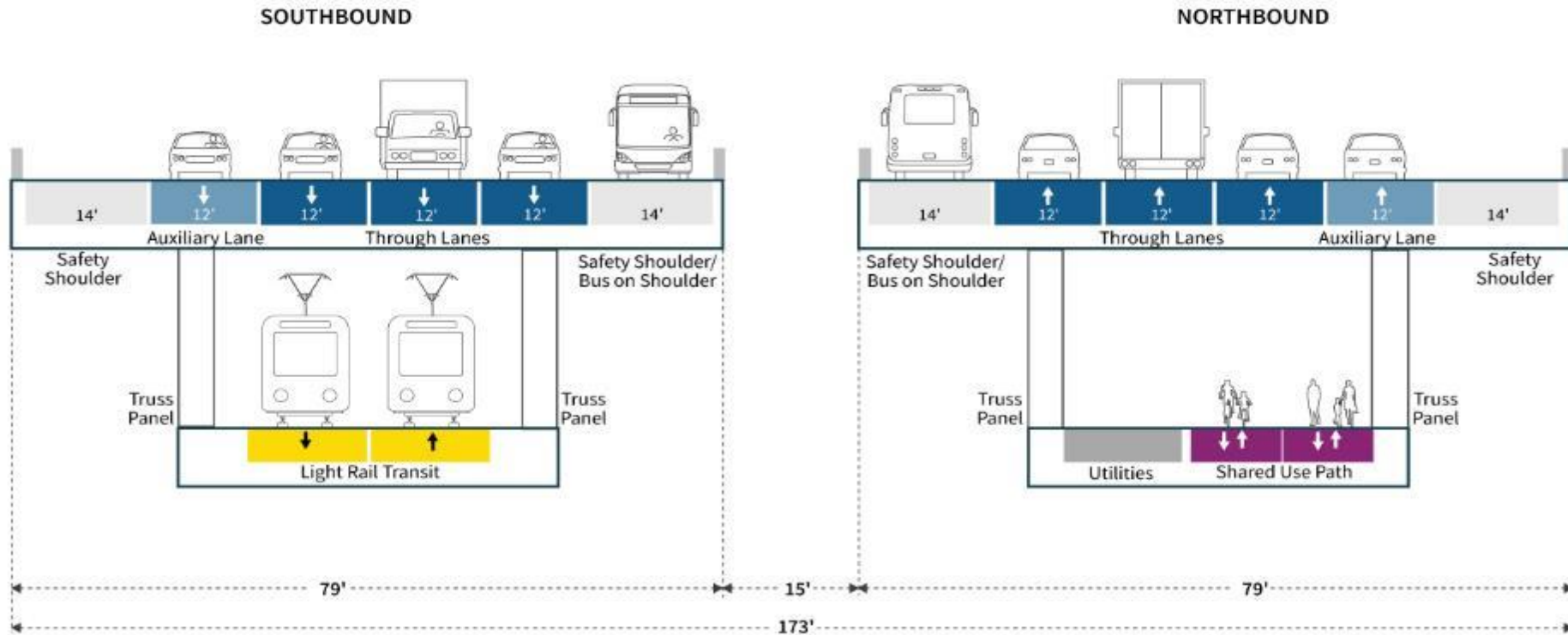


*The existing bridge has three through lanes and a narrow sidewalk in each direction. There are currently no safety shoulders or dedicated space for transit.*

*From 2012 to 2023, **the I-5 Interstate bridge was opened** – or the gate was closed preventing users from crossing the bridge – **an average of 257 times per year**. \* **The average bridge openings/gate closure duration was 13.2 minutes**, with hourly averages ranging between 9.1 and 26.8 minutes.*

*\*Average includes lifts for bridge maintenance.*

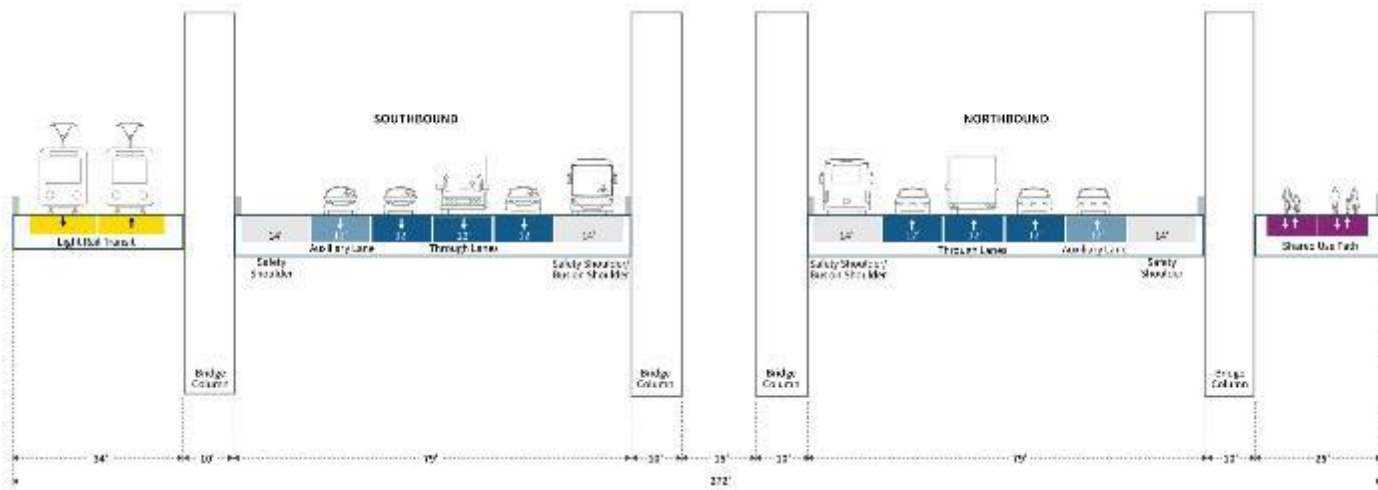
# Columbia River: Double-Deck Fixed-Span



*Typical section; dimensions vary.*



# Columbia River: Single-Level Fixed-Span



*Typical cross section; dimensions may vary*

*Note: The cross section for a girder type bridge would be the same except that it would not have the four 10-foot bridge columns making the total out-to-out width 232 feet.*



Girder



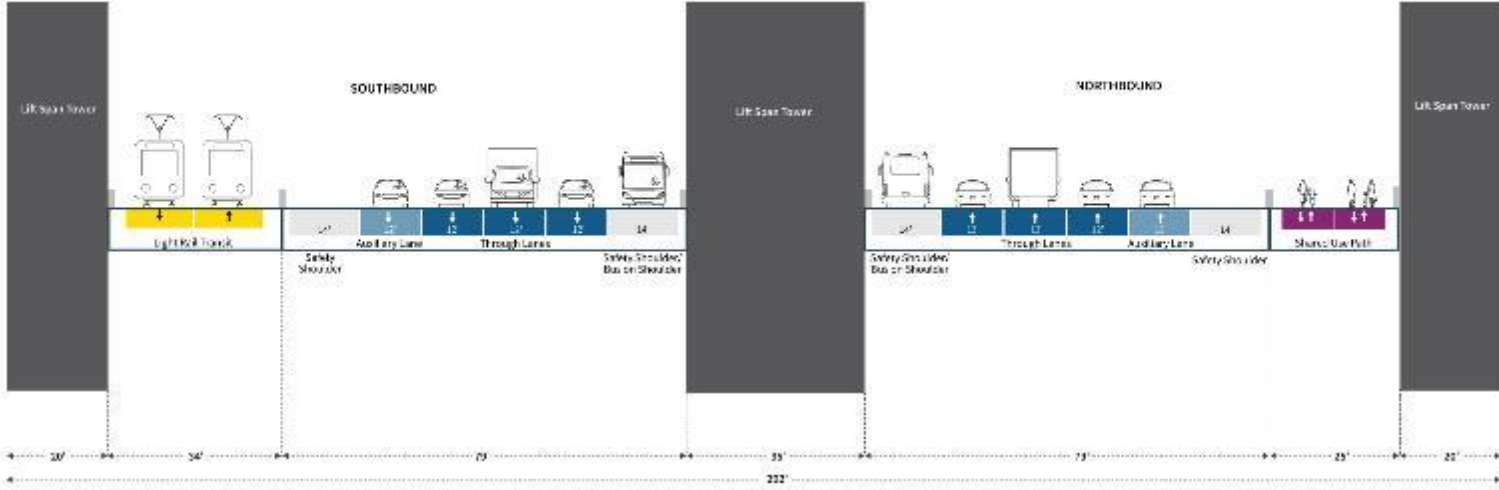
Extradosed



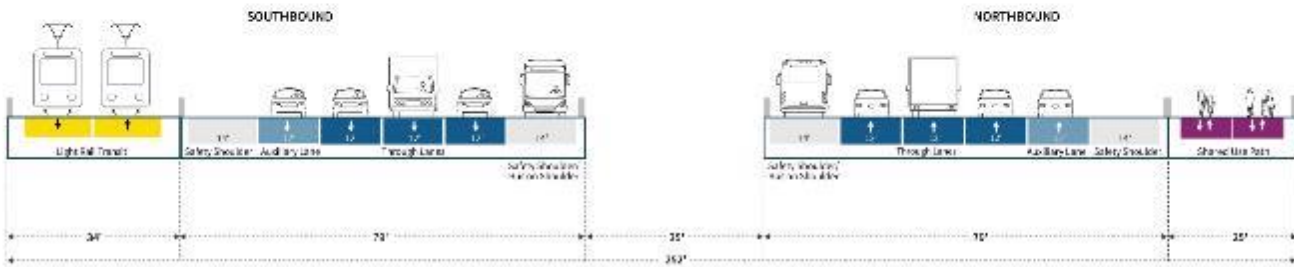
Finback

# Columbia River: Single-Level Movable-Span

Single-level Bridge with Movable Span - Vertical Lift Span Cross-section (Piers 5 and 6)



Single-level Bridge with Movable Span - Fixed Spans Cross-section (Piers 2, 3, 4, and 7)



Typical section; dimensions vary.



# Hayden Island looking north from Center Avenue



Double-deck  
configuration,  
truss



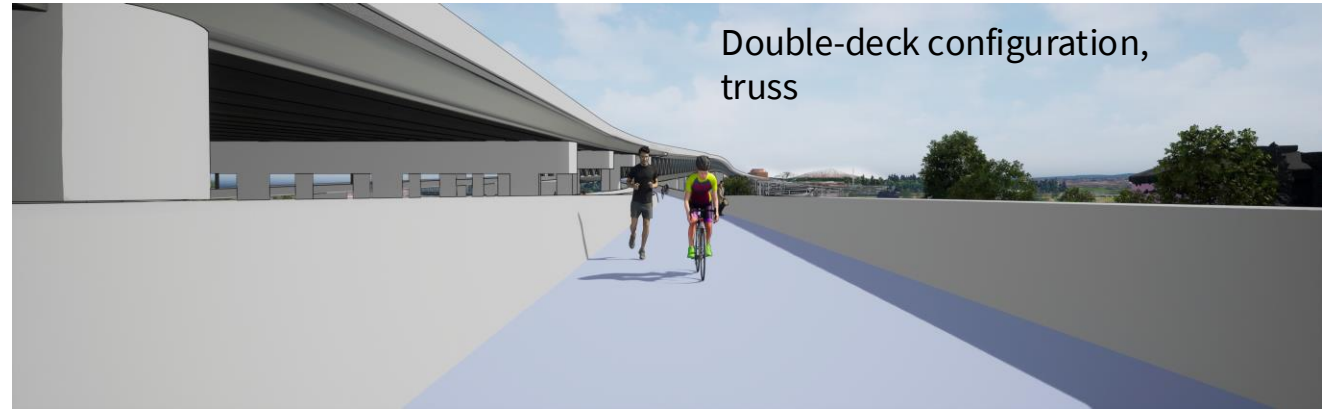
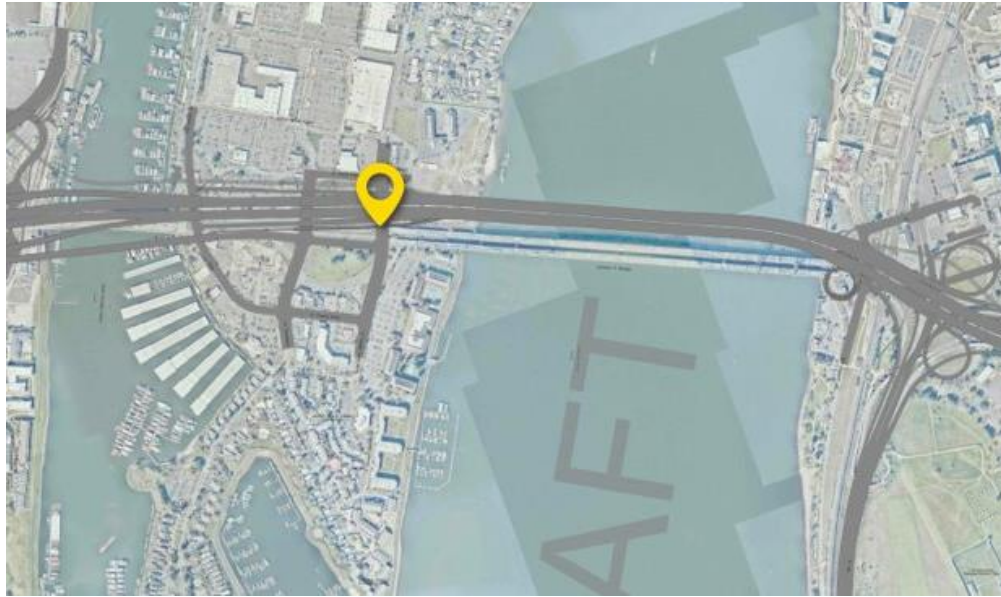
Single-level  
configuration,  
extradosed



Movable span  
configuration,  
steel girder



# Hayden Island looking north from shared use path



Double-deck configuration,  
truss



Single-level configuration,  
extradosed



Movable span configuration,  
steel girder



# Visual

Hayden Island looking west



Existing Condition



Photographic simulation of Modified LPA Columbia River bridges with a double-deck fixed-span configuration



Photographic simulation of Modified LPA with single-level movable-span configuration

Columbia River bridge heading north



Existing Conditions



Photographic Simulation of the Modified LPA with double-deck fixed-span bridge configuration



Photographic simulation of Modified LPA Columbia River bridge with single-level movable-span configuration

Visual elements of the bridge configurations may change as the design progresses.

October 28, 2024



# Cultural Resources

## ► Summary of Potential Effects to Archaeological Sites in the APE

- 12 of the 15 previously recorded archaeological sites in the APE are located within the Modified Locally Preferred Alternative (LPA). These sites have the potential to be impacted by construction-related physical ground disturbance.

## ► Summary of Identified Adverse Effects to Historic Built Environment Resources

- The Modified LPA would result in adverse effects to 12 historic built environment resources, including:
  - 3 properties in Oregon
  - 7 properties in Washington
  - 2 interstate properties





# Draft SEIS Public Comment

# Accessing the Draft SEIS

- ▶ **The document is available now and accessible by all community members**
  - Adheres to ADA standards
  - Online search function to easily locate specific information
  - Executive Summary is interpreted into multiple languages
- ▶ **View the Draft SEIS document, Executive Summary and technical reports online at: [www.interstatebridge.org/DraftSEIS](http://www.interstatebridge.org/DraftSEIS)**
- ▶ **Hard copies are available for in-person review:**
  - IBR office: 500 Broadway, Suite 200, Vancouver (M-Th, 9 a.m. to 4 p.m.)
  - Vancouver City Hall: 415 W 6th Street, Vancouver
  - Vancouver Community Library: 901 C Street, Vancouver
  - The Charles Jordan Community Center: 9009 N Foss Ave, Portland
  - The Portland Building: 1120 SW Fifth Ave, Portland
- ▶ **IBR Office Hours**
  - Sign up online using the links in the office hours event pages on the Program's calendar at: [www.interstatebridge.org/calendar](http://www.interstatebridge.org/calendar)

# How to Comment

- ▶ **Comment through Nov. 18 using one of these methods:**
  - Submit a web-based form at [www.InterstateBridge.org/DraftSEIS](http://www.InterstateBridge.org/DraftSEIS)
  - Email a comment to [DraftSEIS@InterstateBridge.org](mailto:DraftSEIS@InterstateBridge.org)
  - Send a comment to the IBR office through the mail
    - *500 Broadway, Suite 200, Vancouver WA 98660*
  - Call the IBR office to leave a verbal comment at 866-IBR-SEIS (427-7347)
  - Comment at virtual and in-person public hearings
- ▶ **Comments can be provided in your native language.**
- ▶ **Upcoming virtual public hearing events:**
  - Draft SEIS Virtual Public Hearing — Oct. 30 (6:00 pm)

Visit the IBR calendar for a full list of and information about attending briefings, opening houses, public hearings, and other events: [www.interstatebridge.org/calendar](http://www.interstatebridge.org/calendar)



# We want to hear your comments!

- ▶ Comments provide feedback that helps the Program refine design options, update technical analysis and inform the Final SEIS.
- ▶ Comments should be specific to information you learned or read within the Draft SEIS.
- ▶ When sharing an opinion on design options or the analysis, provide information from the analysis that helped form that opinion.
- ▶ For a comment to be included as part of the formal record, it must be submitted in one of the official ways.
- ▶ Comments submitted through social media and informal conversations will not be recorded as formal comments.
- ▶ To ensure the administrative record accurately and completely reflects the documentation received during the public comment period, written comments should not include any hyperlinks to outside materials or information. Any materials or information you wish to have considered should be included within the submitted comment.
- ▶ Attachments to e-mails must be specifically referenced in the comment text, including specific citations to page number and passage from the attachments.
- ▶ All audio/video attachments must be transcribed or submitted via the Draft SEIS voicemail line.



# Section 106 Public Comment



# Section 106 Historic Properties

- ▶ A requirement of the National Historic Preservation Act.
  - Procedural effort relating specifically to historic properties and cultural resources.
    - *Includes any "precontact" or historic district, site, building, structure, or object such as artifacts, records and remains.*
  - Conducted work to identify historic properties and assess effects to significant resources.
  - Determinations of Eligibility (DOE) and Findings of Effect (FOE) documents will be published for public comment.
- ▶ The Section 106 comment period runs separately, yet parallel to the Draft SEIS public comment period.
  - Section 106 public comment period lasts 30 days
  - Begins Oct. 18 until Nov. 18

# Section 106: How to Participate

- ▶ **Section 106 Determinations of Eligibility and Findings of Effect: October 18—November 18, 2024**
  - Learn more in the Cultural Resources online open house
  - Submit a web-based form
  - Email a comment
  - Send a comment through the mail
- ▶ **IBR will share various ways to submit comments closer to the Section 106 comment period opening.**
  - Information about how to comment will be shared in fact sheets, on the website, in the newsletter, and on social media.
- ▶ **Comments do not have to be in English, and the program will provide options for accessibility, ADA, and other needs.**

# Q&A

# Stay Connected & Get Involved



- ▶ Join us for IBR office hours, in person or virtually, and get your questions answered! Visit [interstatebridge.org/calendar](https://interstatebridge.org/calendar) to schedule an appointment, email [info@interstatebridge.org](mailto:info@interstatebridge.org) or call
- ▶ Sign-up for our monthly newsletter: [interstatebridge.org/news](https://interstatebridge.org/news)
- ▶ Attend a program meeting or community engagement event: [interstatebridge.org/calendar](https://interstatebridge.org/calendar)
- ▶ Comments? Questions? Email [info@interstatebridge.org](mailto:info@interstatebridge.org)
- ▶ Follow us on social media: @IBRprogram
- ▶ Evaluate our presentation and share your demographic information with our QR code







For more information contact:

[info@interstatebridge.org](mailto:info@interstatebridge.org)

360-859-0494 or 503-897-9218

888-503-6735

<https://www.interstatebridge.org>

Follow us on social: @IBRprogram



# Thank you!

[www.interstatebridge.org](http://www.interstatebridge.org)