



LLOYD CENTER - MULTIPURPOSE EVENT CENTER

PRESENTED BY WORKS PROGRESS ARCHITECTURE

Portland, OR MAY 2, 2024

EA 24-029290 DA Exh C1

PROJECT SITE AND INFORMATION

C1.1 TEAM INFORMATION

Works Progress Architecture (W.PA) is a medium-sized architectural and urban design firm acclaimed for its commitment to design excellence, diverse representation, and innovative building practices.

Principal Carrie Strickland, FAIA launched W.PA in Portland, Oregon in 2005 and expanded the practice to Los Angeles, California in 2016 to build upon an increasing market. Today, W.PA employs 24 passionate, talented, and conscientious professionals across both studios and manages projects across the United States.

W.PA's architectural archive is deeply rooted in the sustainability and resiliency of urban centers. The firm's earliest undertakings were largely concentrated in Portland's Central Eastside. Strickland's advocacy efforts paved the way for W.PA's first adaptive reuse ventures, which transformed the area's century-old warehouses into a hive of mixed-use creative offices, retail spaces, and restaurants. Similarly in the Arts District in downtown Los Angeles, Strickland's proposed code changes allowed for live/work zoning which spurred adaptive reuse transformations of the neighborhood's abandoned warehouses into affordable spaces for artists.

The firm's provocative and modern designs reinvigorate the vitality of their communities, earning an internationally recognized, award-winning portfolio across a wide spectrum of project types. Receiving more than 50 architectural design awards in 19 years, W.PA has established itself as the most awarded woman owned firm in the world and one of the most acknowledged practices in the nation.

After receiving a 2010 Progressive Architecture Award for a 2,000-capacity concert hall, the firm shifted focus to expand their expertise in amphitheater and music venue design. For the past 10 years, W.PA has collaborated with industry leaders to deliver innovative solutions for modern amphitheaters, music venues, and entertainment centers. In 2019, W.PA designed and managed the construction of Mission Ballroom in Denver, a flexible, purpose-built entertainment space that quickly became one of the hottest live music venues in the country. It anchored a 14-acre master planned entertainment district, North Wynkoop, and maximized connections to the thriving local arts scene.



AURORA | Sacramento, CA



JUPITER NEXT | Portland, OR



MISSION BALLROOM | Denver, CO



bSIDE6 | Portland, OR



FRAMEWORK | Portland, OR

C1.2 SUMMARY OF DEVELOPMENT PROGRAM



ADDRESS: 901 NE Lloyd Center

PROPERTY ID: R182243

STATE ID: 1N1E35BA -00200

ALT ACCOUNT #: R396206500

TAX ROLL: Holladays Add, Block 114&115 TL 200

The proposed development is for a 2-story multi-purpose event center located on NE 9th Avenue at the corner of NE Multnomah Street. The site is currently occupied by the existing Nordstrom building at the southwest corner of the Lloyd Center Mall.

The proposed building preserves the public plaza adjacent to NE Multnomah Street and the existing basement of the current structure for storage and back of house support areas. 2-stories above will include a main event space, loading, lobbies, and a second-floor mezzanine that overlooks the main event space on the ground level.

The main pedestrian entrance will be located off of NE Multnomah Street. Secondary entrances will be located on the east and west sides of the building along NE 9th Ave and where the existing mall is currently. Vehicular access will be located along NE 9th Avenue.

The site is located in the CX - Central Commercial Zone, has a design overlay, and falls within the Central City and Lloyd Plan Districts.

The site lies at the western boundary of the future Lloyd Masterplan. Careful consideration has been given to maximize the proposed building's connection to this new development while remaining in context with the larger architectural dialogue of the Lloyd District following the CCFDG and the Lloyd District Design Guidelines.

Open space at the building's SE and SW corners extend the existing plaza along Multnomah Street encouraging an active intersection, per Central City Guideline C7, and reinforces future pedestrian networks to the Lloyd Masterplan. These features also provide view opportunities and offer a distinct sense of entry and exit to the proposed building, following Lloyd District Design Guidelines C2-1 and A9-1.

Art and landscaping are incorporated as integral elements of design, following the Lloyd District Design Guidelines A5-3 and A5-6. Multiple building step backs at key locations provide public art opportunities and relocate the largely inaccessible green roof requirements to exclusively visible positions, creating a stimulating environment to be

PROJECT SITE AND INFORMATION

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CONTEXT STUDY

ZONING CODE

Base ZoneCX
Overlayd (Design Review) .
DistrictCC (Central City Plan District)
SubdistrictLloyd
Design GuidelinesCCFDG (Central City) & Lloyd District

Max. FAR6:1 Per Map 510-2
Base Zone Height150 ft. per Map 510-3
Bonus Height225 ft. per Map 510-4

Min. Setbacks (Front)None
Min. Setbacks (Civic Corridor).....10 ft. (NA)

Max. Setbacks (Street Lot Line) 10 ft.
Min. Setbacks Civic Corridor.....10 ft. (NA)

Req'd Bldg LinesYes on 9th per Map 510-7
Minimum LandscapingWhen the setback is at least 6 ft.
and less than 12 ft., at least 50% must be planted.

GF WindowsYes per 33.510.2220.B.2. Not
designated on Map 510-8 40% Facing St. or Open Area @ 2 ft. - 10
ft. Optional Artwork through DR.

GF Active UseUses Outlined in Subsection A. Not
designated on Map 510-9. 50% req'd along the plaza. Clear height
of 12 ft. Minimum. 25 ft. deep. Must include windows and doors.

Eco Roof.....Yes per 33.510.243. Req'd if
over 20,000 sf. 100% except up to 40% for mechanical, skylights,
bulkheads, fire evac. routes, solar panels, equipment.

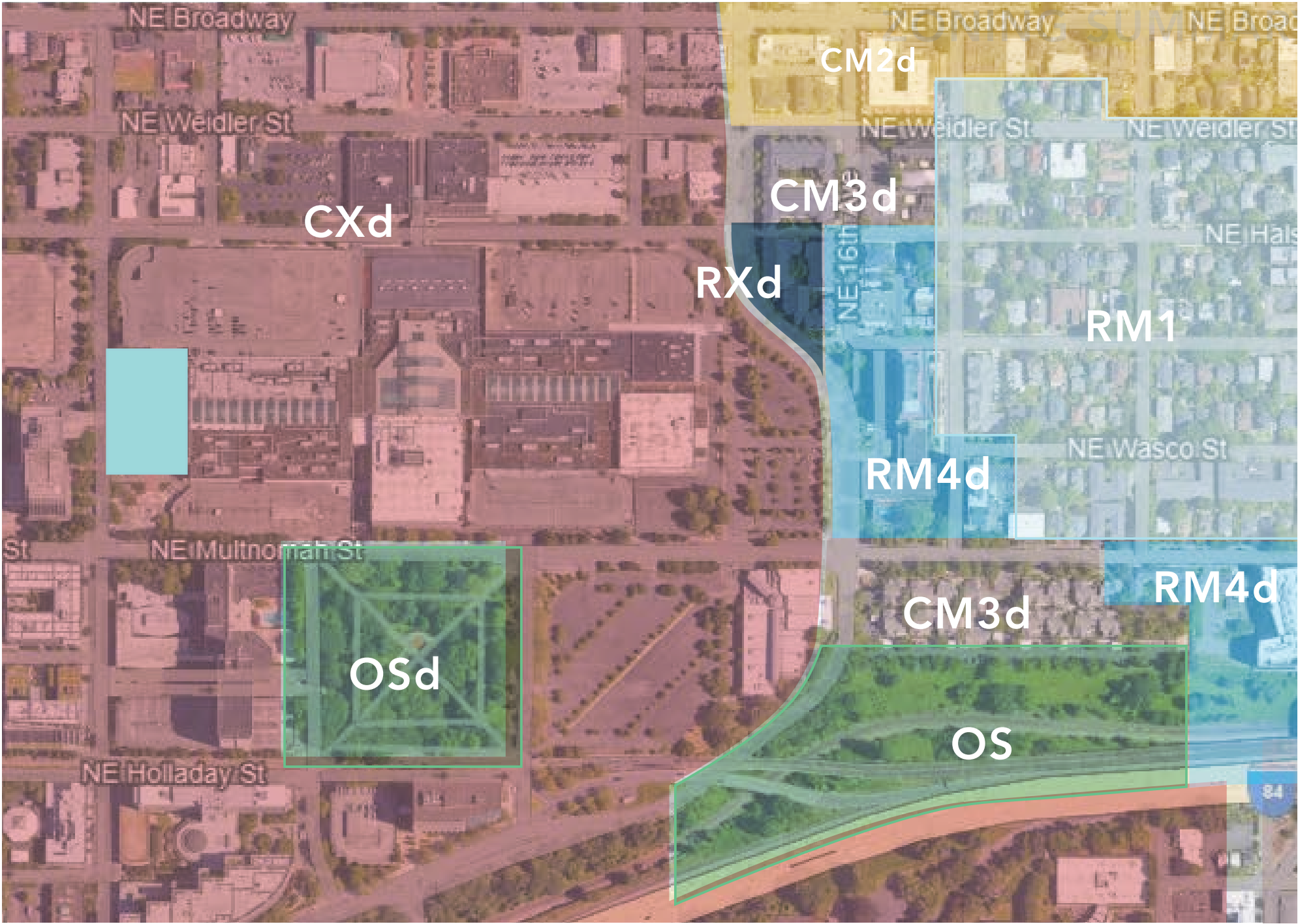
PARKING

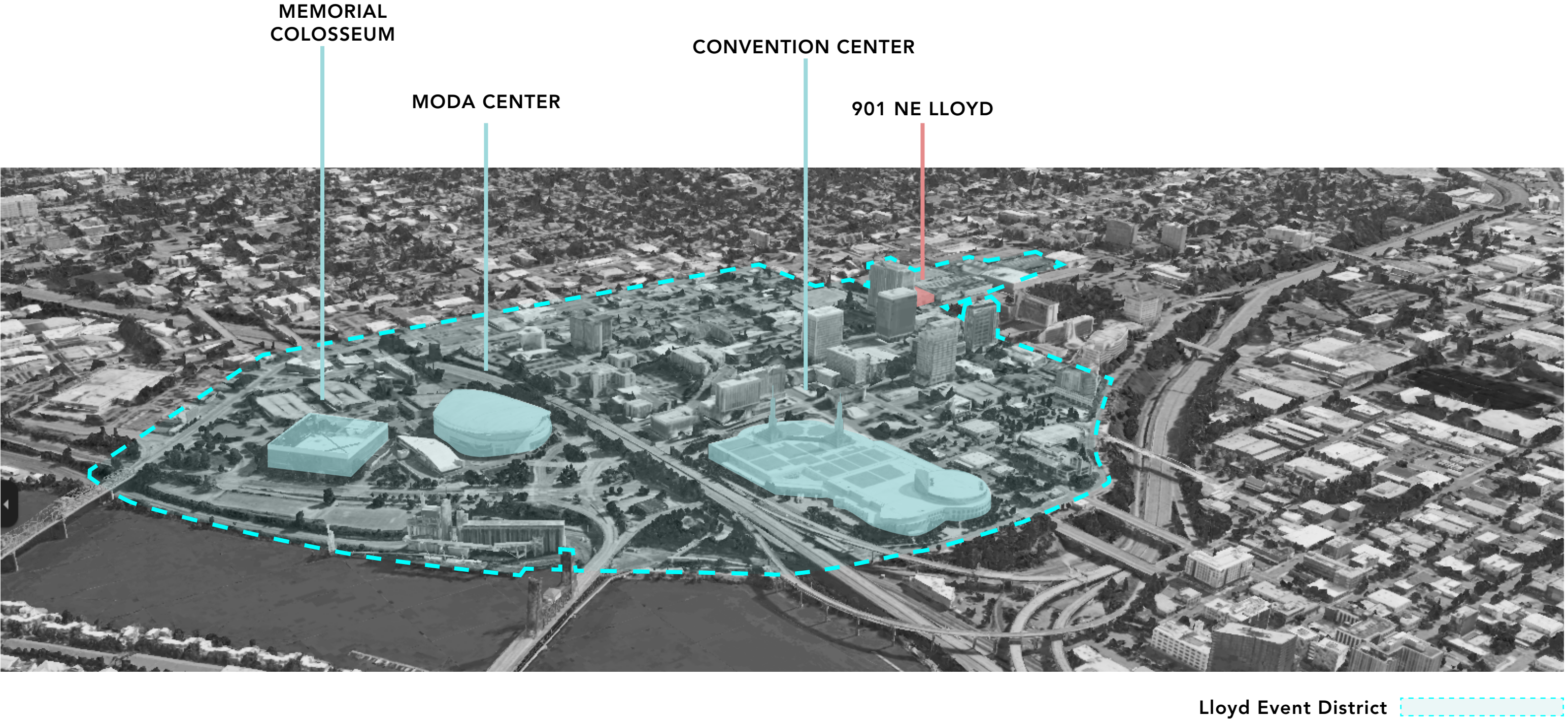
Central City Parking Sector2

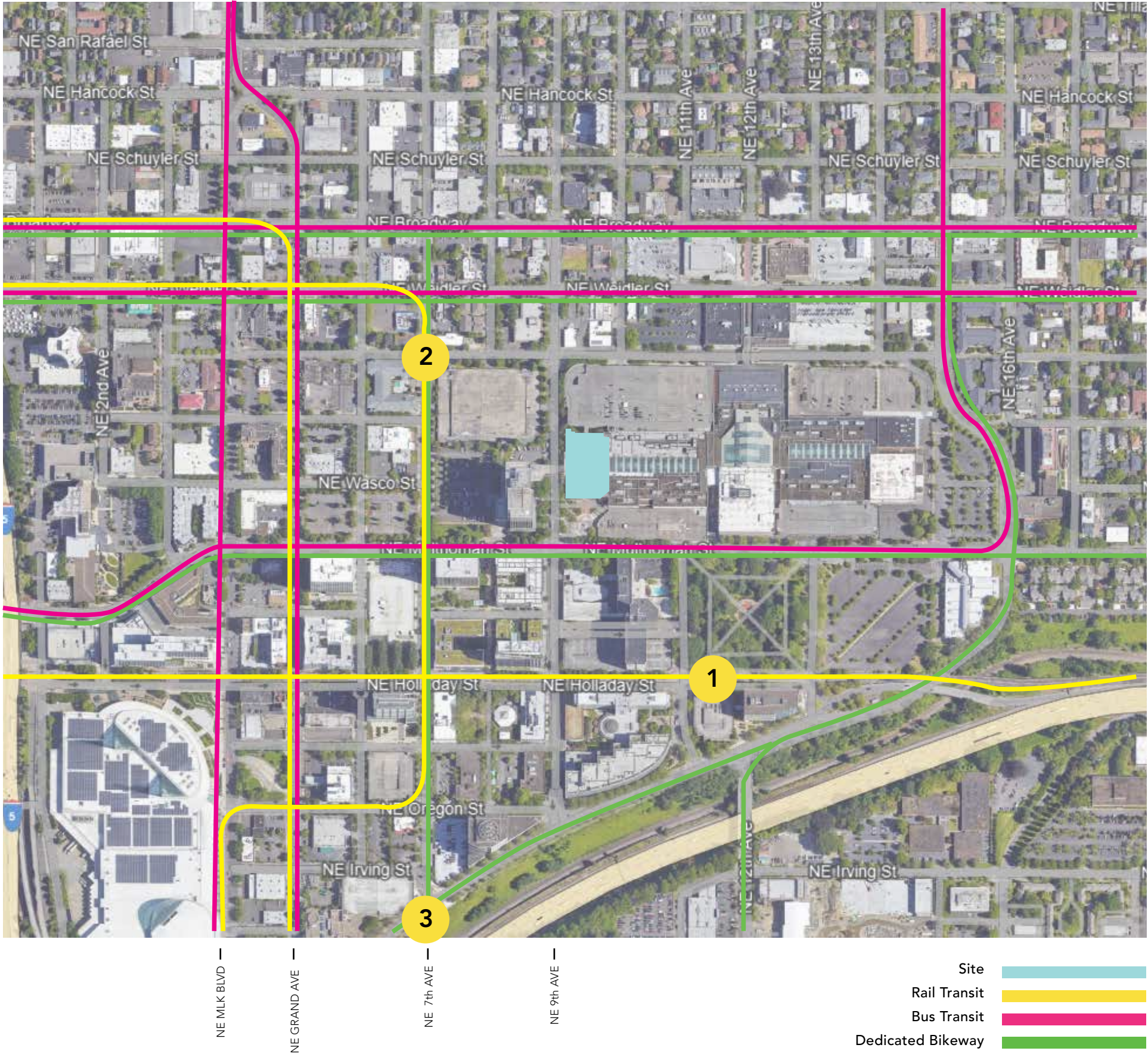
BIKE PARKING

Long-Term Spaces1/10,000 sf
Short-Term Spaces.....1/40 seats

C2.1 ZONING SUMMARY







C2.3 URBAN CONTEXT - TRANSPORTATION



1 Max Station (Red, Blue, Green Lines) - NE 11th and Holladay



2 Streetcar A-Loop Station - NE 7th and Halsey



3 Blumenauer Bicycle and Pedestrian Bridge - NE 7th and Lloyd

C2.4 URBAN CONTEXT - OPEN SPACE

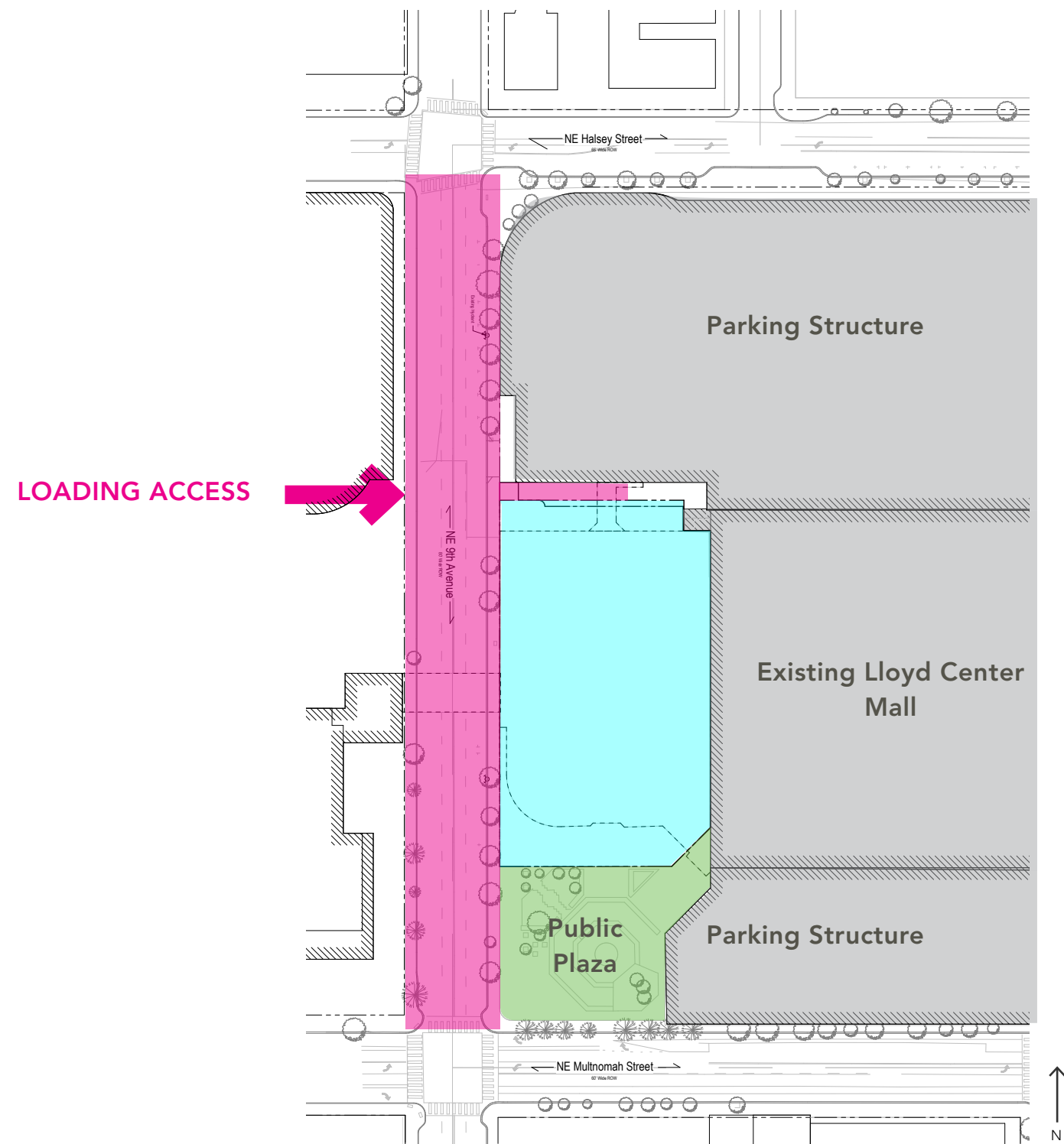


1 Adjacent Plaza - NE Multnomah St and 9th Ave



2 Holladay Park - NE Multnomah St and 11th Ave

C2.5 SITE CONTEXT



NE 9th Ave - Looking SW



NE 9th Ave - Looking NW

C2.6 EXISTING SITE & VICINITY PHOTOS



South Facade from NE Multnomah St and NE 9th Ave - Looking North

C2.7 EXISTING SITE & VICINITY PHOTOS



Existing Basement



West Facade from NE 9th Ave with Existing Skybridge - Looking South



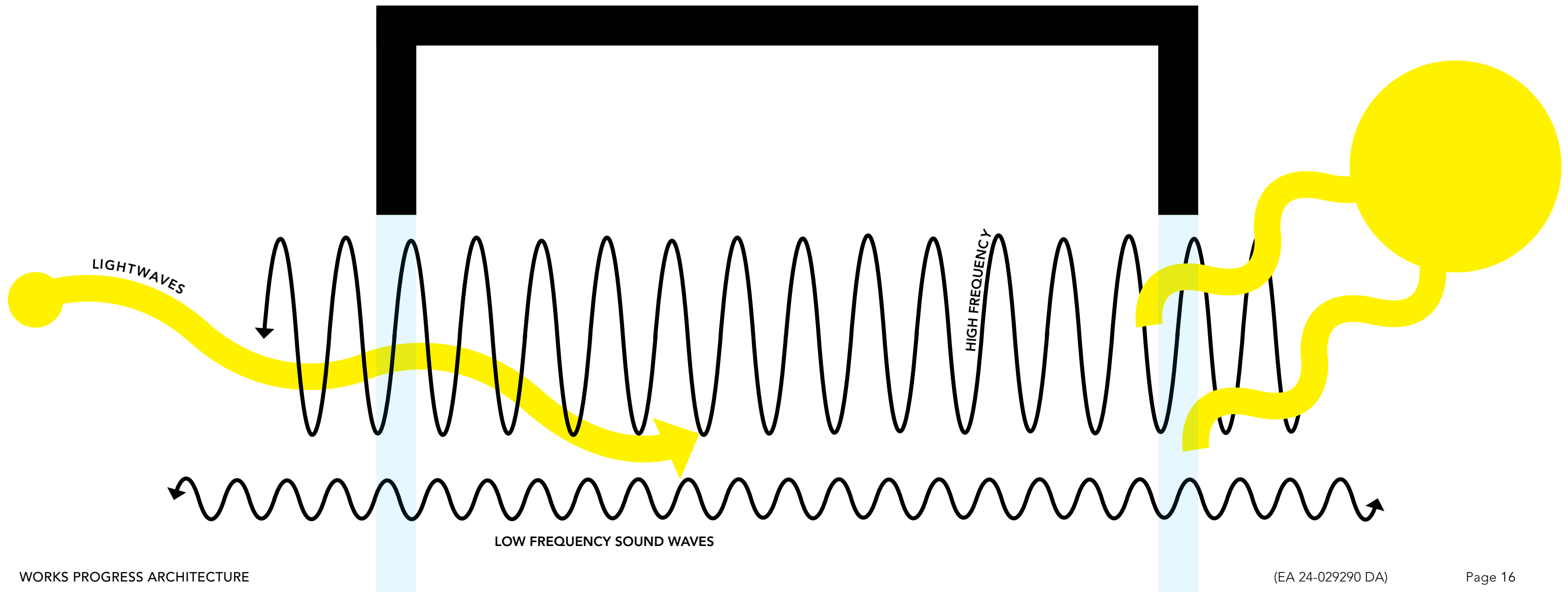
North Facade from 3rd Floor Parking Structure - Looking Southeast
(EA 24-029290 DA)

CONCEPT DESIGN

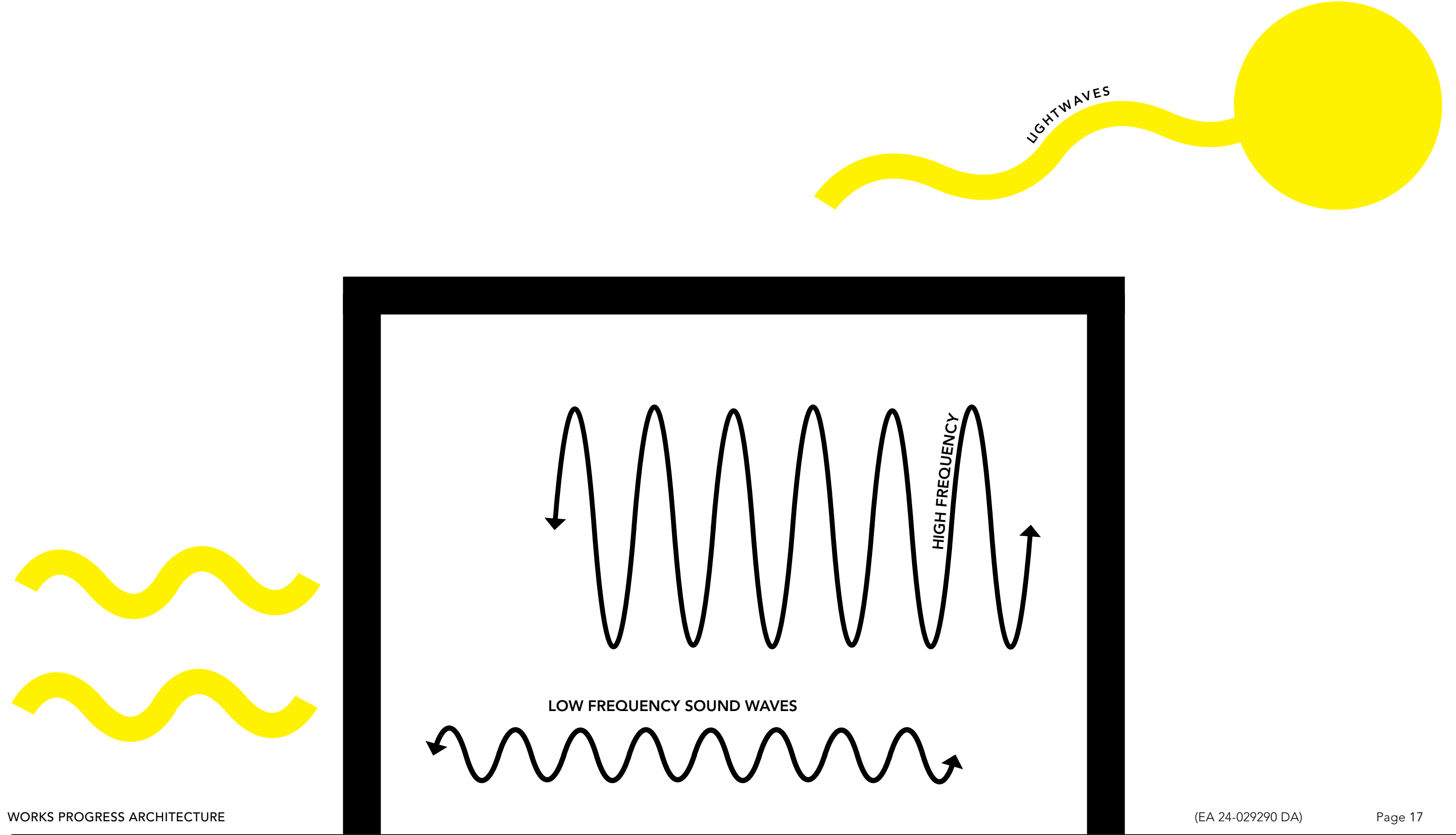
Basis of Design

EVENT SPACE PROGRAMMING





C3.3 ACOUSTIC & LIGHT MITIGATION DIAGRAM



C3.4 PRECEDENT PROJECTS



ARLENE SCHNITZER CONCERT HALL

- Sound and light controlled through a solid facade
 - Ornamental accents along base
 - Glazing only occurs at main entry points
- Openness only occurs in the lobby and at exits



MODA CENTER

- Sound and light controlled through a solid facade
- Glazing only occurs at main entry points and lobby
- Openness only occurs in the lobby and at exits



KELLER AUDITORIUM

- Sound and light controlled through a street-facing solid facade
 - Loading dock is visible
- Openness only occurs in the lobby and at exits
 - Full city block with no windows



DOUBLE TREE HOTEL & EVENT CENTER

- Sound and light controlled through a street-facing solid facade
 - Landscape setbacks from the street frontage
 - Loading dock is visible



REGAL LLOYD CENTER

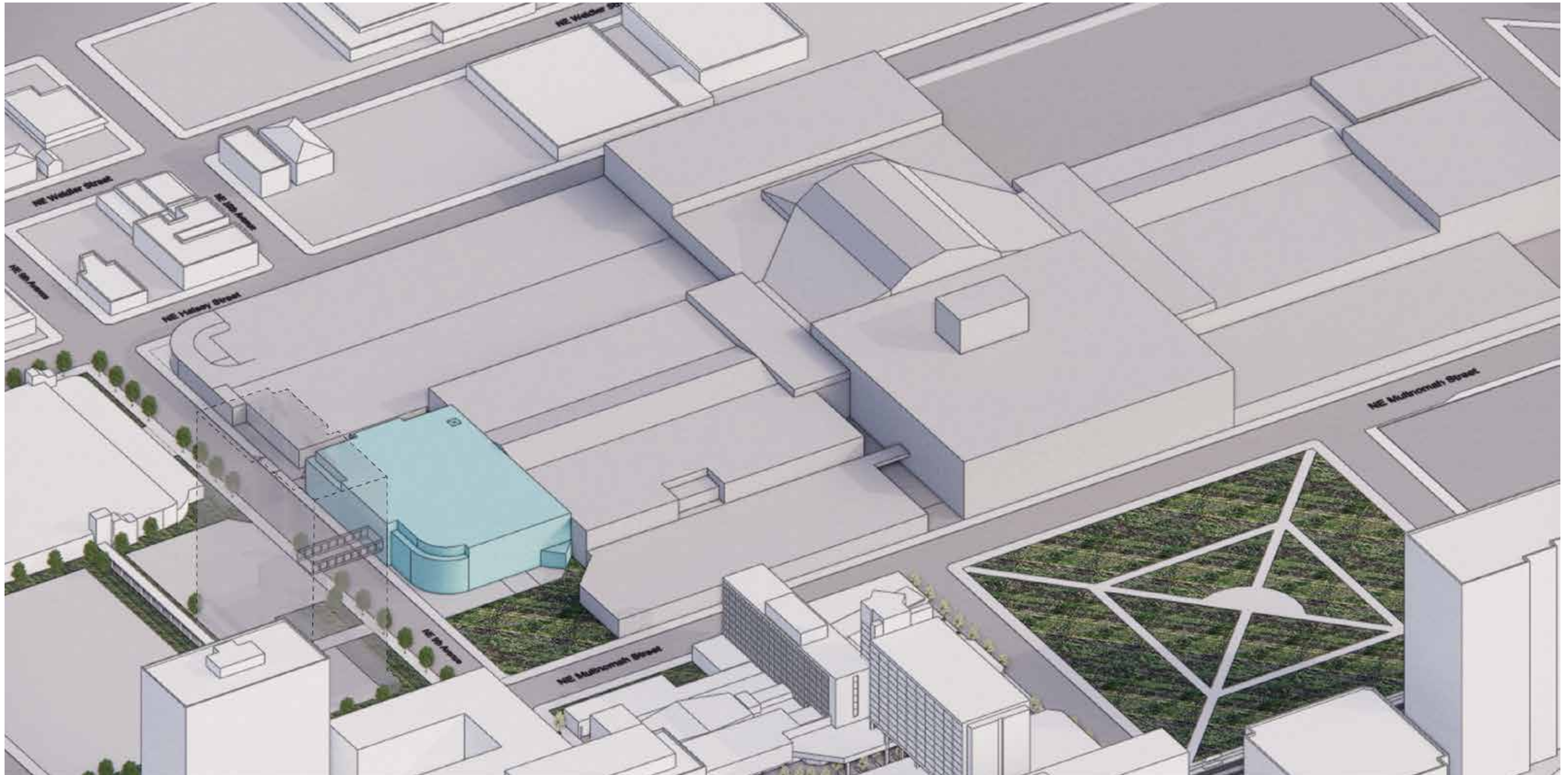
- Sound and light controlled through a street-facing solid facade
 - Glazing only occurs at main entry points

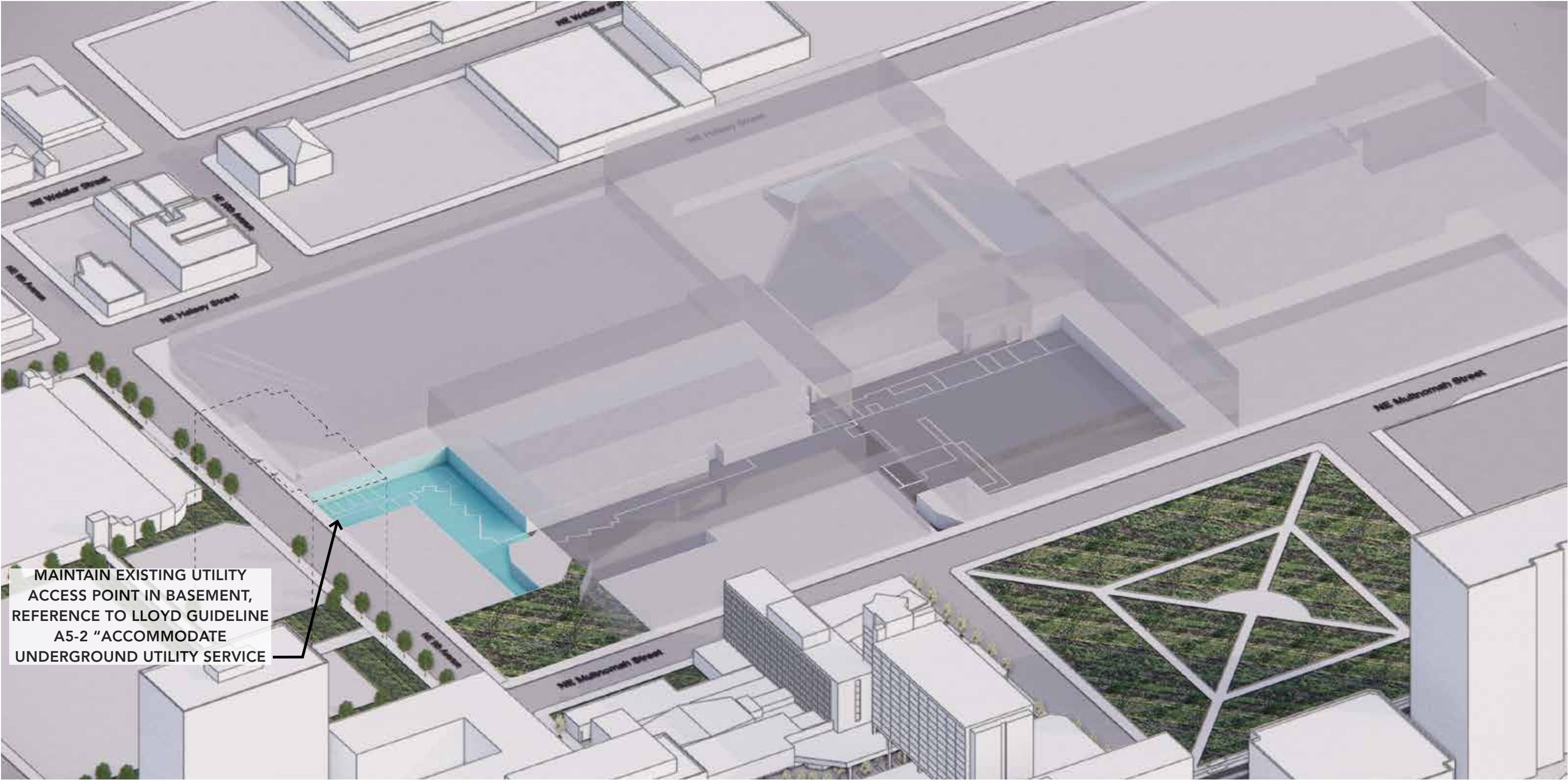


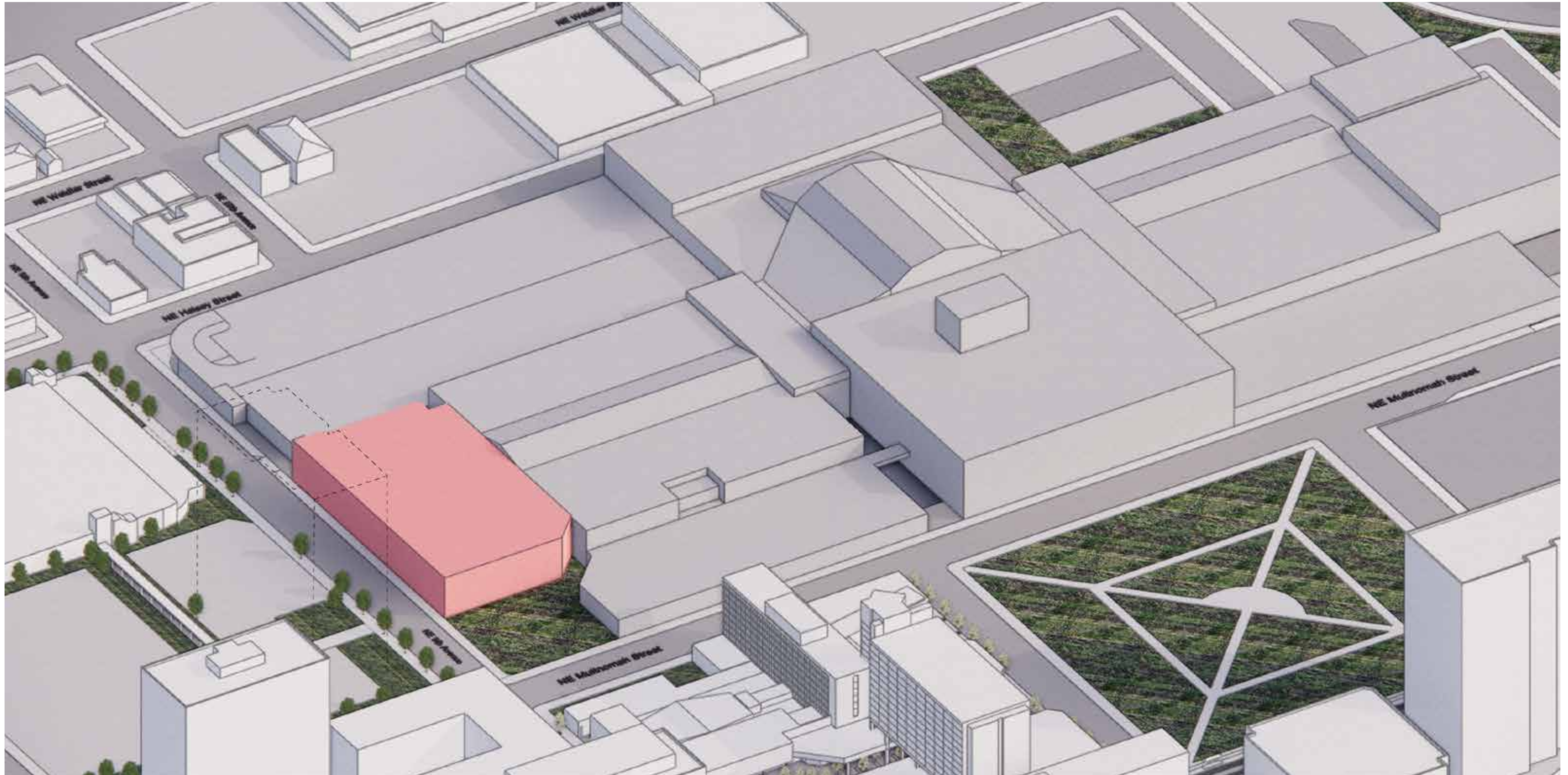
911 FEDERAL BUILDING

- Solid facade at street frontage
- Glazing only occurs at main entry points
 - Security and privacy concerns

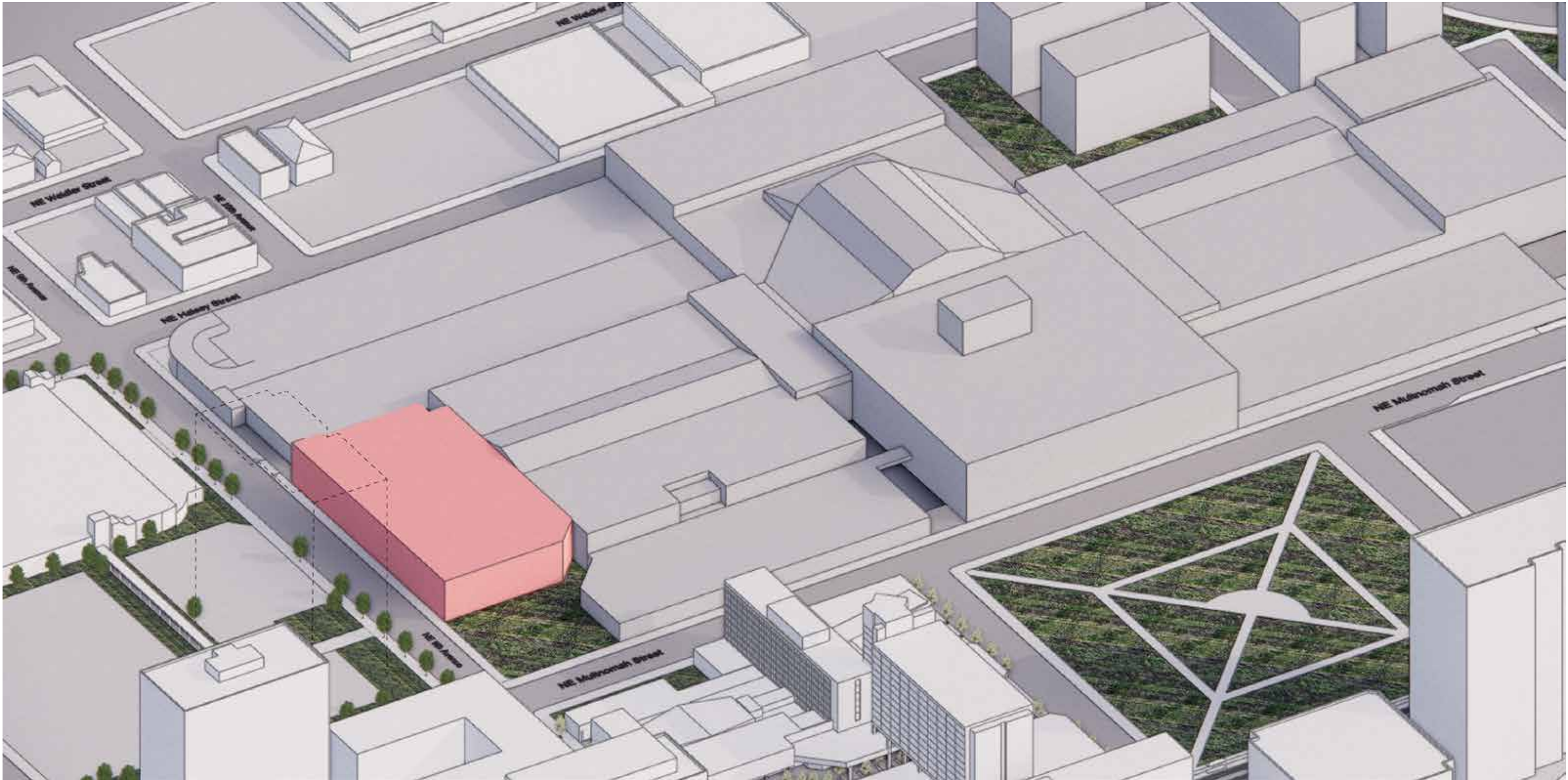
C3.5 PROJECT EVOLUTION - EXISTING MASSING



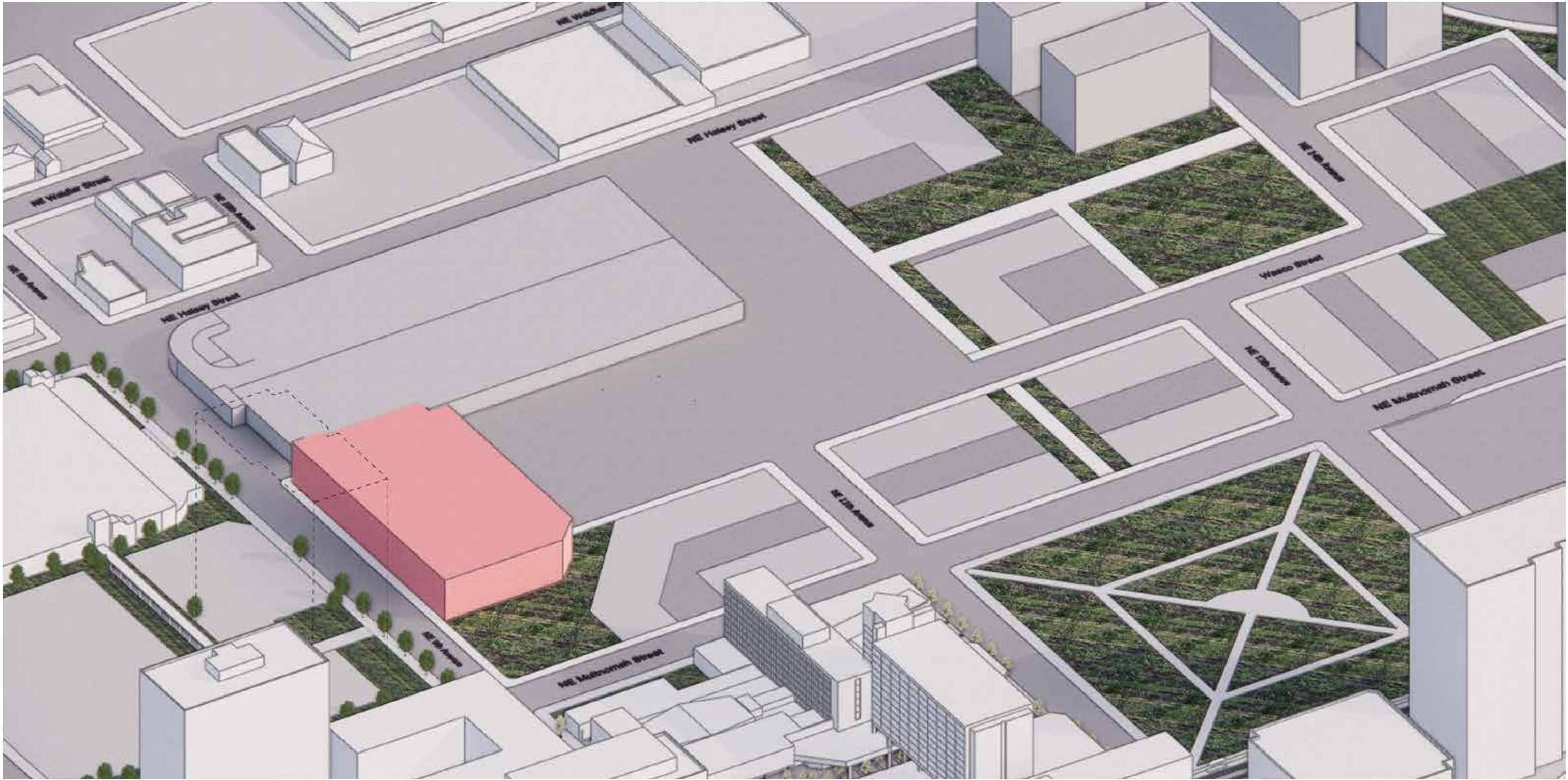




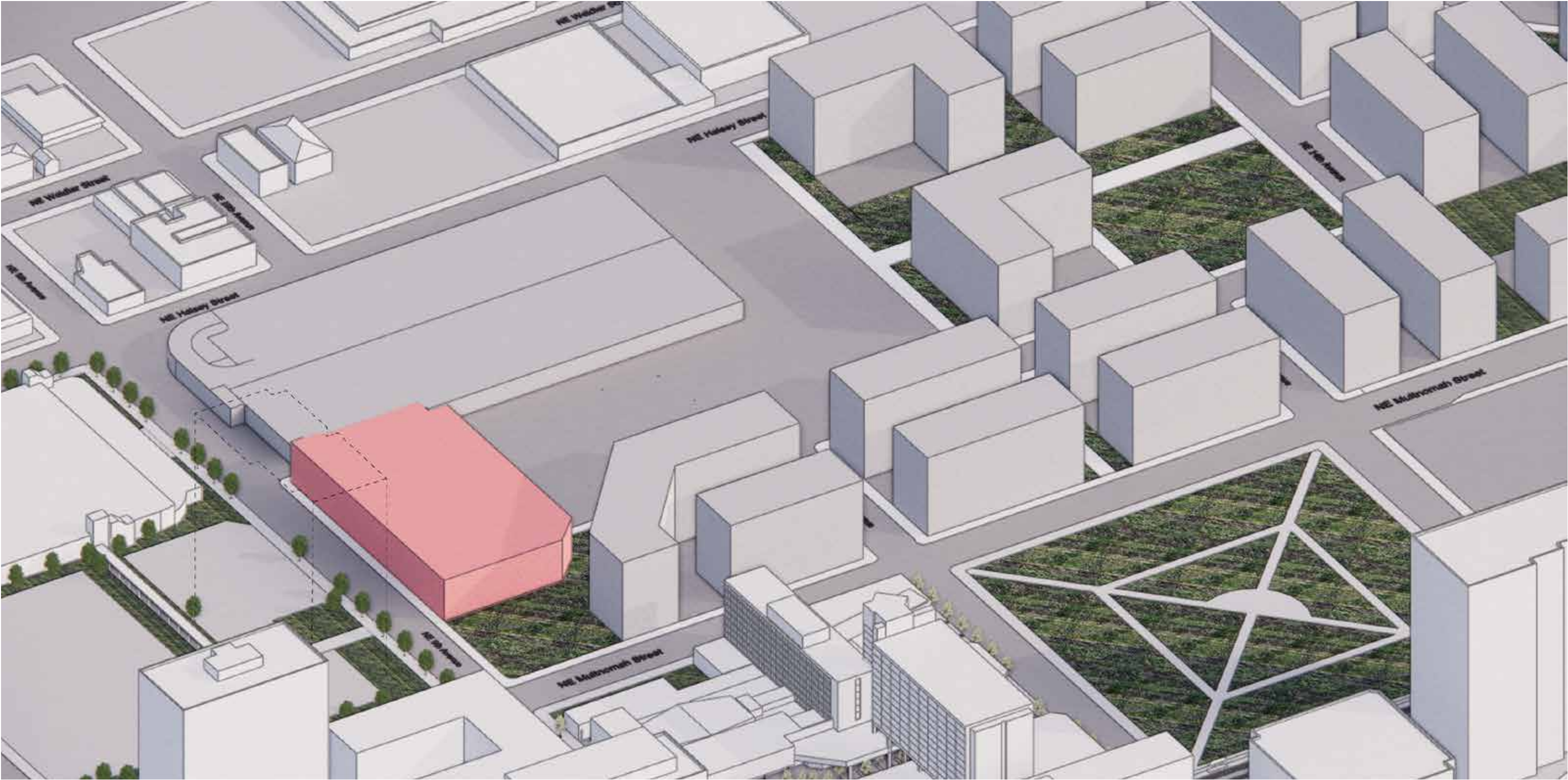
C3.8 PROJECT EVOLUTION - SITE FUTURE CONDITION 1



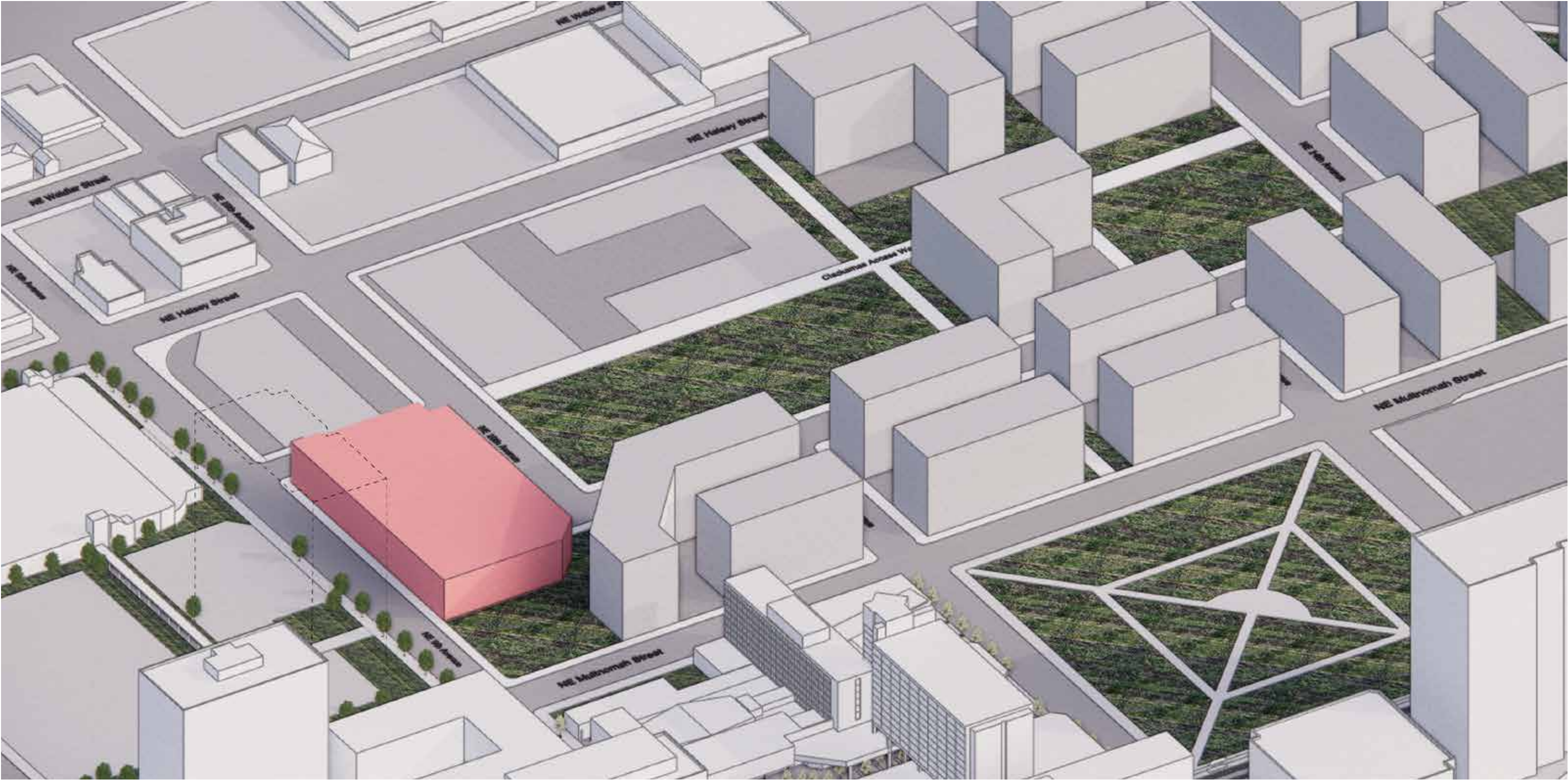
C3.9 PROJECT EVOLUTION - SITE FUTURE CONDITION 2



C3.10 PROJECT EVOLUTION - SITE FUTURE CONDITION 3

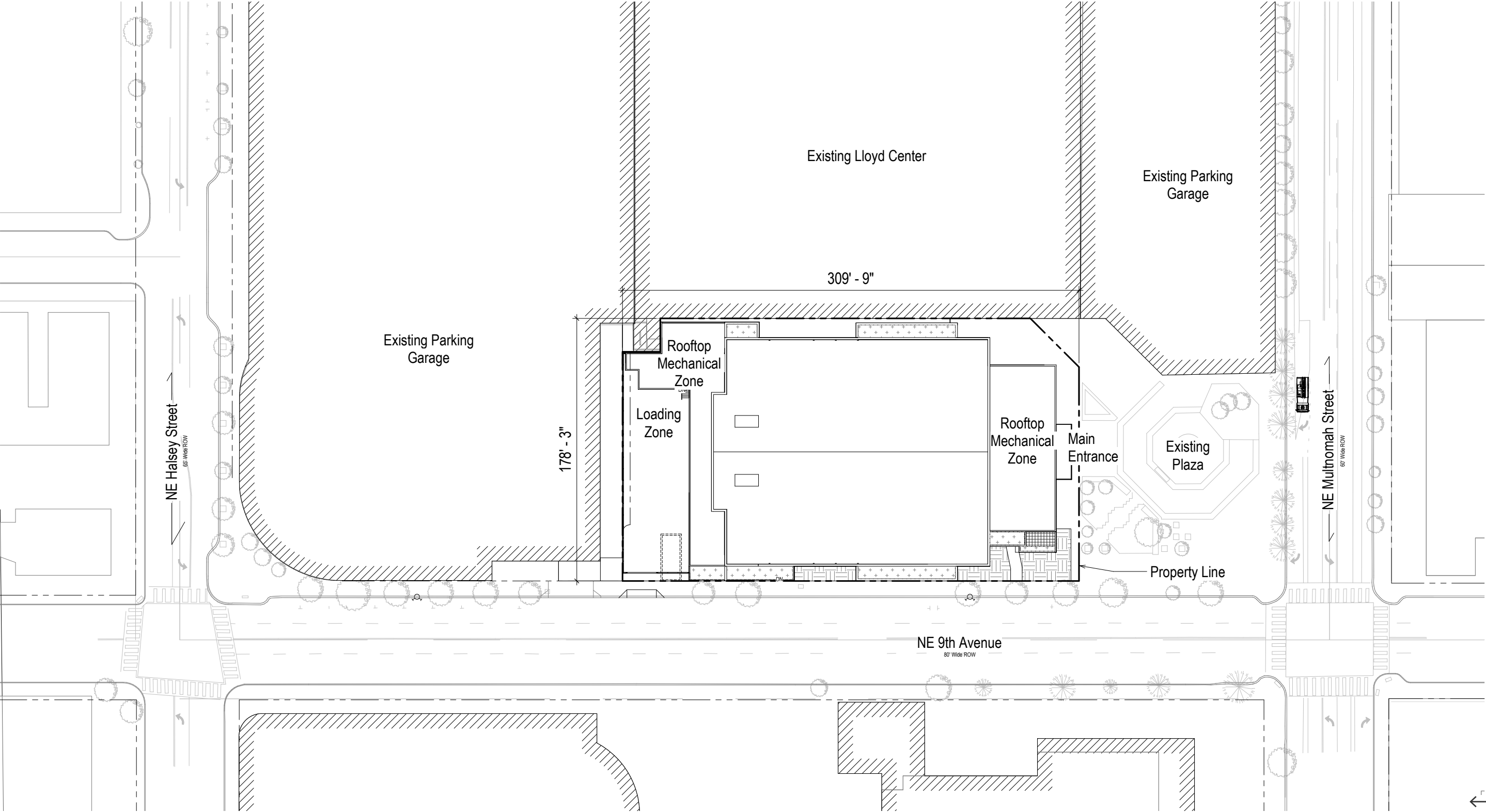


C3.11 PROJECT EVOLUTION - SITE FUTURE CONDITION 4

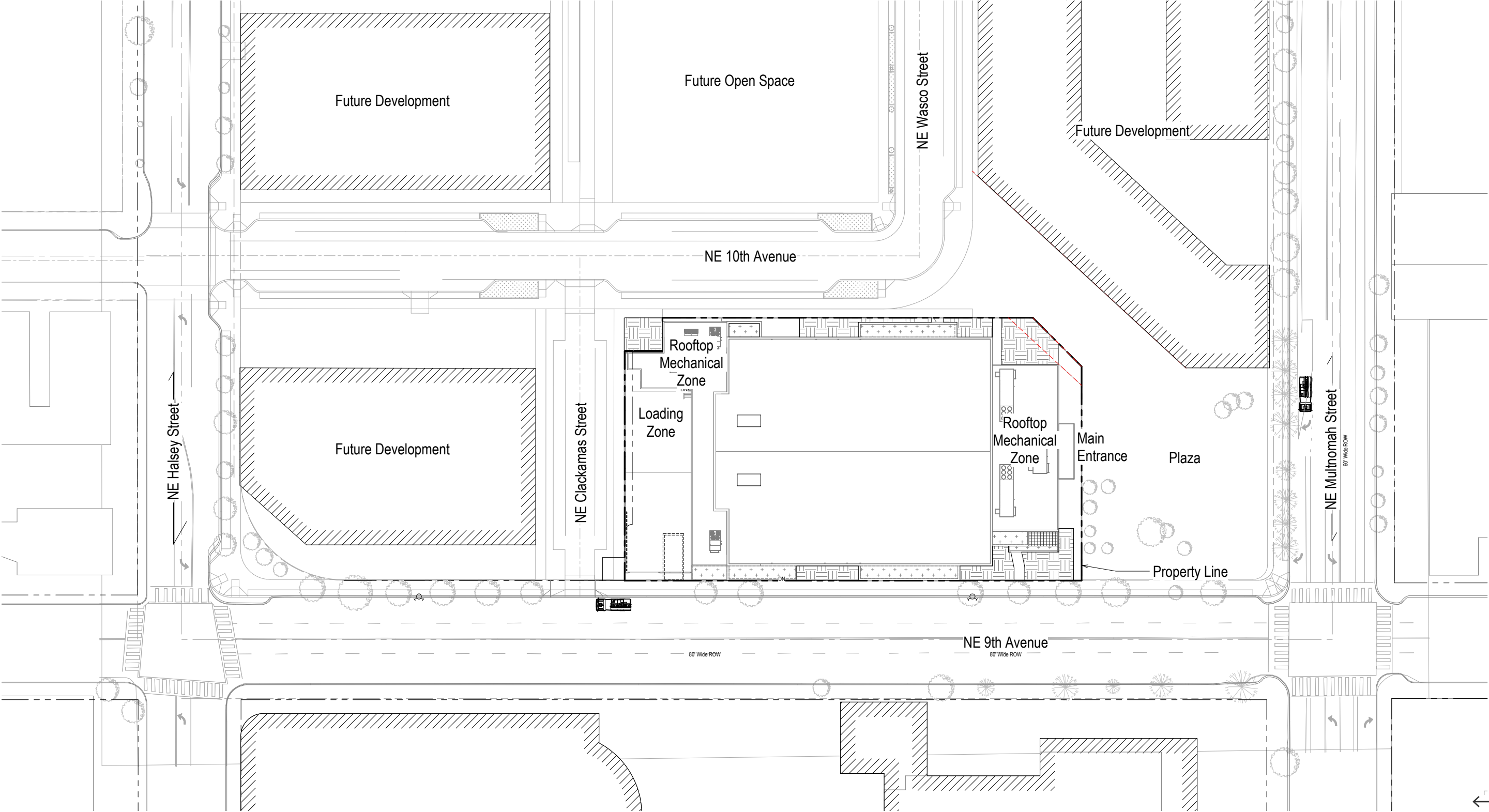


C3.12 PROJECT EVOLUTION - SITE FUTURE CONDITION 5

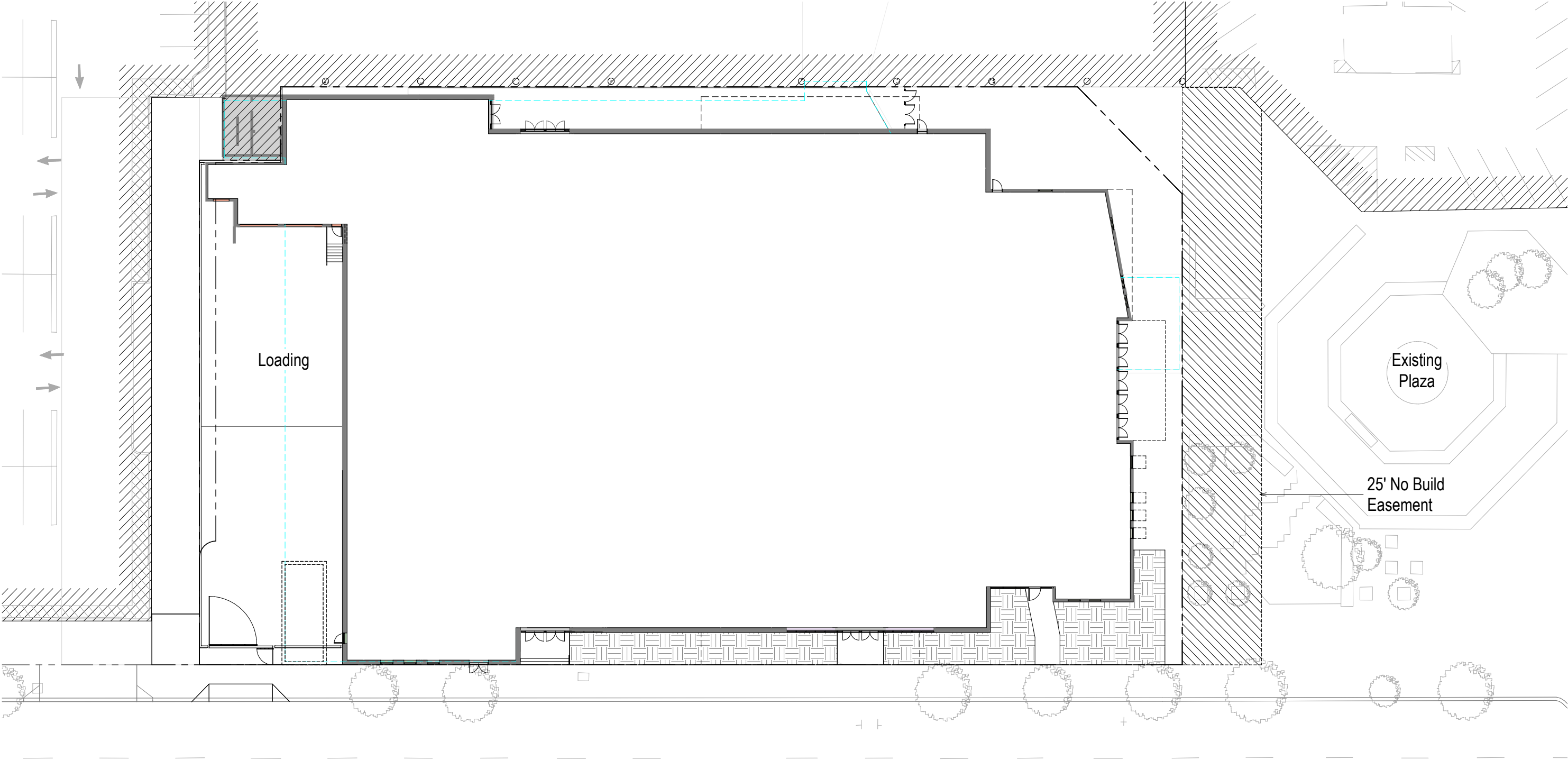




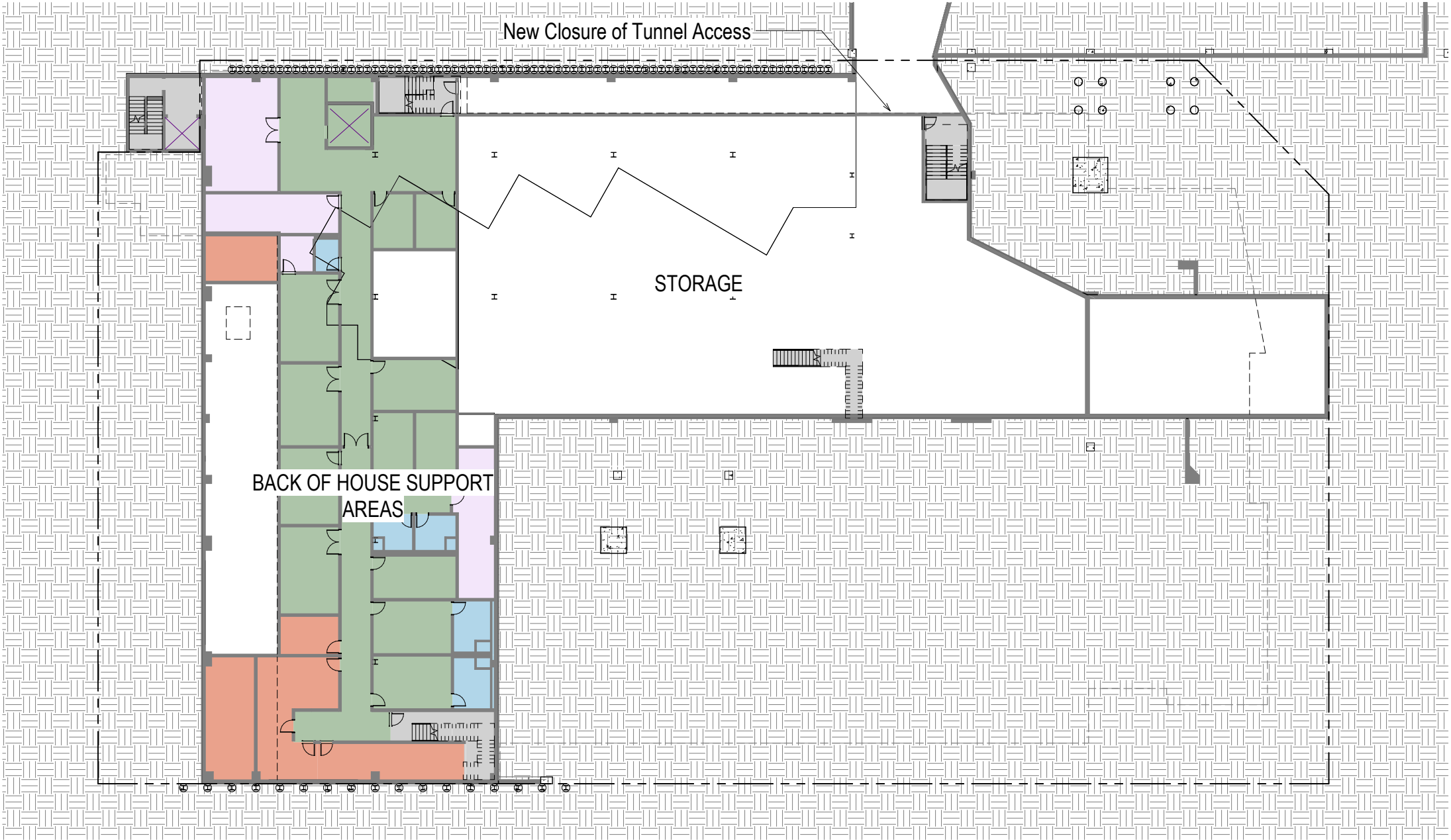
C3.14 PROPOSED SITE PLAN - FUTURE CONDITION






C3.15 PROPOSED SITE PLAN - ENLARGED



C3.16 PROPOSED BASEMENT PLAN









Main Entrance

Secondary Entrance

Vehicular Access







Service Areas

Core

Storage

BOH



Restrooms

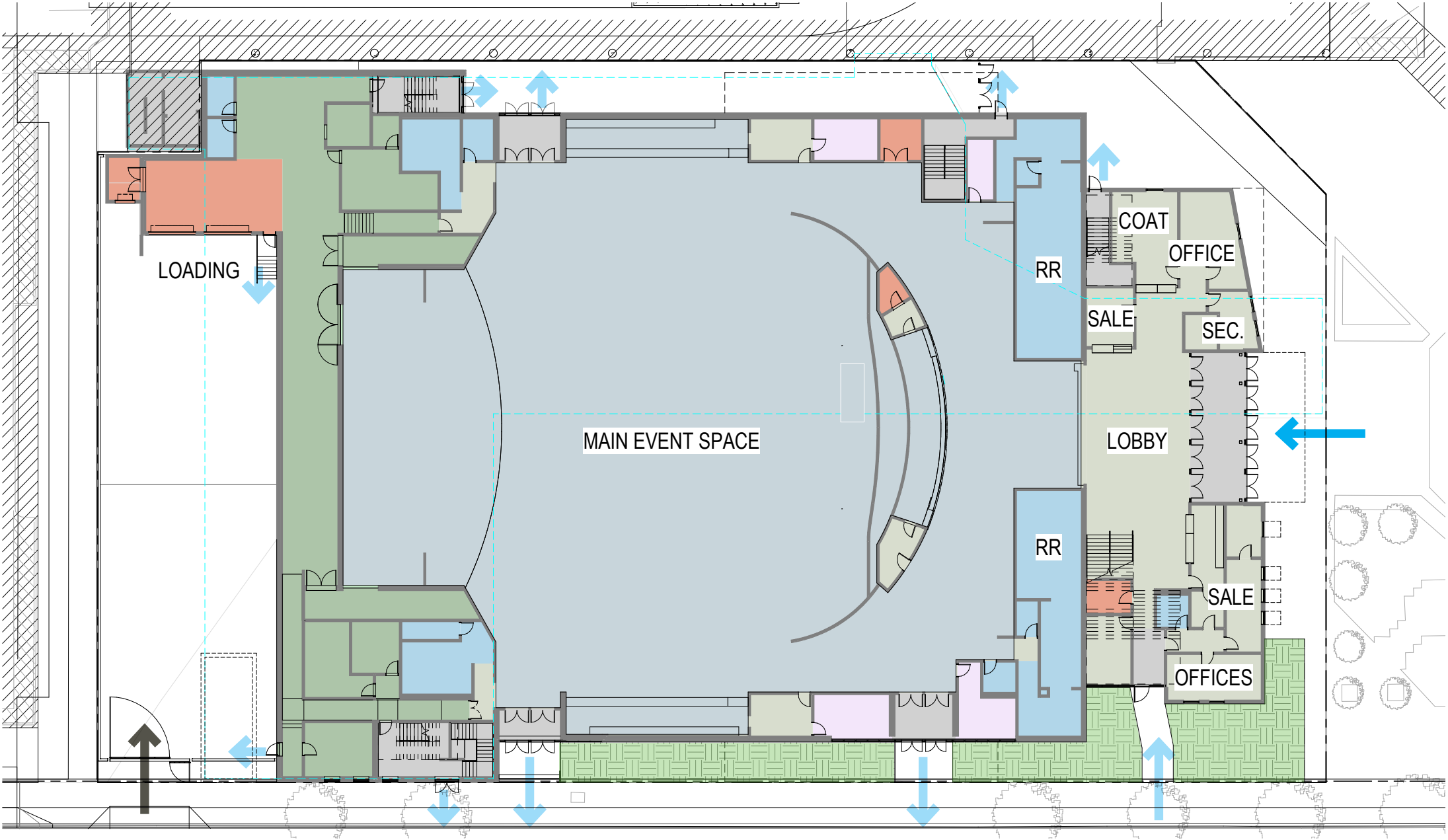
FOH




Event Space

Landscape

← N

(EA 24-029290 DA)









Main Entrance

Secondary Entrance

Vehicular Access







Service Areas

Core

Storage

BOH



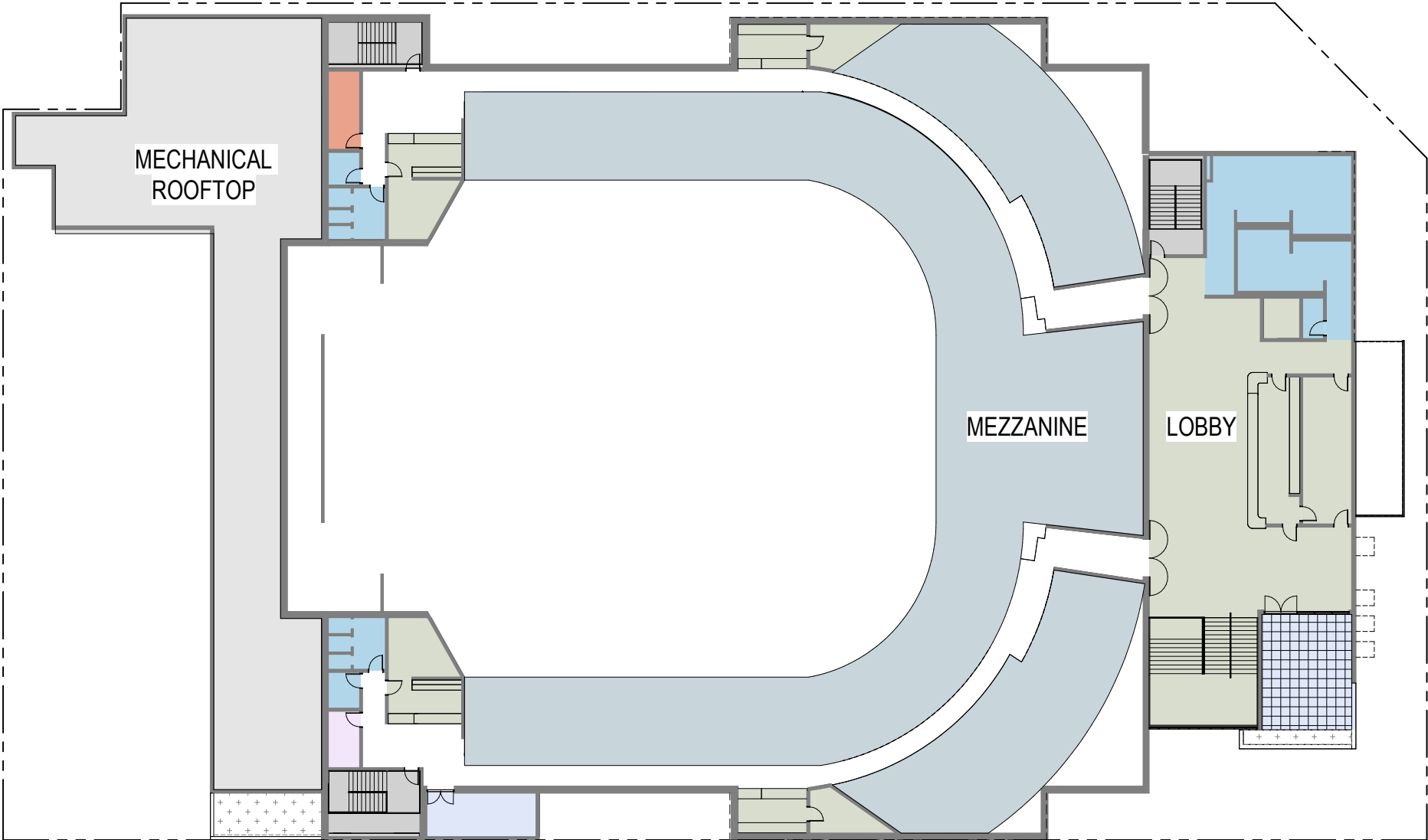
Restrooms




FOH





Event Space

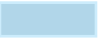


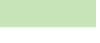
Landscape


← N



 Main Entrance
  Secondary Entrance
  Vehicular Access

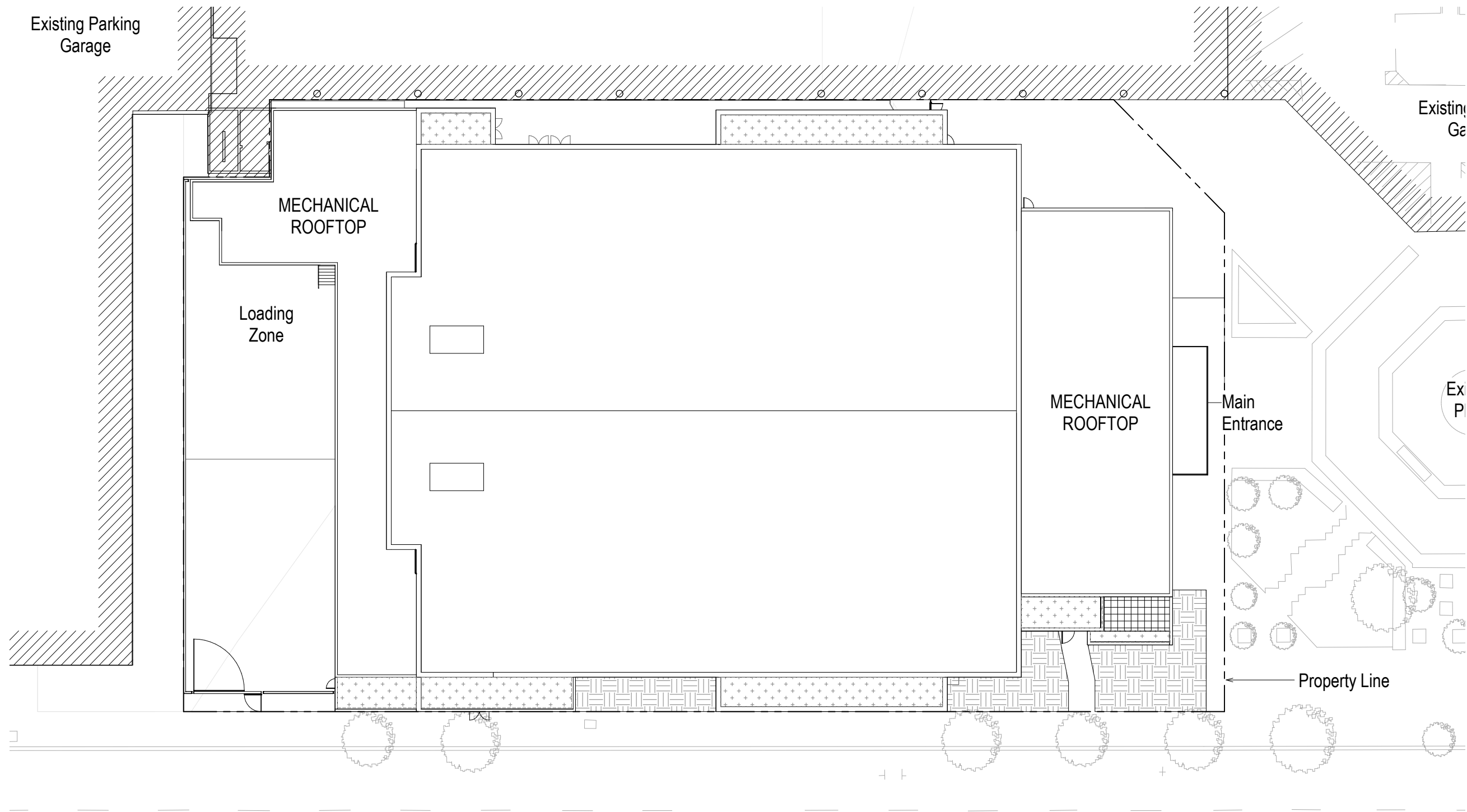
 Service Areas
  Core
  Storage
  BOH

 Restrooms
  FOH
  Event Space
  Landscape

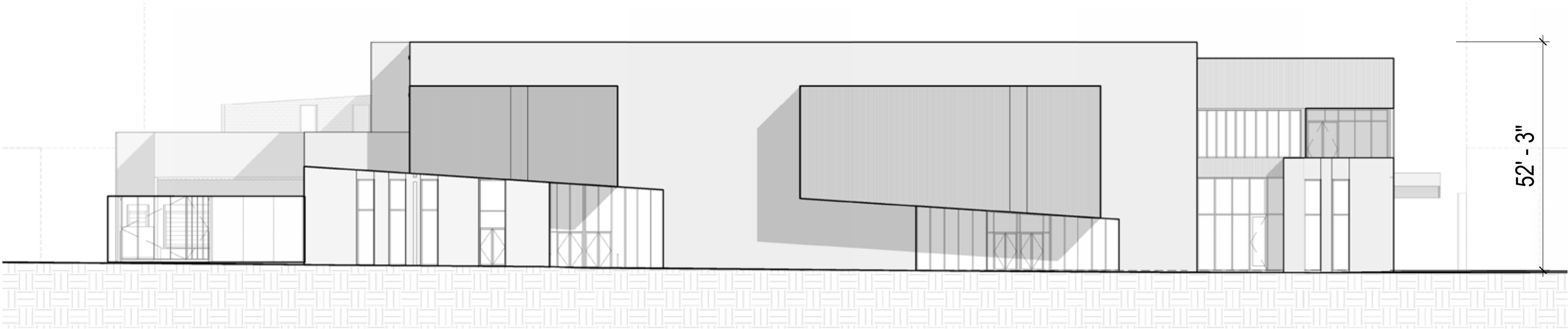
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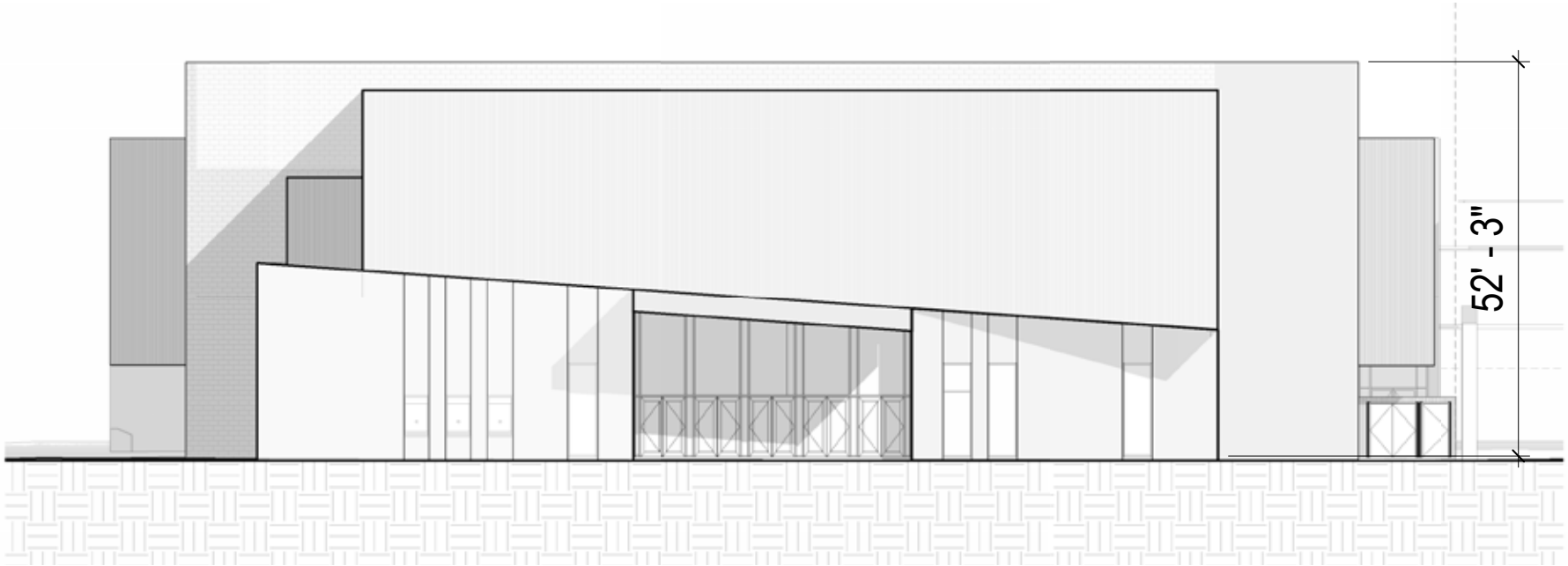
C3.19 PROPOSED ROOF PLAN



C3.20 PRELIMINARY BUILDING ELEVATIONS

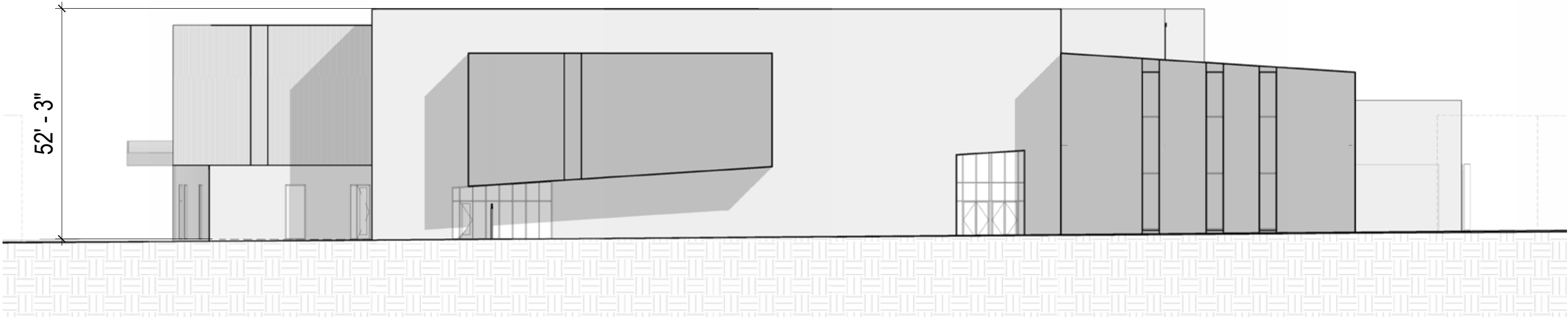


West Elevation - 9th Ave

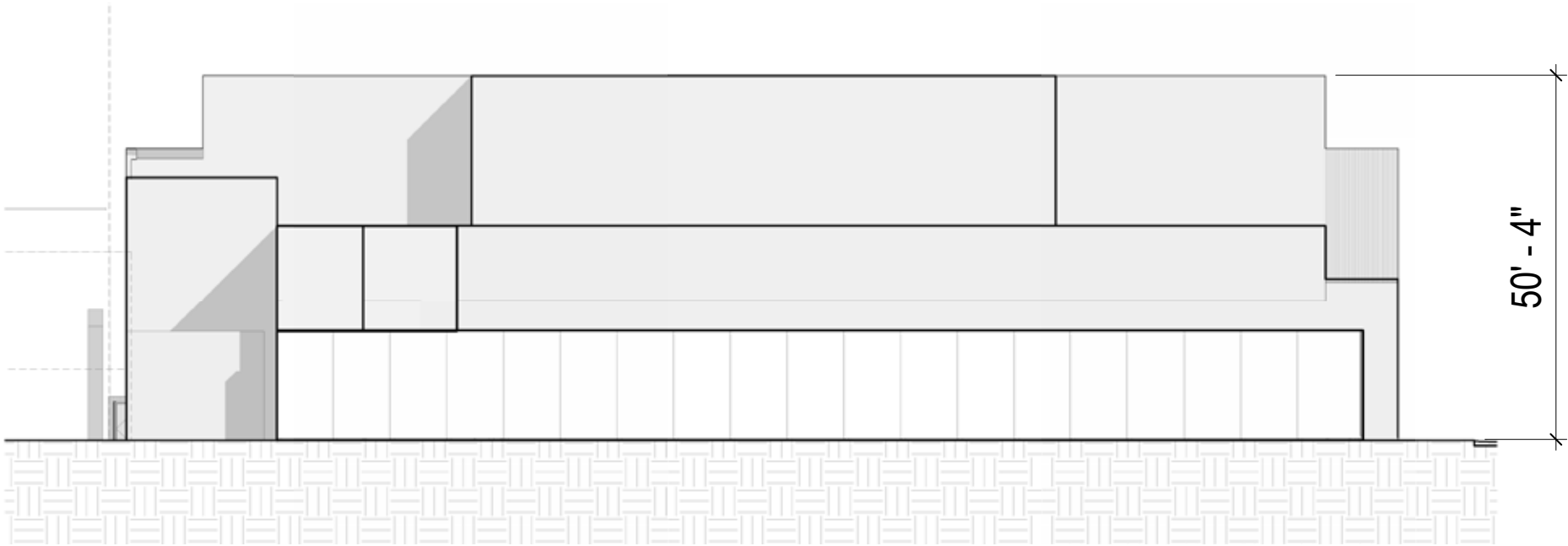


South Elevation - Plaza

C3.21 PRELIMINARY BUILDING ELEVATIONS



East Elevation



North Elevation

C3.22 MATERIAL CONCEPTS - LLOYD DISTRICT



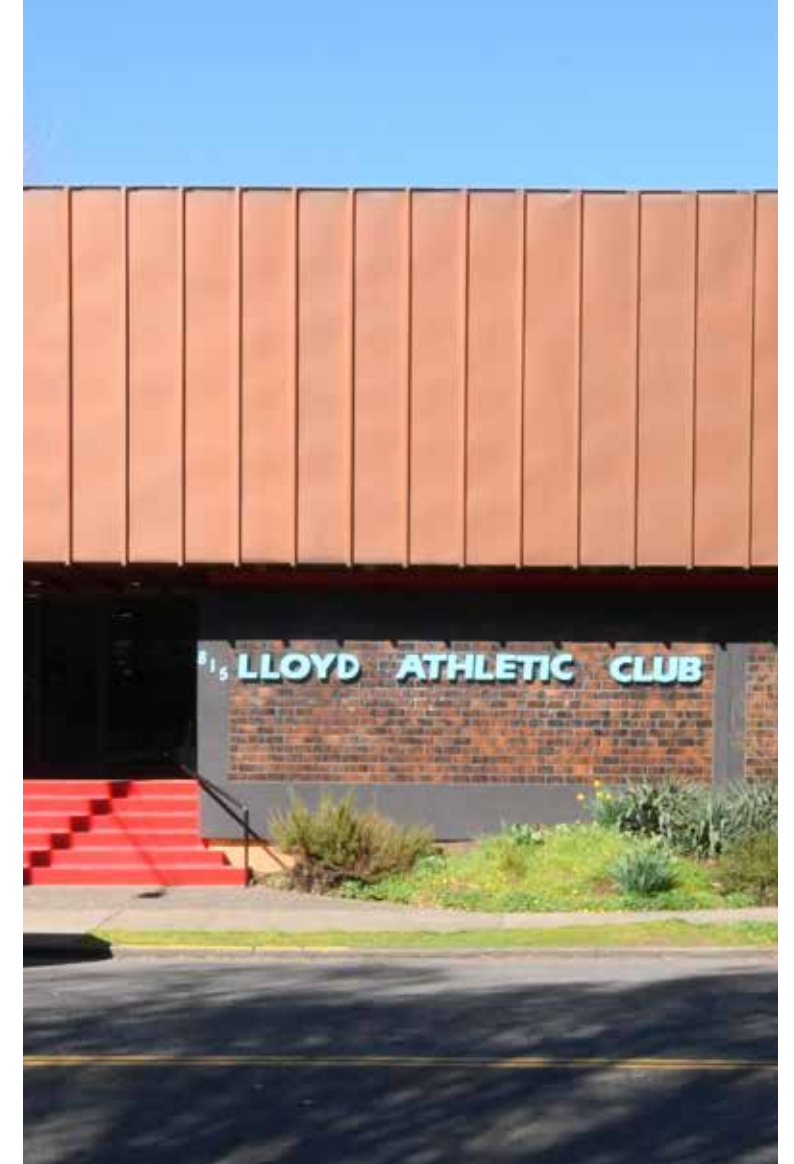
MASONRY
NE GRAND & HOLLADAY - HOTEL



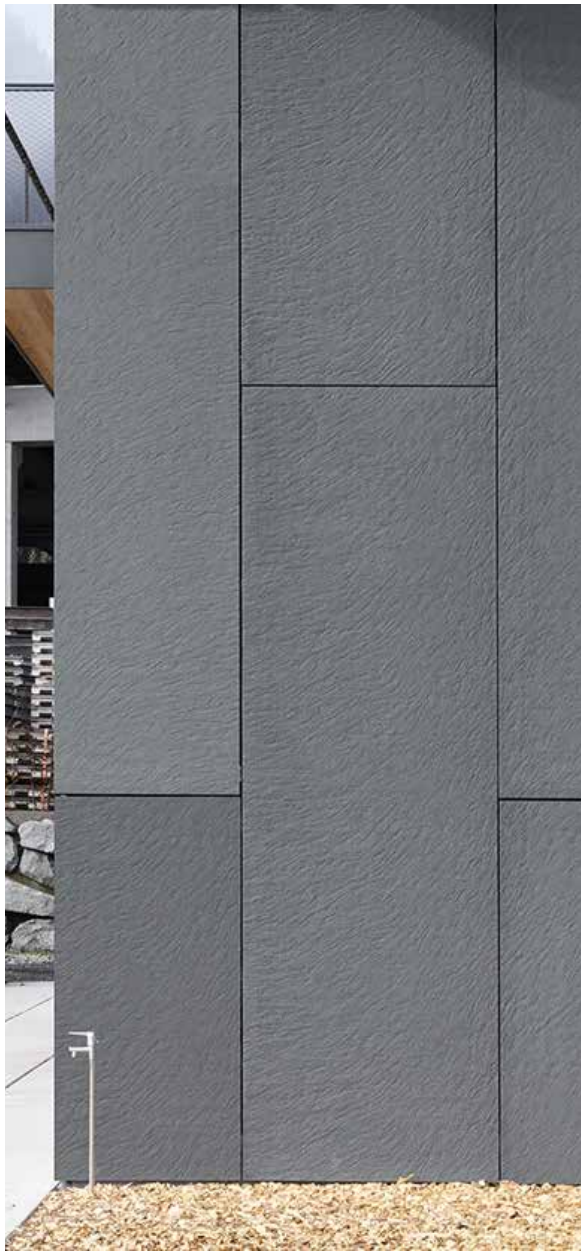
MASONRY & METAL PANEL
NE 7TH & HOLLADAY - APARTMENTS



MASONRY & METAL PANEL
N WEIDLER & WILLIAMS - APARTMENTS



MASONRY & METAL PANEL
NE 9TH & HALSEY - ATHLETIC CLUB



REINFORCED CONCRETE PANEL



METAL PANEL



PERFORATED METAL PANEL

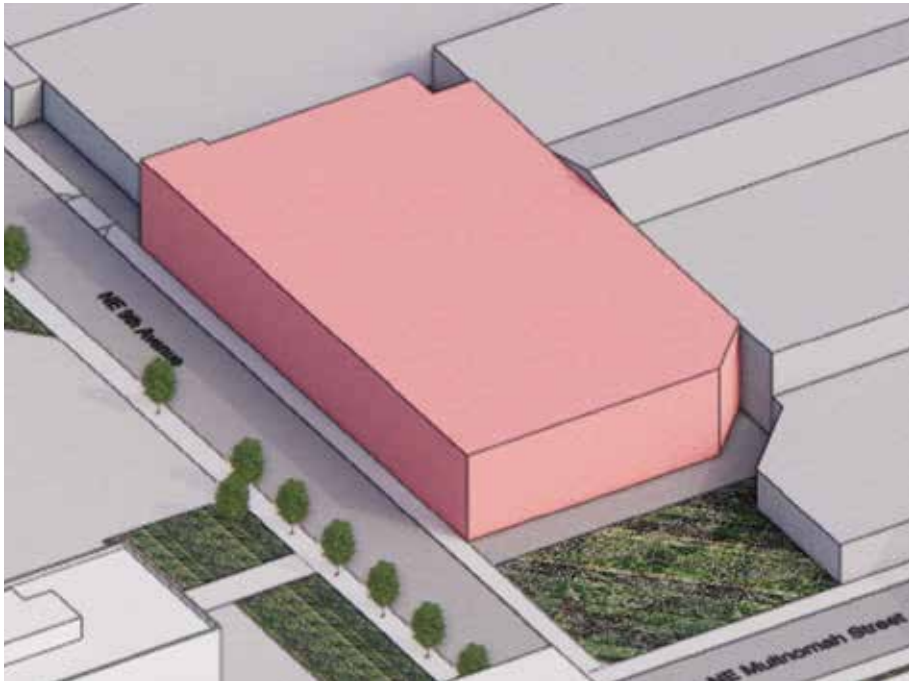


PAINTED CMU BLOCK

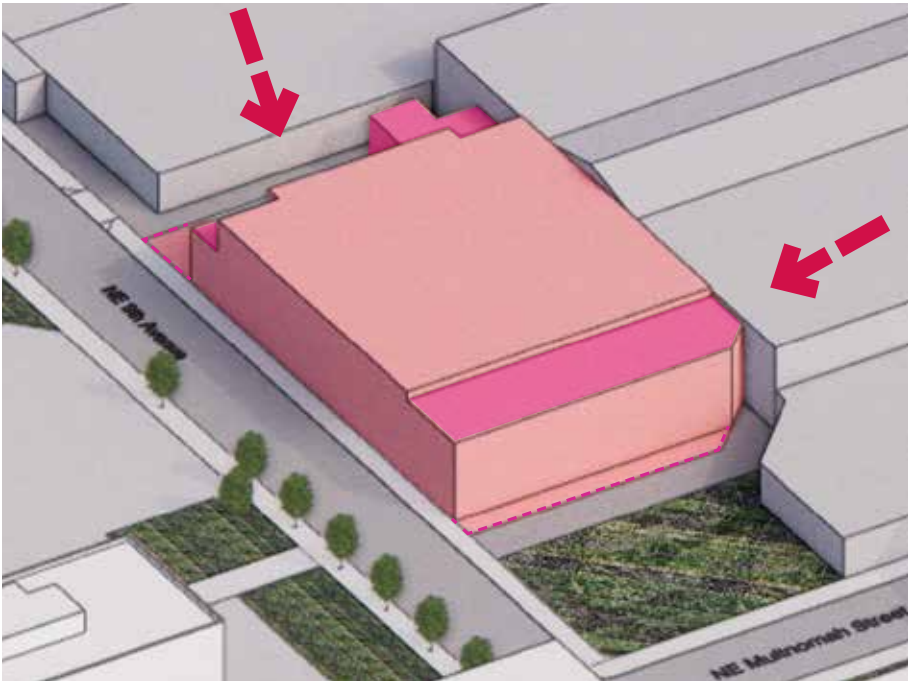


POLYCARBONATE PANEL

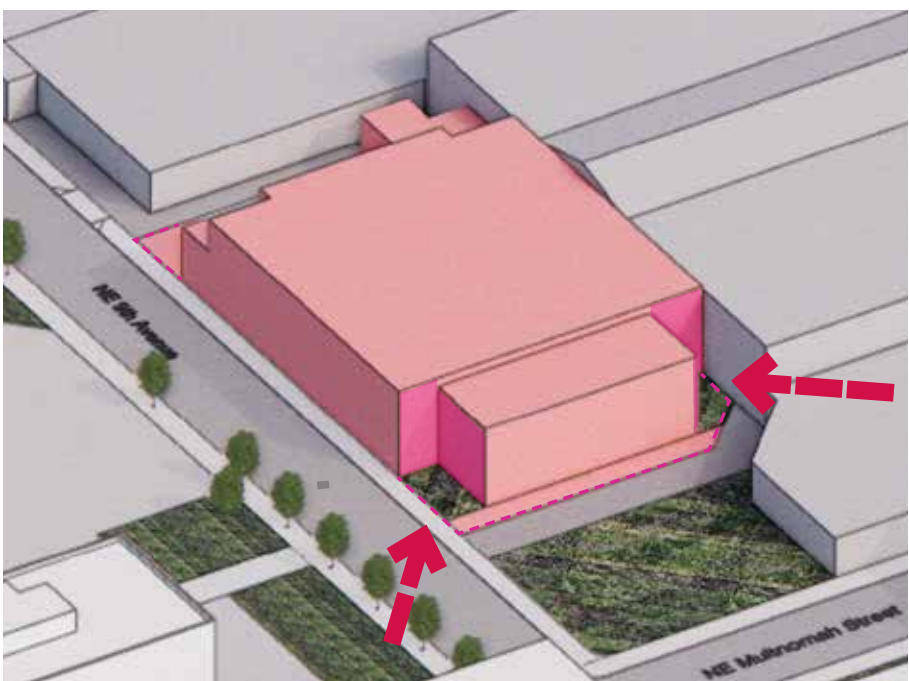
C3.24 PROJECT EVOLUTION - MASSING STRATEGY



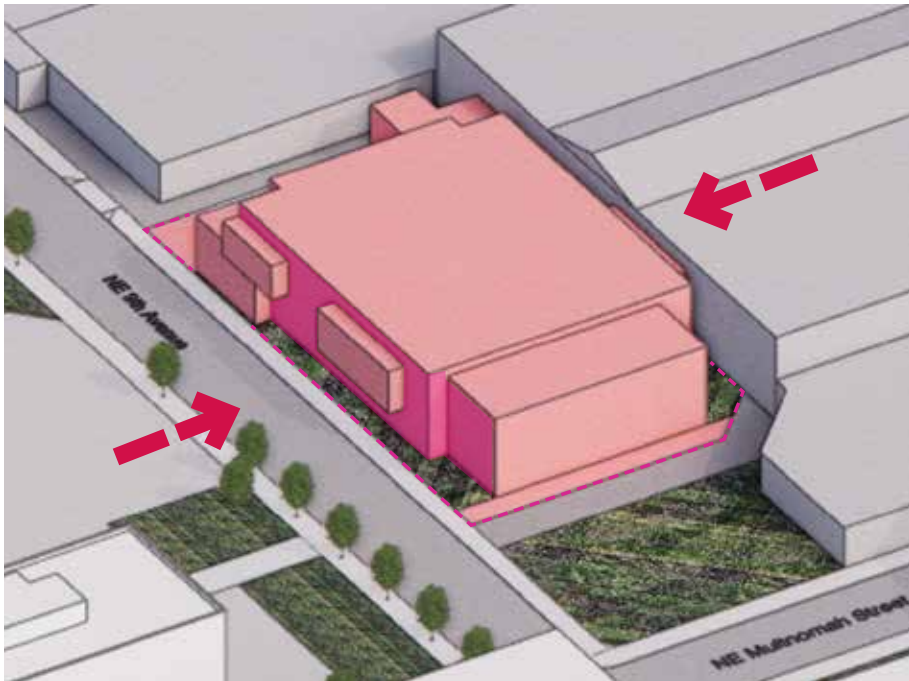
STEP 1
EXISTING PROPERTY LINE EXTENTS
- Maintains Street Wall
- Aligns to Existing Urban Context



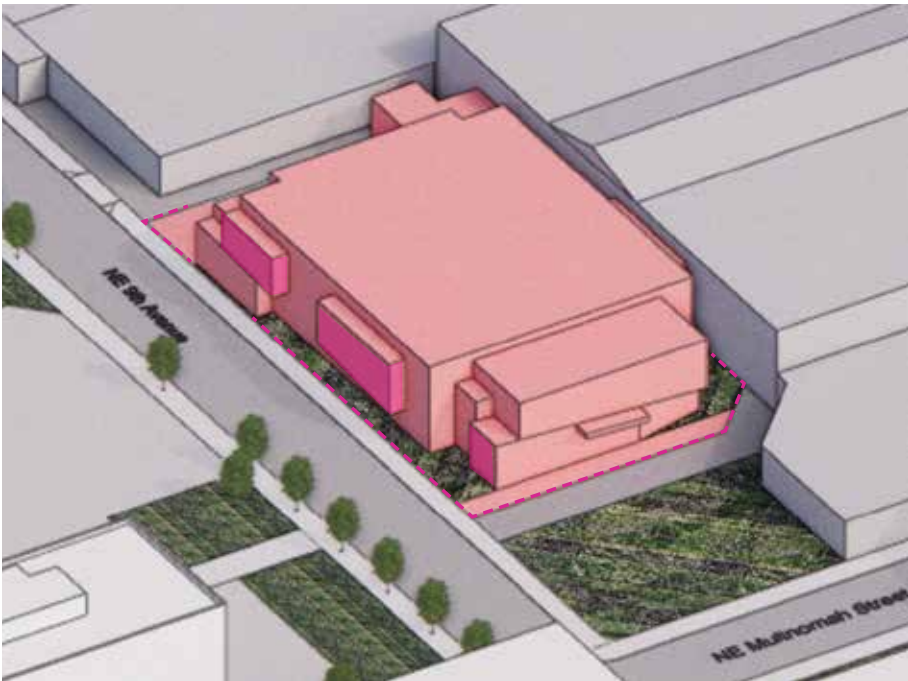
STEP 2
STEP DOWN UPPER FLOOR AT PLAZA & FUTURE RIGHT OF WAY
- Respond to Pedestrian Scale, Lloyd District Guideline C6



STEP 3
STEP BACK AT 9TH AVE & FOR FUTURE LLOYD MASTERPLAN
- Encourage Future Connection to Masterplan Development
- Maximize View Opportunities at corner, Lloyd District Guideline C2-1
- Provide a Distinct Sense of Entry and Exit to the Building, Lloyd District Guideline A9-1



STEP 4
RESPOND TO 9TH AVE & FUTURE 10TH AVE
- Reinforces Pedestrian Experience, Central City Guideline B1
- Incorporating Landscape as Integral Element of Design, Lloyd District Guideline A5-6



STEP 5
UNIFY PROJECTIONS
- Develop Identifying Features, Lloyd District Guideline A5-1



STEP 6
PUSH GREEN ROOF TO AREAS VIEWABLE TO PUBLIC
- Integrate Landscape as Integral Element of Design, Lloyd District Guideline A5-6

CENTRAL CITY PLAN DISTRICT, 33.510.215

B. REQUIRED BUILDING LINE STANDARDS

1. General Standards. Unless otherwise specified in Paragraphs B.2. through B.5., new development and major remodels in the RX, CX and EX zones must meet one of the following standards. Exterior walls of buildings designed to meet the requirements of this Paragraph must be at least 15 feet high measured from the finished sidewalk at the building’s edge:

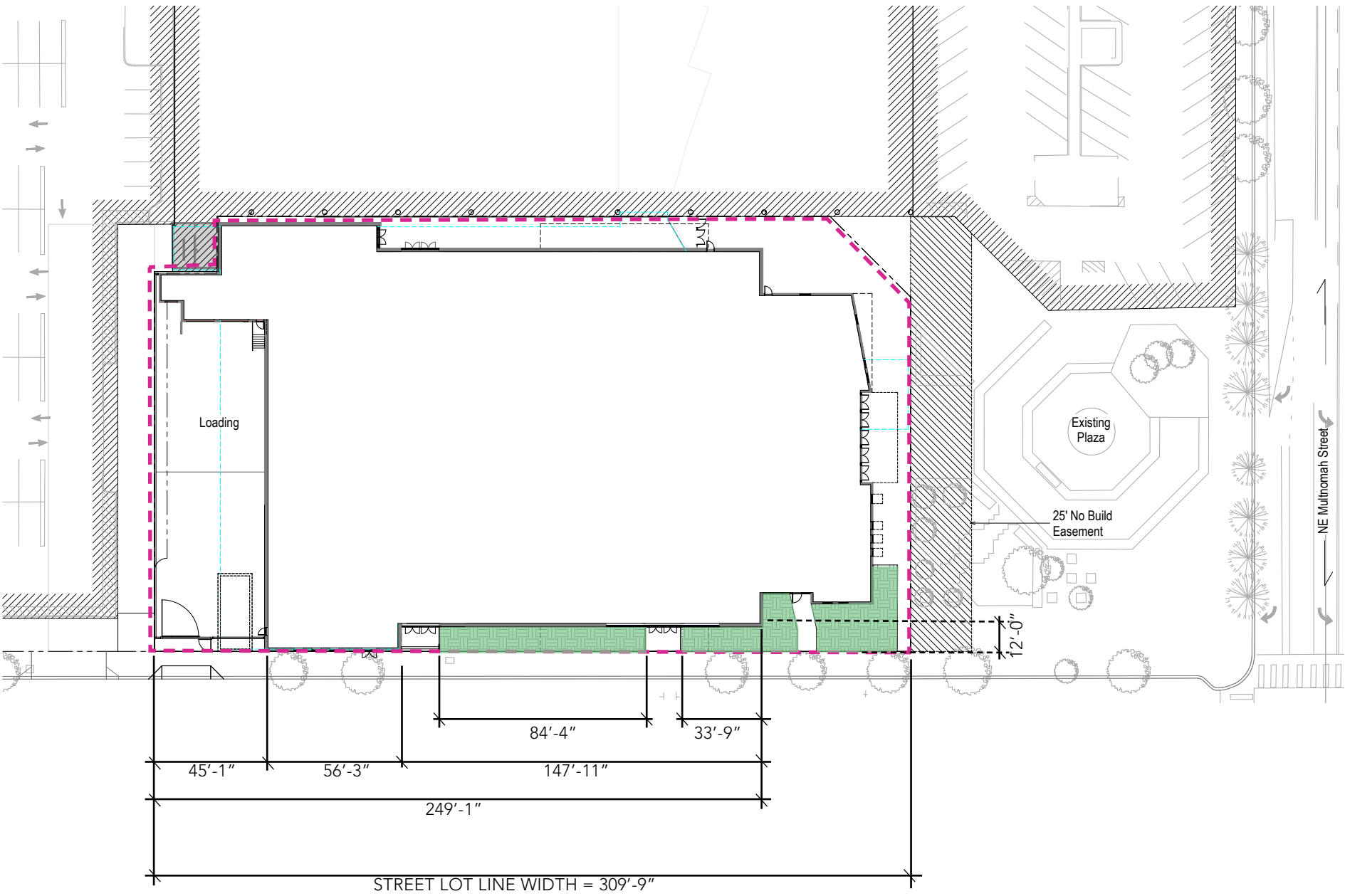
- a.The building must extend to the street lot line along at least 75 percent of the lot line; or
- b. **The building must extend to within 12 feet of the street lot line along at least 75 percent of the length of the street lot line.** The space between the building and the street lot line must be designed as an extension of the sidewalk and 510-30 Chapter 33.510 Title 33, Planning and Zoning 3/1/22 Central City Plan District committed to active uses such as sidewalk cafes, vendor’s stands, or developed as “stopping places.”

2. Standards for sites with frontage on a street shown on Map 510-7. New development and major remodels on a site with frontage on a street shown on Map 510-7 must meet one of the following standards. Exterior walls of buildings designed to meet the requirements of this Paragraph must be at least 15 feet high measured from the finished sidewalk at the building’s edge:

- a.The building must extend to the street lot line along at least 75 percent of the length of the street lot line;or
- b. The building must set back at least 6 feet from the street lot line along at least 75 percent of the length of the street lot line. The space between the building and the street lot line must be landscaped as follows. All plants must be selected from the Portland Tree and Landscaping manual:

- (1) **When the setback area is at least 6 feet and less than 12 feet wide at least 50 percent of the setback must be landscaped with ground cover plants and shrubs;**
- (2) When the setback area is 12 feet wide or more, at least 80 percent of the setback area must be landscaped with ground cover plants and shrubs, and contain one tree per 400 square feet of the setback area.

C3.25 CONFIRMATION OF CRITERIA - BUILDING LINES



CONFIRMATION OF CRITERIA
33.510.215 B.1.B

Overall Street Lot Line Width	=	309'-9"
75% of Street Lot Line Width	=	232'-4"
Building Length at Lot line	=	56'-1"
Building within 12' of street lot line	=	193'-0"
Total	=	249'-1"

CONFIRMATION OF CRITERIA
33.510.215 B.2.B.1

Setback within 6' - 12'	=	147'-11"
50% of Setback Width	=	73'- 11.5"
Setback Width Planted	=	118'-1"
% of Setback Width Planted	=	80%

CENTRAL CITY PLAN DISTRICT, 33.510.220

B. GROUND FLOOR WINDOWS

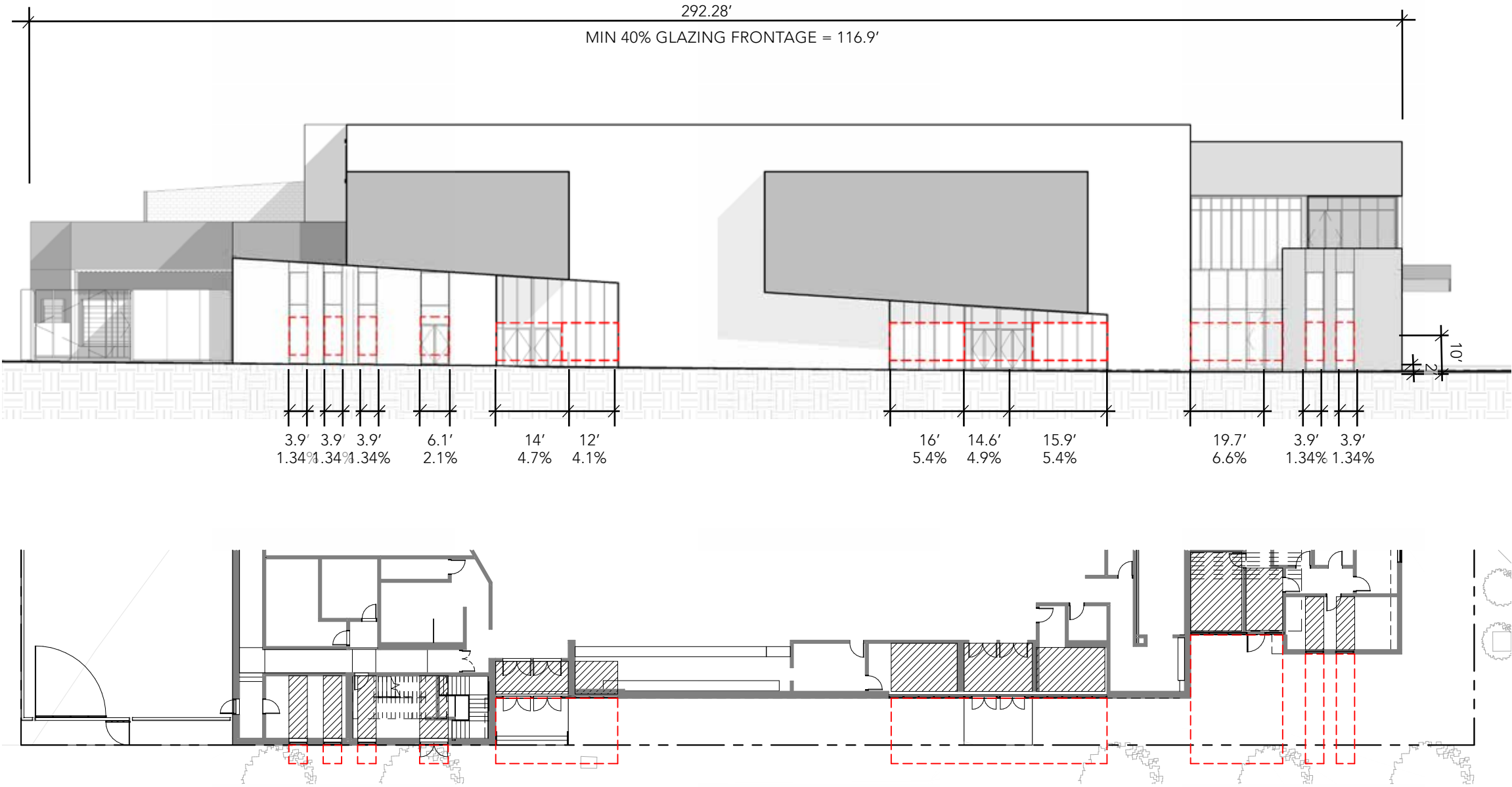
1. The following ground floor window standards apply in the RX, CX and EX zones. The standards of B.1 and B.2 apply to new development and major remodeling projects. B.3. only applies to major remodeling projects. To meet the standards, ground floor windows must be windows that allow views into work areas or lobbies, or be windows in pedestrian entrances. Windows into storage areas, vehicle parking areas, garbage and recycling areas, mechanical and utility areas and display cases attached to outside walls do not qualify. Windows into bicycle parking areas are allowed to qualify for up to 25 percent of the ground floor windows coverage requirement. The bottom of the windows of nonresidential spaces must be no more than 4 feet above the finished grade:

1. Ground level facades that face a street or open area shown on Map 510-8 must have windows that cover at least 60 percent of the ground level wall area. For the purposes of this standard, ground level wall area includes all exterior wall area from 2 feet to 10 feet above the finished grade.

2. All other ground level facades that face a street lot line, sidewalk, plaza, or other publicly accessible open area or right-of-way must have windows that cover at least 40 percent of the ground level wall area. For street facing facades of dwelling units the regulations of 33.130.230.B.4 apply. For the purposes of this standard, ground level wall area includes all exterior wall area from 2 feet to 10 feet above the finished grade.

3. Optional artwork. Projects proposing to use artwork as an alternative to the ground floor window requirements may apply for this through the adjustment procedure. Projects may also apply for a modification through design review if they meet the following qualifications. Buildings having more than 50 percent of their ground level space in storage, parking, or loading areas, or in uses which by their nature are not conducive to windows (such as theaters), may be allowed to use the design review process. Artwork and displays relating to activities occurring within the building are encouraged. In these instances, the artwork will be allowed if it is found to be consistent with the purpose for the ground floor window standard.

C3.26 CONFIRMATION OF CRITERIA - GROUND FLOOR WINDOWS



CENTRAL CITY PLAN DISTRICT, 33.510.220

B. GROUND FLOOR WINDOWS

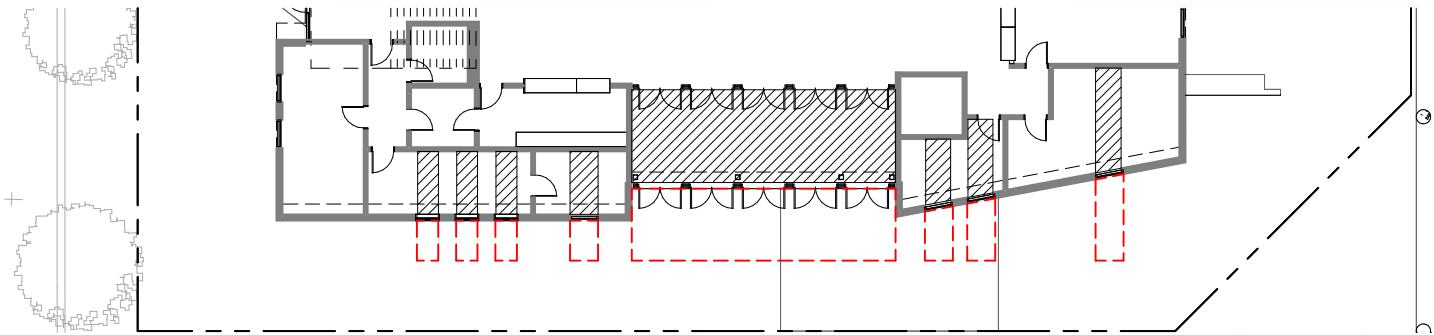
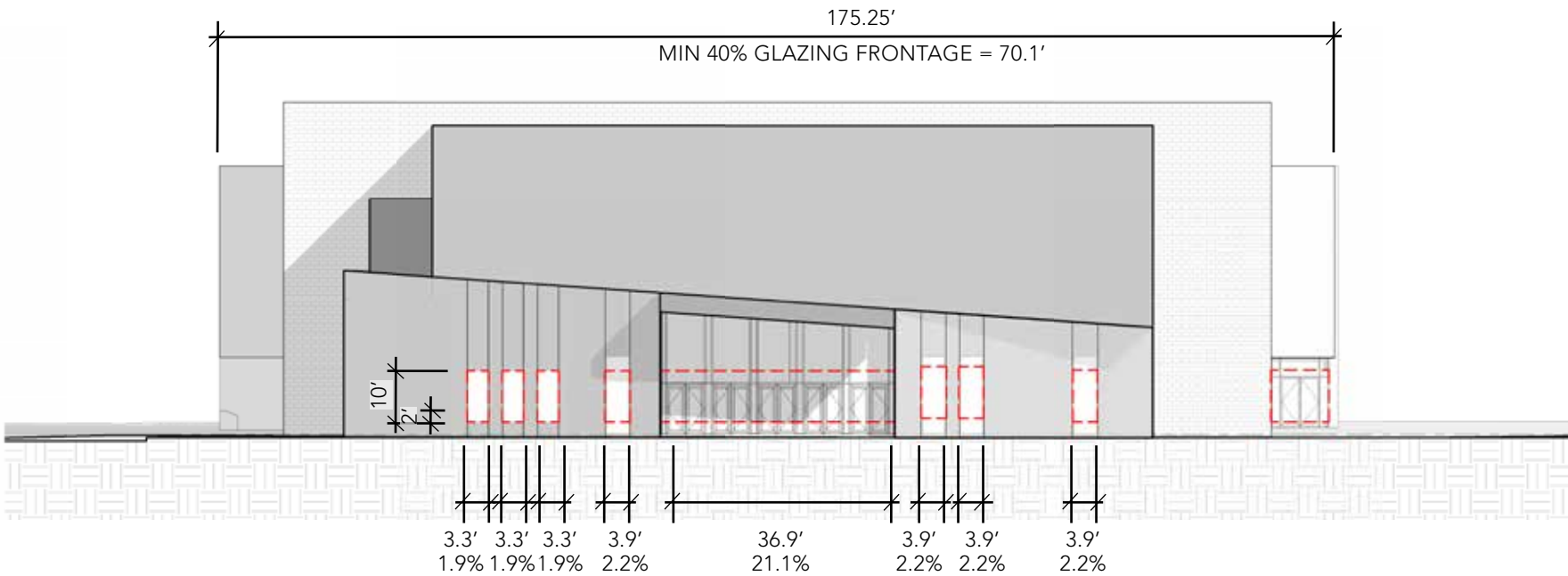
1. The following ground floor window standards apply in the RX, CX and EX zones. The standards of B.1 and B.2 apply to new development and major remodeling projects. B.3. only applies to major remodeling projects. To meet the standards, ground floor windows must be windows that allow views into work areas or lobbies, or be windows in pedestrian entrances. Windows into storage areas, vehicle parking areas, garbage and recycling areas, mechanical and utility areas and display cases attached to outside walls do not qualify. Windows into bicycle parking areas are allowed to qualify for up to 25 percent of the ground floor windows coverage requirement. The bottom of the windows of nonresidential spaces must be no more than 4 feet above the finished grade:

1. Ground level facades that face a street or open area shown on Map 510-8 must have windows that cover at least 60 percent of the ground level wall area. For the purposes of this standard, ground level wall area includes all exterior wall area from 2 feet to 10 feet above the finished grade.

2. All other ground level facades that face a street lot line, sidewalk, plaza, or other publicly accessible open area or right-of-way must have windows that cover at least 40 percent of the ground level wall area. For street facing facades of dwelling units the regulations of 33.130.230.B.4 apply. For the purposes of this standard, ground level wall area includes all exterior wall area from 2 feet to 10 feet above the finished grade.

3. Optional artwork. Projects proposing to use artwork as an alternative to the ground floor window requirements may apply for this through the adjustment procedure. Projects may also apply for a modification through design review if they meet the following qualifications. Buildings having more than 50 percent of their ground level space in storage, parking, or loading areas, or in uses which by their nature are not conducive to windows (such as theaters), may be allowed to use the design review process. Artwork and displays relating to activities occurring within the building are encouraged. In these instances, the artwork will be allowed if it is found to be consistent with the purpose for the ground floor window standard.

C3.27 CONFIRMATION OF CRITERIA - GROUND FLOOR WINDOWS



South Elevation Width = 175.25'
Min 40% Frontage Coverage = 70.1'
Glazing Coverage Shown = 71.65'

CENTRAL CITY PLAN DISTRICT, 33.510.243

B. ECOROOF STANDARD

Ecoroof standard. In the CX, EX, RX, and IG1 zones, new buildings with a net building area of 20,000 square feet or more must have an ecoroof that meets the following standards:

1. The ecoroofs, including required firebreaks between ecoroofs areas, **must cover 100 percent of the building roof area, except that up to 40 percent of the building roof area can be covered with a combination of the following.** Roof top parking does not count as roof area. Roof area that has a slope greater than 25% does not count as roof area:

a. Mechanical equipment, housing for mechanical equipment, and required access to, or clearance from, mechanical equipment;

b. Areas used for fire evacuation routes;

c. Stairwell and elevator enclosures;

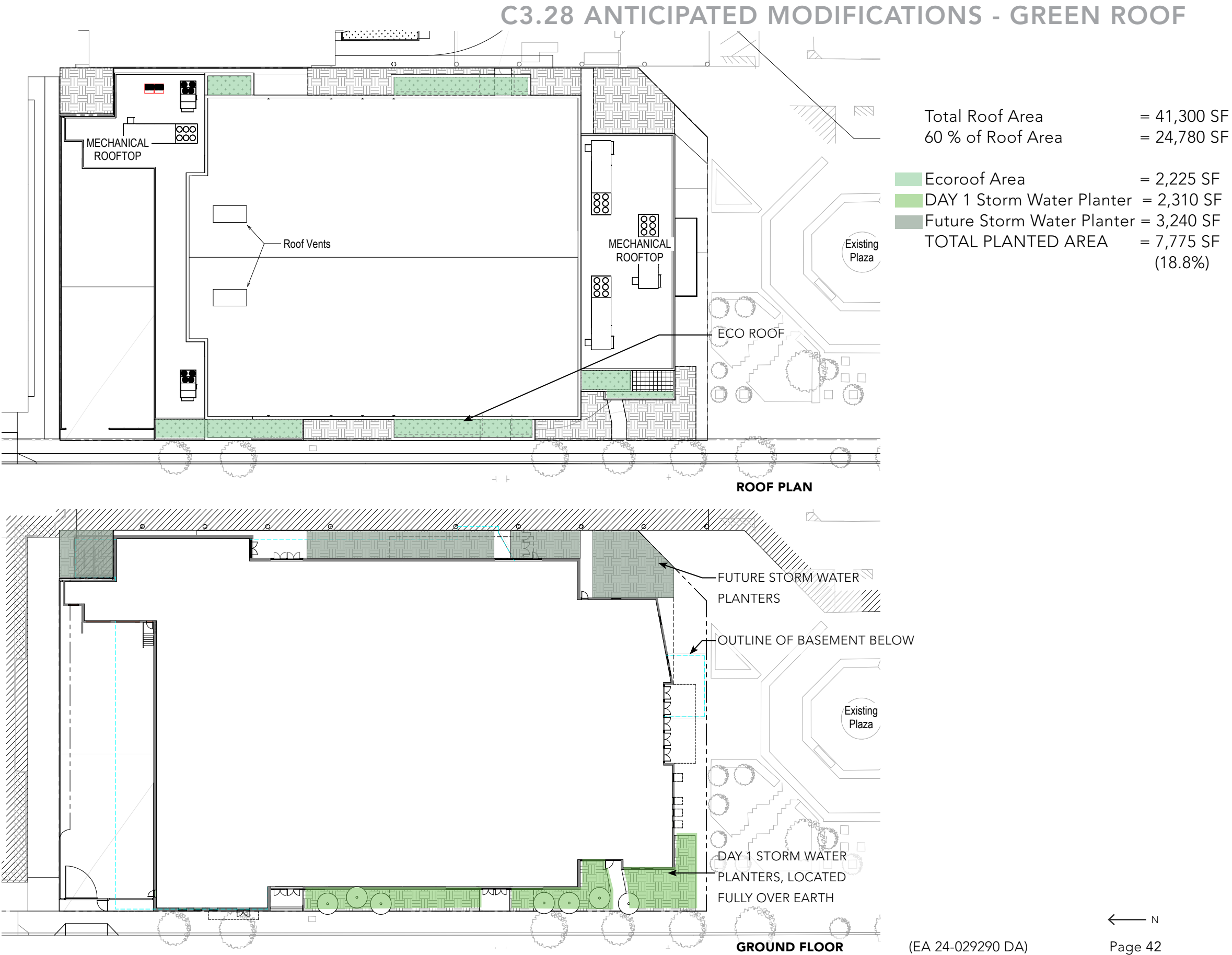
d. Skylights;

e. Solar panels;

f. Wind turbines;

g. Equipment, such as pipes and pre-filtering equipment, used for capturing or directing rainwater to a rainwater harvesting system; or

h. Uncovered common outdoor areas. Common outdoor areas must be accessible through a shared entrance.
2. The ecoroof must be approved by the Bureau of Environmental Services as meeting the Stormwater Management Manual's Ecoroof Facility Design Criteria.



CENTRAL CITY DESIGN GUIDELINES

B6 - DEVELOP WEATHER PROTECTION

Develop integrated weather protection systems at the sidewalk-level of buildings to mitigate the effects of rain, wind, glare, shadow, reflection, and sunlight on the pedestrian environment

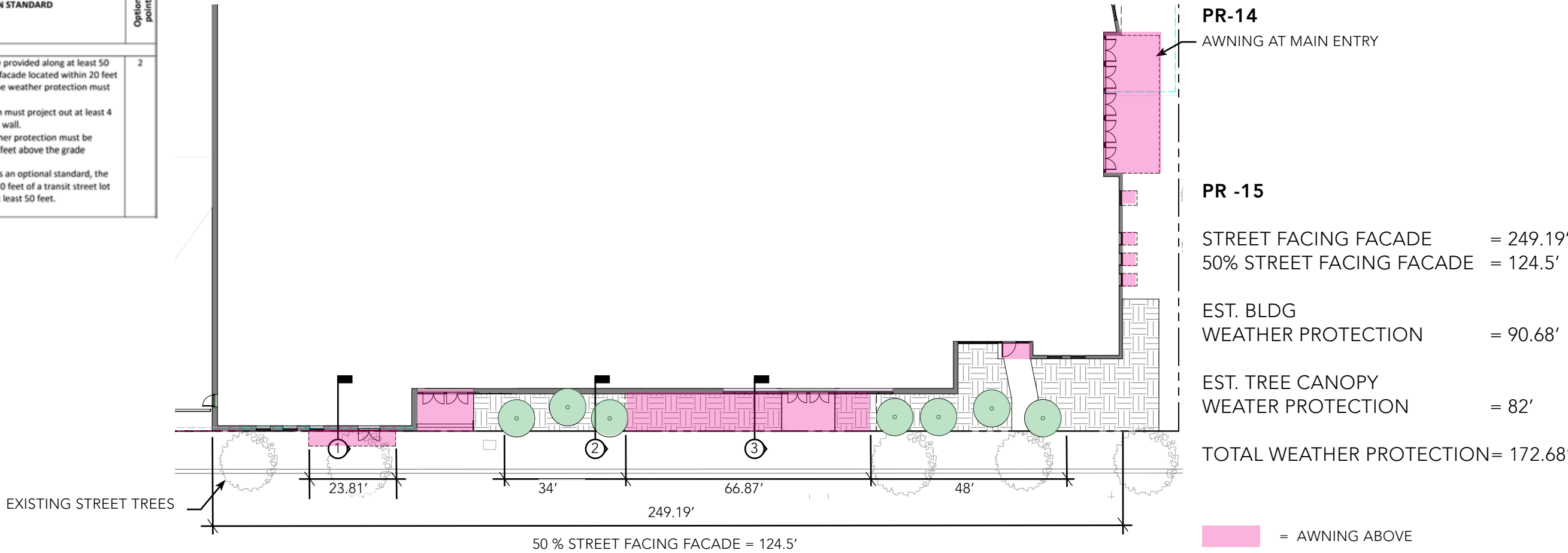
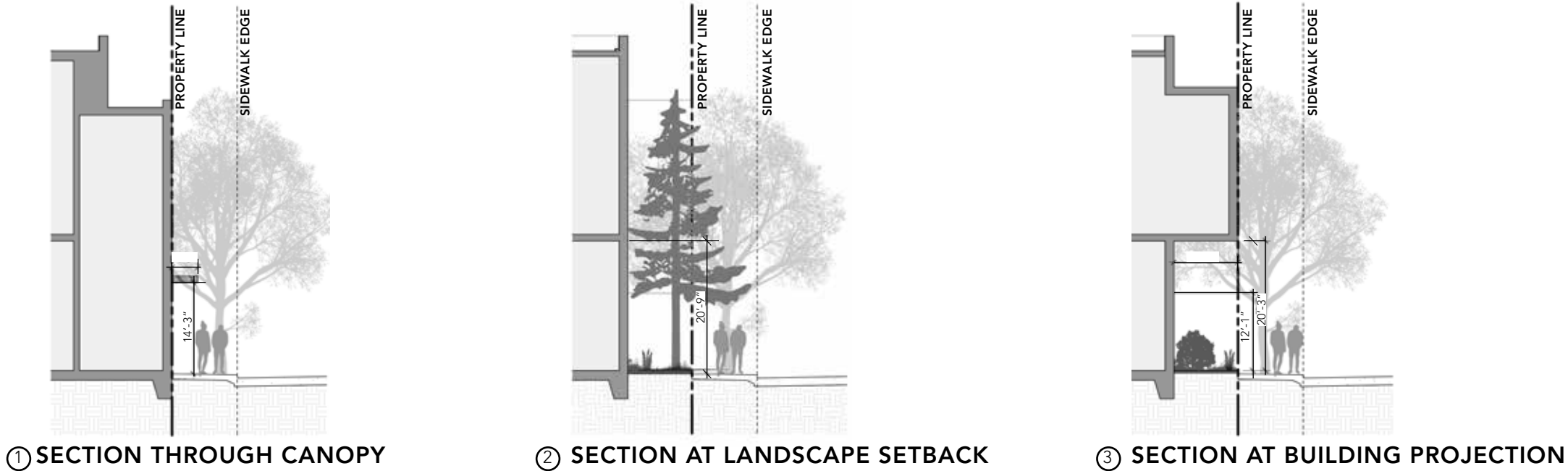
33.420 DESIGN OVERLAY ZONE

PUBLIC REALM, PR 14 & PR 15

WEATHER PROTECTION			
X	PR14	Weather Protection at Entrances	
	New main entrances at a new or existing building		Provide weather protection at new main entrances that face a street lot line. The weather protection may be an awning, a portion of the building, a balcony, or other covered structure. The weather protection must meet the following: <ul style="list-style-type: none">The weather protection must project out at least 4 feet from the wall above the doorway;The weather protection is four feet wider than the doorway, unless there is a building wall that prohibits this width; andThe height of the weather protection must be between 9 feet and 15 feet above the grade underneath it.

Required (X)	APPLIES TO:	THE DESIGN STANDARD	Optional points
X	PR15	Weather Protection Along a Transit Street	2
	New building on a transit street as follows: The standard is required for a new building with a height that exceeds 55 feet. The standard is optional for new buildings that are 55 feet or less in height		
	Weather protection must be provided along at least 50 percent of the street-facing facade located within 20 feet of a transit street lot line. The weather protection must meet the following: <ul style="list-style-type: none">The weather protection must project out at least 4 feet from the adjoining wall.The height of the weather protection must be between 9 feet and 15 feet above the grade underneath it. When this standard is met as an optional standard, the street facing facade within 20 feet of a transit street lot line must have a length of at least 50 feet.		

C3.29 CONFIRMATION OF GUIDELINES - WEATHER PROTECTION



PDOT, TRN-10.40 DRIVEWAYS - OPERATION & LOCATION

D. ADMINISTRATIVE RULE FOR DRIVEWAY OPERATION & LOCATION

1. Separation from Property Corner – No portion of a driveway, excluding wings if required, shall be located closer than 25 feet from the corner of a lot where two streets intersect, including alley intersections. This includes the frontage along the top of a “T” intersection. See Figure 1. For a corner lot with large radius property corners, a driveway shall be placed fully within the tangent segment. Narrow corner lots as referenced in Portland City Code Title 33 with a single house are exempt per Paragraph D.3., below.

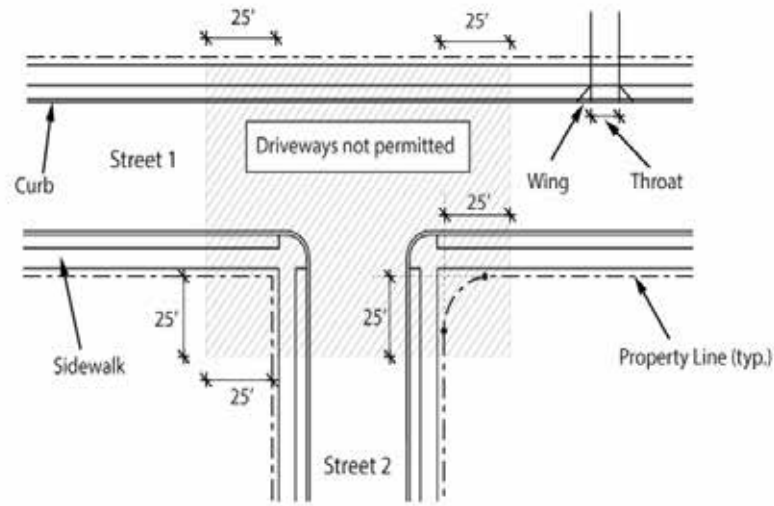
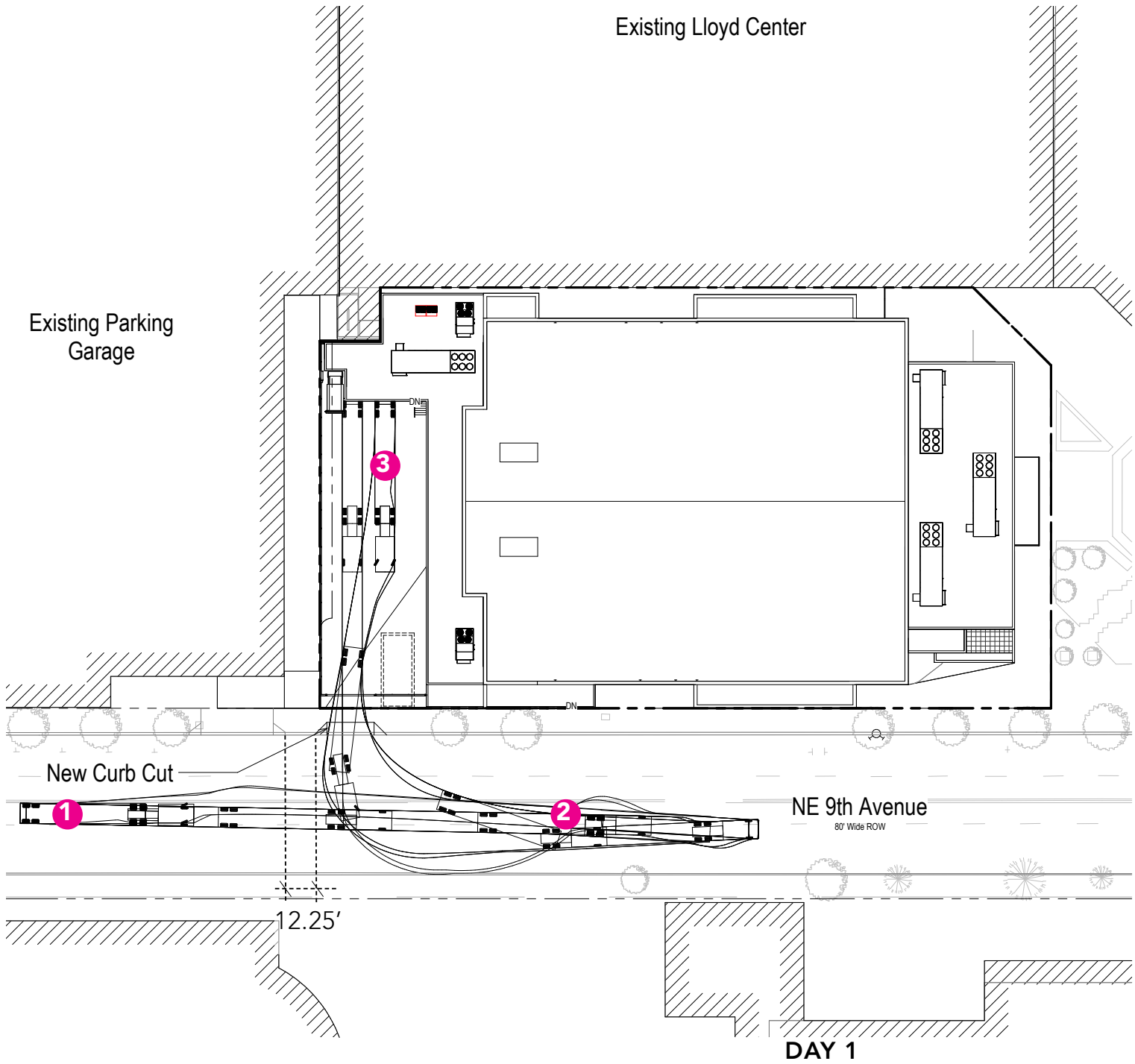


Figure 1

E. DRIVEWAY DIMENSIONS

2. 2. A minimum of 5 feet of straight curb must separate driveways regardless of the type of driveway proposed or property ownership. Where no curb exists, a minimum of 11 feet must separate driveway edges that face each other.

C3.30 CONFIRMATION OF CRITERIA - ROW & CURB CUT



PORTLAND ZONING CODE 33.266.200
 B. NUMBER OF SPACES REQUIRED

Table 266-6 Minimum Required Bicycle Parking Spaces [1]					
		Long-term Spaces		Short-term Spaces	
Uses	Specific Uses	Standard A	Standard B	Standard A	Standard B
Commercial Categories					
Retail Sales and Services		2, or 1 per 3,800 sq. ft. of net building area	2, or 1 per 7,500 sq. ft. of net building area	2, or 1 per 2,700 sq. ft. of net building area	2, or 1 per 4,400 sq. ft. of net building area
	Temporary lodging	2, or 1 per 20 rentable rooms	2, or 1 per 20 rentable rooms	2, or 1 per 40 rentable rooms; and 1 per 5,000 sq. ft. of conference, meeting room	2, or 1 per 40 rentable rooms; and 1 per 10,000 sq. ft. of conference, meeting room
	Restaurant and Bar	2, or 1 per 2,300 sq. ft. of net building area	2, or 1 per 4,800 sq. ft. of net building area	2, or 1 per 1,000 sq. ft. of net building area	2, or 1 per 1,600 sq. ft. of net building area
Office		2, or 1 per 1,800 sq. ft. of net building area	2, or 1 per 3,500 sq. ft. of net building area	2, or 1 per 20,000 sq. ft. of net building area	2, or 1 per 33,000 sq. ft. of net building area
Commercial Parking [3]		10, or 1 per 10 auto spaces	10, or 1 per 10 auto spaces	None	None
Commercial Outdoor Recreation		2, or 1 per 12,500 sq. ft. of net building area	2, or 1 per 25,000 sq. ft. of net building area	2, or 1 per 2 acres	2, or 1 per 3 acres
Major Event Entertainment		10, or 1 per 10,000 sq. ft. of net building area	10, or 1 per 20,000 sq. ft. of net building area	10, or 1 per 40 seats	10, or 1 per 40 seats
Self Service Storage		2, or 1 per 100,000 sq. ft. of net building area	2, or 1 per 200,000 sq. ft. of net building area	2, or 1 per 26,000 sq. ft. of net building area	2, or 1 per 53,000 sq. ft. of net building area

D. STANDARDS FOR LONG-TERM BICYCLE PARKING

1.a. Location Standards.

Long-term bicycle parking may be provided in one or more of the following locations:

- (1) Within a building, including on the ground floor or on individual building floors;
- (2) On-site, including in parking areas and structured parking;
- (3) **In an area where the closest point is within 300 feet of the site; or ...**

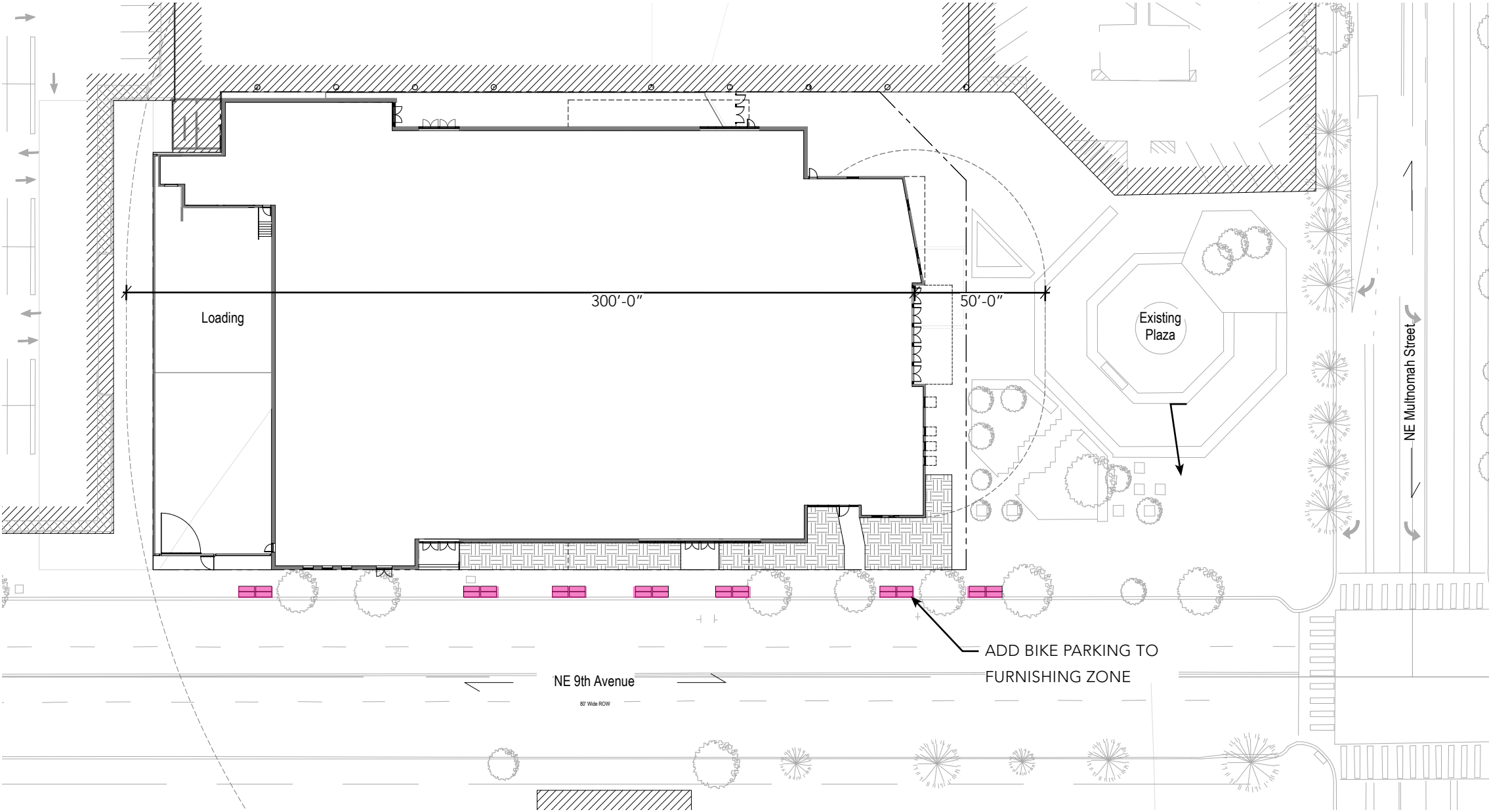
E. STANDARDS FOR SHORT-TERM BICYCLE PARKING

1.a. Location Standards.

Short-term bicycle parking must meet the following location standards:

- (1) On-site, outside a building;
- (2) At the same grade as the sidewalk or at a location that can be reached by an accessible route; and
- (3) **Within the following distances of the main entrance:**
 - **Building with one main entrance. For a building with one main entrance, the bicycle parking must be within 50 feet of the main entrance to the building as measured along the most direct pedestrian access route.**

C3.31 ANTICIPATED MODIFICATIONS - BICYCLE PARKING



BIKE PARKING

Long - Term Spaces	= 1 / 10,000 sf
Building Area	= 67,000 sf
Estimated Long-Term Spaces	= 7 Spaces (Located within Building Footprint)
Short - Term Spaces	= 1/ 40 seats











