



Hearings Office

City of Portland

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DECISION OF THE HEARINGS OFFICER

I. GENERAL INFORMATION

File Number: LU 22-187796 ADP AP (Hearings Office 4220018)

Applicant: Jesse Winterowd
Winterbrook Planning
610 SW Alder Street #810
Portland, OR 97205
jesse@winterbrookplanning.com

Property Owner/Agent: SWS PDX LLC
15110 Boones Ferry Road #500
Lake Oswego, OR 97035

Developer: Sean O'Neill
Scotia Western States Housing, LLC
6340 N Campbell Avenue #240
Tucson, AZ 85718

Hearings Officer: William Guzman

Bureau of Development Services (BDS) Staff Representative: Leah Dawkins

Site Address: 1325 SW Gibbs Street

Legal Description: Partition Plat 2021-41, Lot 1, INC UND INT Tract A

Tax Account Number: R649931610

State ID Number: 1S1E09BD 02801

Quarter Section: 3327

Neighborhood: Homestead

Business District: None

District Neighborhood Coalition: Southwest Neighborhoods Inc.

Plan District: None

Other Designations: Potential Landslide Hazard

Zoning: Residential 7,000 (R7), Residential Multi-Dwelling 2 (RM2), Environmental Conservation Overlay (c), Constrained Sites Overlay (z)

Land Use Review: Type III, Planned Development Amendment (APD), Partition Amendment (AP)

BDS Staff Recommendation to Hearings Officer: Approval with conditions.

Public Hearing: The hearing was opened at 2:02 p.m. on December 28, 2022 via Zoom and was closed at 3:30 p.m. The record was held open until 4:30 p.m. on January 5, 2023 for new evidence from anyone; until 4:30 p.m. on January 12, 2023 for rebuttal to information in the record; and until 4:30 p.m. on January 19, 2023 for the Applicant's final rebuttal. The record closed at 4:30 p.m. on January 19, 2023.

Testified at the Hearing:

Leah Dawkins
Michael Pina
Jesse Winterowd
Sean O'Neill
Ed Fischer
Milt Jones

Proposal: Previously, the Applicant received Land Use approval for a 5-story, 27-unit multi-dwelling structure with 28 vehicle parking spaces and 30 long-term bike parking spaces through a Planned Development Review, a Land Division Partition, and Environmental Modifications submitted in 2019 (LU 19-258445 PD LDP ENM).

In this matter, the Applicant is proposing Amendments to the original Planned Development and Partition approvals to allow for an **increase in the number of units from 27 to 43 units** through the alteration of interior floor plans. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces. The building envelope, façade, site access, and circulation are not proposed to change. The environmental tract created in the 2019 land use review will not change. Previously approved utility access will not change.

In addition, to better meet the outdoor area requirements and comply with new façade articulation requirements, the Applicant has requested modifications through the Planned Development Amendment. The façade modifications are not contested issues.

Approval Criteria: This proposal is reviewed through a Type III procedure because an increase in density is considered a major change to an Approved Planned Development and the original Planned Development application was processed through a Type III procedure (33.854.500-.510). The Land Division Amendment is being processed concurrently through the Type III procedure. In order to be approved, this proposal must comply with the approval criteria of *Title 33*. The applicable approval criteria are found in sections:

- **33.660.320.B, Approval Criteria for Other Changes to an Approved Preliminary Plan**
- **33.854.520,, Approval Criteria for Changes to an Approved Planned Development**
- **33.854.320, Approval Criteria for Modifications of Site-Related Development Standards**

II. ANALYSIS

This land use review process type is known as a quasi-judicial review. The Hearings Officer's role in this matter is to review the evidence in the record and issue a legal decision based on the City Code. The Hearings Officer is an independent and neutral decision maker tasked with answering the following legal question: Does Applicant's proposal meet the applicable approval criteria, either with or without conditions?

The increased density is allowed under the current zoning on the site. The Hearings Officer is tasked with identifying whether the increased density in this proposal is significant in its impacts. In other words, the Hearings Officer's review is limited to the impacts of the increase in density to 16 additional units.

The only contested issue related to the applicable criteria in this matter concerns the transportation impacts. Title 33.660.120.K requires that "[t]he approval criteria of Chapter 33.641, Transportation Impacts, must be met . . ." Also applicable to this issue is Title 33.854.310.E.5 which directs the Applicant to minimize potential negative effects on surrounding residential uses including traffic. Specifically, the neighborhood association and members of the public raise an issue of pedestrian and bicycle safety along the shoulder of the road of SW Gibbs, including the frontage to the site and continuing east to the intersection at 13th and SW Gibbs (hereinafter referred to as the "relevant section of SW Gibbs"). The following frontage improvements along SW Gibbs Street were required through LU 19-258445 and continue to apply to the site:

- The Applicant shall provide a minimum 6-foot-wide paved shoulder widening.
- On-street parking must be removed.
- The existing guardrail must be replaced with current AASHTO-compliant guardrail.

Photographs

In the record, relevant to the contested issue, are photographs that the Hearings Officer finds are true and accurate depictions of the relevant sections of SW Gibbs. The photographs are

relevant to the pedestrian and bicycle transportation issues. These photographs were submitted by the neighborhood association, members of the public, and the BDS Staff Report.

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Gibbs where it changes to Markim (Exhibit H-7).



Intersection at Gibbs and 13th(Exhibit H-7).



SW Gibbs with recent gravel placement and vegetation removal (Exhibit H-7)

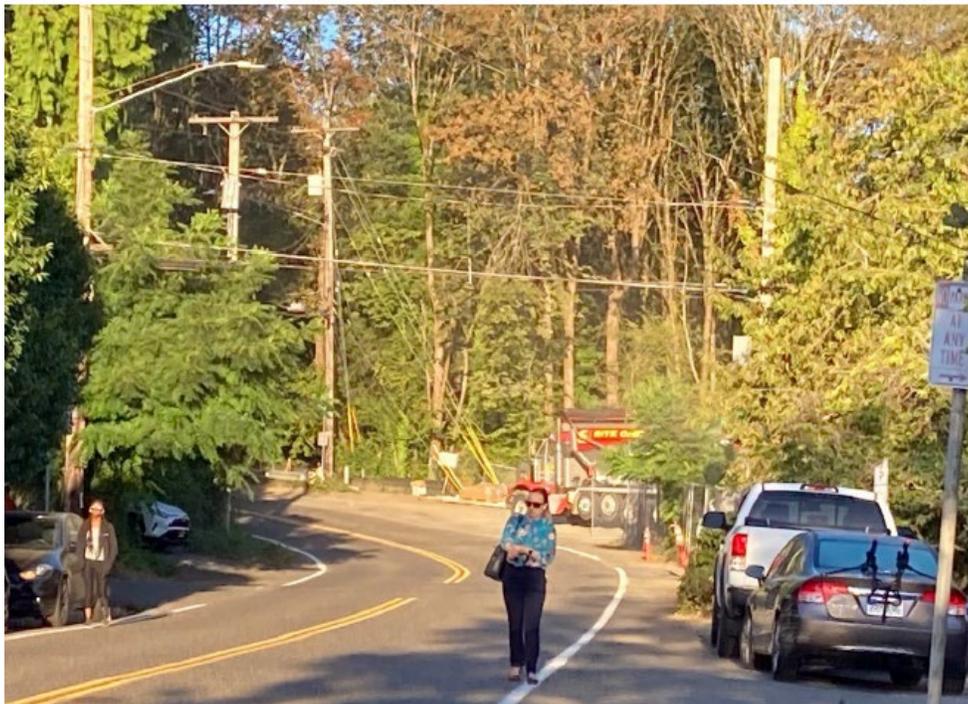


SW Gibbs St, looking west



SW Gibbs St, looking east

(Exhibit F.2)



(Exhibit H-8)



(Exhibit H-8)

Relevant Sections of Law

Title 33.660.320 Approval Criteria applies to this proposal:

“Changes to an approved Preliminary Plan will be approved if the review body finds that the applicant has shown that all of the following approval criteria have been met: * * *

- B. Approval criteria for other changes. All other changes to the Preliminary Plan must meet the following approval criteria:
 - 1. The proposed changes are not substantial enough, singly or in combination, to warrant a new review of the entire Preliminary Plan;
 - 2. The approval criteria addressed by the approval of the Preliminary Plan can still be met, with appropriate conditions of approval.”

Title 33.660.120. Approval Criteria:

“The Preliminary Plan for a land division will be approved if the review body finds that the applicant has shown that all of the following approval criteria

have been met. The approval criteria are: . . . K. Transportation impacts. The approval criteria of Chapter 33.641, Transportation Impacts, must be met[.]”

Title 33.641.010 Purpose states the following:

“The purpose of a transportation impact study is to assess the effects of routing or volume of traffic in the vicinity of the site on traffic conditions, transit, pedestrian and bicycle movement, and neighborhood livability.”

Title 33.641.020 Approval Criterion states the following:

“A. The transportation system must be capable of supporting the proposed development in addition to the existing uses in the area. **Evaluation factors include safety**, street capacity, level of service, connectivity, transit availability, availability of pedestrian and bicycle networks, on-street parking impacts, access restrictions, neighborhood impacts, impacts on pedestrian, bicycle, and transit circulation. Evaluation factors may be balanced; **a finding of failure in one or more factors may be acceptable if the failure is not a result of the proposed development, and any additional impacts on the system from the proposed development are mitigated as required by 33.641.020.B.**

B. Measures proportional to the impacts of the proposed use are proposed to mitigate on and off-site transportation impacts. Measures may include transportation improvements to on-site circulation, public street dedication and improvement, private street improvements, intersection improvements, signal or other traffic management improvements, additional transportation and parking demand management actions, street crossing improvements, improvements to the local pedestrian and bicycle networks, and transit improvements.”

The administrative rules implemented by the Portland Bureau of Transportation (PBOT) include TRN-10.27 - Traffic Capacity Analysis for Land Use Review Cases, which directs the following vehicle trip analysis to be utilized:

- “I. Administrative Rules for Traffic Capacity Analysis in Land Use Review Cases
For traffic impact studies required in the course of land use review or development, the following standards apply:
1. For signalized intersections, adequate level of service is LOS D, based on a weighted average of vehicle delay for the intersection.
 2. For stop-controlled intersections, adequate level of service is LOS E.
Level of service for two-way stop-controlled intersections is based on

- individual vehicle movement, and all-way stop controlled intersections is based on a weighted average of vehicle delay for the intersection.
3. An amendment or other land use application that requires analysis of traffic capacity and allows development that either (1) may cause a transportation facility to perform below the standards established in sections 1 and 2, or (2) adds vehicle trips to a facility that is already performing below the standards established in sections 1 and 2 may be approved if:
 - a. Development resulting from the amendment or other land use application will mitigate the impacts of the amendment or other land use application in a manner that avoids further degradation to the performance of the facility by the time of development through one or more of the following:
 - (i) the development is limited to result in no net increase in vehicle trips over what is allowed by the existing zoning; OR
 - (ii) one or more combination of transportation improvements or measures are imposed to mitigate the transportation impacts of the amendment or other land use application in a manner that avoids further degradation to the performance of the facility by the time of any development.”

Title 33.854.310 Approval Criteria for Planned Developments in All Zones:

“If the Planned Development is not proposing additional height or FAR as allowed by 33.270.100.I, then only criteria E through H apply. . . E. Site Design. Configure the site and development to visually integrate both the natural and built features of the site and the natural and built features of the surrounding area. Aspects to be considered include: . . . 5. Minimizing potential negative effects on surrounding residential uses.[.]”

Traffic Impact Study Addendum Results

On behalf of the Applicant, Lancaster Engineering submitted an addendum to their previously submitted Transportation Impact Study. (Exhibit A-5). The pertinent findings are set forth below:

“The proposed 43-unit apartment building is expected to generate a net increase of 12 morning peak hour trips, 12 evening peak hour trips, and 144 average weekday trips above existing site conditions. With this change in proposed dwelling units, the proposed development scenario is projected to generate an additional 6 morning peak hour trips, 5 evening peak hour trips,

and 50 average weekday trips beyond the trips generated per the January 2020 development scenario.”

“When considering the increase of the proposed development’s dwelling unit count, the trip generation calculations are expected to increase by 6 or less peak hour trips. **These impacts are relatively low, and the existing transportation infrastructure is expected to be minimally impacted. Accordingly, the new site trips generated by the project are not expected to significantly alter the operation or safety of the existing transportation facilities** beyond those detailed in the January 2020 study.”

In Exhibit A.5 at page 13, the following information is included related to pedestrian travel:

“Sidewalks are partially complete along nearby area roadways. **When sidewalks are not available along local streets, roadway speeds (posted and statutory speeds of 20 mph to 25 mph) and traffic volumes are generally lower, allowing pedestrians the ability to safely and comfortably walk along roadway shoulders when necessary.** Marked crossings are provided across SW Gibbs Street and SW Sam Jackson Park Road at six locations between SW 11th Avenue and SW Campus Drive. The proposed use will not remove or block any existing walkways within the area and will construct frontage improvements such as sidewalks along SW Gibbs Street. Accordingly, whereby safety and connectivity in the immediate vicinity will be improved.”

Regarding transit users, the Applicant states the following:

“The nearest bus stops that could reasonably serve transit users of the proposed use and nearby existing land uses are located at/near the intersections of SW Gibbs Street at SW 11th Avenue and SW Sam Jackson Park Road at SW US Veterans Hospital Road. Sidewalks to and from the above-mentioned transit stop locations are generally provided; however, **for segments of roadway where sidewalks are not available, relatively low travel speeds and traffic volumes allow transit users the ability to safely traverse along roadway shoulders when necessary.** In addition, adequate crossing measures (including marked crosswalks across SW Gibbs Street and SW Sam Jackson Park Road) are provided.

Applying the language of the City of Portland’s code TRN-10.27 – Traffic Capacity Analysis for Land Use Review Cases, the study’s authors concluded:

“The proposed development will not generate more trips than what is currently allowed under the existing zone. Accordingly, capacity-related

impacts associated with the proposed development to any transportation facility will not require mitigation as part of the development of the proposed use.

“When sidewalks are not available along local streets, roadway speeds posted and statutory speeds of 20 mph to 25 mph) and traffic volumes are generally lower, allowing pedestrians the ability to safely and comfortably walk along roadway shoulders to access transit stops. A summarized description of the transit lines is shown in Table 4.” (Exhibit A.5 at page 15-16).

PBOT’s Response to Traffic Study

PBOT submitted a response to the proposal and the Traffic Impact Study (TIS) concluding the following (Exhibit E.2 – Emphasis Added):

“Transportation Impacts approval Criterion (33.641.020)

For the previous Planned Development land-use review (19-258445 PD), the applicant submitted a Transportation Impact Study (TIS), professionally prepared by Lancaster Mobley, to support the transportation-related approval criteria, in which **PBOT reviewed and agreed with the conclusions that the transportation-related approval criteria are satisfied.**

With the proposed amendment, the applicant then submitted an addendum to the previous TIS, accounting for increase in trip generation to then site based on the addition of units from 27 to 43, which changes the trip generation increase from 7 PM Peak Hour trips to 12 PM Peak Hour trips. This is still a minimal increase in trips and will not degrade the Level of Service (LOS) below the acceptable LOS threshold of ‘E’ at all nearby stop-controlled intersections. Therefore, PBOT approves the updated TIS and the same comments as previously identified still apply. Therefore, based on the evidence included in the record, **the applicant has demonstrated to PBOT’s satisfaction that the transportation system is capable of supporting the proposed use in addition to the existing uses in the area**

Design of Rights-of-Way Approval Criterion (33.654.120)

For the required frontage improvements, the applicant submitted Public Works permit 19-193047 WT |TH0903 which has received 30% concept approval and bond payment, ensuring that vehicle, bicycle, and pedestrian circulation system is designed to be safe, efficient, and to city required standards.

RECOMMENDATION

PBOT has no objections to the Planned Development Amendment and associated Modifications.

Opposition Evidence and Argument Relevant to SW Gibbs

The primary opponent to the proposal and the decision in the Staff Report is the Homestead Neighborhood Association (Exhibit F.2 at page 2):

“Neglect of pedestrian infrastructure in this neighborhood as it is infilled with walk-to-work residential projects is inexcusable. OHSU, a major pedestrian destination employer, and neighborhood multi-unit residential and mixed-use commercial projects are contributing very significant sums to the City in the form of development charges for transportation and transportation safety improvement, among other things. We believe those funds should be used in the first instance to address the local public infrastructure and safety needs associated with the projects paying those fees.

In the absence of pedestrian safety improvements on SW Gibbs, we find ourselves having to oppose any decision allowing the proposed changes to this project. In the event that the Bureau of Developmental Services (BDS) approves the changes without the necessary improvements to pedestrian safety, the Homestead Neighborhood Association intends to appeal that decision. We hope that BDS does not sidestep this issue and point to another Bureau (Transportation) as the responsible office for a solution (a symptom of the disfunction of Portland city government that caused Portland citizens to this year elect a change in government structure). Instead, we hope that BDS can work with PBOT to solve this issue of safe walkability when approving needed developments in our neighborhoods.”

Joined by the neighborhood association are several members of the public. Milton Jones’ written testimony Exhibit F.1 states the following:

“Pedestrian transportation infrastructure on this part of SW Gibbs, especially in the vicinity of the SW 13th right-of-way, is inadequate. In fact, it is not safe. The City needs to provide sidewalks on SW Gibbs, or at least an improved and safer pedestrian corridor.”

Similarly, in Exhibit H-5 Mr. Jones states:

“It is the position of the Portland Bureau of Transportation (PBOT) in the Project Report that the road shoulder on SW Gibbs allows pedestrians there to walk safely and comfortably. I disagree. . . . The City needs to be told to make this pathway safe.”

Mr. Jones also provided oral testimony at the Hearing and encouraged the Hearings Officer to conduct a site visit to personally observe and experience the safety issues at night when it is dark. Mr. Jones directed his testimony to SW Gibbs and the intersection at 13th street which

he describes as offsite but immediately adjacent. In his opinion, PBOT seems to be ignoring the problem. He reasons that the project is aimed at providing medical workers housing which will include nighttime pedestrian visits. He notes that the City has long standing plans to improve this pedestrian route and he wants the City to put these plans into action.

Ed Fischer contributed oral testimony at the Hearing. Mr. Fischer explained that PBOT analysis utilizes vehicle trips generated by the development to determine whether improvements or mitigation along the public right of way is needed or required. The proposal increases density with 16 more dwelling units without the addition of any parking spaces. Mr. Fischer reasonably concludes this increase in density is not going to cause many additional vehicle trips but rather an increase in pedestrian and bicycle trips. Mr. Fischer pointed out that these trips are not counted or considered as part of the required PBOT analysis.

Mr. Fischer requested PBOT use transportation system development charges (approximately \$90k) towards making improvements for the bicycle and pedestrian traffic. He argued that PBOT needs to change their practices and administrative rules to consider the number of pedestrian and bicycle trips. Additionally, he pointed out that PBOT already identified improvements on Gibbs that should be required because it is not safe for pedestrians and bicycles. Additionally, the study contained no data on the actual speeds on Gibbs which he suggested are higher than posted limits. Lastly, he concluded there is not a reasonably complete network of redundant multiple routes in this area because hundreds of houses cannot get anywhere without traveling on Gibbs which is the main collector.

Mr. Fischer requested that the PBOT Director call for a review of best practices for Traffic Impact Studies making five suggestions: (1) trip generation for pedestrians, (2) reviewers should be required to make a site visit, (3) require documentation of actual traffic volume and speeds, (4) review recent planned or proposed system improvements (TIP projects) that are already on the books addressing these pedestrian concerns, and (5) require within the TIS some acknowledgement of whether there is the ability to use project development funds.

In Exhibit A-3 the Applicant provided questions and responses from a neighborhood meeting, several of which raised concerns with pedestrian and bicycle safety along SW Gibbs. The Applicant's response acknowledged the following: "A safe sidewalk along Gibbs will help not only existing neighbors, but our future tenants as well." A person named Mary commented that cars parking on the shoulder of Gibbs make pedestrian and bicycle travel more dangerous. At that meeting, the notes indicate Ed Fischer said the following: "A lot of people are going to be walking on this narrow ROW that needs to be addressed. I agree we don't need a sidewalk, but rather extra width without having pedestrians go out into the travel lane." (Exhibit A-7 at page 15).

Site Visit Invitation Declined

Mr. Milton suggested that the Hearings Officer visit the site in person and at night to fully appreciate the risks facing pedestrians and bicycles utilizing the shoulder of SW Gibbs.

Generally, the Hearings Officer's role is understood as that of an adjudicator as compared to an investigator. The Hearings Officer's role in a quasi-judicial review process is properly limited to the evidence submitted into the record. With only rare exception is it appropriate to seek out additional information outside the record. A site visit raises the risk that the Hearings Officer visits the site and makes observations not already contained in the record. The factual basis of those observations means the Hearings Officer themselves is acting as a witness providing testimony on the contested issues. This would blur the role of the Hearings Officer as a neutral fact finder as compared to a witness with personal information regarding the conditions along the relevant section of SW Gibbs.

Moreover, the contested issues related to the shoulder of SW Gibbs are not factual disputes (everyone using common sense can see that the shoulder's safety would be improved with some sort of bicycle/pedestrian right of way improvements). Rather, the dispute is the inadequacy of the current laws, policies, and procedures utilized by PBOT that do not count pedestrian trips. The factual record already fully supports a finding that the road shoulder on SW Gibbs is dangerous for pedestrians to traverse at all times but particularly when there is low visibility such as nighttime and fog. Therefore, the Hearings Officer declines to visit the site.

Findings of Fact Relevant to SW Gibbs Improvements

- The road shoulder along the relevant section of SW Gibbs is dangerous for pedestrians to traverse at all times but particularly when there is low visibility such as at nighttime and fog.
- The proposed change to density will not substantially change the approved preliminary plan. There will be no change to the lot or environmental resource tract.
- Pedestrian and bicycle safety concerns pertaining to the relevant section of SW Gibbs are not a result of the proposed development increasing density by 16 units; the safety issue is a pre-existing condition to this amended proposal.
- The following frontage improvements along SW Gibbs Street were required through LU 19-258445 and continue to apply to the site: (1) The Applicant shall provide a minimum 6-foot wide paved shoulder widening; (2) On-street parking must be removed; (3) The existing guardrail must be replaced with current AASHTO-compliant guardrail.
- Anticipated occupancy levels are the same as what was approved in 2019 because the number of bedrooms is staying the same (the amended proposal simply modifies 2- or 3-bedroom apartments to one bedroom or studio apartments).
- The proposed development scenario is projected to generate an additional six morning peak hour vehicle trips, five evening peak hour vehicle trips, and 50 average

weekday vehicle trips beyond the trips generated per the January 2020 development scenario.

- “The site is a full city block with approximately 200-feet of frontage on SW Gibbs Street, which turns into SW Marquam Hill Road, SW 13th Avenue, SW Grover Street, and SW 14th Avenue.” (Staff Report, Exhibit H-4 at page 3).
- “SW Gibbs Street/Marquam Hill Road is classified as Local Service for transit and freight, Neighborhood Collector, City Bikeway, City Walkway, Major Emergency Response, and Community Collector in the TSP.” (Staff Report, Exhibit H-4 at page 3).
- “A Public Recreational Trail designation (SW Trail #1) exists within the SW Gibbs Street right-of-way adjacent to the site.” (Staff Report, Exhibit H-4 at page 3).
- “SW Gibbs Street/Marquam Hill Road is improved with a 24-foot paved roadway surface (fog line to fog line) within a 60-170-foot wide right-of-way. There are no curbs, planter strips, or sidewalks.” (Staff Report, Exhibit H-4 at page 3).
- “Accessible routes have been provided throughout the site, connecting each building entrance to one another and to SW Gibbs, the common open areas, and the parking area.” (Staff Report, Exhibit H-4 at page 12).
- The City acknowledges the following: “The need for a sidewalk was identified in PedPDX our citywide pedestrian master plan. A project to build sidewalks is identified in Southwest in Motion as BP-07, and also in the TSP as project no. 90049.2. . . [T]his project is eligible for use of SDC funds, but those funds alone are not enough to construct the project.” (Exhibit H-10 at page 2, email from PBOT employee).
- “There are no sidewalks on SW Gibbs Street in front of the subject site. The sidewalks on SW Gibbs Street end two blocks (approximately 500 feet) to the east of the site. The shoulder in front of the site is approximately 6 feet wide or greater . . . A designated public trail runs along SW Gibbs Street, connecting the trail network of Marquam Nature Park to the OHSU campus and trails to the east.” (Exhibit A-1 at page 7).
- “The building has been designed to have the main entrance on the second floor, with a short pathway leading from the main entrance to the public ROW of SW Gibbs Street. . . With commercial and employment centers to the east of the site, this reorientation will ensure that pedestrian flow between the main entrance and these destinations will remain unobstructed.” (Exhibit A-1 at page 50).

III. CONCLUSIONS

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The concerns expressed by the public and the Neighborhood Association in written and oral testimony regarding the safety of pedestrians and bicycles traversing the relevant section of SW Gibbs are convincing, valid, and important. However, in a quasi-judicial review, the Hearings Officer must apply the law as it is currently enacted. The current law and procedures at PBOT do not consider pedestrian or bicycle trips, only vehicle trips. Moreover, the increase in dwelling units will not necessarily increase the levels of occupancy.

Therefore, the proposed changes are not substantial enough, singly or in combination, to warrant a new review of the entire Preliminary Plan and the approval criteria addressed by the approval of the Preliminary Plan can still be met, with appropriate conditions of approval. Specifically, the proposed development will not generate more vehicle trips than what is currently allowed under the existing zone. Accordingly, capacity-related impacts associated with the proposed development to any transportation facility will not require mitigation as part of the development of the proposed use. The existing transportation infrastructure is expected to be minimally impacted by the increase in units. Accordingly, the new site trips generated by the project are not expected to significantly alter the operation or safety of the existing transportation facilities. The Applicant is entitled to an application of the rule of law as enacted at the time BDS determined the filed proposal is complete.

Since the law does not require it, the Hearings Officer cannot direct the expenditure of public funds to this project under the facts of this case. In this case, whether to modify the rules to include analysis of pedestrian and bicycle trips and whether to direct PBOT resources to prioritize a fix of the unsafe shoulder is within the sole province of the legislative and executive branch of the City (City Council and the PBOT Director). The Hearing Officer's non-binding recommendation to PBOT is to use their executive and legislative powers to fully fund the projects identified in the PBOT emails with Mr. Fischer (Exhibit H-10 at page 2, email from PBOT employee) and to coordinate their completion with the development of Applicant's project. Additionally, PBOT should conduct an investigation into the recommendations of Mr. Fischer to consider an update to their land use review policies and procedures to make sure they are in compliance with current best practices (recommendations at page 15 above).

The Applicant is in support of the BDS recommendation of approval with conditions as set forth in the Staff Report. With the exception of the traffic related concerns, the public testimony is largely in support of the proposal. The Staff Report is exceptionally thorough in its analysis and well-reasoned in its conclusions of law. Within the context of this proposal, the Hearings Officer finds that off-site improvements to the adjacent intersection are not warranted by the impacts of this development. The Hearings Officer adopts the Staff Report and incorporates it herein by reference and attaches it hereto for convenience.

IV. DECISION

Approval of an Amendment for Changes to an Approved Preliminary Plan; and

Approval of an Amendment to a Planned Development Review to increase the approved number of dwelling units in a multi-dwelling structure from 27 to 43 units; and

Approval of the following Planned Development Review Modifications:

1. Reduce the area of street facing façade articulation from 25 percent to 22 percent of the total street facing surface area (33.120.230)
2. Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.240)
3. Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240)
4. Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240).

These approvals are illustrated with Exhibits C.1-C.7 and are subject to the following conditions:

- A. Development on Parcel 1 must be in substantial conformance with Exhibits C.1-C.4. As part of the building permit revision submittal, all required site plans, building elevations, landscape plans, and any additional drawings must reflect the information and designs approved by this land use review. The sheets on which this information appears must be labeled, "Proposal and design as approved in Case File #LU 22-187796 APD AP. No field changes allowed." Specifically, the permit revision must include the following elements:
 - Increased bike parking spaces
 - Added common outdoor areas, with accessible pathways and related directional signage
 - Additional landscaping as shown on Exhibit C.3.
- B. Conditions C.1–C.6 of LU 19-258445 continue to apply to the site, with the exception that the design approved through the PD is updated to that approved under this review as stated in Condition A above.



William Guzman, Hearings Officer

February 3, 2023

Date

Application Determined Complete: November 7, 2022
Report to Hearings Officer: December 16, 2022
Decision Mailed: February 3, 2023
Last Date to Appeal: 4:30 p.m., February 17, 2023
Effective Date (if no appeal): February 21, 2023

Conditions of Approval. This project may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term “applicant” includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

Appeal of the decision. ANY APPEAL OF THE HEARINGS OFFICER’S DECISION MUST BE E-MAILED TO LANDUSEINTAKE@PORTLANDOREGON.GOV. The appeal application form can be accessed at <https://www.portlandoregon.gov/bds/45477>. If you do not have access to e-mail, please telephone (503) 823-7617 for assistance on how to submit the appeal; please allow one business day for staff to respond. **An appeal fee of \$3,398.00 will be charged (one-half of the application fee for this case, up to a maximum of \$5,000.00).**

Who can appeal: You may appeal the decision only if you wrote a letter which is received before the close of the record on hearing or if you testified at the hearing, or if you are the property owner or applicant. If you or anyone else appeals the decision of the Hearings Officer, only evidence previously presented to the Hearings Officer will be considered by the City Council.

Appeal Fee Waivers: Neighborhood associations recognized by the Office of Neighborhood Involvement may qualify for a waiver of the appeal fee provided that the association has standing to appeal. The appeal must contain the signature of the Chair person or other person authorized by the association, confirming the vote to appeal was done in accordance with the organization’s bylaws.

Neighborhood associations, who wish to qualify for a fee waiver, must complete the Type III Appeal Fee Waiver Request for Organizations Form and submit it prior to the appeal deadline. The Type III Appeal Fee Waiver Request for Organizations Form contains instructions on how to apply for a fee waiver, including the required vote to appeal.

For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this planned development review amendment approval. This approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun. If the approved project includes more than one component, such as multiple buildings, or multiple phases that will be broken into separate building permit applications, at least one permit must be obtained within 3 years of the date of this decision, and all permits must be obtained within ten years of the date of this decision. After that ten-year period, a new land use review will be required before permits will be issued for any remaining project components that have not yet been permitted, subject to the Zoning Code in effect at that time.

Applying for your permits. A building permit, occupancy permit, or development permit may be required before carrying out an approved project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed herein;
- All applicable development standards, unless specifically exempted as part of this land use review;
- All requirements of the building code; and
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

EXHIBITS RECEIVED IN THE HEARINGS OFFICE – SEE NEXT PAGE
(NOT ATTACHED UNLESS INDICATED)

The exhibits in the land use case file are all assigned a letter (example A-1). The Hearings Office accepts exhibits filed online in its case management system. These exhibits are marked in the lower right hand corner that identifies the exhibit as a “Portland Hearings Office” exhibit. All of these exhibits are designated “H Exhibits” (that is, Hearings Office Exhibits). See the BDS Staff Report for a list of exhibits prior to “H.”



Hearing Office

City of Portland
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Exhibits, Orders, and Other Attachments

Number	Title	Status
Exhibit 1	Land Use Scheduling Request	Accepted
Exhibit 2	Hearing Participation Information	Accepted
Exhibit 3	Land Use Hearing Scheduled	Accepted
Exhibit 4 (attached)	STAFF REPORT	Accepted
Exhibit 5	Email from Milton Jones	Accepted
Exhibit 6	Fischer Written Testimony Notes	Accepted
Exhibit 7	LU 22-187796 APD AP Presentation	Accepted
Exhibit 8	Fischer Photos	Accepted
Exhibit 9	Record Closing Information	Accepted
Exhibit 10	Fischer/Homestead NA & PBOT Emails	Accepted
Exhibit 11	Applicant Response	Accepted



City of Portland, Oregon
Bureau of Development Services
Land Use Services
 FROM CONCEPT TO CONSTRUCTION

Dan Ryan, Commissioner
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STAFF REPORT AND RECOMMENDATION TO THE HEARINGS OFFICER

CASE FILE: LU 22-187796 APD AP
 Hearings Office Case #4220018
 PC # 22-144899
 REVIEW BY: Hearings Officer
 WHEN: December 28, 2022 at 2:00 pm

This land use hearing will be held remotely via the Zoom platform.

More information on how to participate is available at www.portland.gov/omf/hearings/land-use and with the Notice of Public Hearing posted at <https://www.portlandoregon.gov/bds/article/801313>.

It is important to submit all evidence to the Hearings Officer. City Council will not accept additional evidence if there is an appeal of this proposal.

BUREAU OF DEVELOPMENT SERVICES STAFF: LEAH DAWKINS / LEAH.DAWKINS@PORTLANDOREGON.GOV

GENERAL INFORMATION

Applicant: Jesse Winterowd, Winterbrook Planning
 610 SW Alder Street, #810
 Portland, OR 97205
 (503) 827-4422 / jesse@winterbrookplanning.com

Owner/Agent: SWS PDX LLC
 15110 Boones Ferry Road, #500
 Lake Oswego, OR 97035

Developer: Sean O'Neill, Scotia Western States Housing, LLC
 6340 N Campbell Avenue, #240
 Tucson, AZ 85718

Site Address: 1325 SW Gibbs Street

Legal Description: Partition Plat 2021-41, Lot 1, INC UND INT Tract A
Tax Account No.: R649931610
State ID No.: 1S1E09BD 02801
Quarter Section: 3327

Neighborhood: Homestead, contact at land-use@homesteadna.org
Business District: None
District Coalition: Southwest Neighborhoods Inc., contact Sylvia Bogert at 503-823-4592.
Plan District: None
Other Designations: Potential Landslide Hazard

Zoning: Residential 7,000 (R7), Residential Multi-Dwelling 2 (RM2), Environmental Conservation Overlay (c), Constrained Sites Overlay (z)

Case Type: Planned Development Amendment (APD), Partition Amendment (AP)

Procedure: Type III, with a public hearing before the Hearings Officer. The decision of the Hearings Officer can be appealed to City Council.

Proposal:

The applicant received Land Use approval for a 5-story 27-unit multi-dwelling structure with 28 vehicle parking spaces and 30 long-term bike parking spaces through a Planned Development Review, a Land Division Partition, and Environmental Modifications submitted in 2019 (LU 19-258445 PD LDP ENM). The applicant is now proposing Amendments to the original Planned Development and Partition approvals to allow for an increase in the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces. The building envelope, façade, site access, and circulation are not proposed to change. The environmental tract created in the 2019 land use review will not change. Previously approved utility access will not change.

To better meet the outdoor area requirements and comply with new façade articulation requirements, the applicant has requested the following modifications through the Planned Development Amendment:

- Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2);
- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

This proposal is reviewed through a Type III procedure because an increase in density is considered a major change to an Approved Planned Development and the original Planned Development application was processed through a Type III procedure (33.854.500-.510). The Land Division Amendment is being processed concurrently through the Type III procedure.

Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of *Title 33*. The applicable approval criteria are found in sections:

- **33.660.320.B, Approval Criteria for Other Changes to an Approved Preliminary Plan**
- **33.854.520,, Approval Criteria for Changes to an Approved Planned Development**
- **33.854.320, Approval Criteria for Modifications of Site-Related Development Standards**

ANALYSIS

Site and Vicinity: The site is located on the north side of SW Gibbs Street as it turns into SW Marquam Hill Road. New development is currently under construction on the site as approved through LU19-258445 PD LDP ENM. The site slopes moderately to steeply down to the north and west beyond the developed portion and is densely forested with a canopy that connects to the adjacent nature park.

Properties immediately east and south of the site are developed with single family homes (R5 zone). Multiple apartment buildings are located immediately north of the site. Marquam Nature Park (OS zone) is located directly west of the site. The OHSU campus is approximately 1,000-feet to the east.

Infrastructure:

- **Streets** – The site is a full city block with approximately 200-feet of frontage on SW Gibbs Street, which turns into SW Marquam Hill Road, SW 13th Avenue, SW Grover Street, and SW 14th Avenue. There are two driveways entering the site from SW Gibbs Street that serve the existing development. At this location, SW 13th Avenue, SW Grover Street, and SW 14th Avenue are classified as Local Service Streets for all modes in the Transportation System Plan (TSP). SW Gibbs Street/Marquam Hill Road is classified as Local Service for transit and freight, Neighborhood Collector, City Bikeway, City Walkway, Major Emergency Response, and Community Collector in the TSP. A Public Recreational Trail designation (SW Trail #1) exists within the SW Gibbs Street right-of-way adjacent to the site. Tri-Met provides transit service approximately 520-feet east of the site at the intersection of SW Gibbs Street and SW 11th Avenue via Bus #8.

At this location, SW 13th Avenue, SW Grover Street, and SW 14th Avenue are all 60-foot wide unimproved rights-of-way. SW Gibbs Street/Marquam Hill Road is improved with a 24-foot paved roadway surface (fog line to fog line) within a 60-170-foot wide right-of-way. There are no curbs, planter strips, or sidewalks.

- **Water Service** – There is an existing 8-inch CI water main in SW Gibbs Street.
- **Sanitary Service** - According to available GIS data, the following sewer infrastructure is located in the vicinity of the project site:
 - Public 8-inch CSP sanitary sewer in SW 13th (BES as-built # 3172). This main is within the conservation zone.
 - Public 12-inch CSP combined sewer in SW Gibbs St east of the property (BES as-built# 1182). This is a newly extended main built through EP555.
 - Public 8-inch CIP combined sewer in the right-of-way north of the property (SW Grover Street) and within a public easement north of the property (BES as-built # 2234).
- **Stormwater Disposal** – There is no public storm-only sewer currently available to this property.

Zoning: Residential 7,000 – The single-dwelling zones are intended to preserve land for housing and to provide housing opportunities for individual households. The zones implement the comprehensive plan policies and designations for single-dwelling housing and provide options for infill housing that is compatible with the scale of the single-dwelling neighborhood.

Residential Multi-Dwelling 2 – The RM2 zone is a medium-scale multi-dwelling zone that is generally applied in and around a variety of centers and corridors that are well-served by transit. Allowed housing is characterized by buildings of up to three or four stories with a higher percentage of building coverage than in the RM1 zone, while still providing opportunities for landscaping and outdoor spaces that integrate with residential neighborhood characteristics. The major types of new housing development will be a diverse range of multi-dwelling structures and other compact housing that contribute to the intended urban scale of centers and corridors, while providing transitions in scale and characteristics to lower-scale residential neighborhoods.

Environmental Zone (33.430) – Environmental Conservation Overlay: The “c” overlay is intended to conserve important environmental features and resources while still allowing compatible development. Land divisions and planned developments must meet environmental standards or go through environmental review. No disturbance area is proposed within the environmental overlay zones as a result of this amendment review.

Environmental Resources: The application of the environmental overlay zones is based on detailed studies that have been carried out within separate areas throughout the City. Environmental resources and functional values present in environmental zones are described in environmental inventory reports for these respective study areas.

The project site is mapped within the Southwest Hills Resource Protection Plan as Site No. 113: Marquam Hill Ravine. Resources and functional values of concern on the project site include wildlife habitat, groundwater recharge, scenic, recreation, perennial and seasonal creeks, and forested wetlands.

Land Use History: City records indicate the following prior land use reviews for this site:

- **CU 075-76:** Approval of a Conditional Use Review for a convalescent home.
- **CU 020-79:** Approval of a Conditional Use review for a welfare institution with variances to minimum site area and minimum front yard requirements.
- **LUR 96-00236:** Approval of an expansion of a conditional use for a group living use in a residential zone at 1325 SW Gibbs, in order to create an enclosed connection between the two existing buildings and to add a vertical lift.
- **LU 19-258445:** Approval of a 1-parcel partition resulting in one parcel and an Environmental Tract; approval of a Planned Development Review for a multi-dwelling structure in an R1 through R2.5 zone, that consists of a 5-story, 27-unit apartment building; and approval of several Environmental Review Modifications.

This proposal is an amendment to the Planned Development Review and Land Division Partition portions of LU 19-258445 PD LDP ENM.

Agency Review: Several Bureaus have responded to this proposal and relevant comments are addressed under the applicable approval criteria. Exhibits “E” contain the complete responses.

Neighborhood Review: A Notice of Public Hearing on a Proposal in Your Neighborhood was mailed on December 5, 2022 and revised notice was mailed on December 12, 2022 to clarify that the hearing will be remote only. Two written responses were received from a notified property owner and the neighborhood association in response to the proposal. Comments expressed support for the overall proposal to add additional units to the approved development, but expressed continued concern about the lack of pedestrian and bicycle improvements within the public right-of-way at the intersection of SW Gibbs and 13th Avenue in addition to drawing attention to a general lack of pedestrian infrastructure within this neighborhood.

This proposal has been reviewed by the Bureau of Transportation for transportation impacts and right-of-way improvements. Within the context of this proposal, Staff found that off-site improvements to the adjacent intersection are not warranted by the impacts of this development. See additional information discussed below under 33.660.320.B.2. The applicant has been working with concerned neighbors and the Bureau of Transportation to make improvements to the adjacent right-of-way in addition to what was required through the previously approved land use review. The applicant has indicated that they will continue to work with the neighborhood outside of the requirements of this and the prior land use review.

ZONING CODE APPROVAL CRITERIA

APPROVAL CRITERIA FOR CHANGES TO AN APPROVED PRELIMINARY PLAN IN OPEN SPACE AND RESIDENTIAL ZONES

33.660.320.B, Approval Criteria for Other Changes to an Approved Preliminary Plan The following criteria apply to other changes to an Approved Preliminary Plan

1. **The proposed changes are not substantial enough, singly or in combination to warrant a new review of the entire preliminary plan; and**

Findings: The applicant is proposing to increase the number of dwelling units within the building envelope approved through **LU 19-258445**. Because the partition was approved concurrent with a Planned Development (PD) and subject the requirement to build the specific development proposed through the PD, an amendment to the approved partition is required to approve the increase in units. However, the proposed change will not substantially change the approved preliminary plan. There will be no change to the lot or environmental resource tract. Criteria K and L, Transportation Impacts and Services and utilities could be impacted by the change to the number of units proposed through the PD. These two approval criteria are addressed below.

2. **The approval criteria addressed by the approval of the Preliminary Plan can still be met, with appropriate conditions of approval.**

Findings: Staff has determined that the following approval criteria are applicable to the proposed change to the approved Preliminary Plan, which involves the increase in the number of units from 27 to 43, because they could be impacted by the increased number of units:

K. Transportation Impacts. The approval criteria of Chapter 33.641, Transportation Impacts, must be met; and

Findings: The transportation system must be capable of supporting the proposed development in addition to the existing uses in the area.

Evaluation factors include: safety, street capacity, level of service, connectivity, transit availability, availability of pedestrian and bicycle networks, on-street parking impacts, access restrictions, neighborhood impacts, impacts on pedestrian, bicycle, and transit circulation. Evaluation factors may be balanced and measures to mitigate impacts may be necessary.

The Development Review Section of the Portland Bureau of Transportation (PBOT) has reviewed the application against the evaluation factors and has provided the following findings (see Exhibit E.2):

For the previous Planned Development land-use review (19-258445 PD), the applicant submitted a Transportation Impact Study (TIS), professionally prepared by Lancaster Mobley, to support the transportation-related approval criteria, in which PBOT reviewed and agreed with the conclusions that the transportation-related approval criteria are satisfied.

With the proposed amendment, the applicant then submitted an addendum to the previous TIS, accounting for increase in trip generation to then site based on the addition of units from 27 to 43, which changes the trip generation increase from 7 PM Peak Hour trips to 12 PM Peak Hour trips [compared to the trips estimated for the prior use at the site]. This is still a minimal increase in trips and will not degrade the Level of Service (LOS) below the acceptable LOS threshold of 'E' at all nearby stop-controlled intersections. Therefore, PBOT approves the updated TIS and the same comments as previously identified still apply. Therefore, based on the evidence included in the record, the applicant has demonstrated to PBOT's satisfaction that the transportation system is capable of supporting the proposed use in addition to the existing uses in the area.

PBOT has reviewed and concurs with the information supplied and available evidence. Given the limited increase in trips, PBOT found that transportation improvements beyond those already required are not warranted (see additional discussion under Criterion L below). These criteria are met.

L. Services and utilities.

Findings: Chapters 33.651 through 33.654 address water service standards, sanitary sewer disposal standards, stormwater management, utilities and rights of way. The criteria and standards are met as shown in the following table:

<p>33.651 Water Service standard – See Exhibit E.3</p> <p>The Water Bureau has indicated that service is available to the site from the 8-inch CI water main in SW Gibbs Street, as noted on page 3 of this report. The water service standards of 33.651 have been verified. Therefore, this criterion is met.</p>
<p>33.652 Sanitary Sewer Disposal Service standards – See Exhibit E.1</p> <p>The Bureau of Environmental Services has indicated that service is available to the site, as noted on page 3 of this report. The sanitary sewer service standards of 33.652 have been verified.</p>
<p>33.653.020 & .030 Stormwater Management criteria and standards – See Exhibit E.1</p> <p>No stormwater tract is proposed or required. Therefore, criterion A is not applicable. The applicant submitted an updated Stormwater Report to address this criterion. BES reviewed the proposed amendments and utility plans and the response is summarized below:</p> <p>Private Property Stormwater Management: Stormwater runoff from this project must comply with all applicable standards of the SWMM and be conveyed to a discharge point along a route of service approved by the BES Director or the Director’s designee.</p> <p>From the submitted site plans, it appears that the proposed Amendment to Planned Development and Amendment to Partition application does not include any changes to the previously approved stormwater management plan. This proposal does not include an increase in impervious area beyond what has been approved under LU 19-258445 PD LDP and 20-194259 CO. The stormwater management system proposed is consistent with the plan approved under 20-194259 CO.</p> <p>Public Right-of-Way Stormwater Management: Stormwater runoff from public right-of-way improvements as required by the City of Portland Bureau of Transportation (PBOT) must be managed according to the standards of the SWMM and the Sewer and Drainage Facilities Design Manual.</p> <p>PBOT requires frontage improvements to include pavement widening along the frontage of the property. Public stormwater management improvements per the standards of the SWMM and the Sewer and Drainage Facilities Design Manual are therefore required. BES understands that given the elevation of the street, the lack of room in the ROW, and the landslide hazard area, stormwater management for the ROW improvements are challenging. BES has approved the Special Circumstance application allowing the applicant to pay an offsite management fee. BES Development Engineering approved the Concept Development plans (i.e. 30% design) for the right-of- way stormwater improvements on 4/23/20; therefore, BES finds that public stormwater infrastructure can be constructed as shown on the applicant’s Preliminary Site Utility Plan.</p> <p>The Bureau of Environmental Services (BES) has determined that the applicant’s stormwater management plan is acceptable for the purpose of reviewing the Amendment to Planned Development and Amendment to Partition application against stormwater management approval criterion. This criterion is met.</p>
<p>33.654.110.B.1 Through streets and pedestrian connections</p> <p>Generally, through streets should be provided no more than 530 feet apart and pedestrian</p>

connections should be provided no more than 330 feet apart. Through streets and pedestrian connections should generally be at least 200 feet apart.

There are several “paper” platted streets surrounding the site, SW 13th Avenue, SW Grover Street, and SW 14th Avenue, which meet the spacing standards. However, PBOT determined in the previous land use review (LU 19-258445) of this site that given the challenging terrain, Environmental overlays, Open Space zoning, landslide hazards, tree canopy, and water features, it is not practical or appropriate to provide street improvements to extend either a public street or pedestrian connection through the subject site or in the existing unimproved rights-of-way in order to further the City’s connectivity goals. The environmental constraints noted above have not changed and PBOT has indicated in their review of the current proposal that no new or additional transportation improvements are required for the current land use review.

For the reasons described above, this criterion is met.

33.654.120.B & C Width & elements of the right-of-way – See Exhibit E.2

33.644.120.H Street Trees – See Exhibit E.6

At this location, SW 13th Avenue, SW Grover Street, and SW 14th Avenue are all 60-foot wide unimproved rights-of-way. SW Gibbs Street/Marquam Hill Road is improved with a 24-foot paved roadway surface (fog line to fog line) within a 60-170-foot wide right-of-way. There are no curbs, planter strips, or sidewalks.

In reviewing this land division, PBOT relies on accepted civil and traffic engineering standards and specifications to determine if existing street improvements for motor vehicles, pedestrians and bicyclists can safely and efficiently serve the proposed new development. As previously noted, SW 13th Avenue, SW Grover Street, and SW 14th Avenue will not be required to be improved in relation to the proposed development due to numerous environmental constraints.

The following frontage improvements along SW Gibbs Street were required through LU 19-258445:

- The applicant shall provide a minimum 6-ft wide paved shoulder widening.
- On-street parking must be removed.
- The existing guardrail must be replaced with current AASHTO-compliant guardrail.

For the required frontage improvements, the applicant submitted Public Works permit 19 193047 WT | TH0903 which has received 30% concept approval and bond payment, ensuring that vehicle, bicycle, and pedestrian circulation system is designed to be safe, efficient, and to city required standards.

Urban Forestry evaluated the provision of street trees and planting areas for the public right-of-way and does not object to this proposal. All street tree permitting is deferred to Public Works Permit TH0903.

No dedication or additional conditions of approval are necessary to provide required right-of-way elements. This criterion is met.

33.654.130.A - Utilities (defined as telephone, cable, natural gas, electric, etc.)

At the time of the final plat for FP 19-258445, time no specific utility easements adjacent to the right-of-way were identified as being necessary. Therefore, this criterion is met.

APPROVAL CRITERIA FOR CHANGES TO AN APPROVED PLANNED DEVELOPMENT**33.854.520 Approval Criteria**

The approval criteria for changes to a Planned Development are those used for approval of the original planned development application. The original planned development application, LU 19-258445 PD ENM LDP, was reviewed under the approval criteria of 33.854.310 Approval Criteria for Planned Developments in All Zones.

33.854.010 Purpose

These regulations assign Planned Development Reviews to an appropriate procedure type. The approval criteria ensure that innovative and creative development is encouraged when it is well designed and integrated into the neighborhood.

33.854.310 Approval Criteria for Planned Developments in All Zones

Criteria A through E apply to proposals for additional height or FAR in the CM2, CM3, CE, and CX zones that are taking advantage of 33.270.100.I. If the Planned Development is not proposing additional height or FAR as allowed by 33.270.100.I, then only criteria E through H apply.

Applicant findings provided below have been modified by staff to provide correct references to exhibit numbers. Applicant findings that have been modified by staff are in **bold**.

Findings: The applicant is proposing an Amendment to a Planned Development review to allow a multi-dwelling structure within an RF-R5 zone, which will consist of a 5-story, 43-unit multi-dwelling structure that will include 28 vehicle parking spaces and 51 long-term bike parking spaces. Therefore, only criteria E through H apply.

- E.** Configure the site and development to visually integrate both the natural and built features of the site and the natural and built features of the surrounding area. Aspects to be considered include:
 1. Orienting the site and development to the public realm, while limiting less active uses of the site such as parking and storage areas along the public realm;

Findings: The applicant provided the following findings:

The site is located on a sloping bend on the ROW of SE Gibbs Street, approximately 15 feet below the grade. The building has been designed to have the main entrance on the second floor, with a short pathway leading from the main entrance to the public ROW of SW Gibbs Street. The site's driveway has been reoriented to provide vehicle access from only the southwest corner of the site (**Sheet C1 of Exhibit C.7**). With commercial and employment centers to the east of the site, this reorientation will ensure that pedestrian flow between the main entrance and these destinations will remain unobstructed. The driveway will circle to the back of the building connecting to vehicle parking on the ground floor. Both the second floor (main lobby) and ground floor (lower lobby) will have secure bicycle parking rooms. All other storage areas are planned on the ground floor, below the grade of the existing roadway and public realm.

The building is setback 3-feet from the front property line and is the prominent feature of the site as viewed from the public realm. The structured parking at the base of the building is imperceptible from the right-of-way based on noted grade differences. A bike storage room and trash enclosure are also located in this area where they are out of sight. The development is oriented to the public realm and less active uses of the site are limited. Therefore, this criterion is met.

- 2. Preservation of natural features on the site, such as stands of trees, water features or topographical elements;

Findings: The applicant provided the following findings:

The site contains a flat, developed area near SW Gibbs Street before dropping steeply downhill to the north. An environmental conservation overlay zone exists on the northern portion of the site which contains a dense forested area. As part of the previously approved PD, a land division was approved to create a protected environmental resource tract. This tract preserves the existing stands of trees and topographical elements on site. No identified continuously flowing water features or wetlands exist on the site. A drainage area is located within the environmental tract as shown on the Vicinity Map (Exhibit A.15). The nearest disturbance area will be approximately 80 feet away, as shown on **Sheet C4 of Exhibit C.7**.

Tree removals onsite have been minimized to the extent possible. Since approval of LU #19-258445 PD, ENM, LDP, additional tree removals within the ROW have been found by PBOT to be necessary. These additional tree removals are addressed in Public Works permit 19-193047 WT | TH0903, which has received 30% approval. All onsite tree preservation proposed and approved on **Sheets L4 and L5 of Exhibit C.6** are proposed to continue to remain preserved. Mitigation plantings are proposed for on-site tree removals.

Natural features are present on the northern portion of the site and include steep slopes with a heavily treed closed canopy area characterized by big leaf maple and Douglas fir trees. As previously noted, this area was placed within an environmental resource tract through LU 19-258445 and will continue to be permanently preserved. The natural features of the site have been preserved to the extent practical. Therefore, this criterion is met.

3. Inclusion of architectural features that complement positive characteristics of surrounding development, such as similar building scale and style, building materials, setbacks, and landscaping;

Findings: The applicant provided the following findings:

The surrounding development includes a mix of single-family dwelling, multi-dwelling structures, and some commercial uses. At times, businesses and residences are carved into the hillsides, using the natural topography to create daylight basements and second floor main entrances like the one proposed in this application. This strategy creates the appearance of smaller scale structures that blend with the range of sizes of single-family residences.

The proposed design was also approved in 2020 (LU #19-258445 PD, ENM, LDP) and the building façade and outdoor features are not proposed to deviate from this previous approval. Since the development was approved, two additional apartment complexes have been approved in this neighborhood. Both apartments did not have to adhere to RM2 standards. The proposed architectural features that mirror the surrounding development include:

- Personal balconies and common decks
- Neutral tones
- Vertical natural wood paneling
- Flat roofs
- 4-stories from grade

The proposed design includes more modern development designs that will match the apartments currently under construction along SW 12th Ave.

The proposed development has incorporated positive characteristics of surrounding development with consideration to scale and style, building materials, setbacks, and landscaping and will not deviate from what was approved in 2019. Staff finds the scale and setbacks of the proposed building to be similar to that of development within 2 to 3 blocks east of the site. Multi-dwelling

structures addressed 3223 SW 11th Avenue, 1033 SW Gibbs Street, and 3211 SW 10th Avenue are between 3 and 4 stories in height. Therefore, this criterion is met.

4. Mitigation of differences in appearance through means such as setbacks, screening, landscaping, and other design features;

Findings: The applicant provided the following findings:

Directly opposite of the site across SW Gibbs Street is a single-family dwelling that sits approximately 5 -10 feet above grade. The home is screened with thick shrubs with a lower story entrance and driveway parking onto SW Gibbs St. and an upper story entrance and driveway parking on SW 13th Street. The site is surrounded by heavily vegetative unimproved ROWs to the east, west, and north.

- Beyond the unimproved ROW to the west is Marquam Nature Park – a densely forested park with mature trees reaching 40' and beyond in height.
- Beyond the unimproved ROW to the east are a series of four older one- and two-story single-family dwellings.
- Beyond the unimproved ROW to the north is the Forest View Apartments, a recently remodeled apartment complex built in 1961 and containing 25 units.

The vegetative unimproved ROWs, the proposed landscaping surrounding the building, and the protected environmental tract along the north and west side of the site (see LU #19-258445 PD, ENM, LDP) will screen the building from these adjacent uses.

The primary difference in appearance is the presence of a multi-dwelling structure within a single-dwelling zone. The proposed development adequately mitigates for this difference in appearance through incorporating existing topography, natural areas, and landscaping. Therefore, this criterion is met.

5. Minimizing potential negative effects on surrounding residential uses; and

Findings: The applicant provided the following findings:

The potential negative effects of the proposed development on surrounding residential uses are typical of any increase in residential units: traffic, noise, and light. These impacts are addressed as follows:

- Traffic: Transportation impact studies are provided in **Exhibit A.5**. As indicated in the TIS, the use will generally match the residential traffic characteristics of the surrounding area and travel patterns will not significantly alter from what is typical for the neighborhood. Impacts to the neighborhood will be minimal. Vehicle types and travel patterns will be similar to existing neighborhood conditions. The proposed 43 units will add an additional 144 weekday total trips to the surrounding transportation system. These new site trips are not expected to significantly alter the operation or safety of the existing transportation facilities, beyond those previously approved in LU #19-258445 PD, ENM, LDP. Further, off-site parking will not be impacted because the proposal will provide 4 additional spaces than required by PZC 33.266. For 43 units, 22 spaces are required; 28 spaces are proposed.
- Noise: The site is buffered by forested vegetation on three sides provided by the terrain and unimproved conditions of the adjacent rights-of-way. Vehicle parking and maneuvering areas will be under and inside the structure, while the small outdoor parking area is located on the far side of the proposed building from adjacent residential property to the east and below the street grade from the adjacent residence to the south. These features will provide a buffer from automobile noise generated on site.

- Light: Sources of light impacts will come from automobile headlights and interior and exterior lighting from the structure and pedestrian walkways. A screen of natural, mature vegetation exists on three sides of the property, and the small outdoor parking area is below the street grade and on the opposite side of the proposed structure from adjacent residential development. Landscaping as shown on Sheet L4, will provide additional screening from headlights in the parking area. Impacts from interior and exterior lights will be mitigated due to the location of the structure below the street grade and within a buffer of mature vegetation.

Potential negative effects to surrounding residential uses are primarily minimized by utilizing the differences in grade to present a less impactful development and preserving approximately half the site in forested open area. As noted above, traffic, noise, and light are minimized to the extent practicable. Therefore, this criterion is met.

6. Preservation of any City-designated scenic resources; and

Findings: There are no City-designated scenic resources on the site. Therefore, this criterion does not apply.

F. Open Area. On sites zoned RF through R2.5

1. Where proposed development includes attached houses, duplexes, triplexes, fourplexes, multi-dwelling structures, or multi-dwelling development, adequate open area to accommodate the proposed development must be provided. Open area does not include vehicle areas.
2. Where multi-dwelling development with detached single dwelling units is proposed, 50 percent of the total number of dwelling units on the site must be oriented around a common outdoor area.

Findings: The applicant provided the following findings:

The proposed multi-dwelling structure is located on property zoned both R7 and RM2, (**see Site Plan-Exhibit C.1**). With 43 units, 2,064 square feet of outdoor area is required. 27 units will have personal balconies, totaling 1,296 square feet. An additional 970 square feet of common outdoor areas are proposed. A total of 2,266 square feet of outdoor areas are proposed, 202 square feet more than the required amount. This is enough additional square footage to accommodate another 4 units in addition to the 43 units proposed.

As for quality, outdoor areas are proposed to be surfaced with either decking materials or grass. The lower common lawn is planned along the main pedestrian pathway between the surface parking/maneuvering areas and the lower lobby entrance. This location will allow for more frequent and natural-feeling use of the space, rather than if it were planned on the opposite side of the site.

The upper deck will be accessed from a main entrance on the second floor of the building, and it will look out towards the forested slopes of Marquam Park. The scenic views and large dimensions will be inviting to visitors and residents. Neither the upper deck nor the lower lawn are proposed to be roofed or enclosed, but both could accommodate tables and seating for users.

The applicant has requested modifications to the common outdoor area standards. The modification approval criteria are addressed later in this report. With the modifications, adequate open area can be provided with the proposed development. Therefore, criterion F.1. can be met. The applicant is not proposing any detached single dwelling units, so criterion F.2. does not apply.

- G.** Accessible connections. Provide one or more accessible routes that connect all buildings on the site to adjacent streets, common open areas, and parking areas. Use landscaping and site furnishings to ensure the accessible route provides a pleasant user experience.

Findings: Accessible routes have been provided throughout the site, connecting each building entrance to one another and to SW Gibbs, the common open areas, and the parking area. A new path is being provided to connect the lower lawn common area to the rear main entrance of the building. Wayfinding signs will be installed to direct users to the lower lawn open space. Additional landscaping and furnishings are proposed to enhance the new lower lawn open space. With the condition that the site paths are provided as shown on the architectural, landscaping, and engineering plans (Exhibits C.1-C.7), this criterion is met.

- H.** Garbage and recycling areas. Garbage and recycling collection areas must be adequate in size to accommodate the proposed development, designed to encourage recycling, and located to facilitate pick-up service. Screening and buffering of garbage and recycling areas must be provided to maintain a clean and attractive development.

Findings: The applicant has proposed to provide a garbage and recycling collection area that will be beneath the upper common area deck adjacent to the parking area on the first floor of the building. The garbage and recycling area is accessible for collection from the driveway adjacent to the building accessing the parking garage. The garbage and recycling collection area is enclosed within the parking garage and is screened from public view. This criterion is met.

APPROVAL CRITERIA FOR MODIFICATIONS OF SITE-RELATED DEVELOPMENT STANDARDS THROUGH A PLANNED DEVELOPMENT

33.854.320 Additional Approval Criteria for Modifications of Site-Related Development Standards

The following criteria apply to modifications of site-related development standards, including parking standards. These modifications are done as part of a Planned Development review and do not have to go through the adjustment process.

Modification to Façade Articulation Standards

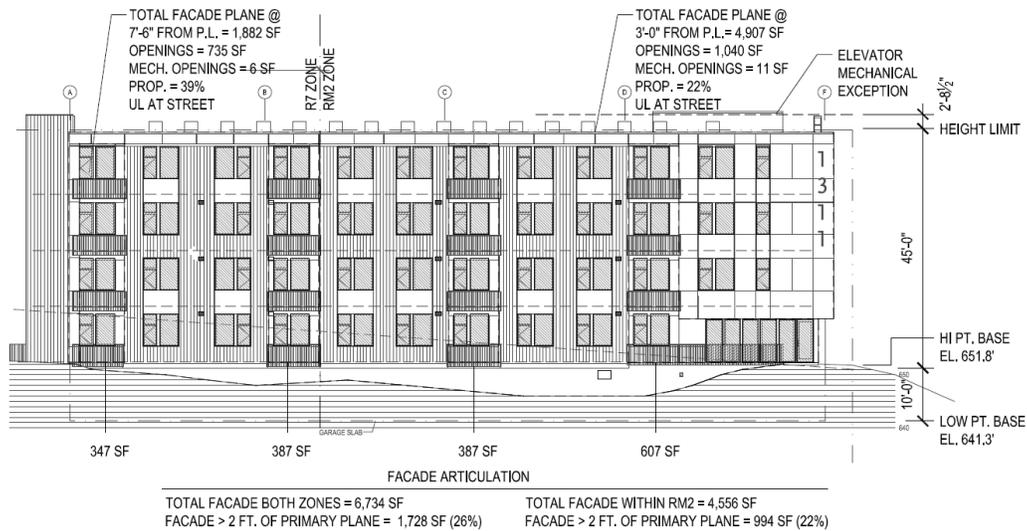
The applicant is requesting a modification to reduce the area of façade articulation from 25 percent to 22 percent of the total street-facing surface area as required by 33.120.230.C.2. This modification is triggered by a change in the Zoning Code since the original land use review was approved. The modification will be approved if the following approval criteria are met:

- A. Better meets approval criteria. The resulting development will better meet the approval criteria of Section 33.854.310, above; and**

Findings: The applicant has provided the following findings:

This standard applies to the development proposal because the building is setback within 20-feet of the front street property line. The building is planned roughly 3 feet from the front lot line because of the presence of the environmental overlays on the northern (rear) and eastern (right) areas of the site. The façade articulation standard is a new standard that did not apply to the original building design and approval. The proposal is for interior changes to floor plans that increase housing units within the original design approval. The original design was determined by City of Portland planning staff and the Hearings Officer to meet planned development approval criteria for site orientation, preservation of natural features, neighborhood compatibility, accessibility and garbage/recycling areas – the same criteria applicable to this modification. Since the design has not changed, these elements continue to be met, as discussed below.

Granting the modification would waive a requirement for an additional 145 square feet of façade articulation. On the 45-foot-tall building sitting 15-feet below the street grade, this amounts to one 3.2-foot-wide strip of additional articulation. This strip width would be virtually unnoticeable to any pedestrian, bicyclist, or motorist.



A portion of the front façade is below the grade of the road (see Figure 8). This small portion – above the garage slab – will not be visible to pedestrians or bicyclists/motorists. Removing this portion of the façade from the total calculations, 25% of the front façade in RM2 will be offset. This meets the minimum façade articulation standard. Further, when we consider the entire front façade in both R7 and RM2, the total façade articulation will be 26%. This total articulation exceeds the minimum standard in RM2.

The proposal better meets applicable PD approval criteria (PZC 33.854.310.E through H) for the following reasons:

- E.1 Orienting the site and development to the public realm, while limiting less active uses of the site such as parking and storage areas along the public realm: The building will be 3-feet from the front lot line and SW Gibbs street. The requested modification is triggered because of the close proximity of the building to the front setback. The modification supports the siting of the building near the ROW, oriented toward the public realm. This approval criterion is positively impacted through the proposed modification.
- E.2 Preservation of natural features on the site, such as stands of trees, water features or topographical elements: As with public realm orientation, siting the building near the front lot line will result in better preservation of natural resource areas. The location of the building allows for the protection of the resource area of the environmental overlays. This criterion will be better met with the proposed modification.
- E.3 Inclusion of architectural features that complement positive characteristics of surrounding development, such as similar building scale and style, building materials, setbacks, and landscaping: The proposed design was previously determined by City of Portland planning staff and the Hearings Officer to meet this standard. Existing multi-family development nearby the site does not include façade articulation elements enough to meet the 25% standard. The proposed development will be the first required to meet the 25% standard. The request to reduce the façade articulation area from 25 to 22% will result in a minimal reduction in façade offset areas. Further, the proposed development will include other architectural elements that are seen in the surrounding development patterns – balconies, neutral tones, vertical wood paneling, modern/contemporary smooth accent facades (Hardie shingle siding and fiber cement panels), flat roofs, and common deck space. This criterion will not be impacted from this modification request.

- E.4 Mitigation of differences in appearance through means such as setbacks, screening, landscaping, and other design features; & E.5 Minimizing potential negative effects on surrounding residential uses; &
- F.1 Where proposed development includes attached houses, duplexes, triplexes, fourplexes, multi-dwelling structures, or multi-dwelling development, adequate open area to accommodate the proposed development must be provided. Open area does not include vehicle areas: These criteria will not be negatively or positively impacted through the requested modification because a loss of 145 square feet of offset area will not be noticeable from the street on a building with a front façade totaling 11,290 square feet. Considering the entire façade in both R7 and RM2, this modification request will reduce façade articulation areas only by 1.2%. These criteria will remain unimpacted.
- G. Accessible connections. Provide one or more accessible routes that connect all buildings on the site to adjacent streets, common open areas, and parking areas. Use landscaping and site furnishings to ensure the accessible route provides a pleasant user experience; & H. Garbage and recycling areas. Garbage and recycling collection areas must be adequate in size to accommodate the proposed development, designed to encourage recycling, and located to facilitate pick-up service. Screening and buffering of garbage and recycling areas must be provided to maintain a clean and attractive development: Façade articulation percentages will not negatively or positively impact site circulation or the placement of garbage and recycling areas. These criteria will not be impacted.

The requested modification to façade articulation positively impacts the site design by allowing the building to be located closer to the street which better orients the site to the public realm and better protects natural resources on the site. Allowing the modification does not impact the original approved site design or existing built features of the building while allowing for additional density in the building permitted through the new RM2 zoning. As the applicant notes, the total façade articulation across the entire building is 26 percent, meeting the intent of the regulation. Façade articulation does not impact open areas, accessible connections, or garbage and recycling areas. This criterion is met.

B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standards for which a modification is requested.

Findings: The purpose of 33.120.230 Building Length and Façade Articulation is as follows:

Purpose. These standards, along with the height and setback standard, limit the bulk of buildings close to the street. These standards help ensure that large buildings will be divided into smaller components that relate to the scale and development patterns of Portland's multi-dwelling residential areas and add visual interest and variety to the street environment.

The applicant has provided the following findings:

As described above, the request for a 3% reduction in façade articulation area totaling 145 square feet is so minimal that it will be indistinguishable for passersby. Other design features, like site topography, front entrance design to utilize the second floor, the proportion of windows, and front landscaping lend more to help limit the bulk of the building so near the street. The requested modification will remain consistent with the purpose of the façade articulation standards.

The overall façade of the building provides 26% façade articulation, which meets the purpose of the standard by breaking up the building into smaller components and adding visual interest to the building for users of the street. The proposal is consistent with the purpose of the façade articulation standards. This criterion is met.

Modifications to Required Outdoor and Common Outdoor Area Requirements

The applicant has requested the following modifications to 33.120.240 Required Outdoor and Common Areas:

- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

The modifications will be approved if the following approval criteria are met:

A. Better meets approval criteria. The resulting development will better meet the approval criteria of Section 33.854.310, above; and

Findings: The applicant has provided the following findings:

The proposal better meets applicable PD approval criteria (PZC 33.854.310.E through H) for the following reasons:

- **E.1 Orienting the site and development to the public realm, while limiting less active uses of the site such as parking and storage areas along the public realm:** The upper common deck is proposed over the trash/recycling areas and three parking spaces. The requested dimension reduction is to allow for this deck to cover these areas on the ground floor. Figure 3 shows the deck and proposed driveway leading to the back of the site. Placing the deck here allows for more site area to plan driveway access to the rear, outside of the public realm. If the upper deck was designed to meet the 20-foot by 20-foot requirement, the outdoor area would need to be placed elsewhere on the site, making site planning more challenging to fit all elements, and most likely resulting in the reduction of the driveway area preventing access from the rear.

The lower common lawn is proposed near the northeast corner of the site, outside of the protected environmental resource areas. This area is behind the building and along a pedestrian pathway linking the lower lobby entrance to the surface parking areas. The request to reduce dimensional requirements will have no impacts on site orientation and layout. The request to locate the open space area farther away from the lower lobby main entrance will not impact the site orientation and relationship with the public realm.

- **E.2 Preservation of natural features on the site, such as stands of trees, water features or topographical elements:** As discussed above, proposing the upper deck over passive use areas (storage and parking) allows for a more compact site design. The requested dimensional modification allows for the building to be set further away from the environmentally sensitive areas resulting in greater protection of natural features. Placing the deck over parking and storage also results in less anticipated grading activities that would have resulted from leveling the steep site topography to include a useable outdoor open space area.

The dimensional modifications are requested in-part to limit disturbance to natural features. The site is steeply sloped both to the north and to the east. Expanding this area or placing this area elsewhere on the site to measure 20 feet by 20 feet and at least 500 square feet will require additional grading and impacts to trees. As it stands, placing the lower common lawn as near as possible to the pedestrian walkway and parking areas will allow for the full protection of natural resource areas and limit impacts associated with grading and fill. The proposal will positively impact preservation efforts.

Placing the lower common lawn nearest to the surface parking area allows for the better protection of natural features. The lower lobby main entrance is planned near the eastern edge of the of the building. The entrance is approximately 10 feet away from the natural resource areas of the protected environmental tract. If the proposed lower common lawn was moved nearer to the lower lobby main entrance, additional impacts from fill and grading activities would result. Locating the proposed common area further away for a

building entrance will allow for the full protection of natural resource areas and limit impacts associated with grading and fill. The proposal will positively impact preservation efforts.

- **E.3** *Inclusion of architectural features that complement positive characteristics of surrounding development, such as similar building scale and style, building materials, setbacks, and landscaping:* The requests do not positively or negatively impact architectural features. A roughly 2.5-foot reduction in width (approximately 107 square feet) to the upper deck will be unnoticeable to users. The reduction in size of lower common lawn dimensions and the location of the lower lawn do not positively or negatively impact architectural features.
- **E.4** Mitigation of differences in appearance through means such as setbacks, screening, landscaping, and other design features; &

E.5 *Minimizing potential negative effects on surrounding residential uses:* The upper deck is planned to be accessed from the second floor and will face the forested natural area of Marquam Park. Placing the largest common open space area here will reduce impacts to the adjacent residential uses to the north, east, and south. All noise expected from active and passive recreational use in this area will be buffered by the proposed building and large, heavily vegetative environmental resource track. The dimensional reduction modification to place the outdoor common area over the parking/storage areas will positively improve mitigation measures for surrounding residential uses.

Reducing the dimensional requirements for the lower common lawn area will not positively or negatively impact surrounding uses or contribute to mitigation measures to blend with existing development.

Increasing the distance between the proposed open space area and the building entrance will not positively or negatively impact surrounding uses or contribute to mitigation measures to blend with existing development.

- **F.1** *Where proposed development includes attached houses, duplexes, triplexes, fourplexes, multi-dwelling structures, or multi-dwelling development, adequate open area to accommodate the proposed development must be provided. Open area does not include vehicle areas;* Although the request is for a dimension reduction from 20 to roughly 17.5 feet wide, the upper deck will still remain a useable common open area. The deck will be 690 square feet with a roughly 10-foot by 10-foot “notch” on the southern portion of the deck to offset the dimension reduction (see figure 9). The deck will be open and surfaced with decking materials. The deck will remain wide enough to accommodate passive uses. This standard is not positively or negatively impacted by the requested modification.

Although the request is for a dimension reduction from 20 to roughly 11.5 feet wide, and a reduction from 500 to 280 square feet, overall combined outdoor area requirements will be met. Further, by allowing an alternative dimensional design for the creation of this open space area will result in an excess of 202 square feet more than what is required. This criterion will be positively impacted through the proposed modifications.

Although the request is for an increase in distance from a main entrance, by creating this open space area the proposal will exceed the open space minimum requirement by 202 square feet. With additional outdoor open space areas, this criterion will be positively impacted through the proposed modifications.

- **G.** *Accessible connections. Provide one or more accessible routes that connect all buildings on the site to adjacent streets, common open areas, and parking areas. Use landscaping and site furnishings to ensure the accessible route provides a pleasant user experience:* Site circulation will not be impacted through this dimensional modification request (for the upper common deck).

The proposed lower common lawn will be adjacent to the surface parking and pedestrian path connecting the parking areas and the lower lobby main entrance. The dimensional modification request will allow for the open space to be planned in a central location, directly adjacent to the main pedestrian pathway. If the open space area is required to meet dimensional minimums, the outdoor area would have to be moved to less active areas of the site, with lower accessibility opportunities. The resulting modifications will have a positive impact to this criterion.

The proposed lower common lawn will be adjacent to the surface parking and pedestrian path connecting the parking areas and the lower lobby main entrance (see Figure 10). The locational modification allows the open space area to be placed in a central location, along the main pathway between the lower lobby entrance and the surface parking areas. The common open space area will be accessible to all along this pathway. The resulting modifications will have a positive impact to this criterion.

- **H. Garbage and recycling areas.** *Garbage and recycling collection areas must be adequate in size to accommodate the proposed development, designed to encourage recycling, and located to facilitate pick-up service. Screening and buffering of garbage and recycling areas must be provided to maintain a clean and attractive development:* The ability to place the deck over the garbage/recycling areas and some parking spaces will allow these passive uses to be screened from public view. This criterion will be positively impacted by the requested modifications.

There will be no impacts to proposed garbage and recycling areas with the granting of the lower common lawn modifications.

The applicant has provided findings indicating that the proposed modifications will better meet the Planned Development approval criteria. The requested modifications will either have a positive impact on the approval criteria, such as improving natural features and hiding the garbage and recycling areas, or will have no impact on the approval criteria. With the condition that the common outdoor areas are constructed as shown on the architectural, landscaping, and engineering plans (Exhibits C.1-C.7), this criterion is met for each of the three requested modifications.

B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standards for which a modification is requested.

Findings: The purpose of 33.120.240 Required Outdoor and Common Areas is follows

Purpose. The required outdoor and common area standards ensure opportunities for outdoor relaxation or recreation. The standards work with the building coverage and minimum landscaped areas standards to ensure that some of the land not covered by buildings is of adequate size, shape, and location to be usable for outdoor recreation or relaxation. The standards also ensure that outdoor areas are located so that residents have convenient access. Required outdoor areas are an important aspect in addressing the livability of a residential property by providing outdoor living opportunities, some options for outdoor privacy, and a healthy environment. These standards also allow for common area requirements to be met by indoor community facilities because they also provide opportunities for recreation and gathering.

The applicant has provided the following findings for each of the three requested modifications:

The request to reduce the upper deck dimension by roughly 2.5 feet will not impact the overall size or location of the upper deck. The dimension reduction request is for roughly 2.5 feet. The deck is proposed to be approximately 17.5 feet wide and 43 feet long. These dimensions still allow for both active and passive

uses to occur with ample room for multiple simultaneous users. Providing this modification would not negatively impact useable open space availability.

The dimension reduction request for the lower common lawn is for roughly 10 feet in both directions and 220 square feet. However, by creating this lower common lawn the proposal will exceed outdoor open space requirements by 202 square feet. Allowing the dimensional reductions will essentially ensure an over provision of outdoor open space areas, improving the livability of both residents and visitors.

The request to exceed the distance from a main door by 25 feet – from 20 to 45 feet – is requested to allow the creation of a second common open space area on a main floor. Without impacting environmental resources, the creation of the lower lawn in this area will allow the development proposal to exceed outdoor open space requirements by 202 square feet. Allowing the locational modification will essentially ensure an over provision of outdoor open space areas, improving the livability of both residents and visitors.

The applicant has requested modifications to the size and location of common outdoor areas in order to add an additional outdoor area, providing additional opportunities for recreation and relaxation for building residents. The proposed outdoor areas will continue to be of adequate size and shape and will still be conveniently located to provide useable space for recreation and relaxation. Signage from interior lobby areas will be provided to guide residents to the lower common lawn, mitigating its distance from a main entrance. With the condition that this signage is provided, this criterion is met for each of the three requested modifications.

DEVELOPMENT STANDARDS

Development standards that are not relevant to the land division review, have not been addressed in the review. Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 11 can be met, and those of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

Maximum density – This is a split zoned site located in the R7 and RM2 zone. Each zoned portion of the site is 20,000 square feet in area. As explained in 33.270.100.M, a PD review allows density to be transferred within a split zoned site. In this case, the total number of dwelling units permitted is determined by calculating the number of dwelling units allowed in the R7 portion of the site and adding it to the number of dwelling units allowed in the RM2 portion of the site. The number of units allowed in the RM2 portion of the site is calculated in terms of floor area; the floor area calculation is converted to dwelling units at the rate of 1 dwelling unit per 1,000 square feet of floor area. The base floor area allowance in the RM2 portion of the site is 1.5 to 1, or 30 units. However, the applicant is providing inclusionary housing and obtained a floor area bonus of .75 to 1. The total floor area permitted in the RM2 portion of the site is 2.25 to 1, or 45 units. The total number of dwelling units allowed on this site is 47 unit. The applicant is proposing a total of 43 units, meeting the maximum permitted density on this site.

OTHER TECHNICAL REQUIREMENTS

Technical decisions have been made as part of this review process. These decisions have been made based on other City Titles, adopted technical manuals, and the technical expertise of appropriate service agencies. These related technical decisions are not considered land use actions. If future technical decisions result in changes that bring the project out of conformance with this land use decision, a new land use review may be required. The following is a summary of technical service standards applicable to this preliminary partition proposal.

Bureau	Code Authority and Topic
Development Services/503-823-7300 www.portlandonline.com/bds	Title 24 – Building Code, Flood plain Title 10 – Erosion Control, Site Development Administrative Rules for Private Rights-of-Way
Environmental Services/503-823-7740 www.portlandonline.com/bes	Title 17 – Sewer Improvements 2008 Stormwater Management Manual
Fire Bureau/503-823-3700 www.portlandonline.com/fire	Title 31 Policy B-1 – Emergency Access
Transportation/503-823-5185 www.portlandonline.com/transportation	Title 17 – Public Right-of-Way Improvements Transportation System Plan
Urban Forestry (Parks)/503-823-4489 www.portlandonline.com/parks	Title 11 – Trees
Water Bureau/503-823-7404 www.portlandonline.com/water	Title 21 – Water availability

As authorized in Section 33.800.070 of the Zoning Code conditions of approval related to these technical standards may have been included in the decision on this proposal.

CONCLUSIONS

The applicant has proposed amendments to prior Planned Development and Partition approvals to allow for an increase in the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces.

The applicant has requested the following modifications through the Planned Development Amendment to address the increased outdoor area requirements from the additional units and thenew façade articulation requirements under the current zoning, :

- Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2);
- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

As discussed in this report, the relevant standards and approval criteria associated with the Land Division Partition Amendment, Planned Development Amendment, and Modifications have been met, or can be met with conditions. Therefore, Staff finds this proposal should be approved.

TENTATIVE STAFF RECOMMENDATION

(May be revised upon receipt of new information at any time prior to the Hearings Officer decision)

Approval of an Amendment for Changes to an Approved Preliminary Plan; and

Approval of an Amendment to a Planned Development Review to increase the approved number of dwelling units in a multi-dwelling structure from 27 to 43 units; and

Approval of the following Planned Development Review Modifications:

1. Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230)

2. Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.240)
3. Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240)
4. Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240)

These approvals are illustrated with Exhibits C.1-C.7 and are subject to the following conditions:

- A. Development on Parcel 1 must be in substantial conformance with Exhibits C.1-C.4. As part of the building permit revision submittal, all required site plans, building elevations, landscape plans, and any additional drawings must reflect the information and designs approved by this land use review. The sheets on which this information appears must be labeled, "Proposal and design as approved in Case File #LU 22-187796 APD AP. No field changes allowed." Specifically, the permit revision must include the following elements:
 - Increased bike parking spaces
 - Added common outdoor areas, with accessible pathways and related directional signage
 - Additional landscaping as shown on Exhibit C.3
- B. Conditions C.1 – C.6 of LU 19-258445 continue to apply to the site, with the exception that the design approved through the PD is updated to that approved under this review as stated in Condition A above.

Procedural Information. The application for this land use review was submitted on September 28, 2022 and was determined to be complete on November 7, 2022.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on September 28, 2022.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period. Unless further extended by the applicant, **the 120 days will expire on: March 7, 2023.**

Some of the information contained in this report was provided by the applicant.

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the recommendation of the Bureau of Development Services with input from other City and public agencies.

Conditions of Approval. If approved, this project may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term “applicant” includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

This report is not a decision. The review body for this proposal is the Hearings Officer who will make the decision on this case. This report is a recommendation to the Hearings Officer by the Bureau of Development Services. The review body may adopt, modify, or reject this recommendation. The Hearings Officer will make a decision about this proposal within 17 days of the close of the record. Your comments to the Hearings Officer can be mailed c/o the Hearings Officer, 1900 SW Fourth Ave., Suite 3100, Portland, OR 97201 or faxed to 503-823-4347.

You will receive mailed notice of the decision if you write a letter received before the hearing or testify at the hearing, or if you are the property owner or applicant. This Staff Report will be posted on the Bureau of Development Services website at <http://www.portlandoregon.gov/bds/35625>. Land use review notices are listed on the website by the District Coalition in which the site is located; the District Coalition for this site is identified at the beginning of this staff report. You may review the file on this case at the Development Services Building at 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201.

Appeal of the decision. The decision of the Hearings Officer may be appealed to City Council, who will hold a public hearing. If you or anyone else appeals the decision of the Hearings Officer, only evidence previously presented to the Hearings Officer will be considered by the City Council.

Who can appeal: You may appeal the decision only if you write a letter which is received before the close of the record for the hearing, if you testify at the hearing, or if you are the property owner/applicant. Appeals must be filed within 14 days of the decision. **Appeals must be filed within 14 days of the decision. An appeal fee of \$3,398.00 will be charged (one-half of the BDS LUS application fee, up to a maximum of \$5,000).**

Appeal Fee Waivers: Neighborhood associations recognized by the Office of Neighborhood Involvement may qualify for a waiver of the appeal fee provided that the association has standing to appeal. The appeal must contain the signature of the Chair person or other person authorized by the association, confirming the vote to appeal was done in accordance with the organization’s bylaws.

Neighborhood associations, who wish to qualify for a fee waiver, must complete the Type III Appeal Fee Waiver Request for Organizations Form and submit it prior to the appeal deadline. The Type III Appeal Fee Waiver Request for Organizations Form contains instructions on how to apply for a fee waiver, including the required vote to appeal.

For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this planned development review amendment approval. This approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun. If the approved project includes more than one component, such as multiple buildings, or multiple phases that will be broken into separate building permit applications, at least one permit must be obtained within 3 years of the date of this decision, and all permits must be obtained within ten years of the date of this decision. After that ten-year period, a new land use review will be required before permits will be issued for any remaining project components that have not yet been permitted, subject to the Zoning Code in effect at that time.

Applying for your permits. A building permit, occupancy permit, or development permit may be required before carrying out an approved project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed herein;
- All applicable development standards, unless specifically exempted as part of this land use review;
- All requirements of the building code; and
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

Planner's Name: Leah Dawkins

Date: December 14, 2022

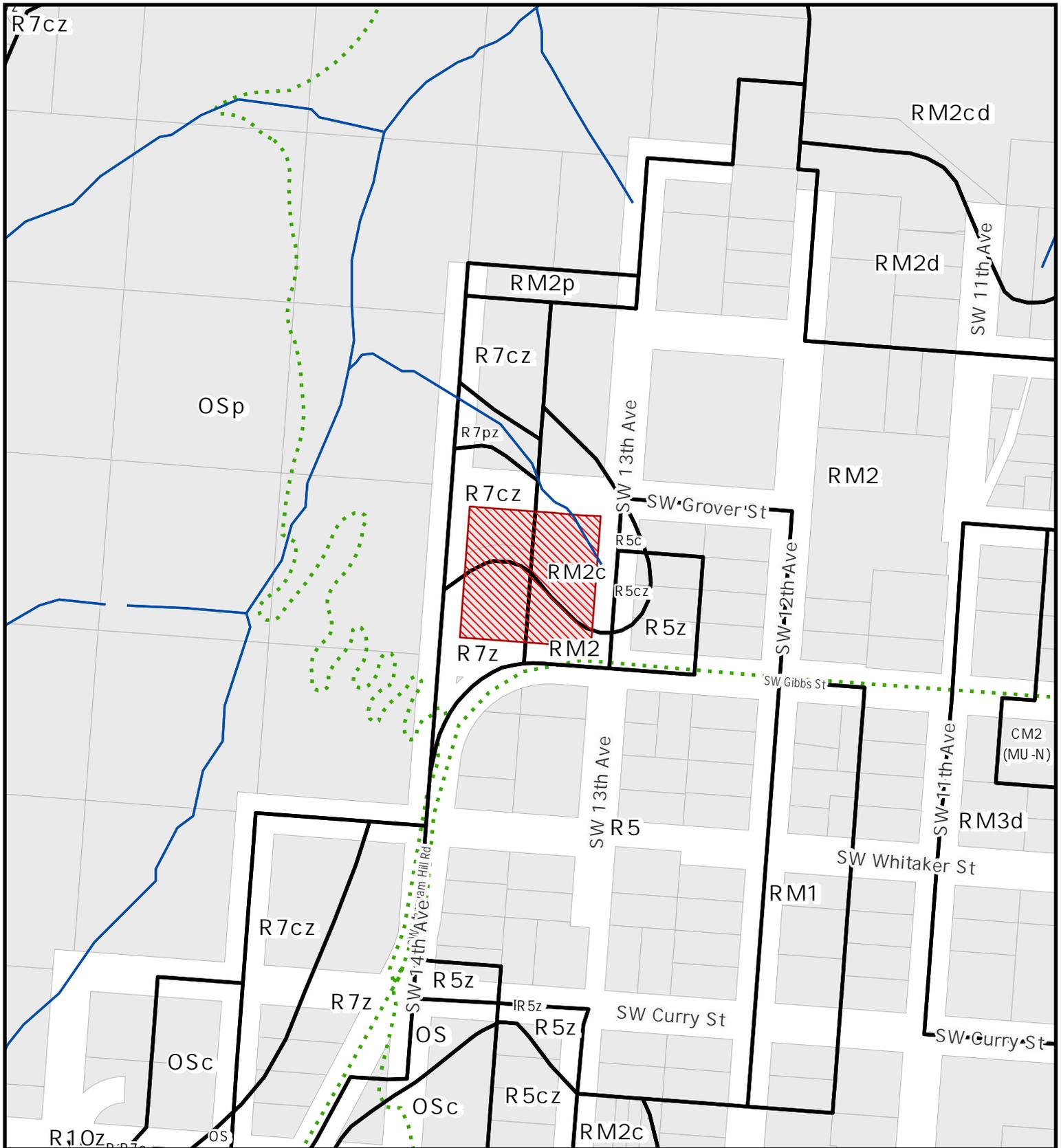
EXHIBITS

NOT ATTACHED UNLESS INDICATED

- A. Applicant's Statement:
 1. Applicant Narrative
 2. Resubmittal Response- 11/07/22
 3. Neighborhood Outreach Documentation
 4. Stormwater Management Report
 5. Transportation Impact Studies
 6. Arborist Report and Tree Protection Plan- July 2020
 7. Surrounding Neighborhood Photographs
 8. Inclusionary Housing Intake Form
 9. Inclusionary Housing Calculation Sheet
 10. Common Area Furniture Product Sheet
 11. Common Area Context Site Photos and Grading
 12. BES Plan Confirmation
 13. Site Development Grading Plan Confirmation
 14. Site Development Completeness Email
 15. Vicinity Map
 16. Early Assistance Meeting Notes (EA 22-144899)
- B. Zoning Map (attached):
 1. Existing Zoning
 2. Proposed Zoning
- C. Plans & Drawings:
 1. Site Plan (attached)
 2. Lower Common Lawn Plan Enlarged (attached)
 3. Lower Lawn Landscaping Plan (attached)
 4. Façade Elevation Plan (attached)
 5. Land Use and Architectural Plan Set (15 sheets)
 6. Landscape Plan Set (5 sheets)
 7. Engineering Plan Set (12 sheets)
 8. Topographic Survey
- D. Notification information:
 1. Request for Response
 2. Posting Letter sent to applicant
 3. Notice to be Posted
 4. Revised Notice to be Posted

5. Applicant's statement certifying posting
 6. Mailing list
 7. Mailed Notice
 8. Revised Mailed Notice
- E. Agency Responses:
1. Bureau of Environmental Services
 2. Bureau of Transportation Engineering and Development Review
 3. Water Bureau
 4. Fire Bureau
 5. Site Development Review Section of Bureau of Development Services
 6. Bureau of Parks, Forestry Division
 7. Life Safety Section of Bureau of Development Services
- F. Letters:
1. Milt Jones, 12/06/22, Pedestrian infrastructure concerns
 2. Homestead Neighborhood Association, 12/08/22, Pedestrian infrastructure concerns
- G. Other:
1. Original LUR Application
 2. Incomplete Letter
- H.

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).



For Zoning Code in Effect July 1, 2022 - Sept. 30, 2022

ZONING F

- Site
- Stream
- Recreational Trails

File No. LU 22 - 187796 APD, AP

1/4 Section 3327

Scale 1 inch = 200 feet

State ID 1S1E09BD 2801

Portland Hearings Office
 Case #42 Exhibit B Nov 10, 2022
 Bureau Case #LU 22-187796 APD AP
 Exhibit #4
 Page 24 of 28

ZONING SUMMARY:

SPLIT ZONING: R7 / RM2
 PROPOSED USE: 5-STORY, 43-UNIT APARTMENTS OVER STRUCTURED PARKING WITH 24 STALLS + 4 ADDITIONAL AT GRADE STALLS = TTL. 28 REFER TO NARRATIVE

LAND USE:
 LOT AREA R7 = 20,000 SF (14,099 SF W/ ENVIRONMENTAL RESOURCE TAKEN OUT)
 LOT AREA RM2 = 20,000 SF (9,400 SF W/ ENVIRONMENTAL RESOURCE TAKEN OUT)

FAR PROPOSED IN RM2:	CONTRIBUTE	NON-CONTRIBUTE (PARKING)
FLR. 1 / GARAGE	-	4,863
FLOOR 2	5,280	-
FLOOR 3	5,487	-
FLOOR 4	5,487	-
FLOOR 5	5,487	-
TTL	27,228	4,863
TTL FAR PROPOSED IN RM2	27,228 SF (1.4 : 1)	

PLANNING AND ZONING DEVELOPMENT STANDARDS

	REQUIRED / ALLOWABLE (PER P.D.)	PROPOSED
HEIGHT (SEE A1.02)	45-FT W/ IN 25-FT FRONT P.L. 49-0" MAX REST OF SITE	45-FT 45-FT
FRONT SETBACK	0 FT (CONSERVATION ZONE)	1'-1" (DECK) / 3'-0" (BLDG.)
SIDE SETBACK	0 FT (CONSERVATION ZONE)	5'-0" (EAST ELEVATION)
BICYCLE PARKING (SEE 1/A1.62)	48 LONG TERM 3 SHORT TERM	48 3

OUTDOOR AREA:

SEE FLOOR PLANS, A2.xx SERIES, FOR INDIVIDUAL DECKS AND PATIOS.

INDIV. OUTDOOR AREA PER UNIT REQUIRED	PROVIDED
48 SF	62 SF
MIN. 6-FT EA. DIRECT.	6'-5" MIN. DIMENSION

COMMON OUTDOOR AREA	PROVIDED
48 SF (x16 UNITS) =	768 SF REQ. (MIN. 500 SF)

PROPOSED:	PROVIDED:
690 SF AT WEST 2ND FLOOR DECK	280 SF AT NE AT GRADE AREA
TTL	970 SF

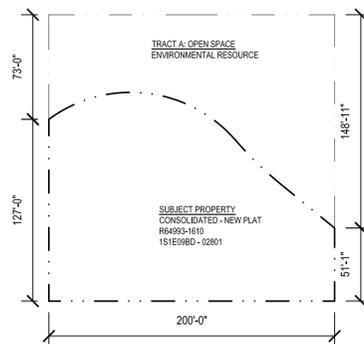
INCLUSIONARY HOUSING CALCULATIONS

UNITS PROVIDED ON-SITE TO MEET BOTH AFFORDABLE HOUSING STANDARDS FOR DEVELOPMENTS OVER 20 UNITS. CALCULATIONS ARE FOR 10% TOTAL AT 60% MFL.

UNITS	REQ. AFFORDABLE UNITS (10%)
43	4.3 = 4 UNITS

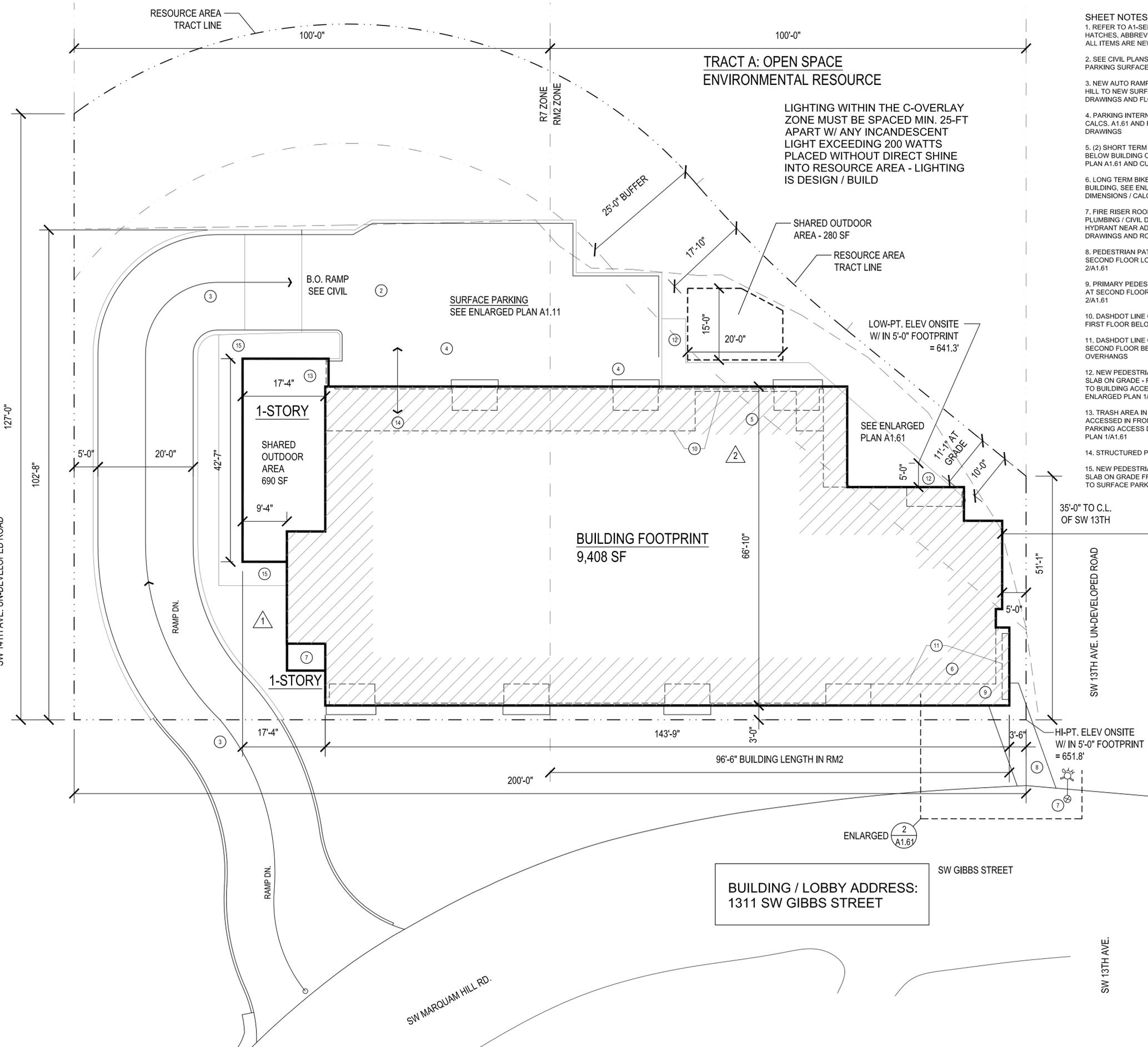
SEE A1.01 FOR FIRE DEPT. RELATED ITEMS

SEE A1.11 AND A1.21 FOR LIFE SAFETY SUMMARY



2. PROPERTY LINE PLAN

SCALE: 1/64" = 1'-0" ON 22x34 1/128" = 1'-0" ON 11x17



1. SITE PLAN

SCALE: 3/32" = 1'-0" ON 22x34 3/64" = 1'-0" ON 11x17

SHEET NOTES:

- REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
- SEE CIVIL PLANS FOR STORMWATER AND PARKING SURFACE DRAINAGE
- NEW AUTO RAMP DOWN OFF SW MARQUAM HILL TO NEW SURFACE PARKING, SEE CIVIL DRAWINGS AND FLOOR PLANS
- PARKING INTERNAL LANDSCAPE AREA, SEE CALCS. A1.61 AND REFER TO LANDSCAPE DRAWINGS
- (2) SHORT TERM BIKE PARKING SPACES BELOW BUILDING OVERHANG, SEE ENLARGED PLAN A1.61 AND CUT SHEET A1.62
- LONG TERM BIKE PARKING INSIDE BUILDING, SEE ENLARGED PLAN A1.62 FOR DIMENSIONS / CALCS. AND CUT SHEETS
- FIRE RISER ROOM 1ST FLOOR - SEE PLUMBING / CIVIL DRAWINGS - NEW FIRE HYDRANT NEAR ADA ACCESS, SEE CIVIL DRAWINGS AND ROW PERMIT
- PEDESTRIAN PATH FROM SW GIBBS TO SECOND FLOOR LOBBY, SEE ENLARGED PLAN 2/A1.61
- PRIMARY PEDESTRIAN ENTRY TO BUILDING AT SECOND FLOOR, SEE ENLARGED PLAN 2/A1.61
- DASHDOT LINE OF BUILDING OUTLINE AT FIRST FLOOR BELOW BUILDING OVERHANGS
- DASHDOT LINE OF BUILDING OUTLINE AT SECOND FLOOR BELOW BUILDING OVERHANGS
- NEW PEDESTRIAN ACCESS PATHS - CONC. SLAB ON GRADE - FROM SURFACE PARKING TO BUILDING ACCESS AT FIRST FLOOR, SEE ENLARGED PLAN 1/A1.61 AND CIVIL
- TRASH AREA IN FIRST FLOOR ENCLOSURE, ACCESSED IN FRONT OF STRUCTURED PARKING ACCESS DOOR, SEE ENLARGED PLAN 1/A1.61
- STRUCTURED PARKING GARAGE ACCESS
- NEW PEDESTRIAN ACCESS PATH - CONC. SLAB ON GRADE FROM STAIR WELL EGRESS TO SURFACE PARKING / BOTTOM OF RAMP

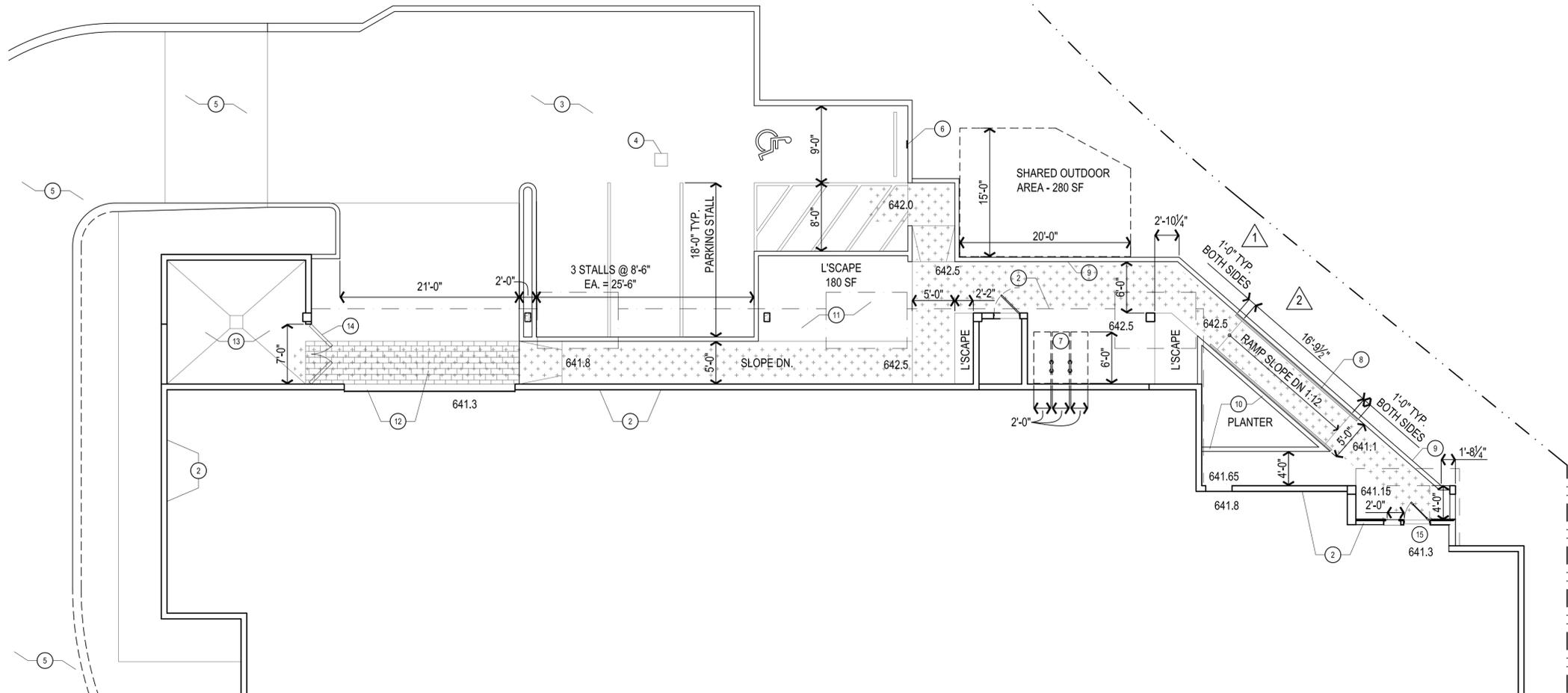
STEELHEAD ARCHITECTURE
 5210 SE 26TH AVE. PORTLAND OREGON 97202



SITE PLAN: COVERAGE + ZONING SUMMARY
 GIBBS DEVELOPMENT
 1325 SW GIBBS STREET
 PORTLAND, OR 97239

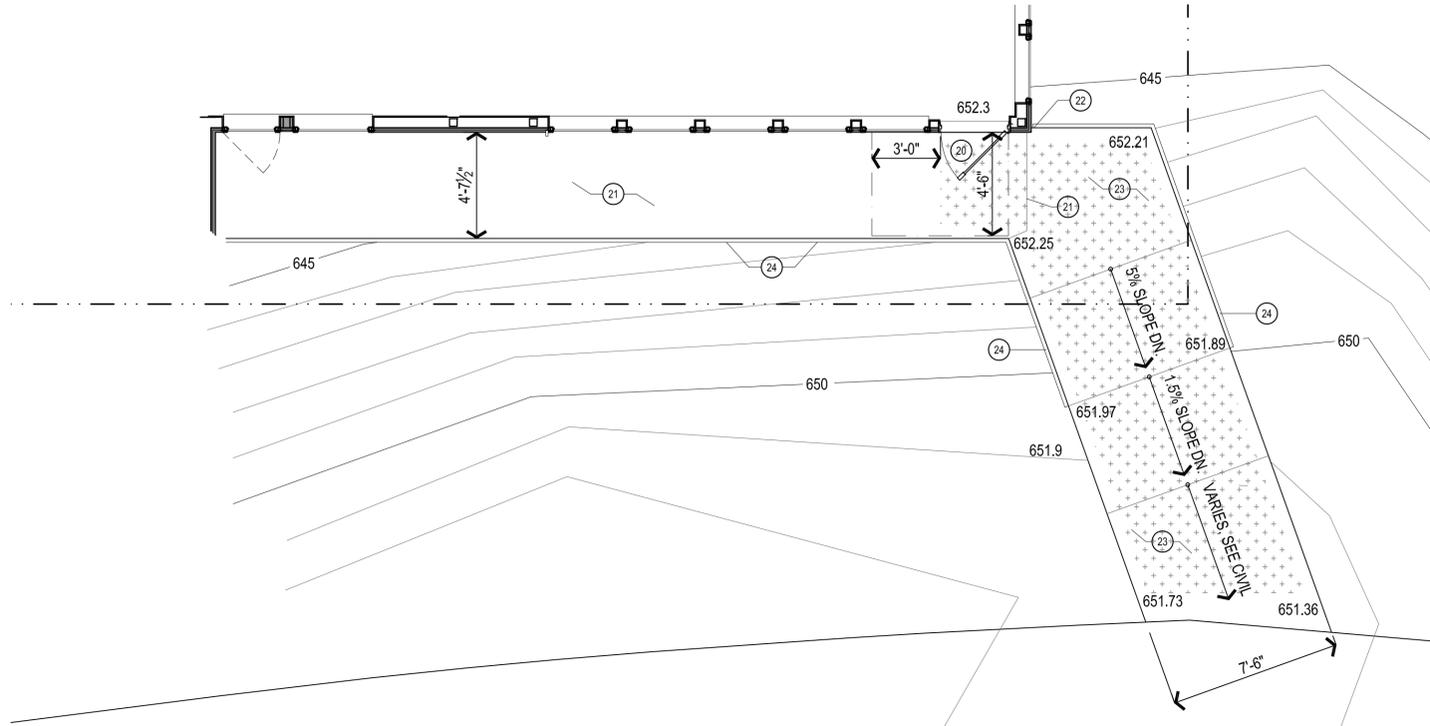
LAND USE SET
 09/01/2022

A1.00
 LU 22-187796 ADP AP
 Portland Hearings Office
 Case # LU 22-187796
 Bureau Case # LU 22-187796 ADP AP
 Project # 22-187796
 Page 25 of 28



1. ENLARGED PARKING LOT + PED ACCESS TO LOWER LOBBY
 SCALE: 1/4" = 1'-0" ON 22x34 1/8" = 1'-0" ON 11x17

- SHEET NOTES:**
- REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 - OUTLINE OF FIRST FLOOR BUILDING, SEE A2.12 PLAN
 - SURFACE PARKING LOT, SEE CIVIL DRAWINGS FOR SLOPES
 - PARKING LOT AREA DRAIN TO STORM WATER CONTAINMENT / TREATMENT, SEE CIVIL DRAWINGS
 - AUTO RAMP UP, SEE CIVIL DRAWINGS AND STRUCT. DRAWINGS
 - ACCESSIBLE PARKING SIGNAGE, SEE 2/A1.62
 - SHORT TERM BICYCLE PARKING, SEE 5/A1.62 - TYP. OF (3)
 - RAMP HANDRAIL, TYP. EA SIDE - SEE 3/A1.62
 - CONC. CURB, MIN. 6" ABOVE ADJACENT GRADE
 - TRIANGULAR CONC. PLANTER, CURB 6" ABOVE HI-SIDE OF RAMP - FLAT TOP FOR APPROX. 26" HEIGHT ABOVE LOW SIDE OF WALKWAY
 - SURFACE PARKING INTERNAL LANDSCAPE AREA, SEE LANDSCAPE DRAWINGS
 CALCS AS FOLLOWS:
 REQ. = 45 SF / STALL (x4) = 180 SF
 PROVIDED = 180 SF
 - TEXTURED CONCRETE PEDESTRIAN PATH TO CONTRAST ADJACENT PAVING - CROSSING AT STRUCTURED PARKING GARAGE ENTRY
 - ENCLOSED / COVERED TRASH AREA W/ HYDRONICALLY ISOLATED SLAB SLOPED MIN. 1/8" PER FT. TO DRAIN, SEE PLUMBING DRAWINGS
 - DOUBLE 42" WIDE GATES, SEE ELEVATIONS
 - LOWER LEVEL LOBBY ENTRY W/ LATCH SIDE APPROACH DOOR CLEARANCE, SEE 7/A1.11
 - 16 - 19. NOT USED
 - UPPER LEVEL LOBBY ENTRY W/ FORWARD APPROACH DOOR CLEARANCE, SEE 6/A1.11
 - LINE OF PT SLAB BELOW - EXPOSED PT SLAB SLOPED 1/4" PER FT. AWAY FROM BUILDING
 - ALIGN CONC. PEDESTRIAN PATH W/ CORNER OF CONC. WALLS
 - CONC. SLAB ON GRADE W/ TURNED DOWN EDGES, S.S.D. - SEE SPOT ELEVATIONS AND TOPO LINES PLUS CIVIL DRAWING C11 FOR MULTI-SLOPE AND CONNECTION POINT AND ELEVATION OF PATH TO ROAD IN PUBLIC RIGHT OF WAY
 - STEEL GUARDRAIL AT CONC. EDGE, SEE 3/A9.1 - TERMINATES AT END OF 5% SLOPE - HANDRAILS NOT ALLOWED IN ROW PER PBOT



2. ENLARGED PED ACCESS TO UPPER LOBBY
 SCALE: 3/32" = 1'-0" ON 22x34 3/64" = 1'-0" ON 11x17

DENOTES PEDESTRIAN ACCESS PATH

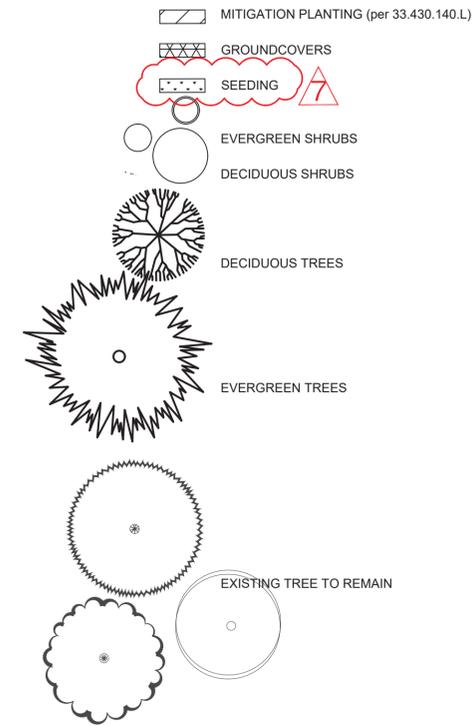


RECEIVED 04/22/21

LAND USE SET
 09/01/2022

PLANTING SITE PLAN

LEGEND

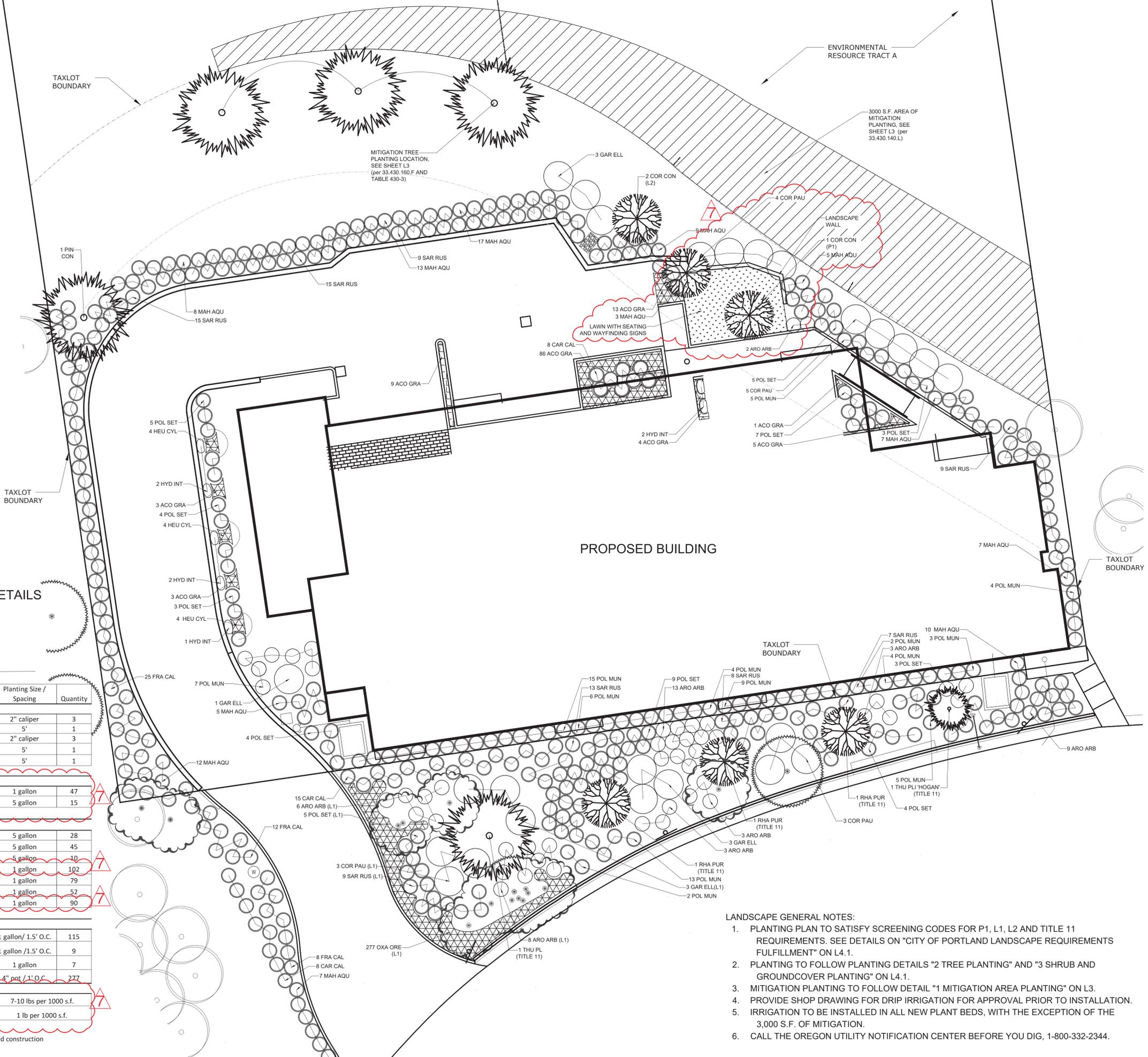


SEE TH0903 FOR APPROVED RIGHT OF WAY DETAILS AND STREET TREE ACTIVITY

PLANT LIST

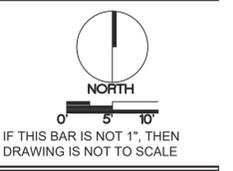
Symbol	Botanical Name	Common Name	Planting Size / Spacing	Quantity
Trees				
COR CON	<i>Cornus controversa</i>	Giant Dogwood	2" caliper	3
PIN CON	<i>Pinus contorta var. contorta</i>	Shore Pine	5"	1
RHA PUR	<i>Rhamnus purshiana</i>	Cascara	2" caliper	3
THU PLI	<i>Thuja plicata</i>	Western Redcedar	5"	1
'HOGAN'	<i>Thuja plicata 'Hogan'</i>	Hogan Western Redcedar	5"	1
Deciduous Shrubs				
ARO ARB	<i>Aronia arbutifolia 'Brilliantissima'</i>	Brilliant Red Chokeberry	1 gallon	47
COR PAU	<i>Corylopsis pauciflora</i>	Buttercup Winter-hazel	5 gallon	15
Evergreen Shrubs				
CAR CAL	<i>Carpenteria californica 'Elizabeth'</i>	Bush Anemone	5 gallon	28
FRA CAL	<i>Frangula californica 'Eve Case'</i>	Eve Case Coffeeberry	5 gallon	45
GAR EL	<i>Garrya elliptica 'James Roof'</i>	Wavy Leaf Silk Tassel	5 gallon	10
MAH AQU	<i>Mahonia aquifolium</i>	Tall Oregongrape	1 gallon	102
POL MUN	<i>Polystichum munitum</i>	Swordfern	1 gallon	79
POL SET	<i>Polystichum setiferum var. diversilobum</i>	Soft Shield Fern	1 gallon	52
SAR RUS	<i>Sarcococca ruscifolia</i>	Sweet Box	1 gallon	90
Groundcovers and Vines				
ACO GRA	<i>Acorus gramineus 'Variegatus'</i>	Variegated Sweet Flag	1 gallon/ 1.5' O.C.	115
HEU CYL	<i>Heuchera cylindrica</i>	Green Alum Root	1 gallon /1.5' O.C.	9
HYD INT	<i>Hydrangea integrifolia</i>	Evergreen Climbing Hydrangea	1 gallon	7
OXA ORE	<i>Oxalis oregana</i>	Redwood Sorrel	4" pot / 1' O.C.	277
Lawn Seeding				
	Pro Time Lawn Seed: PT 305 Shade/Sun Mix		7-10 lbs per 1000 s.f.	
	Pro Time Lawn Seed: PT 313 Super Shade Supplement		1 lb per 1000 s.f.	

Required plantings per LU 22-250445 PD ENM LDP
 Quantities are for permitting, contractor should verify all quantities independently for ordering and construction
 Groundcovers are triangularly spaced and centered as specified



LANDSCAPE GENERAL NOTES:

1. PLANTING PLAN TO SATISFY SCREENING CODES FOR P1, L1, L2 AND TITLE 11 REQUIREMENTS. SEE DETAILS ON "CITY OF PORTLAND LANDSCAPE REQUIREMENTS FULFILLMENT" ON L4.1.
2. PLANTING TO FOLLOW PLANTING DETAILS "2 TREE PLANTING" AND "3 SHRUB AND GROUNDCOVER PLANTING" ON L4.1.
3. MITIGATION PLANTING TO FOLLOW DETAIL "1 MITIGATION AREA PLANTING" ON L3.
4. PROVIDE SHOP DRAWING FOR DRIP IRRIGATION FOR APPROVAL PRIOR TO INSTALLATION.
5. IRRIGATION TO BE INSTALLED IN ALL NEW PLANT BEDS, WITH THE EXCEPTION OF THE 3,000 S.F. OF MITIGATION.
6. CALL THE OREGON UTILITY NOTIFICATION CENTER BEFORE YOU DIG, 1-800-332-2344.



CONSULTANT:
NNA
 Landscape Architecture
 1811 S RIVER DRIVE SUITE 300
 PORTLAND, OR 97201
 503.239.0600

SCOTIA WESTERN STATES HOUSING
 PORTLAND, OR 97239
 PRELIMINARY LANDSCAPE PLAN

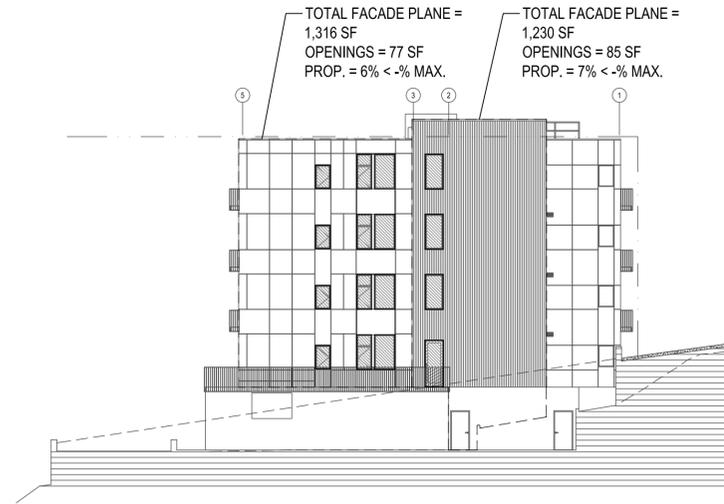
REVISIONS:		
DELTA	DATE	TITLE
Δ	02/05/2021	Rev 1
Δ	09/22/2022	Rev 7

SCALE: 1" = 10'
 DRAWN BY: JJ
 CHECKED BY: DG

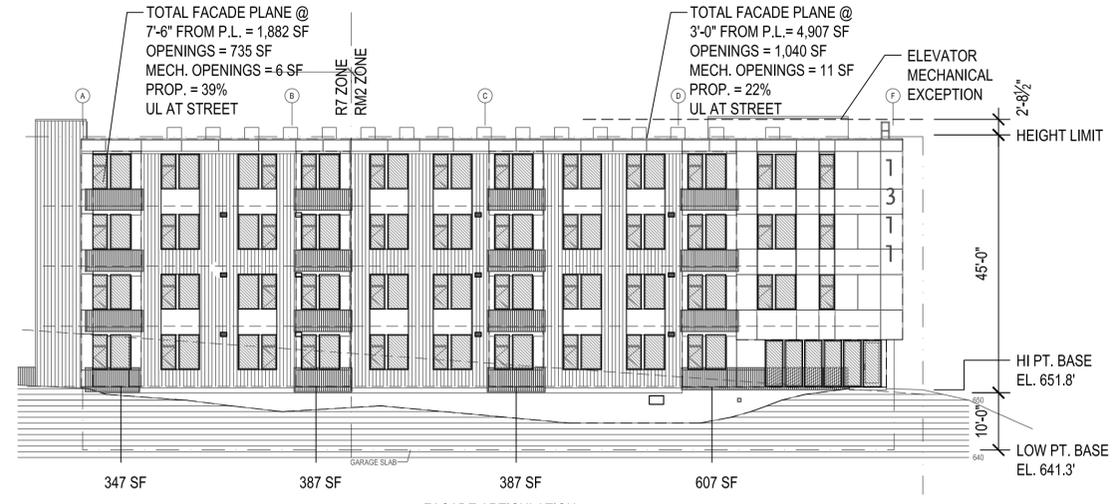
SHEET
L-4

DATE: 09/23/22
 JOB NO. 1914
LU 22-187796 APD AP
Exhibit C.3

SHEET NOTES:
 1. REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 2.

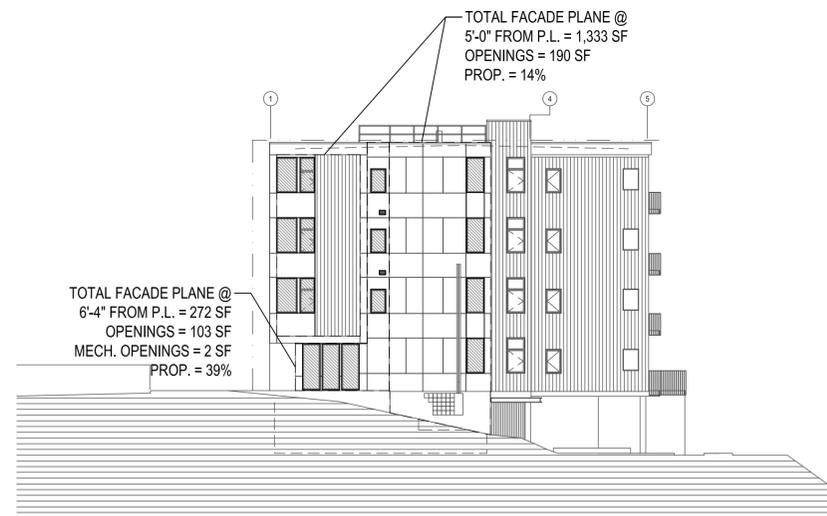


3. HEIGHT + EAST FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



1. HEIGHT + SOUTH FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17

TOTAL FACADE BOTH ZONES = 6,734 SF
 FACADE > 2 FT. OF PRIMARY PLANE = 1,728 SF (26%)
 TOTAL FACADE WITHIN RM2 = 4,556 SF
 FACADE > 2 FT. OF PRIMARY PLANE = 994 SF (22%)



4. HEIGHT + WEST FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



2. HEIGHT + NORTH FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



Opted-out parties for the case #4220018

Jesse Winterowd
Winterbrook Planning
610 SW ALDER ST #810
PORTLAND OR 97205

Sean O'Neill
Scotia Western States Housing LLC
15962 BOONES FERRY RD. STE 202
LAKE OSWEGO OR 97035

SWS PDX LLC
15110 Boones Ferry Road #500
Lake Oswego, Oregon 97035

Milt Jones
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Portland, Oregon 97239

Ed Fischer
Homestead Neighborhood Association
P.O. Box 80191
Portland, Oregon 97280

Michael Pina
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Portland, Oregon 97201

Ed Fischer
3404 SW 13th Avenue
Portland, Oregon 97239

Jaime Crawford
Winterbrook Planning
610 SW Alder St. Suite 810
Portland, Oregon 97205

Email Mailing List for LU 22-187796 ADP AP (4220018)

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jesse@winterbrookplanning.com; sean@scotiawsh.com; land-use@homesteadna.org;
CivicLife@portlandoregon.gov; mjones@miltjones.com; edfischer8@gmail.com;
michael.pina@portlandoregon.gov; jaime@winterbrookplanning.com

General Information

Owner: Sean O'Neill, Scotia Western States Housing
15962 Boones Ferry Road, Suite 202
Lake Oswego, OR 97035
503-956-9307
sean@scotiawsh.com

Representative: Jesse Winterowd, Winterbrook Planning
610 SW Alder Street Suite 810
Portland, OR 97205
503-827-4422 x109
jesse@winterbrookplanning.com

Location: 1311 SW Gibbs Street

State ID Number: 1S1E09BD -02801

Property ID: R712893

Zoning & Overlays: Residential 7,000 (R7), Residential Multi-Dwelling 2 (RM2),
Environmental Conservation (c), Constrained Sites (z)

Neighborhood: Homestead

Case Types: Type III Planned Development Review, &
Type Ix Partition Amendment

Pre-Application Meeting: June 16, 2022; EA 22-144899

Contents

Contents.....	3
List of Figures	4
Introduction	6
Project Summary	6
Existing Conditions	6
Project Description	7
Neighborhood Outreach.....	9
Surrounding Land Uses	10
Compliance with Previous Land Use Decisions	11
Narrative Organization.....	11
Part 1: Base Zone and Development Standards.....	12
1.1 R7 Residential 7,000 (33.110).....	12
1.2 RM2 Residential Multi-Dwelling 2 (33.120)	19
1.3 Inclusionary Housing (33.245)	32
1.4 Landscaping and Screening (33.248)	34
1.5 Parking, Loading, Transportation and Parking Demand Management (33.266) ...	34
Part 2: Planned Development Modification & Partition Amendment	46
2.1 Planned Development (33.270).....	46
2.2 Planned Development Review (33.854).....	48
2.1 Partition Amendment – Review of Land Divisions in Open Space, Residential, and IR Zones (33.660)	63
Part 3: Environmental Review and Tree Protection Standard.....	65
3.1 Environmental Review	65
3.2 Tree Preservation Standards (Title 11).....	65
Conclusion.....	74

Drawing Sets

Architectural Drawings

- ❖ A1.00 Site Plan: Coverage + Zoning Summary (Proposed Development)
- ❖ A1.02 Building Height + Façade Plane/Opening Calculations
- ❖ A1.16 Enlarged Site Plan Areas

- ❖ A1.62 Site Details (Bicycle Storage)
- ❖ A2.12 First Floor Plan
- ❖ A2.22 Second Floor Plan
- ❖ A2.31 Third – Fourth Floor Plan
- ❖ A2.51 Fifth Floor Plan
- ❖ A2.61 Roof Plan
- ❖ A4.11 Exterior Elevations
- ❖ A4.12 Exterior Elevations

Engineering Drawings – Permit # 20-194259-000-00-CO

- ❖ C0 Cover
- ❖ C1 Existing Conditions and Demolition Plan
- ❖ C2 Sanitary and Water Plan
- ❖ C3 Storm Plan
- ❖ C4 Site Plan
- ❖ C5 Wall and Driveway Profiles
- ❖ C6 Grading Plan
- ❖ C7 Early Construction Management Plan
- ❖ C8 Late Construction Management Plan
- ❖ C9 Erosion Construction Details
- ❖ C10 Construction Details
- ❖ C11 Construction Details

Landscape Drawings

- ❖ L1 Existing Site Conditions
- ❖ L3 Mitigation Area Plan
- ❖ L4 Planting Site Plan
- ❖ L4.1 Planting Details
- ❖ L5 Tree Preservation Site Plan
- ❖ Product Sheet – KINN Chair

Surveying Drawings

- ❖ Topographic Survey (Existing Conditions Plan)

Appendices

- ❖ Appendix A: Neighborhood Outreach Documentation
- ❖ Appendix B: Early Assistance Meeting Notes (EA 22-144899)
- ❖ Appendix C: Transportation Impact Study
- ❖ Appendix D: Stormwater Management Report
- ❖ Appendix E: Arborist Report
- ❖ Appendix F: Photographs of Surrounding Neighborhood
- ❖ Appendix G: Vicinity Map

List of Figures

Figure 1. Aerial Overview of Neighborhood..... 1
 Figure 2. Building Rendering facing Northwest from SW Gibbs Street..... 5

Figure 3. Building Rendering facing Southwest..... 5
 Figure 4 Zoning Map. Source: Portland Maps 6
 Figure 5. Proposed Site Plan, as shown on Sheet A1.00..... 8
 Figure 6. Vicinity Map..... 10
 Figure 7 Proposed Common Lawn with Landscaping..... 34
 Figure 8 Facade Articulation 54
 Figure 9. Upper deck as shown on site plan 57
 Figure 10. Lower common lawn on site plan..... 60



Figure 2. Building Rendering facing Northwest from SW Gibbs Street.



Figure 3. Building Rendering facing Southwest.

Introduction

Project Summary

The requested Planned Development (PD) Review will modify approved land use review LU #19-258445 PD, ENM, LDP. The modification request will increase the previously approved unit count of 27 to 43 units. Some two-bedroom units are proposed to be converted to one-bedroom and studio apartments.

The proposal will increase bicycle parking from 32 to 51 spaces and increase required outdoor areas from 1,296 sq. ft. to 2,266 sq. ft. To better meet outdoor area needs and comply with façade articulation requirements, four PD modifications are requested.

The building envelope and façade are not proposed to change. Site access and circulation are not proposed to change. And the protected environmental tract created in a 2019 land division will remain unchanged.

Existing Conditions

The project site is located on the north side of SW Gibbs Street and SW 13th Avenue where SW Gibbs Street turns south and becomes SW Marquam Hill Road. Existing conditions are shown on the **Topographic Survey** and **Sheet C1**. The site history is as follows:

- ❖ **1956:** A one-story and two-story structure were built on the site as a dental student dormitory.
- ❖ **1976:** A conditional use approval (CU 75-76) was granted for a convalescent home with up to 30 beds, however this approval was never acted on and the decision expired.
- ❖ **1979:** A conditional use permit (CU 020-79) for a residential drug treatment program was approved and the program operated until 1991.
- ❖ **1991:** The site operated as a long-term residency program for homeless persons until 1996. Additional land use approval was not required because the use was still considered a group living facility.
- ❖ **1996:** Continued use as a group living facility but as a residential care facility for seniors.
- ❖ **1997:** A type II conditional use permit (LUR 96-00236 CU) to enclose a breezeway connecting the two structures and construct a vertical lift was approved.

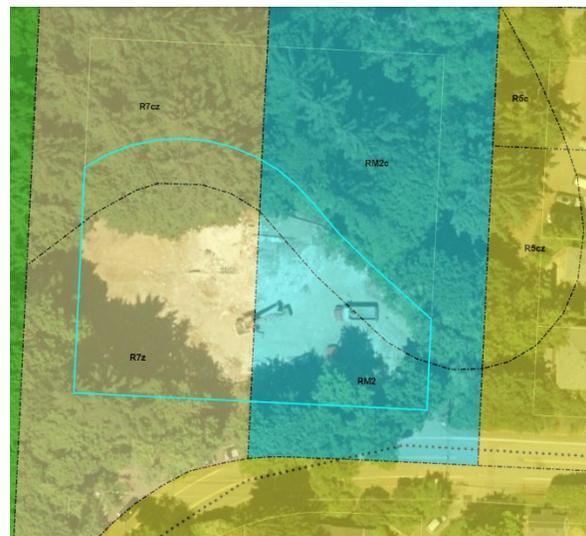


Figure 4 Zoning Map. Source: Portland Maps

- ❖ **2020:** A type III planned development review, land division, and environmental review modifications (LU 19-258445 PD LDP ENM) to demolish an existing structure and construct a 27-unit apartment complex was approved. The PD review was required to spread and move density away from environmentally sensitive areas. The land division was required to create a separate and protected environmental resource tract. The environmental review modifications were required to allow for both the land division and the PD to provide better environmental protections (ex. moving the building further away from the environmental resource tract)

The proposed PD modifications contained in this application are to this most recent PD approval.

Lots and Zoning: The site is split-zoned: the eastern 20,000 square feet is zoned RM2 and the western 20,000 square feet is zoned R7. It is surrounded by City rights-of-way on all sides, however only the south side SW Gibbs Street is an improved right-of-way (ROW). The other three sides are steeply sloping and include an environmental conservation tract and overlay zone. The site was graded extensively in the 1970's to create a flat development area and allow construction of a senior care facility and parking area. Pursuant to 2020-174739-000-00-CO and 2020-174829-000-00-CO, previous buildings on site have been demolished.

Environmental Overlay: The northern half of the site slopes steeply to the north. An environmental conservation area covers 17,107 square feet of the site. The environmental conservation area is heavily forested and contains a drainageway in the northeast corner, however no wetlands or seasonal streams are located on the site according to available City of Portland GIS data. The entire resource area of the environmental zone is within a resource protection tract established through LU 19-258445 PD LDP ENM.

Access: The site was previously accessed from two driveways on both the eastern and western edges. The eastern access has been removed. The western access has been improved to accommodate the steep slopes and correct entry angle onto SW Gibbs Street.

There are no sidewalks on SW Gibbs Street in front of the subject site. The sidewalks on SW Gibbs Street end two blocks (approximately 500 feet) to the east of the site. The shoulder in front of the site is approximately 6 feet wide or greater, providing adequate width to allow for pedestrian access. A designated public trail runs along SW Gibbs Street, connecting the trail network of Marquam Nature Park to the OHSU campus and trails to the east.

Project Description

Scotia Western States Housing, LLC achieved land use approval for an apartment building, a land division, and environmental review modifications for this site in 2020 (LU 19-258445 PD LDP ENM). The current proposal is for an interior change to the approved apartment building – providing one-bedroom and studio apartments instead of two-bedroom apartments and increasing the total number of units from 27 to 43. Existing approvals

relating to the land division, environmental review modifications, and tree protection are not affected by the proposal (See EA 22-144899, **Appendix B**).

As shown in Figure 4, the site is split-zoned and contains an environmental conservation overlay. The PD regulations allow for the transfer of dwelling unit density from the R7 zone to the RM2 zone and alternative residential lot size dimensions. The resource area of the environmental conservation zone is already protected in an environmental resource protection tract, and this is not proposed to change. The site layout and building configuration are shown on the site plan in Figure 5.

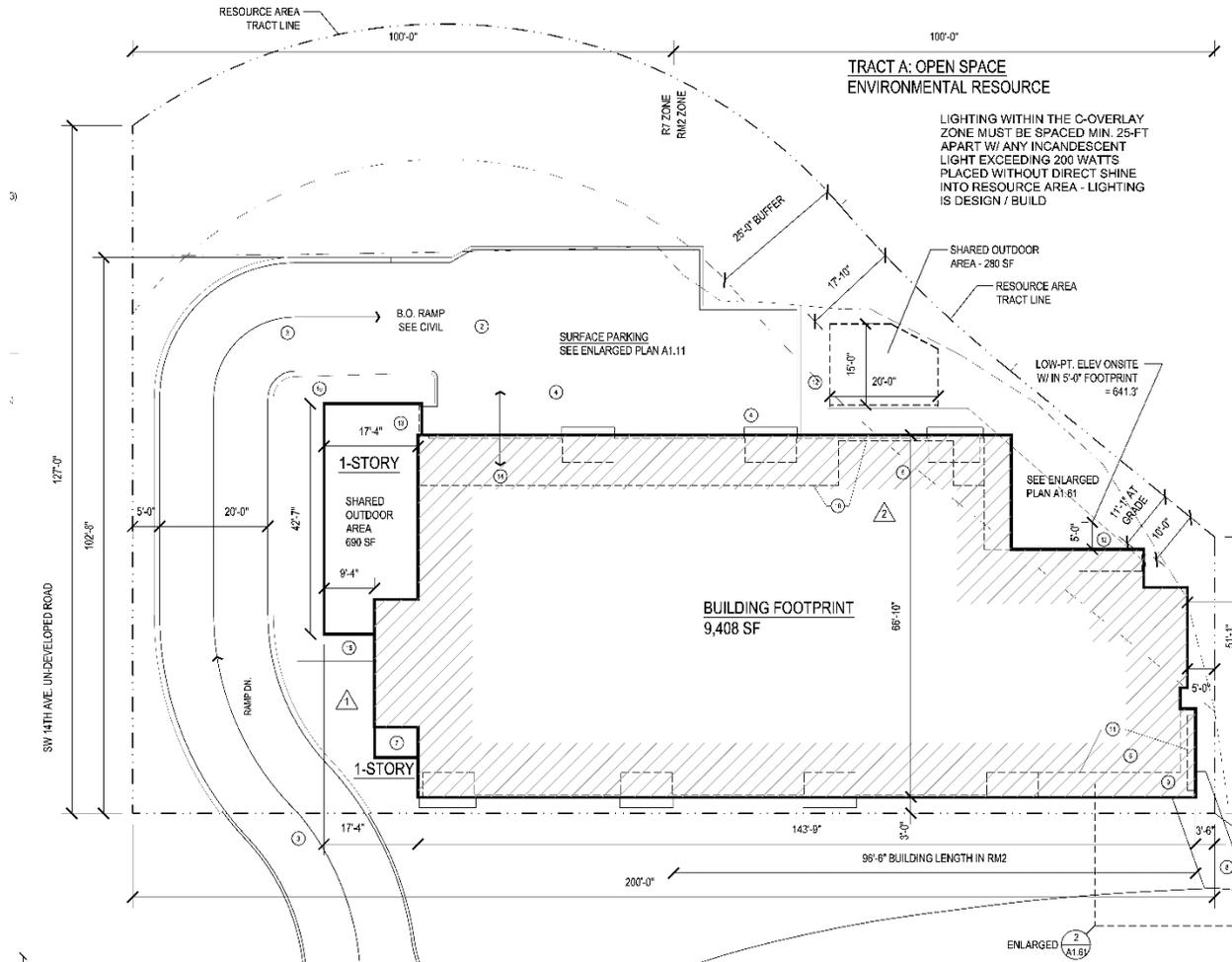


Figure 5. Proposed Site Plan, as shown on Sheet A1.00.

A parking lot for 28 vehicles will be located on the ground level, underneath the apartment building and accessed by a driveway that loops around to the rear of the structure. Four parking spaces will be provided outside of structure, as shown on **Sheet A1.00**. Two secured bicycle rooms will provide 48 long-term bicycle parking spaces. One is planned on the ground floor, adjacent to the vehicle parking area and lower lobby. The second is planned adjacent to the main lobby and main entrance onto SW Gibbs Street. Short-term bicycle parking will be located on the ground level near the lower lobby entrance.

Four site-related development modifications are requested to meet RM2 requirements that did not apply to the original design and building approval but are activated by the proposed interior floor plan redesign to increase the number of units.

- ❖ **Façade Articulation:** One requested modification is to comply with façade articulation standards in RM2. These standards did not previously apply to the development, as approved in the R1 zone. The façade remains unchanged from the previously approved design, but it is now required to meet articulation standards in RM2. This modification only applies to the portion of façade within RM2.
- ❖ **Outdoor Area Dimensions:** Three modifications requests are for dimensional and locational standards related to common outdoor open spaces. Additional outdoor areas are required to serve the new units. To meet these standards, a second outdoor open space area is planned just north of the building, east of the proposed parking area. Previously approved landscaping in this area has been shifted to accommodate the common space.

The building envelope and façade are not proposed to change. Site access and circulation are not proposed to change. And the protected environmental tract created in a 2019 land division will remain unchanged.

Neighborhood Outreach

The proposed modification to the approved PD will not increase building area. Because of this, City Planner Leah Dawkins has confirmed that the neighborhood contact requirements of PZC 33.705 are not applicable. However, Scotia Western States Housing, LLC chose to follow neighborhood contact III requirements to notice and inform neighborhood stakeholders.

The site is located in the Homestead neighborhood association district and within 400 feet of the Southwest Hills Residential League. A request for a neighborhood meeting was sent to the neighborhood association and other parties on July 8, 2022. A meeting was scheduled by the neighborhood on July 12, 2022, and one sign was posted on the improved ROW of SW Gibbs Street that same day. The online neighborhood meeting page was posted on July 12, 2022.

The meeting was held by the Homestead neighborhood association on August 2, 2022. Broadly, the following topics were discussed during the meeting after the project proposal was introduced: pedestrian connections along the entirety of SW Gibbs Street and the future sidewalk sections that are proposed to be completed by other developments; the market research to support small units in this neighborhood; if there would be an increase in the number of inclusionary housing units; how to prevent parking along SW Gibbs Street frontage; and frustrations about the redistribution of system development charges to other areas of Portland. Overall, neighborhood feedback on the proposed modifications was neutral or positive.

Neighborhood meeting notes and an attendance list were sent to all neighborhood stakeholders who initially received an invitation and those who provided their contact information during the meeting. The follow-up letter was sent on August 3, 2022.

Surrounding Land Uses

As shown in Figure 6, the immediate surrounding land use is primarily residential and includes a mix of residential densities (multi-dwelling structures and single-family dwellings). Limited neighborhood commercial amenities exist on SW Gibbs Street. To the east of the project site is an area zoned CM2. Further east is the OHSU campus. The site is adjacent to Marquam nature Park to the west, with a trailhead located near the site.

1311 SW Gibbs Street Vicinity Map



Figure 6. Vicinity Map

Compliance with Previous Land Use Decisions

The previous land use decisions for the site include:

- ❖ **CU 075-76:** Approval of a Conditional Use Review for a convalescent home. According to LUR 96-00236, this approval was never acted on and the permit expired.
- ❖ **CU 020-79:** Approval of a Conditional Use Review for a welfare institution with variances to minimum site area and minimum front yard requirements.
- ❖ **LUR 96-00236:** Approval of an expansion of a conditional use for a group living use in a residential zone at 1325 SW Gibbs, in order to create an enclosed connection between the two existing buildings and to add a vertical lift.
- ❖ **LU 19-258445:** Approval of a Planned Development, Land Division, and Environmental Modifications. The land division and final plat is complete, which included establishment of an environmental protection tract and mitigation plantings. Site preparation is ongoing; the current application retains approved access, utilities, and building facades – as well as approved environmental modifications – but seeks to approve a change to internal unit configuration resulting in additional units through an additional Planned Development application.

These land use decisions have been reviewed for compliance to ensure that all required conditions of approval have been met. Conditions of approval related to final plat for LU 19-258445 have been met. Conditions of approval related to building permits and occupancy have not been completed, due to this application's proposed change to unit configuration. Updated conditions of approval related to building permits and occupancy are expected through this application.

Narrative Organization

This application is organized into three parts:

- ❖ **Part 1** demonstrates compliance with the base zone standards, parking, loading & transportation standards;
- ❖ **Part 2** demonstrates compliance with planned development requirements, requested modifications, and the partition amendment approval criteria; and
- ❖ **Part 3** demonstrates compliance with environmental review and tree protection standards.

Throughout this document, relevant code criteria are shown in *italics*. Responses to code criteria and standards are shown in standard font.

Part 1: Base Zone and Development Standards

Parts 1.1 and 1.2 of this narrative demonstrate compliance with R7 and RM2 base zone standards as well as parking and landscaping development standards.

1.1 R7 Residential 7,000 (33.110)

33.110.100 Primary Uses

- A. *Allowed uses. Uses allowed in the single-dwelling zones are listed in Table 110-1 with a "Y". These uses are allowed if they comply with the development standards and other regulations of this Title. [...]*

Response: Table 110-1 lists household living as an allowed use in the R7 zone. Development standards and other regulations are reviewed as follows.

33.110.200 Housing Types Allowed

- A. *Purpose. [...]*
- B. *Housing Types. The kinds of housing types allowed in the single-dwelling zones are stated in Table 110-2.*

Response: Table 110-2 states, "multi-dwelling structures are allowed only in Planned Developments, See Chapter 33.270." This application is for one multi-dwelling structure proposed as a Planned Development. The requirements of Chapter 33.270 are addressed in Part 2 of this narrative.

33.110.212 When Primary Structures are Allowed

- A. *Purpose. [...]*
- B. *Adjustments. [...]*
- C. *Primary structures allowed. In all areas outside the West Portland Park Subdivision, primary structures are allowed as follows.*
 1. *On lots created on or after July 26, 1979;*
 2. *On lots created through the Planned Development or Planned Unit Development process;*
 3. *On lots, lots of record, lot remnants, or combinations thereof that have not abutted a lot, lot of record, or lot remnant under the same ownership July 26, 1979 or any time since that date.*
 4. *On lots, lots of record, lot remnants, or combinations thereof created before July 26, 1979 that meet the requirements of Table 110-6*
 5. *Primary structures are allowed on lots, lots of record, lot remnants, and combinations thereof that did meet the requirements of Table 110-6 in the past but were reduced below those requirements solely because of condemnation or required dedication by a public agency for right-of-way.*
 6. *On lots, lots of record, lot remnants, and combinations thereof zoned R20 that met the requirements of Table 110-6 in the past but no longer meet the requirements solely due to a zone change effective on May 24, 2018.*

Response: The subject property in its current configuration was approved through a PD review and a land division in 2020 (see LU 19-258445). Primary structures are allowed on the southern portion of the property. The northern portion of the property is protected within an environmental resource tract. No development is proposed on this protected tract.

- D. *Regulations for West Portland Park. [...]*
- E. *Plots. [...]*
- F. *Nonconforming situations. [...]*

Response: The subject property is not located in West Portland Park, is not a Plot, nor is it a nonconforming situation. These criteria do not apply.

33.110.213 Additional Development Standards for Lots and Lots of Record Created before July 26, 1979

- A. *Purpose. [...]*
- B. *Where these Regulations apply.*
 1. *RF through R7 zones. These regulations apply in the RF through R7 zones, if the lot, lot of record, or combination of lots or lots of record is less than 36 feet wide and has not abutted any lot or lot of record owned by the same family or business on July 26, 1979, or any time since that date. [...]*

Response: The subject site was created in its current configuration through a land division approved in 2020. Therefore, these regulations do not apply.

33.110.215 Height

- A. *Purpose. [...]*
- B. *Maximum Height.*
 1. *Generally. The maximum height allowed for all structures is stated in Table 110-3. The maximum height standard for institutional uses is stated in 33.110.245, Institutional Development Standards. The maximum height standards for detached accessory structures are stated in 33.110.250, Detached Accessory Structures.*
 2. *Exceptions. [...]*

Response: Table 110-3 lists the maximum height in the R7 zone as 30-feet. The proposed multi-dwelling structure will extend approximately 64-feet into the R7 zone and have a maximum height of 45 feet. A modification to the maximum height standard was requested and approved through an Environmental Review modification in LU 19-258445 PD LDP ENM.

- C. *Exceptions to the maximum height [...]*
- D. *Alternative height limits for steeply sloping lots*
 1. *Downhill slope from street. On lots that slope downhill from the street with an average slope of 20 percent or greater, the height limit is the higher of either 23 feet above the average grade of the street, or the normal height limit calculated as*

- stated in Chapter 33.930, Measurements. In addition, the alternative height and setback standards of Subsection 33.110.220.D apply.*
2. *Uphill slope from the street. On lots that slope uphill from the street with an average slope of 20 percent or greater the alternative height and setback standards of Subsection 33.110.220.D apply.*

Response: No exceptions or alternative height limits are proposed in this application. A modification to the normal height limit was requested and approved through an Environmental Review modification LU 19-258445 PD LDP ENM.

33.110.220 Setbacks

- A. *Purpose. [...]*
- B. *Required Setbacks. The required setbacks for buildings and garage entrances are stated in Table 110-3. The walls of the garage structure are subject to the front, side, and rear building setbacks stated in Table 110-3. The minimum setbacks for institutional uses are stated in 33.110.245. Other setbacks may apply to specific types of development or situations.*
- C. *Extensions into required building setbacks. [...]*
- D. *Exceptions to the required setbacks. [...]*

Response: The minimum front setback in the R7 zone is 15-feet, however due to the presence of an Environmental Conservation Overlay on the site, this setback is reduced to zero feet and the minimum front setback becomes the maximum setback. Thus, a 15-foot maximum setback applies in the R7 zone. The building is proposed to be setback 3-feet from the front property line as shown on the **Site Plan on Sheet A1.00**.

33.110.225 Building Coverage

- A. *Purpose. [...]*
- B. *Building coverage standards. The maximum combined building coverage allowed on a site for all covered structures is stated in Table 110-4.*

Response: The **Site Plan on Sheet A1.00** depicts a total of 4,084 square feet of the building footprint located in the R7 zone. The building coverage allowed for lots 20,000 sq. ft. or more per Table 110-4 is, 4,500 sq. ft. + 7.5% of the lot area over 20,000 sq. ft. The total lot area is 22,893 square feet, thus allowing for 4,717 square feet of building coverage in the R7 zone based on the calculation listed in Table 110-4. The total building coverage proposed on the R7 portion of the site is 4,084 square feet, which does not exceed the maximum building coverage, 4,717 square feet, allowed in the R7 zone.

33.110.227 Trees

Requirements for street trees and for on-site tree preservation, protection, and overall tree density are specified in Title 11, Trees. See Chapter 11.50, Trees in Development Situations.

Response: Tree preservation and protection standards of Chapter 11.50 are addressed in Part 3 of this narrative.

33.110.230 Main Entrances in R10 through R2.5 Zones

- A. Purpose. [...]
- B. Where these standards apply.
 1. The standards of Subsection C apply to houses, attached houses, manufactured homes, and duplexes in the R10 through R2.5 zones;
 2. The standard of Subsection D applies to attached houses on new narrow lots.
 3. Where a proposal is for an alteration or addition to existing development, the standards of this section apply only to the portion being altered or added;
 4. On sites with frontage on both a private street and a public street, the standards apply to the site frontage on the public street. On all other sites with more than one street frontage, the applicant may choose on which frontage to meet the standards.
 5. Development on flag lots or on lots that slope up or down from the street with an average slope of 20 percent or more is exempt from these standards; and
 6. Subdivisions and PUDs that received preliminary plan approval between September 9, 1990, and September 9, 1995, are exempt from these standards. [...]

Response: This proposal is for one multi-dwelling structure and no houses, attached houses, manufactured homes, or duplexes are proposed. A portion of the multi-dwelling structure will extend into the R7 zone; however, no separate individual dwelling units are proposed on the R7 portion of this property; therefore, this section does not apply.

33.110.232 Street Facing Facades in R10 through R2.5

- A. Purpose. [...]
- B. Where this standard applies. [...]
- C. The standard. At least 15 percent of the area of each facade that faces a street lot line must be windows or main entrance doors. Windows used to meet this standard must allow views from the building to the street. Glass block does not meet this standard. Windows in garage doors do not count toward meeting this standard, but windows in garage walls do count toward meeting this standard. To count toward meeting this standard, a door must be at the main entrance and facing a street lot line.

Response: No individual dwelling units are proposed on the R7 portion of this property; therefore, this section does not apply. Nonetheless, 39 percent of the street facing facades in the R7 zone alone will be windows.

33.110.235 Required Outdoor Areas

- A. Purpose. The required outdoor areas standards assure opportunities in the single-dwelling zones for outdoor relaxation or recreation. The standards work with the maximum building coverage standards to ensure that some of the land not covered by buildings is of an adequate size and shape to be usable for outdoor recreation or relaxation. The location requirements provide options for private or semiprivate areas. The requirement of a required outdoor area serves in lieu of a large rear setback requirement and is an important aspect in addressing the livability of a residential structure.

Response: No individual residential structures or units are proposed on the portion of the property in the R7 zone. Only a portion of the multi-dwelling structure will extend into the R7 zone. Outdoor areas are addressed in the PD section.

33.110.240 Alternative Development Options

[...]

G. *Planned development. See Chapter 33.270, Planned Developments.*

[...]

Response: This application is for a planned development. Chapter 33.270 is addressed in Part 2 of this narrative.

33.110.245 Institutional Development Standards

A. *Purpose. The general base zone development standards are designed for residential buildings. Different development standards are needed for institutional uses which may be allowed in single-dwelling zones. The intent is to maintain compatibility with and limit the negative impacts on surrounding residential areas. [...]*

Response: This development proposal does not include any institutional uses. The criteria in this section does not apply.

33.110.250 Detached Accessory Structures

A. *Purpose. This section regulates detached structures that are incidental to primary buildings to prevent them from becoming the predominant element of the site. The standards limit the height and bulk of the structures, promote compatibility of design for larger structures, provide for necessary access around larger structures, help maintain privacy to abutting lots, and maintain open front setbacks.*

B. *General Standards. [...]*

Response: No detached accessory structures are proposed in this application. The standards of this section do not apply.

33.110.255 Fences

A. *Purpose. [...]*

B. *Types of fences. The standards apply to walls, fences, and screens of all types whether open, solid, wood, metal, wire, masonry, or other material.*

C. *Location and height.*

1. *Front building setbacks. Fences up to 3-1/2 feet high are allowed in required front building setbacks.*

2. *Side and rear building setbacks.*

a. *Fences up to 8 feet high are allowed in required side or rear building setbacks that do not abut a pedestrian connection.*

b. *Fences abutting a pedestrian connection.*

1. *Fences up to 8 feet high are allowed in required side or rear building setbacks that abut a pedestrian connection if the pedestrian connection is part of a right-of-way that is at least 30 feet wide.*

2. *Fences up to 3-1/2 feet high are allowed in required side or rear building setbacks that abut a pedestrian connection if the pedestrian connection is part of a right-of-way that is less than 30 feet wide.*
3. *Exceptions for corner lots. On corner lots, if the main entrance is on the facade facing the side street lot line, the applicant may elect to meet the following instead of C.1 and C.2. See Figure 110-15.*
 - a. *Fences up to 3-1/2 feet high are allowed within the first 10 feet of the side street lot line.*
 - b. *Fences up to 3-1/2 feet high are allowed in required setbacks that abut a pedestrian connection if the pedestrian connection is part of a right-of-way that is less than 30 feet wide;*
 - c. *Fences up to 8 feet high are allowed in the required front building setback, outside of the area subject to 3a.*
 - d. *Fences up to 8 feet high are allowed in all other side or rear building setbacks.*
4. *Not in building setbacks. The height for fences that are not in required building setbacks is the same as the regular height limits of the zone.*

Response: No fencing is proposed in this application.

33.110.257 Retaining Walls

A. *Purpose. [...]*

B. *Where these regulations apply.*

1. *Generally. These regulations apply to the portions of street-facing retaining walls that are in required setbacks along street lot lines. Where there is no required setback, or the setback is less than 10 feet, the regulations apply to the first 10 feet from the line.*
2. *Exceptions. The following are not subject to the regulations of this section:*
 - a. *Retaining walls in the areas described in B.1 that are less than four feet high, as measured from the bottom of the footing.*
 - b. *Retaining walls on sites where the site slopes downward from a street in the area described in B.1.*
 - c. *Retaining walls on sites where the site slopes upward from a street and the existing slope within the area regulated by B.1 is 50 percent or more.*
 - d. *Replacing an existing retaining wall, where the replacement will not be taller or wider than the existing wall.*

C. *Standards.*

1. *Retaining walls are limited to 4 feet in height measured from the bottom of the footing, as shown in Figure 110-16.*
2. *Retaining walls must be set back at least 3 feet from other street-facing retaining walls, as shown in Figure 110-16. The 3 foot setback area must be landscaped to at least the L2 standard, except that trees are not required. A wall or berm may not be substituted for the shrubs*

Response: As shown on the **Grading Plan on Sheet C6** a retaining wall is proposed in the public ROW along SW Gibbs Street to maintain the existing street grade. A retaining wall will also be constructed along either side of the driveway ramp. The walls qualify for an exception under 33.110.257.B.2.b since the site slopes downward from the street and the wall will be below the street grade and will not create an unattractive “fortress-like appearance” from the street level.

33.110.260 Demolitions

- A. *Generally. Demolition on a site that requires a demolition permit is subject to the tree preservation and protection requirements of Title 11, Trees. See Chapter 11.50, Trees in Development Situations.*
- B. *Historic resources. Demolition of historic resources is regulated by Chapter 33.445, Historic Resource Overlay Zone.*

Response: All existing structures on the site have been demolished and all appropriate permits have been obtained. Tree preservation standards are addressed in this narrative in Part 3. There is no historic resource overlay zone on the subject property, nor are there any historic structures.

33.110.270 Nonconforming Development

Existing developments that do not conform to the development standards of this chapter may be subject to the regulations of Chapter 33.258, Nonconforming Situations.

Response: The proposed development will be all new construction in accordance with applicable code standards. No existing nonconforming situations will remain on the property.

33.110.275 Parking and Loading

- A. *Access to parking. Vehicle access to a lot must be from an alley under the following conditions. Modifications to this standard are allowed through Planned Development Review. See Chapter 33.638, Planned Development. Adjustments are prohibited.*
 - 1. *The lot abuts an alley;*
 - 2. *The lot was created by a land division submitted after July 1, 2002; and*
 - 3. *The lot is either:*
 - a. *In the R10 through R5 zones and does not meet the minimum lot width standard of 33.610.200.D.1; or*
 - b. *In the R2.5 zone and does not meet the minimum lot width standard of 33.611.200.C.1.*
- B. *Parking and loading. For all other parking and loading regulations, see Chapter 33.266, Parking and Loading.*

Response: The lots comprising the site do not abut an alley; therefore, this standard is not applicable. Parking and loading regulations of Chapter 33.266 are reviewed in Part 1.3 of this narrative.

33.110.280 Signs

The sign regulations are stated in Title 32, Signs and Related Regulations.

Response: No signs are proposed as part of this application.

R7 Summary

The R7 portion of the site will contain a portion of the multi-dwelling structure and the driveway onto the site that wraps around to the entrance of the ground level parking area underneath the structure accessed from the rear of the building. A modification to the maximum height of the R7 zone was approved through an Environmental Review modification (LU 19-258445 PD LDP ENM).

1.2 RM2 Residential Multi-Dwelling 2 (33.120)

33.120.050 Neighborhood Contact

Neighborhood contact is a set of outreach steps that must be taken before certain developments can be submitted for approval. Neighborhood contact is required as follows:

- A. *Neighborhood contact I. [...]*
- B. *Neighborhood contact II.*
 - 1. *Neighborhood contact II requirements. When the proposed development will add more than 25,000 square feet of net building area to a site, the neighborhood contact steps of 33.705.020.B., Neighborhood contact II, are required. All of the steps in 33.705.020.B. must be completed before an application for a building permit can be submitted.*

Response: The proposed modification to the approved PD will not increase building area. Because of this, City Planner Leah Dawkins has confirmed that the neighborhood contact requirements of PZC 33.705 are not applicable. However, Scotia Western States Housing, LLC chose to follow neighborhood contact III requirements to notice and inform neighborhood stakeholders.

Please see the neighborhood contact section in the beginning of this narrative.

33.120.100 Primary Uses

- A. *Allowed Uses. Uses allowed in the multi-dwelling zones are listed in Table 120-1 with a "Y". These uses are allowed if they comply with the development standards and other regulations of this Title. Being listed as an allowed use does not mean that a proposed use will be granted an adjustment or other exception to the regulations of this Title. In addition, a use or development listed in the 200s series of chapters is also subject to the regulations of those chapters.*
- B. *Limited Uses. [...]*
- C. *Conditional Uses. [...]*
- D. *Prohibited Uses. [...]*

Response: This proposal is for a multi-dwelling structure which is listed as an allowed use in the RM2 zone. No limited or conditional uses are proposed in this application.

33.120.200 Housing Types Allowed

- A. *Purpose. [...]*
- B. *Housing types. The types of housing allowed in the multi-dwelling zones are stated in Table 120-2.*

Response: This proposal is for a multi-dwelling structure which is listed as an allowed housing type in the RM2 zone. No limited or conditional uses are proposed in this application.

33.120.205 Development of Lots and Lots of Record

- A. *Purpose. [...]*
- B. *Where these regulations apply. These regulations apply to existing lots and lots of record in the multi-dwelling zones. The creation of new lots is subject to the lot size standards listed in Chapter 33.612, Lots in Multi-Dwelling Zones.*
- C. *Ownership of multiple lots and lots of record. Where more than one abutting lot or lot of record is in the same ownership, the ownership may be separated as follows:[...].*
- D. *New development on standard lots and lots of record. New development on lots and lots of record that comply with the lot dimension standards in Chapter 33.612, Lots in Multi-Dwelling Zones, is allowed by right subject to the development standards.*
- E. *New development on substandard lots and lots of record. New development is allowed on lots and lots of record which do not conform to the lot dimension standards in Chapter 33.612, Lots in Multi-Dwelling Zones, if both of the following are met:*
 - 1. *The development is proposed for a lot or lot of record. Development on plots that are not lots or lots of record is prohibited; and*
 - 2. *The lot or lot of record did not abut any property owned by the same family or business on July 26, 1979, or any time since that date, unless the ownership was separated as allowed in Subsection C, above.*

Response: The subject site was created in its current configuration through a land division approved in 2020. Four environmental review modifications were requested and approved. Two of which relate to the approved lot configuration: (1) to allow a split zoned lot, and (2) to increase the maximum lot size in the R7 zone.

The approved lots adhere to the lot dimension standards of Table 612-1.

33.120.206 Minimum Required Site Frontage for Development

- A. *Purpose. [...]*
- B. *Where these regulations apply. The minimum required site frontage standard applies in the multi-dwelling zones to sites shown on Map 120-2.*

Response: The site is not within an area shown on Map 120-2.

33.120.210 Floor Area Ratio

- A. *Purpose. [...]*
- B. *FAR standard. The maximum floor area ratios are stated in Table 120-3 and apply to all uses and development [...]*

Response: The maximum FAR in the RM2 zone is 1.5 to 1. 20,000 sq. ft. of the site is zoned RM2. The maximum FAR allowed for this site is 30,000 sq. ft. The proposed building will have a total floor area of 43,400, with 27,228 floor area within the RM2 portion of the site.

33.120.211 Floor Area Bonus Options

Response: No floor area bonus options are requested.

33.120.212 Maximum Density

- A. *Purpose. [...]*
- B. *Maximum density. The maximum density for the RMP zone is stated in Table 120-3. There is no maximum density for any other multi-dwelling zone. [...]*

Response: There is no maximum density in RM2.

33.120.213 Minimum Density

- A. *Purpose. [...]*
- B. *Minimum density. The minimum density requirements for the multi-dwelling zones are stated in Table 120-3. Group living uses are exempt from minimum density requirements. Land within an Environmental zone may be subtracted from the calculation of minimum density. [...]*

Response: The minimum density in RM2 is 1 unit per 1,450 sq. ft. of site area. With 20,000 sq. ft. of site area in RM2, the minimum density for this portion of the site is 14. There are 15 units fully within the RM2. The proposed PD modifications meet this standard.

33.120.215 Height

- A. *Purpose. [...]*
- B. *Height standard.*

Response: The base height in RM2 is 45 ft. The proposed building will have a maximum height of 45 feet with an additional 2 feet and 8.5 inches of height for elevator mechanical equipment (exempted through 33.120.215.C.4.a).

33.120.220 Setbacks

- A. *Purpose. [...]*
- B. *Minimum building setbacks. The required minimum building setbacks apply to all buildings and structures on the site except as specified in this section. Setbacks for parking areas are in Chapter 33.266.*

1. *Generally. The required minimum building setbacks, if any, are stated in Table 120-3. In the RM3 and RM4 zones, the minimum side and rear building setbacks apply as follows: [...]*
2. *Eastern Pattern Area minimum rear building setback. [...]*
3. *Exceptions to the required building setbacks.*
 - a. *Setback matching. [...]*
 - b. *Raised ground floor. [...]*
 - c. *Courtyard. Except in the Eastern Pattern Area shown on Map 120-3, the required minimum front or side street setback may be reduced to zero in the RM2 and RM3 zones, and may be reduced to 5 feet in the RM1 zone when: [...]*
 - d. *Ground floor commercial. [...]*
 - e. *Environmental zone. The required minimum front and street building setback and garage entrance setback may be reduced to zero where any portion of the site is in an environmental overlay zone. Where a side lot line is also a street lot line the side building and garage entrance setback may be reduced to zero. All other provisions of this Title apply to the building and garage entrance.*
 - f. *No setbacks are required from an internal lot line that is also a zoning line on sites with split zoning.*

Response: The proposed building will be located in the southeast corner of the property, at the intersection of SW Gibbs Street and an unimproved section of SW 13th Avenue. Due to the presence of an environmental zone, the minimum front and street building setback is reduced to zero. The side lot lines are also street lot lines, and therefore may be reduced to zero as per criterion B.2.b of this subsection. The presence of the environmental zone also requires a maximum setback of 10 feet in the RM2 zone. The building will be setback 3 feet from the front lot line in RM2.

The site is split zoned, however there is no internal lot line that is also a zoning line.

The building will be located 3 feet from the front lot line.

C. Maximum building setbacks.

1. *Building setbacks on a transit street or in a Pedestrian District. [...]*

Response: The site is bound by rights-of-way on four sides with only one improved street, SW Gibbs Street, where all access to the site is taken from. SW Gibbs Street is a local service transit street and the site is not located in a pedestrian district, as identified on the City TSP. The building will be located 3 feet from the front lot line, meeting the new maximum setback of 10 feet – per the presence of an environmental zone.

D. Extensions into required building setbacks. [...]

Response: The minimum setback is 0. No extensions are proposed.

E. *Garage entrance and structure parking setback. [...]*

Response: The garage entrance will be setback nearly 70 feet from the front property line, at the rear of the building.

33.120.225 Building Coverage

A. *Purpose. [...]*

B. *Maximum building coverage. The maximum building coverages for all covered structures on the site are stated in Table 120-3. In the RM2 zone, maximum building coverage on sites that abut a Civic Corridor or Neighborhood Corridor shown on Map 120-1 is 70 percent of site area. Maximum building coverage on all other RM2 sites is 60 percent of site area.*

Response: The maximum building coverage is 60% of the site area. The site area is 40,000 sq ft., with 20,000 sq. ft. in RM2. A maximum building coverage of 12,000 sq ft. is allowed. The proposed building coverage is 9,516 sq. ft. total, 5,324 square feet on the RM2 portion of the site.

33.120.230 Building Length

A. *Purpose. [...]*

B. *Maximum building length. In the RM1, RM2, and RMP zones, the maximum building length for the portion of buildings located within 30 feet of a street lot line is 100 feet. The portions of buildings subject to this standard must be separated by a minimum of 10 feet. See Figure 120-8. Manufactured dwelling parks are exempt from this standard.*

Response: The total building length in the RM2 portion of the site is 96-feet 6-inches.

C. *Facade articulation*

1. *Where the standard applies. This standard applies in the RM2 through RM4 zones as follows:*

a. *In the RM2 zone, the standard applies to buildings more than 35 feet high that have facade areas of more than 3,500 square feet within 20 feet of a street property line.*

b. *In RM3 and RM4 [...]*

c. *Portions of building facades that are vertically separated by a gap of at least 10 feet in width extending at least 30 feet in depth from the street property line are considered to be separate facade areas for the purposes of the facade area measurements. See Figure 120-8.*

2. *The standard. At least 25 percent of the area of a street-facing facade within 20 feet of a street lot line must be divided into facade planes that are off-set by at least 2 feet in depth from the rest of the facade. Facade area used to meet the facade articulation standard may be recessed behind, or project out from, the primary facade plane, but projections into street right-of-way do not count toward meeting this standard. See Figure 120-9.*

Response: This standard was not in effect when the building was designed and previously approved. The building will have a front façade area of 4,556 sq. ft. in RM2. 22% of the total façade area is recessed from the primary façade plane. A **modification** to this standard is requested.

33.120.231 Main Entrances

A. Purpose. [...]

B. Where these standards apply.

1. *The standards of this section apply to all residential structure types in the multi-dwelling zones except for accessory dwelling units, manufactured dwelling parks, and houseboat moorages. For multi-dwelling development, the standards apply only to residential structures that are located within 40 feet of a street lot line.*

2. [...]

3. [...]

C. Main entrance.

1. *Standard. At least one main entrance for each structure must:*

a. *Be within 8 feet of the longest street-facing wall of the structure; and*

b. *Either:*

(1) Face the street. See Figure 120-10; [...]

Response: These standards apply. A main building entrance will face SW Gibbs Street on the second floor of the structure – at grade with the existing ROW. This main entrance will be setback 7.5 feet from the front lot line, 4.5 feet from the longest street-facing facades.

33.120.232 Street-Facing Facades

A. Purpose. [...]

B. Where these standards apply.

1. *The standards of this section apply to the street-facing facades of all residential structure types except for accessory dwelling units, manufactured dwelling parks, and houseboat moorages. The standards of this section also do not apply in the RMP zone.*

2. *Where a proposal is for an alteration or addition to existing development, [...]*

3. *Development on flag lots [...]*

4. *Subdivisions and PUDs that received preliminary plan approval between September 9, 1990, and September 9, 1995, are exempt from Subsection C.*

5. *For structures subject to ground floor window standard in Subsection D, windows used to meet the ground floor window standard may also be used to meet the requirements of Subsection C.*

C. *Windows. At least 15 percent of the area of each facade that faces a street lot line must be windows or main entrance doors. Windows used to meet this standard must allow views from the building to the street. Glass block does not meet this standard. Windows in garage doors do not count toward meeting this standard, but windows in garage walls do count toward meeting this standard. To count toward meeting this standard*

a door must be at the main entrance and facing the street property line. Development on flag lots or on lots which slope up or down from the street with an average slope of 20 percent or more are exempt from these standards.

- D. Ground floor windows. The following ground floor window standards apply to the portion of a building with ground floor commercial uses*

Response: As shown on the **South Elevation on Sheet A1.02**, the façade area that faces the street in RM2 is 4,907 square feet with 1,040 square feet or 22 percent of windows and main entrance doors.

33.120.235 Landscaped Areas

A. Purpose. [...]

B. Minimum landscaped areas. The required amount of landscaped area is stated in Table 120-3. Sites developed with a house, attached house, duplex, or manufactured dwelling park are exempt from this standard. Any required landscaping, such as for required setbacks or parking lots, applies toward the minimum required landscaped area.

- 1. Except as allowed by Paragraph B.2., required landscaped areas must:*
 - a. Be at ground level or in raised planters that are used to meet minimum Bureau of Environmental Services stormwater management requirements; and*
 - b. Comply with at least the L1 standard described in Chapter 33.248, Landscaping and Screening. However, up to one-third of the required landscaped area may be improved for active or passive recreational use or for use by pedestrians. Examples of active or passive recreational use include walkways, play areas, plazas, picnic areas, garden plots, and unenclosed recreational facilities.*
- 2. Urban green alternative landscaped area. One or more of the following may be used to meet up to 50 percent of the required landscaped area:*
 - a. Ecoroof. An ecoroof area may apply toward meeting the required landscaped area standard at a ratio of 4 square feet of ecoroof area for every 1 square foot of required landscaped area. The ecoroof area must be approved by the Bureau of Environmental Services as being in compliance with the Stormwater Management Manual.*
 - b. Raised landscaped areas. Landscaped area raised above ground level may apply toward meeting the minimum landscaped area standard when landscaped to at least the L1 standard and soil depth is a minimum of 30 inches. Large trees are not allowed in raised landscaped area used to meet this alternative.*

C. Additional landscaping standards

- 1. Building setbacks. The required building setbacks must be landscaped to at least the L1 standard of Chapter 33.248, Landscaping and Screening. Ground-level pedestrian pathways, detached accessory structures and other*

development allowed in the setbacks are exempt from this standard except in the Eastern Pattern Area where allowed development can cover no more than 50 percent of the Eastern Pattern Area minimum rear setback area. Sites that are 10,000 square feet or less in total site area are also exempt from this standard.

2. *Parking areas. Perimeter and internal parking area landscaping standards are stated in Chapter 33.266, Parking And Loading.*

Response: According to Table 120-3, the minimum landscape area in the RM2 zone is 20 percent of the site area. About 43% of the site is proposed to be preserved in an environmental tract; additionally, extensive landscaping is proposed as shown on the **Landscape Plan on Sheet L4**. Proposed landscaped areas comply with the L1 standard. All required setbacks are planned to be landscaped.

33.120.237 Trees

Requirements for street trees and for on-site tree preservation, protection, and overall tree density are specified in Title 11, Trees. See Chapter 11.50, Trees in Development Situations.

Response: Tree preservation standards are reviewed in the Section 3.

33.120.240 Required Outdoor and Common Areas

A. *Purpose. [...]*

B. *Requirements. Outdoor area and common area requirements. In the RM1 through RM4 zones, on sites with a residential use, both outdoor and common areas are required. Required common area may count toward required outdoor area, but individual private outdoor area may not count toward required common area. The standards of this section do not apply in the RX and RMP zones.*

1. *Required outdoor area. Outdoor area is required in the amounts stated below. Outdoor area may be provided as individual private outdoor area, such as a patio or balcony, or may be provided as common area, such as outdoor courtyards, outdoor play area, indoor recreational facilities, or indoor community rooms. There may be a combination of individual or common areas.*
 - a. *RM1 and RM2 zones. In the RM1 and RM2 zones, at least 48 square feet of outdoor area is required per dwelling unit.*
2. *Required common area.*
 - a. *Required common area standard. On sites that are more than 20,000 square feet in total site area, [...]*

Response: No more than 20,000 sq. ft. of the site is zoned RM2. Required common areas are not required. However, common areas are proposed to meet outdoor area standards for all dwelling units.

With 43 units, 2,064 square feet of outdoor area is required. 27 units will have personal balconies, totaling 1,296 square feet. The 16 additional units will require 768 square feet of

common outdoor area. We are proposing 970 square feet of common outdoor areas. A total of 2,266 square feet of outdoor areas are proposed.

C. Size, location and configuration.

1. *Individual unit outdoor area. Where a separate outdoor area is provided for an individual unit, it must be designed so that a 4-foot x 6-foot square will fit entirely within it. The outdoor area must be directly accessible to the unit. Areas used for pedestrian circulation to more than one dwelling unit do not count towards meeting this standard of this subsection. If the area is at ground level, it may extend into the entire required side and rear setback. Individual unit outdoor areas located at ground level may also extend into the entire required street setback, but when located within a required street setback the outdoor area must either be at least 2 feet above the grade of the closest adjoining sidewalk or separated from the street lot line by a minimum 3 foot setback landscaped to at least the L2 standard described in Chapter 33.248, Landscaping and Screening. Covered outdoor areas are subject to Paragraph C.5 below.*

Response: 27 units will have personal balconies, separately accessed from each unit, measuring 6 ft by 8 ft. The required front/street setback is zero. Therefore, no personal balconies will be within a required street setback.

2. *Common areas.*

- a. *Outdoor common area. Where an outdoor, shared common area is provided, it must be designed so that it is at least 500 square feet in area and must measure at least 20 feet in all directions. The outdoor common area must be located within 20 feet of a building entrance providing access to residential units.*

Response: This standard was not in effect when the building was designed and approved. Two common areas are proposed:

- Upper common deck: The 690 square foot deck is proposed on the western side of the building, outside of any required setbacks. The deck will be accessed through an entrance on the second floor, and it will be available to all residents. The common deck will measure approximately 17 feet by 42 feet. A **modification** to the 20 foot by 20 foot sizing standard is requested.
- Lower common lawn: The 280 square foot lawn is proposed near the northwest corner of the building, adjacent to the pedestrian pathway connecting the surface parking area to the lower lobby entrance. The common lawn will measure roughly 20 feet by 15 feet, and it will be located more than 20 feet from the lobby entrance (45 feet). **Modifications** to dimensional and locational standards are requested.

3. *Surfacing materials. Required outdoor areas must be surfaced with lawn, pavers, decking, or sport court paving which allows the area to be used for active or passive recreational use.*
4. *User amenities. User amenities, such as tables, benches, trees, shrubs, planter boxes, garden plots, drinking fountains, spas, or pools, may be placed in the outdoor area. Common, shared outdoor areas may also be developed with amenities such as play areas, plazas, roof-top patios, picnic areas, and open recreational facilities.*
5. *Enclosure. Required outdoor areas may be covered, such as a covered patio, but they may not be fully enclosed. Covered outdoor areas are subject to the setback standards of this chapter.*

Response:

No common or individual outdoor areas will be within the required front, rear, or side setbacks.

- Previously approved individual decks: 27 units will have an individual outdoor area that is surfaced with decking. These individual outdoor areas will be partially covered but not fully enclosed.
- Previously approved upper common deck: The deck is not proposed to be enclosed or roofed. It will be surfaced in decking materials.
- Proposed lower common lawn: The common lawn will be grassed and contain some trees (see **Sheets L4 and L4.2**).

33.120.250 Screening

A. *Purpose. [...]*

B. *Garbage and recycling collection areas. All exterior garbage cans, garbage collection areas, and recycling collection areas must be screened from the street and any adjacent properties. Trash receptacles for pedestrian use are exempt. Screening must comply with at least the L3 or F2 standards of Chapter 33.248, Landscaping and Screening.*

Response: The garbage and recycling collection area is located inside the structure on the ground level, adjacent to the parking area This standard does not apply.

C. *Mechanical Equipment. Mechanical equipment located on the ground, such as heating or cooling equipment, pumps, or generators must be screened from the street and any abutting residential zones by walls, fences, or vegetation. Screening must comply with at least the L2 or F2 standards of Chapter 33.248, Landscaping and Screening, and be tall enough to screen the equipment. Mechanical equipment placed on roofs must be screened in one of the following ways, if the equipment is within 50 feet of an R zone:*

1. *A parapet along facades facing the R zone that is as tall as the tallest part of the equipment;*
2. *A screen around the equipment that is as tall as the tallest part of the equipment;*
or
3. *The equipment is set back from roof edges facing the R zone 3 feet for each foot of height of the equipment.*

Response: The mechanical equipment is located on roof. The condensers will be 3 feet tall and will be setback at least 9 feet from the roof edges. For details, see **Sheet A2.61**.

D. Other screening requirements. Outdoor seating associated with a Retail Sales And Service use must be screened from any abutting residential zones by walls, fences or vegetation. Screening must comply with at least the L3 or F2 standards of Chapter 33.248, Landscaping and Screening. The screening requirements for parking, exterior storage, and exterior display areas are stated with the regulations for those types of development.

Response: There are no exterior storage or display areas proposed. The parking area is located on the ground level beneath the structure and accessed from a driveway looping to the rear of the building. From this location, it is screened by the building itself.

33.120.255 Pedestrian Standards

A. Purpose. [...]

B. The standards. The standards of this section apply to all development except houses, attached houses, manufactured homes on individual lots, and duplexes. The standards of this section do not apply to manufactured dwelling parks. An on-site pedestrian circulation system must be provided. The system must meet all standards of this subsection.

1. Connections. The on-site pedestrian circulation system must provide connections as specified below:

a. Connection between streets and entrances.

(1) Sites with one street frontage.

- Generally. There must be a connection between one main entrance of each building on the site and the adjacent street. The connection may not be more than 20 feet longer or 120 percent of the straight line distance, whichever is less.*
- Tree preservation. If a tree that is at least 12 inches in diameter is proposed for preservation, and the location of the tree or its root protection zone would prevent the standard of this paragraph from being met, the connection may be up to 200 percent of the straight line distance.*

(2) Sites with more than one street frontage. [...]

Response: The site has only one improved street frontage: SW Gibbs Street. The onsite pedestrian circulation system is shown on the **Site Plan on Sheet A1.00**. The building will have a main entrance on the second floor that is accessed directly from the street by a short pedestrian bridge. The straight-line connection from the street to the main entrance is roughly 22 feet. The pedestrian bridge will be 18 feet long. The front entrance will be nearer than 20 feet from the adjacent street.

b. Internal connections. On sites larger than 10,000 square feet, an internal pedestrian connection system must be provided. The system must connect all

main entrances on the site that are more than 20 feet from the street, and provide connections to other areas of the site, such as parking areas, bicycle parking, recreational areas, common outdoor areas, and any pedestrian amenities.

Response: As shown on the **Site Plan on Sheet A1.00**, the site is larger than 10,000 square feet. The site topography requires the main entrance to be located on the second floor to provide direct access to the street. Access to parking areas, long-term and short-term bicycle storage areas, and the common lawn on the ground floor can be accessed by the lower lobby entrance or via the elevator and stairs connecting the main and lower lobbies. The common deck is accessed through an entrance on the second floor.

2. *Materials.*

a. *The circulation system required by the standards of this section must be hard-surfaced and must meet the following minimum width requirements:*

(1) [...]

(2) [...]

(3) The circulation system on sites with more than 20 residential units must be at least 5 feet wide.

b. *Except as allowed in subparagraph d, below, where the system crosses driveways, parking areas, and loading areas, the system must be clearly identifiable, through the use of elevation changes, speed bumps, a different paving material, or other similar method. Striping does not meet this requirement. Elevation changes and speed bumps must be at least 4 inches high.*

c. *Except as allowed in subparagraph d, below, where the system is parallel and adjacent to an auto travel lane, the system must be a raised path or be separated from the auto travel lane by a raised curb, bollards, landscaping or other physical barrier. If a raised path is used it must be at least 4 inches high and the ends of the raised portions must be equipped with curb ramps. Bollard spacing must be no further apart than 5 feet on center.*

d. *The pedestrian circulation system may be within an auto travel lane if the auto travel lane provides access to 16 or fewer parking spaces and the entire auto travel lane is surfaced with paving blocks or bricks.*

Response: The pedestrian walkways are concrete pavement and at least 5-foot wide. The pedestrian walkway crosses the parking area drive aisle to access the garbage and recycling area and is identified by a crosswalk consisting of a contrasting material, as shown on **Sheet A1.61**.

3. *Lighting. The on-site pedestrian circulation system must be lighted to a level where the system can be used at night by the employees, residents, and customers.*

Response: All pedestrian circulation areas will be lit to allow nighttime use.

33.120.260 Recycling areas

Requirements for recycling areas are regulated by the Bureau of Planning and Sustainability. See Section 17.102.270, Businesses and Multifamily Complexes Required to Recycle, of the Portland City Code.

Response: Section 17.102.270 requires that “All multifamily complexes within the City shall establish recycling systems for their tenants’ use, in compliance with administrative rules established by the Director.” A recycling and trash enclosure area is included in the proposed development, as shown on the **First Floor Plan on Sheet A2.12**.

33.120.270 Alternative Development Options

A. *Purpose. [...]*

B. *General requirements for all alternative development options. The alternative development options listed in this section are allowed by right unless it is specifically stated otherwise. They must conform with all other development standards of the base zone unless those standards are superseded by the ones in this section. Sites in the RMP zone are not eligible for alternative development option.*

[...]

H. *Planned Development. See Chapter 33.638, Planned Development.*

Response: This proposal is for a multi-dwelling development constructed as a PD. Planned Development criteria are addressed in Part 2.

33.120.285 Fences

Response: No fences are included in this proposal.

33.120.290 Demolitions

A. *Generally. Demolition on a site that requires a demolition permit is subject to the tree preservation and protection requirements of Title 11, Trees. See Chapter 11.50, Trees in Development Situations.*

B. *Historic resources. Demolition of historic resources is regulated by Chapter 33.445, Historic Resource Overlay Zone. [...]*

Response: Demolition was required for this project. A demolition permit was obtained (see 2020-174829-000-00-CO and 2020-174739-000-00-CO).

Sheet L5 shows tree preservations and root protection zones. Tree preservation is discussed in further detail Part 3 of this narrative. Chapter 33.445 does not apply.

33.120.305 Parking and Loading

The standards for the minimum required and maximum allowed number of auto parking spaces, required number of bike parking spaces, parking lot placement, parking lot setbacks and landscaping, loading areas and driveways are stated in Chapter 33.266, Parking And Loading.

Response: Chapter 33.266 is addressed in Section 1.5.

33.120.310 Signs

The sign regulations are stated in Title 32, Signs and Related Regulations.

Response: No signs are proposed.

33.120.320 Inclusionary Housing

The regulations pertaining to inclusionary housing are stated in Chapter 33.245, Inclusionary Housing.

Response: Inclusionary Housing standards are addressed in the following section.

RM2 Summary

Due to code changes implemented since the original building design and approval, four modifications are requested to meet façade offsets and outdoor area dimensional and locational standards. All other standards in the RM2 zone are met, as evidenced in this section.

1.3 Inclusionary Housing (33.245)

33.245.020 Where These Regulations Apply

The regulations of this chapter apply to the following:

- A. New buildings with 20 or more dwelling units; and*
- B. Alterations to existing buildings that add 20 or more dwelling units.*

Response: 43 dwelling units are proposed. These regulations apply.

33.245.040 Inclusionary Housing Standards

Affordable dwelling units must be provided as follows, or a fee-in-lieu of providing affordable dwelling units must be paid. Adjustment are prohibited:

- A. On-site affordable dwelling units. When the affordable dwelling units will be located on-site, affordable dwelling units must be provided at one of the following rates. For the purpose of this Section, affordable dwelling units located within the boundaries of a Central City Master Plan are considered to be on-site:
 - 1. Central City and Gateway plan districts. Inside the Central City and Gateway plan districts, affordable dwelling units must be provided at one of the following rates:
 - a. 10 percent of the total number of dwelling units in the new building or the alteration must be affordable to those earning no more than 60 percent of the area median family income; or*
 - b. 20 percent of the total number of dwelling units in the new building or the alteration must be affordable to those earning no more than 80 percent of the area median family income; or*
 - c. Alternate calculation method. As a way to encourage the creation of larger affordable dwelling units, using one of the percentages stated above, the number of affordable dwelling units required may be calculated based on***

the total number of bedrooms in the new or altered building. For example, using the 10 percent rate, a new building with 60 two-bedroom dwelling units could provide 6 two-bedroom affordable units or 4 three-bedroom affordable units.

2. *Outside the Central City and Gateway plan districts. Outside the Central City and Gateway plan districts, affordable dwelling units must be provided at one of the following rates.:*
 - a. *Rates before January 1, 2022: [...]*
 - b. *Rates on and after January 1, 2022. The rates shown in Paragraph A.1. apply outside the Central City and Gateway plan districts on and after January 1, 2022.*

Response: The site is located outside the Central City and Gateway plan districts, but the proposal has been revised after January 1, 2022. The rates in in Paragraph A.1 apply. Ten percent of the 43 total units will be affordable to those earning no more than 60 percent of the area median family income. As such, a total of 4 units will be inclusionary.

B. Off-site affordable dwelling units. [...]

Response: The affordable units provided in this proposal will be included within the proposed apartment building.

33.245.050 Compliance

To comply with the inclusionary housing standards in Section 33.245.040, the following must be met. Adjustments are prohibited.

- A. *The applicant must provide a letter from the Portland Housing Bureau certifying that the development meets the standards stated above and any administrative requirements. The letter is required to be submitted before a building permit can be issued for the development, but is not required in order to apply for a land use review; and*
- B. *If affordable dwelling units will be provided the property owner must execute a covenant with the City that complies with the requirements of Section 33.700.060. The covenant must be provided prior to issuance of the building permit for the development that triggers this chapter, and the covenant must ensure that the affordable dwelling units will remain affordable to the household meeting the income restrictions, and will meet the administrative requirements of the Portland Housing Bureau.*

Response: Prior to the issuance of building permits, a letter from the Portland Housing Bureau will be provide that certifies the standards of this section are met. A covenant in compliance with the requirements of this section and Section 33.700.060 will be provided prior to the issuance of building permits.

Inclusionary Housing Summary

A total of 4 units will be provided as affordable to households earning 60 percent or less of the area median family income. Documentation certifying compliance with the inclusionary housing regulations will be provided prior to building permit issuance.

1.4 Landscaping and Screening (33.248)

Proposed landscaping and screening to L2 and P1 standards was previously approved with PD #19-258445 PD, ENM, LDP. To meet RM2 outdoor open area requirements, a new common outdoor lawn is planned between the surface parking area and the lower lobby entrance. This area was previously planned for trees and shrubs that contributed to L2 and P1 landscaping standards. To accommodate the proposed grass area, shrubs and groundcovers have been relocated adjacent to the proposed lawn (see Figure 7).

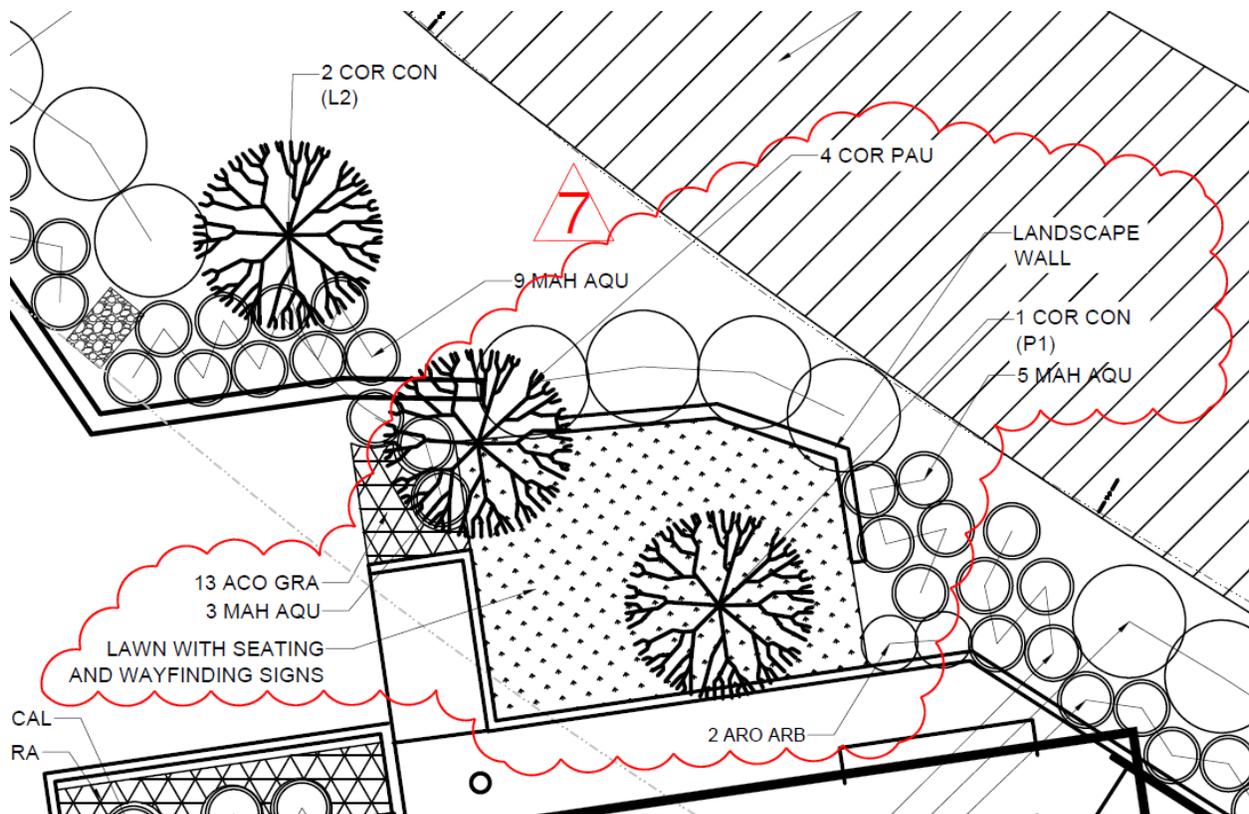


Figure 7 Proposed Common Lawn with Landscaping

The shift in these landscaping and screening elements will still continue to meet L2 and P1 standards, as previously approved in 2020.

1.5 Parking, Loading, Transportation and Parking Demand Management (33.266)

33.266.100 General Regulations

- A. Where the regulations apply. The regulations of this chapter apply to all parking areas in all zones, whether required by this code or put in for the convenience of property

owners or users. Parking areas include those accessory to a use, part of a Commercial Parking use, or for a park and ride facility in the Community Services use category.

- B. *Occupancy. All required parking areas must be completed and landscaped prior to occupancy of any structure except as provided in Chapter 33.248, Landscaping and Screening.*
- C. *Calculations of amounts of required and allowed parking. [...]*
- D. *Use of required parking spaces. Required parking spaces must be available for the use of residents, customers, or employees of the use. Fees may be charged for the use of required parking spaces. Required parking spaces may not be assigned in any way to a use on another site, except for joint parking situations. See 33.266.110.B. Also, required parking spaces may not be used for the parking of equipment or storage of goods or inoperable vehicles.*
- E. *Proximity of parking to use. Required parking spaces must be located on the site of the use or in parking areas whose closest point is within 500 feet of the site.*
- F. *Stacked parking. [...]*
- G. *Office of Transportation review. The Office of Transportation reviews the layout of parking areas for compliance with the curb cut and access restrictions of Section 17.28.110, Driveways – Permits and Conditions.*

Response: This proposal includes 43 dwelling units and will provide 28 parking spaces (see **First Floor Plan on Sheet A2.12**). The majority of the parking spaces will be located underneath the structure. Parking areas will be completed and landscaped as shown on the **Landscape Plan on Sheet L4**, prior to occupancy of the structure. No stacked parking or valet parking is proposed in this application.

33.266.110 Minimum Required Parking Spaces

[...]

- B. *Minimum number of required parking spaces.*
 - 1. *Minimum for sites located close to transit. For sites located 1500 feet or less from a transit station, or 500 feet or less from a transit street with 20-minute peak hour service the following minimum parking requirements apply. [...]*
 - 2. *Minimum for sites located far from transit. For sites located more than 1500 feet from a transit station, or more than 500 feet from a transit street with 20-minute peak hour service, the following minimum parking requirements apply:*
 - a. *Household Living uses.*
 - (1) *Single-dwelling zones. [...]*
 - (2) *All other zones. The minimum number of parking spaces required for Household Living uses in all other zones is stated in Table 266-1*
 - 3. *Joint use parking. [...]*
- C. *Required Carpool parking spaces. [...]*
- D. *Exceptions to the minimum number of parking spaces. [...]*

Response: The site is located on SW Gibbs Street approximately 550 feet from the nearest transit street. The number 8 Trimet bus line stops at SW 11th and SW Gibbs. The site is considered “far from transit”

No joint use parking is proposed. 1 parking space per 2 units are required by Table 266-1. This proposal will provide 28 parking spaces for 43 units.

There are no office, industrial, or institutional uses proposed on the site, therefore carpool parking spaces are not required. Parking minimum is requested.

33.266.115 Maximum Allowed Parking Spaces

[...]

B. Maximum number of required parking spaces. Regulations in a plan district overlay zone may supersede the regulations in this subsection.

- 1. Surface parking. Where more than 25 percent of the parking accessory to a use is on surface parking lots, both the structured and surface parking are regulated as follows. Parking accessory to a use includes accessory parking that is on- and off-site:
 - a. Generally. The maximum number of parking spaces allowed is stated in Tables 266-1 and 266-2, except as specified in Subparagraph B.1.b.;*
 - b. Exception for sites not well served by transit. For sites located more than 1/4 mile from a bus stop with 20-minute peak-hour service and more than 1/2 mile from a Transit Station with 20-minute peak-hour service, the maximum number of parking spaces allowed is 125 percent of the amount stated in Tables 266-1 and 266-2. The Bureau of Transportation will publish a map annually, adopted through Administrative Rule, showing sites that meet these service thresholds. For sites not shown on the map, the applicant may provide current information demonstrating that the site meets the service thresholds.**
- 2. Structured Parking. [...]*
- 3. Exception in the EG and I zones. [...]*

Response: There is no plan district overlay on the site that supersedes the parking regulations of this subsection. The maximum number of parking spaces allowed for household living per Tables 266-1 and 266-2 is none for a site far from transit. There are 28 spaces proposed on site for 43 units.

33.266.130 Development Standards for All Other Development

[...]

B. Where these standards apply. The standards of this section apply to all vehicle areas whether required or excess parking, except for residential parking areas subject to the standards of 33.266.120.

Response: The standards of Chapter 33.266.120 apply to houses and duplexes and do not apply to this proposal for a multi-dwelling structure.

C. *On-site location of vehicle areas.*

1. *Location of vehicle areas. The allowed on-site location of all vehicle areas is stated in Table 266-3. [...]*
2. *Building setbacks for structures that contain vehicle areas. [...]*

Response: The general standard for the RM2 zone found in Table 266-3 states “vehicle areas [are] not allowed between the portion of the building that complies with the maximum street setback and the transit street or streets in a Pedestrian District.” As shown on the **Site Plan on Sheet A1.00**, the vehicle parking area is located in the rear of the proposed multi-dwelling structure and no parking is proposed in the street setback.

3. *Frontage Limitation*

- a. *The standard of this subparagraph applies outside the Central City plan district in the R20 through R2.5, RM1, RM2, RM3, RM4, and RMP zones. No more than 40 percent of the frontage on a street may be used for vehicle areas. On sites with more than one street frontage, this standard applies to the street with the highest transit designation. If two streets have the same highest transit classification, the applicant may choose on which street to meet the standard. Sites where there is less than 100 square feet of net building area are exempt from this standard.*
- b. *[...]*

Response: This application does not propose to use street frontage for vehicle areas. Access to the site is taken via a driveway from SW Gibbs St. side of the site in the area zoned R7. The previous pre-application notes (EA 19-133418) indicated that PBOT requires shoulder widening and the removal of on-street parking in front of the site which will occur with the proposed site development.

4. *Surface parking and driveway paving limitations. In the RM1 through RM4 zones, the following parking area and driveway size and paving material limitations apply:*
 - a. *No more than 30 percent of total site area may be paved or used for surface parking and driveways; and*
 - b. *Asphalt paving for surface parking and driveways may not cover more than 15 percent of total site area.*

Response: The site area is 40,000 square feet. 2,224 square feet is proposed for the asphalt surface parking lot. This is 5% of the total site area. The concrete driveway will be 2,444 square feet. Combined asphalt and concrete vehicle parking and maneuvering areas will be 11.7% of the total site area.

D. *Improvements.*

1. *Paving. In order to control dust and mud, all vehicle areas must be paved. However, some portions of individual parking spaces may be landscaped per the standards of Paragraph F.4, below.*

2. *Striping. All parking areas, except for stacked parking, must be striped in conformance with the parking dimension standards of Subsection F. below.*
3. *Protective curbs around landscaping. All perimeter and interior landscaped areas must have protective curbs along the edges. Curbs separating landscaped areas from parking areas may allow stormwater runoff to pass through them. Tire stops, bollards, or other protective barriers may be used at the front ends of parking spaces. Curbs may be perforated or have gaps or breaks. Trees must have adequate protection from car doors as well as car bumpers.*

Response: As shown on the **Site Plan and First Floor Plan on Sheets A1.00 & A2.12**, all vehicle areas will be paved and striped. Protective curbs are proposed around the perimeter of the parking area as shown on **Sheet C4**.

- E. Stormwater management. Stormwater runoff from parking lots is regulated by the Bureau of Environmental Services. See Chapter 17.38, Drainage and Water Quality, and the City's Stormwater Management Manual, which contain requirements for managing stormwater in parking lot landscaping.*

Response: The **Stormwater Plan on Sheet C3** shows stormwater drainage for the parking area and details of compliance with BES regulations can be found in the **Stormwater Management Report in Appendix D**. Stormwater will be detained on site and discharged into a combined storm / sewer line located in Gibbs Street.

- F. Parking area layouts.*

1. *Access to parking spaces.*
 - a. *All parking areas, except stacked parking areas, must be designed so that a vehicle may enter or exit without having to move another vehicle.*

Response: As shown on the **First Floor Plan on Sheet A2.12**, the parking area includes 28 spaces and an adequate drive aisle that allows maneuvering a vehicle without having to move another vehicle.

- b. *All parking areas must be designed to allow vehicles to enter and exit the roadway in a forward motion, except:*
 1. *Parking areas with one or two spaces whose only access is on a local service street;*
 2. *Parking areas with up to four spaces may be designed so that vehicles back out into an alley. However, there must be a maneuvering area of at least 20 feet between the end of each parking space and the opposite side of the alley. If the alley is less than 20 feet wide, some of this maneuvering area will be on-site.*

Response: The parking area is accessed by a driveway on SW Gibbs Street that allows for vehicles to enter and exit in a forward motion, as shown on the **Site Plan on Sheet A1.00**. No parking spaces will take access directly to the street.

2. *Parking space and aisle dimensions. Parking spaces and aisles must meet the minimum dimensions contained in Table 266-4. For stacked parking areas, see Section 33.266.140 below.*

Response: As shown on **Sheet A2.12**, the dimensions of the parking spaces are 18-feet long by 8-feet 6-inches wide with an aisle width of 20-feet. The two proposed ADA spaces will each be 18-feet by 9-feet. The minimum dimensions shown in Table 266-4 are 8-feet 6-inches by 16-feet, with an aisle width of 20-feet.

3. *Parking for disabled persons. The Bureau of Development Services regulates the following disabled person parking standards and access standards through the Oregon Structural Specialty Code.*
 - *Dimensions of disabled person parking spaces and access aisles;*
 - *The minimum number of disabled person parking spaces required;*
 - *Location of disabled person parking spaces and circulation routes,*
 - *Curb cuts and ramps including slope, width and location;*
 - *Signage and pavement markings.*

Response: Two disabled person parking spaces are located in the parking lot as shown on **Sheet A2.12**. The proposed parking spaces meet Oregon Structural Specialty Code standards and will be reviewed during building permit review process.

4. *A portion of a standard parking space may be landscaped instead of paved, as follows:*
 - a. *As shown in Figure 266-3, up to 2 feet of the front of the space as measured from a line parallel to the direction of the bumper of a vehicle using the space may be landscaped area;*
 - b. *Landscaping must be ground cover plants; and*
 - c. *The portion of the 2-foot wide area described in 4.a that is landscaped counts toward parking lot interior landscaping requirements and toward any overall site landscaping requirements. However, the landscaped area does not count toward perimeter landscaping requirements.*

Response: As shown on the **Site Plan on Sheet A1.00**, there are 4 surface parking spaces located outside of the covered portion of the parking area. Landscaping will be provided around the parking area, as shown on **Sheet L4**.

5. *Large parking areas in R, C, E, IR, and CI zones. In the R, C, E, IR, and CI zones, where a parking area on the site is more than 125,000 square feet, the parking area must contain the following elements. Parking areas in structures are not included in this total [...]*

Response: The property is not located in the R, C, E, IR, or CI zone, nor is a large parking area proposed.

- G. *Parking area setbacks and landscaping.*

1. *All landscaping must comply with the standards of Chapter 33.248, Landscaping and Screening. Trees and shrubs must be fully protected from potential damage by vehicles.*
2. *Setbacks and perimeter landscaping.*
 - a. *Where these regulations apply. The regulations of this paragraph apply to:*
 1. *Surface parking areas abutting a lot line;*
 2. *Any portion of structured parking areas where the parking area is within 4 feet of adjacent grade and there is no roof over it;*
 3. *Driveways.*
 - b. *Exceptions. [...]*
 - c. *Setbacks. The minimum required setbacks for surface parking areas are stated in Table 266-5. Protective curbs, tire stops, bollards or other protective barriers are not allowed within the minimum required setbacks.*
 - d. *Perimeter landscaping. The minimum setbacks and landscaping standards required are provided in Table 266-5.*
 1. *Surface parking abutting streets, and C, E, I, and CI zones. [...]*
 2. *Surface parking abutting OS, R, and IR zones. [...]*

Response: Table 266-5 requires 5-feet of L2 screening for lot lines abutting streets and 5-feet of L3 screening for lot line abutting an R zone lot line. The presence of an environmental conservation overlay allows for setbacks to be reduced to zero feet. Landscaping is shown on the **Landscape Plan on Sheet L4**. As indicated on **Sheet L4**, at least 5 feet of L2 screening is proposed adjacent to the west of the drive, where the site abuts the unimproved SW 14th street right of way. To the north of the drive and outdoor parking area, landscaping and a large forested environmental tract provide extensive screening. The site is surrounded by ROWs on all four sides and does not abut C, E, I, CI, OS, R, or IR zones.

3. *Interior landscaping. The regulations of this paragraph apply to all surface parking areas except stacked parking areas. For stacked parking areas, see Section 33.266.140 below.*

Amount of interior landscaping required. In all zones, interior landscaping must be provided for sites where there are more than 10 parking spaces on the entire site. At least 45 square feet of interior landscaped area must be provided for each parking space.

Response: The **Landscape Plan on Sheet L4** shows interior parking lot landscaping for the 4 surface parking spaces. However, this landscaping is not required per this code section since there are less than 10 surface parking spaces. The remaining 24 parking spaces are located underneath the building.

- a. *Individual tree-planting spaces. Where an individual tree is planted in a space surrounded by pavement, the planting area must have a minimum interior dimension of five feet. See Figure 266-7.*

Response: No individual trees are planted in areas surrounded by pavement.

33.266.140 Stacked parking Areas

Stacked parking areas must comply with all of the development standards of Section 33.266.130 above, except for those standards superseded by this section. [...]

Response: No stacked parking is proposed. This section is not applicable.

33.266.200 Minimum Required Bicycle Parking

B. Number of spaces required.

- 1. The required minimum number of bicycle parking spaces for each use category is shown on Table 266-6. No bicycle parking is required for uses not listed. Minimum bicycle parking is calculated on a geographic hierarchy based on the current and future bicycle usage. Standard A in Table 266-6 applies to the areas shown as Standard A on Map 266-1. Standard B in Table 266-6 applies to all other areas of the city.*
- 2. Until June 30, 2022, [...]*
- 3. The required minimum number of bicycle parking spaces is based on the primary uses on a site. When there are two or more separate primary uses on a site, [...]*

Response: The proposed multi-dwelling structure will contain a total of 43 units. The site is located within Area B on Map 266-1. Table 266-6 requires 1.1 long-term bicycle parking spaces per unit in Area B. The proposed 43 units require 48 long-term bicycle parking spaces. These spaces are provided in two secure bicycle rooms. Three short-term bicycle parking racks are provided outside of the building on the ground floor (**Sheet A2.11**).

33.266.210 Bicycle Parking Development Standards

A. [...]

B. Where these standards apply. The standards of Subsection C and D apply to required long-term bicycle parking, and the standards of Subsection C and E apply to required short-term bicycle parking.

C. Standards for all bicycle parking. The Bureau of Transportation maintains a bicycle parking handbook that includes information on rack standards, siting guidelines and other standards of this code chapter. Long-term and short-term bicycle parking must be provided in lockers or racks that meet the following standards:

- 1. Bicycle parking area standards. The area devoted to bicycle parking must be hard surfaced.*
- 2. Bicycle racks. Where bicycle parking is provided in racks, the racks must meet the following standards:*
 - a. The rack must be designed so that the bicycle frame and one wheel can be locked to a rigid portion of the rack with a U-shaped shackle lock, when both wheels are left on the bicycle;*
 - b. If the rack is a horizontal rack, it must support the bicycle at two points, including the frame; and*
 - c. The rack must be securely anchored with tamper-resistant hardware.*

Response: All bicycle parking areas will be hard surfaced. Racks are proposed for both long- and short-term bicycle parking. Rack details are shown on **Sheet A1.62**, detail 6. They will be anchored and able to accommodate a U-shaped shackle lock with both wheels still attached.

3. *Bicycle Parking Space, Maneuvering Area, and Clearance Dimensions. Bicycle parking spaces, aisles and clearances must meet the minimum dimensions contained in Table 266-7.*
 - a. *Standard Bicycle Parking Space Requirements.*
 1. *The standard required bicycle space is 2 feet wide, 6 feet long and 3 feet 4 inches tall. See Figure 266-8;*
 2. *There must be at least 5 feet behind all bicycle parking spaces to allow room for bicycle maneuvering. Where short-term bicycle parking is adjacent to a sidewalk, the maneuvering area may extend into the right-of-way;*
 3. *A wall clearance of 2 feet 6 inches must be provided. See Figure 266-9.*

Response: Bike room dimensional details are contained on **Sheet A1.62**. Short term bicycle space dimensional details are contained on **Sheet A1.61**. Bicycle racks will be 2 feet wide, 6 feet long, and 33 feet 4 inches tall.

- b. *Alternative Spacing Requirements. The following bicycle parking layouts may be provided as an exception to the standard spacing requirements in Subparagraph C.3.a. See Table 266-7 for the alternative spacing dimensions.*
 1. *Horizontal bicycle parking spaces. Horizontal bicycle parking spaces secure the parked bicycle horizontal to the ground. [...]*
 2. *Vertical bicycle parking space. Vertical bicycle parking secures the parked bicycle perpendicular to the ground. Vertical bicycle parking that is placed as shown in Figure 266-12 may meet the alternative vertical dimensions in Table 266-7.*
 3. *Stacked bicycle parking spaces. [...]*

Response: 28 long-term bicycle spaces are proposed to be vertical and meet the dimensional requirements (3 feet 4 inches wide, with 1 foot 5 inches of space, and 5 foot aisles between rows. For details, see Sheet **A1.62**.

4. *Bicycle lockers. Bicycle lockers are fully enclosed and secure bicycle parking spaces.*
 - a. *The locker must be securely anchored to the ground.*
 - b. *There must be an aisle at least 5 feet wide behind all bicycle lockers to allow room for bicycle maneuvering.*
 - c. *Locker Dimensions. All bicycle lockers must meet one of the following:*

1. *The locker space has a minimum depth of 6 feet and an access door that is a minimum of 2 feet wide.*
 2. *A locker provided in a triangle locker layout for two bicycle parking spaces must have a minimum depth of 6 feet and an access door that is a minimum of 2 feet wide on each end.*
5. *Signage*
- a. *Light rail stations and transit centers. [...]*
 - b. *Other uses. If bicycle parking is not visible from the streets or main building entrances, a sign must be permanently posted at the main entrance indicating the location of the bicycle parking.*
6. *Bicycle parking information in plans. [...]*

Response: As shown on the **First Floor Plan on Sheet A2.12**, covered long-term bicycle parking will be provided inside a locked bicycle room adjacent to the vehicle parking area in the lower lobby, with a second long-term bicycle room in the upper lobby. Both will only be accessible to tenants. Bike lockers are not proposed. All bicycle parking will be provided in a secure bike room. Permanent signage will be posted at the main entrances to show the location of the secure bike room.

D. Standards for Long-Term Bicycle Parking.

1. *Development Standards. Long-term bicycle parking must be provided in lockers or racks that meet the following standards. Long-term bicycle parking for Schools may choose between (1) or (5) or a combination of those two locations:*
 - a. *Location Standards. Long-term bicycle parking may be provided in one or more of the following locations:*
 1. *Within a building, including on the ground floor or on individual building floors;*
 2. *On-site, including in parking areas and structured parking;*
 3. *In an area where the closest point is within 300 feet of the site;*
or
 4. *In a residential dwelling unit. Up to 50 percent of long-term bicycle parking spaces may be provided in a residential dwelling unit, if they meet the following. Long-term bicycle parking provided in a residential dwelling unit does not need to meet the requirements for Paragraph C.2. above. Adjustments and modifications to this Subsubparagraph are prohibited.*
 - *The bicycle parking is located within 15 feet of the entrance to the dwelling unit.*
 - *The bicycle parking is located in a closet or alcove of the dwelling unit that includes a rack that meets the standard bicycle parking spacing dimensions in Table 266-7.*
 - *For buildings with no elevators, long-term bicycle parking must be located in the ground floor units.*

5. *For Schools, [...]*
 - b. *Exceptions [...]*
 - c. *For sites with multiple primary uses, [...]*
 - d. *Covered bicycle parking [...]*
2. *Security Standards.*
 - a. *Long-term bicycle parking must meet the following security standards:*
 1. *Long-term bicycle parking for residential uses must be provided in one of the following:*
 - *A restricted access, lockable room or enclosure, designated primarily for bicycle parking;*
 - *A bicycle locker; or*
 - *In a residential dwelling unit meeting Subsubparagraph 1.a.(4), above.*
 2. *Long-term bicycle parking for all other uses [...]*
 - b. *All access routes and the bicycle parking spaces must be lighted to a level where the system can be used at night by the employees and residents.*

Response: Covered long-term bicycle parking will be provided inside two locked bicycle rooms – one adjacent to the vehicle parking area on the ground floor, the second adjacent to the upper lobby and main entrance. Both will be accessible only to tenants. 48 long-term spaces will be provided. All access routes to the bike rooms and within the bike rooms will be lit to accommodate nighttime use.

3. *Additional Development Standards. The following standards apply to sites with more than 20 long-term bicycle parking spaces:*
 - a. *Minimum number of horizontal bicycle parking spaces. At least 30 percent of spaces must be in a horizontal rack, or on the lower level of a stacked bicycle parking rack. For Schools (K-8), all spaces located outside of the building must be in a horizontal rack.*
 - b. *Parking for larger bicycle space. At least 5 percent of spaces must accommodate a larger bicycle space, placed in a horizontal rack. These spaces may be included to meet the requirement for Subparagraph D.3.a. See Figure 266-14.*
 - c. *Electrical outlet requirement. At least 5 percent of spaces must have electrical sockets accessible to the spaces. Each electrical socket must be accessible to horizontal bicycle parking spaces.*

Response:

- 15 horizontal spaces are proposed (30%)
- 3 large spaces are proposed (5%; these are included in the horizontal space counts)
- At least 5% of bicycle parking spaces will have access to electrical outlets

E. Standards for Short-term bicycle parking.

1. *Development Standards. Short-term bicycle parking must meet the following standards:*
 - a. *Location Standards. Short-term bicycle parking must meet the following location standards:*
 1. *On-site, outside a building;*
 2. *At the same grade as the sidewalk or at a location that can be reached by an accessible route; and*
 3. *Within the following distances of the main entrance:*
 - *Building with one main entrance [...]*
 - *Building with more than one main entrance. For a building with more than one main entrance, the bicycle parking must be along all façades with a main entrance, and within 50 feet of at least one main entrance on each façade that has a main entrance, as measured along the most direct pedestrian access route. (See Figure 266-16) [...]*
 - b. *Bike Parking Fund [...]*

Response: Three short-term bicycle parking spaces are required for this proposal. The structure has 2 entrances, one on the ground level accessed from the rear of the building, and one on the second floor which is accessed from the street by a short pedestrian way at street level. Short-term bicycle parking spaces are located within 50-feet of the ground floor entrance, as shown on **Sheet A2.12**. The “staple style” bicycle rack is located in a covered alcove between the surface parking area and building entrance. This area is also adjacent to the covered vehicle parking and one of the long-term bicycle parking areas.

33.266.310 Loading Standards

- A. *Purpose. [...]*
- B. *Where these regulations apply. [...]*
- C. *Number of Loading Spaces.*
 1. *Buildings where all of the floor area is in Household Living uses must meet the standards of this Paragraph.*
 - a. *One loading space meeting Standard B is required where there are more than 40 dwelling units in the building and the site abuts a street that is not a streetcar alignment or light rail alignment.*
 - b. *One loading space meeting Standard B is required where there are more than 20 dwelling units in a building located on a site whose only street frontage is on a streetcar alignment or light rail alignment.*
 - c. *One loading space meeting Standard A or two loading spaces meeting Standard B are required when there are more than 100 dwelling units in the building. [...]*

Response: This proposal is for a 43-unit multi-dwelling structure located on SW Gibbs Street. There is no streetcar or light rail alignment located on SW Gibbs Street. Parking and

loading can be accommodated for in an off-street parking area. This standard does not apply.

33.266.410 Transportation and Parking Demand Management

B. Transportation and parking demand management in the commercial/mixed use and multi-dwelling zones. In the commercial/mixed use and multi-dwelling zones, a TDM plan is required when new development includes a building with more than 10 dwelling units, or an alteration to existing development includes the addition of more than 10 dwelling units within a building. Sites in the Central City plan district, and sites that are located far from transit, as described in Paragraph 33.266.110.B.2, are exempt from this requirement. To meet the TDM standard, the applicant must choose one of the following:

- 1. Go through the Transportation Impact review process set out in chapter 33.852; or*
- 2. Meet the objective standards of Title 17.107 as verified by the Portland Bureau of Transportation.*

Response: Transportation impacts studies are included in **Appendix C**. The proposed 43 units will add an additional 144 weekday total trips over the existing 52. The project is not expected to significantly alter the operation or safety of the existing transportation facilities.

Parking Loading and Transportation and Parking Demand Management Summary

Parking for 28 vehicles will be provided in an off-street parking lot. A covered and locked bicycle room will be provided for 48 bicycles for residents. Three short-term bicycle parking spaces are also provided. All standards are met.

Part 2: Planned Development Modification & Partition Amendment

2.1 Planned Development (33.270)

33.270.020 Relationship to Other Regulations

- A. Flexibility. [...]*
- B. Density and FAR. Minimum residential density and minimum FAR requirements must be met in a Planned Development. Adjustments to minimum density or minimum FAR are prohibited. Where the density requirement is expressed as a number of lots, it can be met in the Planned Development by providing the same number of dwelling units. Maximum density requirements in Single-Dwelling zones are specified in 33.610.100 and 33.611.100. Maximum FAR requirements are specified in 33.130.205.*

Response: The site is split zoned between R7 and RM2. The combined maximum density allowed across the site with inclusionary housing bonuses is 47 units. 43 units are proposed. The standard is met.

- C. *Land Divisions. A Planned Development may be the only land use review requested for a site, or may be part of a proposal for a Land Division. Certain site conditions or aspects of a proposal require a Land Division, including situations where a tract is required (such as when there is floodway on the site), or where rights-of-way are requested or required.*

Response: A land division was approved in 2020 to create a protected environmental resource tract (see LU #19-258445 PD, ENM, LDP). A land division is not proposed in this application.

33.270.100 Additional Allowed Uses and Development

In addition to the housing types and uses allowed by other chapters of this Title, the following uses and development may be requested through Planned Development Review. More than one of these elements may be requested:

- A. *Attached houses. [...]*
- B. *Duplexes. [...]*
- C. *Attached duplexes. [...]*
- D. *Multi-dwelling structures. Multi-dwelling structures may be requested in the RF through R2.5 zones;*
- E. *Multi-dwelling development. [...]*
- F. *Modification of site-related development standards. Modification of site-related development standards that are not prohibited from being adjusted may be requested through a Planned Development.*
- G. *Alternative residential Dimensions. Proposals for lots that do not meet the minimum lot area, minimum lot depth, or minimum front lot line standards may be requested in RF through R2.5 zones. Proposals for lots that do not meet the minimum lot size dimensions may be requested in the RH through R3 zones.*

Response: A multi-dwelling structure was previously approved through the PD process. The proposed modifications to prior PD approval are internal floor plan changes to increase the residential units from 27 to 43. Four modifications to site-related development standards are requested to accommodate code changes that have occurred since the previous approval, allowing the proposed outdoor common areas and front façade offsets.

- H. *Commercial uses. [...]*
- I. *Additional height and FAR. [...]*
- J. *Transfer of development within a site. Transfer of development rights across zoning lines within the site may be proposed as follows:*
 - 1. *RF through R1 zones. If the site is located in more than one zone, and all the zones are RF through R1, the total number of units allowed on the site is calculated by adding up the number of units allowed by each zone. The dwelling units may be placed without regard to zone boundaries.*

Response: No commercial uses, additional height, or FAR are proposed or requested. Density from the R7 zone will be included with that of the RM2 zone in the total number of

units proposed in the multi-dwelling structure. The structure will be constructed primarily on the RM2 portion of the property with vehicle access, parking, and some of the residential structure and amenities located on the R7 portion of the property.

2. *RH and RX zones. [...]*
 3. *C, E, I, CI and IR zones. [...]*
 4. *All Zones. If the site is located in more than one zone, and at least one of the zones is RF through R1, and at least one of the zones is RH, RX, C, or EX, then the total number of dwelling units allowed on the site is calculated as follows: [...]*
- K. *Transfer of development between sites. [...]*

Response: No portion of the property is in an RH, RX, C, E, I, CI, or IR zones. No transfer of development between sites is proposed.

Planned Development Summary

The proposal is to modify a previously approved PD to change internal floor plans and increase proposed units from 27 to 43. To address code changes relating to outdoor areas and façade articulation, four PD modifications are requested.

2.2 Planned Development Review (33.854)

33.854.200 Review Procedures

- A. *Concurrent reviews. When land use reviews in addition to Planned Development Review are requested or required, all of the reviews must be processed concurrently, except for Design Review for buildings within a Planned Development site when the Planned Development bonus is being utilized (See 33.130.212.E). In this case, Design Review may be processed after the Planned Development Review. [...]*

Response: This application is for a modification to a previously approved PD. The site is not in a commercial/mixed use zone; design review is not required.

- B. *Planned Development Bonus. Proposals that are using the commercial/mixed use zones Planned Development bonus [...]*
- C. *All other Planned Development Reviews.*
1. *Review in conjunction with a land division. [...]*
 2. *Review not in conjunction with a land division. [...]*

Response: 43 units are proposed. The requested modification to the previously-approved PD will be processed through a Type III procedure.

33.854.250 Supplemental Application Requirements

In addition to the application requirements of Section 33.730.060.D, the following information is required for a Planned Development application:

- A. *Supplemental application requirements for Planned Developments in commercial/mixed use zones [...]*
- B. *Supplemental application requirements for all other Planned Developments:*

1. Photographs that show the characteristics of surrounding neighborhood; and
2. Either B.2.a. or B.2.b., must be submitted with the application:
 - a. Proposed building elevations and locations with enough detail to show that all of the approval criteria are met; or
 - b. Proposed standards regulating setbacks, building coverage, landscaping, vehicle areas, materials, and design of structures. The proposed standards must be clear and objective, and specific enough to show how all of the approval criteria are met. Proposed standards may not conflict with the regulations of this Title, except where a modification is requested as part of the Planned Development application. If approved, the standards will apply, in addition to regulations of this Title to all development on the site.

Response: The proposed development is not in a commercial/mixed use zone. Photographs showing the characteristics of the surrounding neighborhood are included in **Appendix F**. Proposed building elevations and locations showing enough detail to show that all of the approval criteria are met are included in the architectural plan set.

33.854.300 Approval Criteria

The approval criteria for Planned Developments are stated below. Planned Developments in all zones must meet the criteria in Section 33.854.310. Some proposals must also meet additional approval criteria, as follows:

- A. *Proposals to modify site-related development standards must meet the criteria in Section 33.854.320.*
- B. *Proposals for commercial uses in residential zones must meet the criteria in Section 33.854.330.*
- C. *Proposals that do not include a land division must meet the criteria in Section 33.854.340. Proposals that are only using the commercial/mixed use zones Planned Development bonus (See 33.270.100.K) are not required to meet the criteria in Section 33.854.340.*

A request for a Planned Development will be approved if the review body finds that the applicant has shown that all of the relevant approval criteria have been met.

Response: Staff determined that the approval criteria applicable to the previous PD approval will apply to the modification request contained in this application. The approval criteria of Section 33.854.310 applicable to the previous approval are reviewed below.

33.854.310 Approval Criteria for Planned Developments in All Zones

Criteria A through F apply to proposals for additional height or FAR in the CM2, CM3, CE, and CX zones that are taking advantage of 33.270.100.I. If the Planned Development is not proposing additional height or FAR as allowed by 33.270.100.I, then only criteria E and F apply. [...]

E. *Configure the site and development to visually integrate both the natural and built features of the site and the natural and built features of the surrounding area. Aspects to be considered include:*

1. *Orienting the site and development to the public realm, while limiting less active uses of the site such as parking and storage areas along the public realm;*

Response: The site is located on a sloping bend on the ROW of SE Gibbs Street, approximately 15 feet below the grade. The building has been designed to have the main entrance on the second floor, with a short pathway leading from the main entrance to the public ROW of SW Gibbs Street. The site's driveway has been reoriented to provide vehicle access from only the southwest corner of the site (see **Site Plan on Sheet A1.00**). With commercial and employment centers to the east of the site, this reorientation will ensure that pedestrian flow between the main entrance and these destinations will remain unobstructed. The driveway will circle to the back of the building connecting to vehicle parking on the ground floor. Both the second floor (main lobby) and ground floor (lower lobby) will have secure bicycle parking rooms. All other storage areas are planned on the ground floor, below the grade of the existing roadway and public realm.

2. *Preservation of natural features on the site, such as stands of trees, water features or topographical elements;*

Response: The site contains a flat, developed area near SW Gibbs Street before dropping steeply downhill to the north. An environmental conservation overlay zone exists on the northern portion of the site which contains a dense forested area. As part of the previously approved PD, a land division was approved to create a protected environmental resource tract. This tract preserves the existing stands of trees and topographical elements on site. No identified continuously flowing water features or wetlands exist on the site. A drainage area is located within the environmental tract as shown on the Vicinity Map in **Appendix G**. The nearest disturbance area will be approximately 80 feet away, as shown on **Sheet C4**.

Tree removals onsite have been minimized to the extent possible. Since approval of LU #19-258445 PD, ENM, LDP, additional tree removals within the ROW have been found by PBOT to be necessary. These additional tree removals are addressed in Public Works permit 19-193047 WT | TH0903, which has received 30% approval. All onsite tree preservation proposed and approved on **Sheets L4 and L5** are proposed to continue to remain preserved. Mitigation plantings are proposed for on-site tree removals.

3. *Inclusion of architectural features that complement positive characteristics of surrounding development, such as similar building scale and style, building materials, setbacks, and landscaping;*
&
4. *Mitigation of differences in appearance through means such as setbacks, screening, landscaping, and other design features;*

Response: Photos of the surrounding neighborhood are contained in **Appendix F**. The surrounding development includes a mix of single-family dwelling, multi-dwelling

structures, and some commercial uses. At times, businesses and residences are carved into the hillsides, using the natural topography to create daylight basements and second floor main entrances like the one proposed in this application. This strategy creates the appearance of smaller scale structures that blend with the range of sizes of single-family residences.

Directly opposite of the site across SW Gibbs Street is a single-family dwelling that sits approximately 5 -10 feet above grade. The home is screened with thick shrubs with a lower story entrance and driveway parking onto SW Gibbs St. and an upper story entrance and driveway parking on SW 13th Street. The site is surrounded by heavily vegetative unimproved ROWs to the east, west, and north.

- Beyond the unimproved ROW to the west is Marquam Nature Park – a densely forested park with mature trees reaching 40’ and beyond in height.
- Beyond the unimproved ROW to the east are a series of four older one- and two-story single-family dwellings.
- Beyond the unimproved ROW to the north is the Forest View Apartments, a recently remodeled apartment complex built in 1961 and containing 25 units.

The vegetative unimproved ROWs, the proposed landscaping surrounding the building, and the protected environmental tract along the north and west side of the site (see LU #19-258445 PD, ENM, LDP) will screen the building from these adjacent uses.

The proposed design was also approved in 2020 (LU #19-258445 PD, ENM, LDP) and the building façade and outdoor features are not proposed to deviate from this previous approval. Since the development was approved, two additional apartment complexes have been approved in this neighborhood. Both apartments did not have to adhere to RM2 standards.

The proposed architectural features that mirror the surrounding development include:

- ❖ Personal balconies and common decks – see Appendix F: F-4, F-6, F-11, F-12, F-13, F-14, F-15, F-18, F-19, and F-22
- ❖ Neutral tones – see Appendix F: F-1, F-3, F-4, F-6, F-8, F-9, F-12, F-14, F-15, F-21, and F-22
- ❖ Vertical natural wood paneling – See Appendix F: F-11
- ❖ Flat roofs – see Appendix F: F-1, F-2, F-4, F-7, F-13, F-14, F-15, and F-19
- ❖ 4-stories from grade – see Appendix F: F-4, F-7, F-8, F-12, F-17, and F-19,

The proposed design includes more modern development designs that will match the apartments currently under construction along SW 12th Ave.

5. Minimizing potential negative effects on surrounding residential uses; and

Response: The potential negative effects of the proposed development on surrounding residential uses are typical of any increase in residential units: traffic, noise, and light. These impacts are addressed as follows:

- **Traffic:** Transportation impact studies are provided in **Appendix C**. As indicated in the TIS, the use will generally match the residential traffic characteristics of the surrounding area and travel patterns will not significantly alter from what is typical for the neighborhood. Impacts to the neighborhood will be minimal. Vehicle types and travel patterns will be similar to existing neighborhood conditions. The proposed 43 units will add an additional 144 weekday total trips to the surrounding transportation system. These new site trips are not expected to significantly alter the operation or safety of the existing transportation facilities, beyond those previously approved in LU #19-258445 PD, ENM, LDP. Further, off-site parking will not be impacted because the proposal will provide 4 additional spaces than required by PZC 33.266. For 43 units, 22 spaces are required; 28 spaces are proposed.
- **Noise:** The site is buffered by forested vegetation on three sides provided by the terrain and unimproved conditions of the adjacent rights-of-way. Vehicle parking and maneuvering areas will be under and inside the structure, while the small outdoor parking area is located on the far side of the proposed building from adjacent residential property to the east and below the street grade from the adjacent residence to the south. These features will provide a buffer from automobile noise generated on site.
- **Light:** Sources of light impacts will come from automobile headlights and interior and exterior lighting from the structure and pedestrian walkways. A screen of natural, mature vegetation exists on three sides of the property, and the small outdoor parking area is below the street grade and on the opposite side of the proposed structure from adjacent residential development. Landscaping as shown on **Sheet L4**, will provide additional screening from headlights in the parking area. Impacts from interior and exterior lights will be mitigated due to the location of the structure below the street grade and within a buffer of mature vegetation.

6. *Preservation of any City-designated scenic resources; and*

Response: There are no City-designated scenic resources located in the site.

F. *Provide adequate open area on sites zoned RF through R2.5 where proposed development includes attached houses, duplexes, attached duplexes, or multi-dwelling structures. Open area does not include vehicle areas.*

Response: The proposed multi-dwelling structure is located on property zoned both R7 and RM2, (see **Site Plan on Sheet A1.00**). With 43 units, 2,064 square feet of outdoor area is required. 27 units will have personal balconies, totaling 1,296 square feet. An additional 970 square feet of common outdoor areas are proposed. A total of 2,266 square feet of outdoor areas are proposed, 202 square feet more than the required amount. This is enough additional square footage to accommodate another 4 units in addition to the 43 units proposed.

As for quality, outdoor areas are proposed to be surfaced with either decking materials or grass. The lower common lawn is planned along the main pedestrian pathway between the

surface parking/maneuvering areas and the lower lobby entrance. This location will allow for more frequent and natural-feeling use of the space, rather than if it were planned on the opposite side of the site.

The upper deck will be accessed from a main entrance on the second floor of the building, and it will look out towards the forested slopes of Marquam Park. The scenic views and large dimensions will be inviting to visitors and residents. Neither the upper deck nor the lower lawn are proposed to be roofed or enclosed, but both could accommodate tables and seating for users.

33.854.320 Additional Approval Criteria for Modifications of Site-Related Development Standards

Response: Four site-related development modifications are requested to meet RM2 requirements that did not apply to the original design and building approval but are activated by the proposed interior floor plan redesign to increase the number of units. Three modifications requests are for dimensional and locational standards related to common outdoor open spaces. One requested modification is to comply with façade articulation standards in RM2.

The approval criteria from the most recent PZC PD chapter version – dated 8/1/21 – are used to assess requested modifications.

Façade Articulation (33.120.230.C.2)

Response: A modification is requested to reduce the area of façade articulation from 25% to 22% of the total street-facing surface area. This is specific to PZC 33.120.230.C.2.

A. Better meets approval criteria. The resulting development will better meet the applicable approval criteria of Section 33.854.310; and

Response: This standard applies to the development proposal because the building is setback within 20-feet of the front street property line. The building is planned roughly 3 feet from the front lot line because of the presence of the environmental overlays on the northern (rear) and eastern (right) areas of the site. The façade articulation standard is a new standard that did not apply to the original building design and approval. The proposal is for interior changes to floor plans that increase housing units within the original design approval. The original design was determined by City of Portland planning staff and the Hearings Officer to meet planned development approval criteria for site orientation, preservation of natural features, neighborhood compatibility, accessibility and garbage/recycling areas – the same criteria applicable to this modification. Since the design has not changed, these elements continue to be met, as discussed below.

Granting the modification would waive a requirement for an additional 145 square feet of façade articulation. On the 45-foot-tall building sitting 15-feet below the street grade, this amounts to one 3.2-foot-wide strip of additional articulation. This strip width would be virtually unnoticeable to any pedestrian, bicyclist, or motorist.

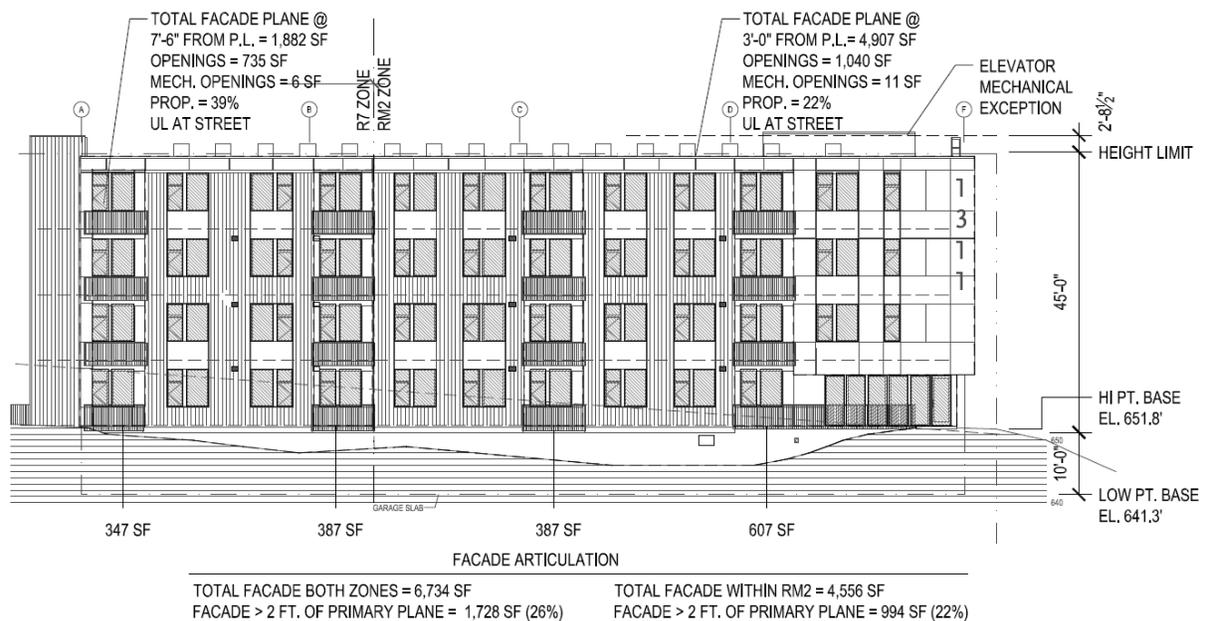


Figure 8 Facade Articulation

A portion of the front façade is below the grade of the road (see Figure 8). This small portion – above the garage slab – will not be visible to pedestrians or bicyclists/motorists. Removing this portion of the façade from the total calculations, 25% of the front façade in RM2 will be offset. This meets the minimum façade articulation standard. Further, when we consider the entire front façade in both R7 and RM2, the total façade articulation will be 26%. This total articulation exceeds the minimum standard in RM2.

The proposal better meets applicable PD approval criteria (PZC 33.854.310.E through H) for the following reasons:

- E.1 Orienting the site and development to the public realm, while limiting less active uses of the site such as parking and storage areas along the public realm:** The building will be 3-feet from the front lot line and SW Gibbs street. The requested modification is triggered because of the close proximity of the building to the front setback. The modification supports the siting of the building near the ROW, oriented toward the public realm. This approval criterion is positively impacted through the proposed modification.
- E.2 Preservation of natural features on the site, such as stands of trees, water features or topographical elements:** As with public realm orientation, siting the building near the front lot line will result in better preservation of natural resource areas. The location of the building allows for the protection of the resource area of the environmental overlays. This criterion will be better met with the proposed modification.
- E.3 Inclusion of architectural features that complement positive characteristics of surrounding development, such as similar building scale and style, building materials, setbacks, and landscaping:** The proposed design was previously determined by City

of Portland planning staff and the Hearings Officer to meet this standard. Existing multi-family development nearby the site does not include façade articulation elements enough to meet the 25% standard. The proposed development will be the first required to meet the 25% standard. The request to reduce the façade articulation area from 25 to 22% will result in a minimal reduction in façade offset areas. Further, the proposed development will include other architectural elements that are seen in the surrounding development patterns – balconies, neutral tones, vertical wood paneling, modern/contemporary smooth accent facades (Hardie shingle siding and fiber cement panels), flat roofs, and common deck space. This criterion will not be impacted from this modification request.

- **E.4 Mitigation of differences in appearance through means such as setbacks, screening, landscaping, and other design features; & E.5 Minimizing potential negative effects on surrounding residential uses; & F.1 Where proposed development includes attached houses, duplexes, triplexes, fourplexes, multi-dwelling structures, or multi-dwelling development, adequate open area to accommodate the proposed development must be provided. Open area does not include vehicle areas:** These criteria will not be negatively or positively impacted through the requested modification because a loss of 145 square feet of offset area will not be noticeable from the street on a building with a front façade totaling 11,290 square feet. Considering the entire façade in both R7 and RM2, this modification request will reduce façade articulation areas only by 1.2%. These criteria will remain unimpacted.
- **G. Accessible connections. Provide one or more accessible routes that connect all buildings on the site to adjacent streets, common open areas, and parking areas. Use landscaping and site furnishings to ensure the accessible route provides a pleasant user experience; & H. Garbage and recycling areas. Garbage and recycling collection areas must be adequate in size to accommodate the proposed development, designed to encourage recycling, and located to facilitate pick-up service. Screening and buffering of garbage and recycling areas must be provided to maintain a clean and attractive development:** Façade articulation percentages will not negatively or positively impact site circulation or the placement of garbage and recycling areas. These criteria will not be impacted.

With no change from the approved design, and no visible adverse impacts expected from granting this request, the modifications do not negatively impact the project's ability to meet PD approval criteria already determined to be met in the previous decision. Further, by siting the building closer to the front street lot line, further away from the natural resource areas, the requested modification supports the better preservation of environmentally sensitive areas while allowing the building to better orient towards the public realm of SW Gibbs Street.

- **B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standards for which a modification is requested.**

Response: The purpose statement for this criterion is repeated below.

Purpose. These standards, along with the height and setback standard, limit the bulk of buildings close to the street. These standards help ensure that large buildings will be divided into smaller components that relate to the scale and development patterns of Portland's multi-dwelling residential areas and add visual interest and variety to the street environment.

As described above, the request for a 3% reduction in façade articulation area totaling 145 square feet is so minimal that it will be indistinguishable for passersby. Other design features, like site topography, front entrance design to utilize the second floor, the proportion of windows, and front landscaping lend more to help limit the bulk of the building so near the street. The requested modification will remain consistent with the purpose of the façade articulation standards.

Upper Common Deck Dimensions (33.120.240.C.2.a)

Response: A modification is requested to reduce the required common outdoor area dimensions from a 20-foot by 20-foot square to a 17.5-foot by 20-foot square (the space is 17.5 feet by 43 feet and significantly larger than minimum size of 500 square feet; this modification is to reduce required width by 2.5 feet). This is a new standard specific to PZC 33.120.230.C.2.a.

A. Better meets approval criteria. The resulting development will better meet the applicable approval criteria of Section 33.854.310; and

Response: The proposal better meets applicable PD approval criteria (PZC 33.854.310.E through H) for the following reasons:

- **E.1 Orienting the site and development to the public realm, while limiting less active uses of the site such as parking and storage areas along the public realm:** The upper common deck is proposed over the trash/recycling areas and three parking spaces. The requested dimension reduction is to allow for this deck to cover these areas on the ground floor. Figure 3 shows the deck and proposed driveway leading to the back of the site. Placing the deck here allows for more site area to plan driveway access to the rear, outside of the public realm. If the upper deck was designed to meet the 20-foot by 20-foot requirement, the outdoor area would need to be placed elsewhere on the site, making site planning more challenging to fit all elements, and most likely resulting in the reduction of the driveway area preventing access from the rear.
- **E.2 Preservation of natural features on the site, such as stands of trees, water features or topographical elements:** As discussed above, proposing the upper deck over passive use areas (storage and parking) allows for a more compact site design. The requested dimensional modification allows for the building to be set further away from the environmentally sensitive areas resulting in greater protection of natural features. Placing the deck over parking and storage also results in less anticipated grading activities that would have resulted from leveling the steep site topography to include a useable outdoor open space area.
- **E.3 Inclusion of architectural features that complement positive characteristics of surrounding development, such as similar building scale and style, building materials, setbacks, and landscaping:** The request does not positively or negatively impact

architectural features. A roughly 2.5-foot reduction in width (approximately 107 square feet) will be unnoticeable to users.

- E.4 Mitigation of differences in appearance through means such as setbacks, screening, landscaping, and other design features; & E.5 Minimizing potential negative effects on surrounding residential uses:** The upper deck is planned to be accessed from the second floor and will face the forested natural area of Marquam Park. Placing the largest common open space area here will reduce impacts to the adjacent residential uses to the north, east, and south. All noise expected from active and passive recreational use in this area will be buffered by the proposed building and large, heavily vegetative environmental resource track. The dimensional reduction modification to place the outdoor common area over the parking/storage areas will positively improve mitigation measures for surrounding residential uses.
- F.1 Where proposed development includes attached houses, duplexes, triplexes, fourplexes, multi-dwelling structures, or multi-dwelling development, adequate open area to accommodate the proposed development must be provided. Open area does not include vehicle areas;** Although the request is for a dimension reduction from 20 to roughly 17.5 feet wide, the upper deck will still remain a useable common open area. The deck will be 690 square feet with a roughly 10-foot by 10-foot “notch” on the southern portion of the deck to offset the dimension reduction (see figure 9). The deck will be open and surfaced with decking materials. The deck will remain wide enough to accommodate passive uses. This standard is not positively or negatively impacted by the requested modification.

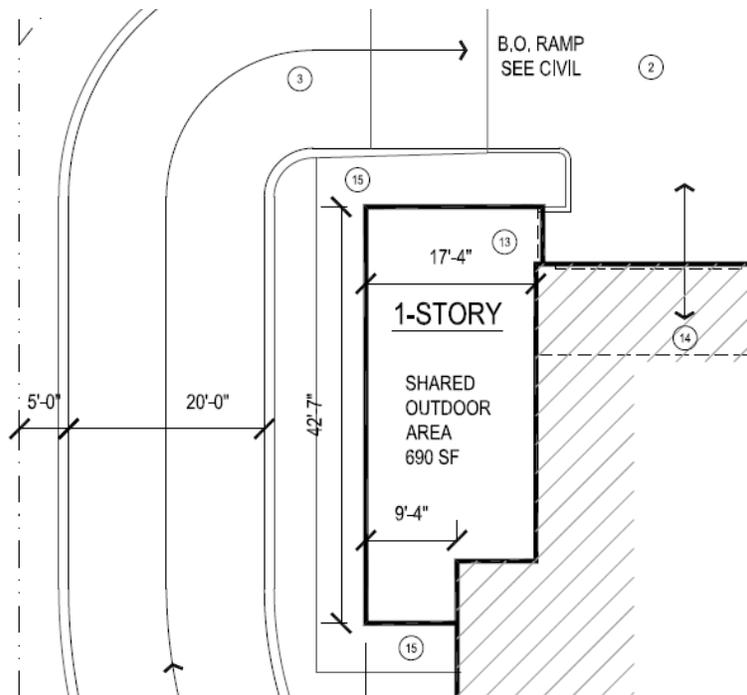


Figure 9. Upper deck as shown on site plan

- G. Accessible connections. Provide one or more accessible routes that connect all buildings on the site to adjacent streets, common open areas, and parking areas. Use**

landscaping and site furnishings to ensure the accessible route provides a pleasant user experience: Site circulation will not be impacted through this dimensional modification request.

- **H. Garbage and recycling areas.** *Garbage and recycling collection areas must be adequate in size to accommodate the proposed development, designed to encourage recycling, and located to facilitate pick-up service. Screening and buffering of garbage and recycling areas must be provided to maintain a clean and attractive development: The ability to place the deck over the garbage/recycling areas and some parking spaces will allow these passive uses to be screened from public view. This criterion will be positively impacted by the requested modifications.*

B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standards for which a modification is requested.

Response: The purpose statement for this criterion is repeated below.

Purpose. The required outdoor and common area standards ensure opportunities for outdoor relaxation or recreation. The standards work with the building coverage and minimum landscaped areas standards to ensure that some of the land not covered by buildings is of adequate size, shape, and location to be usable for outdoor recreation or relaxation. The standards also ensure that outdoor areas are located so that residents have convenient access. Required outdoor areas are an important aspect in addressing the livability of a residential property by providing outdoor living opportunities, some options for outdoor privacy, and a healthy environment. These standards also allow for common area requirements to be met by indoor community facilities because they also provide opportunities for recreation and gathering.

The request to reduce one deck dimension by roughly 2.5 feet will not impact the overall size or location of the upper deck. The dimension reduction request is for roughly 2.5 feet. The deck is proposed to be approximately 17.5 feet wide and 43 feet long. These dimensions still allow for both active and passive uses to occur with ample room for multiple simultaneous users. Providing this modification would not negatively impact useable open space availability.

Lower Common Lawn Dimensions (33.120.240.C.2.a)

Response: A modification is requested to reduce the required common outdoor area dimensions from a 20-foot by 20-foot square to an 11.5-foot by 10-foot square and reduce the minimum area from 500 square feet to 280 square feet. This is a new standard specific to PZC 33.120.230.C.2.a.

A. Better meets approval criteria. The resulting development will better meet the applicable approval criteria of Section 33.854.310; and

Response: The proposal better meets applicable PD approval criteria (PZC 33.854.310.E through H) for the following reasons:

- **E.1 Orienting the site and development to the public realm, while limiting less active uses of the site such as parking and storage areas along the public realm:** The lower common lawn is proposed near the northeast corner of the site, outside of the protected environmental resource areas. This area is behind the building and along a pedestrian pathway linking the lower lobby entrance to the surface parking areas. The request to reduce dimensional requirements will have no impacts on site orientation and layout.
- **E.2 Preservation of natural features on the site, such as stands of trees, water features or topographical elements:** The dimensional modifications are requested in-part to limit disturbance to natural features. The site is steeply sloped both to the north and to the east. Expanding this area or placing this area elsewhere on the site to measure 20 feet by 20 feet and at least 500 square feet will require additional grading and impacts to trees. As it stands, placing the lower common lawn as near as possible to the pedestrian walkway and parking areas will allow for the full protection of natural resource areas and limit impacts associated with grading and fill. The proposal will positively impact preservation efforts.
- **E.3 Inclusion of architectural features that complement positive characteristics of surrounding development, such as similar building scale and style, building materials, setbacks, and landscaping:** The request does not positively or negatively impact architectural features.
- **E.4 Mitigation of differences in appearance through means such as setbacks, screening, landscaping, and other design features; & E.5 Minimizing potential negative effects on surrounding residential uses:** Reducing the dimensional requirements for this open space area will not positively or negatively impact surrounding uses or contribute to mitigation measures to blend with existing development.
- **F.1 Where proposed development includes attached houses, duplexes, triplexes, fourplexes, multi-dwelling structures, or multi-dwelling development, adequate open area to accommodate the proposed development must be provided. Open area does not include vehicle areas;** Although the request is for a dimension reduction from 20 to roughly 11.5 feet wide, and a reduction from 500 to 280 square feet, overall combined outdoor area requirements will be met. Further, by allowing an alternative dimensional design for the creation of this open space area will result in an excess of 202 square feet more than what is required. This criterion will be positively impacted through the proposed modifications.
- **G. Accessible connections. Provide one or more accessible routes that connect all buildings on the site to adjacent streets, common open areas, and parking areas. Use landscaping and site furnishings to ensure the accessible route provides a pleasant user experience:** The proposed open space will be adjacent to the surface parking and pedestrian path connecting the parking areas and the lower lobby main entrance (see Figure 10). The dimensional modification request will allow for the open space to be planned in a central location, directly adjacent to the main pedestrian pathway. If the open space area is required to meet dimensional minimums, the outdoor area would have to be moved to less active areas of the site, with lower accessibility opportunities. The resulting modifications will have a positive impact to this criterion.

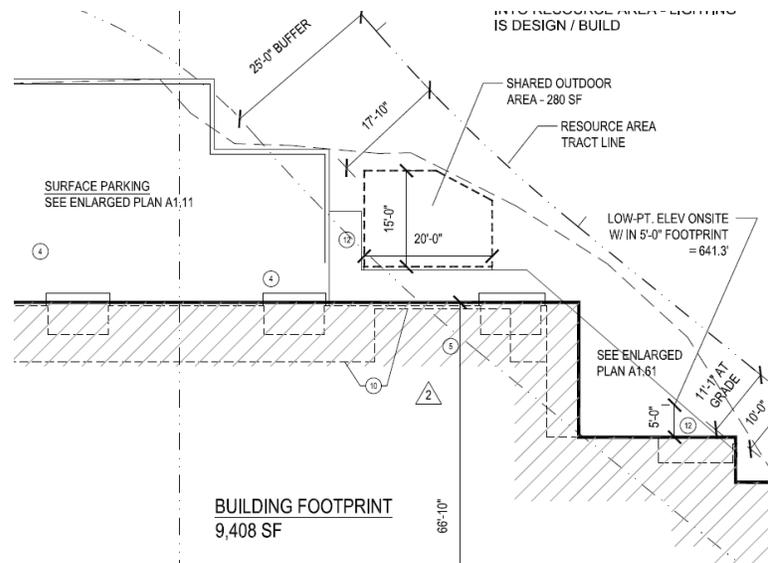


Figure 10. Lower common lawn on site plan

- H. Garbage and recycling areas.** Garbage and recycling collection areas must be adequate in size to accommodate the proposed development, designed to encourage recycling, and located to facilitate pick-up service. Screening and buffering of garbage and recycling areas must be provided to maintain a clean and attractive development: There will be no impacts to proposed garbage and recycling areas with the granting of this modification.

B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standards for which a modification is requested.

Response: The purpose statement for this criterion is repeated below.

Purpose. The required outdoor and common area standards ensure opportunities for outdoor relaxation or recreation. The standards work with the building coverage and minimum landscaped areas standards to ensure that some of the land not covered by buildings is of adequate size, shape, and location to be usable for outdoor recreation or relaxation. The standards also ensure that outdoor areas are located so that residents have convenient access. Required outdoor areas are an important aspect in addressing the livability of a residential property by providing outdoor living opportunities, some options for outdoor privacy, and a healthy environment. These standards also allow for common area requirements to be met by indoor community facilities because they also provide opportunities for recreation and gathering.

The dimension reduction request is for roughly 10 feet in both directions and 220 square feet. However, by creating this lower common lawn the proposal will exceed outdoor open space requirements by 202 square feet. Allowing the dimensional reductions will essentially ensure an over provision of outdoor open space areas, improving the livability of both residents and visitors.

Lower Common Lawn Location (33.120.240.C.2.a)

Response: A modification is requested to increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet. This new standard is specific to PZC 33.120.230.C.2.a.

- A. *Better meets approval criteria. The resulting development will better meet the applicable approval criteria of Section 33.854.310; and*

Response: The proposal better meets applicable PD approval criteria (PZC 33.854.310.E through H) for the following reasons:

- **E.1** *Orienting the site and development to the public realm, while limiting less active uses of the site such as parking and storage areas along the public realm:* The lower common lawn is proposed near the northeast corner of the site, outside of the protected environmental resource areas. This area is behind the building and along a pedestrian pathway linking the lower lobby entrance to the surface parking areas. The request to locate the open space area farther away from the lower lobby main entrance will not impact the site orientation and relationship with the public realm.
- **E.2** *Preservation of natural features on the site, such as stands of trees, water features or topographical elements:* By placing the lower common lawn nearest to the surface parking area allows for the better protection of natural features. The lower lobby main entrance is planned near the eastern edge of the of the building. The entrance is approximately 10 feet away from the natural resource areas of the protected environmental tract. If the proposed lower common lawn was moved nearer to the lower lobby main entrance, additional impacts from fill and grading activities would result. Locating the proposed common area further away for a building entrance will allow for the full protection of natural resource areas and limit impacts associated with grading and fill. The proposal will positively impact preservation efforts.
- **E.3** *Inclusion of architectural features that complement positive characteristics of surrounding development, such as similar building scale and style, building materials, setbacks, and landscaping:* The request does not positively or negatively impact architectural features.
- **E.4** *Mitigation of differences in appearance through means such as setbacks, screening, landscaping, and other design features; & E.5* *Minimizing potential negative effects on surrounding residential uses:* Increasing the distance between the proposed open space area and the building entrance will not positively or negatively impact surrounding uses or contribute to mitigation measures to blend with existing development.
- **F.1** *Where proposed development includes attached houses, duplexes, triplexes, fourplexes, multi-dwelling structures, or multi-dwelling development, adequate open area to accommodate the proposed development must be provided. Open area does not include vehicle areas;* Although the request is for an increase in distance from a main entrance, by creating this open space area the proposal will exceed the open space minimum requirement by 202 square feet. With additional outdoor open space areas, this criterion will be positively impacted through the proposed modifications.
- **G.** *Accessible connections. Provide one or more accessible routes that connect all buildings on the site to adjacent streets, common open areas, and parking areas. Use*

landscaping and site furnishings to ensure the accessible route provides a pleasant user experience: The proposed open space will be adjacent to the surface parking and pedestrian path connecting the parking areas and the lower lobby main entrance (see Figure 10). The locational modification allows the open space area to be placed in a central location, along the main pathway between the lower lobby entrance and the surface parking areas. The common open space area will be accessible to all along this pathway. The resulting modifications will have a positive impact to this criterion.

- **H. Garbage and recycling areas.** *Garbage and recycling collection areas must be adequate in size to accommodate the proposed development, designed to encourage recycling, and located to facilitate pick-up service. Screening and buffering of garbage and recycling areas must be provided to maintain a clean and attractive development:* There will be no impacts to proposed garbage and recycling areas with the granting of this modification

B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standards for which a modification is requested.

Response: The purpose statement for this criterion is repeated below.

Purpose. The required outdoor and common area standards ensure opportunities for outdoor relaxation or recreation. The standards work with the building coverage and minimum landscaped areas standards to ensure that some of the land not covered by buildings is of adequate size, shape, and location to be usable for outdoor recreation or relaxation. The standards also ensure that outdoor areas are located so that residents have convenient access. Required outdoor areas are an important aspect in addressing the livability of a residential property by providing outdoor living opportunities, some options for outdoor privacy, and a healthy environment. These standards also allow for common area requirements to be met by indoor community facilities because they also provide opportunities for recreation and gathering.

The request to exceed the distance from a main door by 25 feet – from 20 to 45 feet – is requested to allow the creation of a second common open space area on a main floor. Without impacting environmental resources, the creation of the lower lawn in this area will allow the development proposal to exceed outdoor open space requirements by 202 square feet. Allowing the locational modification will essentially ensure an over provision of outdoor open space areas, improving the livability of both residents and visitors.

[Planned Development Review Summary](#)

The proposed PD modification to change interior floor plans and increase approved unit counts from 27 to 43 units requires a Type III review. To meet new RM2 standards associated with building façade and outdoor area requirements, four site-related development modifications are requested and have been justified to better meet the PD approval criteria while upholding the purpose of each standard.

The building design is similar to the mix of housing types found throughout the neighborhood, and impacts are mitigated through the site location, topography, landscaping, and design of the structure.

2.1 Partition Amendment – Review of Land Divisions in Open Space, Residential, and IR Zones (33.660)

33.660.120 Approval Criteria

The Preliminary Plan for a land division will be approved if the review body finds that the applicant has shown that all of the following approval criteria have been met. The approval criteria are:

Response: Staff has determined that only Standards K and L are applicable to the requested modification to approved PD.

K. Transportation impacts. The approval criteria of Chapter 33.641, Transportation Impacts, must be met; and,

Response: Approval criteria of Chapter 33.641 are addressed below.

33.641.020 Approval Criterion (Transportation Impacts)

- A. The transportation system must be capable of supporting the proposed development in addition to the existing uses in the area. Evaluation factors include safety, street capacity, level of service, connectivity, transit availability, availability of pedestrian and bicycle networks, on-street parking impacts, access restrictions, neighborhood impacts, impacts on pedestrian, bicycle, and transit circulation. Evaluation factors may be balanced; a finding of failure in one or more factors may be acceptable if the failure is not a result of the proposed development, and any additional impacts on the system from the proposed development are mitigated as required by 33.641.020.B.*
- B. Measures proportional to the impacts of the proposed use are proposed to mitigate on and off-site transportation impacts. [...]*

Response: Transportation impact studies are contained in **Appendix C**. An addendum to the previous transportation analysis dated January 28, 2020 was completed on August 22, 2022. This addendum explains that the additional 16 units are expected to net an increase of 50 weekday trips over the previously evaluated 27-unit scenario. Trips are expected to increase from 94 weekday total trips to 144 weekday total trips.

The addendum concludes:

“These impacts are relatively low, and the existing transportation infrastructure is expected to be minimally impacted. Accordingly, the new site trips generated by the project are not expected to significantly alter the operation or safety of the existing transportation facilities beyond those detailed in the January 2020 study.” (Appendix C, pdf page 3).

No mitigation measures are required or proposed, based on the conclusions of both transportation impact studies contained in Appendix C.

- L. *Services and utilities. The regulations and criteria of Chapters 33.651 through 33.655, which address services and utilities, must be met.*

Response: Approval criteria of Chapters 33.651 through 33.655 are addressed below.

33.651.020 Water Service Standard

Water service must meet the standard of this section. Adjustments are prohibited. The Water Bureau or District and the Fire Bureau have verified that water facilities with adequate capacity and pressure are available to serve the proposed development.

Response: A letter from the Water Bureau was provided in the early assistance meeting notes (EA 22-144899, **Appendix B**). Water availability and capacity is acceptable for the proposed unit increases. Civil details have remained unchanged with proposed unit increases.

33.652.020 Sanitary Sewer Service Standards

- A. *Availability of sanitary sewer. [...]*
- B. *Public sanitary sewage disposal. [...]*
- C. *Private sanitary sewage disposal. [...]*

Response: The Bureau of Environmental Services has confirmed sewer availability (EA 22-144899, **Appendix B**). A Public Works permit (EP555) for a combination sewer extension in SW Gibbs is currently under review. Civil details have remained unchanged with proposed unit increases.

33.653.020 Stormwater Management Approval Criteria

Stormwater management must meet the following approval criteria:

- A. *If a stormwater tract is proposed or required, an adequate amount of land and an appropriate location must be designated on the Preliminary Plan; and*
- B. *The application must show that a stormwater management system can be designed that will provide adequate capacity for the expected amount of stormwater.*

Response: A stormwater tract is not proposed in this application. The Stormwater Management Report in **Appendix D** outlines the design of the proposed stormwater system. Stormwater will be detained on site and discharged into the combined storm / sewer system on Gibbs Street, currently under review as Public Works permit (EP555).

33.654 Rights-of-Way

Response: A Public Works permit for the required frontage improvements has received 30% approval (see 19-193047 WT | TH0903). Portland Bureau of Transportation staff, Micheal Pina, has confirmed that no other frontage or ROW improvements are necessary. Connectivity, design standards, and ownership/maintenance/public use standards are all addressed in the previously approved PD findings (see LU #19-258445 PD, ENM, LDP).

33.655.100 School District Enrollment Capacity Standard

The school district within whose boundaries the land division site lies has verified that enrollment capacity for the district is adequate to serve the proposed development. Adjustments are prohibited.

Response: As required by PZC 33.705.020 (C)(1), a notice for a neighborhood meeting and the meeting follow-up notes were sent to Portland Public Schools representative Dana White, at both pubinfo@pps.net and dwhite2@pps.net. No public comments or attendance were received related to development's conflicts with enrollment capacity.

Part 3: Environmental Review and Tree Protection Standard

3.1 Environmental Review

An environmental review application will be approved if the review body finds that the applicant has shown that all of the applicable approval criteria are met. When environmental review is required because a proposal does not meet one or more of the development standards of Section 33.430.140 through .190, then the approval criteria will only be applied to the aspect of the proposal that does not meet the development standard or standards.

Response: The site contains an Environmental Conservation overlay. Environmental review modifications were requested and approved in 2020 (see LU 19-258445 PD LDP ENM). The entire resource area of the Environmental Conservation overlay is protected within a separate tract. The proposed PD modifications will not revise the building footprint or exterior façade, change any site access or circulation, and will not encroach into environmentally sensitive areas beyond what was approved during Environmental Review in 2020. As such, the proposed modifications meet all development standards, and a subsequent environmental review is not required.

3.2 Tree Preservation Standards (Title 11)

11.30.020 Description of Tree Permits

- A. *Generally. Tree permits are required for tree-related activities not associated with:*
 - [...]*
 3. *Tree plans or activities that require a development permit (see Chapter 11.50).*

Response: A development permit is required as part of this application. Chapter 11.50 Trees in Development Situations is reviewed as follows. Since approval of LU #19-258445 PD, ENM, LDP, additional tree removals within the ROW have been found by PBOT to be necessary. These additional tree removals are addressed in Public Works permit 19-193047 WT | TH0903, which has received 30% approval. Note that the Tree Preservation Plans (**Sheets L4-L5**) and the Arborist Report (**Appendix E**) show these additional ROW trees as preserved.

11.50.020 When a Tree Plan is Required

A tree plan is required in conjunction with all development permits, unless there are no Private Trees 12 inches or more in diameter, no City Trees 6 inches or more in diameter, and/or no Street Trees 3 inches or more in diameter, and the site or activity is exempt from Section 11.50.050 On-Site Tree Density Standards; and Section 11.50.060 Street Tree Planting Standards. If multiple development permits are required for a development proposal, including demolitions and subsequent construction, the same Tree Plan shall be included with each permit. For tree removal when no development permit is required, following completion of the development permit, or when tree preservation does not apply per Subsection 11.50.040 A.1., see Chapter 11.40.

Response: Tree details are contained on **Sheets L4-L5** and in **Appendix E**.

11.50.030 Development Impact Area Option for Large Sites and Streets

Where development is proposed on a site larger than one acre or where work is occurring in the street and is not associated with an adjacent development site, the applicant may choose to establish a development impact area. [...]

Response: The site is less than one acre and all development will be associated with the site. This standard does not apply.

11.50.040 Tree Preservation Standards

A. *Where these regulations apply.*

1. *This Section applies to trees within the City of Portland and trees on sites within the County Urban Pocket Areas in the following situations. On sites where these regulations do not apply, tree removal is subject to the requirements of Chapter 11.40, Tree Permit Requirements.*
 - a. *On sites. Development activities with ground disturbance or a construction staging area greater than 100 square feet on unpaved portions of the site within the root protection zone, as defined in Subsection 11.60.030 C.1.a., of one or more Private Trees 12 or more inches in diameter and/or one or more City Trees 6 or more inches in diameter.*
 - b. *In streets. Development activities with ground disturbance or construction staging not limited to existing paved surfaces where there are one or more Street Trees 3 or more inches in diameter.*
2. *Any Heritage Trees and trees required to be preserved through a land use condition of approval or tree preservation plan cannot be removed using the provisions in this Chapter, but may be counted toward the tree preservation requirements of this Section.*

B. *Exemptions. The following are exempt from the tree preservation standards of this Section:*

1. *On portions of sites located within an IH, IG1, EX, or CX zone.*
2. *On sites that are less than 5,000 square feet in area.*
3. *On sites that have existing or proposed building coverage of 85 percent or more.*

4. *Trees that are dead, dying, dangerous, or a nuisance species, as documented in a Tree Plan per Subsection 11.50.070 B. These are subtracted from the total number of trees to be addressed by the standards.*
5. *Trees exempted from this standard by a land use decision.*
6. *Tree preservation requirements approved in a land division or planned development review under Title 33, Planning and Zoning and the requirements of that review are still in effect.*
7. *Repair and replacement of existing fences and decks that are not changing in footprint or length when no trees are to be removed as a part of the project*

Response: This project proposes development on greater than 100 square feet of unpaved area and does not qualify for an exemption listed in this code section. Therefore, these criteria apply.

C. *Tree Preservation Requirement. Any trees preserved shall be protected in accordance with the specifications in Section 11.60.030. The regulations for Private Trees in Subsection 11.50.040 C.1. sunset after December 31, 2019. After December 31, 2019 the regulations in effect will be those in effect on January 1, 2015.*

1. *Private Trees.*

a. *General tree preservation.*

1. *Retention. An applicant shall preserve and protect at least 1/3 of the non-exempt trees 12 inches and larger in diameter located completely or partially on the development site, unless mitigation occurs per Subsection 11.50.040 C.1.a.(2) below. Retaining trees at least 6 and less than 12 inches in diameter that are documented in a report prepared by an arborist or landscape professional to be Garry Oak (*Quercus garryana*), Pacific Madrone (*Arbutus menziesii*), Pacific Yew (*Taxus brevifolia*), Ponderosa Pine (*Pinus ponderosa*), or Western Flowering Dogwood (*Cornus nuttallii*) species are not included in the total count of trees on the site but may be used toward meeting the preservation standard.*
2. *Mitigation. For each tree not preserved and protected below the 1/3 requirement, payment to the Tree Planting and Preservation Fund is required as shown in Table 50-1. The fee is calculated using the per-inch Restoration Fee for Tree Removal in the adopted fee schedule for Title 11. In cases where more than one tree is proposed for removal in excess of that allowed by Subsection 11.50.040 C.1.a.(1), the mitigation payment required to meet the 1/3 retention standard is based on the largest tree or trees proposed for removal.*

Response: Tree preservation techniques are outlined on the **Arborist Report included in Appendix E and Sheet L5.**

b. *Preservation of trees 36 inches or greater.*

Response: No trees 36 inches or greater are within or near development or disturbance areas on site. This standard does not apply.

- c. *Exception for Capital Improvement Projects. Trees on private property that are part of a capital improvement project and within the development impact area are regulated as City and Street Trees.*

Response: This project is not a capital improvement project. This standard does not apply.

2. *City and Street Trees.*

- a. *Retention. For development on City owned or managed sites, new public streets, or improvements to existing streets, applicants are required to consult with the City Forester at the preliminary project design phase if City or Street Tree removal is likely to occur to complete the project. The purpose of this consultation is to identify potential impacts and opportunities to retain existing trees, as well as any measures required to protect trees on site, on adjacent sites, or in the street.*
- b. *Mitigation. Any required mitigation specified below shall occur on the site, in the street planter strip, or in the same watershed either by planting or a payment into the Tree Planting and Preservation Fund. The City Forester may reduce or waive the following mitigation requirements.*
 1. *Approved Street Tree removal in conjunction with improvements to partially or fully unimproved streets. Each tree at least 12 inches in diameter that is allowed to be removed shall be replaced with at least one tree. Trees planted to meet Street Tree Planting Standards will be credited toward meeting this requirement.*
 2. *Any other Street or City Tree allowed to be removed that is 6 or more inches in diameter shall be replaced with at least one tree in addition to trees required to meet required tree density or Street Tree planting standards.*

Response: Retention of City and street trees is discussed in the **Arborist Report in Appendix E**. Mitigation and tree removal is shown on **Sheets L4-L5**. Additional tree removals within the ROW are associated with Public Works permit 19-193047 WT | TH0903 which has received 30% approval.

11.50.050 On-Site Tree Density Standards

- A. *Where these Regulations Apply. This Section applies to sites within the City of Portland and the County Urban Pocket Areas. Unless exempted in Subsection 11.50.050 B., the following are subject to the On-Site Tree Density Standards:*
 1. *New Development;*
 2. *Exterior alterations to existing development with a project valuation that is more than the threshold stated in Subsection 33.258.070 D.2.a.*

Response: This application proposes new development. These regulations apply.

B. *Exemptions*

1. *The following development activities are exempt from the on-site tree density standards:*
 - a. *A specific condition of land use review approval exempts the site from these density standards;*
 - b. *The site is within the Portland International Airport Plan District or Cascade Station/Portland International Center Plan District and is subject to the Airport Landscape Standards; see Title 33, Planning and Zoning.*
 - c. *On portions of sites located within an IH, IG1, EX, or CX zone.*
 - d. *Work conducted under Demolition, Site Development, Septic, Plumbing or Zoning Permits.*
2. *Sites with the following primary uses are exempt from the on-site tree density standards:*
 - a. *Railroad Yards;*
 - b. *Waste Related;*
 - c. *Agriculture;*
 - d. *Aviation and Surface Passenger Terminals;*
 - e. *Detention Facilities;*
 - f. *Mining;*
 - g. *Radio Frequency Transmission Facilities; or*
 - h. *Rail Lines and Utility Corridors;*

Response: The proposed development activity does not meet any of the exemptions listed above.

C. New development shall meet City specifications and standards in Chapter 11.60 and the on-site tree density requirements in Subsection D., below. Exterior alterations shall meet City specifications and standards in Chapter 11.60 and the on-site tree density requirements in Subsection D., below, but are only required to spend 10 percent of project value on the requirements in Subsection D. and the nonconforming upgrades required by Chapter 33.258, Nonconforming Situations.

Response: On-site tree density requirements of subsection D are reviewed below.

D. On-Site Tree Density Requirements.

1. *Required Tree Area. The required tree area is based on the size of the site and the type and size of proposed and existing development as shown in Table 50-2. Applicants may choose Option A or Option B for calculating required tree area except only Option A may be used to apply standards to a "Development Impact Area".*

Table 50-2 Determining Required Tree Area		
Development Type	Option A	Option B
One and Two Family Residential	40 percent of site or development impact area	Site area minus building coverage of existing and proposed development
Multi Dwelling Residential	20 percent of site or development impact area	
Commercial/Office/Retail/Mixed Use	15 percent of site or development impact area	
Industrial	10 percent of site or development impact area	
Institutional	25 percent of site or development impact area	
Other	25 percent of site or development impact area	

2. *Required Tree Density. The required tree area shall be planted with some combination of large, medium or small canopy trees at the following rates:*

Table 50-3 Number of Required Trees and Minimum Planting Area		
Canopy size category (at maturity)	Number of trees required per size of tree area	Min. required planting area per tree (min. dimension)
Large	1 per 1,000 s.f.	150 s.f. (10' x 10')
Medium	1 per 500 s.f.	75 s.f. (5' x 5')
Small	1 per 300 s.f.	50 s.f. (3' x 3')

Refer to Chapter 11.60, Technical Specifications, to calculate tree canopy size categories. When the canopy size category of the tree species is not or cannot be determined, the tree will be considered a small canopy tree.

Response: Tree density is discussed in the **Arborist Report in Appendix E.**

3. *Tree Density Credits*
 - a. *Trees planted on site to meet any required stormwater or other landscaping requirement may be counted toward the On-site tree density requirements.*
 - b. *Trees that are retained and protected, including trees preserved per Section 11.50.040, may be credited as follows:*

1. *Trees between 1.5 and less than 6 inches in diameter count as one small canopy size tree.*
2. *Trees 6 or more inches in diameter count as one medium canopy size tree for each full increment of 6 diameter inches.*
- c. *Payment in lieu of planting. The applicant may pay a fee to the Tree Planting and Preservation Fund per Section 11.15.010 equivalent to the cost of planting and establishing one 1.5-inch caliper tree. The fee per tree shall be credited at a rate of one medium canopy size tree.*
- d. *On sites less than or equal to 3,000 square feet, healthy non-nuisance species trees planted or retained in the street planting strip may be credited as described in this Subsection.*

Response: As shown on the **Landscape Plan on Sheet L4 and Tree Plan on Sheet L5**, and described in the **Arborist Report in Appendix E**.

11.50.060 Street Tree Planting Standards

- A. *Where these Regulations Apply.*
 1. *This Section applies to projects within or fronting on any City-owned or - managed streets.*
 2. *For alterations where the project value is more than \$25,000, the cost of required Street Tree improvements is limited to 10 percent of the value of the proposed development.*
- B. *Exemptions. The following are exempt from the Street Tree planting standards of this Section:*
 1. *Development activities associated with the following:*
 - a. *Additions, alterations, repair or new construction where the project value is less than \$25,000;*
 - b. *Activity that is limited to the street, and does not modify or create sidewalks, tree wells, or tree planting areas; or*
 - c. *Demolition Permits.*
 2. *Where physical constraints preclude meeting the Street Tree planting requirement because:*
 - a. *Existing above or below grade utilities prevent planting Street Trees; or*
 - b. *The design of the street will not accommodate Street Tree planting because the planting strip is less than 3 feet wide, there is not a planting strip, or there is insufficient space to add tree wells.*

Response: The site fronts a City-owned street. The proposed development does not meet the exemptions listed above, in standard B of this subsection. The criteria of this section apply and are reviewed as follows.

- C. *Street Tree Planting Requirement. Any proposed change in width in a public street right-of-way or any other proposed street improvement, including the development of new public streets, shall include areas for tree and landscape planting where practical. Utility connections and specifications for planting such areas shall be integrated into*

the site plan. Specific locations and species will be determined by the Responsible Engineer and City Forester. Planting in public streets shall meet the specifications in Chapter 11.60 and the following:

- 1. One Street Tree shall be planted or retained for each full increment of 25 linear feet per side of street frontage. When the required number of trees cannot be planted, a fee in lieu of planting may be required. For City projects, required trees that cannot be planted within the improvement area may be planted elsewhere in the same watershed, instead of paying a fee in lieu of planting.*
- 2. For projects affecting 200 linear feet of frontage or more, the applicant shall consult on the design of such improvements with the City Forester early in the project design phase to identify opportunities to integrate existing trees and maximize new Street Tree planting considering the planter width, the location of existing and proposed utilities, and visibility requirements.*
- 3. When new streets are being created in association with a land division, Street Tree planting may be deferred until the completion of the building permit on each new lot, subject to City Forester approval.*

Response: Street improvements are required by PBOT as part of this development proposal. The site has 200 linear feet of street frontage. Existing trees will be retained to the extent practicable, as described in the **Arborist Report in Appendix E** and additional landscaping and tree planting is shown on **Sheets L4 and L5**. Since approval of LU #19-258445 PD, ENM, LDP, additional tree removals within the ROW have been found by PBOT to be necessary. These additional tree removals are addressed in Public Works permit 19-193047 WT | TH0903, which has received 30% approval.

11.50.070 Tree Plan Submittal Requirements

A tree plan submittal shall include the following information. The tree plan information may be combined with other relevant plan sheets. The submittal shall include:

- A. Site Plan Requirements. The site plan shall include the following information with sufficient detail to show that the proposal complies with this Title.*
 - 1. Existing improvements;*
 - 2. Any construction staging areas on site;*
 - 3. Proposed alterations including structures, impervious area, grading, and utilities;*

Response: The existing conditions plan, architectural plan set, and engineering plan set includes existing improvements (**Sheet C1**), construction staging areas on site (**Sheets C7 & C8**), and proposed structure (**Sheet A1.00**), impervious area (**Sheet A1.00**), grading and utilities (**Sheets C2 – C6**).

- 4. Existing trees:*
 - a. Trees on the site. Indicate the location and the diameter size of:*
 - 1. Any Heritage Trees and trees required to be preserved as part of a condition of land use approval. These shall be clearly labeled.*

2. *All Private Trees at least 12 inches in diameter and all City Trees at least 6 inches in diameter located completely or partially on the site. On City-owned or -managed sites, the City Forester may require smaller size trees be shown.*
3. *Trees smaller than 6 inches in diameter shall be shown when proposed to be retained for tree density credit.*

Applicants using the development impact area option [...]

Response: There are no identified heritage trees on site. All private trees at least 12 inches in diameter and all City Trees at least 6 inches in diameter are identified on the **Tree Plan on Sheet L5** and described in the **Arborist Report in Appendix E**.

- b. *Trees in the street. For the street area adjacent to the development site or development impact area, indicate the location and the diameter size of:*
 1. *Any Heritage Trees and trees required to be preserved as part of a condition of land use approval*
 2. *All trees within the adjacent street that are at least 3 inches in diameter.*

Applicants using the development impact area [...]

Response: No heritage trees are located adjacent to the development site. The **Arborist Report in Appendix E**, and **Tree Plan on Sheet L5** show trees that are adjacent to the street.

5. *Proposed tree activity:*
 - a. *Indicate trees to be retained and proposed tree protection measures meeting the specifications in Chapter 11.60. Trees that are retained but are not protected in accordance with the protection requirements in Chapter 11.60 may not be used to meet preservation or density standards.*
 - b. *Indicate trees to be removed. It is the applicant's responsibility to obtain the appropriate consent from the adjacent property owner for tree removal when the tree is only partially on the site.*
 - c. *Show location, species, planting size and number of trees proposed to be planted. Trees to be planted shall meet the specifications in Chapter 11.60.*

Response: Trees that are to be retained and removed are indicated on the **Tree Plan on Sheet L5**, and the **Arborist Report in Appendix E**. Since approval of LU #19-258445 PD, ENM, LDP, additional tree removals within the ROW have been found by PBOT to be necessary. These additional tree removals are addressed in Public Works permit 19-193047 WT | TH0903, which has received 30% approval

B. Narrative requirements.

1. *If alternative tree protection measures are proposed, documentation addressing the requirements in Section 11.60.030, Tree Protection Specifications, shall be included.*

2. *If a tree is to be exempted from tree preservation standards based on poor tree health or condition, supporting documentation from an arborist shall be included.*
3. *If a tree is to be exempted from tree preservation standards based on it being listed on the Nuisance Plants List, supporting documentation from a landscape professional or an arborist shall be included.*

Response: The tree protection measures proposed will follow the performance path outlined in the standards of 11.60.030. These standards are discussed in the **Arborist Report in Appendix E.**

Conclusion

The requested Planned Development (PD) Review will modify approved land use review LU #19-258445 PD, ENM, LDP. The modification request will allow for internal floor plan reconfiguration to increase the previously approved unit count of 27 to 43 units. Some two-bedroom units are proposed to be converted to one-bedroom and studio apartments.

The previous design met PD standards while creating a tract to protect all resource areas on the site. Some R1 and R7 development standards were modified through environmental review to better protect the resource area. However, between the previous approval and now, the R1 zone was replaced with the RM2 zone. To meet new RM2 standards relating to outdoor area needs and address new RM2 façade articulation requirements, four PD modifications are requested.

The building envelope and façade are not proposed to change. Site access and circulation are not proposed to change. And the protected environmental tract created in a 2019 land division will remain unchanged. The proposal will increase bicycle parking from 32 to 51 spaces and increase required outdoor areas from 1,296 sq. ft. to 2,266 sq. ft.

We are proud to say the current proposal (with proposed modifications) meets all relevant code standards and helps achieve Portland's overall housing objectives while continuing to preserve and support sensitive natural resource areas within a protected environmental tract.

Memorandum



To: Leah Dawkins, Planner, Land Use Services Division, City of Portland

From: Jesse Winterowd, AICP, PMP, Managing Principal, Winterbrook Planning
Jaime Crawford, Associate Planner, Winterbrook Planning

Date: November 7, 2022

Re: **LU 22-187796 APD AP – Response to Incompleteness**

City of Portland Bureau of Development Services provided a Letter of Incompleteness on October 19, 2022, for a proposed Planned Development Amendment and Partition Amendment at 1311 SW Gibbs Street. This memorandum constitutes our written statement of response to the items contained in “Information Necessary to Complete Application” section. Content contained in Staff’s Letter of Incompleteness are referenced below in *italics*. Applicant responses are shown in **bold**.

1. *Planned Development Review Modification:*

- *Common Areas: The requested modification for the Lower Common Lawn requires additional information. Please provide details and an elevation drawing of the Lower Common Lawn, including any amenities provided and the landscape wall provided to separate the structured outdoor space from the slope to the north. An elevation drawing of this portion of the site will provide more detail showing the accessibility of the space from the structure and the usability of the space for building tenants. Provide detail on any outdoor furnishings, amenities, or way-finding signage proposed.*
- *Provide photos of the site in its current state, including the area proposed for the new Lower Common Lawn in order to contextualize the location of the common area within the environmental zone boundary as well as against the structure.*

Response: The proposed Lower Common Lawn is anticipated to be relatively flat, with 6-inch curbing and a ramp for access from the surface parking area. No additional site grading is expected. The attached “1. Common Area Context Site Photos and Grading” shows the proposed Lower Common Lawn in context with the previously approved site grading. As shown, there will be no additional disturbances to create the Lower Common Lawn.

Proposed outdoor furnishing will not be ground mounted. Furnishings will be unattached and intended to be moved around the common lawn to allow flexible use of the space. Please see “3.1 Product Sheet – KINN Chair” in the original submission.

2. *Bureau of Environmental Services has requested the following plan revisions:*

- *Revised plans must include the location of existing and proposed connections to the sewer for all lots.*
- *Revised plans and narrative must confirm that the proposed stormwater system is consistent with the approved stormwater management plan in CO 20-194259 or submit an updated stormwater management plan if necessary.*

Response: The attached “2. BES Sheet 2 of 5” is updated to show no changes in the point of connection, as approved in the BES Public Works approval. A statement has also been included to note that there will be

no increase in impervious surfacing. The stormwater management system will remain consistent with the approved stormwater management plan, approved in 20-194259-CO.

The application narrative references Public Works permit EP555 and the unrevised stormwater management plan referenced as “Appendix D - Stormwater Management Report” in the original submission.

3. *Site Development has requested the following information:*

- *The revision to the previously approved land use decision (LU #19-258445 PD, ENM, LDP) includes changes to the disturbed area of the site by increasing the Lower Common Lawn Dimensions and Location. Because the site is within the City’s Regulatory Landslide Hazard Area, these changes must be assessed at it relates to the Landslide Hazard Area Approval Criterion (PCC 33.632.100).*
- *An addendum to the Landslide Hazard Study performed under LU #19-258445 must be submitted that evaluates whether the proposed revisions to the previous approved land use decision meets the Landslide Hazard Approval Criterion.*

Response: With no proposed grading changes, Jed Stoken agreed in an email dated 11/2/2022 that no geotechnical addendum will be necessary. Attached is a PDF of that email exchange, included as “3. RE_LU 22-187796 - Completeness Review”.

A note has also been added to the Grading Plan, Sheet C6. Please referenced attached “3. Sheet C6_Grading plan confirmation”.

Thank you for your review,

Winterbrook Planning
610 SW Alder Street, Suite 810
Portland, OR 97205
503.827.4422 x 109
jesse@winterbrookplanning.com

Appendix A:

Neighborhood Outreach Documentation

Winterbrook (2022)

- **Request for Neighborhood Meeting**
 - A copy of the emails or letters that were sent as required by Paragraphs C.1. and C.4
 - A list of the email or postal addresses to which the email or letter required by Paragraph C.1. and C.4 were sent;
- **Evidence of Sign Posting**
 - A photograph of the sign required by Paragraph C.2. posted on the site;
- **Evidence of Online Access**
 - The applicant must contact the Bureau of Development Services via email, or an online information submittal tool, at least 35 days, but not more than one year, before applying for a land use review or building permit.
- **Neighborhood Meeting Notes and Follow-Up**
 - Notes from the meeting and an explanation of any changes made to the proposal as a result of comments received at the public meeting must be emailed or mailed to the neighborhood association, district neighborhood coalition, business association, school district and any meeting attendees who provide an email or postal address, before an application for a land use review or building permit can be accepted.
 - Includes meeting attendance log
- **Neighborhood Contact – Certification Statement**

Request for Neighborhood Meeting Contact III

DATE: 7/8/2022

TO:

Homestead Neighborhood Association
District Type: Neighborhood Association
Ed Fischer
contact@homesteadna.org
edfischer8@gmail.com
P.O. Box 80191
Portland, OR 97280

Southwest Hills Residential League SWHRL
District Type: Neighborhood Association within
400 feet of the site
landuse@swhrl.org
SWHRL, c/o Neighbors West-Northwest, 2257
NW Raleigh Street, Portland, OR 97210

Southwest Community Services
Leah Fisher
Leah.Fisher@portlandoregon.gov
503-865-6788

Neighbors West-Northwest
District Type: Neighborhood Coalition within
400 feet of the site
coalition@nwnw.org
2257 NW Raleigh Street
Portland OR 97210

Portland Public Schools
Dana White
pubinfo@pps.net and dwhite2@pps.net
501 N. Dixon St.
Portland, OR 97227

FROM:

Applicant's Representative
Jesse Winterowd / Winterbrook Planning
jesse@winterbrookplanning.com
610 SW Alder Street Suite 810
Portland, OR 97205
503-827-4422 x109

Applicant
Sean O'Neill / Scotia Western States Housing
sean@scotiawsh.com
15110 SW Boones Ferry Road, Suite 500
Lake Oswego, OR 97035
503-956-9307

RE: Request for a Neighborhood Meeting before submittal of a land use review or building permit application (Neighborhood Contact Requirement: Notification **33.705.020 (C)(1)**)

SITE: 1311 SW Gibbs St., R712893

Hello Neighborhood Representatives,

I am proposing a development project in your neighborhood. My project must comply with the Neighborhood Contact Requirement in the Portland Zoning Code Section 33.705. This letter provides preliminary information about my project as well as the neighborhood contact process and requirements.

LU 22-187796 APD AP
Exhibit A.3

City of Portland Neighborhood Contact Requirements (Portland Zoning Code Section 33.705.020 [C] [4]) requires applicants to request a public meeting from the neighborhood association, district neighborhood coalition, and business association for the area, and any association or coalition within 400 feet of the proposal site. The neighborhood association has 14 days to reply to this request. A neighborhood meeting must be scheduled within 45 days of this request date. Today is July 8, 2022.

The neighborhood association has until Friday, July 22, 2022, to respond to this request. The meeting must be held before Monday, August 22, 2022.

If the neighborhood association is unable to host a public meeting within this timeframe, a meeting will be scheduled by the applicant to satisfy the requirement. We look forward to discussing our project proposal with neighborhood representatives and interested parties.

SITE ADDRESS AND INTERSECTION: 1311 SW Gibbs St., nearest intersection at SW 13th Ave. and SW Gibbs St.



PROPOSAL: The proposal is to modify Land Use Case 2019-258445. We are requesting that the approved 27-unit multi-dwelling structure be modified to contain 43 units. This would be achieved through the conversion of some proposed 2- and 3-bedroom units to studio apartments. The building footprint and façade will remain unchanged. Site access and circulation will remain unchanged. The protected environmental tract will remain unchanged. Vehicle and bicycle parking is also proposed to remain unchanged.

A conceptual site plan is attached to this email request.

ADDITIONAL INFORMATION ABOUT THE NEIGHBORHOOD CONTACT REQUIREMENT

Neighborhood Contact Requirements

Read more about the Neighborhood Contact Requirements in the Portland Zoning Code Section 33.705 available online at <https://www.portlandoregon.gov/bps/article/748110>

Need More Information?

Contact the applicant listed above.

Call the City of Portland Zoning Information Hotline at (503) 823-7526.

Visit the Development Services Center (see www.portlandoregon.gov/bds for hours and location).

Read the Portland Zoning Code Section 33.705 <https://www.portlandoregon.gov/bps/article/748110>

From: [Ed Fischer](#)
To: [Jaime Crawford](#)
Subject: RE: Request for a Neighborhood Meeting: Modifications to Approved Development at 1311 SW Gibbs St.
Date: Monday, July 11, 2022 9:21:27 PM

Jaime,

I plan to schedule a special meeting via Zoom for August 2 (our normal 1st Tuesday) at 7 PM. I'll send you a link as soon as we can reestablish our link with Zoom. (It's a long story) I expect that before Thursday this week.

---Ed

From: Jaime Crawford <jaime@winterbrookplanning.com>
Sent: Friday, July 08, 2022 8:06 PM
To: Ed Fischer <edfischer8@gmail.com>
Cc: contact@homesteadna.org; landuse@swhrl.org; Fisher, Leah <Leah.Fisher@portlandoregon.gov>; coalition@nwnw.org; dwhite2@pps.net; pubinfo@pps.net; Milt Jones <mjones@miltjones.com>; Inning2 <inning2@comcast.net>; Sean O'Neill <sean@scotiawsh.com>; Jesse Winterowd <jesse@winterbrookplanning.com>; Gabe Headrick <gabe.headrick@steelheadarchitecture.com>
Subject: Re: Request for a Neighborhood Meeting: Modifications to Approved Development at 1311 SW Gibbs St.

Thank you for checking,

If the neighborhood association declines to host an event, we can schedule our own with the invitation to the neighborhood association, and hopefully, your member list.

I'm so sorry to disrupt any plans. We'd like to make this meeting as inclusive as possible, while adhering to the City's strict standards.

Please let us know if this is something that we should begin to schedule and coordinate as separate actors from the association. Thank you for your patience in this,
Jaime

On Jul 8, 2022, at 7:41 PM, Ed Fischer <edfischer8@gmail.com> wrote:

Jaime,

I will have to check with the Board of Directors, but at this point I am not inclined to hold a meeting in August. Our next meeting is now scheduled for September. If that does not change (and I don't expect it to) what does that do to you and your schedule?

---Ed

From: Jaime Crawford <jaime@winterbrookplanning.com>

Sent: Friday, July 08, 2022 6:10 PM

To: Ed Fischer <edfischer8@gmail.com>; contact@homesteadna.org; landuse@swhrl.org; 'Fisher, Leah' <Leah.Fisher@portlandoregon.gov>; coalition@nwnw.org; dwhite2@pps.net; pubinfo@pps.net; Milt Jones <mjones@miltjones.com>; 'Inning2' <inning2@comcast.net>

Cc: 'Sean O'Neill' <sean@scotiawsh.com>; Jesse Winterowd <jesse@winterbrookplanning.com>; 'Gabe Headrick' <gabe.headrick@steelheadarchitecture.com>

Subject: RE: Request for a Neighborhood Meeting: Modifications to Approved Development at 1311 SW Gibbs St.

Hi Ed,

Yes, we did touch on the proposed unit increase during June's meeting, but unfortunately the City requires us to have a more formal meeting, like you touch on below. I understand that the timing isn't the greatest, but hopefully we can find an evening in early August that works with a majority of the neighbors' schedules.

Sorry if there was any confusion in my previous email,
Jaime

From: Ed Fischer <edfischer8@gmail.com>

Sent: Friday, July 8, 2022 5:26 PM

To: Jaime Crawford <jaime@winterbrookplanning.com>; contact@homesteadna.org; landuse@swhrl.org; 'Fisher, Leah' <Leah.Fisher@portlandoregon.gov>; coalition@nwnw.org; dwhite2@pps.net; pubinfo@pps.net; Milt Jones <mjones@miltjones.com>; 'Inning2' <inning2@comcast.net>

Cc: 'Sean O'Neill' <sean@scotiawsh.com>; Jesse Winterowd <jesse@winterbrookplanning.com>; 'Gabe Headrick' <gabe.headrick@steelheadarchitecture.com>

Subject: RE: Request for a Neighborhood Meeting: Modifications to Approved Development at 1311 SW Gibbs St.

Jamie,

Didn't we discuss this at the June Homestead meeting? Wouldn't that qualify or does it need to be formal with minutes showing questions and discussion? We are not planning another meeting until September.

---Ed

From: Jaime Crawford <jaime@winterbrookplanning.com>

Sent: Friday, July 08, 2022 11:44 AM

To: edfischer8@gmail.com; contact@homesteadna.org; landuse@swhrl.org; 'Fisher, Leah' <Leah.Fisher@portlandoregon.gov>; coalition@nwnw.org; dwhite2@pps.net; pubinfo@pps.net

Cc: 'Sean O'Neill' <sean@scotiawsh.com>; Jesse Winterowd

<jesse@winterbrookplanning.com>; Gabe Headrick

<gabe.headrick@steelheadarchitecture.com>

Subject: Request for a Neighborhood Meeting: Modifications to Approved Development at 1311 SW Gibbs St.

Good Morning All,

We would like to formally request a meeting with the Homestead neighborhood association and other interested neighborhood representatives for a proposed development at 1311 SW Gibbs St. The proposal is to modify a previously approved multi-dwelling building (see Land Use Case 2019-258445). The proposal will convert some 2- and 3-bedroom units to studios. This will increase the unit count from 27 to 43. The building footprint, façade, protected environmental tract, and site access will all remain unchanged.

The proposed modifications require neighborhood contact III procedure. The neighborhood association has 14 days to respond to this request, and up to 45 days to schedule and hold a neighborhood meeting. The attached letter contains additional information about Portland's neighborhood contact requirements. We look forward to discussing the project with community members and interested parties.

Thank you for your collaboration,



Jaime Crawford | Associate Planner

610 SW Alder St. | Suite 810 | Portland, OR, 97205

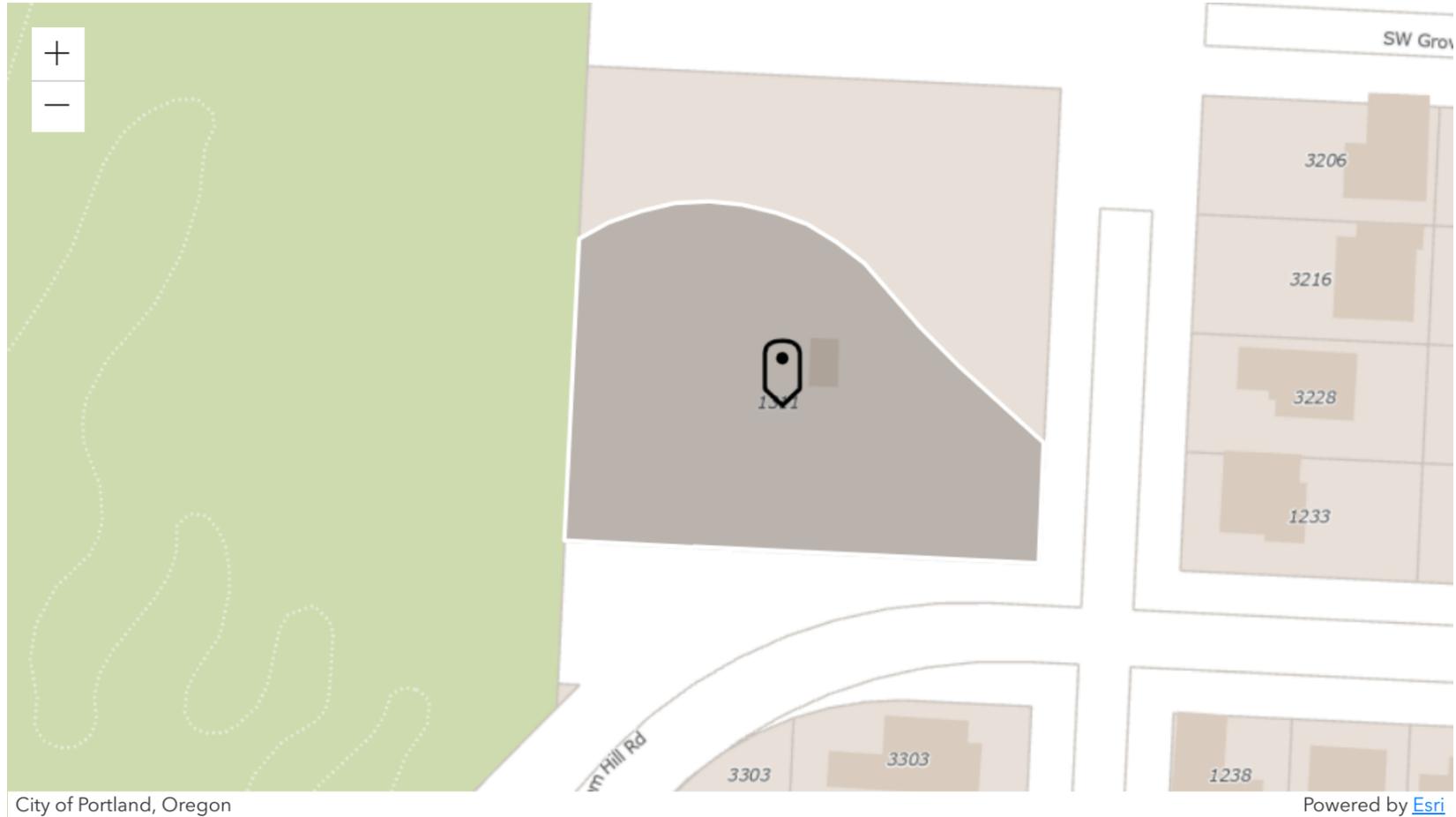
503.827.4422 ext. 107 | winterbrookplanning.com



Thanks!

Thank you for submitting the online application for your project. You may print this page for you records. You should also receive an email containing receipt. If you do not, please check your spam and / or junk folders.

Print



Project Location

Property ID: R712893
Portlandmaps.com
1311 SW GIBBS ST
Portland, OR
97239

Applicant Info

Sean O'Neill, Scotia Western States Housing
jaime@winterbrookplanning.com
(503) 956-9307

Project Info

Gibbs Development

Project size	47,040 ft ²
Date submitted	7/12/2022

The proposal is to modify Land Use Case 2019-258445. We are requesting that the approved 27-unit multi-dwelling structure be modified to contain 43 units. This would be achieved through the conversion of some proposed 2- and 3-bedroom units to studio apartments. The building footprint and façade will remain unchanged. Site access and circulation will remain unchanged. The protected environmental tract will remain unchanged. Vehicle and bicycle parking is also proposed to remain unchanged.

[Site Plan.pdf](#)

Associations and Districts

Neighborhood associations, neighborhood coalitions and business districts within 400' of the submitted site:

Neighbors West-Northwest

District type: Neighborhood Coalition
<http://www.nwnw.org>
coalition@nwnw.org
(503) 823-4288

Southwest Hills Residential League

SWHRL
District type: Neighborhood Association
www.swhrl.org
Correspondence to:
SWHRL, c/o Neighbors West-Northwest,

Required Public Meeting

Projects over 25,000 ft², or projects over 10,000 ft² with the Design (d) overlay zone are required to hold a public meeting. Please include the date, time, and location of the public meeting if required.

Date 8/2/2022, 7:00:00 PM
and
Time

Location Zoom

Notes MEETING TIME AND DATE TO REMAIN UNCHANGE ZOOM LINK TO BE CONFIRMED 7/14. PLEASE CHECK BACK. THIS NOTE WILL BE UPDATED. Join Zoom Meeting [https://us06web.zoom.us/j/83982535094?](https://us06web.zoom.us/j/83982535094?pwd=dGINSzJKZ2RWYzY4UjFibUZPbStsdz09)pwd=dGINSzJKZ2RWYzY4UjFibUZPbStsdz09 Meeting ID: 839 8253 5094 Passcode: 293442 One tap mobile +13462487799,,83982535094#,,,,*293442# US (Houston) +16699006833,,83982535094#,,,,*293442# US (San Jose) Dial by your location +1 346 248 7799 US (Houston) +1 669 900 6833 US (San Jose) +1 253 215 8782 US (Tacoma) +1 312 626 6799 US (Chicago) +1 929 205 6099 US (New York) +1 301 715 8592 US (Washington DC) Meeting ID: 839 8253 5094 Passcode: 293442 Find your local number: [https://us06web.zoom.us/j/83982535094?](https://us06web.zoom.us/j/83982535094?pwd=dGINSzJKZ2RWYzY4UjFibUZPbStsdz09)



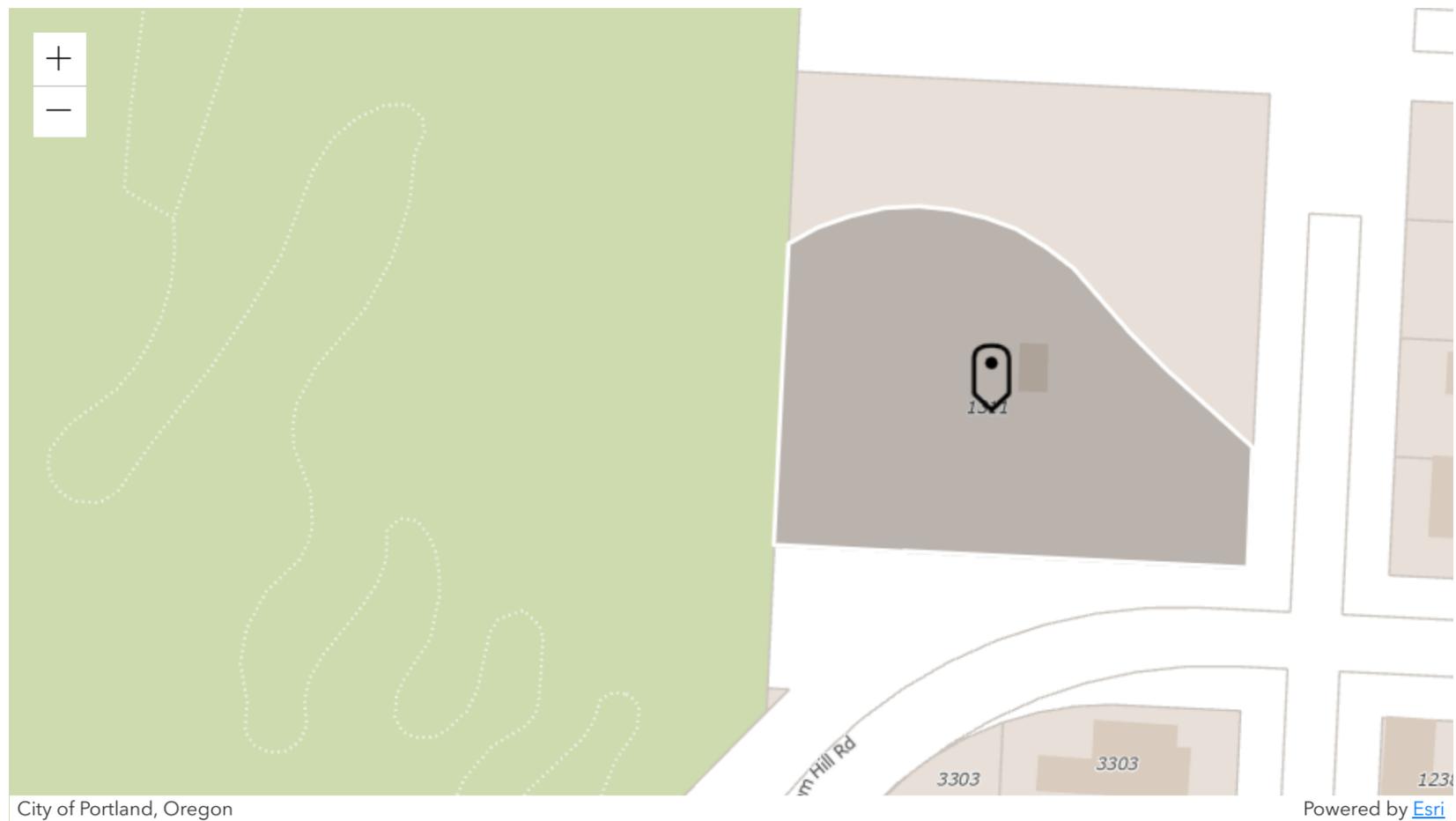
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503-823-7700 // bps@portlandoregon.gov // [@portlandbps](https://www.facebook.com/portlandbps) // [Facebook](#) // [Map and directions](#)

Thanks!

Thank you for submitting the online application for your project. You may print this page for your records. You should also receive an email containing receipt. If you do not, please check your spam and / or junk folders.

Print



Project Location

Property ID: R712893
Portlandmaps.com
1311 SW GIBBS ST
Portland, OR
97239

Applicant Info

Sean O'Neill, Scotia Western States Housing
jaime@winterbrookplanning.com
(503) 956-9307

Project Info

Gibbs Development

Project size	47,040 ft ²
Date submitted	7/19/2022

The proposal is to modify Land Use Case 2019-258445. We are requesting that the approved 27-unit multi-dwelling structure be modified to contain 43 units. This would be achieved through the conversion of some proposed 2- and 3-bedroom units to studio apartments. The building footprint and façade will remain unchanged. Site access and circulation will remain unchanged. The protected environmental tract will remain unchanged. Vehicle and bicycle parking is also proposed to remain unchanged.

[Site Plan.pdf](#)

Associations and Districts

Neighborhood associations, neighborhood coalitions and business districts within 400' of the submitted site:

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District type: Neighborhood Coalition
<http://www.nwnw.org>
coalition@nwnw.org
(503) 823-4288

Southwest Hills Residential League

SWHRL
District type: Neighborhood Association
www.swhrl.org
Correspondence to:
SWHRL, c/o Neighbors West-Northwest,

Required Public Meeting

Projects over 25,000 ft², or projects over 10,000 ft² with the Design (d) overlay zone are required to hold a public meeting. Please include the date, time, and location of the public meeting if required.

Date 8/2/2022, 7:00:00 PM
and
Time

Location Zoom

Notes UPDATED ZOOM LINK INFORMATION Homestead NA is inviting you to a scheduled Zoom meeting. Topic: Homestead NA meeting Time: Aug 2, 2022 07:00 PM Pacific Time (US and Canada) Join Zoom Meeting <https://us06web.zoom.us/j/85192962116?pwd=Z2NxSXU3TzNoSHJqZ2x5dXQyTW5aQT09> Meeting ID: 851 9296 2116 Passcode: 639330 One tap mobile +12532158782,,85192962116#,,,,*639330# US (Tacoma) +13462487799,,85192962116#,,,,*639330# US (Houston) Dial by your location +1 253 215 8782 US (Tacoma) +1 346 248 7799 US (Houston) +1 669 444 9171 US +1 669 900 6833 US (San Jose) +1 301 715 8592 US (Washington DC) +1 312 626 6799 US (Chicago) +1 646 931 3860 US +1 929 205 6099 US (New York) Meeting ID: 851 9296 2116 Passcode: 639330 Find your local number: <https://us06web.zoom.us/j/85192962116?pwd=Z2NxSXU3TzNoSHJqZ2x5dXQyTW5aQT09>



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503-823-7700 // bps@portlandoregon.gov // [@portlandbps](https://www.facebook.com/portlandbps) // [Facebook](#) // [Map and directions](#)

Neighborhood Meeting Notes and Follow-Up Contact III

DATE: 08/03/2022

TO:

Homestead Neighborhood Association
District Type: Neighborhood Association
Ed Fischer
contact@homesteadna.org
edfischer8@gmail.com
P.O. Box 80191
Portland, OR 97280

Southwest Hills Residential League SWHRL
District Type: Neighborhood Association
within 400 feet of the site
landuse@swhrl.org
SWHRL, c/o Neighbors West-Northwest, 2257
NW Raleigh Street, Portland, OR 97210

Southwest Community Services
Leah Fisher
Leah.Fisher@portlandoregon.gov
503-865-6788

Neighbors West-Northwest
District Type: Neighborhood Coalition within
400 feet of the site
coalition@nwnw.org
2257 NW Raleigh Street
Portland OR 97210

Portland Public Schools
Dana White
pubinfo@pps.net and dwhite2@pps.net
501 N. Dixon St.
Portland, OR 97227

FROM:

Applicant's Representative
Jesse Winterowd / Winterbrook Planning
jesse@winterbrookplanning.com
610 SW Alder Street Suite 810
Portland, OR 97205
503-827-4422 x109

Applicant
Sean O'Neill / Scotia Western States Housing
sean@scotiawsh.com
15110 SW Boones Ferry Road, Suite 500
Lake Oswego, OR 97035
503-956-9307

RE: Neighborhood Meeting held August 2nd, 2022, before submittal of a land use review or building permit application (Neighborhood Contact Requirement: Meeting **33.705.020 (C)(4)**):

Notes from the meeting and an explanation of any changes made to the proposal as a result of comments received at the public meeting must be emailed or mailed to the neighborhood association, district neighborhood coalition, business association, school district and any meeting attendees who provide an email or postal address, before an application for a land use review or building permit can be accepted.

SITE: 1311 SW Gibbs St., **R712893**

Hello Neighborhood Representatives,

Thank you for your attendance and questions regarding the proposed modifications to the previously approved Planned Development at 1311 SW Gibbs St. (Land Use Case 2019-258445). These notes and follow-up letter are meant to satisfy Neighborhood Contact Requirement III: Required Information.

A list of paraphrased questions and comments expressed by neighborhood residents and representatives during the meeting and our responses are contained in the table below.

Thank you all for your participation and continued interest in this project. We look forward to continued engagement with the neighborhood as the land use application and construction processes move forward.

PROPOSED PROJECT SUMMARY

The proposal is to modify Land Use Case 2019-258445. We are requesting that the approved 27-unit multi-dwelling structure be modified to contain 43 units. This would be achieved through the conversion of some proposed 2- and 3-bedroom units to one-bedroom and studio apartments. Market research and rental survey of our recent project at Marquam Heights has shown more demand in the area for studio and one-bedroom units than previously thought.

Additionally, parking demands in general are lower than we initially projected because of the high demand for housing for those working and attending school at OHSU – one of the largest employment centers Portland. Vehicle and bicycle parking is proposed to remain unchanged at 27 vehicle parking spaces.

The building footprint and façade will remain unchanged, site access and circulation will remain unchanged, and the protected environmental tract will remain unchanged.

Attendees	
Ed Fischer (edfischer8@gmail.com)	Jackie Phillips
Milt Jones (mjones@miltjones.com)	Ann Knedler (aknedler@yahoo.com)
Luke Rodriguez (luke.r.rodriguez@gmail.com)	Robert (Bob) Bonner (downhillhiker@gmail.com)
Mary E.	Jefferson Morgan (jeffmorgan85@gmail.com)
Michael Harrison (harmicha@ohsu.edu)	
Sean O'Neill (Development Team)	Jaime Crawford (Development Team)

Question/Comment	Response
Ed: (showed a plan illustrating gaps in sidewalk improvements along SW Gibbs St.) The project at the corner of 12th and Gibbs at the NE corner of intersection will be building a half a block of sidewalk within the orange portion of this map. From 12 th heading west, parking is allowed within gravel shoulder but there is not a lot of room for pedestrian and bicyclists.	PBOT has pushed back on us providing a sidewalk along our frontage on SW Gibbs. However, we are proposing a wide shoulder to accommodate pedestrian travel on our property. This should concept is not expected to change since our site design plans are at nearly 60% concept approval. A safe sidewalk along Gibbs will help not only existing neighbors, but our future tenants as well.

A lot of people are going to be walking on this narrow ROW that needs to be addressed. I agree that we don't need a sidewalk, but rather extra width without having pedestrians go out into the travel lane.	
Mary: Did the COVID years skew your sampling for parking demands for Marquam Heights?	We started to rent units in the Marquam Heights building towards the end of 2021 into this year.
Mary: Will affordable housing units increase with the increased units?	Yes – we will provide a mix of affordable units based on unit types. All affordable unit counts will increase from the previously approved affordable unit counts. (Development Team will share affordable housing worksheet that shows Portland's required calculations)
Mary: How will you keep cars from parking along the wider shoulder width? Is there going to be a parking barrier?	The shoulder width will be between 6 to 8 feet. This is a design "sweet spot" to make parking uncomfortable but allow breathing room for pedestrians (reference: a standard parking spot is 9 ft. wide by 18 ft.; a standard sidewalk without gutters is 5 ft. wide) If there are continued parking issues, we will have apartment manager enforcement.
Milt, Jackie: Pleased with development team's communications and that they share the same concerns with the neighborhood. Not pleased with system development charges collected from these new developments not being reinvested in the neighborhood. Michael: Happy to meet with others on how to brainstorm achieving a better pedestrian and bicycling environment.	We agree. We would like to help work with the neighborhood in their contact with the City to see what more can be done to improve pedestrian safety.
Bob: OHSU safety team was surprised that more permanent employees were living in the Marquam Heights apartments.	(comment)

ADDITIONAL INFORMATION ABOUT THE NEIGHBORHOOD CONTACT REQUIREMENT

Read more about the Neighborhood Contact Requirements in the Portland Zoning Code Section 33.705 available online at <https://www.portlandoregon.gov/bps/article/748110>

Need More Information?

Contact the applicant listed above.

Call the City of Portland Zoning Information Hotline at (503) 823-7526.

Visit the Development Services Center (see www.portlandoregon.gov/bds for hours and location).

Read the Portland Zoning Code Section 33.705 <https://www.portlandoregon.gov/bps/article/748110>



Neighborhood Contact – Certification Statement

The Neighborhood Contact requirement provides a way to learn more about a proposed development before a land use review or building permit is submitted. The applicant must provide documentation with the land use review or building permit application to document that Neighborhood Contact requirements are met. There are three different Neighborhood Contact processes, each requiring slightly different documentation. These requirements are listed below. This form can also be used to satisfy the requirements to submit a signed statement certifying certain requirements related to timelines and note distribution were met.

To complete this form:

1. Check the box that corresponds to the Neighborhood Contact process required.
2. Sign the bottom of this form to verify that timeline and note distribution requirements were met.
3. Submit the listed documentation requirements with the land use review or building permit application to verify other Neighborhood Contact requirements were met.

Neighborhood Contact 1

- A copy of the initial notification email or letter sent to the neighborhood association, district neighborhood coalition, and business association;
- A list of email or postal addresses to which the initial notification email or letter was sent;
- Photograph(s) of the sign(s) installed at the proposed development sites that legibly shows the sign’s text;
- A signed statement certifying that:
 - The initial notification email or letter was sent at least 35 days, but not more than 1 year, before applying for the land use review or building permit;
 - The required sign(s) was posted at least 35 days, but not more than 1 year, before applying for the land use review or building permit;

Neighborhood Contact 2

- A copy of the initial notification email or letter sent to the neighborhood association(s), district neighborhood coalition(s), and business association(s);
- A list of email or postal addresses to which the initial notification email or letter was sent;
- A copy of the attendance log that includes attendees’ name and address or email address;
- Photograph(s) of the sign(s) installed at the proposed development sites that legibly shows the sign’s text;
- A signed statement certifying that:
 - The initial notification email or letter was sent at least 35 days, but not more than 1 year, before applying for the land use review or building permit;
 - The required sign(s) was posted at least 35 days, but not more than 1 year, before applying for the land use review or building permit;
 - The required meeting was held at least 14 days before applying for the land use review or building permit and at least 14 days after sending the initial notification email or letter and posting the required sign(s); and
 - Notes from the public meeting were emailed or mailed to the neighborhood association, district neighborhood coalition, business association, school district and any meeting attendees who provided an email or postal address, prior to applying for the land use review or building permit.

continued on back

Neighborhood Contact 3

- A copy of the initial notification email or letter sent to the neighborhood association(s), district neighborhood coalition(s), business association, and school district;
- A list of email or postal addresses to which the initial notification email or letter was sent;
- A copy of the attendance log that includes attendees' name and address or email address;
- Photograph(s) of the sign(s) installed at the proposed development sites that legibly shows the sign's text;
- A signed statement certifying that:
 - The initial notification email or letter was sent at least 35 days, but not more than 1 year, before applying for the land use review or building permit;
 - The required sign(s) was posted at least 35 days, but not more than 1 year, before applying for the land use review or building permit;
 - The required meeting request was sent;
 - The neighborhood association either did not reply or declined the request, or that the neighborhood association meeting took place; and
 - Notes from the public meeting were emailed or mailed to the neighborhood association, district neighborhood coalition, business association, school district and any meeting attendees who provided an email or postal address, prior to applying for the land use review or building permit.

By filling out this form, I acknowledge the Neighborhood Contact requirements for the marked option above have been met.

Printed Name: _____ Date: _____

I acknowledge this typed name as my signature

Additional Printed Name: _____ Date: _____

I acknowledge this typed name as my signature

Appendix D:

Stormwater Management Report

Sisul Engineering

➤ **Preliminary Storm Drain Calculations**

- Date: November 13, 2019
- Revised: September 14, 2020

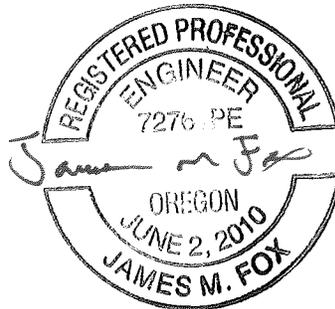
PRELIMINARY STORM DRAIN CALCULATIONS FOR

1325 SW Gibbs Street

Scotia Western State Housing, LLC

J.O. SGL 19-029

November 13, 2019
Revised September 14, 2020



EXPIRES: 12/31/ 20

9-14-20

SISUL ENGINEERING

A Division of Sisul Enterprises, Inc.

375 Portland Avenue
Gladstone, OR 97027
phone: (503) 657-0188
fax: (503) 657-5779

GOAL:

Stormwater runoff from the proposed buildings and shared private driveways will need to be managed. The goal of these calculations is to demonstrate that our stormwater facilities are designed to meet City of Portland water quality and infiltration requirements.

Existing Conditions:

Our project site is 1325 SW Gibbs Street, Tax lots 2800 and 2900. The site is currently developed with a residential care facility. There are two steeply sloped existing driveways along the street frontage heading down to an existing parking lot that is approximately 14 feet below the SW Gibbs street.

Steep slopes are found on all sides of the site outside of the leveled area for the current development. Various retaining and landscaping walls were used to construct the existing residential care facilities. Existing sewer and storm connections points are provided but do not conform to current city codes. The connection points will need to be modified.

Proposed Conditions:

For the proposed condition, the existing residential care facility and existing parking lot will be removed and replaced with a new driveway and apartment complex. The new driveway will be located on the western side of the frontage where better site distance was found. The driveway will be supported by retaining walls. The apartment complex will be composed of a new building with underground parking garage set next to Gibbs right of way as possible.

The building's rooftops will be designed to drain to the north towards the parking lot where stormwater will be combined with runoff from the driveway and parking lot. Stormwater will be directed through a mechanical stormwater device and then detained through an underground detention system. Stormwater is proposed to be piped back under the proposed building and discharged into a combined storm / sewer line located in Gibbs Street. To make stormwater drainage plan work with gravity flow the site's elevation will be raised with structural fill.

SOIL:

Hardman Geotechnical Services, INC performed a preliminary onsite geotechnical evaluation of the site in September 2019. The geotechnical evaluation recommends against onsite infiltration. For additional information see the attached copy of the geotechnical evaluation found under supporting datum dated November 12, 2019.

STORMWATER INFILTRATION AND HIERARCHY:

Based upon the stormwater management manual we are to select the highest technically feasible hierarchy possible. Under the current rating system 1 is the highest hierarchy and would require onsite infiltration. 4 would be the lowest hierarchy and allow offsite discharge to the combined sewer system. Below is our hierarchy reasoning for why our site fits a category 4 hierarchy and vegetated facilities are not feasible.

Category 1 requires onsite infiltration with vegetated infiltration facilities. The geotechnical report does not recommend stormwater infiltration for this site. In addition the site is located within 100 feet of slopes that exceed 20 percent and falls within potential landslide hazard zone per City of Portland maps. Infiltration is not feasible.

Category 2 requires total onsite infiltration with vegetated infiltration facilities that overflow to subsurface infiltration facilities or standalone subsurface infiltration facilities. Hierarchy category 1 explained why vegetated infiltration facilities were ill advised per the geotechnical report. Using a standalone subsurface infiltration facility does not change geotechnical reports conclusion that infiltration is not recommended. Hierarchy category 2 is not feasible.

Hierarchy category 3 requires onsite detention with vegetated facilities that overflow to a drainageway, stream, river, or storm-only pipe. For this site there is an existing drainage way in 13th Street that we could discharge into. However, that will not be allowed by BES because the drainage way discharges into combined public sewer further downstream. Hierarchy category 3 is not feasible.

Hierarchy category 4 requires onsite stormwater managing with a discharge to a combined sewer system. There is an existing combined sewer system located at SW 13th Avenue and SW Gibbs street. For this project we must extend the existing combined public sewer to the site's property line for sanitary discharge, which can also be utilized for stormwater discharge.

The combined sewer depth will be around 16 feet below Gibbs existing street grades. While our project site's existing grades are located around 14 feet below existing street grades. Gravity discharge to the combined public sewer extension is only feasible if structural fill can be used to raise the finish grade elevation of the site allowing a drop for water quality treatment and a storage depth for stormwater detention.

Vegetated facilities aren't practical on this site because of the limited storage depth and compact site plan. Vegetated facilities typically require around 43 inches of fall to account for storage volume, soil depth, and drain rock. Mechanical water treatment devices only required 18 inches of drop. Its not practical to use a vegetated stormwater water facility when it would require 2 feet of additional structural fill. Category 4 appears feasible assuming approval of mechanical water quality device, public combined sewer extension, and approval for landslide hazard zone to be filled.

AREA BREAKDOWN:

The proposed redevelopment will modify the sites impervious area. Below is a table of the existing and redeveloped areas. With steep slopes surrounding the site most pervious areas won't be collected or modified and thus not included in area breakdown. The post developed landscape areas are designed to drain into the proposed stormwater facility and note as pervious area.

Existing Impervious Area Table:

Existing Driveway in Gibbs ROW	2,476 sf	0.057 acres
Existing buildings	4,502 sf	0.103 acres
Existing dwy / parking	8,337 sf	0.191 acre
Exist Concrete Patios	<u>1,989 sf</u>	<u>0.046 acres</u>
Total:	17,304 sf	0.397 acres

Existing Pervious Area Table:

Existing Vegetation	886 sf	0.02 acres
---------------------	--------	------------

Note Existing Basin Area = Impervious + Pervious = 17,304 sf + 886 sf = 18,190 sf

Developed Impervious Area Table:

Private Driveway Gibbs ROW	1,181 sf	0.027 acres
Building '1' Roof top:	9,448 sf	0.217 acres
Hard surfaces (asphalt / concrete):	<u>5,904 sf</u>	<u>0.136 acres</u>
Total Impervious:	16,533 sf	0.380 acres

Private driveway in public R.O.W note: The private driveway area in Gibbs row only represents the impervious area that will drain to the proposed private stormwater facility. The private driveway generates an additional 198 sf of impervious area that will remain in the public row and not included in calculations.

Developed Pervious Area Table:

Landscape Planters	357 sf	0.008 acres
Existing Pervious Areas	<u>1,300 sf</u>	<u>0.030 acres</u>
Total:	1,657 sf	0.038 acres

Note Developed Basin Area = Impervious + Pervious = 16,533 sf + 1,657 sf = 18,190 sf

This development will increase the onsite impervious from 14,828 sf to 15,352 sf, for an increase of 524 sf of impervious area. In return the impervious area in the public R.O.W. used for private development will be decreased by 1,097 sf from 2,476 sf to 1,379 sf. This project produces a net reduction of 573 sf of impervious area for private development. Area table do not account for impervious area generated by required public improvements.

WATER QUALITY CALCULATIONS:

For water quality requirements we are proposing the use of a City of Portland approved manufactured stormwater treatment device. The stormwater runoff from this development's impervious area will be collected and conveyed to the proposed parking lot where a manufactured treatment device will provide water quality treatment.

For the water quality calculations, we are going to assume the impervious area generated by the proposed private driveway in Gibbs right of way will be allowed to flow onto private property down to the manufactured treatment device in the parking lot.

Per 2016 SWMM page 1-40 90 percent treatment for SCS Type 1A rainfall distribution is 0.83 inches per hour over 24 hours.

"24-hour rainfall depths" are as follows:

Pollution Event – 0.83 inches / hour

For calculations we used HydroCAD 10.00-25 to determine the peak water quality flow. The peak water quality flow was determined to be 0.06 CFS. See attached printout for further details.

Peak Water Quality flow = 0.06 CFS

For preliminary calculations we will assume the manufactured device will be a Bayfilter (48-1 manhole) with 530 cartridges to minimize the drop. Per the 2016 Stormwater Management Manual approval list, last updated February 2019, Bayfilter 530 cartridges are approved for a treatment rate of 30 gpm.

$$\text{Cartridge Treatment rate} = \frac{30 \text{ gpm} * 1 \text{ min} * 1 \text{ cf}}{60 \text{ sec} * 7.48 \text{ g}} = 0.066 \text{ cfs}$$

Now we will determine how many cartridges are necessary to treat the water quality event.

$$\text{Cartridge} = \frac{\text{Peak Flow} * \text{Cartridge}}{\text{Treatment Rate}} = \frac{0.06 \text{ cfs} * 1 \text{ cartridge}}{0.066 \text{ cfs}} = 0.91 \text{ cartridges}$$

Required Cartridges = **1 cartridges**

The peak water quality event can be treated with a Bayfilter 48-1 manhole system containing one 530 cartridge. Larger storm events will be by-passed through a 10" overflow standpipe.

DETENTION CALCULATIONS:

Detention Requirements:

Per Table 1-3 of the 2016 Stormwater Management Manual we are required to meet the following detention requirements for a category 4 hierarchy:

- 25-year post-development peak runoff rate to 10-year pre-development peak rate.

Existing and Proposed Hydrographs.

All the impervious area from the site will pass through the Bayfilter 48-1 water quality manhole before being discharged into the detention facility. For detention we are proposing to use 6- SC 740 Stormtech chambers. To start we will compute the existing hydrographs for a SCS Type 1A, 24-hour storm event, for a 10-year event through HydroCAD version 10.00-25. Next, we will compute a 25-year events and route it through our stormwater detention system.

The following assumptions and values were used to compute the hydrographs.

Per Table A-1 “24-hour rainfall depths” are as follows:

10-year - 3.4 inches / hour
25-year - 3.9 inches / hour
100-year 4.4 inches / hour

Per table A-2 “Runoff Curve Numbers for urban areas” are as follows:

Existing Pervious Area = 73 (wood with some forest litter)
Developed Previous Area = 79 (open space fair condition)
Impervious Area = 98 (rooftops, driveways, paved parking, etc.)

Please note runoff curve values are based upon the assumption of Type ‘C’ Soil. The City of Portland online maps indicate the soil type is hydraulic group ‘C’.

Time of concentration

Time of concentration is typically based upon the hydrological longest path. In our case this site was previously developed disturbing original drainage paths. For existing conditions, we will assume a constant slope down the paved driveways with a second slope across the parking lot to the drainage ditch.

Time of Concentration – formula

$$T_{c1} = \frac{0.42 (nL)^{0.8}}{1.58 (s)^{0.4}} = \frac{0.42 (0.011*136)^{0.8}}{1.58 (0.155)^{0.4}} = \mathbf{0.775 \text{ minutes}}$$

Length L = 136 feet,
Manning n = 0.011 (forest)

slope $s = (663-641) / 135 = 0.154$ feet/foot

$$T_{c2} = \frac{0.42 (nL)^{0.8}}{1.58 (s)^{0.4}} = \frac{0.42 (0.011*71)^{0.8}}{1.58 (0.014)^{0.4}} = 1.20 \text{ minutes}$$

Length $L = 71$ feet,

Manning $n = 0.011$ (forest)

slope $s = (641-640) / 71 = 0.014$ feet/foot

$T_c = 5$ minutes (minimum)

Time of Concentration – Developed Conditions

$T_c = T_{c1} + T_{c2} = 1.98$ min. use **5 minutes (minimum)**

For calculations see Hydrocad printouts provided is supporting data

Pre and Post Developed Flow Rate Table

Storm Event	Predeveloped Flow Rate (CFS)	Post developed Flow Rate (CFS)	Target Release Rate
10	0.32	-	0.32
25	-	0.36	0.32
100		0.41	0.41

Detention Routing Calculations:

For the new stormwater facility, we are proposing to use 6 SC-740 StormTech chambers to detain the stormwater runoff from this redevelopment. Stormwater will travel through a Bayfileter 48-1 water quality manhole into the SC-740 StormTech chambers isolation row. Next stormwater will drain into the rock base, and flow to rock drain connected to the flow control manhole. Larger events will fill the chambers and be discharge through a standard outlet pipe to the flow control manhole. The proposed stormwater facility will be fully lined with a waterproof liner to prevent stormwater infiltration.

The Stormtech chambers detention volume and orifice release rates were calculated in HydroCAD by HydroCAD version 10.00-25. See HydroCAD printout in the supporting data.

Detention Summary:

Based upon preliminary HydroCAD calculations we are meeting Portland's requirements by detaining the 25-post developed to a 10-year predeveloped storm event with the use of 6 SC-740 StormTech chambers and a flow control manhole. The StormTech chambers will be fully wrapped with a waterproof liner to prevent infiltration

The flow control manhole will have an 8-inch riser with 2 orifices. The bottom horizontal orifice will be 1.7 inches in diameter set at plan elevation 634.56. The second orifice will be 3-inches in diameter and set at plan elevation 636.56. The riser top will be set to act as an overflow weir for the 100-year storm event or clogging of the orifices.

Detained Flow Rate Table

Storm Event	Postdeveloped Flow Rate (CFS)	Target Flow Rate (CFS)	Actual Release Rate
25	0.37	0.31	0.31
100	0.41	0.41	0.36

Draw Down Time:

Portland codes requires that stormwater facility have a draw down time of 30 hours or less from the peak level. Our stormwater facility reaches the elevation at 637.64 feet at the 8.00 hour mark on the 25-year storm event.

Using HydroCAD hydrograph printouts data out we can determine the stormwater facility will be completely drawn down at hour 25.

Drawn down time = Final hour – Peak hour = 25 – 8 = **17 hours**

The draw down time meets City of Portland standards because it is less than 30 hours from the facility peak stage.

100 YEAR STORM EVENT:

The City of Portland requires an escape route for larger storm events (100-year). In the sites vicinity there is an accessible drainage way for stormwater disposal. The proposed flow control manhole riser will be constructed with overflow weir capable of conveying a 100-year storm event. The flow control manhole riser will have a secondary high flow outlet that can discharge larger storm events to the natural resource area.

SUMMARY:

Our designed stormwater facilities either meets or exceed City of Portland’s stormwater design requirement. Water quality treatment is met using a mechanical treatment device. Hierarchy category 4 detention requirements are met by an underground detention system that discharge by gravity flow to the combined sewer extension in Gibbs street.

SUPPORTING DATA



November 12, 2019
HGSI Project No. 19-2514

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**Subject: GEOTECHNICAL ENGINEERING REPORT
1325 SW GIBBS STREET
PORTLAND, OREGON**

This report presents the results of a geotechnical engineering study conducted by Hardman Geotechnical Services Inc. (HGSI) for the above-referenced project. The purpose of this study was to evaluate subsurface conditions at the site and to provide geotechnical recommendations for site development. This geotechnical study was performed in accordance with HGSI Proposal No. 19-649 dated September 11, 2019, and your subsequent authorization of our proposal and *General Conditions for Geotechnical Services*.

Site development plans are in progress. Grading analysis is ongoing to address the area of the proposed parking lot entrance from SW Marquam Hill Road where a steep slope is present and a retaining wall may be planned. Additional geotechnical input will be needed as the grading / retaining wall plans are developed.

SITE DESCRIPTION AND PROPOSED DEVELOPMENT

The property is about 40,000 square feet total, and rectangular in shape. The site is currently occupied by an existing care facility, located in the southern portion of the overall site. The remainder of the property is vegetated with trees and undergrowth. The area of proposed development is about 15,000 square feet total, within the approximate area of the existing facility.

The site slopes moderately to steeply down to the north. City of Portland maps the majority of the site as being within a "Steep Slope" area, defined as grades 25% and steeper. The site is also mapped as being within a Potential Landslide Hazard area per the City of Portland. A Landslide Hazard Study will be required by the City due to the hazard mapping.

Two residential structures are planned in the vicinity of the existing parking lot and a parking lot is planned in the vicinity of the existing building which will be demolished. The proposed buildings will be a maximum of 3 stories in height, of conventional timber construction. A new entrance is planned in the southwest corner of the site. This area is steeply sloped and we anticipate a retaining wall may be necessary to support the driveway. Appurtenant facilities would include underground utilities and stormwater facilities.

REGIONAL GEOLOGY AND SEISMIC SETTING

The subject site lies within the Portland Basin, a broad structural depression situated between the Coast Range on the west and the Cascade Range on the east. The Portland Basin is a northwest-southwest trending structural basin produced by broad regional downwarping of the area. The Portland Basin is approximately 20

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LU 22-187796 APD AP
Exhibit A.4

November 12, 2019
HGSI Project No. 19-2514

miles wide and 45 miles long and is filled with consolidated and unconsolidated sedimentary rocks of late Miocene, Pliocene and Pleistocene age.

The subject site is underlain by Quaternary age (last 1.6 million years) loess, a windblown silt deposit that mantles older deposits, basalt bedrock, and elevated areas in the Portland region (Beeson et al., 1989; Madin, 1990). The loess generally consists of massive silt deposits following repeated catastrophic flooding events in the Willamette Valley, the last of which occurred about 10,000 years ago. In localized areas, the loess includes buried paleosols that developed between depositional events. Regionally, the total thickness of loess ranges from 5 feet to greater than 100 feet.

Underlying the Willamette Formation is Miocene aged (about 14.5 to 16.5 million years ago) Columbia River Basalt, a thick sequence of lava flows which forms the basement of the basin.

At least three major seismic source zones capable of generating damaging earthquakes are known to exist in the region. These include the Portland Hills Fault Zone, Gales Creek-Newberg-Mt. Angel Structural Zone, and the Cascadia Subduction Zone. These potential earthquake source zones are included in the determination of seismic design values for structures, as presented in the *Seismic Design* section.

FIELD EXPLORATION

The site-specific exploration for this study was conducted on September 27, 2019 and consisted of two borings (designated B-1 and B-2) excavated to depths of 41 and 50 feet below ground surface (bgs). Exploration locations are shown on the attached Site Plan, Figure 2. It should be noted that exploration locations were determined in the field by pacing or taping distances from apparent property corners and other site features shown on the plans provided. As such, the locations of the explorations should be considered approximate.

The boreholes were drilled using a trailer mounted drill rig and solid stem auger methods. SPT (Standard Penetration Test) sampling was performed in general accordance with ASTM D1586 using a 2-inch outside diameter split-spoon sampler and a 140-pound hammer equipped with a rope and cathead mechanism. During the test, a sample is obtained by driving the sampler 18 inches into the soil with the hammer free-falling 30 inches. The number of blows for each 6 inches of penetration is recorded. This resistance, or N-value, provides a measure of the relative density of granular soils and the relative consistency of cohesive soils. At the completion of the borings, the holes were backfilled with bentonite chips.

Explorations were conducted under the full-time observation of HGSI personnel. Soil samples obtained from the borings were classified in the field and representative portions were placed in relatively air-tight plastic bags. These soil samples were then returned to the laboratory for further examination. Pertinent information including soil sample depths, stratigraphy, soil engineering characteristics, and groundwater occurrence was recorded. Soils were classified in general accordance with the Unified Soil Classification System.

Summary exploration logs are attached to this report. The stratigraphic contacts shown on the individual borehole logs represent the approximate boundaries between soil types. The actual transitions may be more gradual. The soil and groundwater conditions depicted are only for the specific dates and locations reported, and therefore, are not necessarily representative of other locations and times.

SUBSURFACE CONDITIONS

The following discussion is a summary of subsurface conditions encountered in our explorations. For more detailed information regarding subsurface conditions at specific exploration locations, refer to the attached

exploration logs. Also, please note that subsurface conditions can vary between exploration locations, as discussed in the *Uncertainty and Limitations* section below.

Soil

On-site soils are anticipated to consist of undocumented fill, topsoil, Portland Hills Silt, residual soil, and basalt as described below.

Undocumented Fill – Undocumented fill was observed in both subsurface explorations and extended to depths of about 14 feet bgs. Fill depth is anticipated to decrease south of our explorations. Localized areas of undocumented fill may be present in areas beyond our exploration locations.

Topsoil – Topsoil was not encountered. Where present, we anticipate it will extend to an average depth of about 9 to 12 inches. Topsoil generally consisted of dark brown, organic silt (OL).

Portland Hills Silt – Underlying the undocumented fill, explorations encountered medium stiff to stiff, silt consistent in appearance to Portland Hills Silt, a Pliocene to Pleistocene age loessal deposit. These soils extended to an average depth of 18 feet bgs.

Residual Soil – Underlying the Portland Hills Silt, explorations encountered medium stiff to hard silt and clay consistent in appearance to residual soil formed by the in place weathering of Columbia River Basalt. These soils extended to a depth of 39 feet in boring B-1 and 49 feet in boring B-2.

Basalt – Underlying the residual soil, borings B-1 and B-2 encountered basalt bedrock. Auger refusal and SPT blow counts greater than 50 indicate a hard, rock layer was encountered at depths of 41 and 50 feet bgs in B-1 and B-2 respectively.

Groundwater

Groundwater was not observed during our site exploration. No seeps or springs were observed. It is anticipated that groundwater conditions will vary depending on the season, local subsurface conditions, changes in site utilization, and other factors. United States Geological Survey mapping of the subject area indicates groundwater lies at a depth of 280 to 300 feet below the ground surface (Snyder, 2008).

CONCLUSIONS AND RECOMMENDATIONS

Results of this study indicate that the proposed development is geotechnically feasible, provided that the recommendations of this report are incorporated into the design and construction phases of the project. As part of the Landslide Hazard Study, HGSI presents the following section addressing slope stability geotechnical engineering considerations. Recommendations are also presented below regarding site preparation, engineered fill, wet weather earthwork, building pad preparation - partial removal / Replacement with crushed rock, structural foundations, below grade structural retaining walls, concrete slabs-on-grade, perimeter footing drains, seismic design, excavating conditions and utility trench backfill, stormwater systems, and erosion control considerations.

Slope Stability – Geotechnical Engineering Considerations

For the purpose of evaluating slope stability, we reviewed published geologic and hazard mapping, reviewed regional site topography and LiDAR images, performed a field reconnaissance, and evaluated subsurface soil conditions in exploratory borings.

November 12, 2019
HGSI Project No. 19-2514

Reconnaissance observations indicate that the topography at the site is generally smooth and uniform, consistent with stable slope conditions. No geomorphic evidence of prior slope instability (such as hummocky topography, benches or old scarps) was observed. No seeps or springs were observed on site.

Regional geologic mapping and the Oregon Department of Geology and Mineral Industries online landslide database (SLIDO) show no mapped landslides, either on or in the vicinity of the subject site (Madin, 1990; DOGAMI, 2017).

To our knowledge, the planned development does not involve any significant cuts or fills, other than the excavation needed for the planned structure and grading for the entrance driveway. Based on our observations and results of the slope stability evaluation, it is our opinion that no special design or construction provisions are needed to address slope issues on the site. Development of the site is not anticipated to have negatively impact slope stability of the site or adjacent properties. The project will be designed and constructed per current building codes, City of Portland requirements, and the current standard-of-practice in geotechnical engineering. As such, it is our opinion that adequate slope stability factors of safety will be maintained for both temporary construction, and long-term conditions.

We understand that the proposed storm water management plan may consist of flow through planters, with overflow to existing City storm or sewer systems. Significant infiltration of stormwater via stormwater chambers or dry wells is not proposed for this site. The most likely storm water facilities (flow through planters) are not anticipated to impact slope stability on site, or to create any unstable conditions. Storm water management systems should be designed such that potential overflow is discharged in a controlled manner away from structures and slopes, and all systems should include an adequate factor of safety.

Site Preparation

The areas of the site to be graded should first be cleared of vegetation and any loose debris; and debris from clearing should be removed from the site. Organic-rich topsoil should then be removed to competent native soils. We anticipate that the average depth of topsoil stripping will be 9 inches over most of the site. The final depth of stripping removal may vary depending on local subsurface conditions and the contractor's methods, and should be determined on the basis of site observations after the initial stripping has been performed. Stripped organic soil should be stockpiled only in designated areas or removed from the site and stripping operations should be observed and documented by HGSI. Existing subsurface structures (tile drains, old utility lines, septic leach fields, etc.) beneath areas of proposed structures and pavement should be removed and the excavations backfilled with engineered fill.

Due to the depth of existing fill on site, HGSI recommends a partial overexcavation of 3 feet and backfill with compacted crushed rock beneath building pad areas. Additional recommendations are presented in a separate report section below.

In construction areas, once stripping has been verified, the area should be ripped or tilled to a depth of 12 inches, moisture conditioned, and compacted in-place prior to the placement of engineered fill. Exposed subgrade soils should be evaluated by HGSI. For large areas, this evaluation is normally performed by proof-rolling the exposed subgrade with a fully loaded scraper or dump truck. For smaller areas where access is restricted, the subgrade should be evaluated by probing the soil with a steel probe. Soft/loose soils identified during subgrade preparation should be compacted to a firm and unyielding condition or over-excavated and replaced with engineered fill, as described below. The depth of overexcavation, if required, should be evaluated by HGSI at the time of construction.

Engineered Fill

In general, we anticipate that on-site soils will be suitable for use as engineered fill in dry weather conditions, provided they are relatively free of organics and are properly moisture conditioned for compaction. Imported fill material must be approved by the geotechnical engineer prior to being imported to the site. Oversize material greater than 6 inches in size should not be used within 3 feet of foundation footings, and material greater than 12 inches in diameter should not be used in engineered fill.

Engineered fill should be compacted in horizontal lifts not exceeding 8 inches using standard compaction equipment. We recommend that engineered fill be compacted to at least 90 percent of the maximum dry density determined by ASTM D1557 (Modified Proctor) or equivalent. On-site soils may be wet or dry of optimum; therefore, we anticipate that moisture conditioning of native soil will be necessary for compaction operations.

Proper test frequency and earthwork documentation usually requires daily observation and testing during stripping, rough grading, and placement of engineered fill. Field density testing should conform to ASTM D2922 and D3017, or D1556. Engineered fill should be periodically observed and tested by HGSI.

Wet Weather Earthwork

The on-site soils are moisture sensitive and may be difficult to handle or traverse with construction equipment during periods of wet weather. Earthwork is typically most economical when performed under dry weather conditions. Earthwork performed during the wet-weather season will probably require expensive measures such as cement treatment or imported granular material to compact fill to the recommended engineering specifications. If earthwork is to be performed or fill is to be placed in wet weather or under wet conditions when soil moisture content is difficult to control, HGSI should be contacted for additional recommendations.

Under wet weather, the construction area will unavoidably become wet and the condition of exposed fill and native soils will degrade. To limit the impacts of wet weather on the finished building pad surface, consideration may be given to placement of a crushed aggregate pad. Where used, we recommend the working pad be constructed using 1½"-0 crushed aggregate, and should have minimum thickness of at least 12 inches. This thickness is considered adequate to support light construction traffic, but will not be sufficient to support heavy traffic such as loaded dump trucks or other heavy rubber-tired equipment.

Building Pad Preparation - Partial Removal / Replacement with Crushed Rock

For building pad preparation, we recommend the undocumented fill soils be removed to a minimum depth of 3 feet below bottom of footing elevation and backfilled with compacted crushed rock. Removals should extend beyond the foundation edges at least 2 feet in all directions. The removal zone should be backfilled using ¾"-0 crushed rock, compacted in lifts no greater than 12 inches thick.

With the uniform "mat" of crushed rock beneath structural foundations supporting maximum 3-story structures, we estimate that total and differential settlements will be less than 1 and ½ inch respectively. One-half inch of differential settlement is generally considered acceptable for residential construction.

Structural Foundations

At the time of this report specific foundation elevations and loading conditions are not known, although preliminary plans do not call for any below-grade structures. The recommendations of this report assume the building pad elevations will not change more than 2 to 3 feet compared to the existing grades. Additional geotechnical recommendations would be needed for more significant grade changes (if planned).

Spread footings may be used to support the proposed structure, provided they are founded on the crushed rock building pad recommended in the previous report section. We recommend a maximum allowable bearing pressure of 2,000 pounds per square foot (psf) for designing spread footings bearing on undisturbed native soils or engineered fill. The recommended maximum allowable bearing pressure may be increased by a factor of 1.33 (one-third increase) for short term transient conditions such as wind and seismic loading. Minimum footing depths and widths should be determined by the project engineer/architect in accordance with applicable design codes.

A footing-to-slope setback of 10 feet is recommended. The setback should be measured from the bottom, outside edge of the footing horizontally to the face of the nearest slope. If needed, foundations can be deepened to achieve the recommended footing-to-slope setback. Minimum footing widths should be determined by the project engineer/architect in accordance with applicable design codes.

Assuming construction is accomplished as recommended herein, and for the foundation loads anticipated, we estimate total settlement of spread foundations of less than about 1 inch and differential settlement between two adjacent load-bearing components supported on competent soil of less than about ½ inch. We anticipate that the majority of the estimated settlement will occur during construction, as loads are applied.

Wind, earthquakes, and unbalanced earth loads will subject the proposed structure to lateral forces. Lateral forces on a structure will be resisted by a combination of sliding resistance of its base or footing on the underlying soil and passive earth pressure against the buried portions of the structure. For use in design, a coefficient of friction of 0.5 may be assumed along the interface between the base of the footing and subgrade soils. Passive earth pressure for buried portions of structures may be calculated using an equivalent fluid weight of 390 pounds per cubic foot (pcf), assuming footings are cast against dense, natural soils or engineered fill. The recommended coefficient of friction and passive earth pressure values do not include a safety factor. The upper 12 inches of soil should be neglected in passive pressure computations unless it is protected by pavement or slabs on grade.

Footing excavations should be trimmed neat and the bottom of the excavation should be carefully prepared. All loose or softened soil should be removed from the footing excavation prior to placing reinforcing steel bars. We recommend that footing excavations be observed by HGSI prior to placing steel and concrete, to verify that the recommendations of this report have been followed, and that an appropriate bearing stratum has been exposed.

Below-Grade Structural Retaining Walls

Lateral earth pressures against below-grade retaining walls will depend upon the inclination of any adjacent slopes, type of backfill, degree of wall restraint, method of backfill placement, degree of backfill compaction, drainage provisions, and magnitude and location of any adjacent surcharge loads. At-rest soil pressure is exerted on a retaining wall when it is restrained against rotation. In contrast, active soil pressure will be exerted on a wall if its top is allowed to rotate or yield a distance of roughly 0.001 times its height or greater. If the subject retaining walls will be free to rotate at the top, they should be designed for an active earth pressure equivalent to that generated by a fluid weighing 35 pcf for level backfill against the wall. For restrained walls, an at-reset equivalent fluid pressure of 54 pcf should be used in design, again assuming level backfill against the wall. These values assume that the recommended drainage provisions are incorporated, and hydrostatic pressures are not allowed to develop against the wall.

During a seismic event, lateral earth pressures acting on below-grade structural walls will increase by an incremental amount that corresponds to the earthquake loading. Based on the Mononobe-Okabe equation and peak horizontal accelerations appropriate for the site location, seismic loading should be modeled using

the active or at-rest earth pressures recommended above, plus an incremental rectangular-shaped seismic load of magnitude $5H$, where H is the total height of the wall.

We assume relatively level ground surface below the base of the walls. As such, we recommend passive earth pressure of 390 pcf for use in design, assuming wall footings are cast against competent native soils or engineered fill. If the ground surface slopes down and away from the base of any of the walls, a lower passive earth pressure should be used and HGSI should be contacted for additional recommendations.

A coefficient of friction of 0.5 may be assumed along the interface between the base of the wall footing and subgrade soils. The recommended coefficient of friction and passive earth pressure values do not include a safety factor, and an appropriate safety factor should be included in design. The upper 12 inches of soil should be neglected in passive pressure computations unless it is protected by pavement or slabs on grade.

The above recommendations for lateral earth pressures assume that the backfill behind the subsurface walls will consist of properly compacted structural fill, and no adjacent surcharge loading. If the walls will be subjected to the influence of surcharge loading within a horizontal distance equal to or less than the height of the wall, the walls should be designed for the additional horizontal pressure. For uniform surcharge pressures, a uniformly distributed lateral pressure of 0.3 times the surcharge pressure should be added.

The recommended equivalent fluid densities assume a free-draining condition behind the walls so that hydrostatic pressures do not build up. This can be accomplished by placing a 12- to 18-inch wide zone of crushed drain rock containing less than 5 percent fines against the walls. A 3-inch minimum diameter perforated, plastic drain pipe should be installed at the base of the walls and connected to an approved outlet to remove water from the crushed drain rock zone. The drain pipe should be wrapped in filter fabric (Mirafi 140N or other as approved by the geotechnical engineer) to minimize clogging. The above drainage measures are intended to remove water from behind the wall to prevent hydrostatic pressures from building up. Additional drainage measures may be specified by the project architect or structural engineer, for damp-proofing or other reasons.

HGSI should be contacted during construction to verify subgrade strength in wall keyway excavations, to verify that backslope soils are in accordance with our assumptions, and to take density tests on the wall backfill materials.

Concrete Slabs-on-Grade

Preparation of areas beneath concrete slab-on-grade floors should be performed as recommended in the *Site Preparation* section. Care should be taken during excavation for foundations and floor slabs, to avoid disturbing subgrade soils. If subgrade soils have been adversely impacted by wet weather or otherwise disturbed, the surficial soils should be scarified to a minimum depth of 8 inches, moisture conditioned to within about 3 percent of optimum moisture content, and compacted to engineered fill specifications. Alternatively, disturbed soils may be removed and the removal zone backfilled with additional crushed rock. For evaluation of the concrete slab-on-grade floors using the beam on elastic foundation method, a modulus of subgrade reaction of 200 kcf (115 pci) should be assumed for the soils anticipated at subgrade depth. This value assumes the concrete slab system is designed and constructed as recommended herein, with a minimum thickness of crushed rock of 8 inches beneath the slab.

Interior slab-on-grade floors should be provided with an adequate moisture break. The capillary break material should consist of ODOT open graded aggregate per ODOT Standard Specifications 02630-2. The minimum recommended thickness of capillary break materials on re-compacted soil subgrade is 8 inches. The total thickness of crushed aggregate will be dependent on the subgrade conditions at the time of

construction, and should be verified visually by proof-rolling. Under-slab aggregate should be compacted to at least 90% of its maximum dry density as determined by ASTM D1557 or equivalent.

In areas where moisture will be detrimental to floor coverings or equipment inside the proposed structure, appropriate vapor barrier and damp-proofing measures should be implemented. A commonly applied vapor barrier system consists of a 10-mil polyethylene vapor barrier placed directly over the capillary break material. With this type of system, an approximately 2-inch thick layer of sand is often placed over the vapor barrier to protect it from damage, to aid in curing of the concrete, and also to help prevent cement from bleeding down into the underlying capillary break materials. Other damp/vapor barrier systems may also be feasible. Appropriate design professionals should be consulted regarding vapor barrier and damp proofing systems, ventilation, building material selection, radon and mold prevention issues, which are outside HGSI's area of expertise.

Perimeter Footing Drains

Due to the potential for perched surface water above fine grained deposits and fill such as those encountered at the site, we recommend the outside edge of perimeter footings be provided with a drainage system consisting of 4-inch minimum diameter perforated plastic pipe embedded in a minimum of 1 ft³ per lineal foot of clean, crushed drain rock. The drain pipe and surrounding drain rock should be wrapped in non-woven geotextile (Mirafi 140N, or approved equivalent) to minimize the potential for clogging and/or ground loss due to piping. Water collected from the footing drains should be directed into the local storm drain system or other suitable outlet. A minimum 0.5 percent fall should be maintained throughout the drain and non-perforated pipe outlet. The footing drains should include clean-outs to allow periodic maintenance and inspection.

Down spouts and roof drains should collect roof water in a system separate from the footing drains in order to reduce the potential for clogging. Roof drain water should be directed to an appropriate discharge point well away from structural foundations. Grades should be sloped downward and away from buildings to reduce the potential for ponded water near structures.

Seismic Design

Structures should be designed to resist earthquake loading in accordance with the methodology described in the current Oregon Structural Specialty Code (OSSC). We recommend Site Class D (Stiff Soils) be used for design per the OSSC, which references ASCE 7. Design values determined for the site using the ASCE 7-16 Hazard Tool are summarized on Table 2, for Risk Category II. A copy of the Hazard Tool output is attached at the end of this report.

Table 2. Recommended Earthquake Ground Motion Parameters (ASCE 7-16)

Parameter	Value
Location (Lat, Long), degrees	45.4994, -122.6938
Mapped Spectral Acceleration Values (MCE, Site Class B):	
Short Period, S_s	0.893 g
1.0 Sec Period, S_1	0.4 g
Design Values for Site Class D (Stiff Soils):	
Peak Ground Acceleration PGA_M	0.481
F_a	1.143
F_v	N.A.
$SD_s = 2/3 \times F_a \times S_s$	0.68 g
$SD_1 = 2/3 \times F_v \times S_1$	N.A.

Soil liquefaction is a phenomenon wherein saturated soil deposits temporarily lose strength and behave as a liquid in response to earthquake shaking. Soil liquefaction is generally limited to loose, granular soils located below the water table. Following development, on-site soils will consist predominantly of engineered fill or medium stiff to hard native soils and soft rock above the water table, which are not considered susceptible to liquefaction. Therefore, it is our opinion that special design or construction measures are not required to mitigate the effects of liquefaction.

Excavating Conditions and Utility Trench Backfill

We anticipate that on-site soils can be excavated to depths of at least 15 feet using conventional heavy equipment such as trackhoes. Weathered basalt bedrock was encountered at depths of about 40 to 50 feet.

Maintenance of safe working conditions, including temporary excavation stability, is the responsibility of the contractor. Actual slope inclinations at the time of construction should be determined based on safety requirements and actual soil and groundwater conditions. All temporary cuts in excess of 4 feet in height should be sloped in accordance with U.S. Occupational Safety and Health Administration (OSHA) regulations (29 CFR Part 1926), or be shored. The existing native soils classify as Type B Soil and temporary excavation side slope inclinations as steep as 1H:1V may be assumed for planning purposes. This cut slope inclination is applicable to excavations above the water table only. Flatter temporary excavation slopes will be needed if groundwater is present, or if significant thicknesses of sandy soils are present in excavation sidewalls.

Perched groundwater conditions often occur over fine-grained native deposits such as those beneath the site, particularly during the wet season. If encountered, the contractor should be prepared to implement an appropriate dewatering system for installation of the utilities. At this time, we anticipate that dewatering systems consisting of ditches, sumps and pumps would be adequate for control of groundwater where encountered during construction conducted during the dry season. Regardless of the dewatering system used, it should be installed and operated such that in-place soils are prevented from being removed along with the groundwater.

Vibrations created by traffic and construction equipment may cause some caving and raveling of excavation walls. In such an event, lateral support for the excavation walls should be provided by the contractor to prevent loss of ground support and possible distress to existing or previously constructed structural improvements.

Utility trench backfill should consist of ¾"-0 crushed rock, compacted to at least 90% of the maximum dry density obtained by Modified Proctor (ASTM D1557) or equivalent. Initial backfill lift thicknesses for a ¾"-0 crushed aggregate base may need to be as great as 4 feet to reduce the risk of flattening underlying flexible pipe. Subsequent lift thickness should not exceed 1 foot. If imported granular fill material is used, then the lifts for large vibrating plate-compaction equipment (e.g. hoe compactor attachments) may be up to 2 feet, provided that proper compaction is being achieved and each lift is tested. Use of large vibrating compaction equipment should be carefully monitored near existing structures and improvements due to the potential for vibration-induced damage.

Adequate density testing should be performed during construction to verify that the recommended relative compaction is achieved. Typically, one density test is taken for every 4 vertical feet of backfill on each 200-lineal-foot section of trench.

Stormwater Systems

Because steep slopes are present to the south and west of the site, HGSI recommends against the infiltration of stormwater due to slope stability issues. We suggest use of flow-through planter boxes with overflow to the City system (if available). If this is not feasible, stormwater should be detained and released. Ideally, stormwater would be released well down slope of the development utilizing a flow spreader type design to minimize erosion.

Erosion Control Considerations

Fine grained soils are susceptible to erosion. Erosion during construction can be minimized by implementing the project erosion control plan, which should include judicious use of bio-bags, silt fences, or other appropriate technology. Where used, erosion control devices should be in place and remain in place throughout site preparation and construction.

Erosion and sedimentation of exposed soils can also be minimized by quickly re-vegetating exposed areas of soil, and by staging construction such that large areas of the project site are not denuded and exposed at the same time. Areas of exposed soil requiring immediate and/or temporary protection against exposure should be covered with either mulch or erosion control netting/blankets. Areas of exposed soil requiring permanent stabilization should be seeded with an approved grass seed mixture, or hydroseeded with an approved seed-mulch-fertilizer mixture.

UNCERTAINTIES AND LIMITATIONS

We have prepared this report for the owner and his/her consultants for use in design of this project only. This report should be provided in its entirety to prospective contractors for bidding and estimating purposes; however, the conclusions and interpretations presented in this report should not be construed as a warranty of the subsurface conditions. Experience has shown that soil and groundwater conditions can vary significantly over small distances. Inconsistent conditions can occur between explorations that may not be detected by a geotechnical study. If, during future site operations, subsurface conditions are encountered which vary appreciably from those described herein, HGSI should be notified for review of the recommendations of this report, and revision of such if necessary.

Sufficient geotechnical monitoring, testing and consultation should be provided during construction to confirm that the conditions encountered are consistent with those indicated by explorations. Recommendations for design changes will be provided should conditions revealed during construction differ from those anticipated, and to verify that the geotechnical aspects of construction comply with the contract plans and specifications.

November 12, 2019
HGSI Project No. 19-2514

Within the limitations of scope, schedule and budget, HGSI executed these services in accordance with generally accepted professional principles and practices in the field of geotechnical engineering at the time the report was prepared. No warranty, expressed or implied, is made. The scope of our work did not include environmental assessments or evaluations regarding the presence or absence of wetlands or hazardous or toxic substances in the soil, surface water, or groundwater at this site.



We appreciate this opportunity to be of service.

Sincerely,

HARDMAN GEOTECHNICAL SERVICES INC.



EXPIRES: 06-30-2021

Scott L. Hardman, P.E., G.E.
Geotechnical Engineer

- Attachments: References
 Figure 1 – Vicinity Map
 Figure 2 – Site Plan
 Logs of Borings B-1 and B-2
 ASCE 7-16 Hazard Tool Output (3 Sheets)



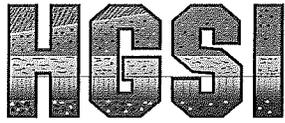
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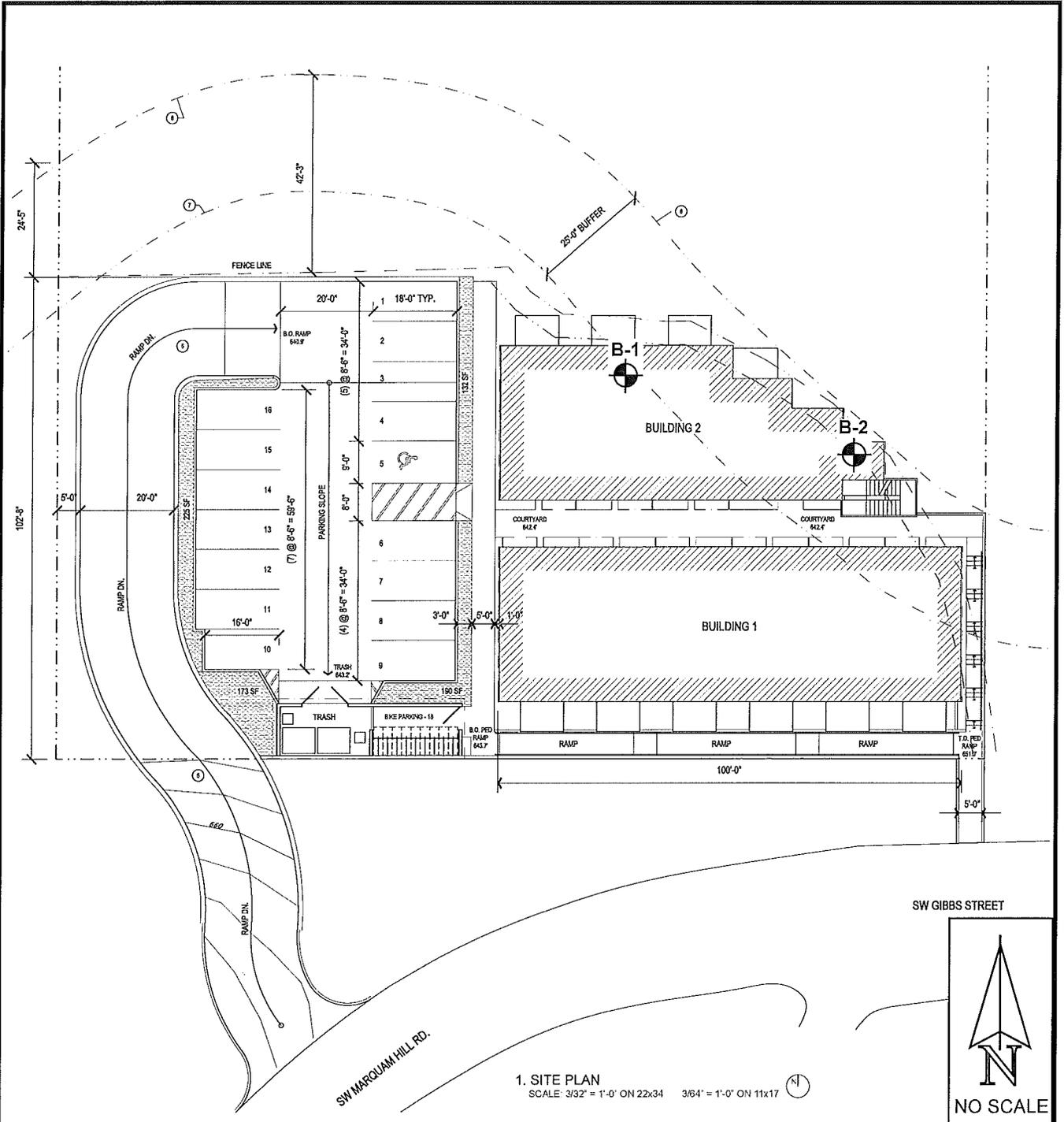
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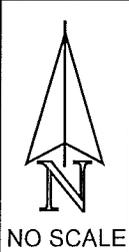
**HARDMAN
GEOTECHNICAL
SERVICES INC.**

Practical, Cost-Effective Geotechnical Solutions

SITE PLAN



1. SITE PLAN
SCALE 3/32" = 1'-0" ON 22x34 3/64" = 1'-0" ON 11x17



Legend Base map provided by Steelhead Architecture

 Boring

Project: 1325 SW Gibbs Street Portland, Oregon	Project No. 19-2514	FIGURE 2
---	---------------------	----------

BORING LOG

Project: 1325 SW Gibbs Street
Portland, Oregon

Project No. 19-2514

Boring No. B-1

Depth (ft)	Sample Interval	SPT N-Value (Blows/foot)	In-Situ Dry Density (lb/ft ³)	Moisture Content (%)	Groundwater	Material Description
	3.1.2	3				Soft, SILT with gravel (ML), brown and gray, moist (Fill)
5	2.1.2	3				Soft, SILT with trace fine sand (ML), brown, moist (Fill)
	2.2.3	5				Medium stiff, silty CLAY (CL), dark gray, moist (Fill)
10	1.2.3	5				Interbedded medium stiff, silty CLAY (CL), dark gray, moist and medium stiff, organic silty CLAY with organic debris (CL), black, moist (Fill)
	1.2.2	4				Soft to medium stiff, SILT with clay (ML), dark brown and dark gray, moist (Fill)
15	3.2.3	5				Medium stiff, SILT with clay (ML), dark gray-brown, moist (Loess)
	2.3.3	6				Medium stiff, SILT with clay and trace fine sand (ML), gray, moist (Loess)
20	3.4.6	10				Stiff, CLAY (CL), orange-brown with pale gray-pink, moist (Residual soil)
25	6.6.8	14				Stiff, SILT with clay and trace fine sand (ML), orange-brown with yellow and gray, moist (Residual soil)
Boring log continued on the following page						

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(503) 830-8076

LEGEND


STP Drive Sample


Water Level at
Time of Drilling

Date Drilled: 9/27/19

Logged By: PBR

BORING LOG

Project: 1325 SW Gibbs Street
Portland, Oregon

Project No. 19-2514

Boring No. B-1

Depth (ft)	Sample Interval	SPT N-Value (Blows/foot)	In-Situ Dry Density (lb/ft ³)	Moisture Content (%)	Groundwater	Material Description
30	5.8.10 	18				Very stiff, SILT with clay and trace fine sand (ML), orange-brown with yellow, gray and black, moist (Residual soil)
35	9.14.17 	31				Hard, SILT with clay and trace fine sand (ML), gray with pale pink seams and small yellow granules, moist (Residual soil)
40	18.50 	>50				Very dense, silty GRAVEL (GM), gray, moist (Weathered basalt)
45						Boring terminated at 41 feet No seepage or groundwater encountered
50						
55						

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LEGEND

STP Drive Sample

Water Level at
Time of Drilling

Date Drilled: 9/27/19
Logged By: PBR

BORING LOG

Project: 1325 SW Gibbs Street
Portland, Oregon

Project No. 19-2514

Boring No. B-2

Depth (ft)	Sample Interval	SPT N-Value (Blows/foot)	In-Situ Dry Density (lb/ft ³)	Moisture Content (%)	Groundwater	Material Description
	4.7.10 	17				Medium dense, silty GRAVEL (GM), brown, moist (Fill)
5	6.6.6 	12				Stiff, SILT with fine sand (ML), brown and dark brown, slightly moist (Fill)
	4.4.7 	11				Stiff, SILT with fine sand (ML), brown, tan and gray, slightly moist (Fill)
10	4.3.5 	8				Medium stiff to stiff, SILT with fine sand and sparse gravel (ML), brown, dark brown, orange and lite gray, moist (Fill)
15	5.3.7 	10				Stiff, SILT with trace fine sand (ML), brown with subtle orange and gray mottling, moist (Loess)
20	5.5.7 	12				Stiff, silty CLAY (CL), brown, lite gray and orange, moist (Residual soil)
25	2.3.5 	8				Medium stiff to stiff, CLAY (CL), brown with orange and tan mottling, moist (Residual soil)
Boring log continued on the following page						

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LEGEND


STP Drive Sample


Water Level at
Time of Drilling

Date Drilled: 9/27/19

Logged By: PBR

BORING LOG

Project: 1325 SW Gibbs Street
Portland, Oregon

Project No. 19-2514

Boring No. B-2

Depth (ft)	Sample Interval	SPT N-Value (Blows/foot)	In-Situ Dry Density (lb/ft ³)	Moisture Content (%)	Groundwater	Material Description
30	9.8.12 	20				Very stiff, SILT with clay and trace fine sand (ML), orange, brown, gray and red-brown, moist (Residual soil)
35	6.6.8 	14				Stiff, CLAY (CL), dark brown, brown, orange, and tan with small black granules, moist (Residual soil)
40	11.10.15 	25				Very stiff, SILT with clay and fine sand (ML), gray with black and tan seams and small green granules, moist (Residual soil)
45						
50	50 	50 for 0				Very dense, silty GRAVEL (GM), gray, moist (Weathered basalt)
55						Boring terminated at 50 feet No seepage or groundwater encountered

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LEGEND


STP Drive Sample


Water Level at
Time of Drilling

Date Drilled: 9/27/19
Logged By: PBR

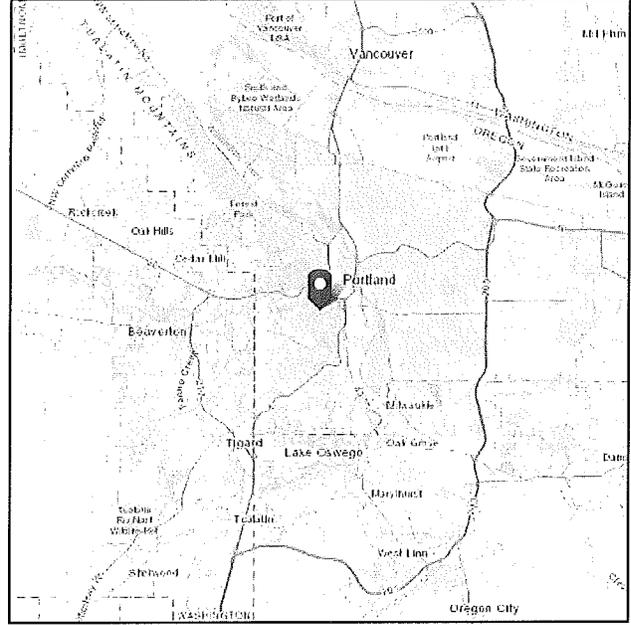


ASCE 7 Hazards Report

Address:
No Address at This
Location

Standard: ASCE/SEI 7-16
Risk Category: II
Soil Class: D - Stiff Soil

Elevation: 643.62 ft (NAVD 88)
Latitude: 45.4994
Longitude: -122.6938





Seismic

Site Soil Class: D - Stiff Soil

Results:

S_s :	0.893	S_{D1} :	N/A
S_1 :	0.4	T_L :	16
F_a :	1.143	PGA :	0.402
F_v :	N/A	PGA _M :	0.481
S_{MS} :	1.02	F _{PGA} :	1.198
S_{M1} :	N/A	I_e :	1
S_{DS} :	0.68	C_v :	1.246

Ground motion hazard analysis may be required. See ASCE/SEI 7-16 Section 11.4.8.

Data Accessed: Tue Nov 05 2019

Date Source: [USGS Seismic Design Maps](#)

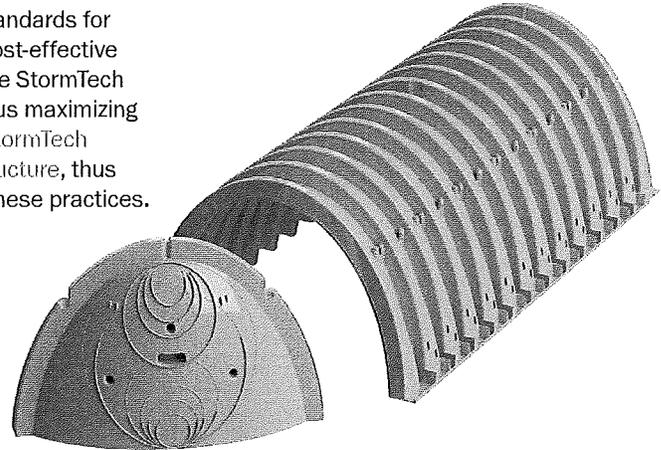
The ASCE 7 Hazard Tool is provided for your convenience, for informational purposes only, and is provided "as is" and without warranties of any kind. The location data included herein has been obtained from information developed, produced, and maintained by third party providers; or has been extrapolated from maps incorporated in the ASCE 7 standard. While ASCE has made every effort to use data obtained from reliable sources or methodologies, ASCE does not make any representations or warranties as to the accuracy, completeness, reliability, currency, or quality of any data provided herein. Any third-party links provided by this Tool should not be construed as an endorsement, affiliation, relationship, or sponsorship of such third-party content by or from ASCE.

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STORMTECH SC-740 CHAMBER

Designed to meet the most stringent industry performance standards for superior structural integrity while providing designers with a cost-effective method to save valuable land and protect water resources. The StormTech system is designed primarily to be used under parking lots, thus maximizing land usage for private (commercial) and public applications. StormTech chambers can also be used in conjunction with Green Infrastructure, thus enhancing the performance and extending the service life of these practices.



STORMTECH SC-740 CHAMBER

(not to scale)

Nominal Chamber Specifications

Size (L x W x H)

85.4" x 51" x 30"

2,170 mm x 1,295 mm x 762 mm

Chamber Storage

45.9 ft³ (1.30 m³)

Min. Installed Storage*

74.9 ft³ (2.12 m³)

Weight

74.0 lbs (33.6 kg)

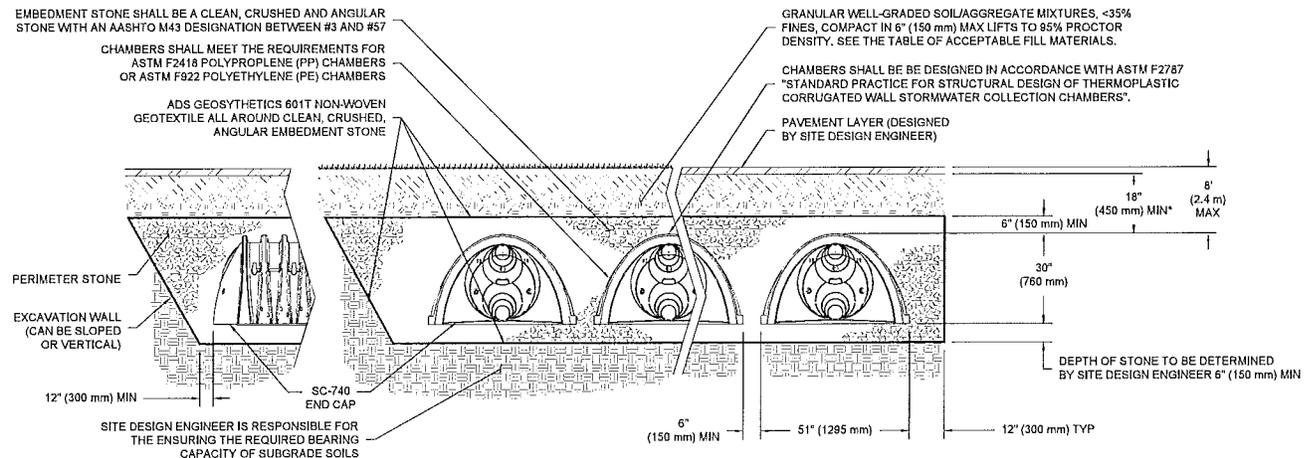
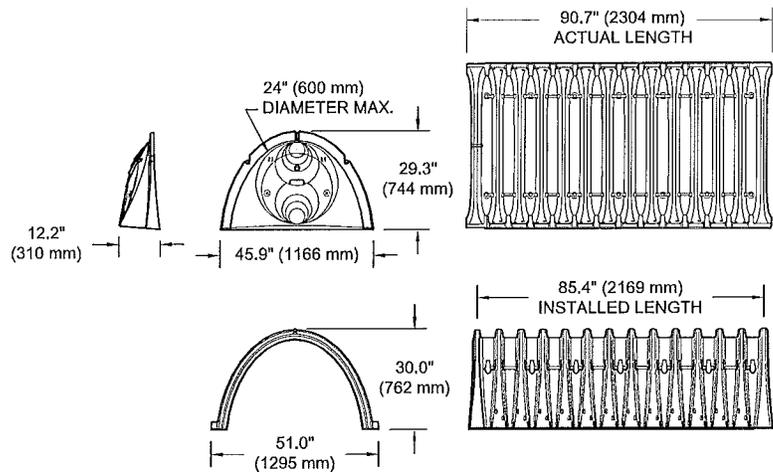
Shipping

30 chambers/pallet

60 end caps/pallet

12 pallets/truck

*Assumes 6" (150 mm) stone above, below and between chambers and 40% stone porosity.



*MINIMUM COVER TO BOTTOM OF FLEXIBLE PAVEMENT. FOR UNPAVED INSTALLATIONS WHERE RUTTING FROM VEHICLES MAY OCCUR, INCREASE COVER TO 24" (600 mm).

SC-740 CUMULATIVE STORAGE VOLUMES PER CHAMBER

Assumes 40% Stone Porosity. Calculations are Based Upon a 6" (150 mm) Stone Base Under Chambers.

Depth of Water in System (inches) (mm)	Cumulative Chamber Storage (ft ³) (m ³)	Total System Cumulative Storage (ft ³) (m ³)
42 (1067)	↑ 45.90 (1.300)	74.90 (2.121)
41 (1041)	↑ 45.90 (1.300)	73.77 (2.089)
40 (1016)	Stone 45.90 (1.300)	72.64 (2.057)
39 (991)	Cover 45.90 (1.300)	71.52 (2.025)
38 (965)	↓ 45.90 (1.300)	70.39 (1.993)
37 (940)	↓ 45.90 (1.300)	69.26 (1.961)
36 (914)	↓ 45.90 (1.300)	68.14 (1.929)
35 (889)	↓ 45.85 (1.298)	66.98 (1.897)
34 (864)	↓ 45.69 (1.294)	65.75 (1.862)
33 (838)	↓ 45.41 (1.286)	64.46 (1.825)
32 (813)	↓ 44.81 (1.269)	62.97 (1.783)
31 (787)	↓ 44.01 (1.246)	61.36 (1.737)
30 (762)	↓ 43.06 (1.219)	59.66 (1.689)
29 (737)	↓ 41.98 (1.189)	57.89 (1.639)
28 (711)	↓ 40.80 (1.155)	56.05 (1.587)
27 (686)	↓ 39.54 (1.120)	54.17 (1.534)
26 (660)	↓ 38.18 (1.081)	52.23 (1.479)
25 (635)	↓ 36.74 (1.040)	50.23 (1.422)
24 (610)	↓ 35.22 (0.977)	48.19 (1.365)
23 (584)	↓ 33.64 (0.953)	46.11 (1.306)
22 (559)	↓ 31.99 (0.906)	44.00 (1.246)
21 (533)	↓ 30.29 (0.858)	41.85 (1.185)
20 (508)	↓ 28.54 (0.808)	39.67 (1.123)
19 (483)	↓ 26.74 (0.757)	37.47 (1.061)
18 (457)	↓ 24.89 (0.705)	35.23 (0.997)
17 (432)	↓ 23.00 (0.651)	32.96 (0.939)
16 (406)	↓ 21.06 (0.596)	30.68 (0.869)
15 (381)	↓ 19.09 (0.541)	28.36 (0.803)
14 (356)	↓ 17.08 (0.484)	26.03 (0.737)
13 (330)	↓ 15.04 (0.426)	23.68 (0.670)
12 (305)	↓ 12.97 (0.367)	21.31 (0.608)
11 (279)	↓ 10.87 (0.309)	18.92 (0.535)
10 (254)	↓ 8.74 (0.247)	16.51 (0.468)
9 (229)	↓ 6.58 (0.186)	14.09 (0.399)
8 (203)	↓ 4.41 (0.125)	11.66 (0.330)
7 (178)	↓ 2.21 (0.063)	9.21 (0.264)
6 (152)	↑ 0 (0)	6.76 (0.191)
5 (127)	↑ 0 (0)	4.51 (0.128)
4 (102)	Stone Foundation 0 (0)	3.38 (0.096)
3 (76)	Foundation 0 (0)	2.25 (0.064)
2 (51)	↓ 0 (0)	1.13 (0.032)
1 (25)	↓ 0 (0)	0 (0)

Note: Add 1.13 ft³ (0.032 m³) of storage for each additional inch (25 mm) of stone foundation.

STORAGE VOLUME PER CHAMBER FT³ (M³)

	Bare Chamber Storage (ft ³) (m ³)	Chamber and Stone Foundation Depth (in) (mm)		
		6 (150)	12 (300)	18 (450)
SC-740 Chamber	45.9 (1.3)	74.9 (2.1)	81.7 (2.3)	88.4 (2.5)

Note: Assumes 6" (150 mm) stone above chambers, 6" (150 mm) row spacing and 40% stone porosity.

AMOUNT OF STONE PER CHAMBER

ENGLISH TONS (yds ³)	Stone Foundation Depth		
	6"	12"	18"
SC-740	3.8 (2.8)	4.6 (3.3)	5.5 (3.9)
METRIC KILOGRAMS (m ³)	150mm	300mm	450mm
SC-740	3,450 (2.1)	4,170 (2.5)	4,490 (3.0)

Note: Assumes 6" (150 mm) of stone above and between chambers.

VOLUME EXCAVATION PER CHAMBER YD³ (M³)

	Stone Foundation Depth		
	6 (150)	12 (300)	18 (450)
SC-740	5.5 (4.2)	6.2 (4.7)	6.8 (5.2)

Note: Assumes 6" (150 mm) of row separation and 18" (450 mm) of cover. The volume of excavation will vary as depth of cover increases.



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Visit us at www.stormtech.com
and utilize the StormTech Design Tool

For more information on the StormTech SC-740 Chamber and other ADS products, please contact our Customer Service Representatives at 1-800-821-6710

THE MOST ADVANCED NAME IN WATER MANAGEMENT SOLUTIONS™

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Advanced Drainage Systems, Inc.
4640 Trueman Blvd., Hilliard, OH 43026
1-800-821-6710 www.ads-pipe.com

LU 22-187796 APD AP
Exhibit A.4

Facility ROUTING DATA 1325 Gibbs:

SGL 19-029

Given: Pipe Diameter: 12.0 inches
 Pipe Inv. Elev: 0.00 feet
 Orifice #1 Diameter: 1 3/4 inches
 Orifice #1 Elevation: -0.28 feet Plan elevation 634.44
 Orifice #2 Diameter: 3 inches
 Orifice #2 Elevation: 2.00 feet Plan elevation 636.74

	B	C	D	E	F	G	H	I	J	K
	Stage	Elevation (ft)	Chamber Storage (sq.ft)	Number Chamber (ea)	Storage Volume (cu.ft.)	Max Capacity (cfs)	Pipe Capacity (cfs)	Orifice #1 Discharge (cfs)	Orifice #2 Discharge (cfs)	Actual Discharge (cfs)
	1	0.00	0.00	6.0	0.00	2.09	0.269	0.044		0.000
Detention - Storage Pond	2	0.25	3.38	6.0	20.28		0.000	0.061		0.073
	3	0.50	6.76	6.0	40.56	3.14	0.974	0.073		0.073
	4	0.75	14.09	6.0	84.54	6.28	2.386	0.084		0.084
	5	1.00	21.31	6.0	127.86		-	0.094		0.094
	6	1.25	28.36	6.0	170.16		-	0.103		0.103
	6	1.50	35.23	6.0	211.38		-	0.111		0.111
	7	1.67	39.67	6.0	238.02		-	0.116		0.116
	8	1.75	41.85	6.0	251.10		-	0.118		0.118
	9	2.00	48.19	6.0	289.14		-	0.125	0.000	0.125
	10	2.25	54.17	6.0	325.02		-	0.132	0.122	0.254
	11	2.50	59.66	6.0	357.96		-	0.139	0.173	0.311
	12	2.75	64.46	6.0	386.76		-	0.145	0.212	0.356
	13	3.00	68.14	6.0	408.84		-	0.151	0.244	0.395
	14	3.25	71.52	6.0	429.12		-	0.156	0.273	0.429
	15	3.50	74.90	6.0	449.40		-	0.162	0.299	0.461

B Stage Number
 C Water Surface Elevation.
 D Manufactures Chamber Storage Volume per stage
 E Number of Chambers
 F Storage Volume = Chamber Storage Volume x Number of Chambers

ORIFICE $Q = 0.62 \times (\text{area}) \times (2 \times g \times h)^{1/2}$
 H Capacity of Discharge Pipe
 I $Q = \text{Orifice Eq.}$
 J $Q = \text{Orifice Eq.}$
 K Less of => columns H or columns I + J

Initial stage: elevation 0.00 = plan elevation 634.72



CITY OF PORTLAND ENVIRONMENTAL SERVICES



1120 SW Fifth Avenue, Room 1000, Portland, Oregon 97204 ■ Nick Fish, Commissioner ■ James Hagerman, Interim Director

February 2019

Stormwater Management Manual Approved Manufactured Stormwater Treatment Technology

BaySaver Technologies BayFilter™ using Enhanced Media Cartridges (EMC)

City of Portland Decision:

The BaySaver Technologies BayFilter™ using Enhanced Media Cartridges (EMC) meets Portland's pollution reduction requirements, per the requirements of the 2016 Stormwater Management Manual, and is approved for use in the City of Portland.

Background:

As part of the application process, BaySaver Technologies submitted the Washington State Department of Ecology (DOE) Technology Assessment Protocol (TAPE) Technology Evaluation Report, including all appendices and performance monitoring data to demonstrate the BayFilter EMC meets the City of Portland's pollution reduction requirements.

BaySaver Technologies gave a technical presentation to City staff, PSU faculty, and the public on November 1, 2017. The presentation was followed by a technical interview with the City review committee to discuss water quality performance, maintenance, and overall use in the public right-of-way.

Additional information is available online for this system, including:

- [BaySaver BayFilter Website](#)
- [WA DOE General Use Level Designation for the BayFilter with EMC](#)

Conditions of Use:

1. All configuration options for the BayFilter EMC are approved for TSS removal. Selection of a specific configuration is the responsibility of the project designer.
2. Use of a BayFilter EMC does not exempt a project or site from required flow control requirements, operations and maintenance requirements, or other applicable requirements of the SWMM.
3. For use in the public right-of-way, the following conditions must be met:
 - Units must meet City of Portland street design requirements, including but not limited to H-20 vehicle load rating, non-slip surface, and American with Disabilities Act tolerances specific to surface grates or vault lids.

- The O&M Plan must call for an assessment during the two-year warranty period of project-specific maintenance requirements and frequencies.
4. Providers certified by BaySaver Technologies should be utilized for activation, inspection and maintenance of the system, unless otherwise trained and certified by the manufacturer.

Project Designer Responsibilities:

1. Ensuring that the Conditions of Approval are met.
2. Ensuring that the project meets all applicable requirements of the Portland SWMM, including the Stormwater Infiltration and Discharge Hierarchy.
3. Ensuring that the design and installation of the units are appropriate for the project goals, site conditions, long-term maintenance requirements, and any other site-specific design requirements on private property or for use in the public right-of-way.
4. Sizing units to meet the current Portland SWMM presumptive design approach and pollution reduction requirements. The pollution reduction capacity is flow-based and assumes a treatment flow intensity of 0.19 inches per hour, 5-minute time of concentration, and a 0.90 runoff coefficient using the Rational Method with treatment rates based upon WA GULD approved flow rates, in lieu of the manufacturer’s standard flow rate. The treatment capacities for BayFilter EMC cartridges, based on those assumptions, are provided in Table 1. For sites with different times of concentration, different rainfall intensities may be appropriate.

Table 1. BayFilter EMC Sizing to Meet City of Portland Pollution Reduction Requirements			
Cartridge Configuration	Maximum Cartridge Flow Rate (gpm)	Maximum Drainage Area (acres/ cartridge)	Maximum Drainage Area (sf/cartridge)
545	45	0.585	25,482
530	30	0.392	17,075
522	22.5	0.292	12,719

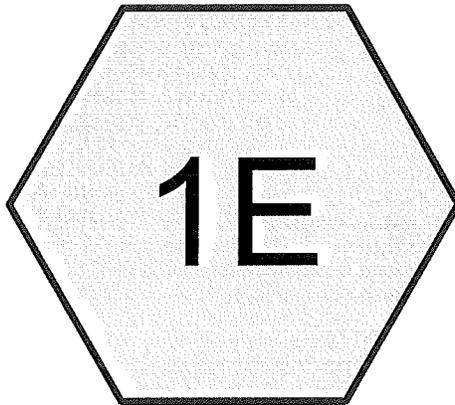
5. Each site plan must undergo review by BaySaver Technologies before the City of Portland can approve the unit(s) for site installation. A letter certifying the project has been designed to the manufacturer’s specification must be submitted to BES prior to the appropriate design milestone. For public improvements, including public works permits, the letter must be submitted to BES prior to 60% plan review. For installation on private property, the letter must be submitted prior to approval of the building permit plan. The project designer is highly encouraged to work with BaySaver Technologies prior to the appropriate review milestone.
6. If the project designer wishes to vary from these conditions of approval, the project designer must use the Performance Design Approach.

General Conditions:

1. BES may at any time suspend or revoke approval if the performance of the technology does not meet performance criteria, if there are changes to the TAPE certification, or the performance criteria change due to local, state, or federal pollution reduction standards.
2. If any changes, updates, or revisions have occurred to the BayFilter EMC, the applicant must obtain WA DOE TAPE GULD certification and re-apply following submission guidelines in effect at the time of application.

Revision History:

Date	Action
January 2018	The device was approved for use in the City of Portland
July 2018	Removed 3-yr expiration date for the approval, per a change in BES policy.
February 2019	Removed "Cartridges per Impervious Acre" from Table 1 due to the potential for associated rounding errors.



Existing Conditions



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Page 2

Area Listing (selected nodes)

Area (acres)	CN	Description (subcatchment-numbers)
0.397	98	Impervious Area (1E)
0.020	73	Pervious Area (1E)
0.418	97	TOTAL AREA

Summary for Subcatchment 1E: Existing Conditions

Existing Tc

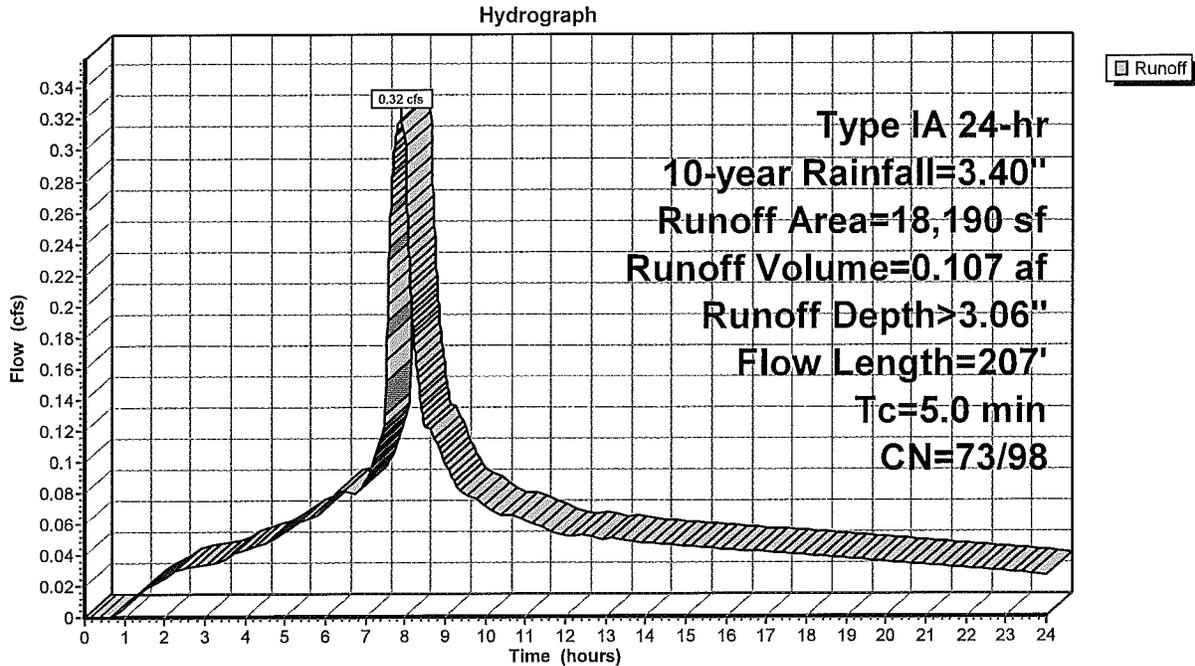
Runoff = 0.32 cfs @ 7.88 hrs, Volume= 0.107 af, Depth> 3.06"

Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-24.00 hrs, dt= 0.01 hrs
Type IA 24-hr 10-year Rainfall=3.40"

	Area (sf)	CN	Description
*	17,304	98	Impervious Area
*	886	73	Pervious Area
	18,190	97	Weighted Average
	886	73	4.87% Pervious Area
	17,304	98	95.13% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
0.8	136	0.1550	2.87		Sheet Flow, T1 Smooth surfaces n= 0.011 P2= 2.40"
1.2	71	0.0140	0.96		Sheet Flow, T2 Smooth surfaces n= 0.011 P2= 2.40"
3.0					Direct Entry, T3 - Meet minimum
5.0	207	Total			

Subcatchment 1E: Existing Conditions



SGL 19-029-hydrocad

Type IA 24-hr 10-year Rainfall=3.40"

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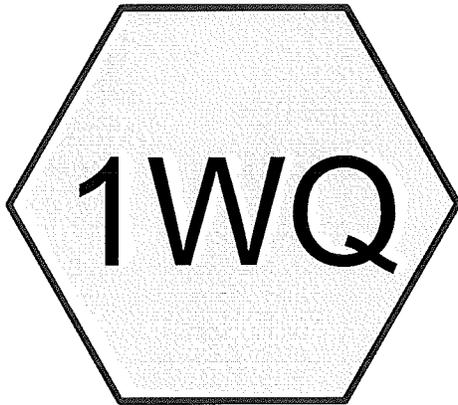
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Hydrograph for Subcatchment 1E: Existing Conditions

Time (hours)	Precip. (inches)	Perv.Excess (inches)	Imp.Excess (inches)	Runoff (cfs)
0.00	0.00	0.00	0.00	0.00
0.50	0.03	0.00	0.00	0.00
1.00	0.07	0.00	0.00	0.01
1.50	0.12	0.00	0.02	0.02
2.00	0.17	0.00	0.05	0.02
2.50	0.22	0.00	0.09	0.03
3.00	0.28	0.00	0.13	0.03
3.50	0.33	0.00	0.17	0.04
4.00	0.39	0.00	0.22	0.04
4.50	0.46	0.00	0.28	0.05
5.00	0.53	0.00	0.35	0.05
5.50	0.61	0.00	0.42	0.06
6.00	0.70	0.00	0.50	0.07
6.50	0.81	0.00	0.60	0.08
7.00	0.91	0.01	0.71	0.09
7.50	1.05	0.02	0.84	0.13
8.00	1.45	0.11	1.23	0.31
8.50	1.63	0.17	1.41	0.12
9.00	1.77	0.22	1.54	0.10
9.50	1.87	0.26	1.65	0.08
10.00	1.96	0.30	1.74	0.07
10.50	2.04	0.34	1.82	0.06
11.00	2.12	0.38	1.89	0.06
11.50	2.19	0.41	1.97	0.06
12.00	2.26	0.44	2.03	0.05
12.50	2.32	0.47	2.09	0.05
13.00	2.38	0.51	2.15	0.05
13.50	2.44	0.54	2.22	0.05
14.00	2.50	0.57	2.27	0.05
14.50	2.56	0.60	2.33	0.05
15.00	2.62	0.63	2.39	0.05
15.50	2.67	0.66	2.44	0.04
16.00	2.72	0.69	2.49	0.04
16.50	2.78	0.72	2.54	0.04
17.00	2.83	0.75	2.59	0.04
17.50	2.88	0.78	2.64	0.04
18.00	2.92	0.81	2.69	0.04
18.50	2.97	0.84	2.74	0.04
19.00	3.02	0.87	2.78	0.04
19.50	3.06	0.89	2.83	0.04
20.00	3.10	0.92	2.87	0.04
20.50	3.14	0.95	2.91	0.03
21.00	3.18	0.97	2.95	0.03
21.50	3.22	1.00	2.99	0.03
22.00	3.26	1.02	3.03	0.03
22.50	3.30	1.05	3.07	0.03
23.00	3.33	1.07	3.10	0.03
23.50	3.37	1.09	3.13	0.03
24.00	3.40	1.11	3.17	0.03



WQ BASIN



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Page 2

Area Listing (selected nodes)

Area (acres)	CN	Description (subcatchment-numbers)
0.380	98	Impervious area (1WQ)
0.380	98	TOTAL AREA

SGL 19-029-hydrocad-rev9-02-20

Type IA 24-hr wq Rainfall=0.83"

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Time span=0.00-30.00 hrs, dt=0.01 hrs, 3001 points

Runoff by SBUH method, Split Pervious/Imperv.

Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

Subcatchment 1WQ: WQ BASIN

Runoff Area=16,533 sf 100.00% Impervious Runoff Depth=0.63"

Tc=5.0 min CN=0/98 Runoff=0.06 cfs 0.020 af

Total Runoff Area = 0.380 ac Runoff Volume = 0.020 af Average Runoff Depth = 0.63"

0.00% Pervious = 0.000 ac 100.00% Impervious = 0.380 ac

Summary for Subcatchment 1WQ: WQ BASIN

Runoff = 0.06 cfs @ 7.91 hrs, Volume= 0.020 af, Depth= 0.63"

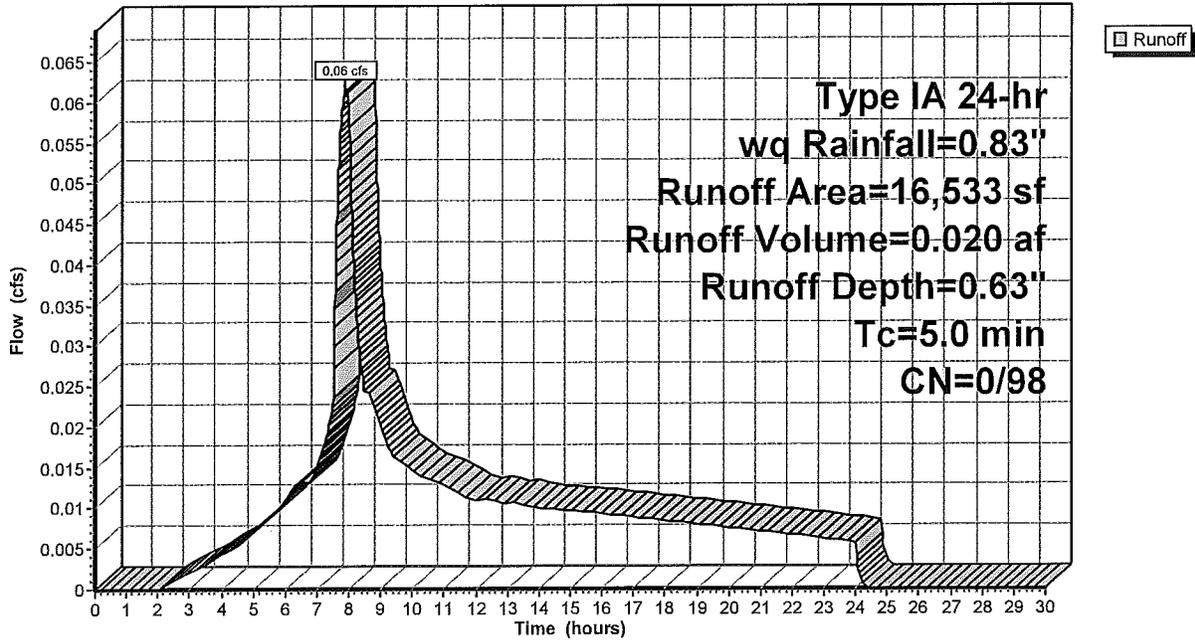
Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-30.00 hrs, dt= 0.01 hrs
 Type IA 24-hr wq Rainfall=0.83"

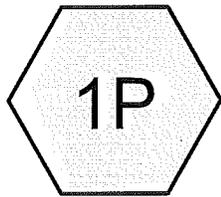
Area (sf)	CN	Description
* 16,533	98	Impervious area
16,533	98	100.00% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, TC

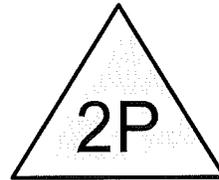
Subcatchment 1WQ: WQ BASIN

Hydrograph





Proposed Area



SC 740 Stormtech
Chambers / Flow
Control MH



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Area Listing (selected nodes)

Area (acres)	CN	Description (subcatchment-numbers)
0.380	98	Impervious Area (1P)
0.038	79	Pervious (1P)
0.418	96	TOTAL AREA

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Pipe Listing (selected nodes)

Line#	Node Number	In-Invert (feet)	Out-Invert (feet)	Length (feet)	Slope (ft/ft)	n	Diam/Width (inches)	Height (inches)	Inside-Fill (inches)
1	2P	634.44	634.09	33.8	0.0104	0.013	8.0	0.0	0.0

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Type IA 24-hr 25-year Rainfall=3.90"

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Time span=0.00-30.00 hrs, dt=0.01 hrs, 3001 points

Runoff by SBUH method, Split Pervious/Imperv.

Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

Subcatchment 1P: Proposed Area

Runoff Area=18,190 sf 90.89% Impervious Runoff Depth=3.50"
Tc=5.0 min CN=79/98 Runoff=0.36 cfs 0.122 af

Pond 2P: SC 740 Stormtech Chambers /

Peak Elev=637.64' Storage=0.010 af Inflow=0.36 cfs 0.122 af
Outflow=0.31 cfs 0.122 af

Total Runoff Area = 0.418 ac Runoff Volume = 0.122 af Average Runoff Depth = 3.50"
9.11% Pervious = 0.038 ac 90.89% Impervious = 0.380 ac

Summary for Subcatchment 1P: Proposed Area

Runoff = 0.36 cfs @ 7.88 hrs, Volume= 0.122 af, Depth= 3.50"

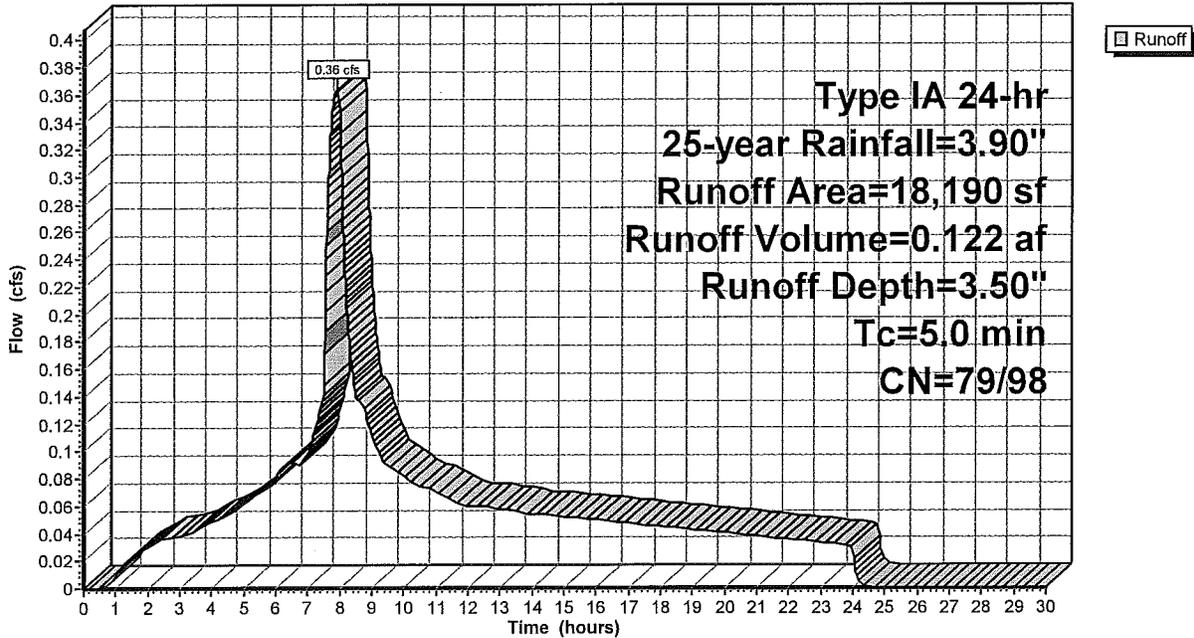
Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-30.00 hrs, dt= 0.01 hrs
 Type IA 24-hr 25-year Rainfall=3.90"

	Area (sf)	CN	Description
*	16,533	98	Impervious Area
*	1,657	79	Pervious
	18,190	96	Weighted Average
	1,657	79	9.11% Pervious Area
	16,533	98	90.89% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, TC - Proposed

Subcatchment 1P: Proposed Area

Hydrograph



Summary for Pond 2P: SC 740 Stormtech Chambers / Flow Control MH

[44] Hint: Outlet device #2 is below defined storage

[92] Warning: Device #4 is above defined storage

Inflow Area = 0.418 ac, 90.89% Impervious, Inflow Depth = 3.50" for 25-year event
 Inflow = 0.36 cfs @ 7.88 hrs, Volume= 0.122 af
 Outflow = 0.31 cfs @ 8.04 hrs, Volume= 0.122 af, Atten= 16%, Lag= 9.8 min
 Primary = 0.31 cfs @ 8.04 hrs, Volume= 0.122 af

Routing by Dyn-Stor-Ind method, Time Span= 0.00-30.00 hrs, dt= 0.01 hrs
 Peak Elev= 637.64' @ 8.04 hrs Surf.Area= 0.006 ac Storage= 0.010 af

Plug-Flow detention time= (not calculated: outflow precedes inflow)
 Center-of-Mass det. time= 17.0 min (684.3 - 667.3)

Volume	Invert	Avail.Storage	Storage Description
#1A	634.72'	0.005 af	11.00"W x 24.98'L x 3.50'H Field A 0.022 af Overall - 0.006 af Embedded = 0.016 af x 30.0% Voids
#2A	635.22'	0.006 af	ADS_StormTech SC-740 +Cap x 6 Inside #1 Effective Size= 44.6"W x 30.0"H => 6.45 sf x 7.12'L = 45.9 cf Overall Size= 51.0"W x 30.0"H x 7.56'L with 0.44' Overlap 6 Chambers in 2 Rows
		0.011 af	Total Available Storage

Storage Group A created with Chamber Wizard

Device	Routing	Invert	Outlet Devices
#1	Primary	634.44'	8.0" Round Culvert L= 33.8' CMP, end-section conforming to fill, Ke= 0.500 Inlet / Outlet Invert= 634.44' / 634.09' S= 0.0104 ' /' Cc= 0.900 n= 0.013 Concrete pipe, bends & connections, Flow Area= 0.35 sf
#2	Device 1	634.44'	1.7" Horiz. Orifice/Grate C= 0.600 Limited to weir flow at low heads
#3	Device 1	636.74'	2.7" Vert. Orifice/Grate C= 0.600
#4	Device 1	638.22'	8.0" Horiz. Orifice/Grate C= 0.600 Limited to weir flow at low heads

Primary OutFlow Max=0.31 cfs @ 8.04 hrs HW=637.64' (Free Discharge)

- 1=Culvert (Passes 0.31 cfs of 2.61 cfs potential flow)
- 2=Orifice/Grate (Orifice Controls 0.14 cfs @ 8.61 fps)
- 3=Orifice/Grate (Orifice Controls 0.17 cfs @ 4.27 fps)
- 4=Orifice/Grate (Controls 0.00 cfs)

Pond 2P: SC 740 Stormtech Chambers / Flow Control MH - Chamber Wizard Field A

Chamber Model = ADS_StormTech SC-740 +Cap (ADS StormTech® SC-740 with cap length)

Effective Size= 44.6"W x 30.0"H => 6.45 sf x 7.12'L = 45.9 cf

Overall Size= 51.0"W x 30.0"H x 7.56'L with 0.44' Overlap

51.0" Wide + 6.0" Spacing = 57.0" C-C Row Spacing

3 Chambers/Row x 7.12' Long +0.81' Cap Length x 2 = 22.98' Row Length +12.0" End Stone x 2 = 24.98' Base Length

2 Rows x 51.0" Wide + 6.0" Spacing x 1 + 12.0" Side Stone x 2 = 11.00' Base Width

6.0" Base + 30.0" Chamber Height + 6.0" Cover = 3.50' Field Height

6 Chambers x 45.9 cf = 275.6 cf Chamber Storage

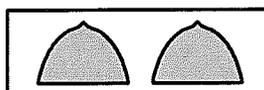
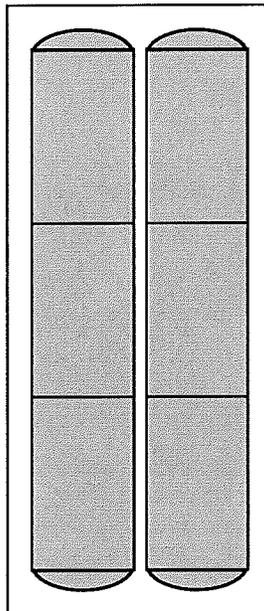
961.6 cf Field - 275.6 cf Chambers = 686.0 cf Stone x 30.0% Voids = 205.8 cf Stone Storage

Chamber Storage + Stone Storage = 481.4 cf = 0.011 af

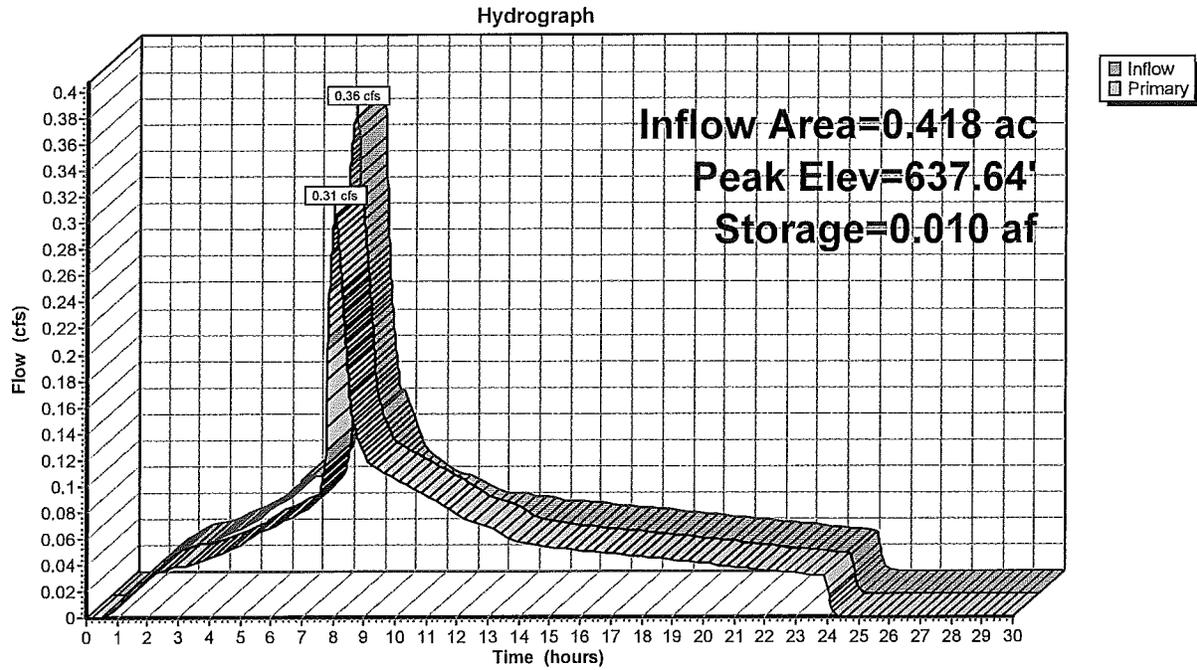
Overall Storage Efficiency = 50.1%

Overall System Size = 24.98' x 11.00' x 3.50'

6 Chambers
35.6 cy Field
25.4 cy Stone



Pond 2P: SC 740 Stormtech Chambers / Flow Control MH



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Type IA 24-hr 100-year Rainfall=4.40"

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Time span=0.00-30.00 hrs, dt=0.01 hrs, 3001 points

Runoff by SBUH method, Split Pervious/Imperv.

Reach routing by Dyn-Stor-Ind method - Pond routing by Dyn-Stor-Ind method

Subcatchment 1P: Proposed Area

Runoff Area=18,190 sf 90.89% Impervious Runoff Depth=3.99"
Tc=5.0 min CN=79/98 Runoff=0.41 cfs 0.139 af

Pond 2P: SC 740 Stormtech Chambers /

Peak Elev=638.13' Storage=0.011 af Inflow=0.41 cfs 0.139 af
Outflow=0.36 cfs 0.139 af

Total Runoff Area = 0.418 ac Runoff Volume = 0.139 af Average Runoff Depth = 3.99"
9.11% Pervious = 0.038 ac 90.89% Impervious = 0.380 ac

Summary for Subcatchment 1P: Proposed Area

Runoff = 0.41 cfs @ 7.88 hrs, Volume= 0.139 af, Depth= 3.99"

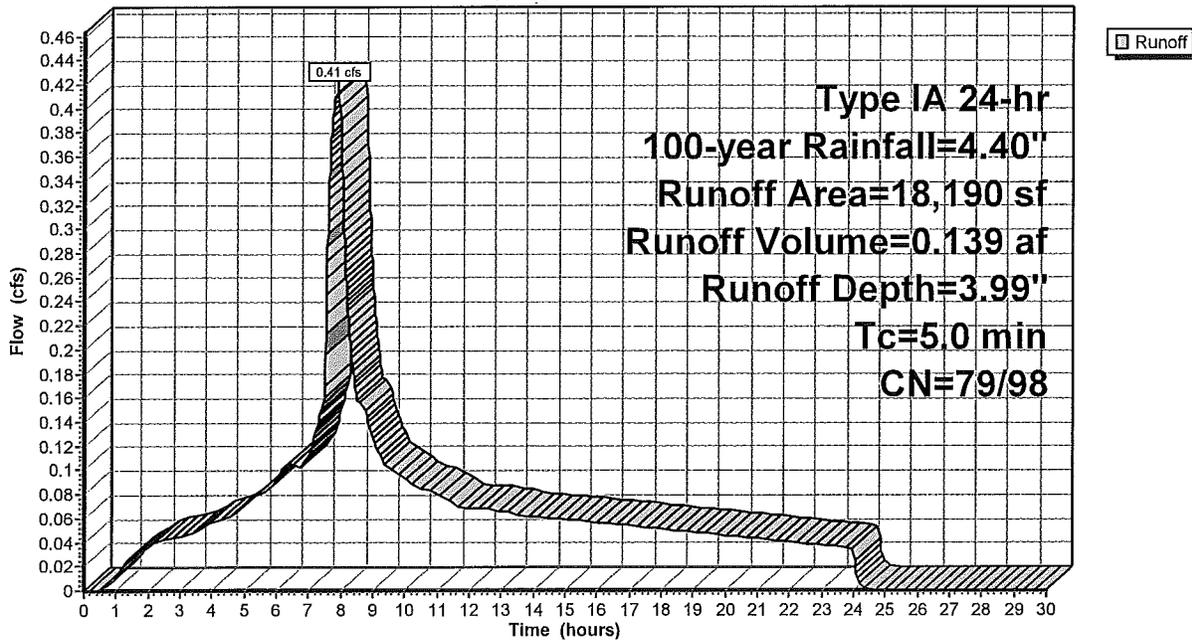
Runoff by SBUH method, Split Pervious/Imperv., Time Span= 0.00-30.00 hrs, dt= 0.01 hrs
 Type IA 24-hr 100-year Rainfall=4.40"

	Area (sf)	CN	Description
*	16,533	98	Impervious Area
*	1,657	79	Pervious
	18,190	96	Weighted Average
	1,657	79	9.11% Pervious Area
	16,533	98	90.89% Impervious Area

Tc (min)	Length (feet)	Slope (ft/ft)	Velocity (ft/sec)	Capacity (cfs)	Description
5.0					Direct Entry, TC - Proposed

Subcatchment 1P: Proposed Area

Hydrograph



Summary for Pond 2P: SC 740 Stormtech Chambers / Flow Control MH

[44] Hint: Outlet device #2 is below defined storage

[92] Warning: Device #4 is above defined storage

Inflow Area = 0.418 ac, 90.89% Impervious, Inflow Depth = 3.99" for 100-year event
 Inflow = 0.41 cfs @ 7.88 hrs, Volume= 0.139 af
 Outflow = 0.36 cfs @ 8.03 hrs, Volume= 0.139 af, Atten= 13%, Lag= 9.1 min
 Primary = 0.36 cfs @ 8.03 hrs, Volume= 0.139 af

Routing by Dyn-Stor-Ind method, Time Span= 0.00-30.00 hrs, dt= 0.01 hrs
 Peak Elev= 638.13' @ 8.03 hrs Surf.Area= 0.006 ac Storage= 0.011 af

Plug-Flow detention time= (not calculated: outflow precedes inflow)
 Center-of-Mass det. time= 19.1 min (683.6 - 664.5)

Volume	Invert	Avail.Storage	Storage Description
#1A	634.72'	0.005 af	11.00'W x 24.98'L x 3.50'H Field A 0.022 af Overall - 0.006 af Embedded = 0.016 af x 30.0% Voids
#2A	635.22'	0.006 af	ADS_StormTech SC-740 +Cap x 6 Inside #1 Effective Size= 44.6"W x 30.0"H => 6.45 sf x 7.12'L = 45.9 cf Overall Size= 51.0"W x 30.0"H x 7.56'L with 0.44' Overlap 6 Chambers in 2 Rows
		0.011 af	Total Available Storage

Storage Group A created with Chamber Wizard

Device	Routing	Invert	Outlet Devices
#1	Primary	634.44'	8.0" Round Culvert L= 33.8' CMP, end-section conforming to fill, Ke= 0.500 Inlet / Outlet Invert= 634.44' / 634.09' S= 0.0104 ' /' Cc= 0.900 n= 0.013 Concrete pipe, bends & connections, Flow Area= 0.35 sf
#2	Device 1	634.44'	1.7" Horiz. Orifice/Grate C= 0.600 Limited to weir flow at low heads
#3	Device 1	636.74'	2.7" Vert. Orifice/Grate C= 0.600
#4	Device 1	638.22'	8.0" Horiz. Orifice/Grate C= 0.600 Limited to weir flow at low heads

Primary OutFlow Max=0.36 cfs @ 8.03 hrs HW=638.13' (Free Discharge)

- 1=Culvert (Passes 0.36 cfs of 2.82 cfs potential flow)
- 2=Orifice/Grate (Orifice Controls 0.15 cfs @ 9.25 fps)
- 3=Orifice/Grate (Orifice Controls 0.22 cfs @ 5.43 fps)
- 4=Orifice/Grate (Controls 0.00 cfs)

Pond 2P: SC 740 Stormtech Chambers / Flow Control MH - Chamber Wizard Field A

Chamber Model = ADS_StormTech SC-740 +Cap (ADS StormTech® SC-740 with cap length)

Effective Size= 44.6"W x 30.0"H => 6.45 sf x 7.12'L = 45.9 cf

Overall Size= 51.0"W x 30.0"H x 7.56'L with 0.44' Overlap

51.0" Wide + 6.0" Spacing = 57.0" C-C Row Spacing

3 Chambers/Row x 7.12' Long +0.81' Cap Length x 2 = 22.98' Row Length +12.0" End Stone x 2 = 24.98' Base Length

2 Rows x 51.0" Wide + 6.0" Spacing x 1 + 12.0" Side Stone x 2 = 11.00' Base Width

6.0" Base + 30.0" Chamber Height + 6.0" Cover = 3.50' Field Height

6 Chambers x 45.9 cf = 275.6 cf Chamber Storage

961.6 cf Field - 275.6 cf Chambers = 686.0 cf Stone x 30.0% Voids = 205.8 cf Stone Storage

Chamber Storage + Stone Storage = 481.4 cf = 0.011 af

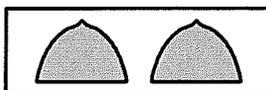
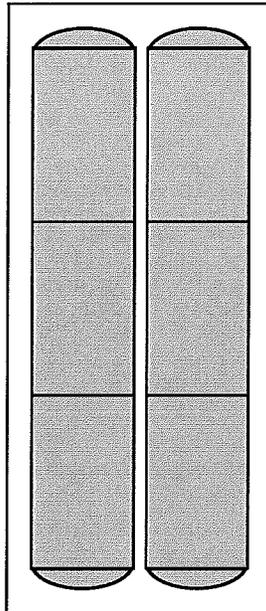
Overall Storage Efficiency = 50.1%

Overall System Size = 24.98' x 11.00' x 3.50'

6 Chambers

35.6 cy Field

25.4 cy Stone



Appendix C:

Transportation Impact Studies

Lancaster Engineering

- **SW Gibbs Street Residential Development Addendum**
 - Date: August 22, 2022
- **SW Gibbs Street Residential Development Land Division
Transportation Analysis**
 - Date: January 28, 2022

Memorandum

To: Sean O'Neill
Scotia Western States Housing, LLC

From: Daniel Stumpf, PE

Date: August 22, 2022

Subject: SW Gibbs Street Residential Development
Addendum



Introduction

This memorandum serves as an addendum to the *SW Gibbs Street Residential Development Land Division Transportation Analysis*, dated January 28, 2020. The project site is located on a single property (tax lot 151E09BD-02801) addressed at 1325 SW Gibbs Street in Portland, Oregon.

Per the January 2020 study, the proposed development reviewed impacts associated with a five-story, 27-unit apartment building. The current development plan will be to divide prior proposed 2-bedroom units into 1 bedroom and 1 studio units, subsequently increasing the dwelling unit count to 43-units. Note the site was previously developed as a senior care facility with a capacity to serve up to 20 seniors. Existing conditions are based on traffic generation of the senior care facility. The senior care facility was removed in 2021 consistent with the current development approval on the site.

The purpose of this memorandum is to re-evaluate the trip generation analysis performed in the original transportation study. Detailed calculations and supporting materials are included as an attachment to this memorandum.

Trip Generation

In the January 2020 study, trip generation of the prior proposed 27-unit apartment building was estimated utilizing data from the *Trip Generation Manual, 10th Edition*¹. Data from land use code 254, *Assisted Living*, was used to estimate the trip generation of the site under existing conditions based on the number of tenant beds. Data from land use code 221, *Multifamily Housing (Mid-Rise)*, was used to estimate the trip generation of the five-story building based on the number of dwelling units.

For this addendum, trip generation of the proposed 43-unit apartment building was estimated utilizing data from the current *Trip Generation Manual, 11th Edition*². Data from land use code 254, *Assisted Living*, was used to estimate the trip generation of the site under existing conditions based on the number of tenant beds. Data

¹ Institute of Transportation Engineers (ITE), *Trip Generation Manual*, 10th Edition, 2017.

² Institute of Transportation Engineers (ITE), *Trip Generation Manual*, 11th Edition, 2021.

from land use code 221, *Multifamily Housing (Mid-Rise)*, was used to estimate the trip generation of the five-story building based on the number of dwelling units.

The proposed 43-unit apartment building is expected to generate a net increase of 12 morning peak hour trips, 12 evening peak hour trips, and 144 average weekday trips above existing site conditions. With this change in proposed dwelling units, the proposed development scenario is projected to generate an additional 6 morning peak hour trips, 5 evening peak hour trips, and 50 average weekday trips beyond the trips generated per the January 2020 development scenario. The trip generation estimates are summarized in Table 1. Detailed trip generation calculations for the new development scenario are included as an attachment to this memorandum.

Table 1: Trip Generation Summary

	ITE Code	Size	Morning Peak Hour			Evening Peak Hour			Weekday Total
			Enter	Exit	Total	Enter	Exit	Total	
Prior 2020 Transportation Study Trip Generation									
Existing Conditions	254	20 beds	3	1	4	2	3	5	52
Proposed Conditions	221	27 units	3	7	10	7	5	12	146
Net New Trips			0	6	6	5	2	7	94
Updated Addendum Trip Generation									
Existing Conditions	254	20 beds	2	2	4	2	3	5	52
Proposed Conditions	221	43 units	4	12	16	10	7	17	196
Net New Trips			2	10	12	8	4	12	144
Net Change in Trip Generation									
Net Increase in Trips Generated			2	4	6	3	2	5	50

When considering the increase of the proposed development’s dwelling unit count, the trip generation calculations are expected to increase by 6 or less peak hour trips. These impacts are relatively low, and the existing transportation infrastructure is expected to be minimally impacted. Accordingly, the new site trips generated by the project are not expected to significantly alter the operation or safety of the existing transportation facilities beyond those detailed in the January 2020 study.

If you have any questions or concerns regarding this analysis or need further assistance, please don’t hesitate to contact us.

ZONING CODE SUMMARY

REFER TO APPROVED PLANNED DEVELOPMENT LAND USE CASE:
 LU 19-258445 PD LDP ENM TYPE III REVIEW
 SEAN WILLIAMS, STAFF PLANNER
 FRED WILSON, HEARINGS OFFICER
 APPROVED MAY 21, 2020
 EFFECTIVE DATE JUNE 5, 2020

PROPOSED AND APPROVED:
 - A 5-STORY, 27 UNIT MULTI-DWELLING STRUCTURE W/ 28 VEHICLE PARKING STALLS AND 30 LONG TERM BICYCLE PARKING SPACES
 - CREATE LOT (PARCEL 1) WITH SPLIT ZONE R7 / R1 AND AN ENVIRONMENTAL RESOURCE TRACT (TRACT A)
 - INCREASE MAX. LOT AREA TO APPROX. 22,893 SF
 - INCREASE HEIGHT LIMIT IN R1 ZONE WITHIN FIRST 10-FT OF FRONT PROP. LINE FROM 25-FT TO 45-FT
 - INCREASE HEIGHT LIMIT WITHIN R7 ZONE TO APPROX. 49-FT

PLANNING AND ZONING DEVELOPMENT STANDARDS

	REQUIRED / ALLOWABLE (PER P.D.)	PROPOSED
UNITS	27	27
HEIGHT (SEE A1.02)	45-FT W/ IN 25-FT FRONT P.L. 49-0" MAX REST OF SITE	45-FT 45-FT
FRONT SETBACK	0 FT (CONSERVATION ZONE)	1'-1" (DECK) / 3'-0" (BLDG.)
SIDE SETBACK	0 FT (CONSERVATION ZONE)	5'-0" (EAST ELEVATION)
VEHICLE PARKING	28	28
BICYCLE PARKING (SEE 1/A1.62)	30 LONG TERM 2 SHORT TERM	34 2

OUTDOOR AREA - 33,120.220
 SEE FLOOR PLANS, A2.xx SERIES, FOR INDIVIDUAL DECKS AND PATIOS.

INDIV. OUTDOOR AREA PER UNIT REQUIRED	PROVIDED
48 SF MIN. 6-FT EA. DIRECT.	62 SF 6'-5" MIN. DIMENSION

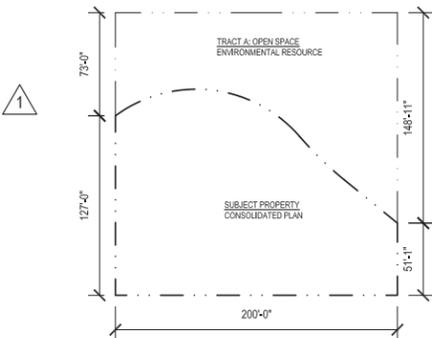
INCLUSIONARY HOUSING CALCULATIONS
 UNITS PROVIDED ON-SITE TO MEET BOTH AFFORDABLE HOUSING STANDARDS FOR DEVELOPMENTS OVER 20 UNITS, AND DENSITY BONUS. CALCULATIONS ARE FOR BUILDINGS W/ MORE THAN 20 UNITS ON SITE FOR OPTION A, 8% TOTAL AT 60% MFL.

UNITS	REQ. AFFORDABLE UNITS (8%)
27	2.16 = 2 UNITS

SEE A1.01 FOR FIRE DEPT. RELATED ITEMS

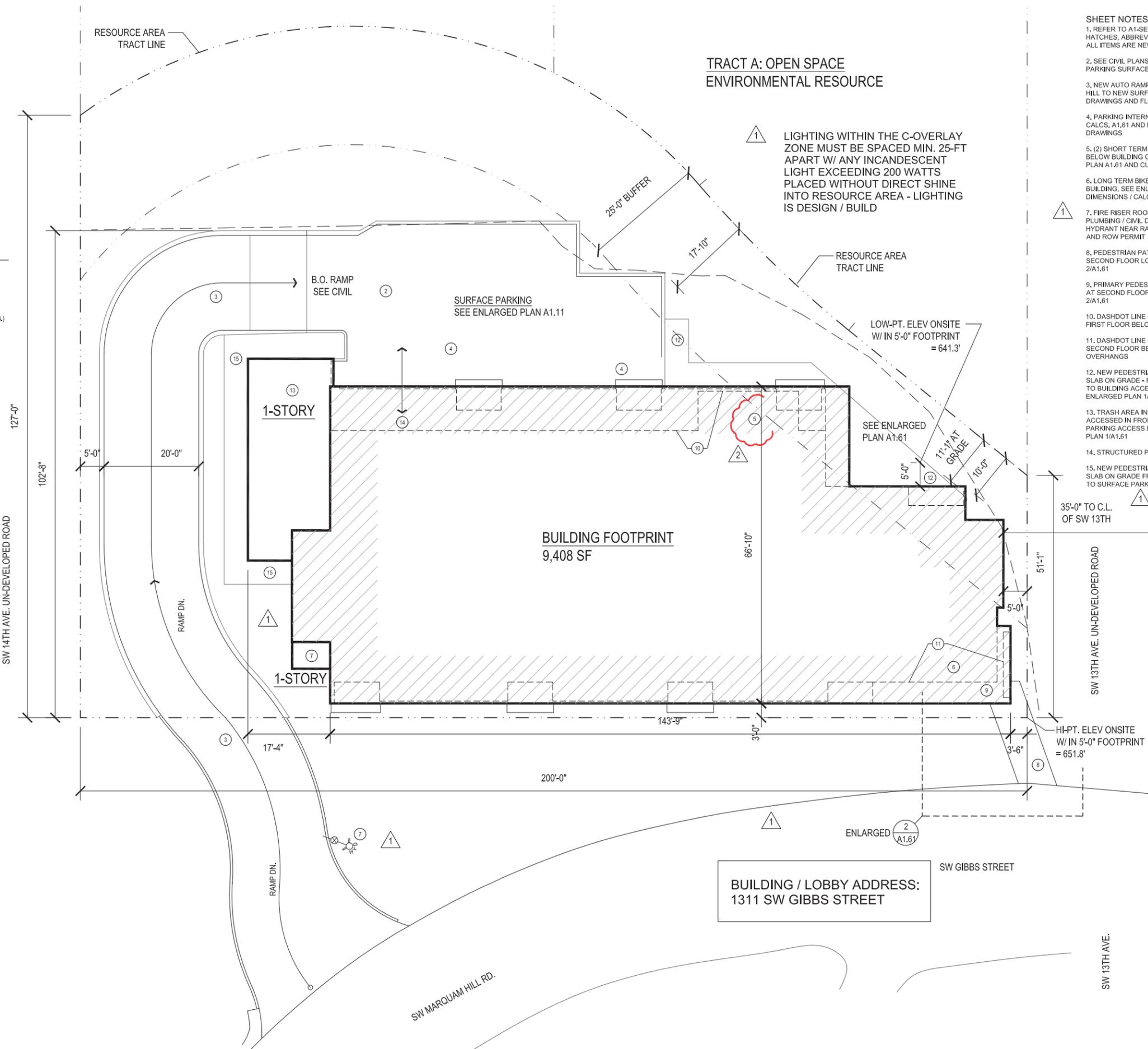
SEE A1.11 AND A1.21 FOR LIFE SAFETY SUMMARY

RELATED PERMITS:
 FP 19-258445 FINAL PLAT PERMIT
 SEAN WILLIAMS, STAFF PLANNER



2. PROPERTY LINE PLAN

SCALE: 1/64" = 1'-0" ON 22x34 1/128" = 1'-0" ON 11x17



1. SITE PLAN

SCALE: 3/32" = 1'-0" ON 22x34 3/64" = 1'-0" ON 11x17

TRACT A: OPEN SPACE ENVIRONMENTAL RESOURCE

1 LIGHTING WITHIN THE C-OVERLAY ZONE MUST BE SPACED MIN. 25-FT APART W/ ANY INCANDESCENT LIGHT EXCEEDING 200 WATTS PLACED WITHOUT DIRECT SHINE INTO RESOURCE AREA - LIGHTING IS DESIGN / BUILD

- SHEET NOTES:**
- REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 - SEE CIVIL PLANS FOR STORMWATER AND PARKING SURFACE DRAINAGE
 - NEW AUTO RAMP DOWN OFF SW MARQUAM HILL TO NEW SURFACE PARKING. SEE CIVIL DRAWINGS AND FLOOR PLANS
 - PARKING INTERNAL LANDSCAPE AREA. SEE CALCS, A1.61 AND REFER TO LANDSCAPE DRAWINGS
 - (2) SHORT TERM BIKE PARKING SPACES BELOW BUILDING OVERHANG. SEE ENLARGED PLAN A1.61 AND CUT SHEET A1.62
 - LONG TERM BIKE PARKING INSIDE BUILDING. SEE ENLARGED PLAN A1.62 FOR DIMENSIONS / CALCS. AND CUT SHEETS
 - FIRE RISER ROOM 1ST FLOOR - SEE PLUMBING / CIVIL DRAWINGS - NEW FIRE HYDRANT NEAR RAMP. SEE CIVIL DRAWINGS AND ROW PERMIT
 - PEDESTRIAN PATH FROM SW GIBBS TO SECOND FLOOR LOBBY. SEE ENLARGED PLAN 2/A1.61
 - PRIMARY PEDESTRIAN ENTRY TO BUILDING AT SECOND FLOOR. SEE ENLARGED PLAN 2/A1.61
 - DASHDOT LINE OF BUILDING OUTLINE AT FIRST FLOOR BELOW BUILDING OVERHANGS
 - DASHDOT LINE OF BUILDING OUTLINE AT SECOND FLOOR BELOW BUILDING OVERHANGS
 - NEW PEDESTRIAN ACCESS PATHS - CONC. SLAB ON GRADE - FROM SURFACE PARKING TO BUILDING ACCESS AT FIRST FLOOR. SEE ENLARGED PLAN 1/A1.61 AND CIVIL
 - TRASH AREA IN FIRST FLOOR ENCLOSURE. ACCESSED IN FRONT OF STRUCTURED PARKING ACCESS DOOR. SEE ENLARGED PLAN 1/A1.61
 - STRUCTURED PARKING GARAGE ACCESS
 - NEW PEDESTRIAN ACCESS PATH - CONC. SLAB ON GRADE FROM STAIR WELL EGRESS TO SURFACE PARKING / BOTTOM OF RAMP

**BUILDING / LOBBY ADDRESS:
 1311 SW GIBBS STREET**



RECEIVED 04/22/21

PERMIT SET
 SEPT. 23, 2020
 REV. 1 02/12/21
 REV. 2 04/21/21



TRIP GENERATION CALCULATIONS
Source: Trip Generation Manual, 11th Edition
Existing Conditions

Land Use: Assisted Living
Land Use Code: 254
Land Use Subcategory: All Sites
Setting/Location: General Urban/Suburban
Variable: Beds
Trip Type: Vehicle
Variable Quantity: 20

AM PEAK HOUR

Trip Rate: 0.18

	Enter	Exit	Total
Directional Split	60%	40%	
Trip Ends	2	2	4

PM PEAK HOUR

Trip Rate: 0.24

	Enter	Exit	Total
Directional Split	39%	61%	
Trip Ends	2	3	5

WEEKDAY

Trip Rate: 2.60

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	26	26	52

SATURDAY

Trip Rate: 2.93

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	29	29	58



TRIP GENERATION CALCULATIONS
Source: Trip Generation Manual, 11th Edition
Proposed Conditions

Land Use: Multifamily Housing (Mid-Rise)
Land Use Code: 221
Land Use Subcategory: Not Close to Rail Transit
Setting/Location: General Urban/Suburban
Variable: Dwelling Units
Trip Type: Vehicle
Variable Quantity: **43**

AM PEAK HOUR

Trip Rate: 0.37

	Enter	Exit	Total
Directional Split	23%	77%	
Trip Ends	4	12	16

PM PEAK HOUR

Trip Rate: 0.39

	Enter	Exit	Total
Directional Split	61%	39%	
Trip Ends	10	7	17

WEEKDAY

Trip Rate: 4.54

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	98	98	196

SATURDAY

Trip Rate: 4.57

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	98	98	196

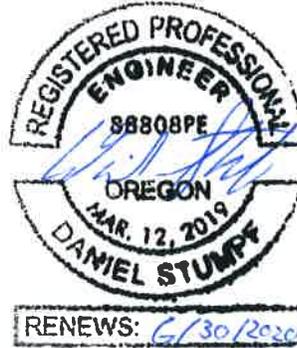
Technical Memorandum

To: Sean O'Neill
Scotia Western States Housing, LLC

From: Daniel Stumpf, PE
Melissa Webb, PE

Date: January 28, 2020

Subject: SW Gibbs Street Residential Development
Land Division Transportation Analysis



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Introduction

This memorandum describes our findings regarding transportation safety and operation within the vicinity of the proposed SW Gibbs Street Residential Development, located at 1325 SW Gibbs Street in Portland, Oregon. The report assesses the applicable transportation-related analyses described in Section 33.641.020 – *Transportation Impacts*, Section 33.654 – *Rights-of-Way*, and Section 33.854.310.B – *Transportation System*, of the City of Portland’s Municipal Code. Transportation-related approval criteria for land divisions, as identified in Section 33.641.020.A of the City of Portland’s *Title 33 – Planning and Zoning* code, requires an analysis showing that the transportation system is capable of supporting the proposed use in addition to existing uses.

Location Description

Project Site Description

The subject site is located within a predominately residential area of southwest Portland, with undeveloped land to the north and west, and single-family detached houses to east and south. The project site includes tax lots 2700, 2800, 2900, and 3000, which comprises approximately 0.92 acres. The property currently accommodates a senior care facility which may serve up to 12 off-street parked vehicles (the existing parking lot is occasionally used to park more vehicles than what is currently striped).

The proposed project will include construction of a five-story, 27-unit apartment building with 28 off-street vehicle parking spaces and storage for up to 30 bicycles. The existing senior care facility and parking lot will be removed to accommodate the planned housing. The current site access will be relocated to a position further west along SW Gibbs Street.

Vicinity Roadways

The proposed use is expected to impact six nearby roadways. Table 1 provides a description of each of the vicinity roadways.

Table 1: Vicinity Roadway Descriptions

Roadway	Jurisdiction	Functional Classification	Cross-Section	Speed	On-street Parking	Bicycle Lanes	Curbs	Sidewalks
SW Marquam Hill Road	City of Portland	City Walkway, City Bikeway, LS Transit Street, LS Truck Street, Neighborhood Collector, Major ER	2 Lanes	25 mph Posted	Partially Permitted	None	None	None
SW Gibbs Street	City of Portland	City Walkway, City Bikeway, LS/Major Transit Priority Street, LS Truck Street, Neighborhood Collector, Major ER	2 Lanes	20-25 mph Posted	Partially Permitted	None	Partial Both Sides	Partial Both Sides
SW 11 th Avenue	City of Portland	LS/City Walkway, LS/City Bikeway, LS/Major Transit Priority Street, LS Truck Street, LS Traffic Street/Neighborhood Collector, Minor ER	2 Lanes	20 mph Statutory	Partially Permitted	None	Partial Both Sides	Partial Both Sides
SW Sam Jackson Park Road	City of Portland	City Walkway, City Bikeway, LS/Major Transit Priority Street, Truck Access Street, Neighborhood Collector, Major ER	2-3 Lanes	20 mph Posted	Partially Permitted	None	Partial Both Sides	Partial Both Sides
SW US Veterans Hospital Road	City of Portland	City Walkway, City Bikeway, Major Transit Priority/Transit Access Street, LS/Truck Access Street, Neighborhood Collector, Minor ER	2-3 Lanes	25 mph Posted	Partially Permitted	None	Partial Both Sides	Partial Both Sides
SW Terwilliger Boulevard	City of Portland	City Walkway, Major City Bikeway, Major Transit Priority/Transit Access Street, LS/Truck Access Street, Neighborhood Collector, Major ER	2-3 Lanes	25 mph Posted	Not Permitted	Both Sides	Partial Both Sides	Partial Both Sides

Notes: Functional Classification based on the City of Portland's TSP.
 Local Service denoted as LS. Emergency Response denoted as ER.

Vicinity Intersections

A majority of site trips generated by the planned use are expected to impact three intersections of significance in the site vicinity. A summarized description of these intersections is provided in Table 2.

Table 2: Vicinity Intersection Descriptions

Number	Intersection	Geometry	Traffic Control	Phasing/Stopped Approaches
1	SW Sam Jackson Park Road at SW Terwilliger Boulevard	Three-Legged	Signalized	Protected WB Left-turn, Protected NB Left-turn, Protected NB Right-turn
2	SW Sam Jackson Park Road at SW US Veterans Hospital Road	Three-Legged	Stop-Controlled	NB Stop-Controlled
3	SW Gibbs Street at SW 11 th Avenue	Four-Legged	Stop-Controlled	NB/SB Stop-Controlled

Figure 1 presents an aerial image of the nearby vicinity with the study intersections circled and project site outlined in yellow.

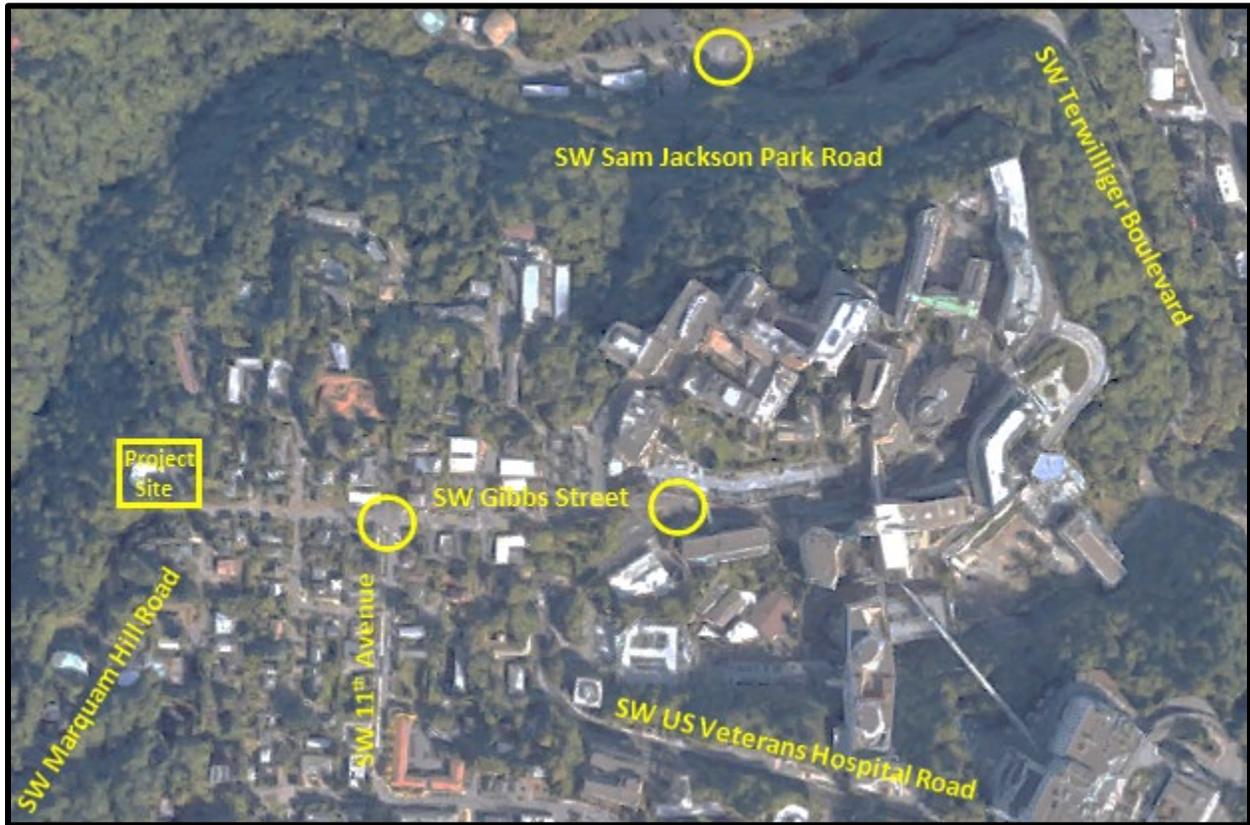


Figure 1: Aerial Photo of Site Vicinity (Image from Google Earth)

Trip Generation & Distribution

The proposed SW Gibbs Street Residential Development will include the construction of a five-story, 27-unit apartment building. To estimate the number of trips generated by the proposed use, trip rates from the *Trip Generation Manual*¹ were used. Data from land use code 254, *Assisted Living*, was used to estimate the trip generation of the site under existing conditions. Data from land use code 221, *Multifamily Housing (Mid-Rise)*, was used to estimate the trip generation of the five-story building based on the number of dwelling units.

The trip generation calculations show that the proposed project is projected to generate 6 additional morning peak hour trips, 7 additional evening peak hour trips, and 94 additional average weekday trips. The trip generation estimates are summarized in Table 3. Detailed trip generation calculations are included as an attachment to this study.

¹ Institute of Transportation Engineers (ITE), *Trip Generation Manual*, 10th Edition, 2017.

Table 3: Trip Generation Summary

	ITE Code	Size	Morning Peak Hour			Evening Peak Hour			Weekday Total
			Enter	Exit	Total	Enter	Exit	Total	
Existing Assisted Living	254	20 beds	3	1	4	2	3	5	52
Proposed Multifamily Housing (Mid-Rise)	221	27 dwelling units	3	7	10	7	5	12	146
Net New Site Trips			0	6	6	5	2	7	94

The trip distribution of site trips to/from the project site was estimated based on the locations of likely trip destinations and major transportation facilities in the site vicinity. The following trip distribution was estimated and used for analysis:

- Approximately 60 percent of site trips will travel to/from the north along SW Terwilliger Boulevard;
- Approximately 25 percent of site trips will travel to/from the south along SW 11th Avenue;
- Approximately 10 percent of site trips will travel to/from the south along SW US Veterans Hospital Road; and
- Approximately 5 percent of site trips will travel to/from the south along SW Marquam Hill Road.

Satisfaction of Land Division Evaluation Factors

Regarding the proposed 27-unit residential development, City of Portland's *Title 33 – Planning and Zoning* code identifies several evaluation factors in *Section 33.641.020 – Transportation Impacts* that are used to determine if the transportation system can safely support the planned use. These factors include the following:

- Safety;
- Street capacity;
- Level of service;
- Connectivity;
- Transit availability;
- Availability of pedestrian and bicycle networks;
- On-street parking impacts;
- Access restrictions;
- Neighborhood impacts; and
- Impacts on pedestrian bicycle and transit circulation.

Safety

Motor Vehicle Safety

Using data obtained from the Oregon Department of Transportation's (ODOT) Crash Data System, a review of the most recent available five years of crash history (from January 2013 to December 2017) at the study intersections was performed. The crash data was evaluated based on the number of crashes, the type of collisions, and the severity of collisions at the intersections.

Eight crashes were reported at the intersection of SW Sam Jackson Park Road at SW Terwilliger Boulevard during the analysis period. The crashes consisted of five rear-end collisions, one turning movement collision, one sideswipe collision, and one fixed-object collision. Four crashes were classified as *Property Damage Only* (PDO), and four were classified as *Possible Injury – Complaint of Pain* (Injury C).

Four crashes were reported at the intersection of SW Sam Jackson Park Road at SW US Veterans Hospital Road during the analysis period. The crashes consisted of two rear-end collisions and two turning movement collisions. Three of the crashes were classified as *Property Damage Only* (PDO), and one was classified as *Possible Injury – Complaint of Pain* (Injury C).

One crash was reported at the intersection of SW 11th Avenue at SW Gibbs Street during the analysis period. The crash was a backing collision which was classified as *Property Damage Only* (PDO).

Based on the most recent five years of available crash data, no significant safety hazards were identified at any of the study intersections. Additionally, no design deficiencies which would impact the safety of area

roadways and intersections were noted based on observations conducted within the site vicinity. Crash reports for the study intersections are included as an attachment to this memorandum.

Bicyclist Safety

There are six nearby bicycle routes within an approximate one-mile walking/biking distance of the site: SW Sam Jackson Park Road/SW Gibbs Street/SW Marquam Hill Road, SW 11th Avenue/SW Gaines Street/SW 6th Avenue Drive, SW Campus Drive, SW Terwilliger Boulevard, SW Fairmont Boulevard, and SW Council Crest Drive. Four of the bicycle routes generally serve light traffic volumes traveling at low speeds (statutory and posted speeds of 20 mph to 25 mph), while the other two provide striped bicycle lanes (specifically SW Campus Drive and SW Terwilliger Boulevard). Although considered a shared roadway with bicyclists, the segment of SW Sam Jackson Park Road between SW US Veterans Hospital Road and SW Campus Drive serves high volumes of traffic and should be traversed with caution. In addition, neighborhood streets in the immediate site vicinity that are not classified as bicycle routes are typically low-stress roadways that provide alternative routes of travel to these and other nearby bicycle paths. Accordingly, the surrounding bicycle infrastructure provides a safe and comfortable environment for bicyclists.

Pedestrian Safety

Sidewalks are partially complete along nearby area roadways. When sidewalks are not available along local streets, roadway speeds (posted and statutory speeds of 20 mph to 25 mph) and traffic volumes are generally lower, allowing pedestrians the ability to safely and comfortably walk along roadway shoulders when necessary. Marked crossings are provided across SW Gibbs Street and SW Sam Jackson Park Road at six locations between SW 11th Avenue and SW Campus Drive. The proposed use will not remove or block any existing walkways within the area and will construct frontage improvements such as sidewalks along SW Gibbs Street. Accordingly, whereby safety and connectivity in the immediate vicinity will be improved.

Transit User Safety

The nearest bus stops that could reasonably serve transit users of the proposed use and nearby existing land uses are located at/near the intersections of SW Gibbs Street at SW 11th Avenue and SW Sam Jackson Park Road at SW US Veterans Hospital Road. Sidewalks to and from the above-mentioned transit stop locations are generally provided; however, for segments of roadway where sidewalks are not available, relatively low travel speeds and traffic volumes allow transit users the ability to safely traverse along roadway shoulders when necessary. In addition, adequate crossing measures (including marked crosswalks across SW Gibbs Street and SW Sam Jackson Park Road) are provided.

Based on the safety analysis, the transportation system is capable of safely supporting the proposed use in addition to the nearby existing land uses within the site vicinity.

Street Capacity and Level of Service

Observations of traffic conditions during the weekday morning and evening peak periods were conducted to ensure that the transportation system can adequately accommodate future trips to/from the proposed use in addition to the existing uses within the site vicinity. Specifically, traffic observations were conducted at the three intersections described in Table 2 during the morning and evening peak periods of Wednesday, October 16th, 2019, from approximately 8:15 AM to 8:50 AM and from approximately 4:10 PM to 4:40 PM.

Intersections are generally evaluated based on the average control delay experienced by vehicles at an intersection and are assigned a grade according to their operation. The level of service (LOS) of an intersection can range from LOS A, which indicates very little or no delay experienced by vehicles, to LOS F, which indicates a high degree of congestion and delay. The City of Portland LOS standards require unsignalized intersections to operate at LOS E or better, and signalized intersections to operate at LOS D or better. Detailed LOS descriptions are included as an attachment to this report.

The intersection of SW Sam Jackson Park Road at SW Terwilliger Boulevard was observed for approximately 10 minutes during both the morning and evening peak periods. During both peak periods, traffic entering the intersection from each of the approaches appeared to be relatively even. During both peak periods, the maximum observed queues at the intersection ranged between 0 to 5 vehicles for all approaches, all of which cleared within their respective green times per cycle. Accordingly, the intersection was estimated as currently operating at LOS A/B during both the morning and evening peak periods.

The intersection of SW Sam Jackson Park Road at SW US Veterans Hospital Road was observed having 14 vehicles enter the intersection from the minor-street approach over the course of approximately 10 minutes during the morning peak period, while 19 vehicles were observed entering the intersection over the course of approximately 10 minutes during the evening peak period. Observed queues never exceeded more than 3 vehicles during the morning peak period and 5 vehicles during the evening peak period. Traffic volumes along the major-street were observed to be in a state of free-flow, where gaps to enter the major-street traffic stream from the minor-street approach were available within an average control delay of approximately 10 to 15 seconds during both peak periods. Accordingly, the intersection was estimated to currently operate at LOS B during both the morning and evening peak periods.

The intersection of SW Gibbs Street at SW 11th Avenue was observed having 11 vehicles enter the intersection from the minor-street approaches over the course of approximately 10 minutes during both the morning and evening peak periods. Observed queues never exceeded more than 2 vehicles during either peak period. Traffic volumes along the major-street were observed to be in a state of free-flow, where gaps to enter the major-street traffic stream from either minor-street approach were available within an average control delay of approximately 5 to 10 seconds during both peak periods. Accordingly, the intersection was estimated to currently operate at LOS A during both the morning and evening peak periods.

The results of the capacity observations indicate that all three study intersections operate within City standards for LOS during the weekday morning and evening peak periods under existing conditions. Based on the low volumes of additional weekday morning and evening peak hour trips expected to be added to the transportation system following buildout and occupancy of the proposed apartment building, the study intersections are expected to continue operating within acceptable City standards in the future either with or without the proposed use. Therefore, no mitigation is necessary or recommended with regard to intersection capacity or operation as part of the proposed development.

It should be noted that according to City of Portland's code *TRN-10.27 – Traffic Capacity Analysis for Land Use Review Cases*:

3. *An amendment or other land use application that requires analysis of traffic capacity and allows development that either (1) may cause a transportation facility to perform below the standards established in sections 1 and 2, or (2) adds vehicle trips to a facility that is already performing below the standards established in sections 1 and 2 may be approved if:*
 - a. *Development resulting from the amendment or other land use application will mitigate the impacts of the amendment or other land use application in a manner that avoids further degradation to the performance of the facility by the time of development through one or more of the following:*
 - (i) *the development is limited to result in no net increase in vehicle trips over what is allowed by the existing zoning; OR*
 - (ii) *one or more combination of transportation improvements or measures are imposed to mitigate the transportation impacts of the amendment or other land use application in a manner that avoids further degradation to the performance of the facility by the time of any development.*

Based on the above code, the proposed development will not generate more trips than what is currently allowed under the existing zone. Accordingly, capacity-related impacts associated with the proposed development to any transportation facility will not require mitigation as part of the development of the proposed use.

Connectivity

The project site is located within a neighborhood where the transportation grid system is reasonably complete, providing multiple redundant routes to/from the site via motor vehicle and other modes of travel. The nearby local roads provide connectivity to other nearby higher classification roadways including SW Sam Jackson Park Road, SW Marquam Hill Road, SW US Veterans Hospital Road, and SW Terwilliger Boulevard.

From these roadways, access to other major north/south corridors (including SW Barbur Boulevard, Interstate 5, and Interstate 405) as well as major east/west corridors (including Highway 26) are available. The aforementioned roadways will serve as the primary routes of travel between the site and the greater Portland Metro Area.

Transit Availability

The project site is located near six TriMet transit bus lines. The bus lines have several stops located within a half-mile walking/biking distance of the site. Partial sidewalks are available between the site and the nearest stops which serve the transit lines. When sidewalks are not available along local streets, roadway speeds (posted and statutory speeds of 20 mph to 25 mph) and traffic volumes are generally lower, allowing pedestrians the ability to safely and comfortably walk along roadway shoulders to access transit stops. A summarized description of the transit lines is shown in Table 4.

Table 4: Transit Line Descriptions

Transit Line (TriMet)	Service Area	Service Time			Typical Headways (Minutes)	Nearest Stops
		Day	From	To		
Bus Line #8 - Jackson Park/NE 15 th	Marquam Hill, Portland City Center, Old Town/Chinatown, Union Station, Rose Quarter, NE Portland	Wk	4:45 AM	2:00 AM	10 to 60	SW Gibbs Street at SW 11 th Avenue
		Sat	5:30 AM	2:00 AM	15 to 60	
		Sun	6:00 AM	1:00 AM	15 to 60	
Bus Line #61 - Marquam Hill/Beaverton	Beaverton Transit Center, Veterans Hospital, OHSU, Marquam Hill	Wk	5:10 AM	7:10 PM	15 to 60	SW Sam Jackson Park Road at SW US Veterans Hospital Road
		Sat	-	-	-	
		Sun	-	-	-	
Bus Line #64 - Marquam Hill/Tigard	Tigard Transit Center, Barbur Transit Center, Veterans Hospital, OHSU, Marquam Hill	Wk	5:00 AM	7:30 PM	15 to 60	SW Sam Jackson Park Road at SW US Veterans Hospital Road
		Sat	-	-	-	
		Sun	-	-	-	
Bus Line #65 - Marquam Hill/Barbur Blvd	Burlingame, Veterans Hospital, OHSU, Shriners Hospital, Marquam Hill	Wk	6:15 AM	5:45 PM	30 to 60	SW Sam Jackson Park Road at SW US Veterans Hospital Road
		Sat	-	-	-	
		Sun	-	-	-	
Bus Line #66 - Marquam Hill/Hollywood	Hollywood Transit Center, SE Portland, Veterans Hospital, OHSU, Marquam Hill	Wk	4:45 AM	7:30 PM	10 to 60	SW Sam Jackson Park Road at SW US Veterans Hospital Road
		Sat	-	-	-	
		Sun	-	-	-	
Bus Line #68 - Marquam Hill/Collins Cirde	Collins Cirde, Portland City Center, PSU, Marquam Hill	Wk	5:00 AM	7:00 PM	15 to 40	SW Sam Jackson Park Road at SW US Veterans Hospital Road
		Sat	-	-	-	
		Sun	-	-	-	

BOLDED text indicates frequent service.

In addition to the above-mentioned TriMet transit lines, the Portland Aerial Tram has a Marquam Hill Upper Tram Terminal located approximately one-half mile from the proposed residential development. This aerial commuter tram connects Marquam Hill with the South Waterfront.

Availability of Pedestrian and Bicycle Networks

Sidewalks are partially complete along nearby area roadways. When sidewalks are not available along local streets, roadway speeds (posted and statutory speeds of 20 mph to 25 mph) and traffic volumes are generally lower, allowing pedestrians the ability to safely and comfortably walk along roadway shoulders. In addition, the proposed development will include the construction of frontage improvements, such as sidewalks, along SW Gibbs Street.

There are several pedestrian routes near the proposed residential development. The *Southwest Portland Walking Map* identifies a 4-T trail and signed urban trail along SW Gibbs Street and SW Marquam Hill Road, plus several additional pedestrian off-street paths/trails that can be accessed from SW Gibbs Street and SW Marquam Hill Road.

The project site is located within an approximate one-mile walking/biking distance of six bicycle routes: SW Sam Jackson Park Road/SW Gibbs Street/SW Marquam Hill Road, SW 11th Avenue/SW Gaines Street/SW 6th Avenue Drive, SW Campus Drive, SW Terwilliger Boulevard, SW Fairmont Boulevard, and SW Council Crest Drive.

SW Sam Jackson Park Road/SW Gibbs Street/SW Marquam Hill Road will likely serve as the primary east/west bicycle route in the immediate site vicinity. From this route, access to other north/south bicycle routes, such as SW Campus Drive, SW 11th Avenue/SW Gaines Street/SW 6th Avenue Drive, and SW Fairmont Boulevard, are available. Four of the bicycle routes generally serve light traffic volumes traveling at low speeds (statutory and posted speeds of 20 mph to 25 mph), while the other two provide striped bicycle lanes (specifically SW Campus Drive and SW Terwilliger Boulevard). Although considered a shared roadway with bicyclists, the segment of SW Sam Jackson Park Road between SW US Veterans Hospital Road and SW Campus Drive serves high volumes of traffic and should be traversed with caution. Additionally, neighborhood streets in the immediate site vicinity are typically low-stress roadways that provide alternative routes of travel to these and other nearby bicycle paths as depicted in the City of Portland's *Southwest Portland Bicycle Map*.

Based on an assessment of surrounding area, sufficient pedestrian and bicycle facilities are available within the site vicinity to adequately and safely serve the proposed use in addition to the surrounding existing uses.

On-Street Parking Impacts

City of Portland's *Title 33 – Planning and Zoning* code identifies minimum required and maximum allowed parking spaces based on use in *Section 33.266.110 – Minimum Required Parking Spaces*. Specifically, Table 266-1 and Table 266-2 provide a minimum standard of 1 parking space per residential dwelling unit, and have no maximum standard. Given the proposed development will be designed to provide 28 off-street vehicular

parking spaces, the proposed use will have sufficient off-street parking to accommodate residents as required per City code.

Access Restrictions

Upon reviewing the surrounding transportation system, there do not appear to be any existing restrictions for access to or from the site.

The proposed use is not expected to generate significant volumes of vehicular traffic nor will it generate heavy vehicle traffic. In addition, the proposed use will largely match the residential traffic characteristics of the surrounding vicinity, whereby travel patterns will not significantly alter from what is currently observable in the area. Accordingly, no access restrictions along any nearby vicinity roadways are necessary or recommended in conjunction with the proposed use.

Neighborhood Impacts

The daily activities associated with the proposed development are expected to minimally impact the local neighborhood. As described within this study, the trips generated by the proposed residential development are not expected to have a significant impact on the operation or safety of the nearby street system, and the development will not have a significant impact on the on-street parking supply in the neighborhood.

In addition, because the proposed use is residential in nature and predominately surrounded by other similar residential uses, the proposed use will not substantially alter existing travel patterns or vehicle types in the site vicinity, and will be in conformance with the residential character of the neighborhood.

Based on the detailed analysis, the project will have minimal impacts to the surrounding neighborhood. Accordingly, no mitigation is necessary or recommended.

Impacts on Pedestrian, Bicycle, and Transit Circulation

As described within the *Safety and Availability of Pedestrian and Bicycle Networks* sections, there are comfortable and safe walking/biking routes as well as safe routes to access transit facilities available within the site vicinity. The proposed use will not create any new barriers to these travel modes and safe circulation routes will remain available within the site vicinity following approval of the proposed use.

Section 33.654.110 – Connectivity and Location of Rights-of-Way

Regarding the proposed 27-unit residential development, City of Portland's *Title 33 – Planning and Zoning* code identifies approval criteria in *Section 33.654 - Rights-of-Way* to ensure provision of efficient access to as many

lots as possible, and enhance direct movement by pedestrians, bicycles, and motor vehicles between destinations. Specifically, *Section 33.654.110.B* states “*In OS, R, C, E, CI, and IR zones, through streets and pedestrian connections are required where appropriate and practicable.*”

The project site consists of four lots, the southern two of which will be redeveloped as an apartment building and parking lot. The proposed land division will create an environmental tract along the northern section of the site where a protected resource area is located, inclusive of the two existing northern lots. Accordingly, through street and pedestrian/bicycle connections to this tract are not reasonable or practical.

Regarding off-site lots, properties to the north and east of the site are already developed as residential uses and have both vehicle and pedestrian/bicycle access to the nearby transportation system via routes not currently provided on-site. Property to the west of the site is zoned as *Open Space (OS)*, and in conjunction with topography, cannot reasonably redevelop in the future. Accordingly, vehicular access to the west of the site is not reasonable or practical. Pedestrian paths through this OS zoned land are already provided and the proposed development will not restrict or limit access to these existing pedestrian paths.

Section 33.854.310 – Approval Criteria for Planned Developments in All Zones

Under City of Portland’s *Title 33 – Planning and Zoning* code, approval criteria related to planned developments in all zones are identified in *Section 33.854.310*. Upon reviewing the approval criteria, subsection B pertains to the transportation system where:

The transportation and circulation system provides multimodal connections that support the development of the site, and limit impacts to adjacent neighborhoods.

Although the proposed use is considered a planned development, subsection B is applicable only to specific commercial zones with uses applying for additional height or FAR. Specifically, the code section reads:

Criteria A through F apply to proposals for additional height or FAR in the CM2, CM3, CE, and CX zones that are taking advantage of 33.270.100.I. If the Planned Development is not proposing additional height or FAR as allowed by 33.270.100.I, then only criteria E and F apply.

Accordingly, code Section 33.854.310 is satisfied for the propose use with regard to addressing subsection B. However, it should be noted that throughout this technical memorandum, it has been demonstrated that the transportation and circulation system provides significant opportunities within the site vicinity for multimodal travel, whereby transportation impacts associated with the proposed use can be sufficiently accommodated without significant impacts to the surrounding neighborhood.

Sight Distance Analysis

Sight distances at the proposed site access along SW Marquam Hill Road were measured and evaluated in accordance with the standards established in *A Policy on Geometric Design of Highways and Streets*². These measurements were conducted to determine whether the proposed access could operate safely and efficiently with regard to sight distance at its current proposed location.

Sight Distance Definitions and Methodologies

According to the AASHTO manual, “If the available sight distance for an entering or crossing vehicle is at least equal to the appropriate stopping sight distance for the major road, then drivers have sufficient sight distance to anticipate and avoid collisions. However, in some cases, a major-road vehicle may need to stop or slow to accommodate the maneuver by a minor-road vehicle. To enhance traffic operations, intersection sight distances that exceed stopping sight distances are desirable along the major road.”

Intersection sight distance is an operational measure, intended to provide sufficient line of sight along the major-street so that a driver can enter the roadway without impeding the flow of through traffic. For intersection sight distance, the driver’s eye is assumed to be 15 feet from the near edge of the nearest travel lane (or traveled way) of the intersecting street and at a height of 3.5 feet above the minor-street approach pavement. The oncoming vehicle driver’s eye height along the major-street approach is assumed to be 3.5 feet above the cross-street pavement.

Stopping sight distance is considered the minimum requirement to ensure safe operation of the driveway. This distance allows the driver of a vehicle traveling on the major-street to react to a turning vehicle, or other object in the roadway, and come to a complete stop to avoid a collision. To ensure safe operation of a driveway, the extent of available intersection sight distance must at least equal the minimum required stopping sight distance.

Sight Distance Measurements

Based on a posted speed of 25 mph along the applicable roadway segments of SW Gibbs Street and SW Marquam Hill Road, the minimum recommended intersection sight distance to ensure safe operation of the site access onto the roadway is 280 feet, while the minimum required stopping sight distance is 155 feet (assuming an approach grade no greater than 3 percent). At the proposed access location, sight distances were measured to be in excess of 300 feet to the east along SW Gibbs Street, while sight distance to the south along SW Marquam Hill Road was limited to 196 feet by foliage along the inside turn in the road.

² American Association of State Highway and Transportation Officials (AASHTO), *A Policy on Geometric Design of Highways and Streets*, 6th Edition, 2011.

Although sight distance to the south of the proposed access location was measured to be less the minimum recommended intersection sight distance standard, based on an average roadway approach grade of 10.9 percent (downhill) between the proposed access location to a location 200 feet to the south, there is sufficient stopping sight distance to accommodate a northbound approaching vehicle traveling at 26 mph. It should be noted that on this approach there is a posted advisory speed sign of 15 mph prior to entering the turn in the road, whereby vehicles are generally expected to be traveling at speeds slower than 25 mph in order to safely navigate through the turn. Therefore, it is expected that vehicles approaching from this direction will have sufficient stopping sight distance to either slow down or come to a complete stop to accommodate a site egressing vehicle turning left from the access.

Based on the detailed sight distance analysis of the proposed access intersection, there is sufficient intersection and stopping sight distances to the east and south of the access, respectively, to ensure safe operation for major-street approaching vehicles along SW Gibbs Street and SW Marquam Hill Road. Accordingly, no sight distance related mitigation is necessary or recommended as part of the proposed development.

Conclusions

The impacts of the proposed 27-unit residential development to the existing transportation infrastructure are expected to be minimal and acceptable. New site trips generated by the project are not expected to significantly alter the operation or safety of the existing transportation facilities. The proposed residential development is not expected to impact on-street parking availability along vicinity streets because the parking lot will maintain sufficient supply to accommodate observed demand.

The City of Portland's transportation impact approval criteria are met for the proposed residential development, as the transportation system is capable of safely supporting the proposed use in addition to the existing land uses in the area. No safety or operational issues are expected to occur upon implementation of the proposed project. Accordingly, no mitigation is necessary or recommended.

If you have any questions regarding this technical memorandum, please don't hesitate to contact us.

Appendix

Attachment A

TRAFFIC SCOPE APPROVAL FORM

Prior to starting a traffic study, a Traffic Scope Approval Form must be completely filled out, submitted for review, and approved by PBOT's Development Review Traffic Engineer. The approved form shall be included in every traffic study submittal as Attachment A. PBOT may require additional analysis/information during the course of the review of the project. This Traffic Scope Approval Form is for City requirements only. Consultants must contact ODOT to determine requirements related to access permits and work in ODOT right-of-way.

SITE / PROJECT INFORMATION

PROJECT NAME: SW Gibbs Street Residential Dev. ASSOCIATED APPLICATION (EA#/LU#) EA 19-133418

SITE LOCATION: 1325 SW Gibbs Street
(ADDRESS/ID #)

BRIEF PROJECT DESCRIPTION (NUMBER OF STORIES, TOTAL AREA, NUMBER OF PARKING SPACES, ETC.):

The project will include constructing a four-story, 30-unit multifamily building as well as two duplexes (a total of 34 dwelling units with off-street parking and bike storage provided). The site is currently developed as a senior care facility with a building square footage of approximately 8,000 SF with 20 rooms. The current site access will be relocated to a position further west along SW Gibbs Street.

Possibly revised to be 20-unit multi-family.

APPLICANT: Sean O'Neill, Scotia Western States Housing, LLC

TRAFFIC ENGINEER / FIRM: Lancaster Engineering DATE: 9/26/2019

SITE PLAN – ATTACH SITE PLAN

REQUIRED LAND USE REVIEW(S)

- LAND DIVISION (33.641) CONDITIONAL USE (33.815) ADJUSTMENT (33.805)
- CENTRAL CITY PARKING REVIEW (33.808) COMPREHENSIVE PLAN AMENDMENT (33.810)
- ZONING MAP AMENDMENT (33.855) TRANSPORTATION IMPACT REVIEW (33.852)
- OTHER: 33.654 (specifically connectivity), 33.854.310.B



APPROVAL CRITERIA (LIST ALL EVALUATION FACTORS)

33.641.020.A & B: Safety; Street Capacity; Level of Service; Connectivity; Transit Availability; Availability of Pedestrian and Bicycle Networks; On-street Parking Impacts; Access Restrictions; Neighborhood Impacts; and Impacts on Pedestrian, Bicycle, and Transit Circulation.

33.654: Connectivity

33.854.310.B: The transportation and circulation system provides multimodal connections that support the development of the site, and limit impacts to adjacent neighborhoods.

REQUIRED TRAFFIC STUDY REVIEW(S)

- | | | |
|--|---|---|
| <input checked="" type="checkbox"/> TRAFFIC IMPACT STUDY
(ATTACHMENT A.1) | <input type="checkbox"/> PARKING IMPACT STUDY
(ATTACHMENT A.2) | <input type="checkbox"/> LOADING DEMAND STUDY
(ATTACHMENT A.3) |
| <input type="checkbox"/> ASTR TRAFFIC STUDY
(ATTACHMENT A.4) | <input type="checkbox"/> TDM PLAN
(ATTACHMENT A.5) | <input checked="" type="checkbox"/> SIGHT DISTANCE STUDY |
| <input type="checkbox"/> QUEUING ANALYSIS | <input type="checkbox"/> ALT TRIP RATE STUDY | <input type="checkbox"/> OTHER: _____ |

ADDITIONAL COMMENTS:

There is limited on-street parking available near the site. The proposed use will provide off-street vehicular and bike parking/storage facilities. Accordingly, no Parking Impact Study is expected to be necessary.

APPROVALS

APPROVED BY: Amanda Owings, PE
TRAFFIC ENGINEER
PBOT DEVELOPMENT REVIEW

SIGNATURE: 
DATE: 10/10/2019

Email approval forms and supporting documents in pdf format to PBOTDevRevTrafficScopes@portlandoregon.gov.
Option to mail or hand-deliver forms to:

Attn: Amanda Owings
City of Portland, Building Plan & Development Review
1900 SW 4th Avenue, Suite 5000 (5th floor)
Portland, Oregon 97201

PBOT

PORTLAND BUREAU OF TRANSPORTATION

1900 SW Fourth Avenue, Suite 5000 Portland, OR 97201 503.823.5185
 Fax 503.823.7576 TTY 503.823.6868 www.portlandoregon.gov/transportation

Chloe Eudaly Commissioner Chris Warner Interim Director

Attachment A.1 TRAFFIC IMPACT STUDY

This traffic study attachment must be completely filled out and submitted for review along with the Traffic Scope Approval Form – Attachment A.

STUDY PARAMETERS

LIST OF PROPOSED STUDY INTERSECTIONS	
1. Sam Jackson Park Rd/Terwilliger Blvd	2. Sam Jackson Park Rd/US Veterans Hosp. Rd
3. SW Gibbs St / SW 11th Ave	4.
5.	6.
7.	8.

TRAFFIC COUNTS TO BE COLLECTED

24-HR VOLUME (HOSE) COUNTS, LOCATION: _____

TURNING MOVEMENT COUNTS AT ALL STUDY INTERSECTIONS

AM PEAK HOUR PM PEAK HOUR OTHER: _____

TYPE OF TRAFFIC COUNT DATA

DATA WITHIN THE LAST 2 YEARS ARE AVAILABLE

PBOT Traffic Counts page has recent speed data on SW Marquam Hill Road

NEW DATA WILL BE COLLECTED ON (DAY/TIME): _____

FUTURE COUNTS WILL BE ESTIMATED BASED ON A _____ GROWTH RATE

IN-PROCESS TRIPS – APPLICANT TO RESEARCH AND DOCUMENT APPROVED DEVELOPMENT IN THE AREA (PBOT WILL NOT PROVIDE THIS INFORMATION)



TRIP GENERATION RATES – ATTACH TRIP GENERATION TABLE

FROM ITE OTHER: _____

MODE SPLIT _____ PASSBY _____ INTERNAL CAPTURE _____

TRIP DISTRIBUTION – ATTACH TRIP DISTRIBUTION AND TRIP ASSIGNMENT FIGURE

KEY ANALYSIS ITEMS – CHECK ALL RELEVANT ANALYSES

OPERATIONAL LEVEL-OF-SERVICE (LOS) STUDY AT STUDY INTERSECTIONS IN THE PEAK HOUR(S)

FIELD MEASURED CONTROL DELAY STUDY AT STUDY INTERSECTIONS IN THE PEAK HOUR(S)

TRAFFIC SIGNAL WARRANTS AT _____

LEFT/RIGHT TURN LANE WARRANTS AT _____

QUEUING ANALYSIS AT _____

CRASH HISTORY ANALYSIS AT STUDY INTERSECTIONS

PARKING IMPACT STUDY (ATTACHMENT A.2)

SIGHT DISTANCE STUDY AT STUDY INTERSECTIONS AND PROPOSED DRIVEWAYS

FINDINGS MADE RELATED TO APPLICABLE APPROVAL CRITERIA

OTHER _____

SUBMIT WITH ATTACHMENT A.

EMAIL APPROVAL FORMS AND SUPPORTING DOCUMENTS IN PDF FORMAT TO:

PBOTDevRevTrafficScopes@portlandoregon.gov

DELIVERABLES – ALL FINAL STUDIES MUST BE SUBMITTED AT THE TIME OF LAND USE APPLICATION. ALL FINAL STUDIES AND SUBSEQUENT REVISIONS MUST BE PROVIDED TO THE ASSIGNED PBOT PLANNER IN THE FORM OF TWO (2) HARD COPIES, ONE (1) ELECTRONIC WORD DOC, AND ONE (1) ELECTRONIC ADOBE PDF.

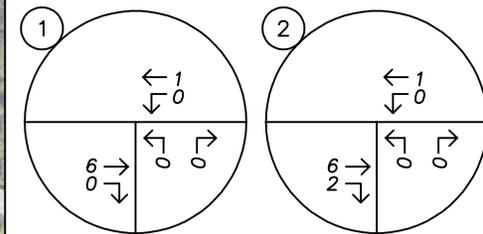


LEGEND

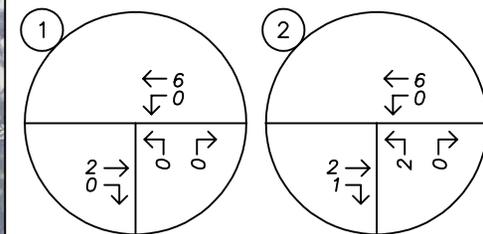
XX% PERCENT OF PROJECT TRIPS

NET TRIP GENERATION			
	IN	OUT	TOTAL
AM	1	9	10
PM	9	3	12

AM PEAK HOUR



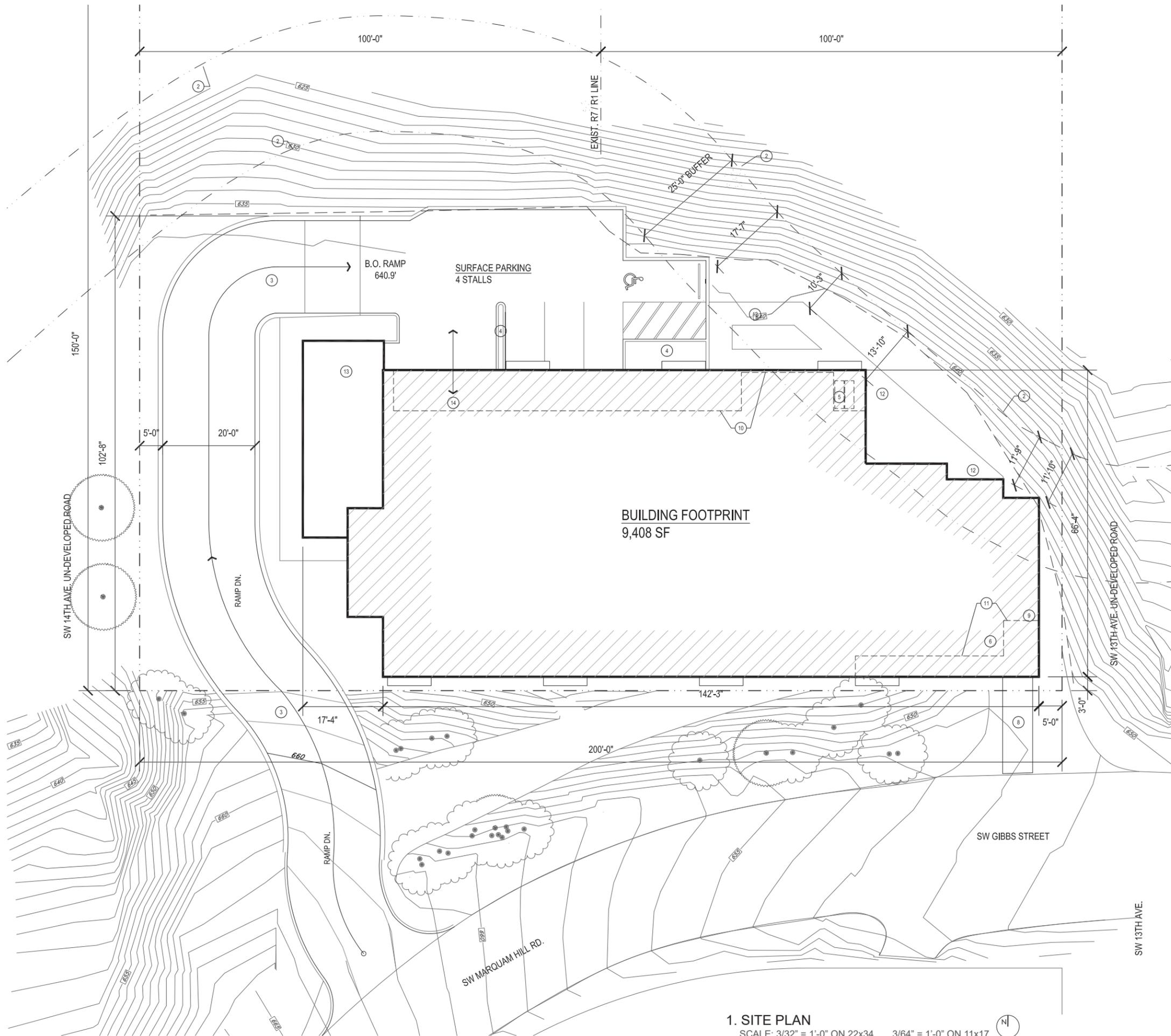
PM PEAK HOUR



SITE TRIP DISTRIBUTION AND ASSIGNMENT



FIGURE 1



- SHEET NOTES:**
- REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 - LINE OF CONSERVATION ZONE AND 25-FT BUFFER ZONE
 - NEW AUTO RAMP DOWN OFF SW MARQUAM HILL TO NEW SURFACE PARKING. SEE CIVIL DRAWINGS AND FLOOR PLANS
 - PARKING INTERNAL LANDSCAPE AREA. SEE CALCS. THIS SHEET AND REFER TO LANDSCAPE DRAWINGS
 - (2) SHORT TERM BIKE PARKING SPACES BELOW BUILDING OVERHANG. SEE FIRST FLOOR PLAN - CONCRETE PAD W/ 'STAPLE-STYLE' BIKE RACK
 - LONG TERM BIKE PARKING INSIDE BUILDING. SEE FIRST FLOOR PLAN AND CALCS THIS SHEET
 - DASHED LINE OF BELOW GRADE VAULTS WITH ACCESS COVERS FLUSH W/ SLAB. SEE CIVIL DRAWINGS
 - PEDESTRIAN BRIDGE FROM SW GIBBS TO SECOND FLOOR LOBBY. SEE FLOOR PLANS
 - PRIMARY PEDESTRIAN ENTRY TO BUILDING AT SECOND FLOOR
 - DASHDOT LINE OF BUILDING OUTLINE AT FIRST FLOOR BELOW BUILDING OVERHANGS
 - DASHDOT LINE OF BUILDING OUTLINE AT SECOND FLOOR BELOW BUILDING OVERHANGS
 - NEW PEDESTRIAN ACCESS PATHS - CONC. SLAB ON GRADE - FROM SURFACE PARKING TO BUILDING ACCESS AT FIRST FLOOR. SEE FLOOR PLANS
 - TRASH AREA IN FIRST FLOOR ENCLOSURE. ACCESSED IN FRONT OF STRUCTURED PARKING ACCESS DOOR. SEE FLOOR PLANS
 - STRUCTURED PARKING GARAGE ACCESS
 - NEW PEDESTRIAN ACCESS PATH - CONC. SLAB ON GRADE FROM STAIR WELL EGRESS TO SURFACE PARKING / BOTTOM OF RAMP

PLANNING AND ZONING DEVELOPMENT STANDARDS

BLDG.	DECK	TTL.	
UNITS:	27	27	27
BLDG. COVERAGE (SF):	9,408	108	9,516
LOT AREA:	23,500 SF (DIV. 9,516)		
COVERAGE:	40%		
BLDG. AREA (SF):			
FL.1	8,513	-	8,513
FL.2	8,092	614	8,706
FL.3	8,284	443	8,727
FL.4	8,284	443	8,727
FL.5	8,284	443	8,727
TOTALS	41,457 SF	1,943	43,400

	REQUIRED / ALLOWABLE	MAX. PROPOSED
HEIGHT	45 FT (R1) / 30 FT (R7)	44'-6"
FRONT SETBACK	0 FT (CONSERVATION ZONE)	1'-1" (DECK) / 3'-0" (BLDG.)
SIDE SETBACK	0 FT (CONSERVATION ZONE)	5'-0" (EAST ELEVATION)

STREET FACING FACADES:
15% GLAZING REQUIRED. 33,120,232

	FACADE AREA - SF	GLAZ. AREA / % - SF / %
SW GIBBS:		

PARKING:

BIKE PARKING: (33,266,220)	1,1 / UNIT 27 UNITS = 30 REQ. PROPOSED: BIKE ROOM DOUBLE STACK = 6 BIKE ROOM SINGLE STACK = 24 TOTAL = 30 PROVIDED
VEHICLE PARKING: (33,266)	1 PER UNIT = 27 DEDUCT 2 FOR TREE PRESERVATION = 25 REQ. 28 PROVIDED
SURFACE PARKING	45 SF / SPACE (x4) = 180 SF
INTERNAL LANDSCAPE	184 SF PROVIDED (SEE 1ST FLOOR PLAN)

1. SITE PLAN
SCALE: 3/32" = 1'-0" ON 22x34 3/64" = 1'-0" ON 11x17

NOT FOR CONSTRUCTION

SITE PLAN

1325 SW GIBBS STREET
PORTLAND, OR 97239

PLANS
JAN. 20, 2020



TRIP GENERATION CALCULATIONS
Proposed Conditions (Three-Story Building)

Land Use: Multifamily Housing (Mid-Rise)
Land Use Code: 221
Setting/Location: General Urban/Suburban
Variable: Dwelling Units
Variable Value: 27

AM PEAK HOUR

Trip Rate: 0.36

	Enter	Exit	Total
Directional Distribution	26%	74%	
Trip Ends	3	7	10

PM PEAK HOUR

Trip Rate: 0.44

	Enter	Exit	Total
Directional Distribution	61%	39%	
Trip Ends	7	5	12

WEEKDAY

Trip Rate: 5.44

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	73	73	146

SATURDAY

Trip Rate: 4.91

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	66	66	132

Source: TRIP GENERATION, Tenth Edition



TRIP GENERATION CALCULATIONS Existing Conditions

Land Use: Assisted Living
Land Use Code: 254
Setting/Location: General Urban/Suburban
Variable: Beds
Variable Value: 20

AM PEAK HOUR

Trip Rate: 0.19

	Enter	Exit	Total
Directional Distribution	63%	37%	
Trip Ends	3	1	4

PM PEAK HOUR

Trip Rate: 0.26

	Enter	Exit	Total
Directional Distribution	38%	62%	
Trip Ends	2	3	5

WEEKDAY

Trip Rate: 2.60

	Enter	Exit	Total
Directional Distribution	50%	50%	
Trip Ends	26	26	52

Source: TRIP GENERATION, Tenth Edition

OREGON.. DEPARTMENT OF TRANSPORTATION - TRANSPORTATION DEVELOPMENT DIVISION
TRANSPORTATION DATA SECTION - CRASH ANALYSIS AND REPORTING UNIT
URBAN NON-SYSTEM CRASH LISTING

CITY OF PORTLAND SW, MULTNOMAH COUNTY

SAM JACKSON PARK RD at TERWILLIGER BLVD, City of Portland SW, Multnomah County, 01/01/2013 to 12/31/2018

1 - 4 of 8 Crash records shown.

SER#	P	R	J	S	W	DATE	CLASS	CITY STREET	INT-TYPE	SPCL USE	ACT	EVENT	CAUSE																		
INVEST	E	A	U	I	C	O	DAY	DIST	FIRST STREET	RD CHAR	(MEDIAN)	INT-REL	OFFERD	WTHR	CRASH	TRLR QTY	MOVE	A	S												
RD DPT	E	L	G	N	H	R	TIME	FROM	SECOND STREET	DIRECT	LEGS	TRAF-	RNDBT	SURF	COLL	OWNER	FROM	PRTC	INJ	G	E	LICNS	PED								
UNLOC?	D	C	S	V	L	K	LAT	LONG	LRS	LOCTN	(#LANES)	CONTL	DRVWY	LIGHT	SVRTY	V#	TYPE	TO	P#	TYPE	SVRTY	E	X	RES	LOC	ERROR	ACT	EVENT	CAUSE		
11850	N	N	N				10/06/2016	17	SW TERWILLIGER BLVD	INTER	3-LEG	N	N	CLR	S-1STOP	01	NONE	0	STRGHT									013	29		
NO RPT							TH	0	SW SAM JACKSON PARK RD	E		TRF SIGNAL	N	DRY	REAR		PRVTE		E -W									000	00		
N							2P			06	0		N	DAY	INJ		PSNGR CAR			01	DRVR	NONE	28	F	OTH-Y	026	000	000	29		
N							45 30 10.32	-122 41 15.22																							
																	02	NONE	0	STOP								011	013	00	
																	PRVTE		E -W								000	000	00	00	
																	PSNGR CAR			01	DRVR	NONE	44	F	OR-Y	000	000	000	00	00	
																	03	NONE	0	STOP								022	00	00	
																	PRVTE		E -W								000	000	00	00	
																	PSNGR CAR			01	DRVR	INJC	43	F	OTH-Y	000	000	000	00	00	
																	03	NONE	0	STOP								022	00	00	
																	PRVTE		E -W								000	000	00	00	
																	PSNGR CAR			02	PSNG	INJC	00	F		000	000	000	00	00	
01679	N	N	N				02/20/2013	17	SW SAM JACKSON PARK RD	INTER	3-LEG	N	N	RAIN	S-1STOP	01	NONE	0	STRGHT										07		
NONE							WE	0	SW TERWILLIGER BLVD	S		TRF SIGNAL	N	WET	REAR		PRVTE		S -N									000	00		
N							4P			06	0		N	DAY	INJ		PSNGR CAR			01	DRVR	NONE	00	M	OR-Y	026	000	000	07		
N							45 30 10.3237919	-122 41 15.220176																							
																	02	NONE	0	STOP									011	00	
																	PRVTE		S -N									000	000	00	00
																	PSNGR CAR			01	DRVR	INJC	49	F	OR-Y	000	000	000	00	00	
01653	N	N	N				02/07/2014	17	SW SAM JACKSON PARK RD	INTER	3-LEG	N	N	CLR	S-1STOP	01	NONE	0	STRGHT										29		
NONE							FR	0	SW TERWILLIGER BLVD	S		TRF SIGNAL	N	WET	REAR		PRVTE		S -N									000	00		
N							4P			06	1		N	DAY	PDO		PSNGR CAR			01	DRVR	NONE	60	F	OR-Y	026,014	000	000	29		
N							45 30 10.3237919	-122 41 15.2202119																							
																	02	NONE	0	STOP									011	00	
																	PRVTE		S -N									000	000	00	00
																	PSNGR CAR			01	DRVR	NONE	70	F	OR-Y	000	000	000	00	00	
08557	N	N	N				08/10/2015	17	SW SAM JACKSON PARK RD	INTER	3-LEG	N	N	CLR	ANGL-OTH	01	UNKN	0	TURN-R										08		
NONE							MO	0	SW TERWILLIGER BLVD	S		TRF SIGNAL	N	DRY	TURN		UNKN		W -S									000	00		
N							8A			06	1		N	DAY	INJ		UNKNOWN			01	DRVR	NONE	00	Unk	UNK	001	000	000	00	08	
N							45 30 10.32	-122 41 15.22																							
																	02	NONE	0	TURN-L									000	00	
																	PRVTE		S -W									000	000	00	00
																	PSNGR CAR			01	DRVR	INJC	32	M	OR-Y	000	000	000	00	00	

Disclaimer: The information contained in this report is compiled from individual driver and police crash reports submitted to the Oregon Department of Transportation as required in ORS 811.720. The Crash Analysis and Reporting Unit is committed to providing the highest quality crash data to customers. However, because submittal of crash report forms is the responsibility of the individual driver, the Crash Analysis and Reporting Unit can not guarantee that all qualifying crashes are represented nor can assurances be made that all details pertaining to a single crash are accurate. Note: Legislative changes to DMV's vehicle crash reporting requirement, effective 01/01/2004, may result in fewer property damage only crashes being eligible for inclusion in the Statewide Crash Data File.

CITY OF PORTLAND SW, MULTNOMAH COUNTY

SAM JACKSON PARK RD at TERWILLIGER BLVD, City of Portland SW, Multnomah County, 01/01/2013 to 12/31/2018

5 - 8 of 8 Crash records shown.

SER#	P	R	J	S	W	DATE	CLASS	CITY STREET	INT-TYPE	SPCL USE	ACT	EVENT	CAUSE																	
INVEST	E	A	U	I	C	O	DAY	DIST	FIRST STREET	RD CHAR	(MEDIAN)	INT-REL	OFFERD	WTHR	CRASH	TRLR QTY	MOVE	A	S											
RD DPT	E	L	G	N	H	R	TIME	FROM	SECOND STREET	DIRECT	LEGS	TRAF-	RNDBT	SURF	COLL	OWNER	FROM	PRTC	INJ	G	E	LICNS	PED							
UNLOC?	D	C	S	V	L	K	LAT	LONG	LRS	LOCTN	(#LANES)	CONTL	DRVWY	LIGHT	SVRTY	V#	TYPE	TO	F#	TYPE	SVRTY	E	X	RES	LOC	ERROR	ACT	EVENT	CAUSE	
00491	N	N	N			01/15/2013	17	SW SAM JACKSON PARK RD	INTER	3-LEG	N		N	CLR	S-1STOP	01	NONE	0	STRGHT											07
NONE						TU	0	SW TERWILLIGER BLVD	W			TRF SIGNAL	N	DRY	REAR		UNKN		W -E									000	00	
N						10A			06	0			N	DAY	PDO		PSNGR CAR			01	DRVR	NONE	00	M	UNK	UNK	026	000	07	
N						45 30	-122 41																							
						10.3237919	15.220176																							
																	02	NONE	0	STOP									011	00
																	PRVTE		W -E									000	00	
																	PSNGR CAR			01	DRVR	NONE	61	F	OR-Y	OR<25	000	000	00	
																	02	NONE	0	STOP								011	00	
																	PRVTE		W -E									000	00	
																	PSNGR CAR			02	PSNG	NONE	82	M			000	000	00	
11833	Y	N	N			11/02/2013	17	SW SAM JACKSON PARK RD	INTER	3-LEG	N		Y	CLR	FIX OBJ	01	NONE	0	STRGHT										079	01
NONE						SA	0	SW TERWILLIGER BLVD	W			TRF SIGNAL	N	WET	FIX		PRVTE		E -W									000	079	00
N						9A			05	0			N	DAY	PDO		PSNGR CAR			01	DRVR	NONE	60	F	OR-Y	OR<25	047,080	017	01	
N						45 30	-122 41																							
						10.3237919	15.220176																							
12502	N	N	N			11/20/2014	17	SW SAM JACKSON PARK RD	INTER	3-LEG	N		N	RAIN	S-1STOP	01	NONE	0	TURN-R											07
NONE						TH	0	SW TERWILLIGER BLVD	CN			TRF SIGNAL	N	WET	REAR		PRVTE		S -E										000	00
N						4P			04	0			N	DUSK	INJ		PSNGR CAR			01	DRVR	NONE	48	F	OR-Y	OR<25	026	000	07	
N						45 30 10.32	-122 41																							
						15.22																								
																	02	NONE	0	STOP									013	00
																	PRVTE		S -E									000	000	00
																	PSNGR CAR			01	DRVR	NONE	34	F	OR-Y	OR>25	000	000	00	
																	02	NONE	0	STOP								013	00	
																	PRVTE		S -E									000	000	00
																	PSNGR CAR			02	PSNG	INJC	12	F			000	000	00	
11418	N	N	N			10/17/2017	17	SW SAM JACKSON PARK RD	INTER	3-LEG	N		N	CLR	S-STRGHT	01	NONE	9	STRGHT											14,13
NONE						TU	0	SW TERWILLIGER BLVD	CN			TRF SIGNAL	N	DRY	SS-O		N/A		E -W										000	00
N						7A			02	1			N	DAY	PDO		PSNGR CAR			01	DRVR	NONE	00	Unk	UNK	UNK	000	000	00	
N						45 30 10.32	-122 41																							
						15.22																								
																	02	NONE	9	STRGHT									000	00
																	N/A		E -W									000	00	
																	PSNGR CAR			01	DRVR	NONE	00	Unk	UNK	UNK	000	000	00	

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OREGON.. DEPARTMENT OF TRANSPORTATION - TRANSPORTATION DEVELOPMENT DIVISION
TRANSPORTATION DATA SECTION - CRASH ANALYSIS AND REPORTING UNIT
URBAN NON-SYSTEM CRASH LISTING

CITY OF PORTLAND SW, MULTNOMAH COUNTY

SAM JACKSON PARK RD at VETERANS HOSP RD, City of Portland SW, Multnomah County, 01/01/2013 to 12/31/2018

1 - 4 of 4 Crash records shown.

SER#	P	R	J	S	W	DATE	CLASS	CITY STREET	INT-TYPE	SPCL USE	ACT	EVENT	CAUSE																
INVEST	E	A	U	I	C	O	DAY	FIRST STREET	RD CHAR	(MEDIAN)	INT-REL	OFFERD	WTHR	CRASH	TRLR	QTY	MOVE	A	S										
RD DPT	E	L	G	N	H	R	TIME	SECOND STREET	DIRECT	LEGS	TRAF-	RNDBT	SURF	COLL	OWNER	FROM	PRTC	INJ	G	E	LICNS	PED							
UNLOC?	D	C	S	V	L	K	LAT	LONG	LOCTN	(#LANES)	CONTL	DRVWY	LIGHT	SVRTY	V#	TYPE	TO	P#	TYPE	SVRTY	E	X	RES	LOC	ERROR	ACT	EVENT	CAUSE	
03481	N	N	N			04/08/2014	17	SW SAM JACKSON PARK RD	INTER	3-LEG	N	N	RAIN	S-1STOP	01	NONE	0	STRGHT										07	
NONE						TU	0	SW VETERANS HOSP RD	E		STOP SIGN	N	WET	REAR		PRVTE	E -W										000	00	
N						11P			06	0		N	DARK	PDO		UNKNOWN			01	DRVR	NONE	00	M	UNK	UNK	026	000	07	
N						45 29 57.16	-122 41 17.06									02	NONE	0	STOP								011	00	
																PRVTE	E -W										000	000	00
																PSNGR	CAR		01	DRVR	NONE	21	F	OR-Y	OR<25	000	000	00	
07445	N	N	N			07/13/2015	17	SW SAM JACKSON PARK RD	INTER	3-LEG	N	N	CLR	S-1STOP	01	NONE	0	STRGHT										29	
NONE						MO	0	SW VETERANS HOSP RD	SW		STOP SIGN	N	DRY	REAR		PUBLIC	SW-NE										000	00	
N						4P			06	0		N	DAY	INJ		OTH BUS			01	DRVR	NONE	38	F	OTH-Y	N-RES	026	000	29	
N						45 29 57.16	-122 41 17.06									02	NONE	0	STOP								011	00	
																PRVTE	SW-NE										000	000	00
																PSNGR	CAR		01	DRVR	INJC	38	M	OR-Y	OR<25	000	000	00	
01107	N	N	N			02/02/2015	17	SW SAM JACKSON PARK RD	INTER	3-LEG	N	N	CLR	ANGL-OTH	01	NONE	0	TURN-R										02	
NONE						MO	0	SW VETERANS HOSP RD	CN		STOP SIGN	N	DRY	TURN		PRVTE	SW-E										015	00	
N						7P			04	0		N	DLIT	PDO		PSNGR	CAR			01	DRVR	NONE	68	F	OR-Y	OR<25	028	000	02
N						45 29 57.16	-122 41 17.06									02	NONE	0	STRGHT								000	000	00
																PRVTE	W -E										000	000	00
																PSNGR	CAR		01	DRVR	NONE	21	M	OR-Y	OR<25	000	000	00	
13169	N	N	N			11/23/2015	17	SW SAM JACKSON PARK RD	INTER	3-LEG	N	N	CLR	ANGL-OTH	01	UNKN	9	STRGHT										02	
NO RPT						MO	0	SW VETERANS HOSP RD	CN		STOP SIGN	N	DRY	TURN		UNKN	W -E										000	00	
N						9A			04	0		N	DAY	PDO		PSNGR	CAR			01	DRVR	NONE	00	M	UNK	UNK	000	000	00
N						45 29 57.16	-122 41 17.06									02	NONE	0	TURN-L								015	00	
																PRVTE	SW-W										000	000	00
																PSNGR	CAR		01	DRVR	NONE	17	M	OR-Y	OR<25	028	000	00	

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LEVEL OF SERVICE

Level of service is used to describe the quality of traffic flow. Levels of service A to C are considered good, and rural roads are usually designed for level of service C. Urban streets and signalized intersections are typically designed for level of service D. Level of service E is considered to be the limit of acceptable delay. For unsignalized intersections, level of service E is generally considered acceptable. Here is a more complete description of levels of service:

Level of service A: Very low delay at intersections, with all traffic signal cycles clearing and no vehicles waiting through more than one signal cycle. On highways, low volume and high speeds, with speeds not restricted by other vehicles.

Level of service B: Operating speeds beginning to be affected by other traffic; short traffic delays at intersections. Higher average intersection delay than for level of service A resulting from more vehicles stopping.

Level of service C: Operating speeds and maneuverability closely controlled by other traffic; higher delays at intersections than for level of service B due to a significant number of vehicles stopping. Not all signal cycles clear the waiting vehicles. This is the recommended design standard for rural highways.

Level of service D: Tolerable operating speeds; long traffic delays occur at intersections. The influence of congestion is noticeable. At traffic signals many vehicles stop, and the proportion of vehicles not stopping declines. The number of signal cycle failures, for which vehicles must wait through more than one signal cycle, are noticeable. This is typically the design level for urban signalized intersections.

Level of service E: Restricted speeds, very long traffic delays at traffic signals, and traffic volumes near capacity. Flow is unstable so that any interruption, no matter how minor, will cause queues to form and service to deteriorate to level of service F. Traffic signal cycle failures are frequent occurrences. For unsignalized intersections, level of service E or better is generally considered acceptable.

Level of service F: Extreme delays, resulting in long queues which may interfere with other traffic movements. There may be stoppages of long duration, and speeds may drop to zero. There may be frequent signal cycle failures. Level of service F will typically result when vehicle arrival rates are greater than capacity. It is considered unacceptable by most drivers.



*LEVEL OF SERVICE CRITERIA
FOR SIGNALIZED INTERSECTIONS*

LEVEL OF SERVICE	CONTROL DELAY PER VEHICLE (Seconds)
A	<10
B	10-20
C	20-35
D	35-55
E	55-80
F	>80

*LEVEL OF SERVICE CRITERIA
FOR UNSIGNALIZED INTERSECTIONS*

LEVEL OF SERVICE	CONTROL DELAY PER VEHICLE (Seconds)
A	<10
B	10-15
C	15-25
D	25-35
E	35-50
F	>50



Portland Tree Consultancy

Lou Phemister
ASCA Registered Consulting Arborist #590
(573) 999-3886 / louphemister@outlook.com

FINAL ARBORIST REPORT

Scotia Western States Property

DATE: July 19th, 2020

PROPERTY ADDRESS: 1325 SW Gibbs St., Portland OR 97221

CLIENT REFERENCE: Winterbrook Planning

PROJECT DESCRIPTION: Land Use Review for 30-unit multi-dwelling building.

Introduction

Improvements to this property consist of construction of a 30-unit multi-dwelling unit within the footprint of the existing building and existing paved footprint. Proposed right of way improvements and a new access point to the development site will result in further paving and fill within the vegetated bank sloping down to the main development site on the south side of the property. The site contains conservation zoning. The building construction, construction work itself, and construction access will occur outside the resource core area of the Conservation zoned part of the property.

The lot contains semi-natural vegetation cover within the north section of the property, most of which is contained within the Conservation Overlay District. Some native vegetation is still existing on the disturbed south section of the site. Self-sown Big-leaf maple and Douglas fir are the primary upper canopy; sword fern and salal still grows on the south half of the property. The disturbed south section is heavily covered with English Ivy.

This report details tree preservation, tree density requirements, and tree protection requirements for Land Division and Planned Development review. A tree survey, site inspection and tree preservation scoping was completed on November 23rd, 2019. Table 1 and Figures 1 and 1a provide the details of this survey.

Tree Preservation within the two Rights of Way

Table 1 details the trees proposed to be removed, retained, and retained & protected to meet the City of Portland standards for tree preservation. Figure 1 gives the locations of the trees detailed in Table 1. Tables 2 and 3 summarize tree removal and preservation on the two rights-of-way. Tree density requirements are one tree for every 25 linear feet of frontage. In addition, tree replacement of up to 2 trees is required for every tree removed on rights-of-way. However, if the removals are for improvements to a partially improved right-of-way the replacement ratio can be reduced to 1:1.

Table 2. Summary of Tree Preservation Information Gibbs ROW

Existing Total Trees 6" DBH and over	31
Existing Total Trees under 6" DBH	3
Existing Total Trees required for density purposes (200-ft frontage / 25-ft)	8
Existing Total Trees that will be preserved and protected	18
Existing Total Trees that will be removed and require replacement (not dead, dying dangerous)	5

Table 3. Summary of Tree Preservation Information for SW 13th Ave ROW

Existing adjacent Trees 6" DBH and over	5
Existing Total Trees required for density purposes (150-ft / 25-ft)	6
Existing Total Trees that will be preserved and protected	3
Existing Total Trees that will be removed and require replacement (not dead, dying dangerous)	1

Tree Preservation within Private Property

The development proposal shows no trees within the private property that will require removal for development purposes. Within the existing building complex there is a windmill palm growing in a planter (T 30), but this plant is not regulated.

To the north of the proposed developed area of the property is a heavily treed closed canopy area characterized by Big-leaf maple and Douglas fir. Most of these trees are within the Conservation Overlay Zoning District. The nearest of these trees to the proposed development is between 9 and 15-ft. It appears that all of these trees may be saved if construction is executed in a controlled manner and high erosion standards are maintained.

Tree Preservation within Public Property

There are 5 trees surveyed that appear to be within public property to the west of the subject property. These trees were surveyed as they may be impacted by the proposed development. None are defined as requiring removal at this time; protection of these trees is discussed in the next section.

Title 33 Tree Preservation

No calculation of the number of trees or the total trunk diameter of the trees to be preserved within the Conservation Zoning Area, or north of the existing developed area, was completed. There are no healthy trees within these areas that are likely to be removed, therefore the Tree Preservation requirements of CC 33.630.100 will be met

Tree Protection Discussion

- a. **Private Property:** If the north side of the construction area needs subsurface reinforcement or additional fill for support, the standard root protection zone of some of the private trees may be affected. However, the nearest trees are between 9 and 15-ft from the existing raised building surface and appear to be far enough away not to be significantly affected. City approved Tree Protection Fencing will be installed as close as possible to the line of fill or excavation.
- b. **Right-of-way:** There are multiple self-sown trees on the steep bank between the Gibbs St. ROW and the proposed development. Trees #6 & #7 will be faced with additional construction impacts to the north and a significantly raised and extended roadway to the south. Trees #6 and #7 will be able to survive the impacts with appropriate tree protection standards in place. Trees #9 to #21 are small stems and will be significantly less affected by the roadway expansion and construction to the north than the trees further east and can all be preserved with appropriate standards for protection. Trees #27, #28, #29 will be adjacent to the demolition of existing paved surface and the construction of the new driveway, but the trees can be preserved with appropriate tree protection standards and the oversight of a project arborist. The existing driveway crossing this south bank area will be removed and appropriate standards for the removal will be needed to allow the adjacent trees (#22 and #24) to remain in place. City approved Tree Protection Fencing will be installed as close as possible to the line of fill or excavation.
- c. **Public Land:** The surface elevation drops significantly to the west onto public land. There are few trees that will be significantly affected by direct construction and fill impacts. The main threat to trees here is from erosion of the surface due to construction impacts. Two trees will be significantly impacted by demolition of existing structures: Tree 33 is a high value tree within 2-ft of a small building that will need to be removed; demolition of the building should be completed with appropriate tree protection standards in place. A qualified arborist will need to be present to oversee tree protection adjacent to surface disturbance. Tree 32 is within 8-ft of a concrete pad that will be removed and will also need appropriate tree protection standards in place. Both of these trees are also likely to be affected by grading for the new driveway access. City approved Tree Protection Fencing will be installed as close as possible to the line of fill or excavation to protect these trees and a qualified on-site arborist will need to be present to oversee disturbance activity.
- d. Due to the steep slopes on this site it is requested that the specified Tree Protection Fencing is by heavy duty T-post and orange safety fencing (as specified by drawings on Tree Protection Plan sheet). Figure 2 details the proposed Tree Protection Fence locations.
- e. Removal of trees will occur adjacent to preserved trees and within areas fenced for tree protection. It is important that a qualified arborist will be on-site at all times to supervise this work and ensure that the preserved trees are not damaged.

Trees requiring Performance Protection

Trees # 6 and #7 and # 20

These trees will have a small portion of their critical root zones covered with fill but this is insufficient to cause significant harm. No fill should be placed near the base of these trees. Tree Protection Fencing with erosion control protection incorporated or located in parallel must be used. Removal of the retired asphalt driveway to the north should be completed using low impact machinery, the existing surface shows a significant density of roots just below the asphalt surface. Adherence to the Tree Protection Standards will allow these trees to remain viable.

Trees #22 & #24

These trees are sufficiently far from the tax lot boundary to be unaffected by disturbance for construction of the new building; Tree protection fencing should be placed at the boundary line. Asphalt removal will be within a few feet of the base of these trees and a qualified arborist must be present to ensure that the removal is completed carefully and root damage is minimized. Adherence to the Tree Protection Standards will allow these trees to remain viable (See Tree Protection Standard 2 and 7).

Tree #27 and #29

These trees are in close proximity to grade changes required by the location of the new entrance driveway. There will be fill placed in a considerable portion of their expected root zone to the north and east. These trees are semi-mature and are in sufficiently good condition to survive some root loss. It will be important to have robust erosion control devices placed at the line of the protection fencing to prevent ingress of spoil. Adherence to the Tree Protection Standards will allow these trees to remain viable (See Tree Protection Standards 2 and 7).

Tree #33

This tree is a semi-mature fir in good condition and should be resilient to significant disturbance. There is a small building within 2-ft of the base of the tree that straddles the lot line of the subject property. The building demolition and removal of foundations should be completed under the supervision of a qualified arborist. Adherence to the Tree Protection Standards will allow this tree to remain viable (See Tree Protection Standard 7).

Tree Protection Plan Standards

The following **Tree Protection Standards** will condition all project construction, excavation and tree removal at the site. The Tree Protection Standards will be noted on the site construction plans:

1. All Tree Protection Fencing as specified on Tree Plan sheet must be placed before any construction, excavation or storage of materials or equipment takes place at the site.
2. All Tree Protection Fencing will incorporate erosion control devices sufficient to prevent ingress of spoil or liquid runoff into the protected areas.
3. No construction activity, including vehicle access, or any storage of spoil, materials or equipment will occur within the area bounded by the Tree Protection Fence.
4. The fence locations as detailed in the construction plans will not be altered or breached at any time without the explicit approval of City of Portland Urban Forestry Inspectors.
5. Any damage to the Tree Protection Fencing must be remedied within 24 hrs.
6. Removal of any roots from trees to be preserved on the public right-of-way must only occur under permit approval from City of Portland, Urban Forestry.
7. Any excavation work, removal of surface asphalt or placement of fill placement within 10-ft of any preserved tree, as detailed in the Tree Protection Plan, must be done in the presence of a qualified arborist (a currently qualified ISA Certified Arborist).
8. Any roots evident during excavations or severed or badly damaged roots of any preserved tree must be cut cleanly using hand-held tools (e.g. hand saw, reciprocating saw, circular saw, angle grinder or other as approved by an ISA Certified Arborist.
9. Any approved removal of any private or right-of-way trees must be supervised on-site by an ISA Certified Arborist.
10. Any trees on this project required to be removed will be cut to ground level and the stump either left in place or stump ground by machinery designed for that purpose. No roots are to be ripped out of the ground.

Figure 1. Tree Survey of 11-23-19. Location of trees only. (RPZ's are not necessarily valid; Some tree locations are not instrument surveyed)

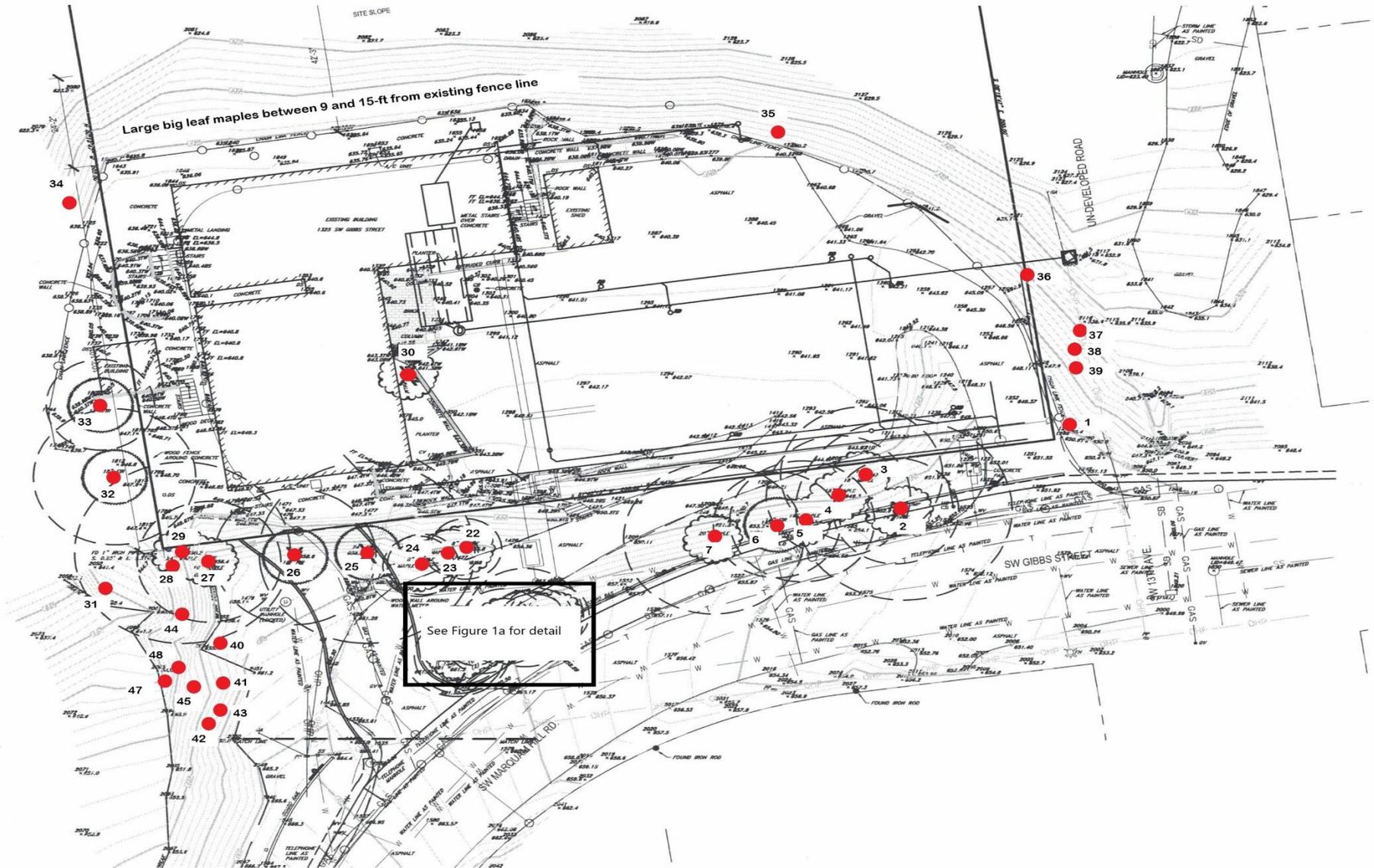


Figure 1a. Tree Survey of 11-23-19. Location of trees only. (RPZ's are not necessarily valid)

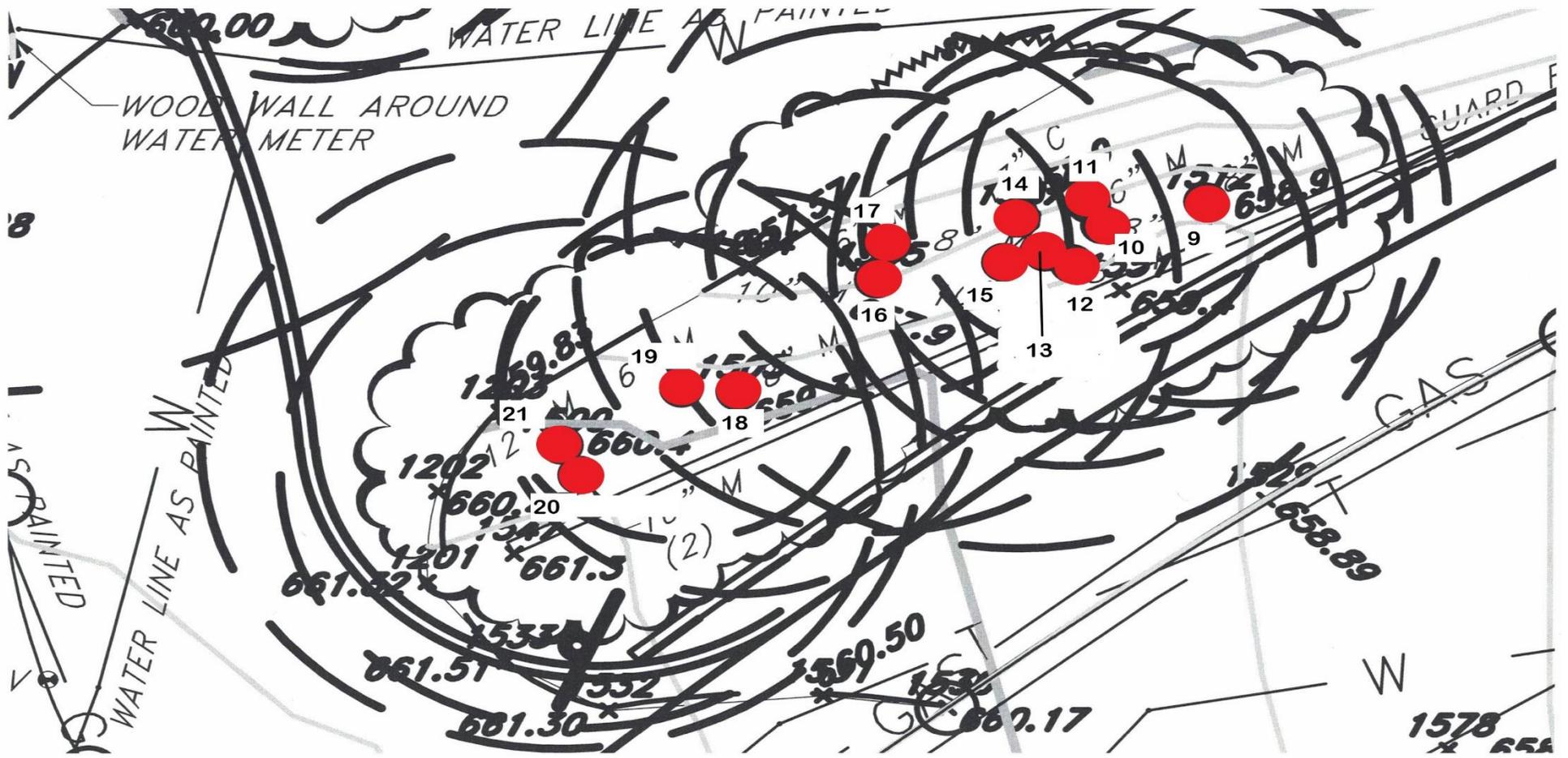


Figure 2. Tree Protection Fence Locations

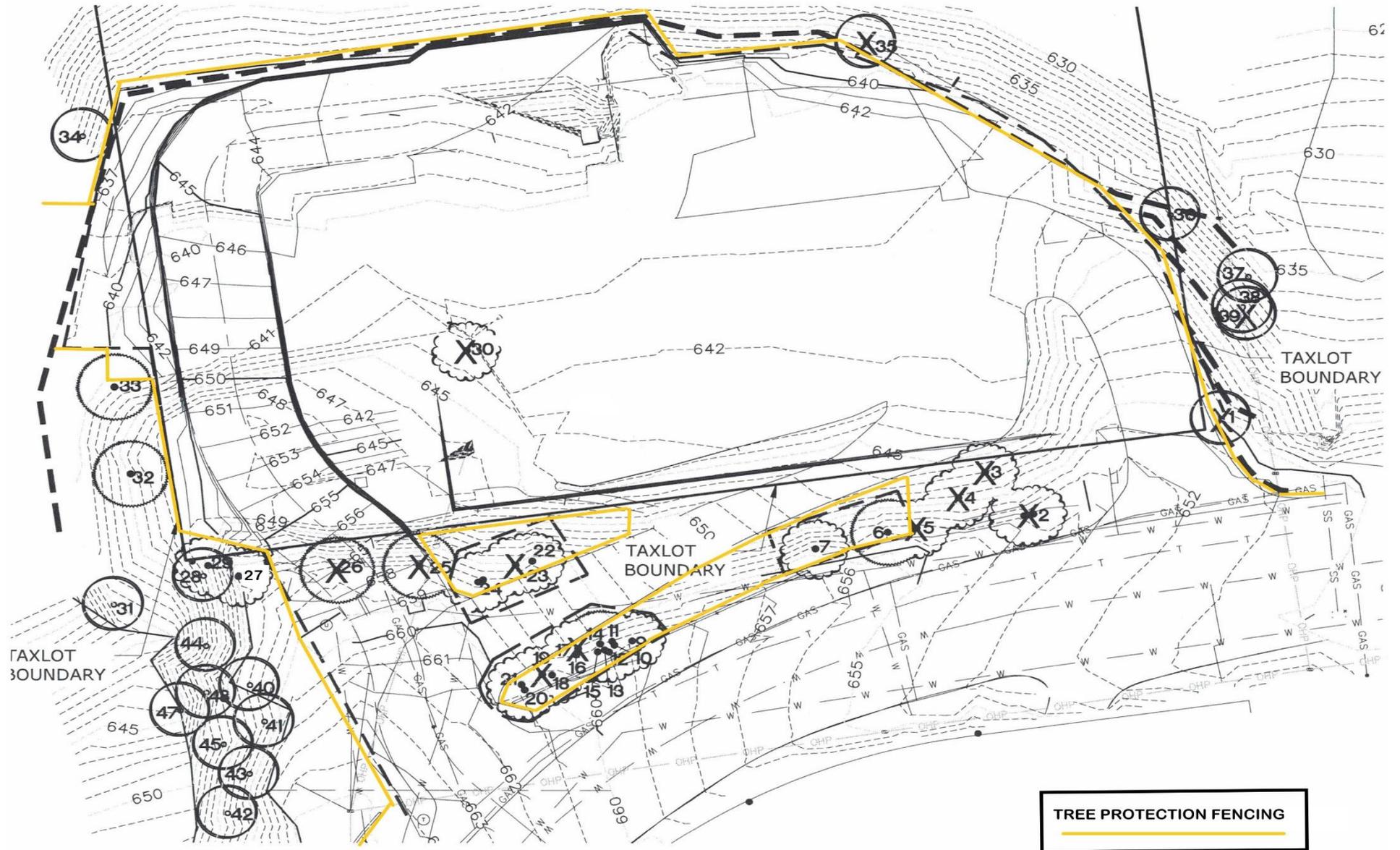


Table 1.

Tree Survey of November 23rd, 2019

#	Tree Species	DBH	RPZ	Condition	Loc	Tree & Location Notes	Actions
1	Big leaf maple <i>Acer macrophyllum</i>	25	25	Fair	ROW	Spreading form. Three stems from 2-ft. Ivy to mid CR. Half CRZ paved. Tree 1-ft from end of CL fence and 1-ft from asphalt driveway.	Remove. Cut within RPZ sig. and bldg. 5-ft from tree
2	Black Walnut <i>Juglans nigra</i>	32	32	Fair	ROW	Three stems (18/15/13) joined at ground. Spreading CR. Some decline upper CR. Minor vehicle damage. 5-ft from asphalt	Remove. New road paving incursion
3	Big leaf maple <i>Acer macrophyllum</i>	18	18	Fair	ROW	Some CR dieback due to shading. Roots visible under adj driveway. Asphalt driveway 5-ft distance	Remove. Adj to bldg. excavation zone
4	Big leaf maple <i>Acer macrophyllum</i>	13	13	Dangerous	ROW	Bending, twisting stem over ROW. Sig lesions and stem damage at 15-ft. Half-way down bank	Remove. Hazard to ROW
5	Big leaf maple <i>Acer macrophyllum</i>	15	15	Fair/Poor	ROW	CR shouldered by other trees. Stem lesions and wetwood on stem. 8.5 ft from asphalt; 2-ft below TOB	Remove Paving and fill in root zone
6	Douglas fir <i>Pseudotsuga menzeisii</i>	14	14	Good	ROW	Semi-mature tree, upright with good vigor. 9-ft from asphalt; 2-ft below TOB	Preserve. Performance
7	Big leaf maple <i>Acer macrophyllum</i>	26	26	Fair	ROW	Low LCR, leaning over travel lanes. Minor CR dieback. 10.5-ft from asphalt; 2-ft below TOB	Preserve. Performance
9	Big leaf maple <i>Acer macrophyllum</i>	9	9	Fair	ROW	Dieback mid-crown. 10.5 from asphalt. 1-ft from CB	Preserve, Prescriptive
10	Big leaf maple <i>Acer macrophyllum</i>	5	5	Fair	ROW	Closely spaced SS stems. Low to mid CR shaded by adjacent cedar	Preserve, Prescriptive
11	Big leaf maple <i>Acer macrophyllum</i>	5	5	Fair	ROW	Closely spaced SS stems. Low to mid CR shaded by adjacent cedar	Preserve, Prescriptive
12	Big leaf maple <i>Acer macrophyllum</i>	7	7	Fair	ROW	Closely spaced SS stems. Low to mid CR shaded by adjacent cedar	Preserve, Prescriptive
13	Big leaf maple <i>Acer macrophyllum</i>	7	7	Fair	ROW	Closely spaced SS stems. Low to mid CR shaded by adjacent cedar	Preserve, Prescriptive
14	Western red cedar <i>Thuja plicata</i>	9	9	Fair/Good	ROW	Semi-mature tree. Strong developing tree, with high LCR 3.5-ft from CB	Preserve, Prescriptive
15	Big leaf maple <i>Acer macrophyllum</i>	8	8	Poor	ROW	Noted stem disease and stem lesions. 2-ft from CB	Preserve, Prescriptive
16	Big leaf maple <i>Acer macrophyllum</i>	8	8	Fair	ROW	Narrow forest-form CR. 3.5-ft from CB	Preserve, Prescriptive
17	Big leaf maple <i>Acer macrophyllum</i>	5	5	Dying	ROW	Strongly suppressed. 5-ft from CB	Remove. Remove due to condition

#	Tree Species	DBH	RPZ	Condition	ROW	Tree & Location Notes	Actions
18	Big leaf maple <i>Acer macrophyllum</i>	9	9	Fair/Poor	ROW	Stem cracks. 1.5-ft from CB	Preserve, Prescriptive
19	Big leaf maple <i>Acer macrophyllum</i>	6	6	Dead	ROW	Suppressed. 2.5-ft from CB	Remove. Remove due to condition
20	Scouler willow <i>Salix scouleriana</i>	21	21	Fair	ROW	Twin stems (16/9) from ground level. Lesions on stem. Dieback from enclosed stem. At CB	Preserve. Performance
21	Big leaf maple <i>Acer macrophyllum</i>	10	10	Fair	ROW	Very narrow CR form. 2.5-ft from CB	Preserve, Prescriptive
22	Big leaf maple <i>Acer macrophyllum</i>	11	11	Fair	ROW	Strong CR development non-shaded half. Strong branching. 3-ft from asphalt drive	Preserve. Performance
23	Big leaf maple <i>Acer macrophyllum</i>	7	7	Dead/ Dying	ROW	Suppressed	Remove. Remove due to condition
24	Big leaf maple <i>Acer macrophyllum</i>	14	14	Fair	ROW	Twin stems (10/7). Tree partially suppressed. 3-ft from asphalt drive.	Preserve. Performance
25	Douglas fir <i>Pseudotsuga menzeisii</i>	30	30	Good	ROW	Strong growing, full, upright CR. High LCR. No defects evident. On 45-deg bank	Remove. Within limit of development
26	Douglas fir <i>Pseudotsuga menzeisii</i>	25	25	Good	ROW	Strong growing, full, upright CR. High LCR. No defects evident. On 45-deg bank	Remove. Within limit of development
27	Big leaf maple <i>Acer macrophyllum</i>	16	16	Fair	ROW	30-deg slope to W	Preserve. Performance
28	Big leaf maple <i>Acer macrophyllum</i>	13	13	Fair	ROW	Narrow stem, partially suppressed codominant	Preserve, Prescriptive
29	Big leaf maple <i>Acer macrophyllum</i>	21	21	Fair/Good	ROW	Full strong upright CR; On steep slope to W	Preserve. Performance
30	Chinese windmill Palm <i>Trachycarpus fortunei</i>	8	8	Fair/Good	PR	12-ft height. In planter.	Remove. In planter. Unregulated tree
31	Big leaf maple <i>Acer macrophyllum</i>	20	20	Fair/Poor	Pub	Weak declining CR	Preserve, Prescriptive
32	Big leaf maple <i>Acer macrophyllum</i>	23	23	Fair	Pub	Basal lesions. Minor decay. 8-ft from concrete deck	Preserve, Prescriptive
33	Douglas fir <i>Pseudotsuga menzeisii</i>	14	14	Good	Pub	Semi-mature tree growing at canopy edge of large firs. 1.5-ft from base of bldg. to N	Preserve. Performance
34	Big leaf maple <i>Acer macrophyllum</i>	22	22	Fair	Pub	Spreading CR over CL fence. Will need Crown raised over fence. 5-ft from CL fence	Preserve, Prescriptive
35	Big leaf maple <i>Acer macrophyllum</i>	14	14	Dead	PR	55-ft tree, needs removed. 6-ft from existing fence	Remove. Remove due to condition

#	Tree Species	DBH	RPZ	Condition	Loc	Tree & Location Notes	Actions
36	Big leaf maple <i>Acer macrophyllum</i>	15	15	Fair/Poor	ROW C	Ivy covered high but base appears sound. 10-ft from CL fence. Location not accurately surveyed	Preserve, Prescriptive
37	Big leaf maple <i>Acer macrophyllum</i>	13	13	Fair	ROW C	Approx 13-ft from CL fence. Location not accurately surveyed.	Preserve, Prescriptive
38	Big leaf maple <i>Acer macrophyllum</i>	7	7	Fair	ROW	Semi-mature tree. 12-ft from CL fence. Location not accurately surveyed	Preserve, Prescriptive
39	Weeping willow <i>Salix chrysocoma</i>	20	20	Dying	ROW	Fractured and decaying stem, caught up in other trees. 12-ft from CL fence	Remove. Remove due to condition
40	Big leaf maple <i>Acer macrophyllum</i>	25	25	Fair	ROW	Twin-stem from 1-ft. Sig wound from limb loss. Location not accurately surveyed	Preserve, Prescriptive
41	Big leaf maple <i>Acer macrophyllum</i>	15	15	Fair/Good	ROW	Strong upright growth. Location not accurately surveyed	Preserve, Prescriptive
42	Big leaf maple <i>Acer macrophyllum</i>	14	14	Fair	ROW	Slender upright CR. Location not accurately surveyed	Preserve, Prescriptive
43	Big leaf maple <i>Acer macrophyllum</i>	9	9	Fair/Poor	ROW	Some damage in upper CR. Location not accurately surveyed.	Preserve, Prescriptive
44	Big leaf maple <i>Acer macrophyllum</i>	22	22	Fair/Poor	ROW	Damaged stem. Location not accurately surveyed	Preserve, Prescriptive
45	Big leaf maple <i>Acer macrophyllum</i>	10	10	Fair	ROW	Unexamined, ivy covered. Location not accurately surveyed	Preserve, Prescriptive
47	Big leaf maple <i>Acer macrophyllum</i>	18	18	Fair	Pub	On edge of steep bank. Location not accurately surveyed	Preserve, Prescriptive
48	Big leaf maple <i>Acer macrophyllum</i>	6	6	Fair/Poor	Pub	Weak and thin CR. Location not accurately surveyed	Preserve, Prescriptive

Table Notes:

Definitions – CB: Crash Barrier; CL: Chain Link; CRZ: Critical Root Zone; DBH: (Diameter Breast Height in inches); RPZ: (Regulatory root protection zone radius); Loc: Location (ROW right-of-way, PR Private property, Pub: Publicly owned, C Conservation Zone); TOB Top of Bank

This appendix includes photographs that show the characteristics of the surrounding neighborhood as required by Chapter 33.854.250, Supplemental Application Requirements. Generally, the neighborhood is a mix of single-family residential homes and multi-dwelling residential structures. The table below is a descriptive key to the photographs included on the following pages of this appendix.

Page	Location	Direction	Description
F-1	998 SW Gibbs St.	Southeast	A two-story apartment with vertical wood siding building built below street grade.
F-2	1000 SW Gibbs St.	East	Neighborhood Amenity - Plaid Panty
F-3	1100 SW Gibbs St.	Southeast	Neighborhood Amenity - Bus stop at 11th & Gibbs St. / multi-dwelling structure and single-family residential development.
F-4	1100 SW Gibbs St.	North	Two multi-dwelling residential structures
F-5	1106 SW Gibbs St.	East	Multi-dwelling residential structures. End of sidewalks on Gibbs St.
F-6	1106 SW Gibbs St.	West	SW Gibbs St. heading towards project site.
F-7	1136 SW Gibbs St.	East	A multi-dwelling structure across from a single-family residence.
F-8	1215 SW Gaines St.	West	A three-story multi-dwelling structure on a slope adjacent to a single-family dwelling
F-9	1239 SW Woods St.	North	A multi-dwelling structure built on a steep hillside, below street grade.
F-10	1298 SW Gibbs St.	Southwest	Single-family homes along 13 th Avenue, across from the project site
F-11	1299 SW Grover St.	Northwest	A multi-dwelling structures located on steep slopes behind project site.
F-12	2953 SW 11 th Ave.	Northwest	A multi-dwelling structures located on steep slopes.
F-13	3026 SW 10 th Ave.	North	A multi-dwelling structures developed on steep slopes above and below street grades.
F-14	3130 SW 12 th Ave.	Southwest	A multi-dwelling structure across from a single-family home.
F-15	3201 SW 12 th Ave.	Northwest	A multi-dwelling structure near the subject site.
F-16	3203 SW 12 th Ave.	South	A single-family dwelling across from a recently permitted multi-dwelling apartment complex.
F-17	3211 SW 10 th Ave.	Southwest	Marquam Manor apartment building, built in 1930.
F-18	3223 SW 11 th Ave.	Southeast	A multi-dwelling across from the bus stop at 11 th & Gibbs.
F-19	3301 SW 10 th Ave.	Northwest	Multi-dwelling structures on Gibbs St. down the hill from the project site.
F-20	3326 SW 13 th Ave.	North	Single-family homes on SW 13 th Ave south of the project site.
F-21	3377 SW Marquam Hill Road	Northeast	A PWB water storage tank across from single-family homes, near the project site.
F-22	3409 SW 12 th Ave.	North	Single and multi-dwelling structures near the project site.



998 SW Gibbs St

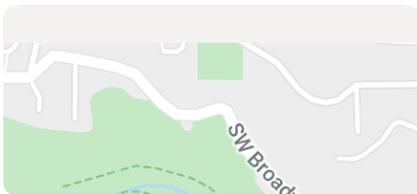


Image capture: Sep 2017 © 2019 Google

Portland, Oregon



Street View - Sep 2017



Google Maps 1000 SW Gibbs St

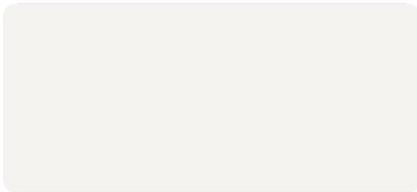


Image capture: Sep 2017 © 2019 Google

Portland, Oregon



Street View - Sep 2017





1100 SW Gibbs St

Bus Stop

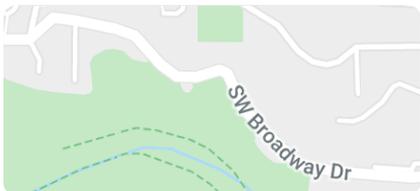


Image capture: Aug 2017 © 2019 Google

Portland, Oregon



Street View - Aug 2017



Google Maps 1100 SW Gibbs St

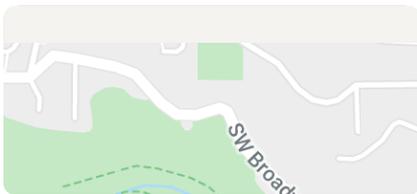


Image capture: Aug 2017 © 2019 Google

Portland, Oregon



Street View - Aug 2017



Google Maps 1106 SW Gibbs St

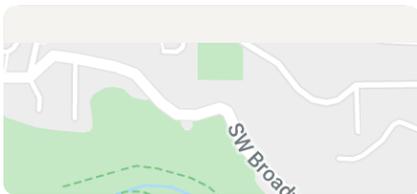


Image capture: Aug 2017 © 2019 Google

Portland, Oregon



Street View - Aug 2017



Google Maps 1106 SW Gibbs St

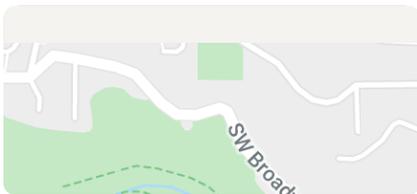


Image capture: Aug 2017 © 2019 Google

Portland, Oregon



Street View - Aug 2017



Google Maps 1136 SW Gibbs St

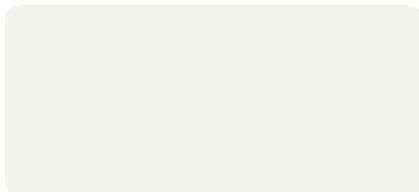


Image capture: Aug 2017 © 2019 Google

Portland, Oregon



Street View - Aug 2017



Google Maps 1215 SW Gaines St



Image capture: Mar 2016 © 2019 Google

Portland, Oregon



Street View - Mar 2016





1239 SW Woods St

Multi-dwelling residential development / steep slopes

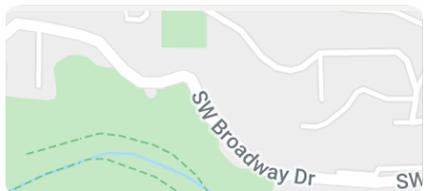


Image capture: Mar 2016 © 2019 Google

Portland, Oregon



Street View - Mar 2016





1298 SW Gibbs St



Image capture: Aug 2017 © 2019 Google

Portland, Oregon



Street View - Aug 2017





1299 SW Grover St

Multi-dwelling residential development



Image capture: Mar 2016 © 2019 Google

Portland, Oregon



Street View - Mar 2016



Google Maps 2953 SW 11th Ave

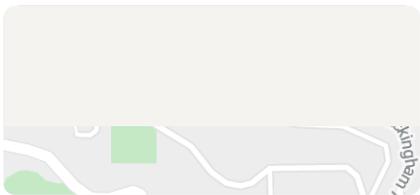


Image capture: Mar 2016 © 2019 Google

Portland, Oregon



Street View - Mar 2016



Google Maps 3026 SW 10th Ave



Image capture: Aug 2017 © 2019 Google

Portland, Oregon



Street View - Aug 2017





3130 SW 12th Ave

Mixed multi-dwelling and single-family residential development



Image capture: Mar 2016 © 2019 Google

Portland, Oregon



Street View - Mar 2016





3201 SW 12th Ave
Hill Terrace Apartments



Image capture: Mar 2016 © 2019 Google

Portland, Oregon



Street View - Mar 2016



Google Maps 3203 SW 12th Ave



Image capture: Mar 2016 © 2019 Google

Portland, Oregon



Street View - Mar 2016



Google Maps 3211 SW 10th Ave

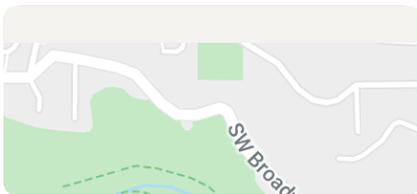


Image capture: Jun 2016 © 2019 Google

Portland, Oregon



Street View - Jun 2016



Google Maps 3223 SW 11th Ave



Image capture: Jul 2016 © 2019 Google

Portland, Oregon



Street View - Jul 2016



Google Maps 3301 SW 10th Ave

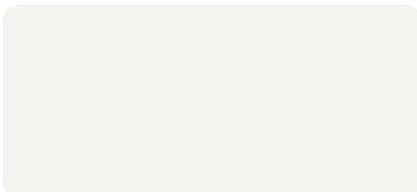


Image capture: Mar 2016 © 2019 Google

Portland, Oregon



Street View - Mar 2016





3326 SW 13th Ave

Looking North towards project site

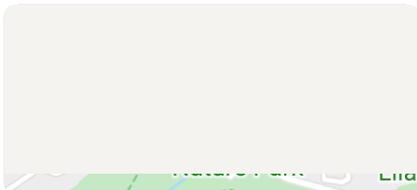


Image capture: Aug 2017 © 2019 Google

Portland, Oregon



Street View - Aug 2017





3377 SW Marquam Hill Rd

PWB storage tank on left. Project site straight ahead.

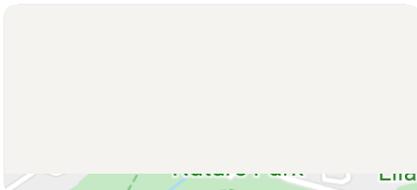


Image capture: Aug 2017 © 2019 Google

Portland, Oregon



Street View - Aug 2017



Google Maps 3409 SW 12th Ave



Image capture: Mar 2016 © 2019 Google

Portland, Oregon



Street View - Mar 2016





Portland Housing Bureau

Mayor Ted Wheeler • Director Shannon Callahan

421 SW 6th Avenue, Suite 500
Portland, OR 97204
503-823-2375 PHONE
503-823-2387 FAX
portlandoregon.gov/phb

INCLUSIONARY HOUSING (IH) SELECTION FORM – INTAKE

APPLICANT/OWNER	CONTACT INFORMATION
Contact Name: Jesse Winterowd	Work: 503-827-4422
Ownership Entity: Winterbrook Planning <input checked="" type="checkbox"/> For Profit <input type="checkbox"/> Non-Profit <input type="checkbox"/> Public Entity <input type="checkbox"/> Other:	Mobile:
Mailing Address:	E-mail: jesse@winterbrookplanning.com

PROJECT INFORMATION			
Project Name:	1311 SW Gibbs	Permit #:	
Address:	1311 SW Gibbs Street	Property Tax Account #s:	R712893
Description of Work:	Multi-dwelling development		
Type of Permit:	<input checked="" type="checkbox"/> New Construction	<input type="checkbox"/> Adding Additional Units to Existing Structure	
Type of Building:	<input checked="" type="checkbox"/> Residential Only	<input type="checkbox"/> Mixed-Use	
Intent of Units:	<input checked="" type="checkbox"/> Rental (Apartments)	<input type="checkbox"/> For-Sale (Condos)	
Construction Timeline:	Estimated Start Date:	Estimated Final Permit Date:	
Property Management Contact or Contact During Construction: Sean O'Neill, Scotia Western States Housing, LLC.	Contact Email: sean@scotiaawsh.com Contact Phone: 503-956-9307		

INCLUSIONARY HOUSING (IH) REGULATORY OPTIONS

IH requires that all residential buildings proposing 20 or more new units provide a percentage of the new units at restricted rent or sale prices affordable to households at 80% of median family income (MFI) or below. The City has defined additional regulatory options under the umbrella of this requirement, and all subject permit applications must include one of these options.

REGULATORY OPTIONS: Identify the Inclusionary Housing regulation option selected for this project.				
Option 1: 80% MFI Units* <input type="checkbox"/>	Option 2: 60% MFI Units* <input checked="" type="checkbox"/>	Option 3: Build Off-Site Units <input type="checkbox"/>	Option 4: Designate Existing Off-Site Units <input type="checkbox"/>	Option 6: Fee-in-Lieu <input type="checkbox"/>
*If selecting Opt. 1 or 2, utilizing On-Site Consolidation by transferring IH Units into another building on-site?				<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
*If choosing Option 1 or 2, is this property providing more the minimum number of required IH Units?				<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
If so, please list the total number of affordable units provided & what MFI level(s) they will be offered at:				@ MFI,
RECONFIGURATION (Option 5)				
If you are including affordable units on-site (Options 1 & 2), will you be reconfiguring the required affordable units to units with two or more bedrooms?				<input checked="" type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> NA



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If so, what is your proposed Reconfiguration make-up? 5 total bedrooms in 1 two bedroom units, and 3 one-bedroom and studio apartments

ADDITIONAL DOCUMENTATION REQUIRED

All Intakes must submit Inclusionary Housing Unit & Square Footage Calculation Sheet with this form.

Based on the Regulatory Option selected above, please complete and submit the following additional documentation:

<u>Option 1:</u> 80% AMI Units	<u>Option 2:</u> 60% AMI Units	<u>Option 3: Build Off-site Units & Option 4:</u> Designate Existing Off-site Units
1. Inclusionary Housing Unit & Square Footage Calculation Sheet 2. Attachment (02)- IH MULTE Application 3. Attachment (03)- Affordable Housing CET Exemption Application	1. Inclusionary Housing Unit & Square Footage Calculation Sheet 2. Attachment (02)- IH MULTE Application 3. Attachment (03)- Affordable Housing CET Exemption Application 4. Attachment (04)- SDC Exemption Application	1. Inclusionary Housing Unit & Square Footage Calculation Sheet 2. Attachment (05)- IH Receiving Site Identification Intake 3. Receiving Site's development schedule
If utilizing On-Site Consolidation Attachment (06)- IH Consolidated Site Identification Form must be attached.		

SIGNATURE OF RESPONSIBLE PARTY: (property owner or owner's agent)

I am the property owner, or an authorized agent thereof, and certify that the information provided is accurate. I understand that the Inclusionary Housing option selected affects incentives that are available to the project.

Signature:  <small>(A wet signature is required)</small> Name: Jesse Winterowd	<input type="checkbox"/> Property Owner <input checked="" type="checkbox"/> Other Applicant/Representative	Date:
Address: 610 SW Alder Street City, State & Zip: Portland, Oregon 97205	Phone: 503-827-4422	Email: jesse@winterbrookplanning.com

This Intake Form is required to be submitted with the permit application.

Email respective attachments to Inclusionary-Housing@portlandoregon.gov

Inclusionary Housing Unit Schedule

# of Bedrooms/ Unit Type	Sub-Type	Quantity	Unit Area	Unit Total Area	Average Area per Unit Type (Total)	Minimum Area of IH Units Required	# of I.H. Units Required %8	# of I.H. Units Required %10	# of I.H. Units Required %15	# of I.H. Units Required 20%	# of I.H. Units Required 25%
0	A			0							
	B			0							
	C			0							
	D			0							
	E			0							
	F			0							
	G			0							
	H			0							
	I			0							
	J			0							
	K			0							
	L			0							
	M			0							
	N			0							
	O			0							
TOTAL		0		0	#DIV/0!	#DIV/0!	0.00	0.00	0.00	0.00	0.00
L1B	A			0							
	B			0							
	C			0							
	D			0							
	E			0							
	F			0							
	G			0							
	H			0							
	I			0							
	J			0							
	K			0							
	L			0							
	M			0							
	N			0							
	O			0							
TOTAL		0		0	#DIV/0!	#DIV/0!	0.00	0.00	0.00	0.00	0.00
W1B	A			0							
	B			0							
	C			0							
	D			0							
	E			0							
	F			0							
	G			0							
	H			0							
	I			0							
	J			0							
	K			0							
	L			0							
	M			0							
	N			0							
	O			0							
TOTAL		0		0	#DIV/0!	#DIV/0!	0.00	0.00	0.00	0.00	0.00
W2B	A			0							
	B			0							
	C			0							
	D			0							
	E			0							
	F			0							
	G			0							
	H			0							
	I			0							
	J			0							
	K			0							
	L			0							
	M			0							
	N			0							
	O			0							
TOTAL		0		0	#DIV/0!	#DIV/0!	0.00	0.00	0.00	0.00	0.00
Unit Grand Total:		0		0	#DIV/0!	#DIV/0!	0.00	0.00	0.00	0.00	0.00

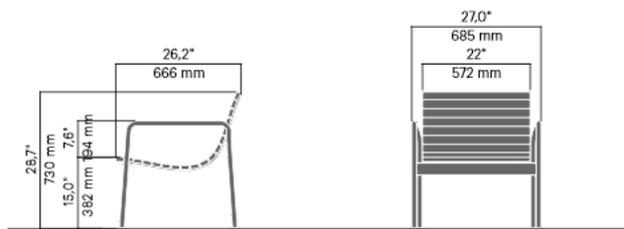
***Note: All IH units have to be comparable to market rate units.

KINN chair

6164

Designed by
Espen Voll, Torbjørn Anderssen

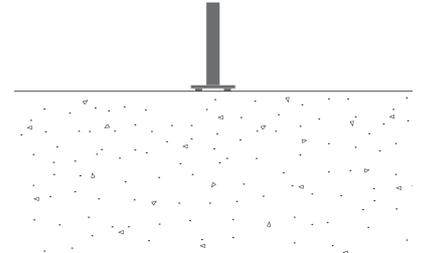
Kinn chair The Kinn lounge chair is made of Hydro CIRCAL which consists of at least 75% recycled aluminum. This is the highest content of recycled material on the market, which means drastically reduced energy consumption, while the product is also of high quality and durable. Kinn is comfortable, stackable, and easy to move around as needed. It is part of a larger furniture collection that will be launched in 2022.



29 kg / 628

Anchoring/assembly

Free-standing



Primary material

Hot-dip galvanised steel

All steel components are hot-dip galvanised, which produces a matt grey surface after a while. We offer a lifetime anti-rust warranty.



Sustainability

Indicators	Global warming	Total energy used	Recycled materials
Unit	kg CO2	MJ	%
Cradle to Gate A1-A3	0	0	0

Certifications

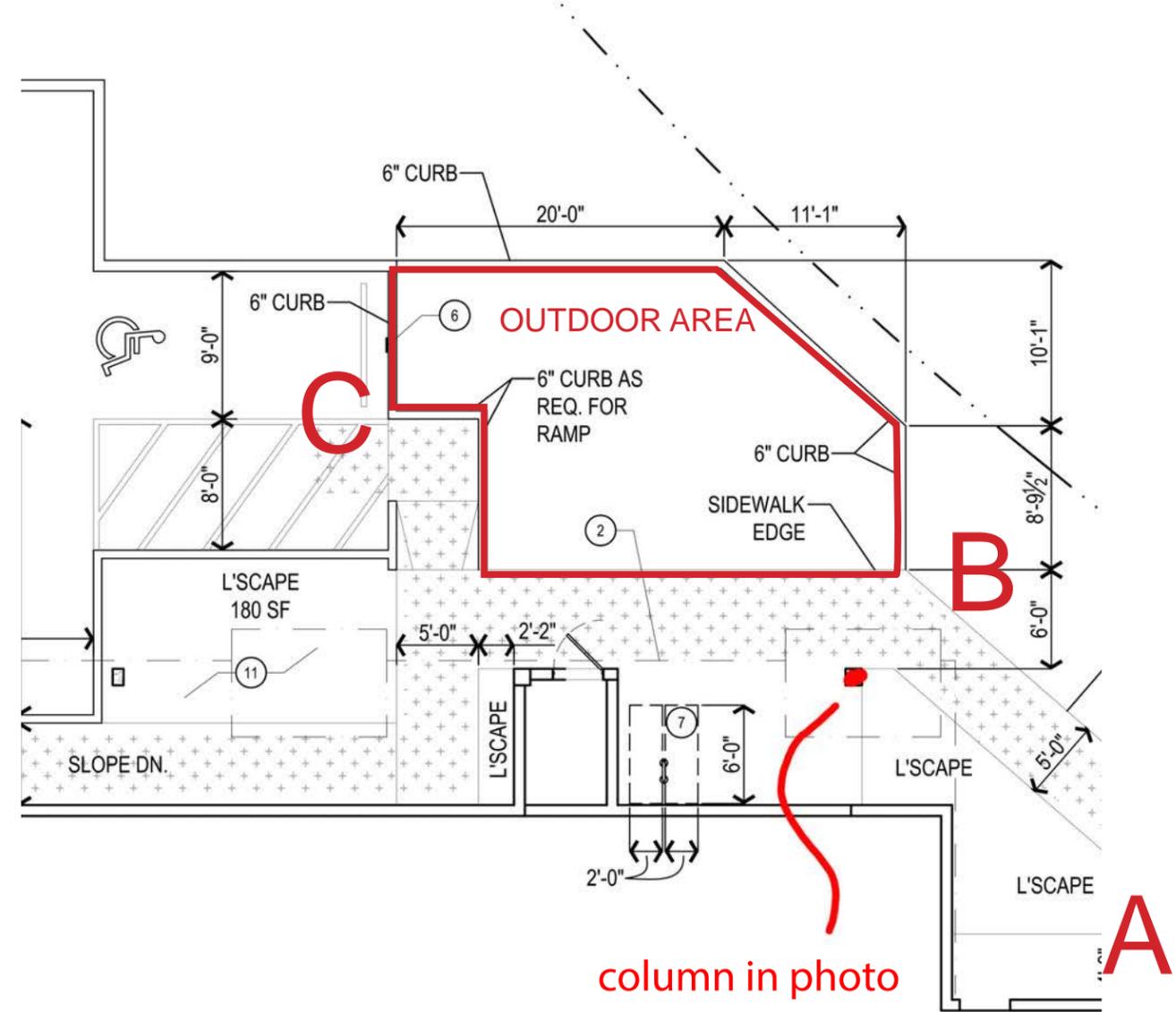


Warranty

- Lifetime warranty against rust
- 15 year warranty on powder coating
- 15 year warranty on wood
- Spare parts always available

LU 22-187796 APD, AP

A



B



C



BEDDING AND BACKFILL REQUIREMENTS FOR SEWER MAIN AND LATERALS:

BEDDING:
FLEXIBLE PIPE BEDDING PER CITY OF PORTLAND STD SPEC. 00405.12. BEDDING SHALL BE INSTALLED PER 00405.45.

BACKFILL:
PER CITY OF PORTLAND STD SPEC. 00405.14. IMPORTED GRANULAR BACKFILL SHALL BE INSTALLED PER 00405.46.

SEWER LATERAL NOTE:
CONTRACTOR SHALL INSTALL ALL SEWER LATERALS TO PROPERTY LINES, INSTALL CAP AND MARK END OF LATERAL PER NOTE 12 ON COVER SHEET.

CONTRACTOR NOTE:
CONTRACTOR SHALL MAINTAIN FLOWS IN EXISTING SEWER SYSTEMS AT ALL TIMES, AND IS RESPONSIBLE FOR SUBMITTING A PLAN DETAILING METHODS OF CONSTRUCTION NECESSARY TO MAINTAIN FLOW TO BES CONSTRUCTION 14 DAYS PRIOR TO STARTING WORK.

FIELD VERIFICATION / POT HOLE NOTE:
LOCATION OF EXISTING STRUCTURES AND UTILITIES (INCLUDING DEPTHS) ARE FOR INFORMATION PURPOSE ONLY. APPLICANT/CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH (ELEVATION) OF EXISTING UTILITIES AND OTHER FIELD CONDITIONS PRIOR TO CONSTRUCTION.

CONTRACTOR SHALL POT HOLE ALL EXISTING UTILITY CROSSINGS PRIOR TO EXCAVATION OF PROPOSED COMBINED SEWER MAIN EXTENSION

CONSTRUCTION NOTES:

- PRIOR TO INSTALLATION OF THE PUBLIC SEWER MAIN AND LATERALS THE CONTRACTOR SHALL POT HOLE UTILITY CROSSING. CONTRACTOR SHALL NOTIFY THE ENGINEER IF PLAN MODIFICATION ARE REQUIRED.
- CONTRACTOR SHALL MAINTAIN FLOWS IN EXISTING SEWER SYSTEMS AT ALL TIMES, AND IS RESPONSIBLE FOR SUBMITTING A PLAN DETAILING METHODS OF CONSTRUCTION NECESSARY TO MAINTAIN FLOW TO BES CONSTRUCTION 14 DAYS PRIOR TO STARTING WORK.
- A ENGINEERED UTILITY PROTECTION PLAN IS REQUIRED TO BE SUBMITTED TO ROY MARTINEZ FOR REVIEW AND APPROVAL PRIOR TO PWP PERMIT ISSUANCE. ROY'S PHONE NUMBER IS (503)-823-4518 AND EMAIL ROY.MARTINEZ@PORTLANDOREGON.GOV

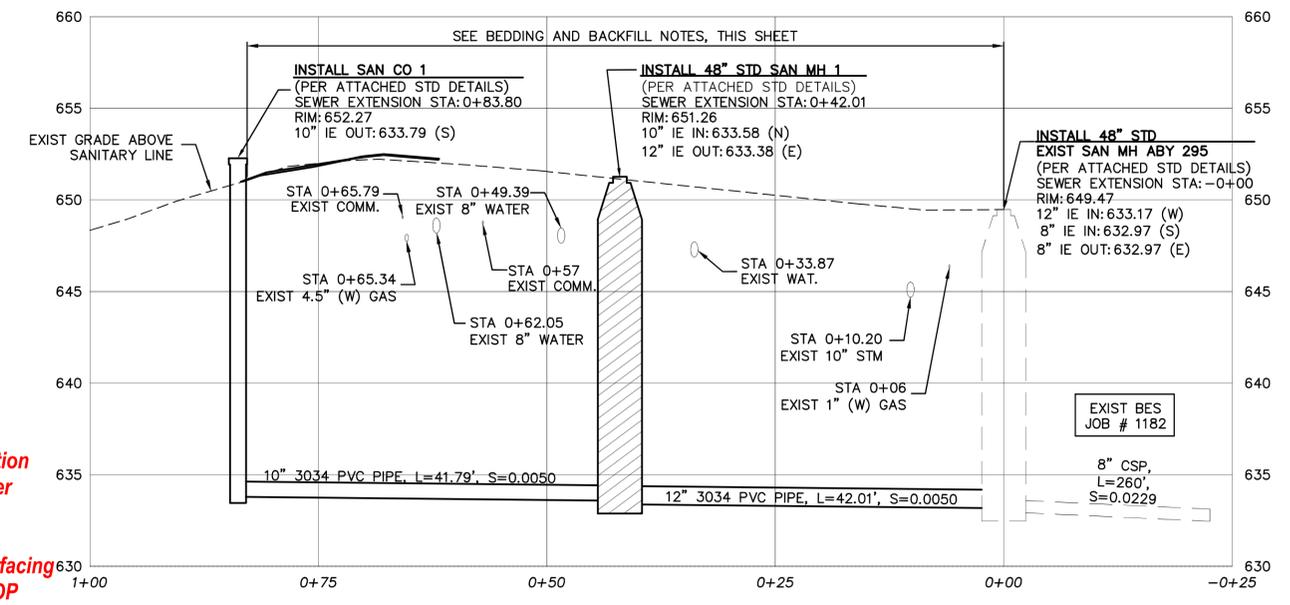
FOR MORE INFORMATION ABOUT UTILITY PROTECTION PLAN, PLEASE VISIT:
[HTTP://WWW.PORTLANDOREGON.GOV/WATER/ARTICLE/415225](http://www.portlandoregon.gov/water/article/415225)
- THE REQUIRED HORIZONTAL CLEARANCE BETWEEN THE EXISTING WATER MAIN AND PROPOSED MANHOLE IS 5'. THE MINIMUM VERTICAL CLEARANCE (SKIN TO SKIN) BETWEEN PROPOSED SANITARY SEWER LATERAL AND EXISTING WATER MAIN IS 1.5 FEET.

WATER CONSTRUCTION NOTES:

- "THE CONTRACTOR SHALL MAINTAIN A MINIMUM 5' HORIZONTAL CLEARANCE FROM CAST IRON LINES WITH LESS THAN 24" OF COVER (AFTER EXCAVATION) WITH ALL CONSTRUCTION AND MATERIAL HANDLING EQUIPMENT.
- ALL BASE ROCK WILL BE PLACED BY DUMPING, SPREADING AND/OR PUSHING THE MATERIAL FROM THE BASE ROCK SURFACE WHEN IN AN AREA 5' FROM CENTER LINE OF CI PIPE WITH LESS THAN 24" OF COVER.
- VIBRATORY COMPACTION WILL NOT BE ALLOWED OVER CI PIPE WITH LESS THAN 24" OF COVER.
- HAN COMPACT BASE MATERIAL NEAR WATER MAIN SERVICE LINES, VALVE, BLOW OFFS AND OTHER WATER MAIN APPURTENANCES WHEN EXPOSED DURING EXCAVATION

PRIVATE SANITARY CONNECTION:
THE PRIVATE SANITARY CONNECTION WILL BE GRAVITY FED. NO PUMPING SYSTEM PLANNED

STORM DISPOSAL:
STORM DISPOSAL WILL BE INTO COMBINED SEWER EXTENSION IN SW GIBBS ST THROUGH A WYE CONNECTION TO SEWER LATERAL AS SHOWN ON PLANS..

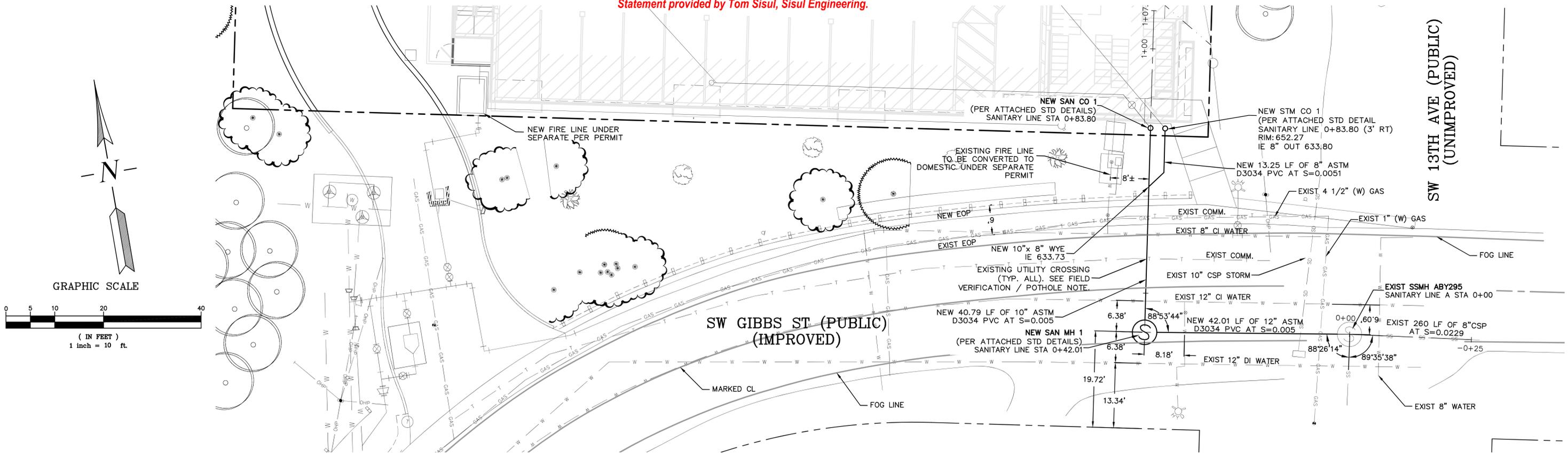


COMBINED SEWER EXTENSION
HORIZONTAL SCALE: 1"=10'
VERTICAL SCALE: 1"=5'

There will be no changes to the point of connection for sanitary sewer or stormwater as approved per this BES Public Works approval.

There will not be any increase in impervious surfacing to that area approved under LU 19-258445 PD LDP and 20-194259-CO. The stormwater management system will remain consistent with the approved stormwater management system per 20-194259-CO.

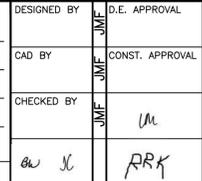
Statement provided by Tom Sisul, Sisul Engineering.



FILE: 19-029 BES-GIBBS.DWG CO2 6/21/2022 12:57:20 PM - JIMMYF

NO.	DATE	DESCRIPTION	APPD.

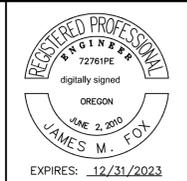
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PROJECT COMPLETED _____	CAD BY <u>JMF</u>	CONST. APPROVAL _____
MAP CORRECTED BY _____	CHECKED BY <u>JMF</u>	
CHECKED BY _____		



APPROVAL: _____
BES CHIEF ENGINEER REG. PROF. ENGR. 82245PE

BUREAU OF ENVIRONMENTAL SERVICES
MINGUS MAPPS COMMISSIONER
PAUL SUTO, P.E. CHIEF ENGINEER

SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 19-029 BES GIBBS.DWG
version: ACAD 2019



PUBLIC COMBINED SEWER EXTENSION FOR 1325 SW GIBBS ST
EA 19-110286
SW GIBBS PLAN & PROFILE

BES JOB NO. EP555
SHEET NO. 2 OF 5

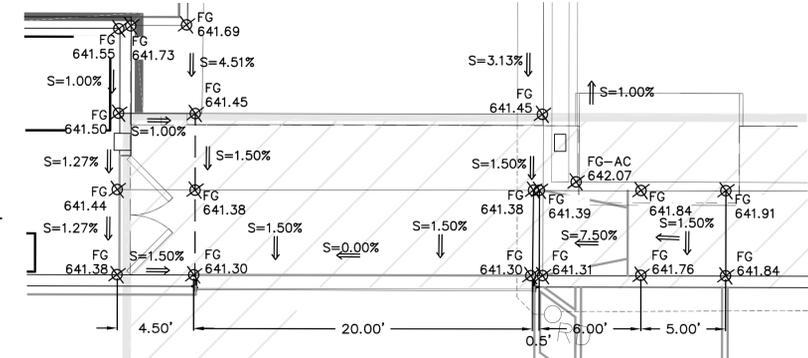
REVISIONS	BY
REVISED PLANS (1-31-2020)	JJM
REVISED PLANS (4-21-2020)	JJM
2/05/2021-1ST BDS REVIEW	JMF
4/20/2021-2ND BDS REVIEW	JMF
6/07/2021-3RD BDS REVIEW	JMF
COP 5TH REVIEW (11-16-2021)	JMF

1325 SW GIBBS STREET
SCOTIA WESTERN STATES HOUSING

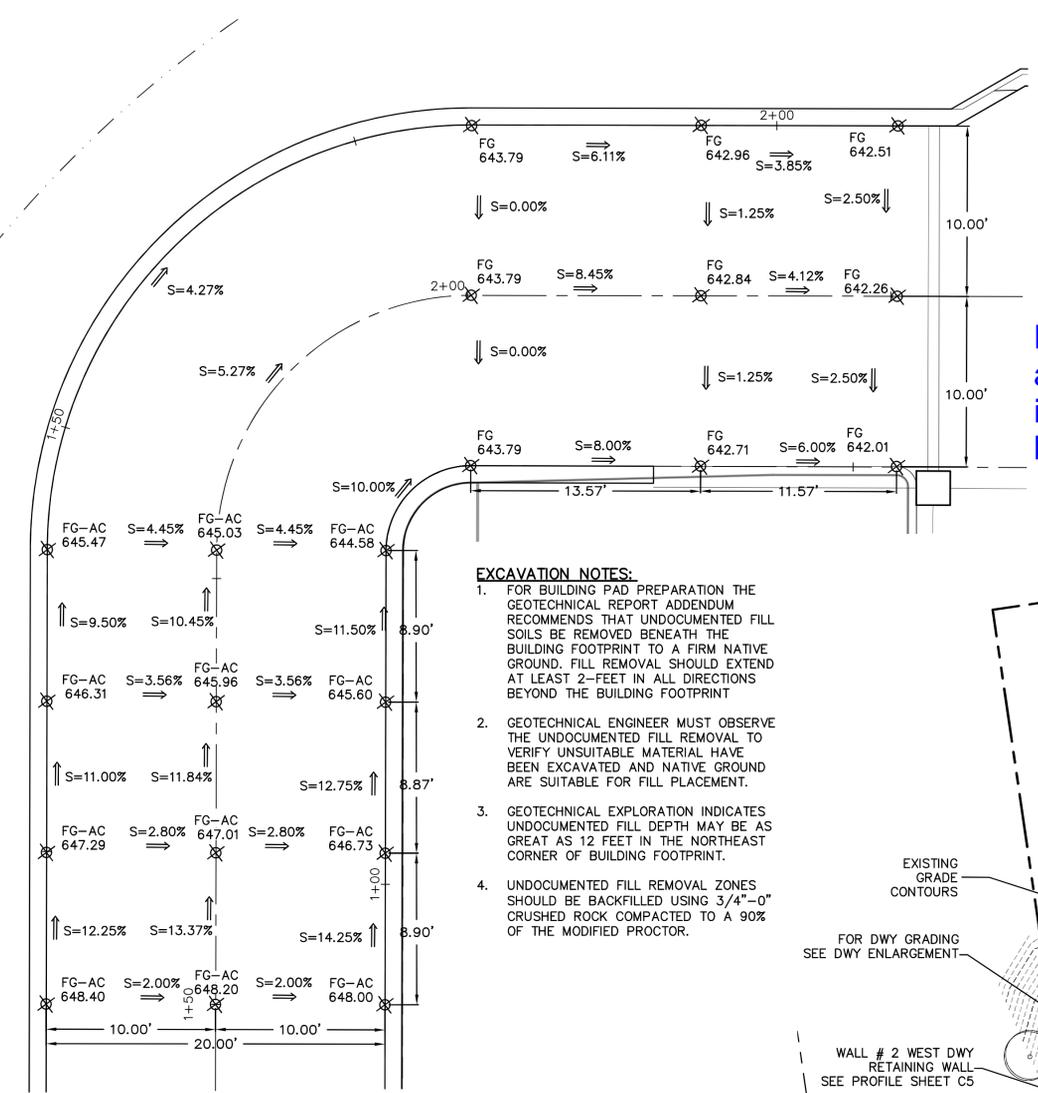
City of Portland
REVIEWED FOR
CODE COMPLIANCE
Date: 01/03/22
Permit # 20-194239-000-00

SISUL ENGINEERING
376 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0186
DRAWING: 19-022P-PRIVATE-COM-PLAN-PLAN.dwg

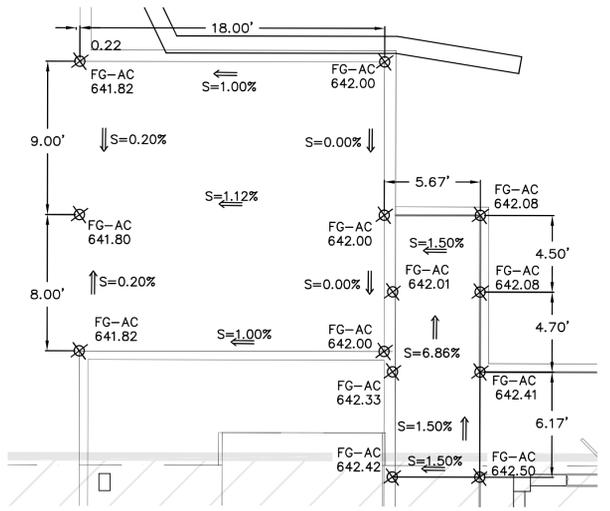
DATE NOV. 2019
SCALE 1"=20'
DRAWN JMF
JOB SGL19-029
SHEET
C6
OF C11 SHEETS



ADA RAMP / CROSSWALK
SCALE 1"=5'

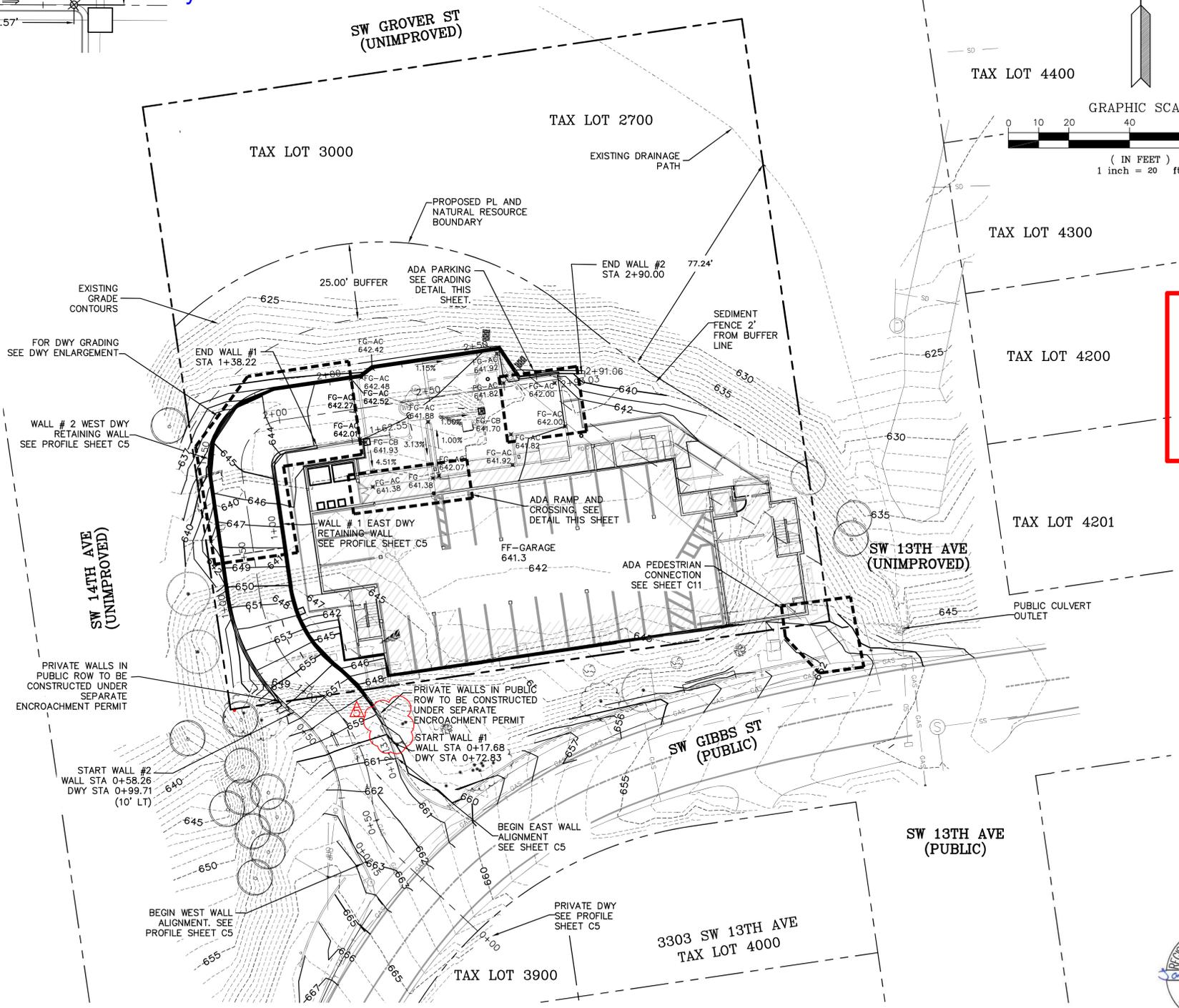


DWY GRADING ENLARGEMENT
SCALE 1"=5'

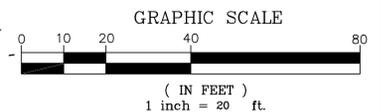


ADA PARKING GRADING DETAIL
SCALE 1"=5'

No changes to the approved grading plan is proposed.
by Tom Sisul 11-7-22



GRADING PLAN
SCALE 1"=20'



EXPIRES: 12/31/2021
11-16-21

From: [Stoken, Jed](#)
To: [Jaime Crawford](#)
Cc: [Dawkins, Leah](#)
Subject: RE: LU 22-187796 - Completeness Review
Date: Wednesday, November 2, 2022 7:20:01 AM

Hi Jamie,

If the site grades at the site will not change then the Geotechnical Addendum is not required. Could you please just include a note on the plans that the site grades to remain the same from the original land use decision.

Regards,
Jed

Jed Stoken, PE | Geotechnical Engineer
City of Portland | Bureau of Development Services
Site Development Section | Plan Review Services Division
1900 SW Fourth Avenue, Suite 5000
Portland, OR 97201

-
jed.stoken@portlandoregon.gov

phone: 503-823-8759

*Work Hours: Monday-Friday, 7:00AM - 4:30PM
Alternating Fridays Off*

Click through [here](#) for news about services at the Development Services Center.

From: Jaime Crawford <jaime@winterbrookplanning.com>
Sent: Tuesday, November 1, 2022 4:14 PM
To: Stoken, Jed <Jed.Stoken@portlandoregon.gov>
Cc: Dawkins, Leah <Leah.Dawkins@portlandoregon.gov>
Subject: LU 22-187796 - Completeness Review

Hi Jed,

I'm working with Leah Dawkins on our resubmittal to address completeness items for a land use application at 1311 SW Gibbs St. I wanted to touch base with you on your completeness item relating to a geotechnical addendum. Although we are technically increasing the outdoor common area to meet RM2 requirements, we are not actually proposing to increase site disturbance area. Instead, we are proposing to shift required landscaping (shrubs/trees) in one area to other locations around the site, allowing for a common grassed open space.

Attached are site photos and the site plan in context. As you can see, the proposed area planned for

the common lawn is level. The grading for this area was approved in the original LU approval and no additional grading/clearing/fill work is proposed. There are no retaining walls. The common lawn will be defined by a simple 6-inch curb.

With no additional site disturbance, we believe the original geotechnical study will be sufficient to address Landslide Hazard Approval Criterion. With this additional information, can you confirm our approach on not providing an addendum to the Geotech?

Thanks for your help,



Jaime Crawford | Assistant Planner

610 SW Alder St. | Suite 810 | Portland, OR, 97205

503.827.4422 ext. 107 | winterbrookplanning.com

1311 SW Gibbs Street Vicinity Map



Note: Entire map area is within potential landslide hazard area.

Map includes requirements of PZC 33.730.060.D.1.c

Zone - Comp. Plan Designation

- OS - Open Space
- R1 - Multi-Dwelling 1,000
- R5 - Single-Dwelling 5,000
- R7 - Single-Dwelling 7,000
- R10 - Single-Dwelling 10,000
- RM1 - Residential Multi-Dwelling 1
- RM2 - Residential Multi-Dwelling 2
- RM3 - Residential Multi-Dwelling 3
- CM2 - Mixed Use - Neighborhood

Overlay Zones

- p - Environmental Protection Zone
- c - Environmental Conservation Zone
- d - Design

- ⬢ 800' Vicinity Area
- ~ Stream / Drainage
- Project Site
- Utilities
- Bus Stop
- Zone Boundary
- City Owned Lot
- Sidewalk
- Recreation Trail



Appendix B:

Early Assistance Meeting Notes (EA 22-144899)

- **BDS – Land Use Planner Response: Leah Dawkins**
- **PHB – Inclusionary Housing Response: Chris Flanary**
- **BDS – Site Development Response: Jed Stoken**
- **PWB – EA Response: Kris Calvert**
- **BES – EA Comments: Ella Indarta**
- **PBOT – Development Review: Michael Pina**
- **PP&R – Urban Forestry: Mariano Masolo**

BDS – Land Use Planner Response

Pre-Application Conference

Date: June 15, 2022

To: Matt Wickstrom, Conference Facilitator
503-865-6513, matt.wickstrom@portlandoregon.gov

From: Leah Dawkins
503-865-6734, Leah.Dawkins@portlandoregon.gov

File No.: 22-144899

Location: 1311 SW GIBBS ST

Tax Account: R712893

Proposal: A Pre-Application Conference to discuss changes to an approved land use review (LU #19-258445 PD, ENM, LDP) to increase the number of dwelling units from 27 to 43 by converting 16 of the two and three-bedroom units to 32 studio and one-bedroom units. Proposed changes are internal to the building. No façade or building footprint changes are proposed. Increasing the number of dwelling units in an approved planned development requires approval through a Type III Planned Development Review.

The information provided at the conference and included in this response is based on the information you provided prior to and at the conference and reflects regulations in effect at the time of the conference. This response provides information and guidance only. It is preliminary in nature and based on the information the applicant provided to BDS staff. It is neither a land use review nor a final decision regarding this project. References are to the Portland Zoning Code available online at www.portlandonline.com/zoningcode.

A. Key Issues and Requirements

The following issues and requirements have been summarized for the applicant to pay special attention to as they may impact the proposed project.

- Adding dwelling units to the previously approved Planned Development (PD) review will require a Type III Amendment to the approved PD (see additional discussion below). Additionally, a Type Ix Partition Amendment will be required. The original land division approval was specific to the original development proposal allowed through the PD. Because the PD proposal is being modified, the land division approval criteria must also be reevaluated. The scope of the Partition Amendment will be limited to those criteria from the original review that are affected by the proposed change (see 33.660.320.B). The relevant approval criteria from 33.660.120 that should be addressed through the Partition Amendment are:
 - K. Transportation Impacts
 - L. Services and Utilities
- The provisions of the previous Environmental Review and the modifications approved through that portion of the review will continue to apply to site development.

- Per 33.854.500, Changes to an Approved Planned Development, this proposal constitutes an increase in density and would be reviewed through a Type III procedure.
- The maximum density for the site is based on the maximum floor area ratio permitted in the RM2 district divided by 1,000 (33.270.100.M.4). This number is added to the maximum density allowed in the R7 portion of the site. With Inclusionary Housing bonuses, the maximum number of units allowed on the site is 47 units. Without Inclusionary Housing bonuses, the maximum number of units allowed on the site is 32. These maximum density calculations are based on the area of the entire site, including the Environmental Tract.
- There are several new standards that must be met in the RM2 zone, including outdoor area and common areas. These standards may be challenging to meet on the site as currently designed. The new standards are addressed in **Section B** of this response.

Development Standards

The development standards that will apply to the potential project include, but are not necessarily limited to, those from the following chapters:

- [33.110 Single-Dwelling Zones](#)
- [33.120 Multi-Dwelling Zones](#)
- [33.245 Inclusionary Housing](#)
- [33.248 Landscaping and Screening](#)
- [33.266 Parking and Loading](#)

Planned Development Review (33.270 & 33.854)

The key issues related to the Planned Development approval criteria and review process are summarized, below:

- The Approval criteria for the proposed Planned Development Amendment are the criteria used for the original PD review. Therefore, the criteria from 2019 will be used, which were 33.854.310.E and F. You can find past versions of the zoning code here: [Efiles - Search Results \(portlandoregon.gov\)](#)
- The proposed site plan and elevations appear to demonstrate the development is being oriented towards the public realm with less active uses such as parking located behind the building.
- Natural features on the site such as trees and water features will continue to be preserved within an open space (environmental resource) tract as a result of the prior land division approval.
- The building must include architectural elements that compliment positive characteristics of surrounding development. Scale will be important to address considering the mix of single-dwelling and multi-dwelling structures in the vicinity. The building will be unchanged from the prior review.
- It will be important to identify the relationship between the proposed development and single-dwelling development within the vicinity.
- Renderings demonstrating the scale/appearance of the development from SW Gibbs Street would be helpful in addressing these criteria.

PD Approval Criteria for Modifications to Site-related Development Standards

- **Modifications to site-related development standards** can be requested as part of the Planned Development review, and are subject to the additional approval criteria for modifications to development standards in **33.854.320**. Development standards that are permitted to be adjusted can be modified. Your application needs to identify all modifications requested. Please keep in mind that these modifications must contribute to your proposal meeting the approval criteria for all zones (33.854.310, described above) and meet the purpose statement of the standard to be modified.

B. Questions Raised at the Conference

1. *Our preference would be to convert 16 units to 32 units for a total of 43 units. Would this be acceptable to staff?*

With a PD amendment review, it is likely that the addition of 16 units would be supportable if the standards of the RM2 zone can be met.

2. *Does the City see any issues with the revised project meeting the updated Code Standards?*

The revised project will have to meet the new standards of the RM2 zone. The building design may need to be altered to meet the following standards:

Building Height: No additional review appears to be necessary for height because the Environmental Modification was approved to allow the entire height of the structure to be 45-49 feet tall.

Bicycle Parking: The addition of new units will increase the required bicycle parking spaces to 56 long-term spaces.

Required Outdoor Areas: The RM2 zone requires outdoor and common areas (33.120.240). RM2 requires 48 square feet of outdoor area per unit for a total of 2,064 square feet for 43 units, or an additional 768 square feet for 16 new units. You are permitted to use the required outdoor common area to meet the required outdoor areas of individual units. The outdoor area for additional units can be provided through the outdoor portion of the common area.

Required Common Area Standard: Required common area is triggered on multi-dwelling residential sites that are greater than 20,000 square feet in size. In this case, the portion of the site in the RM2 zone is exactly 20,000 square feet. Therefore the common area standard does not apply.

Asphalt Limitation: Because the asphalt limitation is only required in the RM2 zone, the limit will be based on the RM2 side of the site (33.266.130.C.4). However, in terms of site planning, it is recommended that you carry this across the entire site for more cohesive site design.

Building Length and Façade Articulation: In the RM2 zone, the maximum building length for the portion of buildings located with 30 feet of a street lot line is 100 feet (33.120.230.B.). Façade articulation applies to buildings more than 35 feet in height that have façade areas of more than 3,500 square within 20 feet of a street property line. If this is the case, at least 25% of the area of a street-facing façade within 20 feet of a street lot line must be divided into façade planes that are offset by at least 2 feet in depth from the rest of the façade. If this standard is not met on the existing building, a modification can be requested through the PD Amendment process.

3. *The site is split zoned R7 and RM2. Proposed buildings extend across zone boundaries. No parking is required for Household Living in the R7 zone; One space per two units is required for RM2 "far from transit". How would parking requirements apply to the proposed amendment?*

Parking standards would have to be met based on the new zoning requirements in RM2 zone for sites far from transit. Using the entire unit count, 22 parking spaces are required. 28 spaces are provided on the plans, so parking requirements should be met.

The number of bicycle parking spaces will need to be increased to 56 spaces to accommodate the addition of 16 units.

4. *What does the process look like for this amendment to occur?*

Staff has determined that the application should be reviewed through a Type III Planned Development Review Amendment and concurrent Type IX Partition Amendment. These will be processed together under one Type III review. The modifications approved through Environmental Review are not impacted by the proposed change.

5. *Can we start the building permit process simultaneously with the land use?*

The current building permit reviews and construction can be continued through the PD review. The revision to the building permit for the new units cannot be reviewed until the PD and Partition Amendments are approved. It is recommended that the applicant have a meeting with Life Safety and Permitting Services in order to determine what building permit process will be required to add the units and if Life Safety will approve the addition of the 16 units.

C. Land Use Reviews Required

The following table identifies land use reviews required for your project. Please refer to the identified code citations for additional information. Information and handouts on [land use reviews](#) are available at <https://www.portlandoregon.gov/bds/37424>. For information on review procedures and timelines, see the [Summary of Procedure Types](#) which can be accessed using the following link: <http://www.portlandonline.com/shared/cfm/image.cfm?id=71804>

Review Procedure	Land Use Review	Approval Criteria
Type III	Planned Development Amendment	33.854.310.E-F 33.854.320 (if mods requested)
Type IX	Partition Amendment	33.660.320.B

D. Development Standards

▪ **Base Zone Development Standards**

Proposed development must meet development standards of the RM2 and R7 base zone. Existing development may not go further out of conformance with standards.

- Refer to Zoning Code Chapter 33.110 and 33.120 for standards applicable in this zone.

▪ **Overlay Zone Standards**

This site is also located in the Environmental Conservation overlay zone.

- Refer to Zoning Code Chapter 33.430 for regulations applicable in this overlay zone.

▪ **Parking and Loading**

Proposed development must comply with the requirements of [Zoning Code Chapter 33.266, Parking and Loading](#) which can be accessed online at <https://www.portlandoregon.gov/bps/article/53320>.

▪ **Landscaping and Screening**

Development must comply with landscaping and screening requirements in the following Zoning Code chapters:

- [Chapter 33.120.235, Landscaped Areas](#);
- [Chapter 33.248, Landscaping and Screening](#) accessible at: <https://www.portlandoregon.gov/bps/article/53315>.

Additional landscaping standards specific to parking and loading areas for uses other than houses, duplexes, triplexes and fourplexes are identified in [Zoning Code Section 33.266.130.G](#) accessible at: <https://www.portlandoregon.gov/bps/article/53320>

- **11.50.050 Tree Density Standards (Tree Planting Requirements)**

Title 11 requires trees to be planted to provide a canopy equivalent to 20% of the site area. Trees that are preserved may count toward the planting requirement, but they must be preserved in accordance with Chapter 11.60 (see above). Please refer to the Plant Materials chapter of the Tree and Landscaping Manual for tree sizes: <http://www.portlandoregon.gov/bds/index.cfm?a=71964>

Large trees count as 1,000 square feet of area, medium trees as 500 square feet, and small trees as 300 square feet. Note per Table 50-3 that minimum planting areas are also required based on the size of the tree.

Please show the size, location and species of all trees proposed to be preserved and planted on your site plan. If trees are proposed to be preserved to meet this standard, please show and label the required tree protection area and fence materials on your site plan.

As an alternative to meeting the tree planting requirements, you may elect to make a payment to the Tree Planting and Preservation Fund at the time of permit review. The rate is \$487.50 per 500 square feet of tree canopy.

E. Previous Land Use Reviews.

As part of your application, address relevant conditions of approval from previous land use reviews on the site and discuss the current status of compliance. Below are the relevant land use case reviews that the City of Portland has on record for the subject site:

- **CU 075-76:** Approval of a Conditional Use Review for a convalescent home.
- **CU 020-79:** Approval of a Conditional Use review for a welfare institution with variances to minimum site are and minimum front yard requirements.
- **LUR 96-00236:** Approval of an expansion of a conditional use for a group living use in a residential zone at 1325 SW Gibbs, in order to create an enclosed connection between the two existing buildings and to add a vertical lift.
- **LU 19-258445 PD LDP ENM:** Approval of a Planned Development (PD), Land Division Partition (LDP), Environmental Review Modification (ENM) to allow 5-story, 27-unit multi-dwelling structure that will include 28 vehicle parking spaces and 30 long-term bike parking spaces

F. Neighborhood Contact Requirements (33.705): Neighborhood Contact is not triggered by this proposal because building area is not proposed to be increased. However, if you would like to proceed with neighborhood contact in order to inform the neighbors of your amended proposal, you are encouraged to do so.

G. Submittal Requirements for Land Use

This list identifies the materials you must submit for your application to be considered complete.

	Item to submit	Details
1.	Completed application form	Application Form available at: https://www.portland.gov/bds/land-use-review-fees-and-types/land-use-reviews-and-final-plat-applications
2.	Fee	https://www.portland.gov/bds/current-fee-schedules#toc-city-of-portland-fee-schedules
3.	Requirements for written narrative, maps, plans, etc.	General Submittal Requirements (Zoning Code Chapter 33.730.060) accessible online at: https://www.portlandoregon.gov/bps/article/53468
4.	Optional: Request for an	Allows new facts and evidence (an “evidentiary hearing”)

	Item to submit	Details
	Evidentiary Hearing and Waiver of Right to a Decision within 120 Days	<p>to be presented if your project is appealed to City Council. You must submit the request form within 21 days of submitting your land use review application.</p> <p>The 120 day waiver can be accessed at:</p> <p>https://www.portland.gov/sites/default/files/2021/waiver_within_120days_041421.pdf</p>

To submit Land Use Reviews and other LUS applications:

When you are ready to submit a land use review application, please see the BDS Website at for current submittal requirements: <https://www.portland.gov/bds/land-use-review-fees-and-types/land-use-reviews-and-final-plat-applications>. Currently, we are accepting electronic land use applications via email at LandUseIntake@portlandoregon.gov. A Land Use Services technician will contact you with instructions for providing payment for emailed applications.

To submit Permits:

When you are ready to submit a permit, please see our website for updated information on how to apply for permits: <https://www.portland.gov/bds/permit-review-process/apply-or-pay-permits> .



Portland Housing Bureau

PHB- Inclusionary Housing Response Pre-Application Conference

Date: June 17, 2022

To: Sean O'Neill (SWS PDX., LLC)

From: Chris Flanary, Housing Program Specialist, Chris.Flanary@portlandoregon.gov

Case File: 22-144899

Location: 1311 SW Gibbs St

Property ID: R712893

Proposal: A Pre-Application Conference to discuss changes to an approved land use review (LU #19-258445 PD, ENM, LDP) to increase the number of dwelling units from 27 to 43 by converting 16 of the two and three-bedroom units to 32 studio and one-bedroom units. Proposed changes are internal to the building. No façade or building footprint changes are proposed. Increasing the number of dwelling units in an approved planned development requires approval through a Type III Planned Development Review.

Section I: Inclusionary Housing Code Overview

Pursuant to Inclusionary Housing Code Section PCC [30.01.120](#) and Zoning Code Chapter PCC [33.245](#), effective February 1, 2017, all residential buildings proposing 20 or more new residential units must provide a percentage of the new units at rents affordable to households at 80% of the Median Family Income (MFI). The City has defined four different options for meeting the Inclusionary Housing (IH) requirements. All permit applications must include one of these options in their proposal or pay a fee-in-lieu.

The IH affordability period is 99 years and the property owner must enter into an Inclusionary Housing Covenant with Portland Housing Bureau (PHB), the details of which are specific to the IH option chosen, and must submit to annual monitoring by PHB Risk Analysis & Compliance team, including the submission of annual rents and tenant income.

PHB has reviewed the materials submitted as well as the input from the meeting to determine the minimum requirements of the proposed building and provide the following comments.

Section II: Property Specifics

A. Subjectivity to Inclusionary Housing

The applicant was informed that this project, in its current configuration, triggers the IH requirements.

The applicant's development proposal includes (43) housing units with the following designations:

- (16) Studio units
- (16) Windowed One-bedroom (W1B) units

- (11) Windowed Two-bedroom (W2B) units

The options discussed below specifically correlate to the information provided. PHB asks the applicant to be aware that any change in the building plans detailing the number of units, types, and sizes will require additional PHB review.

B. IH Options Discussed in Pre-Application Conference

Option 2: 60% MFI units

Option 2 would require that the building offer 10% of its residential units at 60% MFI. Current rent restriction figures can be found on page 2 on the document located here:

<https://portland.gov/sites/default/files/2020-04/2020-ami-rents-phb.pdf>

Reasonable Equivalency:

As per Inclusionary Housing Administrative Rules, applicants are required to make Inclusionary Housing Units (“IH Rental Units”) reasonably equivalent to market rate units. PHB will assess reasonable equivalency using the following criteria:

- **Bedroom Distribution and Unit Count:** IH Rental Units must be provided at the same ratio within the development as market rate units
- **Unit Sizes:** IH Rental Units must be at least 95% the size of the average of the total units with the same bedroom count, as measured in square feet.
- **Unit Distribution:** No more than 25% of the total units on any floor shall be designated as IH Rental Units, excluding the top floor of a development.
- **Unit Amenities:** IH Rental Units must have like or equal performing finishes and appliances as far as durability and sustainability to the market rate units, which will be certified by a development’s architect prior to receiving its final certificate of occupancy.

To meet the Reasonable Equivalency standard of Bedroom Distribution and Unit Count, the building must provide 10% of each unit type as IH Rental Units. With the currently proposed plans, this equates to a total of four (4) IH Rental Units, distributed as follows:

- (2) Studio units
- (1) Windowed One-bedroom (W1B) units
- (1) Windowed Two-bedroom (W2B) units

Reconfiguration:

The applicant may also elect to provide an alternative mix of IH Rental Units based on the total number of bedrooms included in the development – Reconfiguration. This option allows for redistribution of bedrooms into IH Rental Units of two bedrooms or more. This results in a building with a smaller overall number of IH Rental Units that are greater in size.

The following configurations meet the IH requirements under Reconfiguration:

Current Unit Requirements (4 IH Units)	Total Bedrooms in Current Unit Requirements	Reconfiguration Option A (3 IH Units)
---	--	--

(2) Studio units (1) W1B units (1) W2B units	5 bedrooms	(1) Studio/W1B (2) W2B units
--	------------	---------------------------------

Additional requirements that must be met when utilizing Reconfiguration:

1. After Reconfiguration is applied:

- There must be at least one Market Rate Unit that has two or more Bedrooms per reconfigured Dwelling Unit with three Bedrooms; and
- There must be at least one Market Rate Unit that has three or more Bedrooms per reconfigured Dwelling Unit with four Bedrooms.

2. The reconfigured Dwelling Units must be reasonably equivalent to the size of the average square footage of:

- The one-bedroom Dwelling Units as follows by reconfigured Bedroom count:
 - 130% for reconfigured two-Bedroom Dwelling Units;
 - 160% for reconfigured three-Bedroom Dwelling Units; and
 - 190% for reconfigured four-Bedroom Dwelling Units; or
- If there are no one-Bedroom units to consider, the reconfigured Dwelling Units must be reasonably equivalent to the size of the average square footage of the studio Dwelling Units as follows by reconfigured Bedroom count:
 - 195% for reconfigured two-Bedroom Dwelling Units;
 - 240% for reconfigured three-Bedroom Dwelling Units; and
 - 285% for reconfigured four-Bedroom Dwelling Units; and
- The reconfigured Dwelling Units must be larger than the size of the average square footage of each of the smaller Dwelling Units by Bedroom count.

Incentives:

As proposed above, the project would be eligible to receive:

- 10-year property tax exemption on the IH Rental Units, associated percentage of the square footage of the common residential area and residential designated parking.
 - *Multiple Unit Limited Tax Exemption (MULTE) Program City Code 3.103:*
<https://www.portlandoregon.gov/citycode/28466>
 - If the project will be providing affordable units in addition to the minimum number of required units to fulfill the IH Program requirements, the MULTE Program may be available for the additional portion of the project with restricted affordability. Additional program information is available online at www.portlandoregon.gov/phb/multe.
 - If the property will be owned and operated by an eligible non-profit housing provider, in accordance with ORS 307.540, for low-income tenants (at or below 60% MFI), the property owner can utilize the Non-Profit Limited Tax Exemption (NPLTE) Program, which has an ongoing annual certification process. The NPLTE applies to the percentage of a property that is dedicated to low income tenants, including the value of the structural improvements and applicable common areas and parking, plus the value of the land. Additional program information is available online at <https://portland.gov/phb/nplte>.
- Affordable Housing Construction Excise Tax (AHCET) exemption for the IH Rental Units
 - *Affordable Housing Construction Excise Tax FAQ:*
<https://www.portlandoregon.gov/bds/article/584417>

- *AHCET Exemption application:*
<https://www.portlandoregon.gov/bds/article/584608>
- System Development Charge (SDC) exemption for the IH Rental Units
 - *System Development Charge Exemption Program website:*
<https://www.portlandoregon.gov/bds/article/166412>
 - If the project will be providing affordable units in addition to the minimum number of required units to fulfill the IH Program requirements, the SDC Exemption Program may be available for the additional portion of the project with restricted affordability. Additional program information is available online at <https://www.portlandoregon.gov/phb/74642>.
- Full or partial exemption from parking requirements as detailed in Portland Zoning Code, administered by Bureau of Development Services, Planning and Zoning.
- FAR density bonus as detailed in Portland Zoning Code, administered by Bureau of Development Services, Planning and Zoning.

Accessibility:

For Projects that receive the 10-year property tax exemption through the MULTE Program, at least 5% of the Rental Units restricted under the MULTE Regulatory Agreement must be built to be Type A as defined by the Oregon Structural Specialty Code, according to 3.103.040(D).

Section III: Important Reminders

Below is a list of common items that can cause a delay in PHB signing-off on your permit. Please make sure your permit application, project plans, and project timelines all account for the following:

- A. **Inclusionary Housing Intake Form**—PHB needs a completed Intake form in order to determine IH requirements and corresponding incentives for the project. It is best practice to submit the intake form with permit application.
- B. **Gross Square Footage (GSF) of Units within the Plan Set**—PHB needs the GSF of each residential dwelling unit in a project clearly identified within the plan set. It is PHB’s preference that this information be included in the architectural pages within the individual units on the floor plans. If that is not possible, please provide a chart with the unit number, unit type (studio, 1BR, etc.), and GSF either in the code summary or architectural pages. For PHB, GSF should be calculated using Life Safety’s definition.
- C. **MULTE Approval**—All projects that apply for the MULTE must go before Portland City Council for approval. The MULTE Council process takes 5-7 weeks to complete. Please work this process into your project timeline and provide a completed MULTE Application as soon as possible. As a reminder, this 7 to 9-week process does not include finalizing regulatory agreements, which can take several more weeks.
- D. **Water Meter Sizing Worksheet (W-4):** Projects that are mixed-use, and applying for SDC Exemptions, are required to submit two W-4s to the Portland Water Bureau. One W-4 showing only the residential space and a second W-4 showing only the commercial space. Please label each as either “Residential” or “Commercial” at the top of the corresponding W-4 form. These should be submitted with your SDC application packet with the permit application. If the W-4 forms submitted do not follow the guidelines above, you will need to resubmit the W-4 forms before SDC exemption estimates can be calculated, which may delay the permit approval timeline. W-4 Forms are available at: <https://www.portlandoregon.gov/water/article/582849>. If you have specific questions about completing the W-4, please contact the Portland Water Bureau.
- E. **SDC Loan/Deferral Contracts:** When requesting an SDC loan or deferral contract, applicants should wait until after the SDC Exemption is approved by PHB to make the loan or deferral contract

request to the SDC Bureau. This will ensure that the loan/deferral contract is for the correct amount. More information on the assessment of SDCs can be found at <https://www.portlandoregon.gov/bds/article/166412>.

- F. **Rent Determination:** PHB determines rent using the HUD's annually published Median Family Income and Rent Chart (MFI and Rent Chart) for the Portland Metropolitan Area. The current MFI and Rent Chart is Available at <https://www.portland.gov/sites/default/files/2021/2021-ami-rents-phb.pdf>. The MFI and Rent Chart is posted and updated annually on the PHB website. Maximum rent paid by tenants includes any utility allowance (see below) or required expenses to live in an IH Rental Unit (i.e. parking, meal plans, and amenities). Details and instructions on how to apply the MFI and Rent Chart can be found in PHB's Risk Analysis & Compliance Policies & Guidelines located at <https://www.portland.gov/phb/development-incentive-project-compliance>.
- G. **Utility Allowance:** IH Rental Units are subject to a utility allowance (UA). Utility allowances are passed along to the tenant in the form of reduced rent for those utilities which are paid for by the tenant.

Owners may choose from one of two methods to determine the utility allowance:

1. Utility Allowance Schedule. The Utility Allowance Schedule takes averages obtained from local utility providers to determine the utility allowance based on the Building's types of utilities. <https://www.portland.gov/sites/default/files/2021/2020-utility-allowances-eff-05-01-2020.pdf>
2. Energy Consumption Model Analysis. Applicant may commission an Energy Consumption Model Analysis through an Oregon Housing and Community Service (OHCS) approved Energy Consumption Model Analyst to determine the utility allowance based on the specifics of the Building.

To the extent IH Rental Unit tenants pay their own utilities directly or are billed back for reimbursement of utilities by the Owner, the Owner must deduct that amount from the maximum allowable rent charged to the tenant. No UA is required for utilities paid by the Owner and not reimbursed by the tenant.

Example: a one-Person household earning 80% MFI occupies a studio IH Rental Unit. The maximum monthly rent for that unit, based on the MFI and Rent Chart, is \$1,140 and the UA is \$84. The maximum an Owner could collect from this household is \$1,056 = (\$1,140 - \$84).

For more information, you may refer to the Inclusionary Housing Program Administrative Rules: <https://www.portlandoregon.gov/citycode/article/626683> or visit the Inclusionary Housing website at <https://portland.gov/inclusionary-housing/inclusionary-housing-comprehensive-guide>. To reach PHB's Inclusionary Housing staff, please call 503-823-9042 or email Inclusionary-Housing@portlandoregon.gov.



City of Portland, Oregon
Bureau of Development Services
Land Use Services
FROM CONCEPT TO CONSTRUCTION

Dan Ryan, Commissioner
Rebecca Esau, Director
Phone: (503) 823-7300
Fax: (503) 823-5630
TTY: (503) 823-6868
www.portland.gov/bds

Site Development

Early Assistance Meeting Response

Date: June 27, 2022

To: Sean O'Neill
SWS PDX, LLC
503-956-9307, Sean@scotiawsh.com
15962 Boones Ferry Rd, Ste. 202
Portland, OR 97035

From: Jed Stoken
BDS Site Development Section
503-823-7579, Jed.Stoken@portlandoregon.gov

Case File: EA 22-144899

Location: 1311 SW GIBBS ST

R#: R712893

Proposal: A Pre-Application Conference to discuss changes to an approved land use review to increase the number of dwelling units from 27 to 43 by converting 16 of the two and three-bedroom units to 32 studio and one-bedroom units.

The Site Development Section of the Bureau of Development Services (BDS) has reviewed the pre-application conference materials to identify potential issues and requirements.

A. Geotechnical Engineering Requirements

In order to evaluate whether the foundation design of the proposed structure complies with the Oregon Structural Specialty Code, at the time of plan review the applicant must submit a geotechnical report.

B. Stormwater Disposal and Treatment

Site Development will review the setback distance from the stormwater infiltration features to the edge of site structures for conformance to the 2020 Stormwater Management Manual. The Bureau of Environmental Services will review the project for conformance to all other aspects of the 2020 Stormwater Management Manual.

C. Erosion Control

Erosion prevention and sediment control requirements found in **Title 10** apply to both site preparation work and development. Full compliance with the erosion control requirements of Title 10, as well as maintenance of the erosion control elements, such as silt fences on private property, storm drain inlet protection and bio bags in the public right-

of-way, is the responsibility of the property owner, the developer, and the builders. An erosion control plan must be submitted at the time of plan review. The project area meets the criteria specified in City Code 10.30.030 as a Special Site with additional requirements for erosion, sediment and pollution control. An erosion control plan prepared by a Certified Professional in Erosion and Sediment Control (CPESC) or State of Oregon registered professional engineer, and special inspections by the CPESC or P.E. during construction, will be required at the time of building permit review.

D. Permit Requirements

1. Commercial building permit.

Water Bureau

Early Assistance Appointment Response

Date: June 16, 2022

To: Matt Wickstrom, 503-865-6513, matt.wickstrom@portlandoregon.gov

From: Kris Calvert, 503-865-6373, Kris.Calvert@portlandoregon.gov

Case File: EA 22-144899

Location: 1311 SW GIBBS ST

Property ID: R712893

Proposal: A Pre-Application Conference to discuss changes to an approved land use review (LU #19-258445 PD, ENM, LDP) to increase the number of dwelling units from 27 to 43 by converting 16 of the two and three-bedroom units to 32 studio and one-bedroom units. Proposed changes are internal to the building. No façade or building footprint changes are proposed. Increasing the number of dwelling units in an approved planned development requires approval through a Type III Planned Development Review.

The Portland Water Bureau (PWB) has reviewed the early assistance materials to identify potential issues and requirements.

A. KEY ISSUES

1. This development was reviewed under 20-194259-CO. A 3" meter was approved and that meter will have substantial unused capacity. Any changes to the fixture count will need to be documented in a new W-4 form submitted at the time of permit application however an increased meter size is unlikely.



CITY OF PORTLAND ENVIRONMENTAL SERVICES



1120 SW Fifth Avenue, Suite 613, Portland, Oregon 97204 ■ Mingus Mapps, Commissioner ■ Michael Jordan, Director

Early Assistance Comments

Date: June 16, 2022

To: SWS PDX., LLC *SEAN O'NEILL*, Applicant

From: Ella Indarta, BES Systems Development
503-823-2073, Ella.Indarta@portlandoregon.gov

Case File: EA 22-144899

Location: 1311 SW GIBBS ST

The Bureau of Environmental Services (BES) has reviewed the submitted materials to identify potential issues and requirements and provide the following comments. Some references to Portland City Code (PCC) are included below; the applicant may refer to the Auditor's Office [Online Charter and Code](#).

A. KEY ISSUES AND REQUIREMENTS

Following is a brief summary of issues and requirements that may impact your proposed project or are submittal requirements that will require time to prepare prior to submittal of the application.

1. The applicant must submit a site plan showing the sanitary sewer connection location(s) and stormwater disposal system(s) for all proposed sanitary connections and stormwater systems.
2. The applicant must submit a stormwater report, including the results of infiltration testing, with the land use and building permit application.
3. Required public right-of-way improvements and a sewer extension have triggered stormwater improvements, which are currently being reviewed through a Public Works Permit.

B. SANITARY SERVICE

1. *Sanitary Infrastructure:* According to available GIS data, the following sewer infrastructure is located in the vicinity of the project site:
 - a. Public 8-inch CSP sanitary sewer in SW 13th Ave (BES as-built # 3172). This main is within the conservation zone.
 - b. Public 8-inch CSP combined sewer in SW Gibbs St east of the property (BES as-built # 1182). This main is not within the frontage of the property.
 - c. Public 8-inch CSP combined sewer in SW 13th Ave south of the property (BES as-built # 1182). This main is not within the frontage of the property.
 - d. Public 8-inch CIP combined sewer in the right-of-way north of the property and within a public easement north of the property (BES as-built # 2234).
 - e. A Public Works permit (EP555) for a combination sewer extension in SW Gibbs is currently under review.
2. *Water Main and Sewer Main Locations:* According to available GIS data, three water mains are located between the proposed development site and the combined main in SW Gibbs St. The location of these water mains may make the extension of and connections to the sanitary sewer challenging. Sanitary laterals must meet required separation distances according to the Water Bureau; the applicant should contact the assigned Water Bureau

Ph: 503-823-7740 Fax: 503-823-6995 ■ www.portlandoregon.gov/bes ■ Using recycled paper ■ An Equal Opportunity Employer

The City of Portland complies with all non-discrimination laws including Title VI (Civil Rights) and Title II (ADA).

To request a translation, accommodation or additional information, please call 503-823-7740, or use City TTY 503-823-6868, or Oregon Relay Service 711.

LU 22-187796 APD AP
Exhibit A.16

reviewer or the Water Bureau hotline (503-823-7368) with questions related to required separation distances. Note that BES may require proof of feasible connections for each lot prior to approval of the land use review.

3. *Connection Requirements:* Connections to the City sewer system must meet the standards of the City of Portland's [Sewer and Drainage Facilities Design Manual](#), [PCC 17.32.090](#), administrative rules [ENB-4.07](#) and [ENB-4.17](#), and all other relevant City codes and rules. Sanitary sewage from private property must be separately conveyed to the property line and connected through individual laterals for discharge to the City separate sanitary or combined sewer. Per [ENB-4.07](#), sewer connection permits are required to make new connections to City mains and laterals, relocate or upsize existing laterals, and repair sewers in City right-of-way. The permittee is responsible for verifying the location, depth and size of an existing sewer lateral and for ensuring the lateral is clear of obstructions prior to connection.

C. **STORMWATER MANAGEMENT**

1. *Stormwater Infrastructure:* According to available GIS data, the following stormwater infrastructure is located in the vicinity of the project site:
 - a. There are no public storm-only sewers available to this property.
 - b. A Public Works permit (EP555) for a combination sewer extension in SW Gibbs is currently under review.
2. *General Stormwater Management Requirements:* Development and redevelopment sites that include any of the triggers listed in PCC 17.38.040 are subject to the policies and standards of PCC 17.38.035, Portland's [Stormwater Management Manual](#) (SWMM) and [Source Control Manual](#) (SCM). Projects must comply with the current adopted version of the SWMM as of the permit application date. A fundamental evaluation factor in the SWMM is the Stormwater Infiltration and Discharge Hierarchy (Section 1.3.3), which sets the framework that will be used to determine when a project's stormwater runoff must be infiltrated onsite and when offsite discharge will be permitted, and the parameters that must be met for either scenario. If tested infiltration rates on a property are greater than or equal to 2 inches per hour, onsite infiltration will be required unless the site qualifies for the ecoroof exception per Section 3.2.1 or infiltration is determined infeasible based on site conditions described in Chapter 2 of the SWMM. Note that maximum building coverage allowed by the zoning code, including below grade development, does not exempt the applicant from stormwater requirements. Pollution reduction and flow control requirements must be met using vegetated facilities to the maximum extent feasible, though roof runoff and some paved impervious surfaces are exempt when discharging directly to a UIC (refer to Sections 1.3.2, 1.3.4, 3.2.4 and 4.2.2 of the SWMM).
3. *Public Right-of-Way Stormwater Management:* Stormwater runoff from the public right-of-way must comply with all applicable standards of the SWMM and Sewer and Drainage Facilities Design Manual and be conveyed to a discharge point along a route of service approved by the BES Director or the Director's designee. Additional guidance on meeting the 2020 SWMM for projects in the public right-of-way is available at <https://www.portland.gov/bes/stormwater/swmm-and-right-way-projects>.
 - 1) Special Circumstances for frontage improvements were approved and an offsite management fee has been paid.
4. *Private Property Stormwater Management:* Stormwater runoff from this project must comply with all applicable standards of the SWMM and SCM and be conveyed to a discharge point along a route of service approved by the BES Director or the Director's designee.

- a. The applicant must submit a Presumptive (SWMM Section 2.5.2) or Performance Approach (SWMM Section 2.5.3) stormwater report and a preliminary utility plan showing stormwater management facilities sized according to SWMM standards. The report must follow the outline included in Section 3.4.3 of the SWMM and be stamped by an Oregon registered engineer or other qualified design professional. Required elements of the report include:
 - 1) Results of infiltration tests, unless an Oregon-licensed engineer, certified engineering geologist, or registered geologist presents compelling evidence that site conditions make onsite infiltration of stormwater infeasible. The stormwater report narrative must incorporate this evidence in addressing how the project will meet the Stormwater Infiltration and Discharge Hierarchy. Infiltration testing may still be required at the discretion of BES. If testing is completed, the design team must test infiltration rates at depths that appear conducive to infiltration using field-based decisions and by referencing available soil information, geotechnical analyses and/or boring logs. To the extent feasible, proposed infiltration facilities must correspond with the location and depth of the completed infiltration testing. Indicate on a plan the approximate location of the test(s). Refer to Section 2.3.2 of the SWMM for complete infiltration testing requirements.
 - 2) Calculations prepared by an engineer using the [Presumptive Approach Calculator \(PAC\)](#). If using other software under the Performance Approach, the principles of Section 2.5.3 must be followed.
 - 3) If BES approves offsite discharge to the combined sewer, PCC 17.38 and the SWMM require stormwater discharge to be controlled so that the post-development 25-year peak flow rate is limited to the pre-development 10-year peak flow rate. The applicant must show through the Presumptive or Performance Approach stormwater report how flow and volume control standards that apply to the proposed discharge point will be met.

D. GENERAL PUBLIC WORKS PERMIT INFORMATION

For questions related to the public improvements described throughout these notes, please contact Rod Krauter at (503) 823-7064 or rodney.krauter@portlandoregon.gov or the BES Development Engineering hotline at (503) 823-7761, option 3.

1. *General Public Works Permit Information:* Information on the City's public works permit (PWP) process, including submittal requirements and review timelines, is available at www.portlandoregon.gov/publicworks. All submitted public works plans must meet the City's Sewer and Drainage Facilities Design Manual (SDFDM), SWMM, and public works permitting plan submittal requirements and drafting standards. Contact Public Works Permitting at (503) 823-1987 or pwp@portlandoregon.gov with questions related to the general public works permit process.
2. *Hazardous Substances Code:* The City's Hazardous Substances Code (PCC 17.24.067) requires the excavation and removal of disturbed contaminated soils from right-of-way access areas and utility corridors. The soils must be replaced with clean fill at a minimum depth of 5 feet. A demarcation/contaminant barrier is also required when it has been determined the soils are contaminated at depth. Erosion control measures for contaminated soils (Section 8.2.1 of the SCM) must be met. Soil stockpiles must be covered and contained with a barrier on all four sides, with an impervious layer underneath the stockpile to inhibit contaminants from leaching back into the soil.

E. SUBMITTAL REQUIREMENTS FOR LAND USE

1. Full land use plan set, including preliminary utility plan showing all existing and proposed sanitary and storm facilities and connections.
2. A Presumptive or Performance approach stormwater report as described in this memo.

F. PERMIT INFORMATION

At the time of permit review the applicant should be aware of the following:

1. *Connection Fees*: Sewage system connection fees and system development charges are assessed at the time of building plan review and change every fiscal year on July 1st. For additional information on these fees, navigate [here](#) or call the BES Development Review Team at 503-823-7761.
2. *Connection Requirements*: Connection to public sewers must meet the standards of the City of Portland's [Sewer and Drainage Facilities Design Manual](#).
3. *Source Control Requirements*: Source control requirements from the [Source Control Manual](#) (SCM), [Portland City Code \(PCC\) Title 17](#), and [BES Administrative Rules](#) that may be applicable to this project are listed below with the corresponding chapter, section, code, or rule. For specific questions on the following, please contact BES Source Control at 503-823-7122.
 - a. *Site Use and Activity-Based Source Control Requirements (SCM Chapter 6)*: BES recommends the applicant review the following SCM sections to understand the structural, treatment, and operational BMP requirements that may impact the project design.
 - 1) *Waste and Recycling Storage (SCM Section 6.1)*



PORTLAND BUREAU OF TRANSPORTATION

1900 SW Fourth Ave., Suite 5000 Portland, OR 97201 503-823-5185
Fax 503-823-7576 TTY 503-823-6868 www.portlandoregon.gov/transportation

Jo Ann Hardesty Commissioner Chris Warner Director

PBOT – Development Review Pre-Application Conference Response

Date: June 8, 2022

To: Sean O’Neill, SWS PDX
503-965-9307, sean@scotiawsh.com

From: Michael Pina, PBOT Development Review
503-823-4249, Michael.Pina@portlandoregon.gov

Case File: EA 22-144899

Location: 1311 SW GIBBS ST

R#: R712893

Proposal: A Pre-Application Conference to discuss changes to an approved land use review (LU #19-258445 PD, ENM, LDP) to increase the number of dwelling units from 27 to 43 by converting 16 of the two and three-bedroom units to 32 studio and one-bedroom units. Proposed changes are internal to the building. No façade or building footprint changes are proposed. Increasing the number of dwelling units in an approved planned development requires approval through a Type III Planned Development Review.

Portland Bureau of Transportation/Development Review (PBOT) staff has reviewed the pre-application conference materials to identify potential issues and requirements.

A. KEY ISSUES AND REQUIREMENTS

- The applicant has initiated a Public Works permit for required frontage improvements which has received 30% concept approval and bond paid. No further improvements necessary.

B. GENERAL COMMENTS

TSP Classifications: At this location, the City’s Transportation System Plan (TSP) classifies the abutting street(s) as follows:

Street Name	Traffic	Transit	Bicycle	Pedestrian	Freight	Emergency Response	Street Design
SW Gibbs/ SW Marquam Hill Rd	Neighborhood Collector	Local Service	City Bikeway	City Walkway	Local Service	Major	Community Corridor
SW 13 th /SW 14 th /SW Grover	Local Service	Local Service	Local Service	Local Service	Local Service	Minor	Local Service

Existing Improvements: At this location, according to the City’s GIS data, the site’s abutting Rights-of-Way (ROW) are improved as follows:

Street Name	ROW Width*	Roadway Width*/Condition	Pedestrian Corridor Width*/Configuration			
			Curb	Furnishing	Sidewalk	Frontage

SW Gibbs/SW Marquam Hill Rd	24-ft pavement (fog line to fog line)	None	60-170 ft	SW Gibbs/SW Marquam Hill Rd	24-ft pavement (fog line to fog line)	None
SW 13 th /SW 14 th /SW Grover	Un-improved	Un-improved	60-ft	SW 13 th /SW 14 th /SW Grover	Un-improved	Un-improved

NOTE: The applicant is advised the information contained herein is derived from City GIS and other databases typically used by city staff, as well as information from the applicant. It has not been confirmed via a survey. This evaluation is not implied to be more accurate than the sources that the information was obtained from. It is the applicant's responsibility to provide a current survey to document the location of the abutting rights-of-way to challenge any anticipated dedication amount that is being required.

C. REQUIRED ROW IMPROVEMENTS

Title 17.88.020 provides PBOT the authority to require the abutting frontage be brought up to City standards when development results in an Increase in Occupancy/Trips and/or is defined as a Significant Alteration (17.88.010). The proposal triggers both thresholds, thus requiring dedication and abutting frontage improvements.

The applicant has initiated Public Works permit 19-193047 WT | TH0903 for the required frontage improvements identified in the previous EA and PD. The review and has received 30% concept approval. No further improvements necessary. Performance Guarantee (bond) must be paid prior to issuance of the building permit.

D. SUBMITTAL REQUIREMENTS FOR LAND USE

This list identifies Portland Transportation submittal requirements. Please see the Conference Summary Memo for all of the materials you must submit for your application to be considered complete. All submittal requirements should be provided with the application.

1. Written narrative adequately addressing all land-use and transportation related approval criteria.
2. Plans showing required dedication(s) and Right-of-Way improvements.



Urban Forestry

Early Assistance Response

Date: June 14, 2022
From: Mariano Masolo
503-823-4560, Mariano.Masolo@portlandoregon.gov
Case File: EA 22-144899
Location: 1311 SW GIBBS ST
Proposal: A Pre-Application Conference to discuss changes to an approved land use review (LU #19-258445 PD, ENM, LDP) to increase the number of dwelling units from 27 to 43 by converting 16 of the two and three-bedroom units to 32 studio and one-bedroom units. Proposed changes are internal to the building. No façade or building footprint changes are proposed. Increasing the number of dwelling units in an approved planned development requires approval through a Type III Planned Development Review.

Portland Parks, Urban Forestry staff has reviewed the Early Assistance materials to identify potential issues and requirements in accordance with Title 11, Trees. This response identifies potential issues and/or impacts on existing street and heritage trees, and trees on city-owned or managed sites, if applicable. Trees on private property are subject to development standards from the Bureau of Development Services. See planner requirements for private property trees.

Please note that there may be other applicable tree requirements in Title 33 Planning & Zoning.

A. Response Summary

The development will be subject to Urban Forestry standards and requirements during the permit review process as detailed below.

All street tree permitting is deferred to Public Works Permit TH0903

B. Tree Plan (11.50.060)

A tree plan must be submitted with each phase of review including land use reviews, building permit applications, and public works permits. The plan must include the following information for street trees:

- a. The size and location of street trees adjacent to the subject property.
- b. Trees proposed to be preserved including tree protection specifications in accordance with 11.60.030.
- c. Tree(s) proposed for removal.
- d. Tree planting plan (tree species and location(s)).



C. Street Trees

1. Existing Street Conditions

- a. SW Gibbs St: The site has approximately 200 feet of street frontage. The right-of-way is unimproved pavement only. There are no overhead high voltage power lines. There are no street trees.
- b. SW 13th Ave: The site has approximately 150 feet of street frontage. The right-of-way is unimproved in undeveloped road. There are no overhead high voltage power lines. There are many street trees.

2. Street Tree Preservation (11.50.040)

If the applicant believes the tree must be removed to facilitate development, the applicant must provide adequate technical analysis demonstrating why the tree cannot be preserved while developing the site to City standards.

3. Street Tree Protection Specifications (11.60.030)

Tree protection is required in accordance with Title 11 Trees, Protection Methods (11.60.030). Tree protection shall follow either the Prescriptive or Performance path. Protection methods must be shown on the tree plan. If using the Performance path, the alternate tree protection plan must be prepared by an arborist who has visited the site

4. Street Tree Planting (11.50.060.C)

The applicant has not provided a conceptual street tree planting plan. One street tree must be planted or retained for each full increment of 25 linear feet (11.50.060.C.1). Street trees must be planted at a minimum 2 caliper inches. Trees will be required to be planted through the building permit and public works permit TH0903.





URBAN FORESTRY TREE REQUIREMENTS

Early Assistance and Land Use Review

Portland Parks & Recreation Urban Forestry staff review Early Assistance and Land Use Review materials to identify potential issues and requirements in accordance with Title 11, Trees and Title 33, Zoning Code. The purpose of these reviews is to identify potential issues and/or impacts on existing street trees, heritage trees, and trees on City-owned or managed sites (if applicable), as well as to provide adequate areas for future street tree planting on existing and proposed public streets. Trees on private property are subject to development standards from the Bureau of Development Services. See planning requirements for private property trees or call the Zoning Hotline at 503-823-7526.

Tree Plan Submittal Requirements (11.50.070)

A tree plan must be submitted with each phase of review including land use reviews, building permit applications, and public works permits. The tree plan information may be combined with other relevant plan sheets. The tree plan submittal shall include the following information:

- existing improvements;
- proposed alterations;
- existing street trees ≥ 3 " DBH including size and location;
- existing on-site trees ≥ 6 " DBH within 15' of the limits of disturbance;
- trees proposed for removal;
- tree planting proposal, including tree size, species and location; and
- trees to be retained and proposed tree protection measures meeting the specification in Chapter 11.60.

Any changes to an approved Tree Plan, including amending tree species must be approved by the City Forester. Please note that the City Forester may not approve revised tree planting plans based on the lack of species availability. To facilitate species availability, it is recommended that tree procurement occur approximately 6 months prior to installation.

Tree Mitigation (11.50.040.C.2)

Healthy street trees ≥ 6 " DBH that are approved for removal shall be replanted with two trees in addition to trees required to be planted to meet Street Tree Planting Standards, below. When street improvements are to partially or fully unimproved streets, healthy street trees ≥ 12 " DBH approved for removal shall be replanted with two trees, with trees planted to meet Street Tree Planting Standards credited towards meeting this requirement. Tree replacement for trees removed shall occur in the street planter strip, on site, or in the same watershed either by planting or by paying a fee in lieu of planting in accordance with table 60-1, below.

On City-owned or managed sites, healthy, non-nuisance trees ≥ 6 " DBH that are approved for removal shall be replanted per the Administrative Rule for tree replacement standards, below:



Tree Replacement for Development on City Owned or Managed Sites

Size of tree to be removed (inches in diameter)	Number of trees to be planted
6 and up to 12	Up to 2
More than 12 and up to 20	Up to 3
More than 20 and up to 25	Up to 5
More than 25	Up to 6

Street Tree Planting Standards (11.50.050)

One street tree shall be planted or retained for each full increment of 25 linear feet per side of street frontage. Planting is exempt when existing above or below grade utilities prevent planting of street trees, or if the existing design of the street will not accommodate street tree planting because the planting strip is less than 3 feet wide, there is not a planting strip, or there is insufficient space to add tree wells. Trees planted to meet street tree planting standards are credited toward mitigation requirements when street improvements are to partially or fully unimproved streets. When the required number of trees cannot be planted, a fee in lieu of planting will be required, in accordance with Table 60-1, below.

Table 60-1 Broadleaf Tree Size Requirements

Development Type	Tree Size	
	On Site	Street
One and Two Family Residential	1.5”	1.5”
Multi Dwelling Residential	1.5”	2”
All others	1.5”	2.5”

Tree Planting Specifications

If there are fewer than 8 required trees, they may all be the same species. If there are between 8 and 24 required trees, no more than 40 percent can be of one species. If there are more than 24 required trees, no more than 24 percent can be of one species. Street tree species shall conform to the appropriate “City of Portland Approved Street Tree Planting List.” The City Forester may approve or require an alternate or unlisted species.

All required street trees shall be planted in-ground following Standard Drawing Number P-581 “Typical Street Tree installation,” except when in raised planters that are used to meet Bureau of Environmental Services storm water management requirements. Please include the Standard



Drawing Number P-581 as part of the Public Works permit application. Plant materials shall be installed to current nursery industry standards and proper arboricultural practices [American National Standards Institute, *ANSI A300 Part 6: Tree, Shrub, and Other Woody Plant Maintenance-Standard Practices (Planting and Transplanting)* 2012, Tree Care Industry Association, Inc. Londonderry, NH]. Plant materials shall be properly supported to ensure survival.

All trees required or approved to be planted by Title 11 shall be planted or payment in lieu of planting made prior to the expiration of the permit or City's final acceptance of the project, as applicable. However, it is encouraged that planting occur during the wet months or as per City Forester recommendations. Street tree planting may be deferred between May 1 and September 30 upon filing a performance guarantee as provided in Section 11.10.060 or other assurance deemed acceptable by the City Forester or BDS Director as applicable.

Tree Protection Specifications (11.60.030)

Trees to be retained shall be protected in accordance with Title 11 Trees, Protection Specifications (11.60.030.C). Tree protection shall be shown on the tree plan and include the distance from the trunk of the tree to the fence. A standard root protection zone is established as follows; a minimum of 1 foot radius (measured horizontally away from the face of the tree trunk) for each inch of tree diameter. Protection fencing shall be a minimum 6-foot high metal chain link construction fence, secured with 8-foot metal posts established at the edge of the root protection zone and permissible encroachment area.



ZONING SUMMARY:

SPLIT ZONING: R7 / RM2
 PROPOSED USE: 5-STORY, 43-UNIT APARTMENTS OVER STRUCTURED PARKING WITH 24 STALLS + 4 ADDITIONAL AT GRADE STALLS = TTL. 28 REFER TO NARRATIVE

LAND USE:
 LOT AREA R7 = 20,000 SF (14,099 SF W/ ENVIRONMENTAL RESOURCE TAKEN OUT)
 LOT AREA RM2 = 20,000 SF (9,400 SF W/ ENVIRONMENTAL RESOURCE TAKEN OUT)

FAR PROPOSED IN RM2:	CONTRIBUTE	NON-CONTRIBUTE (PARKING)
FLR. 1 / GARAGE	-	4,863
FLOOR 2	5,280	-
FLOOR 3	5,487	-
FLOOR 4	5,487	-
FLOOR 5	5,487	-
TTL	27,228	4,863
TTL FAR PROPOSED IN RM2	27,228 SF (1.4 : 1)	

PLANNING AND ZONING DEVELOPMENT STANDARDS

	REQUIRED / ALLOWABLE (PER P.D.)	PROPOSED
HEIGHT (SEE A1.02)	45-FT W/ IN 25-FT FRONT P.L. 49'-0" MAX REST OF SITE	45-FT 45-FT
FRONT SETBACK	0 FT (CONSERVATION ZONE)	1'-1" (DECK) / 3'-0" (BLDG.)
SIDE SETBACK	0 FT (CONSERVATION ZONE)	5'-0" (EAST ELEVATION)
BICYCLE PARKING (SEE 1/A1.62)	48 LONG TERM 3 SHORT TERM	48 3

OUTDOOR AREA:
 SEE FLOOR PLANS, A2.xx SERIES, FOR INDIVIDUAL DECKS AND PATIOS.

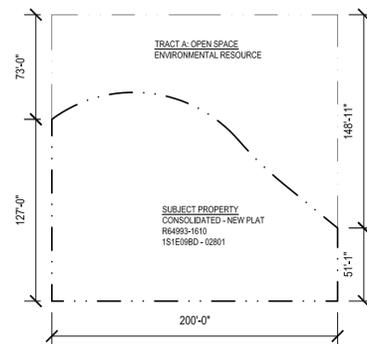
INDIV. OUTDOOR AREA PER UNIT REQUIRED	PROVIDED
48 SF	62 SF
MIN. 6-FT EA. DIRECT.	6'-5" MIN. DIMENSION
COMMON OUTDOOR AREA 48 SF (x16 UNITS) =	768 SF REQ. (MIN. 500 SF)
PROPOSED:	690 SF AT WEST 2ND FLOOR DECK 280 SF AT NE AT GRADE AREA
TTL	970 SF

INCLUSIONARY HOUSING CALCULATIONS
 UNITS PROVIDED ON-SITE TO MEET BOTH AFFORDABLE HOUSING STANDARDS FOR DEVELOPMENTS OVER 20 UNITS. CALCULATIONS ARE FOR 10% TOTAL AT 60% MFL.

UNITS	REQ. AFFORDABLE UNITS (10%)
43	4.3 = 4 UNITS

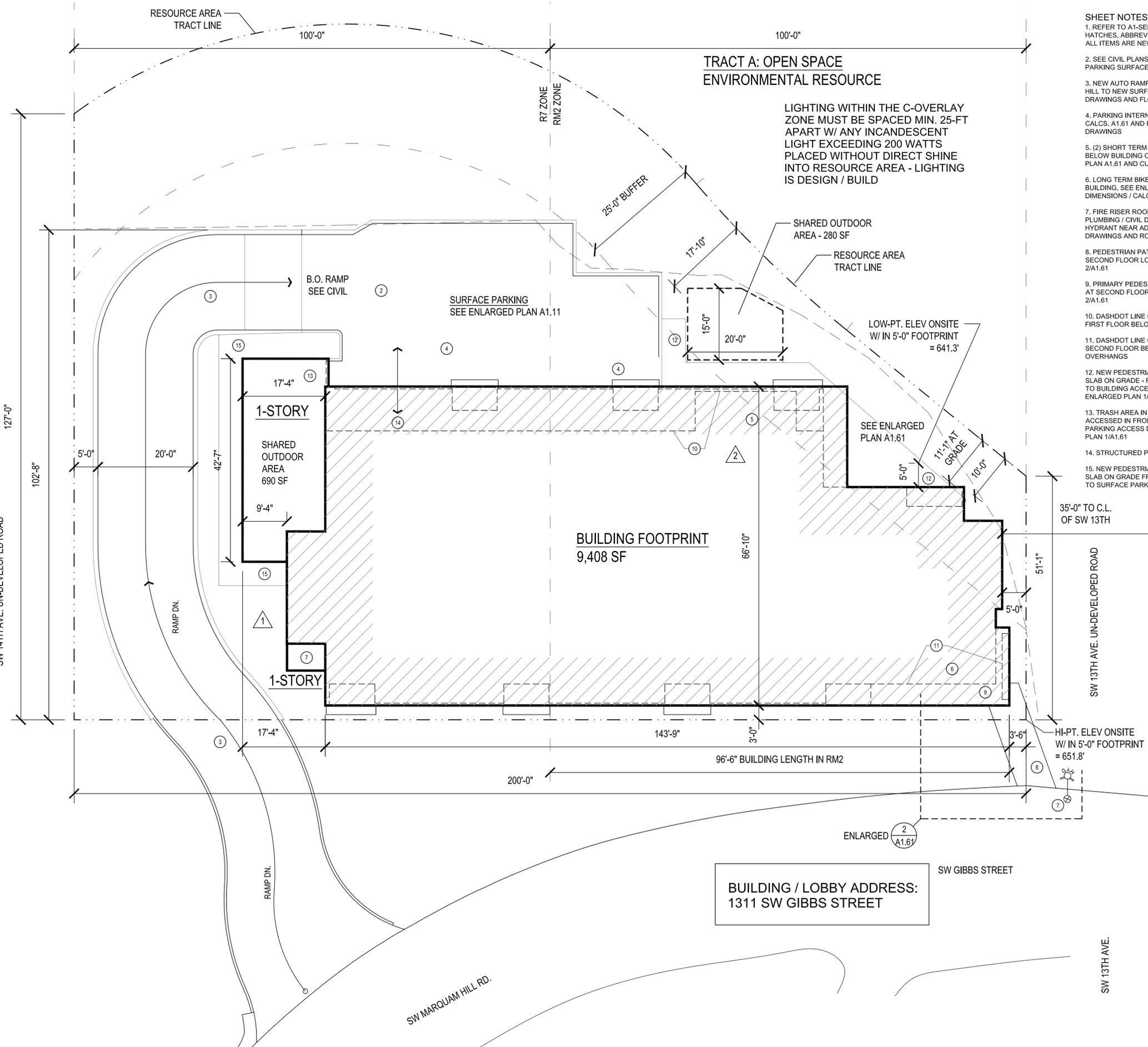
SEE A1.01 FOR FIRE DEPT. RELATED ITEMS

SEE A1.11 AND A1.21 FOR LIFE SAFETY SUMMARY



2. PROPERTY LINE PLAN

SCALE: 1/64" = 1'-0" ON 22x34 1/128" = 1'-0" ON 11x17



1. SITE PLAN

SCALE: 3/32" = 1'-0" ON 22x34 3/64" = 1'-0" ON 11x17

BUILDING / LOBBY ADDRESS:
 1311 SW GIBBS STREET

- SHEET NOTES:**
- REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 - SEE CIVIL PLANS FOR STORMWATER AND PARKING SURFACE DRAINAGE
 - NEW AUTO RAMP DOWN OFF SW MARQUAM HILL TO NEW SURFACE PARKING, SEE CIVIL DRAWINGS AND FLOOR PLANS
 - PARKING INTERNAL LANDSCAPE AREA, SEE CALCS. A1.61 AND REFER TO LANDSCAPE DRAWINGS
 - (2) SHORT TERM BIKE PARKING SPACES BELOW BUILDING OVERHANG, SEE ENLARGED PLAN A1.61 AND CUT SHEET A1.62
 - LONG TERM BIKE PARKING INSIDE BUILDING, SEE ENLARGED PLAN A1.62 FOR DIMENSIONS / CALCS. AND CUT SHEETS
 - FIRE RISER ROOM 1ST FLOOR - SEE PLUMBING / CIVIL DRAWINGS - NEW FIRE HYDRANT NEAR ADA ACCESS, SEE CIVIL DRAWINGS AND ROW PERMIT
 - PEDESTRIAN PATH FROM SW GIBBS TO SECOND FLOOR LOBBY, SEE ENLARGED PLAN 2/A1.61
 - PRIMARY PEDESTRIAN ENTRY TO BUILDING AT SECOND FLOOR, SEE ENLARGED PLAN 2/A1.61
 - DASHDOT LINE OF BUILDING OUTLINE AT FIRST FLOOR BELOW BUILDING OVERHANGS
 - DASHDOT LINE OF BUILDING OUTLINE AT SECOND FLOOR BELOW BUILDING OVERHANGS
 - NEW PEDESTRIAN ACCESS PATHS - CONC. SLAB ON GRADE - FROM SURFACE PARKING TO BUILDING ACCESS AT FIRST FLOOR, SEE ENLARGED PLAN 1/A1.61 AND CIVIL
 - TRASH AREA IN FIRST FLOOR ENCLOSURE, ACCESSED IN FRONT OF STRUCTURED PARKING ACCESS DOOR, SEE ENLARGED PLAN 1/A1.61
 - STRUCTURED PARKING GARAGE ACCESS
 - NEW PEDESTRIAN ACCESS PATH - CONC. SLAB ON GRADE FROM STAIR WELL EGRESS TO SURFACE PARKING / BOTTOM OF RAMP

SITE PLAN: COVERAGE + ZONING SUMMARY

GIBBS DEVELOPMENT
 1325 SW GIBBS STREET
 PORTLAND, OR 97239

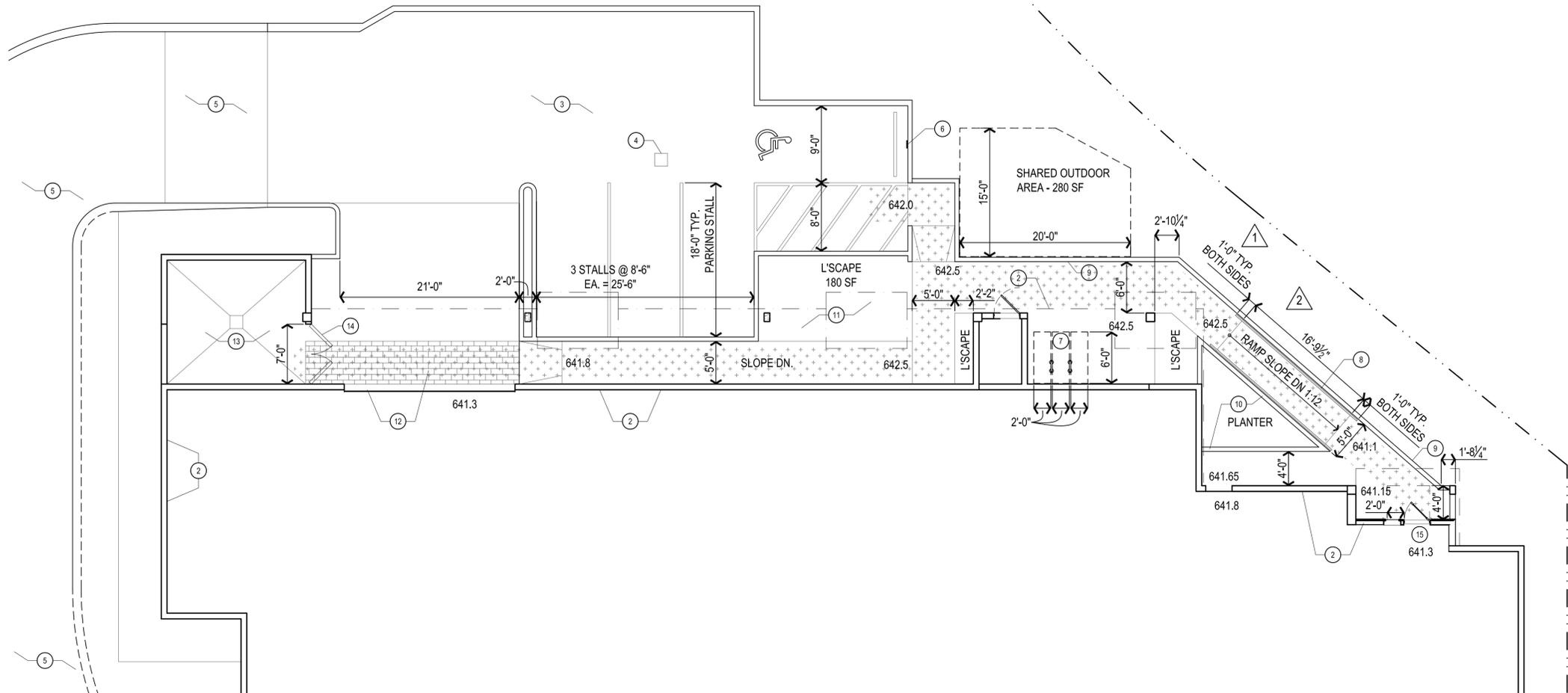
LAND USE SET
 09/01/2022

A1.00

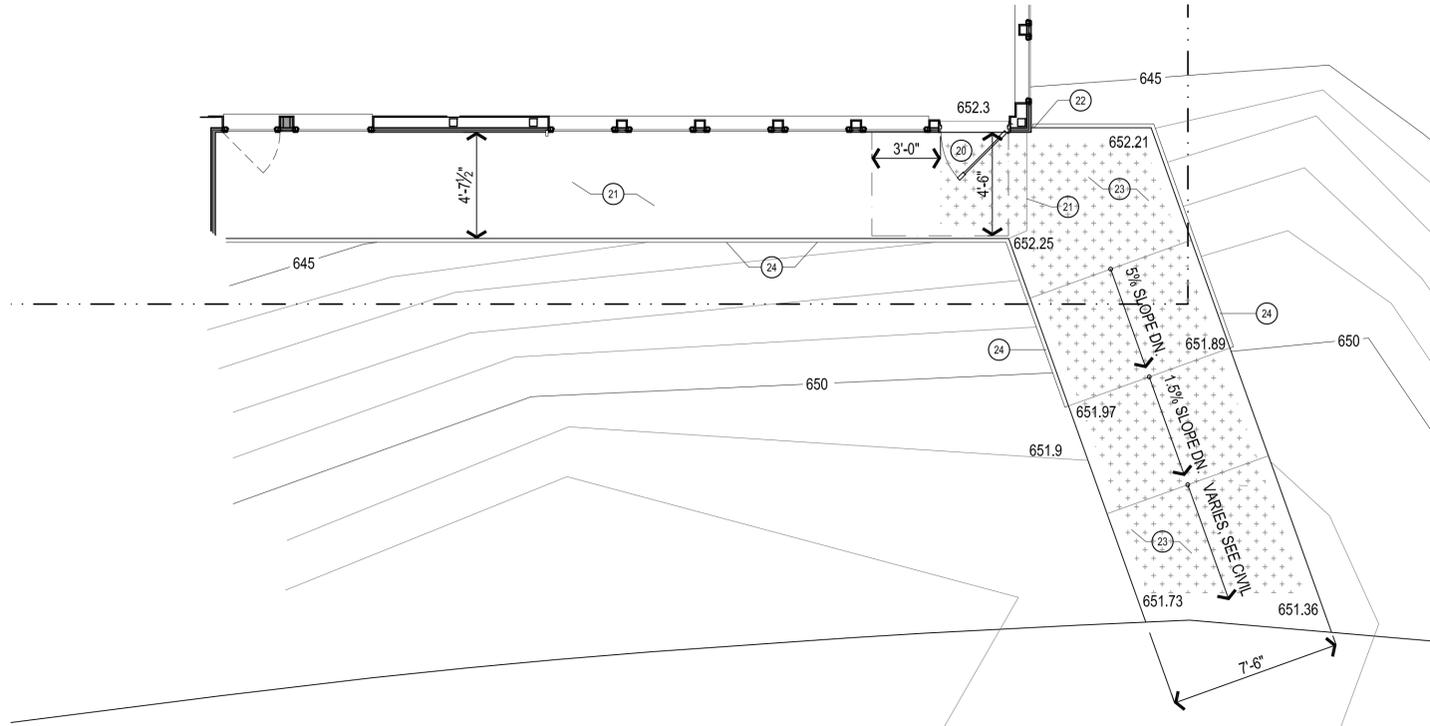
LU 22-187796 APD AP C.1

STEELHEAD ARCHITECTURE
 5210 SE 26TH AVE. PORTLAND OREGON 97202





1. ENLARGED PARKING LOT + PED ACCESS TO LOWER LOBBY
 SCALE: 1/4" = 1'-0" ON 22x34 1/8" = 1'-0" ON 11x17



2. ENLARGED PED ACCESS TO UPPER LOBBY
 SCALE: 3/32" = 1'-0" ON 22x34 3/64" = 1'-0" ON 11x17

- SHEET NOTES:**
- REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 - OUTLINE OF FIRST FLOOR BUILDING, SEE A2.12 PLAN
 - SURFACE PARKING LOT, SEE CIVIL DRAWINGS FOR SLOPES
 - PARKING LOT AREA DRAIN TO STORM WATER CONTAINMENT / TREATMENT, SEE CIVIL DRAWINGS
 - AUTO RAMP UP, SEE CIVIL DRAWINGS AND STRUCT. DRAWINGS
 - ACCESSIBLE PARKING SIGNAGE, SEE 2/A1.62
 - SHORT TERM BICYCLE PARKING, SEE 5/A1.62 - TYP. OF (3)
 - RAMP HANDRAIL, TYP. EA SIDE - SEE 3/A1.62
 - CONC. CURB, MIN. 6" ABOVE ADJACENT GRADE
 - TRIANGULAR CONC. PLANTER, CURB 6" ABOVE HI-SIDE OF RAMP - FLAT TOP FOR APPROX. 26" HEIGHT ABOVE LOW SIDE OF WALKWAY
 - SURFACE PARKING INTERNAL LANDSCAPE AREA, SEE LANDSCAPE DRAWINGS
 CALCS AS FOLLOWS:
 REQ. = 45 SF / STALL (x4) = 180 SF
 PROVIDED = 180 SF
 - TEXTURED CONCRETE PEDESTRIAN PATH TO CONTRAST ADJACENT PAVING - CROSSING AT STRUCTURED PARKING GARAGE ENTRY
 - ENCLOSED / COVERED TRASH AREA W/ HYDRONICALLY ISOLATED SLAB SLOPED MIN. 1/8" PER FT. TO DRAIN, SEE PLUMBING DRAWINGS
 - DOUBLE 42" WIDE GATES, SEE ELEVATIONS
 - LOWER LEVEL LOBBY ENTRY W/ LATCH SIDE APPROACH DOOR CLEARANCE, SEE 7/A1.11
 - 16 - 19. NOT USED
 - UPPER LEVEL LOBBY ENTRY W/ FORWARD APPROACH DOOR CLEARANCE, SEE 6/A1.11
 - LINE OF PT SLAB BELOW - EXPOSED PT SLAB SLOPED 1/4" PER FT. AWAY FROM BUILDING
 - ALIGN CONC. PEDESTRIAN PATH W/ CORNER OF CONC. WALLS
 - CONC. SLAB ON GRADE W/ TURNED DOWN EDGES, S.S.D. - SEE SPOT ELEVATIONS AND TOPO LINES PLUS CIVIL DRAWING C11 FOR MULTI-SLOPE AND CONNECTION POINT AND ELEVATION OF PATH TO ROAD IN PUBLIC RIGHT OF WAY
 - STEEL GUARDRAIL AT CONC. EDGE, SEE 3/A9.1 - TERMINATES AT END OF 5% SLOPE - HANDRAILS NOT ALLOWED IN ROW PER PBOT

 DENOTES PEDESTRIAN ACCESS PATH

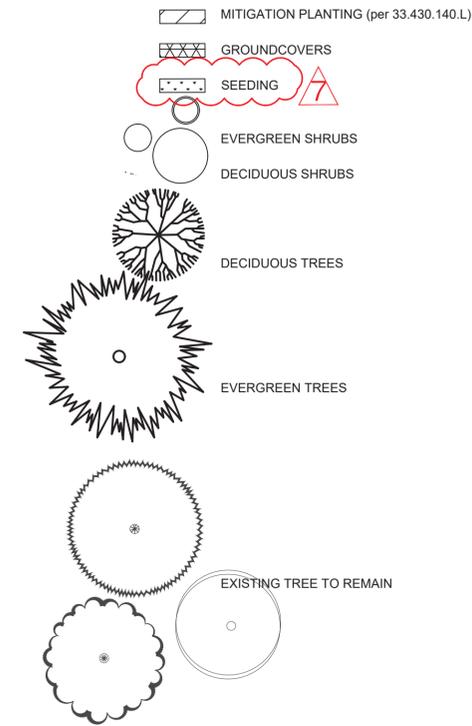


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LAND USE SET
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PLANTING SITE PLAN

LEGEND

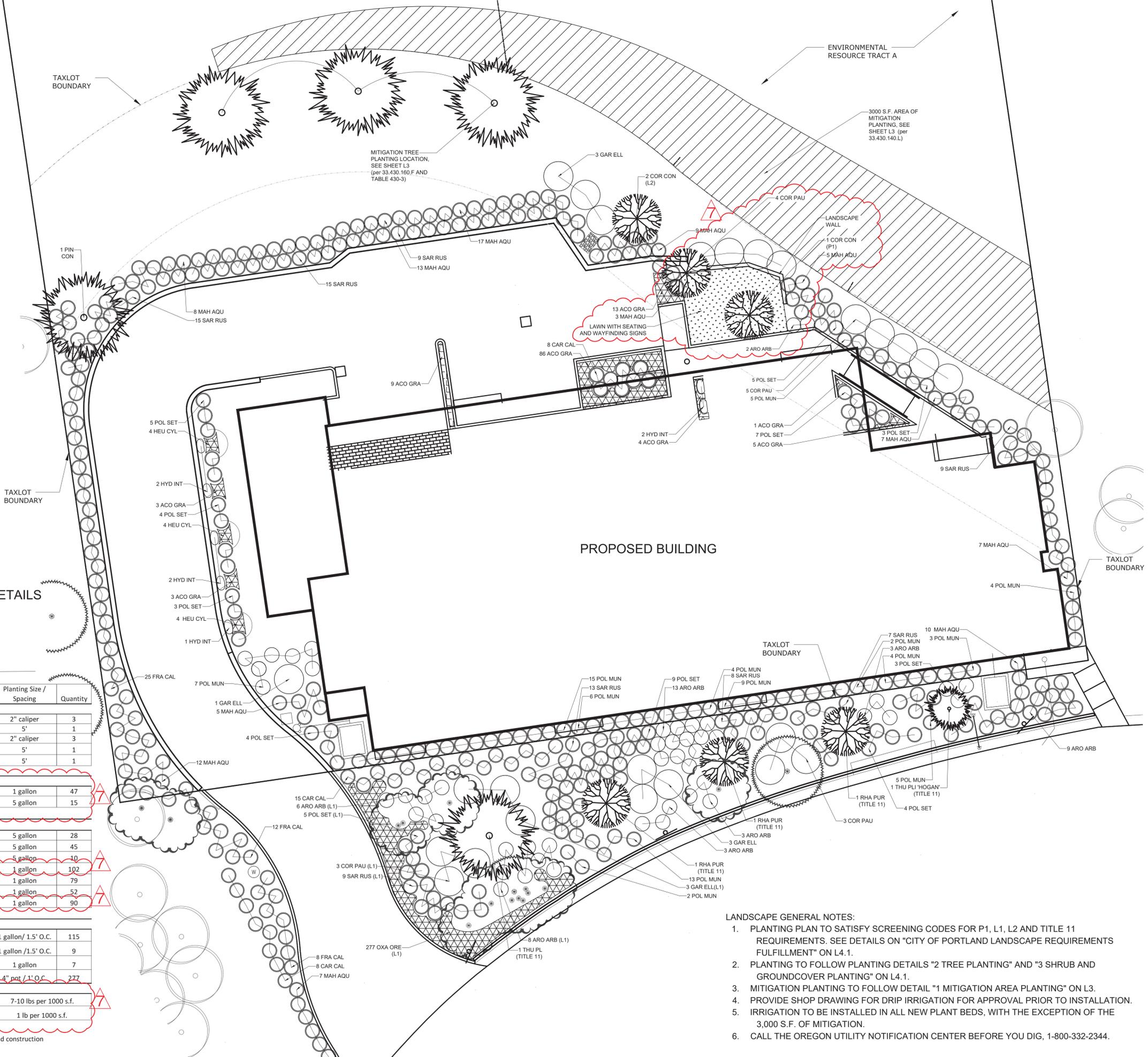


SEE TH0903 FOR APPROVED RIGHT OF WAY DETAILS AND STREET TREE ACTIVITY

PLANT LIST

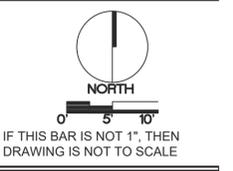
Symbol	Botanical Name	Common Name	Planting Size / Spacing	Quantity
Trees				
COR CON	<i>Cornus controversa</i>	Giant Dogwood	2" caliper	3
PIN CON	<i>Pinus contorta var. contorta</i>	Shore Pine	5"	1
RHA PUR	<i>Rhamnus purshiana</i>	Cascara	2" caliper	3
THU PLI	<i>Thuja plicata</i>	Western Redcedar	5"	1
'HOGAN'	<i>Thuja plicata 'Hogan'</i>	Hogan Western Redcedar	5"	1
Deciduous Shrubs				
ARO ARB	<i>Aronia arbutifolia 'Brilliantissima'</i>	Brilliant Red Chokeberry	1 gallon	47
COR PAU	<i>Corylopsis pauciflora</i>	Buttercup Winter-hazel	5 gallon	15
Evergreen Shrubs				
CAR CAL	<i>Carpenteria californica 'Elizabeth'</i>	Bush Anemone	5 gallon	28
FRA CAL	<i>Frangula californica 'Eve Case'</i>	Eve Case Coffeeberry	5 gallon	45
GAR EL	<i>Garrya elliptica 'James Roof'</i>	Wavy Leaf Silk Tassel	5 gallon	10
MAH AQU	<i>Mahonia aquifolium</i>	Tall Oregongrape	1 gallon	102
POL MUN	<i>Polystichum munitum</i>	Swordfern	1 gallon	79
POL SET	<i>Polystichum setiferum var. diversilobum</i>	Soft Shield Fern	1 gallon	52
SAR RUS	<i>Sarcococca ruscifolia</i>	Sweet Box	1 gallon	90
Groundcovers and Vines				
ACO GRA	<i>Acorus gramineus 'Variegatus'</i>	Variegated Sweet Flag	1 gallon/ 1.5' O.C.	115
HEU CYL	<i>Heuchera cylindrica</i>	Green Alum Root	1 gallon /1.5' O.C.	9
HYD INT	<i>Hydrangea integrifolia</i>	Evergreen Climbing Hydrangea	1 gallon	7
OXA ORE	<i>Oxalis oregana</i>	Redwood Sorrel	4" pot / 1' O.C.	277
Lawn Seeding				
	Pro Time Lawn Seed: PT 305 Shade/Sun Mix		7-10 lbs per 1000 s.f.	
	Pro Time Lawn Seed: PT 313 Super Shade Supplement		1 lb per 1000 s.f.	

Required plantings per LU 22-250445 PD ENM LDP
 Quantities are for permitting, contractor should verify all quantities independently for ordering and construction
 Groundcovers are triangularly spaced and centered as specified



LANDSCAPE GENERAL NOTES:

1. PLANTING PLAN TO SATISFY SCREENING CODES FOR P1, L1, L2 AND TITLE 11 REQUIREMENTS. SEE DETAILS ON "CITY OF PORTLAND LANDSCAPE REQUIREMENTS FULFILLMENT" ON L4.1.
2. PLANTING TO FOLLOW PLANTING DETAILS "2 TREE PLANTING" AND "3 SHRUB AND GROUNDCOVER PLANTING" ON L4.1.
3. MITIGATION PLANTING TO FOLLOW DETAIL "1 MITIGATION AREA PLANTING" ON L3.
4. PROVIDE SHOP DRAWING FOR DRIP IRRIGATION FOR APPROVAL PRIOR TO INSTALLATION.
5. IRRIGATION TO BE INSTALLED IN ALL NEW PLANT BEDS, WITH THE EXCEPTION OF THE 3,000 S.F. OF MITIGATION.
6. CALL THE OREGON UTILITY NOTIFICATION CENTER BEFORE YOU DIG, 1-800-332-2344.



CONSULTANT:
NNA
 Landscape Architecture
 1811 S RIVER DRIVE SUITE 300
 PORTLAND, OR 97201
 503.239.0600

SCOTIA WESTERN STATES HOUSING
 PORTLAND, OR 97239
 PRELIMINARY LANDSCAPE PLAN

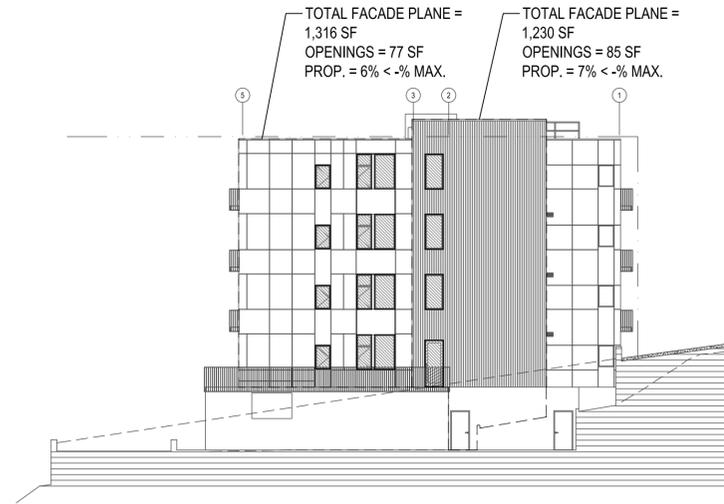
REVISIONS:	DELTA	DATE	TITLE
	Δ	02/05/2021	Rev 1
	Δ	09/22/2022	Rev 7

SCALE: 1" = 10'
 DRAWN BY: JJ
 CHECKED BY: DG

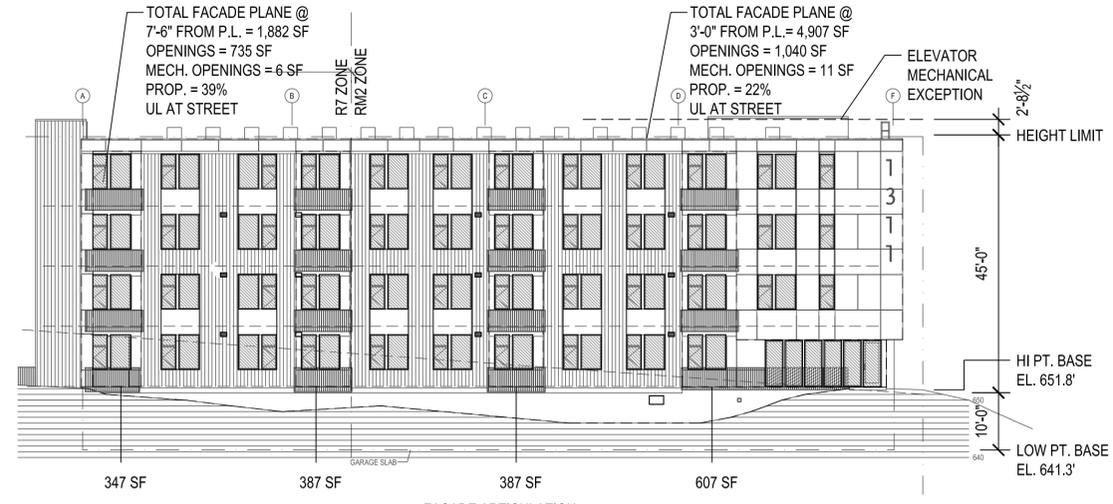
SHEET
L-4

DATE: 09/23/22
 JOB NO. 1914
 LU 22-187796 APD AP
 Exhibit C.3

SHEET NOTES:
 1. REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 2.

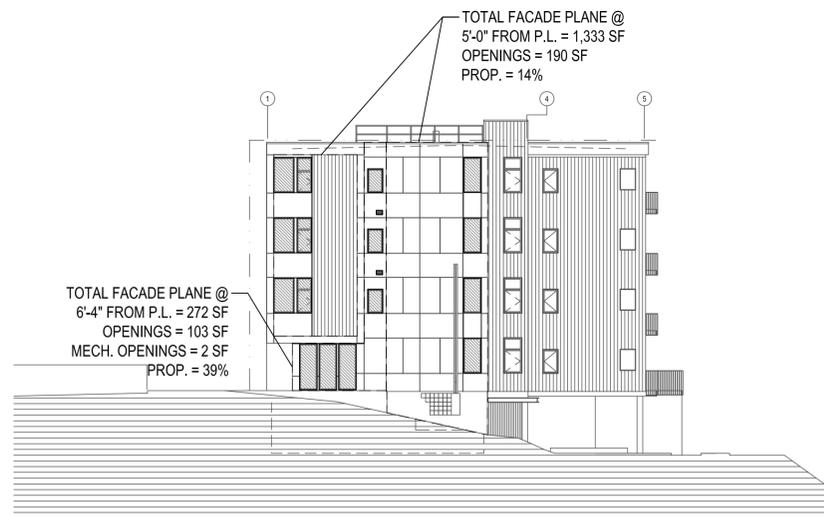


3. HEIGHT + EAST FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



TOTAL FACADE BOTH ZONES = 6,734 SF
 FACADE > 2 FT. OF PRIMARY PLANE = 1,728 SF (26%)
 TOTAL FACADE WITHIN RM2 = 4,556 SF
 FACADE > 2 FT. OF PRIMARY PLANE = 994 SF (22%)

1. HEIGHT + SOUTH FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



4. HEIGHT + WEST FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



2. HEIGHT + NORTH FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



GIBBS DEVELOPMENT



STEELHEAD
ARCHITECTURE

LAND USE SET

SEPT. 1, 2022

ZONING SUMMARY:

SPLIT ZONING: R7 / RM2
 PROPOSED USE: 5-STORY, 43-UNIT APARTMENTS OVER STRUCTURED PARKING WITH 24 STALLS + 4 ADDITIONAL AT GRADE STALLS = TTL. 28 REFER TO NARRATIVE
 LAND USE: REFER TO NARRATIVE

LOT AREA R7 = 20,000 SF (14,099 SF W/ ENVIRONMENTAL RESOURCE TAKEN OUT)
 LOT AREA RM2 = 20,000 SF (9,400 SF W/ ENVIRONMENTAL RESOURCE TAKEN OUT)

FAR PROPOSED IN RM2:	CONTRIBUTE	NON-CONTRIBUTE (PARKING)
FLR. 1 / GARAGE	-	4,863
FLOOR 2	5,280	-
FLOOR 3	5,487	-
FLOOR 4	5,487	-
FLOOR 5	5,487	-
TTL	27,228	4,863
TTL FAR PROPOSED IN RM2	27,228 SF (1.4 : 1)	

PLANNING AND ZONING DEVELOPMENT STANDARDS

	REQUIRED / ALLOWABLE (PER P.D.)	PROPOSED
HEIGHT (SEE A1.02)	45-FT W/ IN 25-FT FRONT P.L. 49-0" MAX REST OF SITE	45-FT 45-FT
FRONT SETBACK	0 FT (CONSERVATION ZONE)	1'-1" (DECK) / 3'-0" (BLDG.)
SIDE SETBACK	0 FT (CONSERVATION ZONE)	5'-0" (EAST ELEVATION)
BICYCLE PARKING (SEE 1/A1.62)	48 LONG TERM 3 SHORT TERM	48 3

OUTDOOR AREA:

SEE FLOOR PLANS, A2.xx SERIES, FOR INDIVIDUAL DECKS AND PATIOS.

INDIV. OUTDOOR AREA PER UNIT REQUIRED	PROVIDED
48 SF	62 SF
MIN. 6-FT EA. DIRECT.	6'-5" MIN. DIMENSION

COMMON OUTDOOR AREA	PROVIDED
48 SF (x16 UNITS) =	768 SF REQ. (MIN. 500 SF)

PROPOSED:	PROVIDED:
690 SF AT WEST 2ND FLOOR DECK	280 SF AT NE AT GRADE AREA
TTL	970 SF

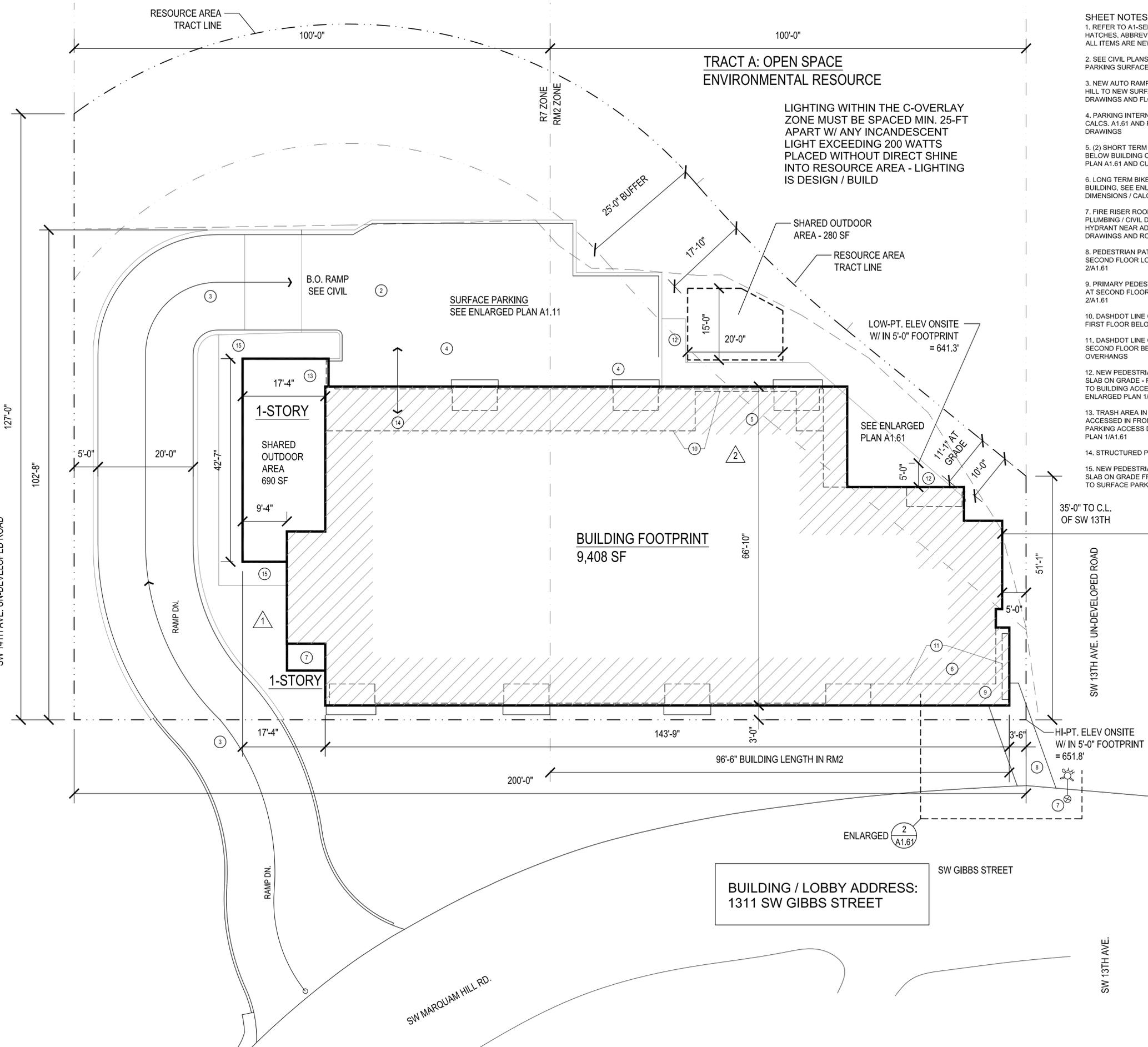
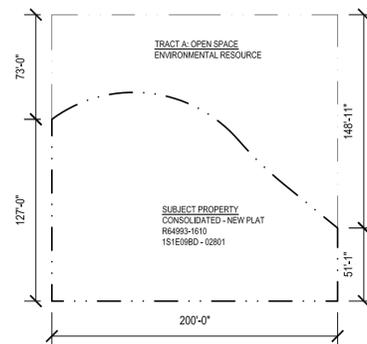
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UNITS PROVIDED ON-SITE TO MEET BOTH AFFORDABLE HOUSING STANDARDS FOR DEVELOPMENTS OVER 20 UNITS. CALCULATIONS ARE FOR 10% TOTAL AT 60% MFL.

UNITS	REQ. AFFORDABLE UNITS (10%)
43	4.3 = 4 UNITS

SEE A1.01 FOR FIRE DEPT. RELATED ITEMS

SEE A1.11 AND A1.21 FOR LIFE SAFETY SUMMARY

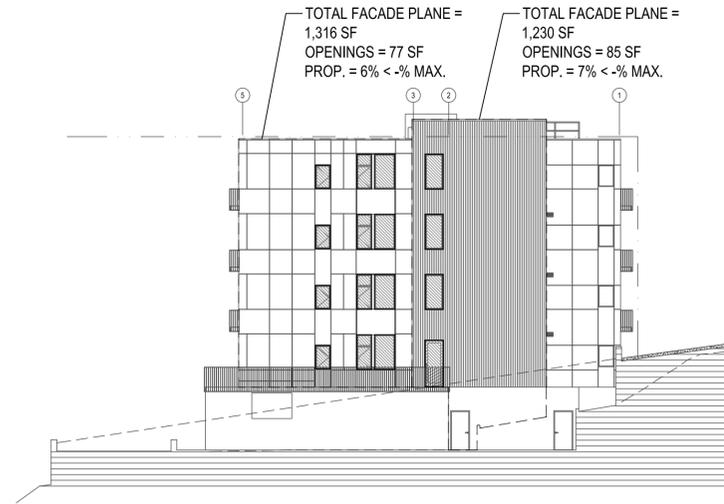


SHEET NOTES:

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- NEW AUTO RAMP DOWN OFF SW MARQUAM HILL TO NEW SURFACE PARKING, SEE CIVIL DRAWINGS AND FLOOR PLANS
- PARKING INTERNAL LANDSCAPE AREA, SEE CALCS. A1.61 AND REFER TO LANDSCAPE DRAWINGS
- (2) SHORT TERM BIKE PARKING SPACES BELOW BUILDING OVERHANG, SEE ENLARGED PLAN A1.61 AND CUT SHEET A1.62
- LONG TERM BIKE PARKING INSIDE BUILDING, SEE ENLARGED PLAN A1.62 FOR DIMENSIONS / CALCS. AND CUT SHEETS
- FIRE RISER ROOM 1ST FLOOR - SEE PLUMBING / CIVIL DRAWINGS - NEW FIRE HYDRANT NEAR ADA ACCESS, SEE CIVIL DRAWINGS AND ROW PERMIT
- PEDESTRIAN PATH FROM SW GIBBS TO SECOND FLOOR LOBBY, SEE ENLARGED PLAN 2/A1.61
- PRIMARY PEDESTRIAN ENTRY TO BUILDING AT SECOND FLOOR, SEE ENLARGED PLAN 2/A1.61
- DASHDOT LINE OF BUILDING OUTLINE AT FIRST FLOOR BELOW BUILDING OVERHANGS
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- TRASH AREA IN FIRST FLOOR ENCLOSURE, ACCESSED IN FRONT OF STRUCTURED PARKING ACCESS DOOR, SEE ENLARGED PLAN 1/A1.61
- STRUCTURED PARKING GARAGE ACCESS
- NEW PEDESTRIAN ACCESS PATH - CONC. SLAB ON GRADE FROM STAIR WELL EGRESS TO SURFACE PARKING / BOTTOM OF RAMP



SHEET NOTES:
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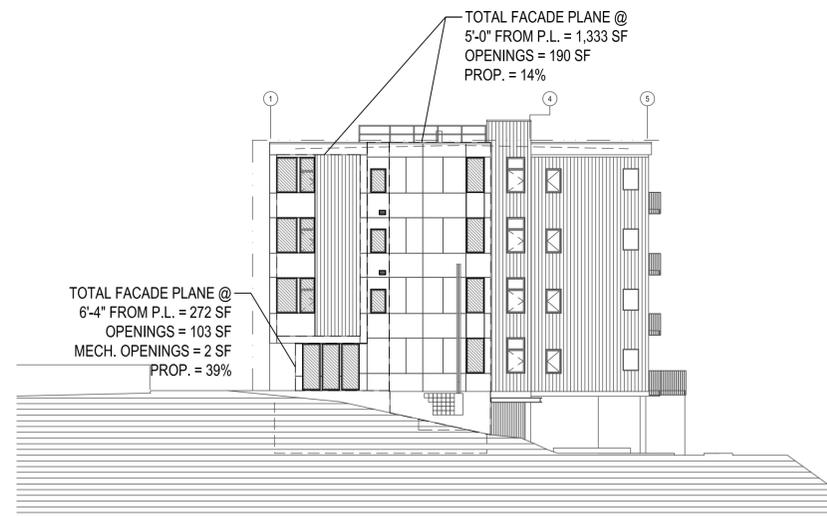


3. HEIGHT + EAST FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



TOTAL FACADE BOTH ZONES = 6,734 SF
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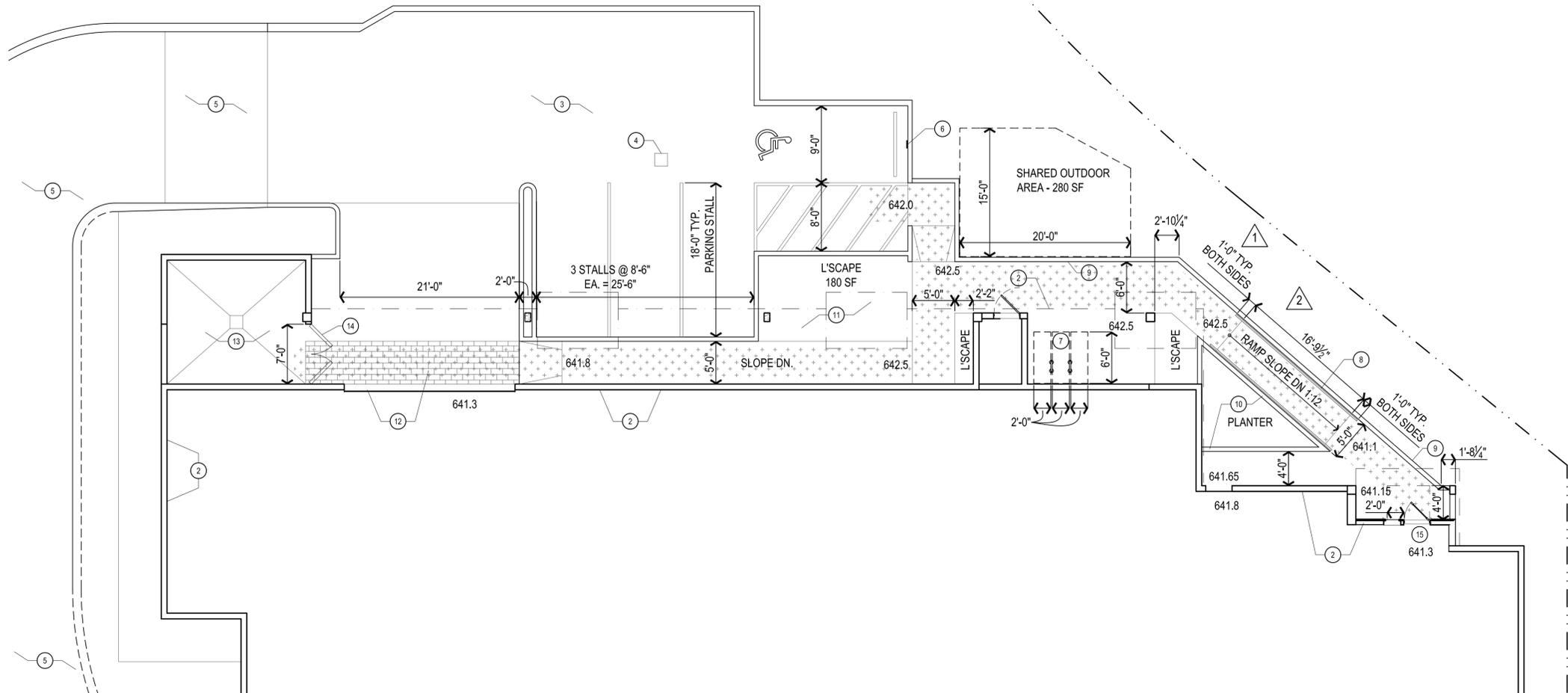


4. HEIGHT + WEST FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17

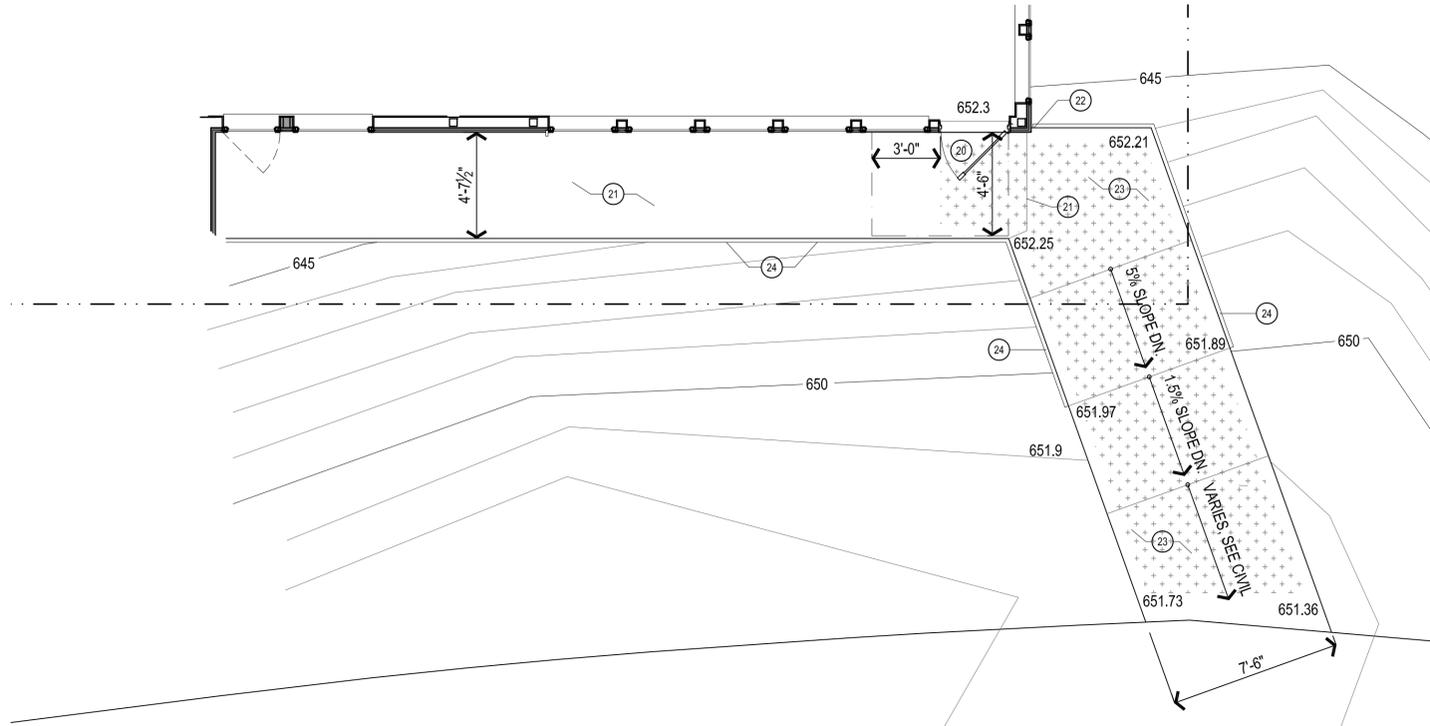


2. HEIGHT + NORTH FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17





1. ENLARGED PARKING LOT + PED ACCESS TO LOWER LOBBY
 SCALE: 1/4" = 1'-0" ON 22x34 1/8" = 1'-0" ON 11x17



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 - SURFACE PARKING INTERNAL LANDSCAPE AREA, SEE LANDSCAPE DRAWINGS
 CALCS AS FOLLOWS:
 REQ. = 45 SF / STALL (x4) = 180 SF PROVIDED = 180 SF
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 - LOWER LEVEL LOBBY ENTRY W/ LATCH SIDE APPROACH DOOR CLEARANCE, SEE 7/A1.11
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 - ALIGN CONC. PEDESTRIAN PATH W/ CORNER OF CONC. WALLS
 - CONC. SLAB ON GRADE W/ TURNED DOWN EDGES, S.S.D. - SEE SPOT ELEVATIONS AND TOPO LINES PLUS CIVIL DRAWING C11 FOR MULTI-SLOPE AND CONNECTION POINT AND ELEVATION OF PATH TO ROAD IN PUBLIC RIGHT OF WAY
 - STEEL GUARDRAIL AT CONC. EDGE, SEE 3/A9.1 - TERMINATES AT END OF 5% SLOPE - HANDRAILS NOT ALLOWED IN ROW PER PBOT

 DENOTES PEDESTRIAN ACCESS PATH



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LAND USE SET
 09/01/2022

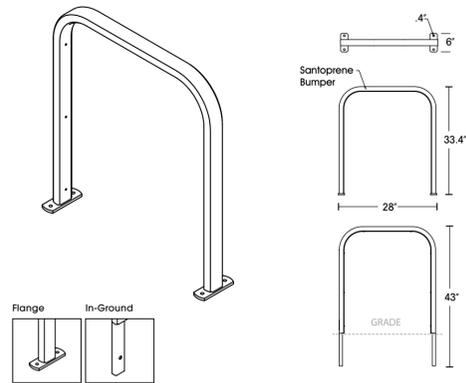
🚲 x 2

THE BURNSIDE

The refined, high-security Burnside, with its no-scratch protective bumper, is a superior means to show your respect for cyclists by protecting their investment.

CONSTRUCTION/MATERIAL
Outer: 2" x 1" Stainless Steel Rectangular Tubing
Inner Bar: .25" x 2" Stainless Steel Flat Bar
Bumper: Santoprene TPV
Flanges: 2" x 6" Stainless Steel

DIMENSIONS
- 28" Length
- 6" Width
- 33.4" Height



RECOMMENDED LAYOUT

6. LONG AND SHORT TERM FLOOR MTD. BIKE RACK

🚲 x 3, 5, 7

HAWTHORNE LINE WALL FRAME

Great for wall-mounting, U-Lock Compatible, the customizable Hawthorne wall frame rack is the next level in vertical bike parking.

CONSTRUCTION/MATERIAL
Hooks: 1.0" Square Mild Steel Tube
Wheel Cushion: Santoprene TPV
Anti-Sag Truss Bar: Galvanized Steel Tubing

DIMENSION/CAPACITY OPTIONS (BASED ON 16" SPACING)
- 30.7" Depth
- 40" Height

□ 3 Bike 52.8" Length
□ 5 Bike 84.8" Length
□ 7 Bike 116.8" Length

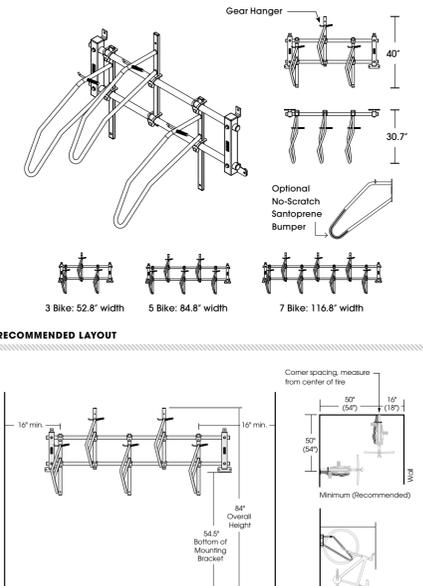
Based on 16" spacing, wider spacing available to meet city requirements.

Total Bike Capacity needed: _____

MOUNTING
(6) .56" Mounting Holes

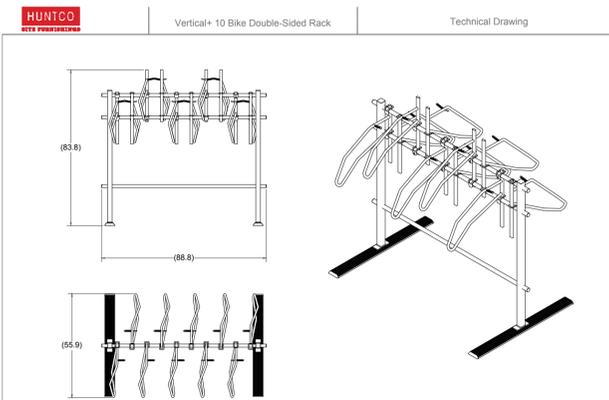
RECOMMENDED MOUNTING
- Concrete or wood studs.

FINISH OPTIONS
□ Charcoal Powder Coat
□ Black Powder Coat

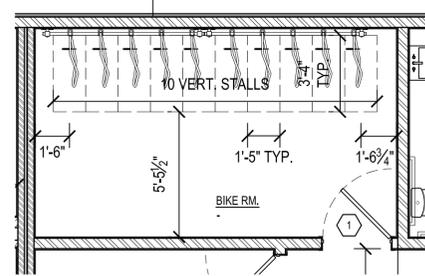


RECOMMENDED LAYOUT

7. LONG TERM VERTICAL WALL MTD. BIKE RACK



8. LONG TERM VERTICAL FLOOR MTD. BIKE RACK



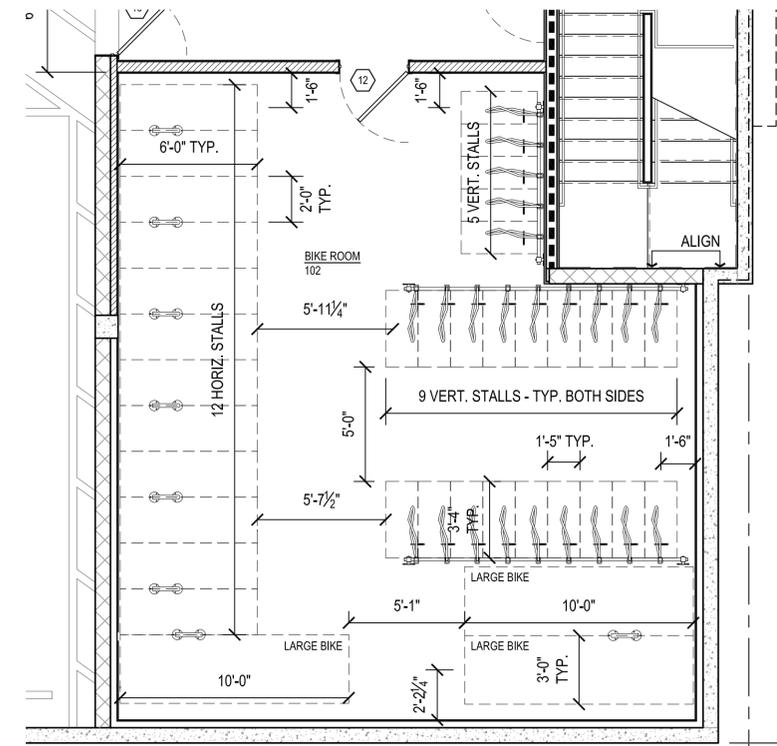
2. ENLARGED LONG TERM BIKE ROOM - FLOOR 2

LONG TERM BICYCLE STALL COUNT:

REQUIRED:
43 UNITS (x1.1) = 47.3 = 48 TOTAL
30% HORIZ. = 15 (INCLUDING LARGE)
5% LARGE = 3

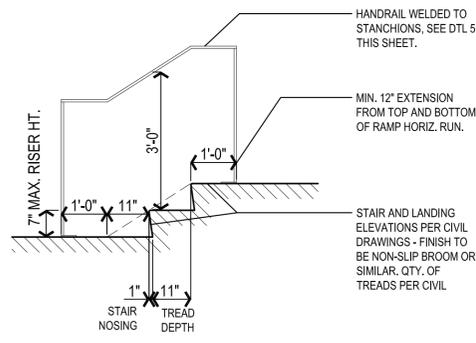
PROVIDED:
FLOOR 1: 3 LARGE + 12 HORIZ = 15
23 VERTICAL
FLOOR 2: 10 VERTICAL
TTL: 48

SEE CUTSHEETS THIS PAGE

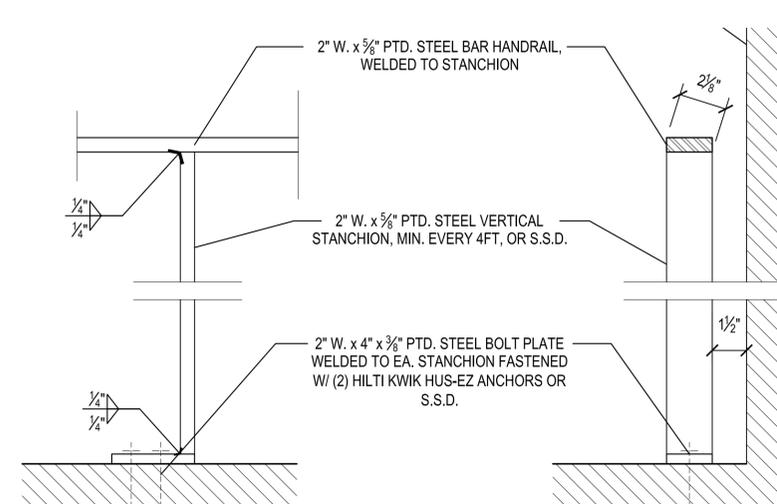


1. ENLARGED LONG TERM BIKE ROOM - FLOOR 1

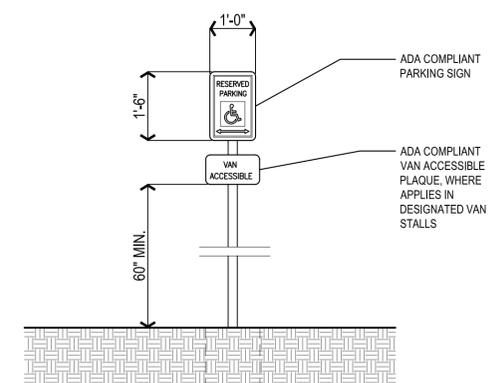
SCALE: 1/4" = 1'-0" ON 22x34 1/8" = 1'-0" ON 11x17



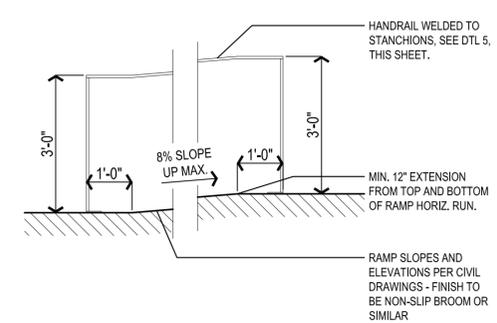
4 TYPICAL STAIR TREAD + HANDRAIL
1/2"=1'-0" ELEVATION VIEW



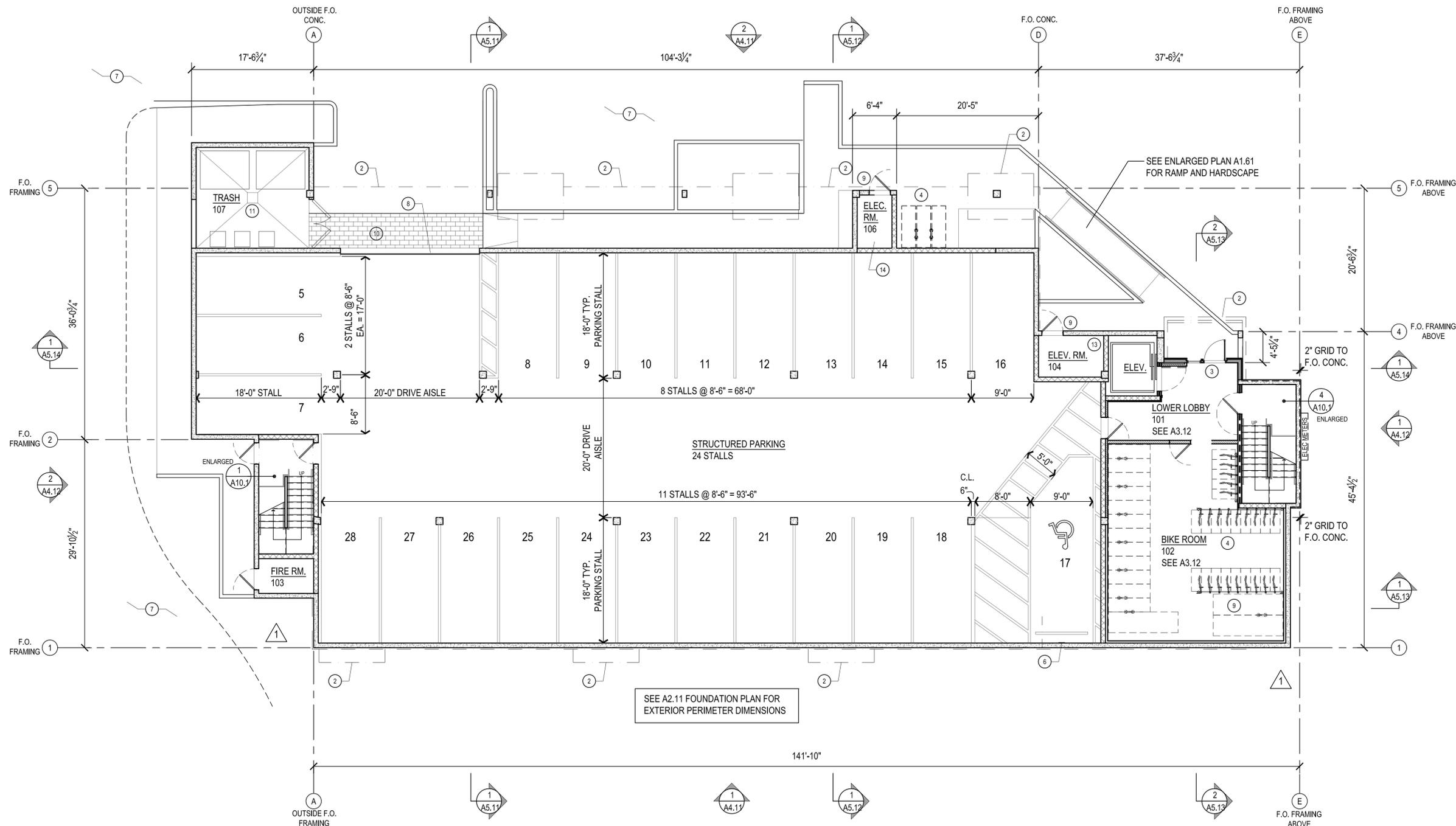
5 STEEL HANDRAIL DETAIL
3"=1'-0" - SECTION VIEWS



2 ACCESSIBLE PARKING SIGNAGE
1/2"=1'-0" ELEVATION VIEW



3 TYPICAL HANDRAIL - RAMPS
1/2"=1'-0" ELEVATION VIEW



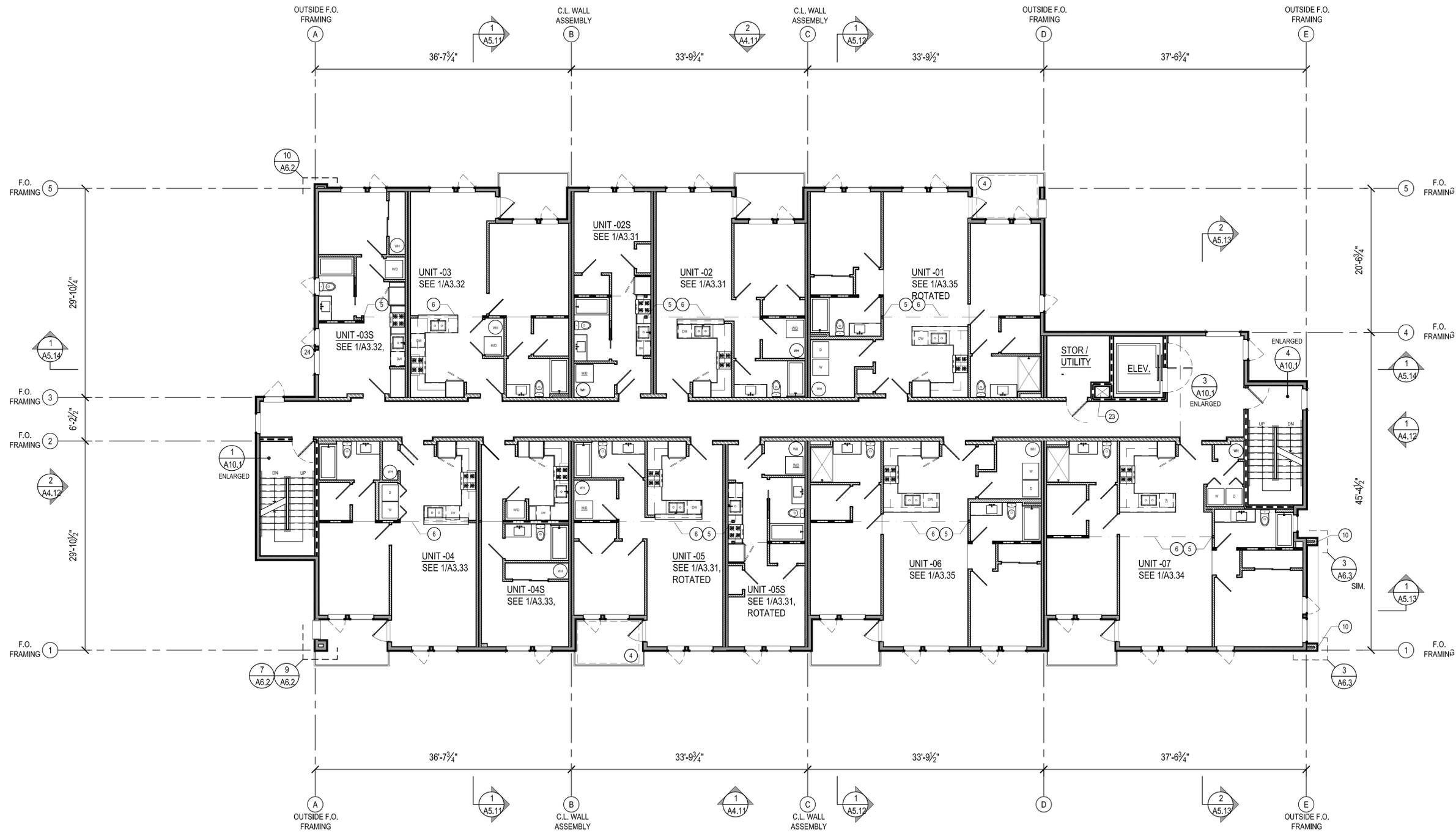
- SHEET NOTES.**
1. REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 2. DASH DOT LINE OF ELEMENT ABOVE, SEE RCP A3 SERIES
 3. REFER TO NOTED ENLARGED FLOOR PLAN FOR DIMENSION TO WINDOWS / DOORS
 4. REQ. BIKE PARKING - SEE NOTED ENLARGED PLAN FOR CALCS AND DIMENSIONS
 5. SEE A1.11 FOR EGRESS PATH
 6. ACCESSIBLE PARKING SIGNAGE, SEE 2/A1.61
 7. SEE A1.11 SITE PLAN + CIVIL DRAWINGS FOR AUTO RAMP + SURFACE PARKING
 8. PERFORATED COILING ROLL-UP DOOR, SEE ELEVS - TRENCH DRAIN OUTSIDE BASE OF DOOR, SEE PLUMBING AND CIVIL DRAWINGS
 9. PROVIDE REQ. DOOR SIGNAGE W/ MIN. 1" TALL RAISED LETTERS. SIGNS SHALL BE CONSTRUCTED OF DURABLE MATERIALS. PERMANENTLY INSTALLED & READILY VISIBLE TEXT ON DOORS:
FIRE PROTECTION EQUIPMENT ROOM
ELEVATOR CONTROL ROOM
ELECTRICAL ROOM
STANDBY GENERATOR ROOM
BICYCLE STORAGE ROOM
 10. PEDESTRIAN CROSSING TO TRASH AREA - PAVING WITH CONTRASTING COLOR AND TEXTURE
 11. HYDRONICALLY ISOLATED DRAIN, SEE PLUMBING DRAWINGS - MIN. SLOPE CONC. SLAB 1/8" PER FT. TO DRAIN
 12. NOT USED
 13. DAS / ERRC HEAD-END + 2-WAY COMMUNICATION SYSTEM THIS ROOM, SEE NOTES ON ENLARGED PLAN A3.12
 14. NOT USED

WALL HATCH SCHEDULE
SEE A1.31 FOR LIFE SAFETY PLANS

[Hatch 1]	1-HR FIRE RATED EXTERIOR WALL, SEE E1/A1.31
[Hatch 2]	2-HR FIRE RATED BARRIER AT EGRESS SHAFT, SEE S1/A1.31 - RATING PER PFC ALTERNATE TO AERIAL FIRE APPARATUS ROAD
[Hatch 3]	2-HR FIRE RATED CMU BARRIER AT EGRESS SHAFT, SEE S3/A1.31
[Hatch 4]	1-HR FIRE RATED PARTITION BETWEEN UNITS, SEE P1/A1.31
[Hatch 5]	0.5-HR FIRE RATED PARTITION AT CORRIDOR, SEE P2/A1.31
[Hatch 6]	1-HR FIRE RATED INTERIOR BEARING WALL, SEE P3/A1.31

1. 1ST FLOOR PLAN
SCALE: 1/8" = 1'-0" ON 22x34 1/16" = 1'-0" ON 11x17

STEELHEAD ARCHITECTURE
 REGISTERED ARCHITECT
 GABRIEL HEADRICK, AIA
 PORTLAND OREGON 5016
 STATE OF OREGON
 5210 SE 26TH AVE. PORTLAND OREGON 97202
FIRST FLOOR PLAN
 GIBBS DEVELOPMENT
 1325 SW GIBBS STREET
 PORTLAND, OR 97239
 RECEIVED 04/22/21
 LAND USE SET
 09/01/2022
A2.12
 LU 22-187796 APD AP
 Exhibit C.5



- SHEET NOTES.**
- REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 - DASH-DOT LINE OF ELEMENT ABOVE, SEE RCP, A3-SERIES
 - REFER TO NOTED ENLARGED FLOOR PLAN FOR DIMENSIONS TO WINDOWS / DOORS
 - DASHED LINE OF 6'-0" x 8'-0" REQUIRED OUTDOOR AREA, TYP. EA DECK SAME DIMENSION
 - PRIMARY FRAME STRUCTURAL COLUMN TO BE INDIVIDUALLY ENCASED IN 5/8" TYPE X GYP. BOARD FOR FULL LENGTH OF COLUMN AND CONNECTION TO OTHER STRUCTURAL MEMBERS (BEAMS, SEE BELOW) - SEE A/A1.31
 - DASHDOT LINE OF PRIMARY FRAME STRUCTURAL BEAM FLUSH WITH JOISTS, S.S.D. - TO BE INDIVIDUALLY ENCASED IN 5/8" TYPE X GYP. BOARD FOR FULL LENGTH OF BEAM AND CONNECTION TO POSTS - SEE A/A1.31
 - EXTERIOR WALL STRUCTURAL COLUMN TO BE INDIVIDUALLY ENCASED IN 5/8" TYPE X GYP. BOARD FOR FULL LENGTH OF COLUMN AND CONNECTION TO OTHER STRUCTURAL MEMBERS
 - DAS / ERRC BACKBONE LINE RUN IN 2-HR RATED STAIR SHAFT ALL FLOORS - CONNECT TO REQUIRED RELAY AT EACH FLOOR
 - STEEL CANOPY, SEE A2.21 FOR DIMS AND SEE DETAILS OFF NOTED SECTION + 1/S2.81
 - METAL FRAMED 'TUBE', S.S.D. NOTES DETAILS
 - 19. NOT USED
 - ROOF ACCESS SHIPS LADDER, SEE STAIR ENLARGED PLANS, A10 SERIES
 - ROOF DECK WITH GUARDRAIL, SEE SECTIONS, A5-SERIES
 - SCUPPER TO DRAIN, SEE ROOF DETAILS A8-SERIES
 - SHAFT FOR MECH VENTS, SEE MECH SHEETS - 2 HR. RATED ASSEMBLY PER WALL HATCH SCHEDULE THIS SHEET
 - FL-4 - SEE ELEVATIONS FOR WINDOW ORIENTATION CHANGE

WALL HATCH SCHEDULE
SEE A1.31 FOR LIFE SAFETY PLANS

[Hatch Pattern]	1-HR FIRE RATED EXTERIOR WALL, SEE E1/A1.31
[Hatch Pattern]	2-HR FIRE RATED BARRIER AT EGRESS SHAFT, SEE S1/A1.31 - RATING PER PFC ALTERNATE TO AERIAL FIRE APPARATUS ROAD
[Hatch Pattern]	2-HR FIRE RATED CMU BARRIER AT EGRESS SHAFT, SEE S3/A1.31
[Hatch Pattern]	1-HR FIRE RATED PARTITION BETWEEN UNITS, SEE P1/A1.31
[Hatch Pattern]	0.5-HR FIRE RATED PARTITION AT CORRIDOR, SEE P2/A1.31
[Hatch Pattern]	1-HR FIRE RATED INTERIOR BEARING WALL, SEE P3/A1.31

1. 3RD - 4TH FLOOR PLAN
SCALE: 1/8" = 1'-0" ON 22x34 1/16" = 1'-0" ON 11x17

REGISTERED ARCHITECT
 GABRIEL HEADRICK, AIA
 PORTLAND OREGON
 5016
 STATE OF OREGON

STEELHEAD ARCHITECTURE
 5210 SE 26TH AVE. PORTLAND OREGON 97202

THIRD - FOURTH FLOOR PLAN

GIBBS DEVELOPMENT
 1325 SW GIBBS STREET
 PORTLAND, OR 97239

LAND USE SET
 09/01/2022

A2.31
 LU 22-187796 APD AP
 Exhibit C.5



- SHEET NOTES.**
- REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 - DASH-DOT LINE OF ELEMENT ABOVE, SEE RCP, A3-SERIES
 - REFER TO NOTED ENLARGED FLOOR PLAN FOR DIMENSIONS TO WINDOWS / DOORS
 - DASHED LINE OF 6'-0" x 8'-0" REQUIRED OUTDOOR AREA, TYP. EA DECK SAME DIMENSION
 - PRIMARY FRAME STRUCTURAL COLUMN TO BE INDIVIDUALLY ENCASED IN 5/8" TYPE X GYP. BOARD FOR FULL LENGTH OF COLUMN AND CONNECTION TO OTHER STRUCTURAL MEMBERS (BEAMS, SEE BELOW) - SEE A/A1.31
 - DASHDOT LINE OF PRIMARY FRAME STRUCTURAL BEAM FLUSH WITH JOISTS, S.S.D. - TO BE INDIVIDUALLY ENCASED IN 5/8" TYPE X GYP. BOARD FOR FULL LENGTH OF BEAM AND CONNECTION TO POSTS - SEE A/A1.31
 - EXTERIOR WALL STRUCTURAL COLUMN TO BE INDIVIDUALLY ENCASED IN 5/8" TYPE X GYP. BOARD FOR FULL LENGTH OF COLUMN AND CONNECTION TO OTHER STRUCTURAL MEMBERS
 - DAS / ERRC BACKBONE LINE RUN IN 2-HR RATED STAIR SHAFT ALL FLOORS - CONNECT TO REQUIRED RELAY AT EACH FLOOR
 - STEEL CANOPY, SEE A2.21 FOR DIMS AND SEE DETAILS OFF NOTED SECTION + 1/S2.81
 - METAL FRAMED 'TUBE', S.S.D. NOTES DETAILS
 - 19. NOT USED
 - ROOF ACCESS SHIPS LADDER, SEE STAIR ENLARGED PLANS, A10 SERIES
 - ROOF DECK WITH GUARDRAIL, SEE SECTIONS, A5-SERIES
 - SCUPPER TO DRAIN, SEE ROOF DETAILS A8-SERIES
 - SHAFT FOR MECH VENTS, SEE MECH SHEETS - 2 HR. RATED ASSEMBLY PER WALL HATCH SCHEDULE THIS SHEET
 - FL-4 - SEE ELEVATIONS FOR WINDOW ORIENTATION CHANGE

WALL HATCH SCHEDULE
SEE A1.31 FOR LIFE SAFETY PLANS

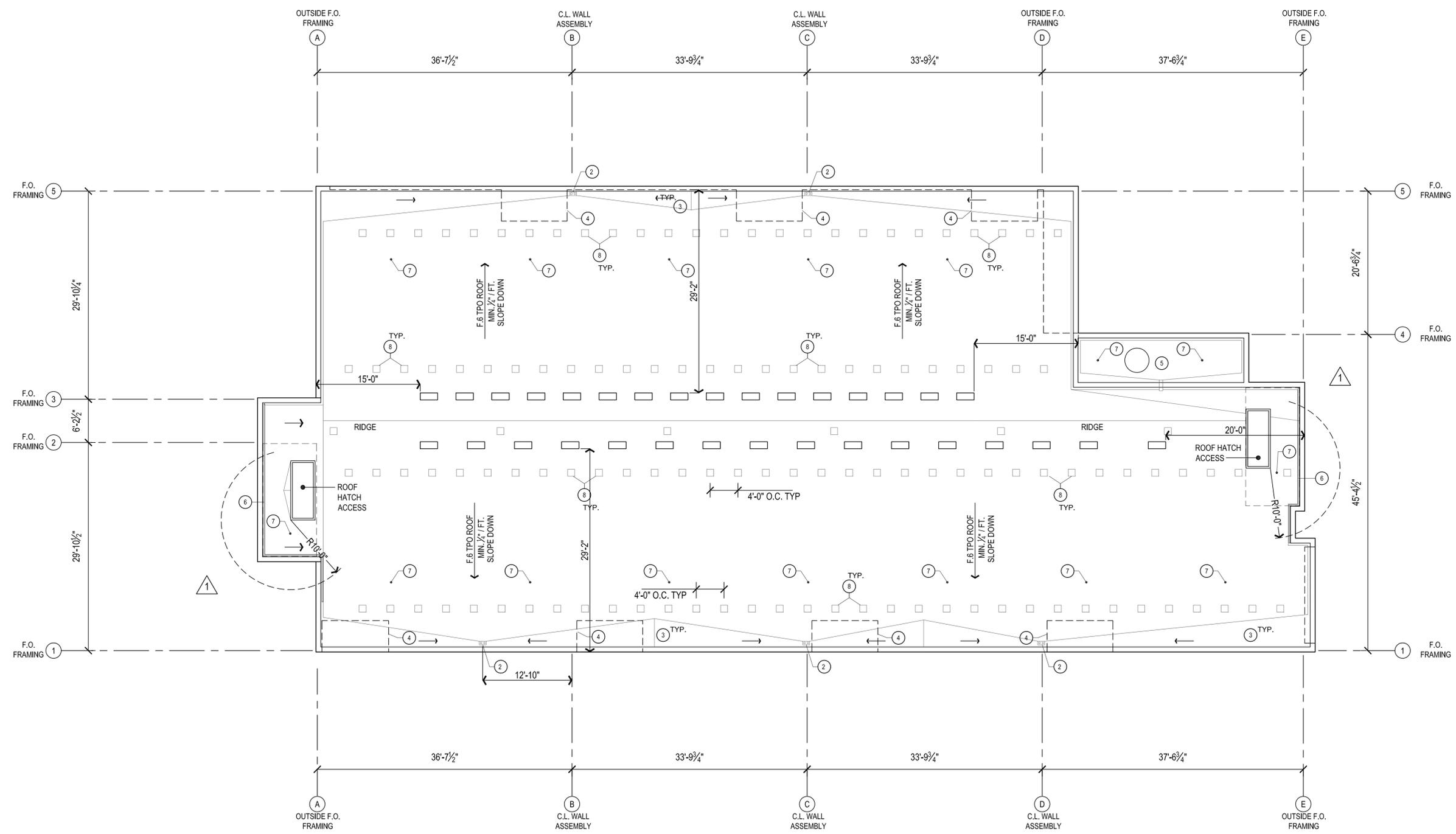
	1-HR FIRE RATED EXTERIOR WALL, SEE E1/A1.31
	2-HR FIRE RATED BARRIER AT EGRESS SHAFT, SEE S1/A1.31 - RATING PER PFC ALTERNATE TO AERIAL FIRE APPARATUS ROAD
	2-HR FIRE RATED CMU BARRIER AT EGRESS SHAFT, SEE S3/A1.31
	1-HR FIRE RATED PARTITION BETWEEN UNITS, SEE P1/A1.31
	0.5-HR FIRE RATED PARTITION AT CORRIDOR, SEE P2/A1.31
	1-HR FIRE RATED INTERIOR BEARING WALL, SEE P3/A1.31

1. 5TH FLOOR PLAN
SCALE: 1/8" = 1'-0" ON 22x34 1/16" = 1'-0" ON 11x17

STEELHEAD ARCHITECTURE
 5210 SE 26TH AVE. PORTLAND OREGON 97202
 REGISTERED ARCHITECT
 GABRIEL HEADRICK, AIA
 PORTLAND OREGON
 5016
 STATE OF OREGON
 FIFTH FLOOR PLAN
 GIBBS DEVELOPMENT
 1325 SW GIBBS STREET
 PORTLAND, OR 97239
 LAND USE SET
 09/01/2022
 A2.51
 LU 22-187796 APD AP
 Exhibit C.5



- SHEET NOTES:**
1. REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 2. ROOF DRAIN WITH OVERFLOW - SEE A8.10 AND SEE PLUMBING PLANS FOR DRAINAGE ROUTING
 3. CRICKETS SLOPING TO ROOF DRAINS, MIN. 3/8" PER FT.
 4. DASHED LINE OF 5TH FLOOR EXTERIOR WALL BELOW
 5. ELEVATOR RAISED SHAFT, SEE ELEVATIONS AND SECTIONS - ELEVATOR VENT IS EXEMPT FROM GUARD REQUIREMENT WITH PERSONAL FALL ARREST ANCHORAGE THAT COMPLIES WITH ANSI Z359.1, SEE NOTE 7
 6. ROOF GUARD WITHIN 10-FT OF ACCESS HATCH, SEE 4/A9.1
 7. PERSONAL FALL ARREST ANCHORAGE LOCATIONS - COMPLIES WITH ANSI Z359.1
 8. ROOF VENT JACKS FOR VENTED ROOF ASSEMBLY - AREA OF CONCEALED SPACES LIMITED TO 160 CUBIC FEET, SEE SECTION 1/A5.13 FOR AREAS, SO FIRE SPRINKLERS ARE NOT REQUIRED PER NFPA13 8.14.1.2.6

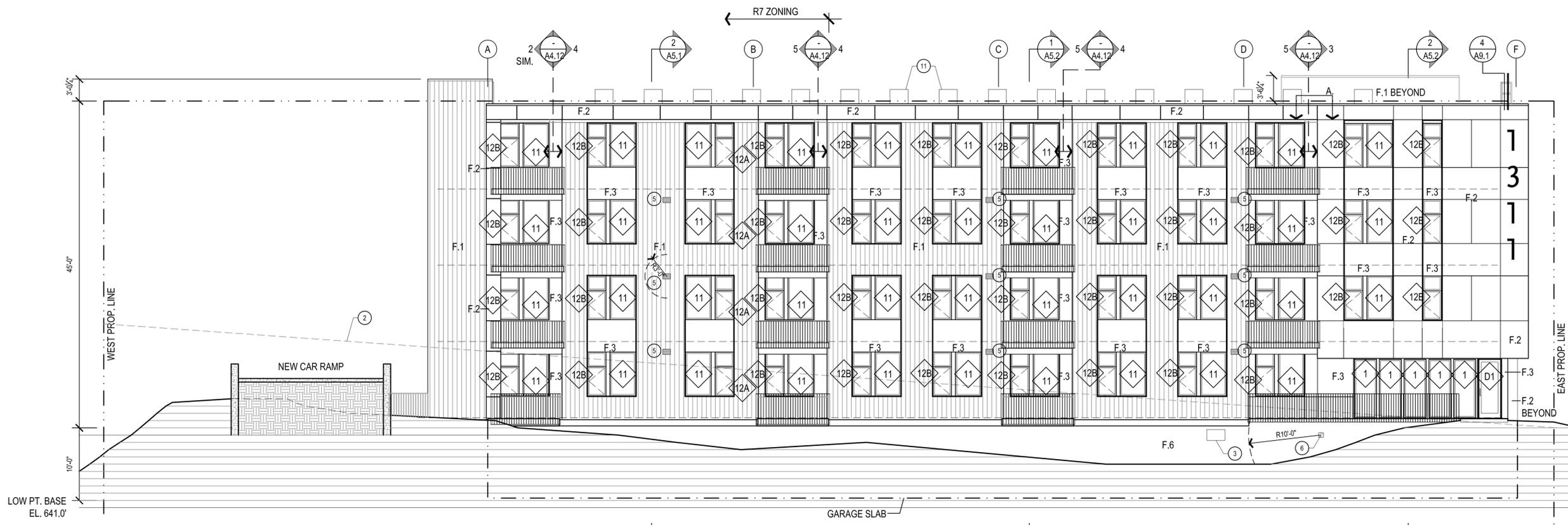


1. ROOF PLAN
SCALE: 1/8" = 1'-0" ON 22x34 1/16" = 1'-0" ON 11x17

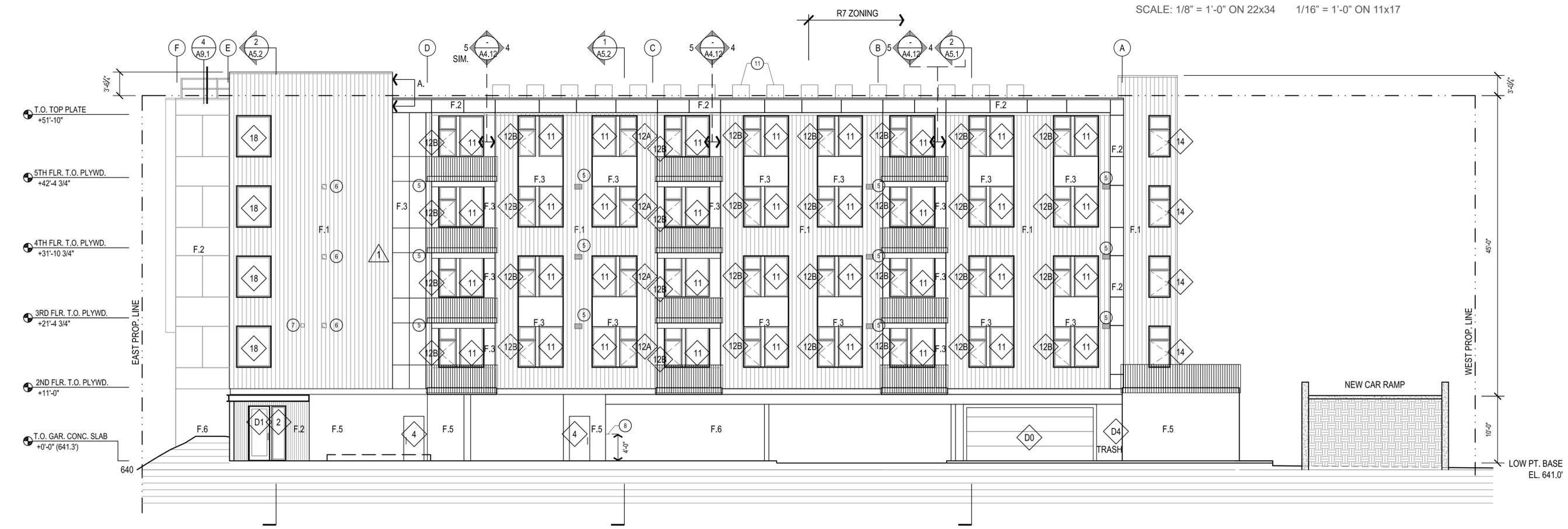
WALL HATCH SCHEDULE
SEE A1.31 FOR LIFE SAFETY PLANS

[Hatch Pattern]	1-HR FIRE RATED EXTERIOR WALL, SEE E1/A1.31
[Hatch Pattern]	2-HR FIRE RATED BARRIER AT EGRESS SHAFT, SEE S1/A1.31 - RATING PER PFC ALTERNATE TO AERIAL FIRE APPARATUS ROAD
[Hatch Pattern]	2-HR FIRE RATED CMU BARRIER AT EGRESS SHAFT, SEE S3/A1.31
[Hatch Pattern]	1-HR FIRE RATED PARTITION BETWEEN UNITS, SEE P1/A1.31
[Hatch Pattern]	0.5-HR FIRE RATED PARTITION AT CORRIDOR, SEE P2/A1.31
[Hatch Pattern]	1-HR FIRE RATED INTERIOR BEARING WALL, SEE P3/A1.31

LU 22-187796 APD AP
Exhibit C.5



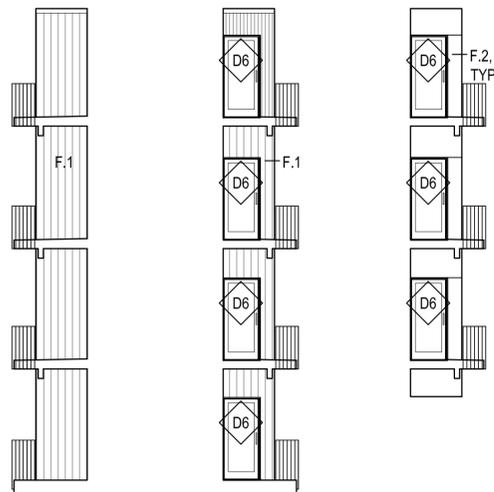
1. SOUTH ELEVATION
SCALE: 1/8" = 1'-0" ON 22x34 1/16" = 1'-0" ON 11x17



2. NORTH ELEVATION
SCALE: 1/8" = 1'-0" ON 22x34 1/16" = 1'-0" ON 11x17

- SHEET NOTES:**
1. REFER TO A0.1 FOR SYMBOLS, HATCHES AND ABBREVIATIONS, A4 SERIES FOR WINDOW, DOOR AND FINISH SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 2. DASHED LINE OF ROAD OR RAMP IN FOREGROUND. SEE SECTIONS
 3. MECH VENT FROM GARAGE. SEE M1.1
 4. OMIT MECH. SCREEN - MECH. UNITS HEIGHT MEETS DISTANCE FROM EDGE OF ROOF REQUIREMENT. SEE SECTIONS FOR DIAGRAM
 5. MECH VENT TERMINATION - COMBO GRILL PER MECH DETAIL - SEE M1-SERIES
 6. MECH. EXHAUST, SEE MECHANICAL DRAWINGS - M1 SERIES FOR ADDITIONAL INFORMATION - MAINTAIN 3 FT. CLEARANCE FROM OPERABLE OPENINGS INTO BUILDING & 10 FT. MIN. FROM MECH. INTAKES
 7. MECH. VENT FROM FCU - SEE M1.2
 8. NOT USED
 9. NOT USED
 10. NOT USED
 11. CONDENSER UNITS - SEE SECTIONS FOR DISTANCE SET BACK FROM ROOF EDGE

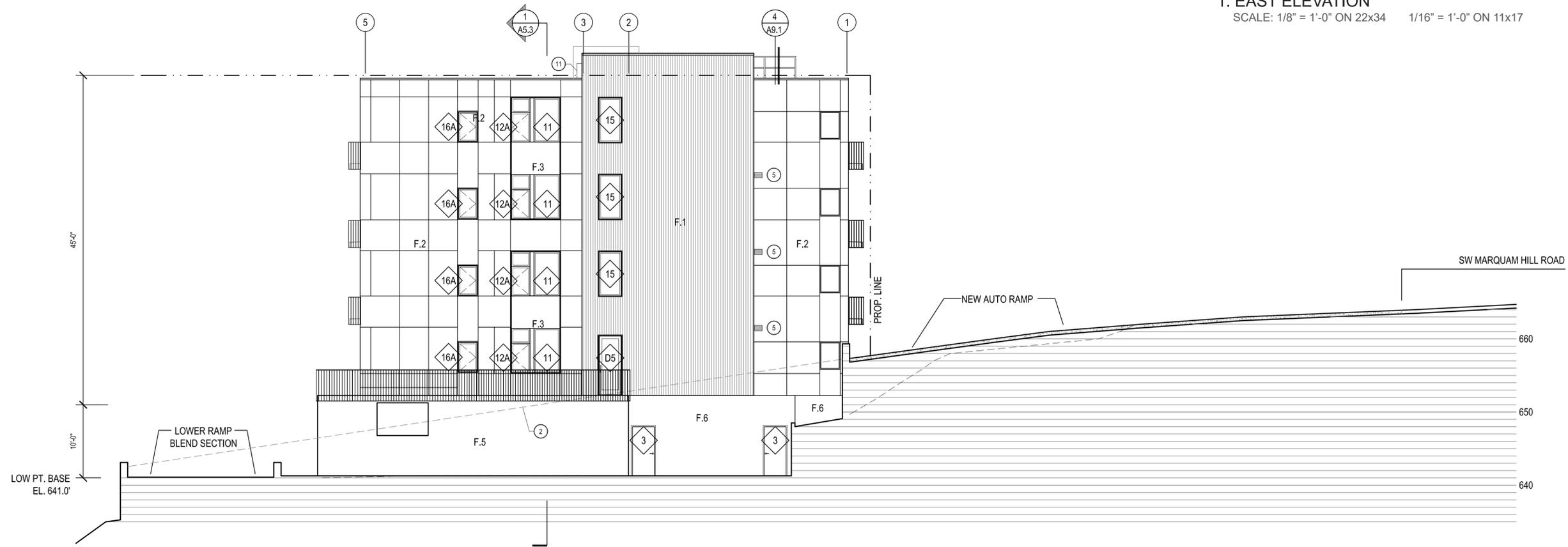




5. DECK SIDE 4. DOOR TO DECK 3. DOOR TO DECK
 SCALE: 1/8" = 1'-0" ON 22x34 1/16" = 1'-0" ON 11x17



1. EAST ELEVATION
 SCALE: 1/8" = 1'-0" ON 22x34 1/16" = 1'-0" ON 11x17

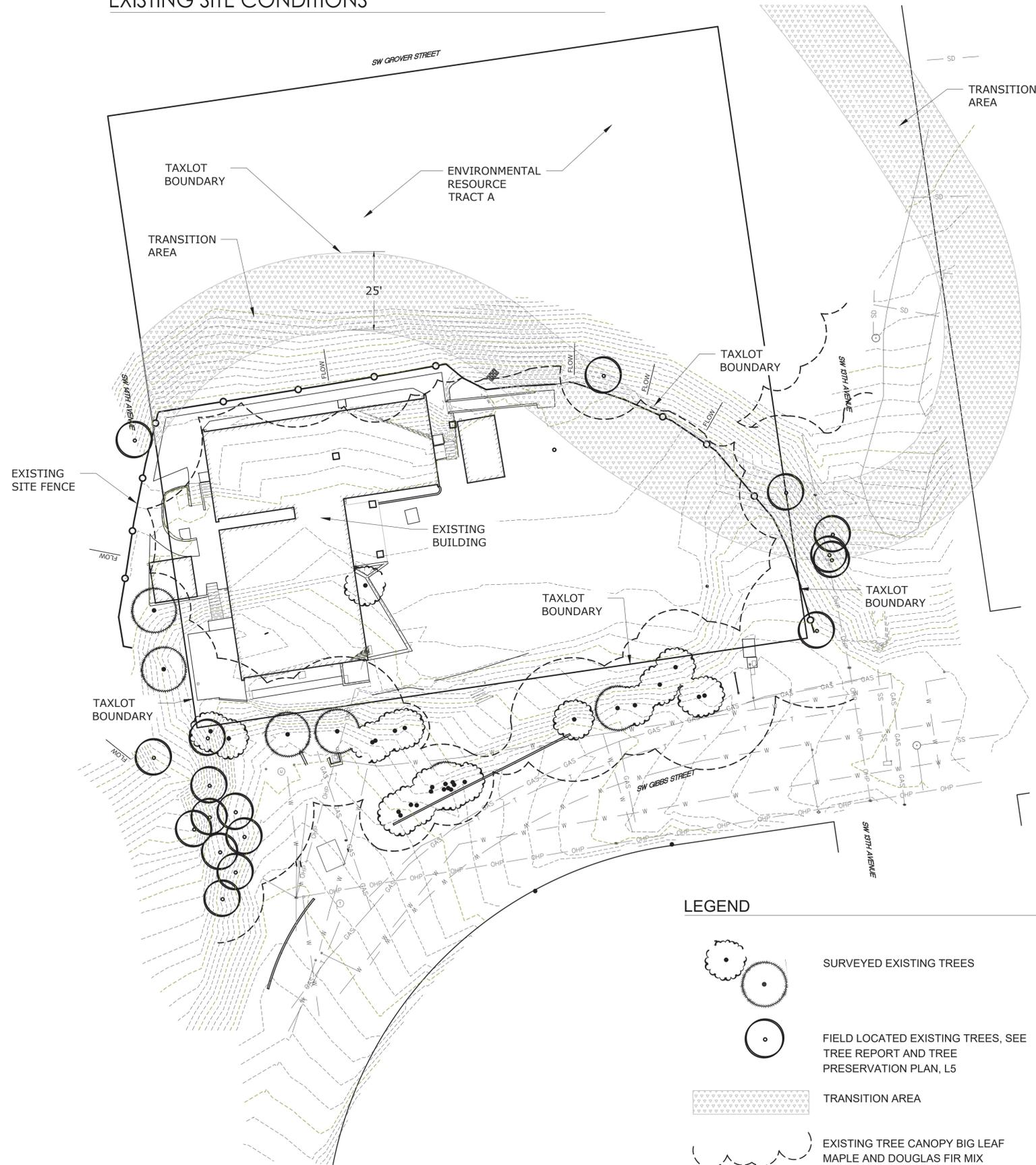


2. WEST ELEVATION
 SCALE: 1/8" = 1'-0" ON 22x34 1/16" = 1'-0" ON 11x17

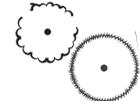
- SHEET NOTES:**
1. REFER TO A0.1 FOR SYMBOLS, HATCHES AND ABBREVIATIONS, A4 SERIES FOR WINDOW, DOOR AND FINISH SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 2. DASHED LINE OF ROAD OR RAMP IN FOREGROUND. SEE SECTIONS
 3. NOT USED
 4. OMIT MECH. SCREEN - MECH. UNITS HEIGHT MEETS DISTANCE FROM EDGE OF ROOF REQUIREMENT. SEE SECTIONS FOR DIAGRAM
 5. MECH VENT TERMINATION - COMBO GRILL PER MECH DETAIL - SEE M1-SERIES
 6. MECH. EXHAUST, SEE MECHANICAL DRAWINGS - M1 SERIES FOR ADDITIONAL INFORMATION - MAINTAIN 3 FT. CLEARANCE FROM OPERABLE OPENINGS INTO BUILDING & 10 FT. MIN. FROM MECH. INTAKES
 7. NOT USED
 8. NOT USED
 9. NOT USED
 10. NOT USED
 11. CONDENSER UNITS - SEE SECTIONS FOR DISTANCE SET BACK FROM ROOF EDGE



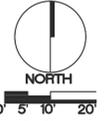
EXISTING SITE CONDITIONS



LEGEND

-  SURVEYED EXISTING TREES
-  FIELD LOCATED EXISTING TREES, SEE TREE REPORT AND TREE PRESERVATION PLAN, L5
-  TRANSITION AREA
-  EXISTING TREE CANOPY BIG LEAF MAPLE AND DOUGLAS FIR MIX

REGISTERED
 767
 DIGITALLY SIGNED 2022.09.23 18:29:25 -0700
 David S. Goodyke
 OREGON
 05/11/12
 LANDSCAPE ARCHITECT
 RENEWS: 5-11-23



IF THIS BAR IS NOT 1", THEN DRAWING IS NOT TO SCALE

CONSULTANT:
NNA
 Landscape Architecture
 1811 S RIVER DRIVE SUITE 300
 PORTLAND, OR 97201
 503.239.0600

SCOTIA WESTERN STATES HOUSING
 PORTLAND, OR 97239
 EXISTING SITE PLAN

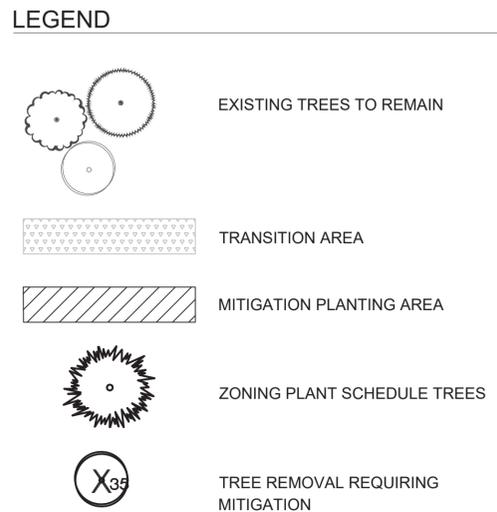
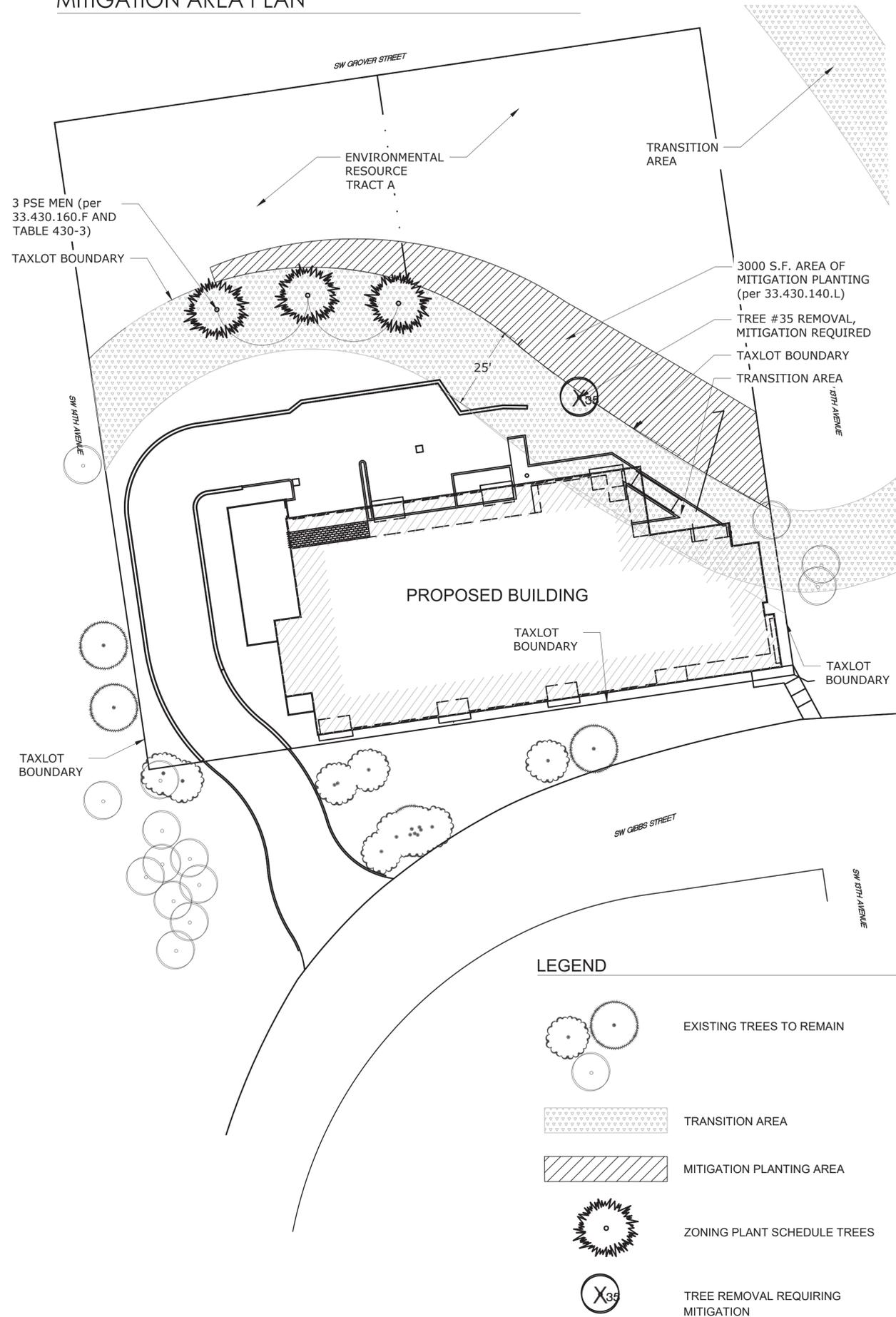
REVISIONS:

DELTA	DATE	TITLE

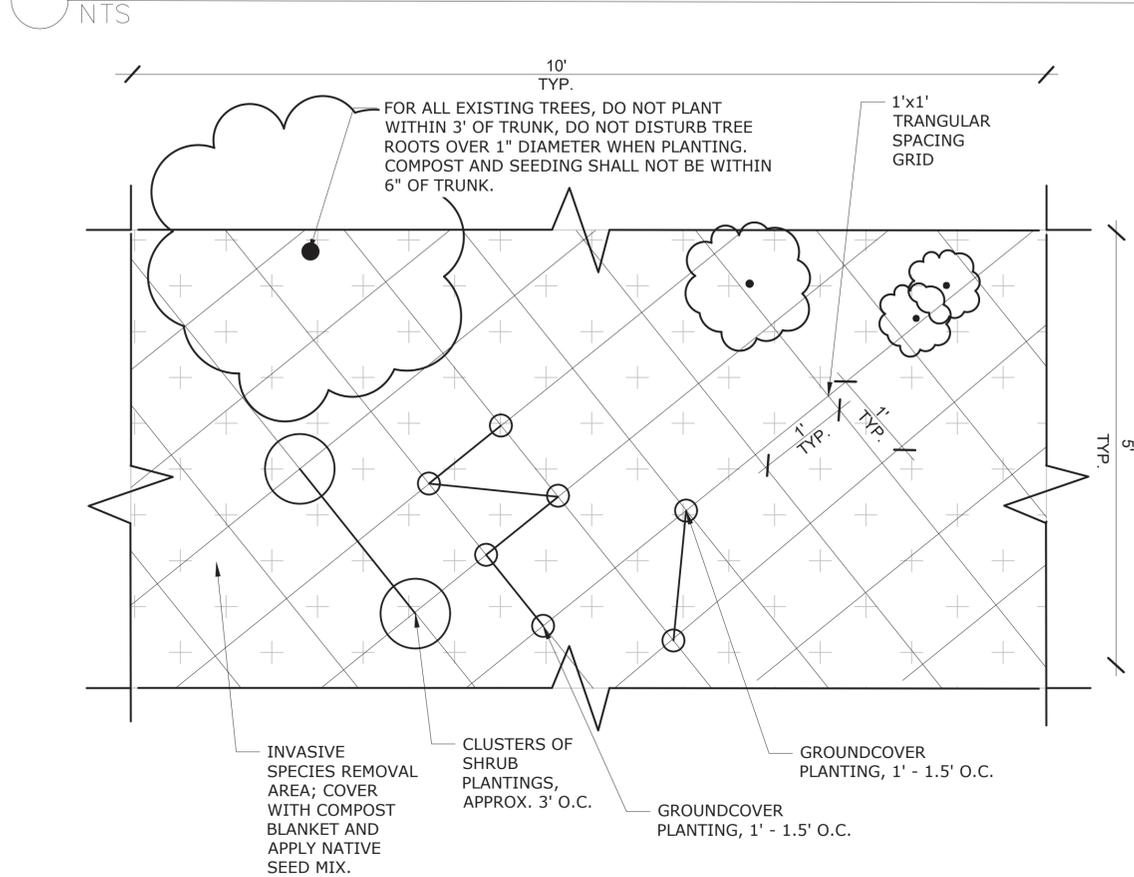
SCALE: 1" = 20'
 DRAWN BY: JJ
 CHECKED BY: DG

SHEET
L-1

MITIGATION AREA PLAN



TYPICAL MITIGATION AREA PLANTING DETAIL



MITIGATION PLANT SCHEDULE

Symbol	Botanical Name	Common Name	Planting Size / Spacing	Quantity
Trees				
PSE MEN	<i>Pseudotsuga menziesii</i>	Douglas Fir	5'	3
Required plantings, per LU 19-258445 PD ENM LDP				
Deciduous Shrubs				
SYM ALB	<i>Symphoricarpos albus</i>	Snowberry	1 gallon	40
OEM CER	<i>Oemleria cerasiformis</i>	Oso berry	1 gallon	20
Evergreen Shrubs				
POL MUN	<i>Polystichum munitum</i>	Swordfern	1 gallon	60
Groundcover				
GAL SHA	<i>Gaultheria shallon</i>	Salal	4 inch	140
RIB LAX	<i>Ribes laxiflorum</i>	Western Black Currant	4 inch	140
VAN HEX	<i>Vancouveria hexandra</i>	Inside-out flower	4 inch	140
Seed Mix				
	<i>Bromus carinatus</i>	California Brome	PLS	20%
	<i>Deschampsia elongata</i>	Slender Hairgrass	PLS	20%
	<i>Geum macrophyllum</i>	Large-leaved Avens	PLS	10%
	<i>Lupinus latifolius</i>	Large Leaf Lupine	PLS	10%
	<i>Prunella lanceolata</i>	Selfheal	PLS	20%
	<i>Tellima grandifolia</i>	Fringe Cups	PLS	20%
per 33.430.140.L				

NOTES

- WITHIN THE MITIGATION PLANTING AREA, REMOVE INVASIVE PLANT SPECIES IDENTIFIED IN THE CITY OF PORTLAND BUREAU OF ENVIRONMENTAL SERVICES' "PORTLAND PLANT LIST, '4.1 NUISANCE PLANTS' " <https://www.portlandoregon.gov/citycode/article/322280>.
- INSTALL A COMPOST BLANKET FOR EROSION CONTROL ONLY AFTER THE EFFECTIVE REMOVAL OF INVASIVE OR NUISANCE SPECIES. COVER ALL EXPOSED SOIL.
- INSTALL SHRUB AND GROUNDCOVER PLANTS THROUGH THE COMPOST BLANKET, TAKING CARE NOT TO DISTURB OUTSIDE THE PLANTING PIT. FIELD ADJUST LOCATIONS TO FIND GAPS IN THE TREE CANOPY AND AVOID EXISTING TREE ROOTS, USE ONLY HAND TOOLS FOR DIGGING IN THESE AREAS.
- FOR SHRUB LAYOUT, USE A 3' O.C. TRIANGULAR SPACING; ACTUAL SPACING MAY BE 2'-4' O.C. SHRUBS SHALL BE PLANTED IN GROUPS OF THE SAME SPECIES, AT THE DENSITY OF 2 SHRUBS PER 50 S.F.
- FOR GROUNDCOVER LAYOUT, USE 1' O.C. TRIANGULAR SPACING; ACTUAL SPACING MAY BE 1'-2' O.C. GROUNDCOVER PLANTS SHALL BE PLANTED IN GROUPINGS OF 2 - 5, AT THE DENSITY OF 7 GROUNDCOVER PLANTS PER 50 S.F.
- INSTALL THE TOTAL QUANTITY OF SHRUB AND GROUNDCOVER PLANTS LISTED ON THE PLANT SCHEDULE, DISTRIBUTE THROUGHOUT THE MITIGATION AREA.
- INSTALL PURE LIVE SEED (PLS) SHALL BE SOWN ON TO THE COMPOST BLANKET AFTER THE INSTALLATION OF THE SHRUB AND GROUNDCOVER PLANTS. DO NOT APPLY SEE WITHIN 6-INCHES OF EXISTING TREE TRUNKS OR THE STEMS OF SHRUB OR GROUNDCOVER PLANTS. DO NOT APPLY SEED TO THE FOLIAGE OF SHRUB OR GROUNDCOVER PLANTS.

REGISTERED
767
DIGITALLY SIGNED 2022.09.23 18:30:01 -07'00'
David S. Goodyke
OREGON
05/11/12
LANDSCAPE ARCHITECT
RENEWS: 5-11-23

NORTH
0 5 10 20
IF THIS BAR IS NOT 1", THEN DRAWING IS NOT TO SCALE

CONSULTANT:
NNA
Landscape Architecture
1811 S RIVER DRIVE SUITE 300
PORTLAND, OR 97201
503.239.0600

SCOTIA WESTERN STATES HOUSING
PORTLAND, OR 97239

MITIGATION PLANTING PLAN

REVISIONS:

DELTA	DATE	TITLE
Δ	02/05/2021	Rev 1

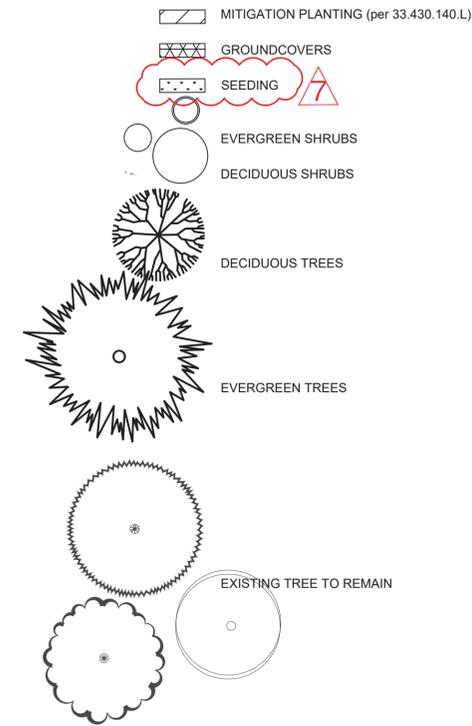
SCALE: 1" = 20'
DRAWN BY: JJ
CHECKED BY: DG

SHEET
L-3

DATE: 09/23/22
JOB NO. 1914
LU 22-187796 APD AP
Exhibit C.6

PLANTING SITE PLAN

LEGEND

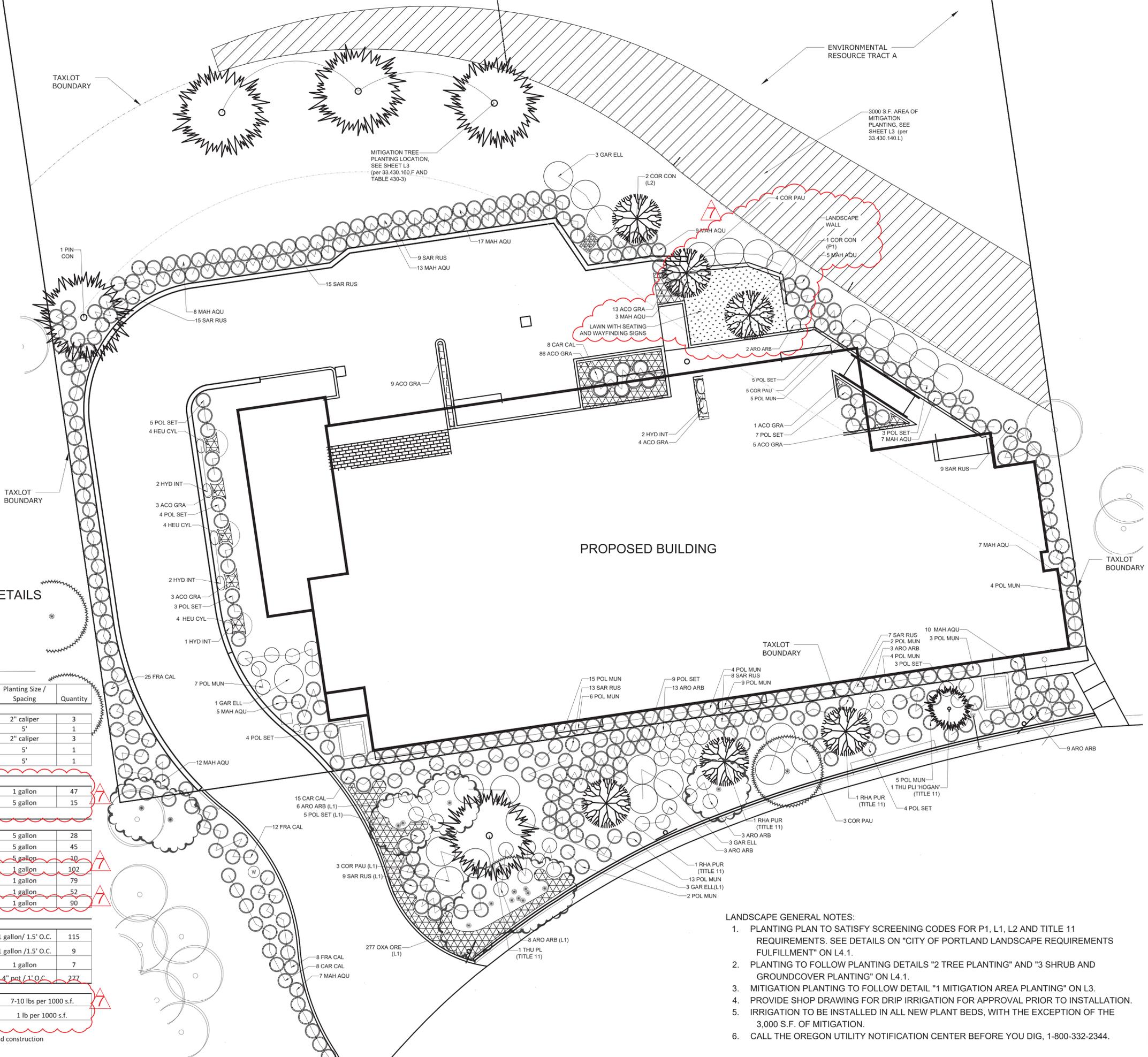


SEE TH0903 FOR APPROVED RIGHT OF WAY DETAILS AND STREET TREE ACTIVITY

PLANT LIST

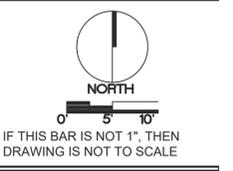
Symbol	Botanical Name	Common Name	Planting Size / Spacing	Quantity
Trees				
COR CON	<i>Cornus controversa</i>	Giant Dogwood	2" caliper	3
PIN CON	<i>Pinus contorta var. contorta</i>	Shore Pine	5"	1
RHA PUR	<i>Rhamnus purshiana</i>	Cascara	2" caliper	3
THU PLI	<i>Thuja plicata</i>	Western Redcedar	5"	1
'HOGAN'	<i>Thuja plicata 'Hogan'</i>	Hogan Western Redcedar	5"	1
Deciduous Shrubs				
ARO ARB	<i>Aronia arbutifolia 'Brilliantissima'</i>	Brilliant Red Chokeberry	1 gallon	47
COR PAU	<i>Corylopsis pauciflora</i>	Buttercup Winter-hazel	5 gallon	15
Evergreen Shrubs				
CAR CAL	<i>Carpenteria californica 'Elizabeth'</i>	Bush Anemone	5 gallon	28
FRA CAL	<i>Frangula californica 'Eve Case'</i>	Eve Case Coffeeberry	5 gallon	45
GAR EL	<i>Garrya elliptica 'James Roof'</i>	Wavy Leaf Silk Tassel	5 gallon	10
MAH AQU	<i>Mahonia aquifolium</i>	Tall Oregongrape	1 gallon	102
POL MUN	<i>Polystichum munitum</i>	Swordfern	1 gallon	79
POL SET	<i>Polystichum setiferum var. diversilobum</i>	Soft Shield Fern	1 gallon	52
SAR RUS	<i>Sarcococca ruscifolia</i>	Sweet Box	1 gallon	90
Groundcovers and Vines				
ACO GRA	<i>Acorus gramineus 'Variegatus'</i>	Variegated Sweet Flag	1 gallon/ 1.5' O.C.	115
HEU CYL	<i>Heuchera cylindrica</i>	Green Alum Root	1 gallon /1.5' O.C.	9
HYD INT	<i>Hydrangea integrifolia</i>	Evergreen Climbing Hydrangea	1 gallon	7
OXA ORE	<i>Oxalis oregana</i>	Redwood Sorrel	4" pot / 1' O.C.	277
Lawn Seeding				
	Pro Time Lawn Seed: PT 305 Shade/Sun Mix		7-10 lbs per 1000 s.f.	
	Pro Time Lawn Seed: PT 313 Super Shade Supplement		1 lb per 1000 s.f.	

Required plantings per LU 18-250445 PD ENM LDP
 Quantities are for permitting, contractor should verify all quantities independently for ordering and construction
 Groundcovers are triangularly spaced and centered as specified



LANDSCAPE GENERAL NOTES:

1. PLANTING PLAN TO SATISFY SCREENING CODES FOR P1, L1, L2 AND TITLE 11 REQUIREMENTS. SEE DETAILS ON "CITY OF PORTLAND LANDSCAPE REQUIREMENTS FULFILLMENT" ON L4.1.
2. PLANTING TO FOLLOW PLANTING DETAILS "2 TREE PLANTING" AND "3 SHRUB AND GROUNDCOVER PLANTING" ON L4.1.
3. MITIGATION PLANTING TO FOLLOW DETAIL "1 MITIGATION AREA PLANTING" ON L3.
4. PROVIDE SHOP DRAWING FOR DRIP IRRIGATION FOR APPROVAL PRIOR TO INSTALLATION.
5. IRRIGATION TO BE INSTALLED IN ALL NEW PLANT BEDS, WITH THE EXCEPTION OF THE 3,000 S.F. OF MITIGATION.
6. CALL THE OREGON UTILITY NOTIFICATION CENTER BEFORE YOU DIG, 1-800-332-2344.



CONSULTANT:
NNA
 Landscape Architecture
 1811 S RIVER DRIVE SUITE 300
 PORTLAND, OR 97201
 503.239.0600

SCOTIA WESTERN STATES HOUSING
 PORTLAND, OR 97239
 PRELIMINARY LANDSCAPE PLAN

REVISIONS:		
DELTA	DATE	TITLE
Δ	02/05/2021	Rev 1
Δ	09/22/2022	Rev 7

SCALE: 1" = 10'
 DRAWN BY: JJ
 CHECKED BY: DG

SHEET
L-4

DATE: 09/23/22
 JOB NO. 1914

LU 22-187796 APD AP
 Exhibit C.6

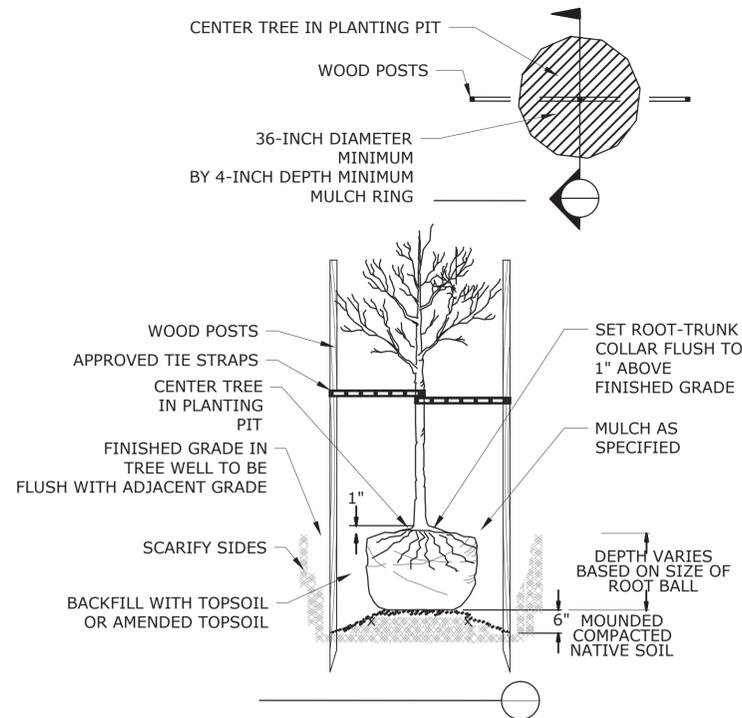
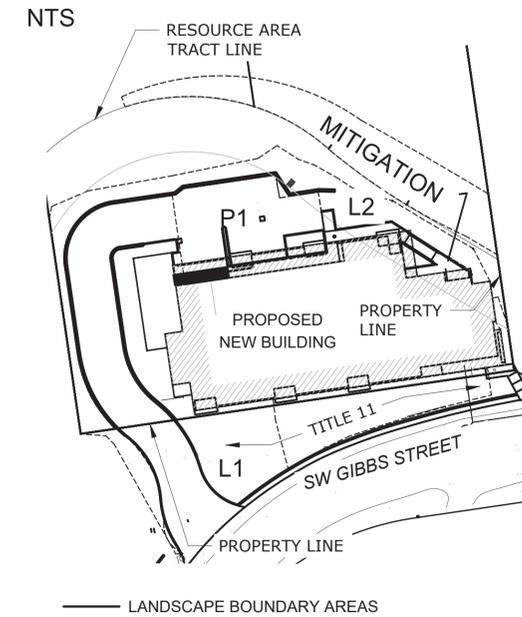
CITY OF PORTLAND LANDSCAPING REQUIREMENTS FULFILLMENT

	Trees		Shrubs		Groundcovers	
	required	provided	required	provided	required	provided
P1 Parking Lot (interior requirements)	2 large 2 small	1 small	1 shrubs	4 shrubs	to cover	74 groundcover plants
<i>Determining Rules</i>	20% evergreen, 1 large tree per 4 spaces, 1 small tree per 2 spaces		1 shrub per space for parking spaces where front 2' have landscape area		Remainder landscape area to be fully covered with groundcover plants	
L1 General Landscaping (perimeter requirements)			1347 s.f. total			
<i>Determining Rules</i>	See Title 11 When area is less than 30' deep, one large tree per 30 l.f., one medium tree per 22 l.f. or one small tree per 15 l.f.		7 high or low shrubs approx. 34 high and low shrubs Where width is greater than 30' deep, two high shrubs or low shrubs per 400 s.f. in addition to tree planting		to cover	approx. 166 groundcover plants
L2 Low Screen (perimeter requirements)	1 large 2 medium 2 small	2 small			to cover	N/A
<i>Determining Rules</i>	One large tree per 30 l.f., one medium tree per 22 l.f. or one small tree per 15 l.f.		Enough evergreen low shrubs to form a continuous screen 3' high		Remainder landscape area to be fully covered with groundcover plants	
Title 11 (property frontage)	5 trees	5 trees				
<i>Determining Rules</i>	One large tree per 25 l.f.; On properties with more than 200' frontage owner is to work with City Forester to integrate existing trees where possible and maximize opportunities to new Street Tree planting considering planter width, location of existing and proposed utilities, and visibility requirements.					

NOTES

1. PLANTING PLAN TO SATISFY PLANT CODES P1, L1, L2, AND TITLE 11 WHERE SHOWN IN DIAGRAM. REQUIRED 3,000 S.F. OF MITIGATION LOCATION IS ALSO SHOWN IN DIAGRAM.
2. PLANTS COUNTED TOWARD LANDSCAPING REQUIREMENTS ARE FOUND IN THE BUREAU OF DEVELOPMENT SERVICES "TREE AND LANDSCAPING MANUAL - PLANT MATERIALS" LIST.
3. NOT ALL PLANTS ON PLANT LIST COUNT TOWARDS CITY OF PORTLAND LANDSCAPING REQUIREMENTS.

LANDSCAPE REQUIREMENTS DIAGRAM

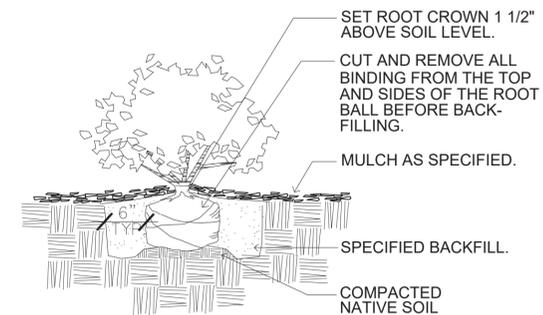


NOTES

1. EXCAVATE AT LEAST 2-1/2 TIMES THE SIZE OF THE ROOT BALL. SCARIFY ALL SIDE OF PLANTING PIT AND THE OUTER EDGE OF THE BOTTOM, LEAVING THE CENTER OF THE BOTTOM COMPACTED TO SUPPORT THE TREE.
2. CAREFULLY REMOVE AS MUCH OF THE CONTAINER OR BURLAP/ WIRE BASKET AS POSSIBLE WITHOUT CAUSING DAMAGE TO ROOTBALL.
3. AMEND BACKFILL SOIL WITH COMPOST (10%-20% OF TOTAL VOLUME) AND MYCORRHIZAL INOCULANTS PER MANUFACTURER'S RECOMMENDATION; MIX THOROUGHLY BEFORE BACKFILLING.
4. FILL PLANTING PIT WITH WATER AND ALLOW IT TO DRAIN COMPLETELY BEFORE INSTALLING TREE.
5. PLACE ROOTBALL ON MOUND OF UNDISTURBED OR COMPACTED SOIL TO PREVENT SETTLEMENT; SPREAD ROOTS AWAY FROM BALL. NO CIRCLING OR UNREASONABLY BENT ROOTS.
6. AFTER BACKFILL IS PLACED, SATURATE AND SETTLE WITH WATER TO REMOVE AIR POCKETS.
7. IF PLANTING ON A SLOPE, CONSTRUCT A 3" HIGH RAIN BASIN MADE OF BACKFILL MATERIAL ON THE DOWNHILL SIDE ONLY. ORIGINAL SLOPE SHOULD PASS THROUGH THE POINT WHERE THE TRUNK BASE MEETS SUBSTRATE/SOIL.
8. WOOD POSTS WITH APPROVED TIE STRAPS ARE REQUIRED FOR STREET TREE PLANTINGS AND MAY BE NECESSARY FOR TREE STABILITY.
9. MULCH A 3-FOOT DIAMETER AREA WITH 4-INCH DEPTH COMPOST, KEEP MULCH 1-INCH AWAY FROM TRUNK.

NOTES

1. EXCAVATE 2½ TIMES THE SIZE OF THE ROOT BALL. ONLY USE HAND TOOLS WHEN WORKING WITHIN TREE PROTECTION FENCES.
2. CAREFULLY REMOVE THE CONTAINER WITHOUT CAUSING DAMAGE TO ROOTBALL.
3. DO NOT CUT OR DAMAGE TREE ROOTS WHILE PLANTING, ADJUST PLANTING LOCATIONS AS NECESSARY TO AVOID CUTTING OR DAMAGING ROOTS.
4. MULCH WITH 4-INCH DEPTH COMPOST AND SATURATE AND SETTLE WITH WATER.



2 TREE PLANTING NTS

3 SHRUB AND GROUNDCOVER PLANTING NTS



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SCOTIA WESTERN STATES HOUSING
 PORTLAND, OR 97239
 PRELIMINARY LANDSCAPE PLAN

REVISIONS:

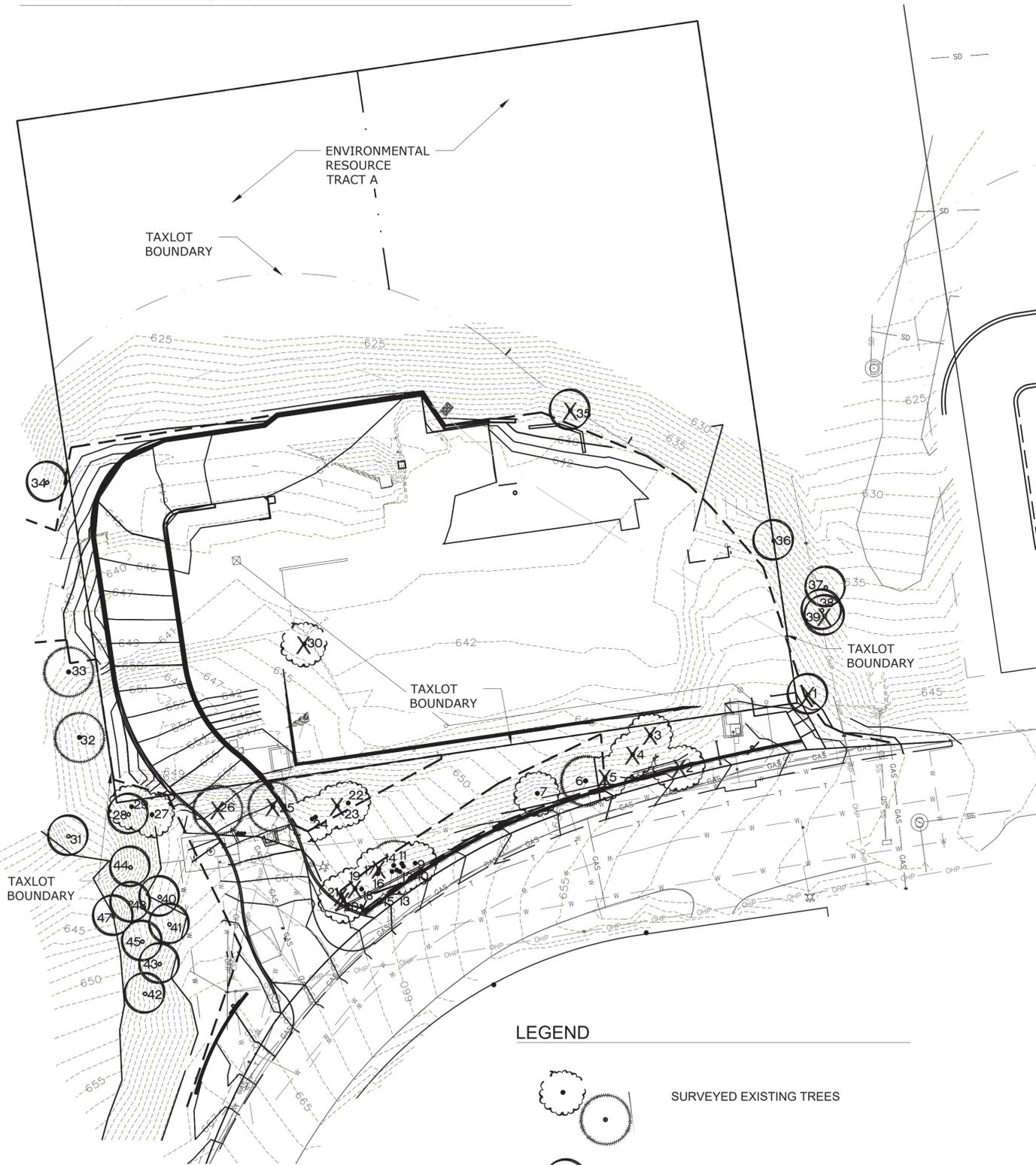
DELTA	DATE	TITLE
Δ	02/05/2021	Rev 1

SCALE: NTS
 DRAWN BY: JJ
 CHECKED BY: DG

SHEET
L-4.1

DATE: 09/23/22
 JOB NO: 1914

TREE PRESERVATION SITE PLAN



LEGEND

- SURVEYED EXISTING TREES
- FIELD LOCATED EXISTING TREES, SEE TREE REPORT
- TREE REPORT TREE NUMBER
- TREE TO BE REMOVED
- TREE PROTECTION FENCING
- NEW UTILITY LINE

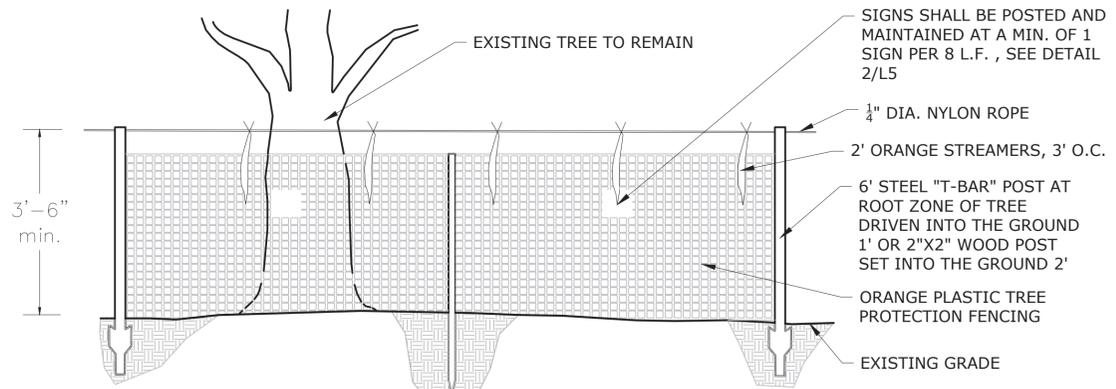
TREE REMOVAL GENERAL NOTES

1. All tree removal shall comply with the Federal Migratory Bird Treaty Act of 1918.
2. Contractors shall obtain all necessary permits for tree removal, tree pruning and tree root removal prior to starting any work with Tree Root Protection Zones.

TREE PROTECTION PLAN STANDARDS

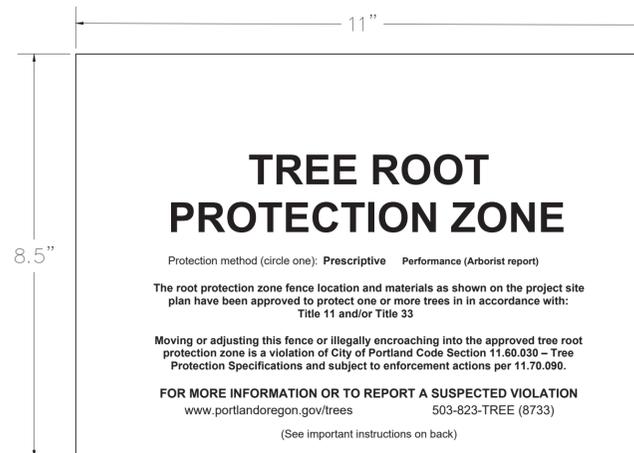
THE FOLLOWING TREE PROTECTION STANDARDS WILL CONDITION ALL PROJECT CONSTRUCTION, EXCAVATION AND TREE REMOVAL AT THE SITE. THE TREE PROTECTION STANDARDS WILL BE NOTED ON THE SITE CONSTRUCTION PLANS:

1. ALL TREE PROTECTION FENCING AS SPECIFIED ON TREE PLAN SHEET MUST BE PLACED BEFORE ANY CONSTRUCTION, EXCAVATION OR STORAGE OF MATERIALS OR EQUIPMENT TAKES PLACE AT THE SITE.
2. ALL TREE PROTECTION FENCING WILL INCORPORATE EROSION CONTROL DEVICES SUFFICIENT TO PREVENT INGRESS OF SPOIL OR LIQUID RUNOFF INTO THE PROTECTED AREAS.
3. NO CONSTRUCTION ACTIVITY, INCLUDING VEHICLE ACCESS, OR ANY STORAGE OF SPOIL, MATERIALS OR EQUIPMENT WILL OCCUR WITHIN THE AREA BOUNDED BY THE TREE PROTECTION FENCE.
4. THE FENCE LOCATIONS AS DETAILED IN THE CONSTRUCTION PLANS WILL NOT BE ALTERED OR BREACHED AT ANY TIME WITHOUT THE EXPLICIT APPROVAL OF CITY OF PORTLAND URBAN FORESTRY INSPECTORS.
5. ANY DAMAGE TO THE TREE PROTECTION FENCING MUST BE REMEDIED WITHIN 24 HRS.
6. REMOVAL OF ANY ROOTS FROM TREES TO BE PRESERVED ON THE PUBLIC RIGHT-OF-WAY MUST ONLY OCCUR UNDER PERMIT APPROVAL FROM CITY OF PORTLAND, URBAN FORESTRY.
7. ANY EXCAVATION WORK, REMOVAL OF SURFACE ASPHALT OR PLACEMENT OF FILL PLACEMENT WITHIN 10-FT OF ANY PRESERVED TREE, AS DETAILED IN THE TREE PROTECTION PLAN, MUST BE DONE IN THE PRESENCE OF A QUALIFIED ARBORIST (A CURRENTLY QUALIFIED ISA CERTIFIED ARBORIST).
8. ANY ROOTS EVIDENT DURING EXCAVATIONS OR SEVERED OR BADLY DAMAGED ROOTS OF ANY PRESERVED TREE MUST BE CUT CLEANLY USING HAND-HELD TOOLS (E.G. HAND SAW, RECIPROCATING SAW, CIRCULAR SAW, ANGLE GRINDER OR OTHER AS APPROVED BY AN ISA CERTIFIED ARBORIST).
9. ANY APPROVED REMOVAL OF ANY PRIVATE OR RIGHT-OF-WAY TREES MUST BE SUPERVISED ON-SITE BY AN ISA CERTIFIED ARBORIST.
10. ANY TREES ON THIS PROJECT REQUIRED TO BE REMOVED WILL BE CUT TO GROUND LEVEL AND THE STUMP EITHER LEFT IN PLACE OR STUMP GROUND BY MACHINERY DESIGNED FOR THAT PURPOSE. NO ROOTS ARE TO BE RIPPED OUT OF THE GROUND.



1 TREE PROTECTION FENCING

NTS



2 TREE ROOT PROTECTION ZONE REQUIRED SIGNAGE

NTS

INSTRUCTIONS

- TREE PROTECTION FENCING MUST BE INSTALLED AND APPROVED PRIOR TO ANY GROUND DISTURBING ACTIVITIES ON SITE AND/OR OTHER INSPECTIONS
- For RS, CO, or SD permits that list a #507 inspection, call 503-823-7000
 - For ZP or Field Issuance Remodel (FIR) permits call 503-823-TREE(8733) and request a tree inspection
 - Tree fencing for other permits is checked with first building inspection
1. Affix this sign to your tree protection fencing using zip or twist ties.
 2. The sign must be placed in a prominent location that, to the greatest extent possible, will allow the sign to be visible from the street.
 3. The sign must remain fixed to the tree protection fencing and fencing must remain in place until the final inspection is approved.
 4. Any changes to the approved tree protection plan must be reviewed and approved through a revision to the original permit.
 5. The sign is available for printing from the City of Portland website: <https://www.portlandoregon.gov/trees/>



IF THIS BAR IS NOT 1" = 20', THEN DRAWING IS NOT TO SCALE

CONSULTANT:



1811 S RIVER DRIVE SUITE 300
 PORTLAND, OR 97201
 503.239.0600

SCOTIA WESTERN STATES HOUSING
 PORTLAND, OR 97239

PRELIMINARY TREE PLAN

REVISIONS:

DELTA	DATE	TITLE
Δ	02/05/2021	Rev 1

SCALE: 1" = 20'

DRAWN BY: JJ

CHECKED BY: DG

SHEET

L-5

DATE: 09/23/22

JOB NO. 1914

1325 SW GIBBS STREET

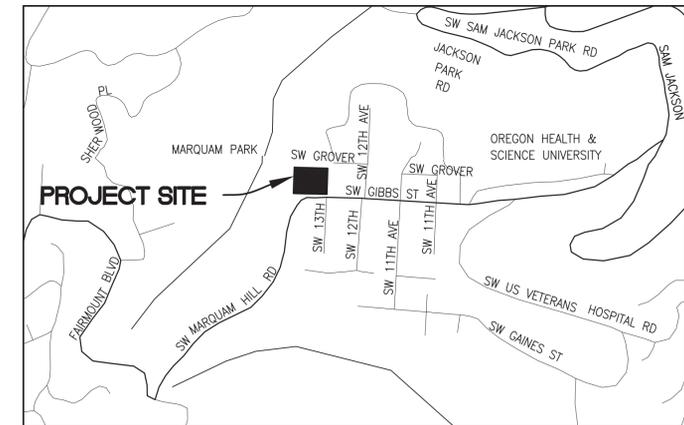
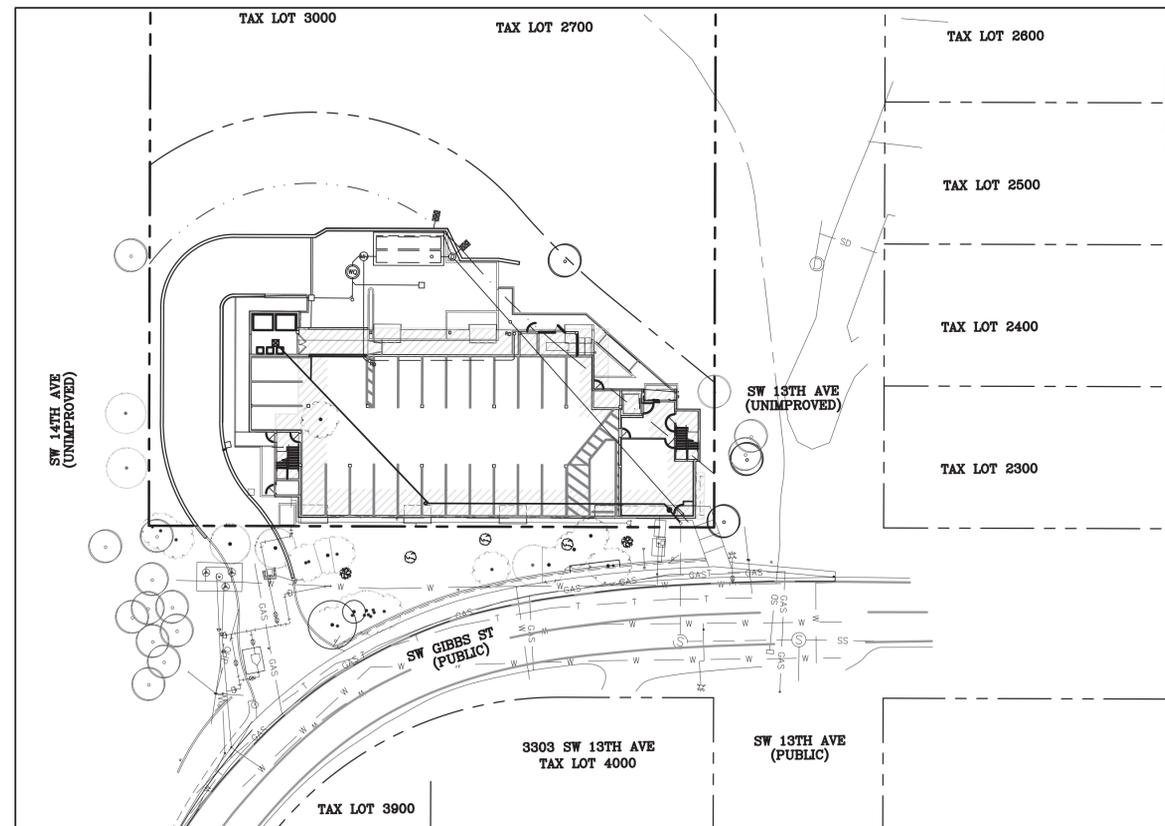
1325 SW GIBBS STREET, PORTLAND, OREGON
REVIEW NUMBER: EA-19-133418

APPLICANTS:

SCOTIA WESTERN STATES HOUSING

CONTACT: SEAN O'NEIL

6340 N. CAMPBELL AVENUE, #240
TUCSON, AZ 85718



VICINITY MAP
1" = 600'

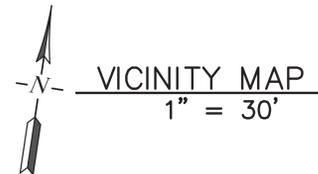
INDEX

SHEET	DESCRIPTION
C0	COVER
C1	EXISTING CONDITIONS AND DEMOLITION PLAN
C2	SEWER AND WATER PLAN
C3	STORM PLAN
C4	SITE PLAN
C5	WALL AND DRIVEWAY PROFILES
C6	GRADING PLAN
C7	EARLY CONSTRUCTION MANAGEMENT PLAN
C8	LATE CONSTRUCTION MANAGEMENT PLAN
C9	EROSION CONSTRUCTION DETAILS
C10	CONSTRUCTION DETAILS
C11	CONSTRUCTION DETAILS

City Of Portland
REVIEWED FOR
CODE COMPLIANCE

Date: 01/03/22
Permit #: 20-194259-000-00-00

NOVEMBER 2019



SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188

BES NOTES / ADDITIONAL PERMIT NOTE:

- BES STORMWATER FACILITY INSPECTION REQUIRED AT TIME OF TIME OF CONSTRUCTION. SEE BES INSPECTION CARD. TO SCHEDULE, CONTACT THE AUTOMATED INSPECTION REQUEST (AIR) SYSTEM AT 503-823-7000 AND REQUEST INSPECTION #487 BES ON-SITE STORMWATER FACILITY EVAL - OR -CONTACT BES AT 503-823-7761 FOR ASSISTANCE.
- SEPARATE BDS COMMERCIAL PLUMBING PERMIT REQUIRED FOR INSPECTION OF DOWNSPOUTS AND PRIVATE STORM SEWER PIPING OUTSIDE OF STORM FACILITY.
- CONNECT TO SEWER SERVICE LATERAL BUILT UNDER PUBLIC WORKS PERMIT JOB NO. TH0903

ATTENTION EXCAVATORS: OREGON LAW REQUIRES COMPLIANCE WITH OAR 952-001-0010 THROUGH 952-001-0090. THESE RULES MAY BE OBTAINED BY CALLING OREGON UTILITY NOTIFICATION CENTER (503) 232-1987. YOU MUST NOTIFY THE CENTER AT LEAST TWO WORKING DAYS BEFORE, BUT NOT MORE THAN TEN DAYS PRIOR TO EXCAVATION.

CALL BEFORE YOU DIG (503) 246-6699

MOST RECENT REVISION TO THIS SET OF PLANS:
11-16-21
6TH SUBMITTAL

- ▲ - 1st COP BDS REVIEW
- ▲ - 2nd COP BDS REVIEW
- ▲ - 3rd COP BDS REVIEW
- ▲ - 4th COP BDS REVIEW
- ▲ - 5th COP BDS REVIEW

NOTICE OF EXCAVATORS:
 ATTENTION: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER.
 (NOTE: THE TELEPHONE NUMBER FOR THE OREGON UTILITY NOTIFICATION CENTER IS (503) 232-1987).

POTENTIAL UNDERGROUND FACILITY OWNER
DIG SAFELY.

CALL THE OREGON ONE-CALL CENTER
 1-800-332-2344

EMERGENCY TELEPHONE NUMBERS

NW NATURAL GAS
 M-F 7am-6pm 503-220-2415
 AFTER HOURS 1-800-882-3377
 COMCAST 503-617-1212
 CENTURY LINK 1-800-573-1311
 PGE 1-800-464-7777
 CITY OF PORTLAND 503-823-4023
 PORTLAND PARKS 503-823-1600

PERMANENT EROSION CONTROL:
 PERMANENT EROSION CONTROL IS REQUIRED WITH THIS PROJECT. CONTRACTOR SHALL ALL REESTABLISH VEGETATION ON ALL DISTURBED SOIL AT EARLIEST POINT PRACTICAL.

SPECIAL CONSTRUCTION NOTES:

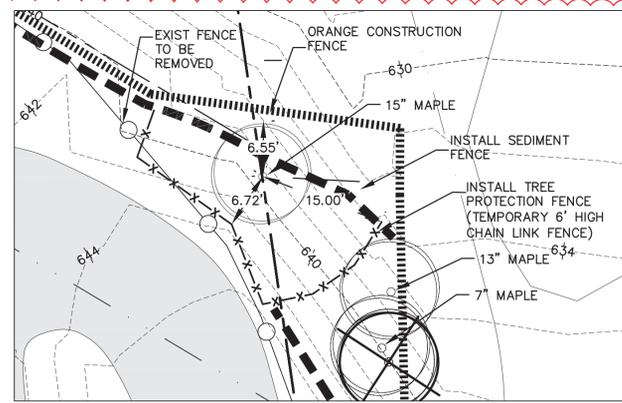
1. THE EXCAVATION SUBGRADE SHALL BE INSPECTED AND APPROVED BY THE SPECIAL INSPECTOR AND BDS PRIOR TO ANY FILL PLACEMENT. THE GROUND SURFACE SHALL BE PREPARED TO RECEIVE THE FILL BY REMOVING UNSUITABLE MATERIAL OR SOILS. ALL BASEMENT WALLS, FOUNDATIONS AND FLOOR SLABS THAT ARE NOT TO BE USED FOR NEW CONSTRUCTION SHALL BE REMOVED PER CITY OF PORTLAND TITLE 24.55
2. THE FILL MATERIAL SHALL BE APPROVED BY THE SPECIAL INSPECTOR PRIOR TO FILL PLACEMENT.
3. PARTICLE SIZES GREATER THAN SIX INCHES MAY NOT BE PLACED WITHIN TEN FEET OF THE FINAL GRADE.
4. FILL TO BE PLACED IN LIFTS NO GREATER THAN NINE INCHES MAXIMUM LOOSE THICKNESS, AND WITHIN THE EFFECTIVE COMPACTION LIMITS OF THE EQUIPMENT BEING USED.
5. FILL TO BE COMPACTED TO AT LEAST 90 PERCENT OF THE MAXIMUM DRY DENSITY, DETERMINED IN ACCORDANCE WITH ASTM D1557.
6. THE SPECIAL INSPECTOR SHALL TEST THE COMPACTION OF THE FILL AS THE FILL IS BEING PLACED EVERY TWO FEET OF ELEVATION OR EVERY 250 CUBIC YARDS, WHICHEVER PROVIDES THE GREATEST NUMBER OF TESTS, TO DEMONSTRATE UNIFORM COMPACTION THROUGHOUT THE FILL VOLUME
7. THE FINAL GRADES OF THE FILLED EXCAVATION SHALL MEET THE GRADES OF THE ADJACENT GROUND SURFACE AND SHALL NOT EXCEED A GRADIENT OF 2H:1V.
8. THE SPECIAL INSPECTOR SHALL OBSERVE THE WORK FOR CONFORMANCE WITH APPROVED PERMIT DOCUMENTS AND THESE BACKFILL REQUIREMENTS. THE SPECIAL INSPECTOR SHALL SUBMIT A FINAL SUMMARY REPORT TO BDS'S SPECIAL INSPECTIONS SECTION STATING WHETHER THE BACKFILL WORK WAS COMPLETED IN CONFORMANCE WITH THE APPROVED PERMIT DOCUMENTS AND THESE REQUIREMENTS.

DEMOLITION NOTES:

1. AT 1325 SW GIBBS STREET, TAX LOT 2800 AND 2900, CONTRACTOR TO REMOVE ALL HARD SURFACES, RETAINING WALLS, AND BUILDING STRUCTURES. ALL BASEMENT WALL, FOUNDATIONS AND FLOOR SLABS THAT ARE NOT TO BE USED FOR NEW CONSTRUCTION SHALL BE REMOVED PER CITY OF PORTLAND TITLE 24.55
2. ALL PRIVATE UTILITIES TO BE PROPERLY REMOVED AND ABANDONED. CONTRACTOR TO COORDINATE WORK WITH UTILITY PROVIDES.
3. UNDERGROUND PARKING SPOTS, IF ENCOUNTERED TO BE REMOVED AND TEMPORARY SHORING INSTALLED IF NECESSARY.
4. ALL OIL TANKS ENCOUNTERED TO BE DECOMMISSIONED AND REMOVED PER CURRENT DEQ RULES AND REGULATIONS
5. ALL DEMOLITION WORK IN THE PUBLIC RIGHT OF WAY SHALL BE COMPLETED UNDER SEPARATE PBOT PERMIT # TH0903. THIS INCLUDES REMOVAL OF HARD SURFACES, RETAINING WALLS, AND TREES IN PUBLIC R.O.W.

DEWATERING NOTES:

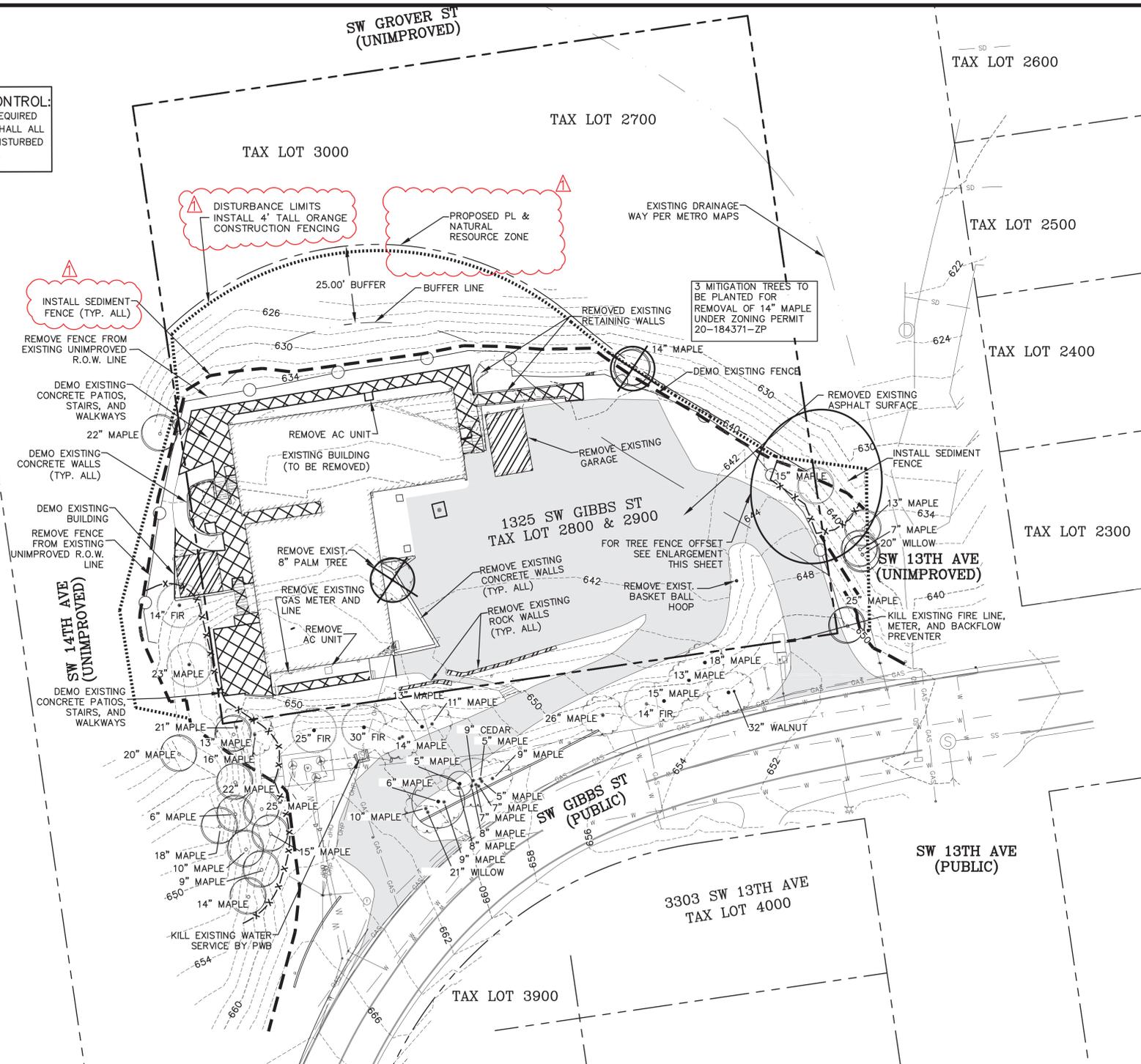
1. CONSTRUCTION DEWATERING NOT AUTHORIZED. IF REQUIRED: BATCH DISCHARGE AUTHORIZATION PERMIT IS REQUIRED FOR TEMPORARY DISCHARGES OF GROUNDWATER OR CONSTRUCTION RELATED STORMWATER (CHANNELIZED, COLLECTED, AND/OR PUMPED) TO THE CITY'S PUBLIC SANITARY OR STORM SEWER SYSTEM. IF DE-WATERING TO A CITY SANITARY OR STORM SEWER SYSTEM IS NECESSARY, PRE-AUTHORIZATION MUST BE OBTAINED FROM THE BES PERTREATMENT OR STORMWATER PROGRAMS. CALL 503-823-5600 OR CONTACT JOE BLANCO (JOE.BLANCO@PORTLANDOREGON.GOV) FOR BATCH DISCHARGE INFORMATION AND REQUESTS.



TREE FENCE ENLARGEMENT

SCALE: 1" = 10'

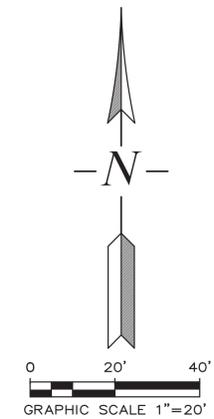
ENLARGEMENT ADDED THROUGH DEMO PERMIT REVIEW. SEDIMENT FENCE ADDED THROUGH 1ST REVIEW



DEMO PLAN

SCALE: 1" = 20'

DEMO WORK NOTE:
 ALL ONSITE DEMO WORK SHOWN ON THIS PLAN SET IS PROPOSED UNDER A SEPARATE PERMIT (20-174739/829 CO) AND IS NOT PART OF THIS PERMIT'S PROJECT SCOPE.



City of Portland
 REVIEWED FOR
 CODE COMPLIANCE
 Date: 01/03/22
 Permit #: 20-19429-00-00-00



RECEIVED 10/20/2021

REVISIONS	BY
5/13/2020-PL UPDATE-DEMO	JMF
8/31/2020-CITY COMMENTS	JJM
10/13/2020-CITY COMMENTS	JJM
10/16/2020-ENLARGEMENT	JJM
12/11/2020-BUFFER LINE	JJM
2/05/2021-1ST BDS REVIEW	JMF

1325 SW GIBBS STREET
 SCOTIA WESTERN STATES HOUSING

Existing Condition and Demolition Plan

SISUL ENGINEERING
 375 PORTLAND AVENUE
 GLADSTONE, OREGON 97027
 (503) 657-0188
 DRAWING: 19-029-BDS-DEMO.dwg

DATE NOV. 2019
 SCALE 1"=20'
 DRAWN JMF
 JOB SGL19-029
 SHEET

OF 11 SHEETS
 Exhibit C.7

TRASH ENCLOSURE NOTES:

- TRASH ENCLOSURE DRAINS MUST BE TRAPPED, PRIMED, AND VENTED. IF AN AREA DRAIN IS USED IT MUST BE OF THE SANITARY TYPE WITH GAS-TIGHT THREADED CLEANOUT. TRASH ENCLOSURE DRAIN DETAILS AREA FOUND ON THE PRIVATE PLUMBING PLANS.
- CONTRACTOR TO STOP 3034 PVC PIPE 5 FEET BEFORE TRASH ENCLOSURE AND CHANGE TO APPROPRIATE PIPE. SEE PRIVATE SANITARY SEWER NOTE #1

PRIVATE WATERLINE NOTES:

- WATERLINE FACILITIES BEHIND THE METER AND BACKFLOW PREVENTOR ARE PRIVATE IMPROVEMENTS. A CITY OF PORTLAND PLUMBING PERMIT IS REQUIRED.
- WATERLINE PIPE SHALL MEET THE REQUIREMENTS OF THE 2017 OREGON STATE PLUMBING SPECIALTY CODE.
- BACKFLOW DEVICES ARE REQUIRED ON THE FIRE, DOMESTIC, AND IRRIGATION WATER SUPPLY LINES. THE DOMESTIC SYSTEMS BACKFLOW PREVENTOR SHALL BE A PI RPBA (PREMISES ISOLATION REDUCED PRESSURE BACKFLOW ASSEMBLY). THE FIRE LINES BACKFLOW DEVICE SHALL BE PI DCDA (DOUBLE CHECK DETECTOR ASSEMBLY). ALL BACKFLOW DEVICES SHALL BE INSTALLED PER WATER BUREAU REQUIREMENTS. BACKFLOW DEVICES SHALL BE FROM THE OREGON STATE HEALTH DIVISION "APPROVED BACKFLOW PREVENTION DEVICES" LIST AND ACCEPTABLE TO THE CITY OF PORTLAND BUILDING DEPARTMENT AND THE PORTLAND WATER BUREAU.
- WATERLINE AND APPURTENANCES ARE TO CONFORM TO MATERIALS, INSTALLATION AND TESTING REQUIREMENTS OF THE 2017 OREGON STATE PLUMBING SPECIALTY CODE, THE CITY OF PORTLAND BUILDING DEPARTMENT AND THE OREGON HEALTH DIVISION ADMINISTRATIVE RULES, CHAPTER 333.
- ALL PIPE SHALL BE BEDDED AND BACKFILLED TO SURFACE WITH CRUSHED AGGREGATE BACKFILL (3/4-0"). CRUSHED AGGREGATE BACKFILL SHALL BE COMPACTED TO 95% OF MAXIMUM DRY DENSITY PER ASTM D-1557 TEST METHOD.

WATER QUALITY BACKFLOW DEVICES:

PI BACKFLOW:
PREMISES-ISOLATION BACKFLOW PROTECTION REQUIRED BY WATER QUALITY BACKFLOW (WQBF) REVIEW (503-823-7480).
WATER BUREAU BACKFLOW ASSEMBLY INSTALLATION REQUIREMENTS:
WWW.PORTLANDOREGON.GOV/WATER/BACKFLOWINSTALLATIONREQUIREMENTS
TITLE 21.12.320, 28.08.020 AND/ OR OAR 333-061-0070, 333-061-0071
PLAN APPROVED FOR CONSTRUCTION: ERROR AND OMISSIONS EXCEPTED.

DOMESTIC WATER SERVICE:

BACKFLOW PREVENTION ASSEMBLIES MUST CONFORM TO EPA LEAD FREE REQUIREMENTS. REDUCED PRESSURE BACKFLOW ASSEMBLY (RPBA) REQUIRED. ASSEMBLIES APPROVE TO BE INSTALLED INSIDE BUILDINGS OR STRUCTURES: INSTALLATION MUST BE LOCATED AT THE WATER RISER, ON THE CENTERLINE OF THE CITY WATER SERVICE AS IT RUNS PERPENDICULAR FROM THE RIGHT-OF-WAY. THE RISER MUST BE LOCATED 12" FROM THE FOUNDATION WALL TO CENTERLINE OF PIPE. WHERE APPLICABLE, FOUNDATIONS AND/OR FOOTINGS MUST BE ENGINEERED TO ACCOMMODATE THE RISER LOCATION. WHEN REQUIRED, WATER PIPING MUST BE SLEEVED APPROPRIATELY. PIPING MUST BE SLEEVED COMPLIANT WITH FIRE CODE NFPA 12 - 9.3.4 AND/OR PLUMBING CODE 313.9.

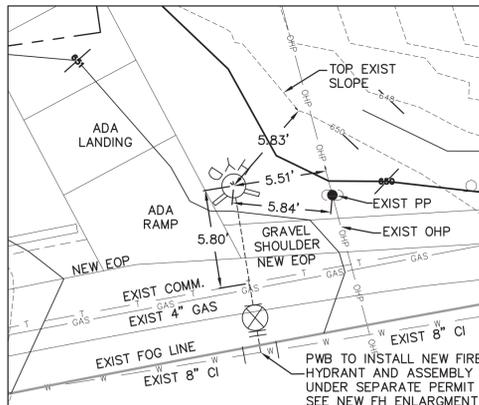
IF SERVICE ENTERS BUILDING GREATER THAN 5' AFF (ABOVE FINISH FLOOR) THE ASSEMBLY MUST BE DROPPED DOWN TO BE AT 5' AFF TO TOP OF ASSEMBLY BODY. IF SERVICE ENTERS BUILDING AT LESS THAN 1' AFF THE ASSEMBLY MUST BE RAISED TO BE AT 1' AFF TO BOTTOM OF ASSEMBLY BODY. SEE WATER BUREAU BACKFLOW ASSEMBLY INSTALLATION REQUIREMENT PAGE 1, ITEM 1 FOR OTHER CRITICAL ASSEMBLY AND RISER REQUIREMENTS. INSTALLATION OF PREMISE-ISOLATION BACKFLOW ASSEMBLY WILL CREATE A CLOSED SYSTEM AND MAY RESULT IN PROBLEMS ASSOCIATED WITH THERMAL EXPANSION. LANDSCAPE IRRIGATION POINT-OF-CONNECTION (POC) TO DOMESTIC PLUMBING SYSTEM MUST OCCUR DOWNSTREAM OF THE DOMESTIC WATER SERVICE PREMISES-ISOLATION (PI) BACKFLOW PROTECTION.

FIRE SPRINKLER WATER SERVICE:

IT IS UNLAWFUL TO USE TO USE A DEDICATED FIRE SPRINKLER WATER SUPPLY FOR PURPOSES OTHER THAN SUPPRESSION AND CODE REQUIRED SYSTEM TESTING. DOUBLE CHECK DETECTOR ASSEMBLY (DCDA) REQUIRED. MUST BE INSTALLED ON PRIVATE PROPERTY LINE ON THE CENTERLINE OF THE SERVICE IMMEDIATELY ADJACENT TO SERVICE CONNECTION. NEW WATER SERVICES WILL BE INSTALLED AT A DEPTH OF 3-4 FT. IT IS THE RESPONSIBILITY OF THE OWNER/CONTRACTOR TO MAKE THE PROPERTY SIDE CONNECTION. (NOTE DCDA IS SHOWN @ TOE OF SLOPE BECAUSE ROW LINE LANDS IN STEEP BANK WITH SLOPE GREATER THAN 2:1)

PUBLIC WATER SERVICE NOTES:

- WATER BUREAU TO DO ALL WATER SERVICE, HYDRANT, AND WATER MAINS WORK IN THE PUBLIC RIGHT-OF-WAY UP TO THE POINT OF CONNECTION.
- WATER SERVICES WILL BE INSTALLED AT A DEPTH OF 3'-4' WITH A SHORT STUB INSTALLED ON THE PROPERTY SIDE OF THE METER OR VALVE. EXCAVATION WILL BE BACKFILLED BY WATER BUREAU AT TIME OF SERVICE INSTALLATION. CONTRACTOR RESPONSIBLE TO MAKE PROPERTY SIDE CONNECTION TO METER OR VALVE. EXCAVATION AND BACKFILLING REQUIRED FOR CONNECTION IS RESPONSIBILITY OF CONTRACTOR.
- TO OBTAIN WATER SERVICE/WORK AND PAY SYSTEM DEVELOPMENT CHARGES, SUBMIT A COMPLETED FEE STATEMENT REQUEST (W-6), IF NOT SUBMITTED DURING REVIEW PROCESS. FORM LOCATED AT [HTTP://WWW.PORTLANDOREGON.GOV/WATER/ARTICLE/357251](http://www.portlandoregon.gov/water/article/357251) TO DEVREV@PORTLANDOREGON.GOV. EMAIL SUBJECT LINE SHOULD CONTAIN "FEE STATEMENT REQUEST" AND SITE ADDRESS.
- BUILDING PERMIT MUST BE ISSUED AND PUBLIC WORKS FINAL PLAN MUST BE APPROVED BEFORE FEE STATEMENT CAN BE PREPARED. FEE STATEMENT AND PAYMENT INSTRUCTIONS WILL BE EMAILED TO APPLICANT LISTED ON W-6 FORM.
- IF SITE SPECIFIC ESTIMATE IS REQUIRED, ALLOW ADDITIONAL 3 WEEKS FOR FEE STATEMENT PREPARATIONS.
- 48-72 HOURS AFTER FEES ARE PAID, SCHEDULE WORK BY CALLING PWB SCHEDULING (503-823-1526). SERVICE WORK MAY BEGIN 4-6 WEEKS AFTER RECEIPT OF PBOT STREET OPENING PERMIT. PERMITTEE/CONTRACTOR IS RESPONSIBLE FOR MARKING LOCATION OF ALL SERVICES AND HYDRANTS WITH FINISHED GRADE AND CURB LOCATIONS IDENTIFIED. ALL MARKED LOCATIONS MUST MATCH THE APPROVED LOCATION ON BUILDING PERMIT AND PUBLIC WORKS PLANS OR A REVISION WILL BE REQUIRED FOR REVIEW AND APPROVAL PRIOR TO INSTALLATION. THE APPLICANT IS RESPONSIBLE FOR THE ENTIRE COST OF RECOATING ANY INSTALLED SERVICE OR HYDRANT MARKED IN ERROR.
- IF SITE CONDITIONS ARE SIGNIFICANTLY DIFFERENT FROM THOSE SHOWN ON APPROVED PLANS, APPLICANT MAY BE RESPONSIBLE FOR ADDITIONAL FEES.



NOTICE TO EXCAVATORS:
ATTENTION: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER. (NOTE: THE TELEPHONE NUMBER FOR THE OREGON UTILITY NOTIFICATION CENTER IS (503)-232-1987).

POTENTIAL UNDERGROUND FACILITY OWNERS

Dig Safely.

Call the Oregon One-Call Center
DIAL 811 or 1-800-332-2344

EMERGENCY TELEPHONE NUMBERS

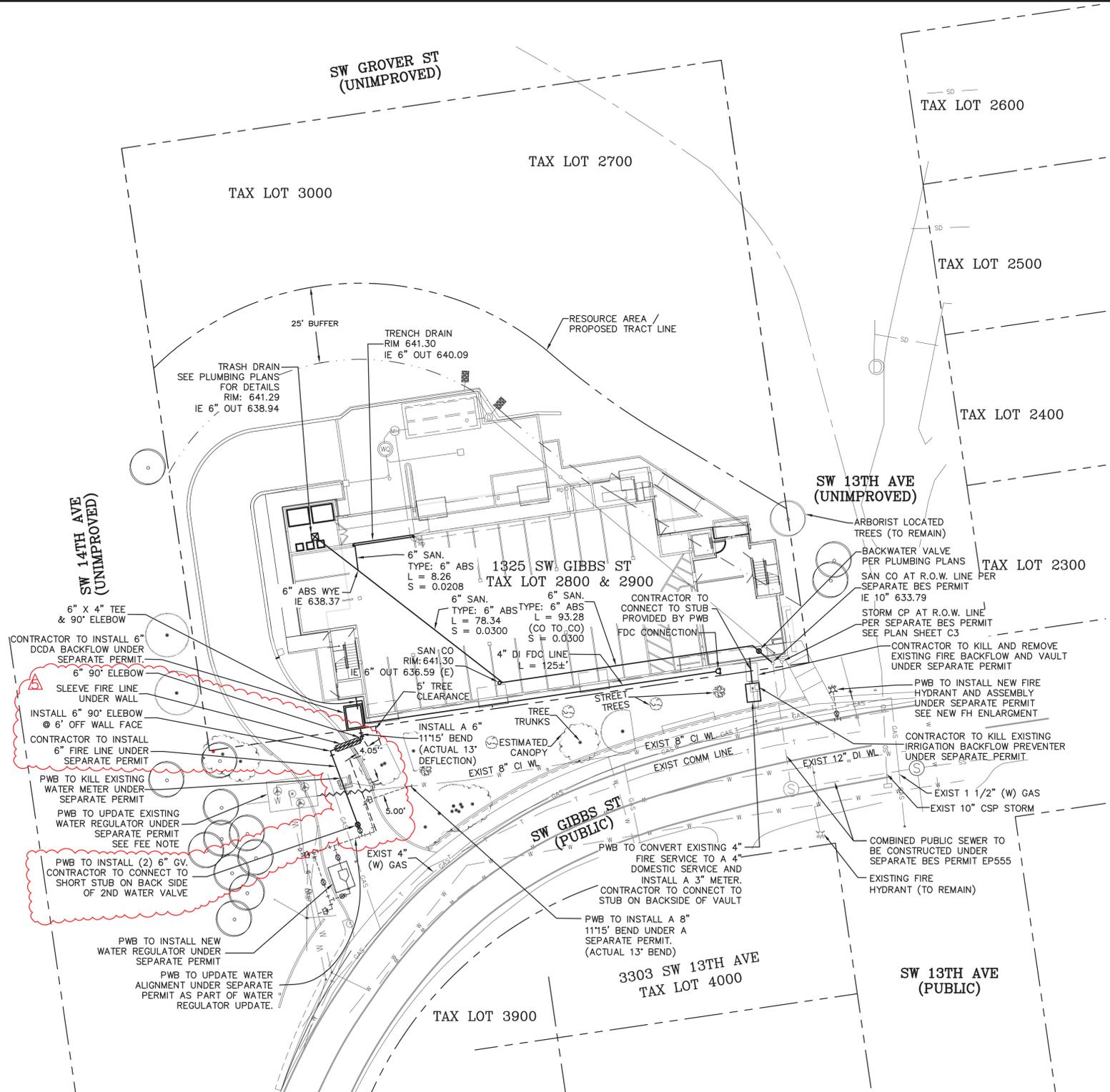
NW NATURAL GAS	
M-F 7am-6pm	503-220-2415
AFTER HOURS	1-800-882-3377
COMCAST	503-617-1212
CENTURY LINK	1-800-573-1311
PGE	1-800-464-7777
CITY OF PORTLAND	503-823-4023
PORTLAND PARKS	503-823-1600

WATER REGULATOR FEE NOTE:
PWB WILL REQUIRE THAT THE CONSTRUCTION FEE FOR THE WATER REGULATOR BE PAID PRIOR TO WORK COMMENCING. FEE STATEMENT (20-274) WAS SENT TO THE DEVELOPER. PWB HAS NOTED THAT DESIGN FEE FOR THE REGULATOR WERE RECEIVED ON 4/29/21.

FDC CONNECTION:
THE FDC CONNECTION RISER SHALL BE EXTENDED VERTICALLY UP DRIVEWAY RETAINING WALL TO AN ELEVATION ABOVE THE DRIVEWAY. FDC CONNECTION TO BE INSTALLED SO FIRE PERSONNEL CAN EASILY ACCESS FROM DRIVEWAY'S SURFACE.

PRIVATE SANITARY SEWER NOTES:

- SANITARY SEWER PIPE LOCATED MORE THAN (5) FIVE FEET FROM ANY STRUCTURE MAY BE PVC PIPE CONFORMING TO ASTM D3034-SDR 35 WITH JOINTS BEING ELASTOMERIC GASKET CONFORMING TO ASTM 3212. OTHERWISE SANITARY SEWER PIPE SHALL BE ABS PIPING. ALL SANITARY SEWER LATERALS SHALL ENTER IN THROUGH A WYE FITTING.
- CLEANOUT PIPE, FITTINGS AND JOINTS SHALL BE THE SAME SPECIFICATIONS AS FOR PIPE. CLEANOUTS SHALL MEET THE REQUIREMENTS OF SECTION 707.0 OF THE 2017 OREGON STATE PLUMBING SPECIALTY CODE. CLEANOUTS IN PAVED AREAS ARE TO HAVE AN IRON FRAME AND COVER.
- ALL PIPE SHALL BE BEDDED AND BACKFILLED TO SURFACE WITH CRUSHED AGGREGATE BACKFILL (3/4-0"). CRUSHED AGGREGATE BACKFILL SHALL BE COMPACTED TO 95% OF MAXIMUM DRY DENSITY PER ASTM D-1557 TEST METHOD.
- TESTING ON THE SANITARY SEWER SYSTEM MAY BE REQUIRED AT THE DISCRETION OF THE ENGINEER, THE OWNER OR THE PORTLAND BUILDING DEPARTMENT. TESTING SHALL CONFORM WITH SECTION 712.0 OF THE 2017 OREGON STATE PLUMBING SPECIALTY CODE.
- ALL MATERIALS, INSTALLATION, TESTS AND INSPECTIONS TO BE MADE IN STRICT ACCORDANCE WITH THE 2017 OREGON STATE PLUMBING SPECIALTY CODE AND THE CITY OF PORTLAND BUILDING DEPARTMENT.



WATER OWNERSHIP NOTE:
PWB TYPICALLY MAINTAINS FIRE LINES BETWEEN THE FIRST AND SECOND VALVE.
FOR THE EXISTING FIRE LINE PROPOSED TO BE CONVERTED TO DOMESTIC WL PWB HAS MAINTENANCE RIGHT TO 2ND VALVE FROM WM. CONTRACTOR TO COORDINATE KILLING AND IRRIGATION BACKFLOW ASSEMBLIES WITH PWB.



EXPIRES: 12/31/2021

REVISIONS	BY
DESIGN UPDATE (7-10-2020)	JMF
COP 1ST REVIEW (2-05-2021)	JMF
COP 2nd REVIEW (4-21-2021)	JMF
COP 3rd REVIEW (6-07-2021)	JMF
COP 4TH REVIEW (10-12-2021)	JMF
COP 5TH REVIEW (11-16-2021)	JMF

1325 SW GIBBS STREET
SCOTIA WESTERN STATES HOUSING

Sewer & Water Plan

SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 19-1029 - B05-SAN&WAT.dwg

DATE NOV. 2019
SCALE 1"=20'
DRAWN JMF
JOB SGL19-029
SHEET

C2

OF 11 SHEETS

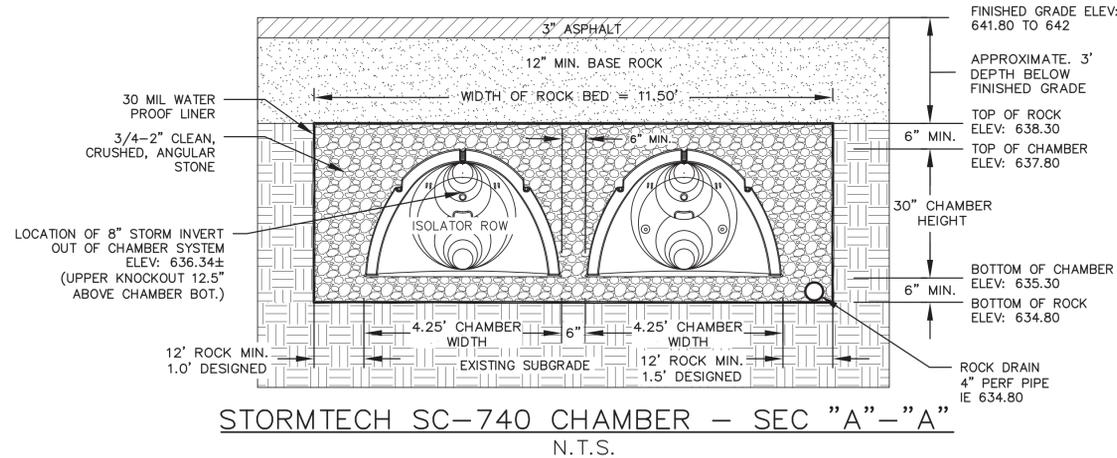
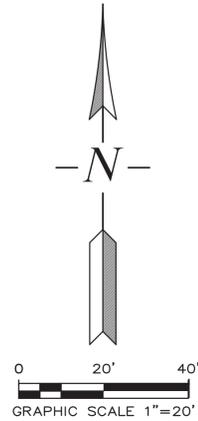
Exhibit C.7

STORM DRAIN NOTES:

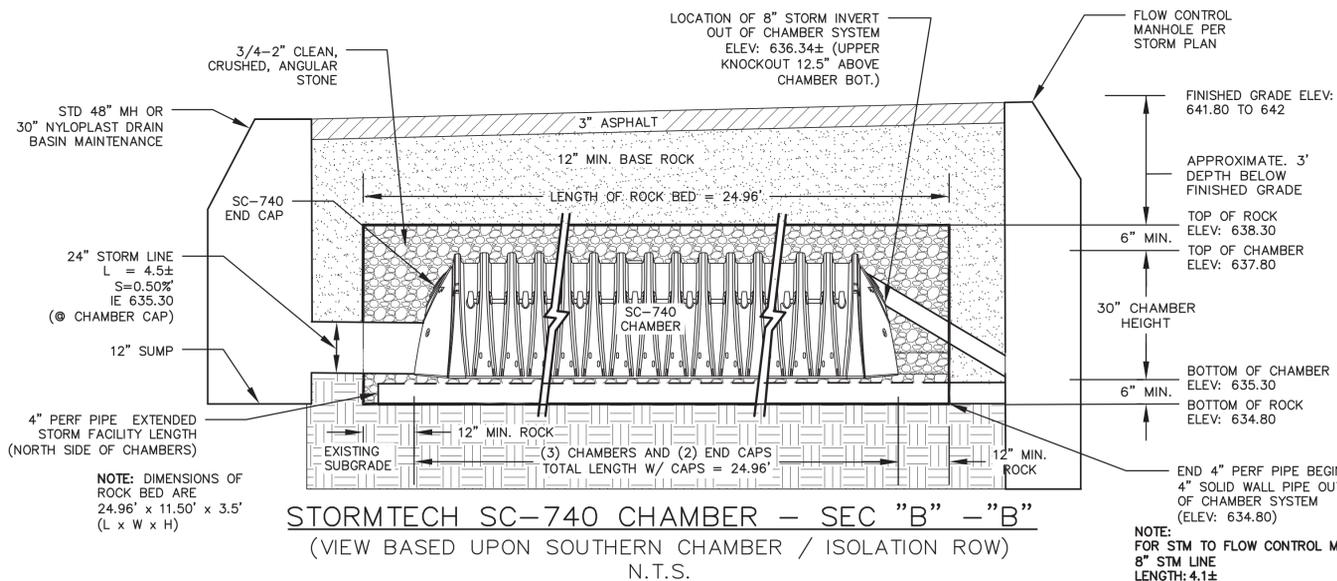
- STORM DRAIN PIPE INSTALLED MORE THAN (2) TWO FEET FROM ANY BUILDING. SHALL BE PVC PIPE CONFORMING TO ASTM D3034-SDR 35, UNLESS OTHERWISE SPECIFIED ON PLANS. PIPE INSTALLED WITHIN (2) TWO FEET OF ANY BUILDING SHALL BE SCHEDULE 40 PVC DWV PIPE OR SCHEDULE 40 ABS DWV PIPE. DUCTILE IRON PIPE SHALL BE CLASS 52, CEMENT-MORTAR LINED AND SEAL COATED AND SHALL CONFORM WITH ASTM 536, AWWA C151, AWWA C104, AND AWWA C111. BRANCH STORM DRAIN LINES SHALL ENTER MAINS THROUGH WYE TYPE FITTINGS.
- CATCHBASINS MAY BE PRE-FABRICATED CATCHBASINS. CATCHBASINS ARE TO BE "LYNCH" TYPE WITH 18" SUMPS. ROOF DRAINS MUST BE PIPED DIRECTLY INTO THE STORM DRAIN PIPING.
- CLEANOUT PIPE, FITTINGS AND JOINTS SHALL BE THE SAME SPECIFICATIONS AS FOR PIPE. CLEANOUTS SHALL MEET THE REQUIREMENTS OF SECTION 707.0 OF THE OREGON STATE PLUMBING SPECIALTY CODE. CLEANOUTS IN PAVED AREAS ARE TO HAVE AN IRON FRAME AND COVER.
- BACKWATER VALVE REQUIRED AT THE DOWNSTREAM END OF ALL PERFORATED PIPE RUNS, INCLUDING FOOT DRAINS AND WHEREVER ELSE PERFORATED PIPE IS NOTED ON THE CONSTRUCTION PLANS AND CONNECTION TO SOLID WALL PIPE. BACKWATER VALVE SHALL BE CONSTRUCTED OF ABS PIPING WHEN IT LIES WITHIN 2 FEET OF BUILDINGS OR ELSEWHERE WHERE PVC WOULD NOT BE ALLOWED UNDER THE OREGON SPECIALTY PLUMBING CODE.
- BACKWATER VALVE SHALL BE NDS GRAVITY BACKWATER VALVE OR APPROVED EQUAL.
- ALL PIPE SHALL BE BEDDED AND BACKFILLED TO SURFACE WITH CRUSHED AGGREGATE BACKFILL (3/4-0"). CRUSHED AGGREGATE BACKFILL SHALL BE COMPACTED TO 95% OF MAXIMUM DRY DENSITY PER ASTM D-1557 TEST METHOD.
- ALL MATERIALS, INSTALLATION, TESTS AND INSPECTIONS TO BE MADE IN STRICT ACCORDANCE WITH THE 2017 OREGON STATE PLUMBING SPECIALTY CODE AND THE CITY OF PORTLAND BUILDING DEPARTMENT.

STORMTECH CHAMBER WATER PROOF NOTES:

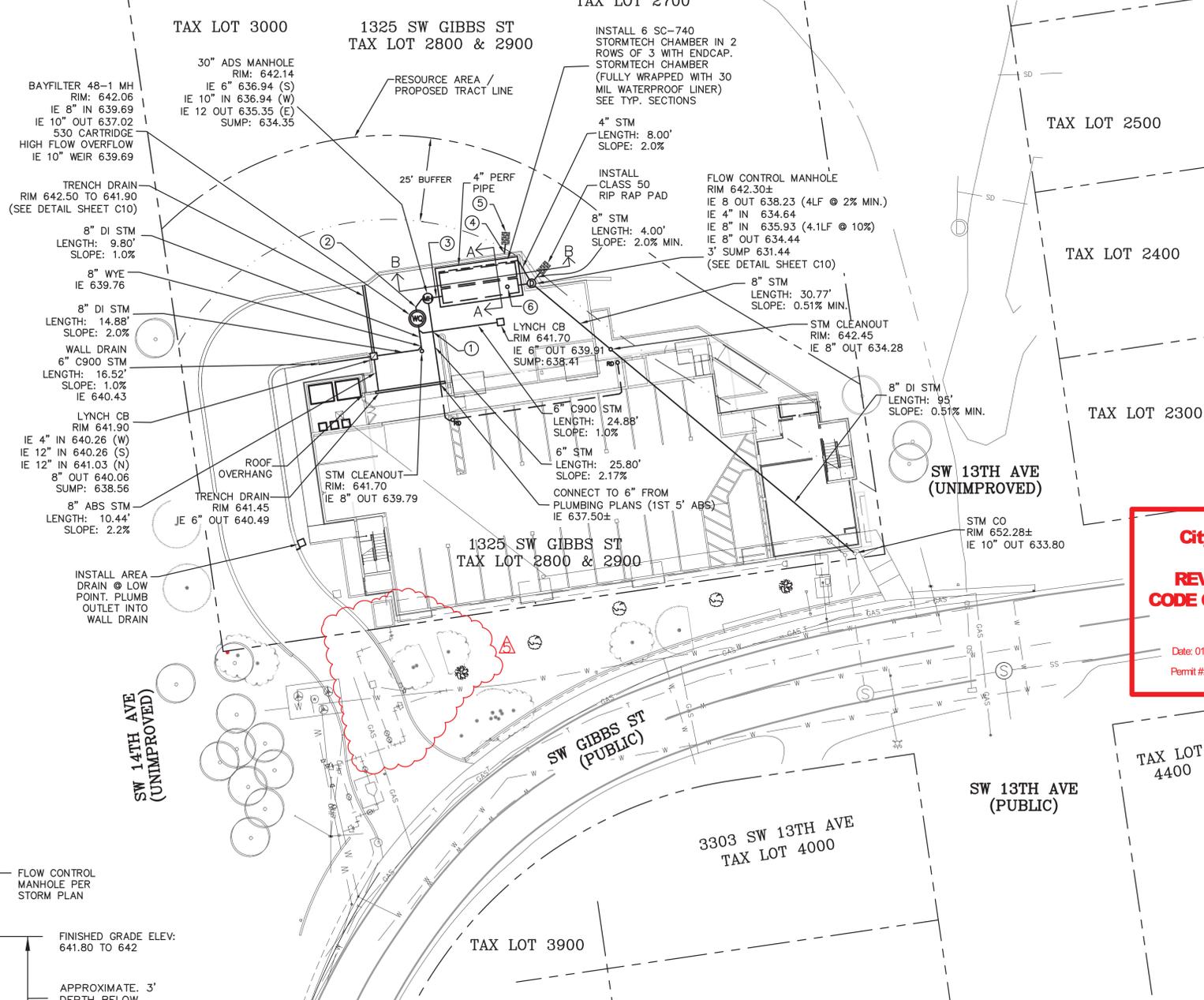
- THE STORMTECH CHAMBERS ARE REQUIRED TO BE WATER TIGHT AND INSTALLED IN A MANNER TO PREVENT STORMWATER FROM SPRINGING AND SEEPING FACILITY.
- WATER PROOF LINER TO BE INSTALLED IN ACCORDANCE WITH ADS REQUIREMENTS AS OUTLINED IN STORMTECH THERMOPLASTIC BROCHURE. SEE BROCHURE @ https://www.stormtech.com/download_files/pdf/AD501216StormTechThermoplasticBrochure.pdf
- CONTRACTOR SHALL INSTALL BOOTS AND HOSE CLAMPS TIGHTENED TO THE VALLEY OF PIPE AS NECESSARY TO PREVENT STORMWATER SPRINGING AND SEEPAGE FROM FACILITY AT PIPE CONNECTIONS.



STORMTECH SC-740 CHAMBER - SEC "A" - "A"
N.T.S.



STORMTECH SC-740 CHAMBER - SEC "B" - "B"
(VIEW BASED UPON SOUTHERN CHAMBER / ISOLATION ROW)
N.T.S.



STORM PLAN
SCALE: 1" = 20'

- STORM CONSTRUCTION NOTES:**
- 6" STORM DRAIN LINE FROM PLUMBING PLAN TO CROSS UNDER 6" STORM DRAIN LINE FROM CATCH BASIN. LINES TO CROSS WITH 1" MINIMUM CLEARANCE.
IE CB LEAD @ BOT: 639.71 IE 6" FROM PLUMBING: 637.16 (TOP 637.66)
 - INSTALL 8.25 LF OF 10" 3034 PVC STORM LINE @ 1.0% SLOPE (LINE INCLUDES (2) 45' BENDS AND (1) TEE (SEE NOTE 3 BELOW)
 - INSTALL 4.5± LF OF 24" STORM LINE @ 0.5% SLOPE
 - AT WALL STA 2+51 (SHEETS C4 & C5) INSTALL 4" WALL DRAIN START IE: 633.42 END IE: 633.36± L = 3'±, S=2.0% MIN.
 - INSTALL CLASS 50 RIP RAP PAD
 - OPTIONAL INSPECTION PORT RECOMMENDED BY ADS- SEE DETAIL ON SHEET C11 INSPECTION PORT RIM: 641.85±

City of Portland
REVIEWED FOR
CODE COMPLIANCE

Date: 01/03/22
Permit #: 20-19429-00-00-00

REVISIONS	BY
PRELIM PLANS (11-16-19)	JMF
REVISED PLANS (4-21-20)	JJM
COP 1ST REVIEW (2-05-2021)	JMF
COP 2ND REVIEW (4-21-2021)	JMF
PUBLIC REVIEW (7-07-2021)	JMF
COP 5TH REVIEW (11-16-2021)	JMF

1325 SW GIBBS STREET
SCOTIA WESTERN STATES HOUSING

Storm Plan

SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 19-029 - B05-Storm.dwg

DATE: NOV. 2019
SCALE: 1"=20'
DRAWN: JMF
JOB: SGL19-029
SHEET: C3

REGISTERED PROFESSIONAL ENGINEER
72761PE
OREGON
JAMES M. FOX
EXPIRES: 12/31/2021

PRIVATE PAVING, CURB, & SIDEWALK NOTES:

1. THE PRIVATE DRIVEWAYS SHALL BE PROPERLY STRIPPED AND APPROVED BY THE GEOTECHNICAL ENGINEER PRIOR TO PLACING STRUCTURAL FILL OR BASE ROCK.
2. THE STRUCTURAL SECTION FOR PRIVATE DRIVEWAYS SHALL CONSIST OF A MINIMUM OF 3" OF LEVEL 2, 1/2" DENSE ASPHALT CONCRETE, OVER 8" CRUSHED AGGREGATE (1"-0") BASE COURSE, OVER GEOTEXTILE FABRIC SUCH AS MIRAFI 500X OR EQUIVALENT.
3. THE ASPHALT CONCRETE SHALL BE THOROUGHLY AND UNIFORMLY COMPACTED BY ROLLING UNTIL IT IS COMPACTED TO AT LEAST 91% OF A RICE DENSITY. ALL FINAL GRADES SHALL BE WITHIN +/- 0.1 INCH OF THAT SPECIFIED.
4. ALL PAVEMENT SUBGRADES ARE REQUIRED TO PASS A PROOFROLL PRIOR TO PAVING. SOFT AREAS SHOULD BE REPAIRED BY OVER EXCAVATING THE AREA AND INSTALLING A STABILIZATION GEOTEXTILE, SUCH AS MIRAFI 500X OR EQUIVALENT.
5. THE CRUSHED AGGREGATE LEVELING AND BASE COURSES SHALL BE COMPACTED TO 95% MAXIMUM DRY DENSITY OR AS APPROVED BY THE GEOTECHNICAL ENGINEER. OWNER MAINTAINS THE RIGHT TO REJECT UNACCEPTABLE STONE.
6. CONCRETE CURBS AND SIDEWALKS SHALL BE CONSTRUCTED WITH CONCRETE HAVING A MINIMUM COMPRESSIVE STRENGTH OF 3,300 PSI. SIDEWALK CONCRETE DEPTH SHALL BE 4" AND SHALL BE PLACED ON A MINIMUM 4" CRUSHED ROCK (3/4"-0") BASE COMPACTED TO 95% MAXIMUM DRY DENSITY OR AS APPROVED BY THE GEOTECHNICAL ENGINEER. SIDEWALKS SHALL SLOPE TOWARD PAVEMENT AT A MAXIMUM GRADE OF 2% AND A MINIMUM GRADE OF 1%.

URBAN FORESTRY NOTES:

1. STREET TREE ACTIVITY IS NOT APPROVED AS PART OF THIS PERMIT. SEE PUBLIC WORKS PERMIT TH0903 FOR APPROVED RIGHT OF WAY DETAILS AND STREET TREE ACTIVITY.

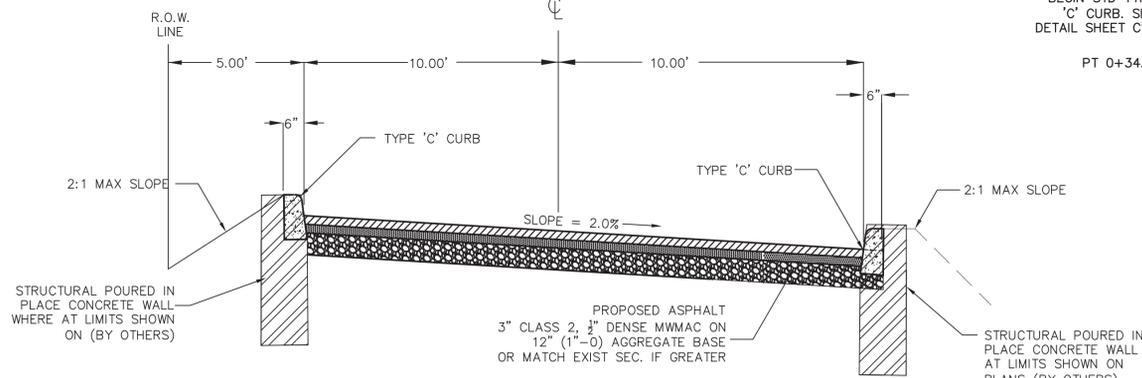
PUBLIC TRANSPORTATION NOTE:

1. PUBLIC RIGHT OF WAY IMPROVEMENTS ARE TO BE CONSTRUCTED UNDER SEPARATE PUBLIC WORKS PERMIT TH0903.

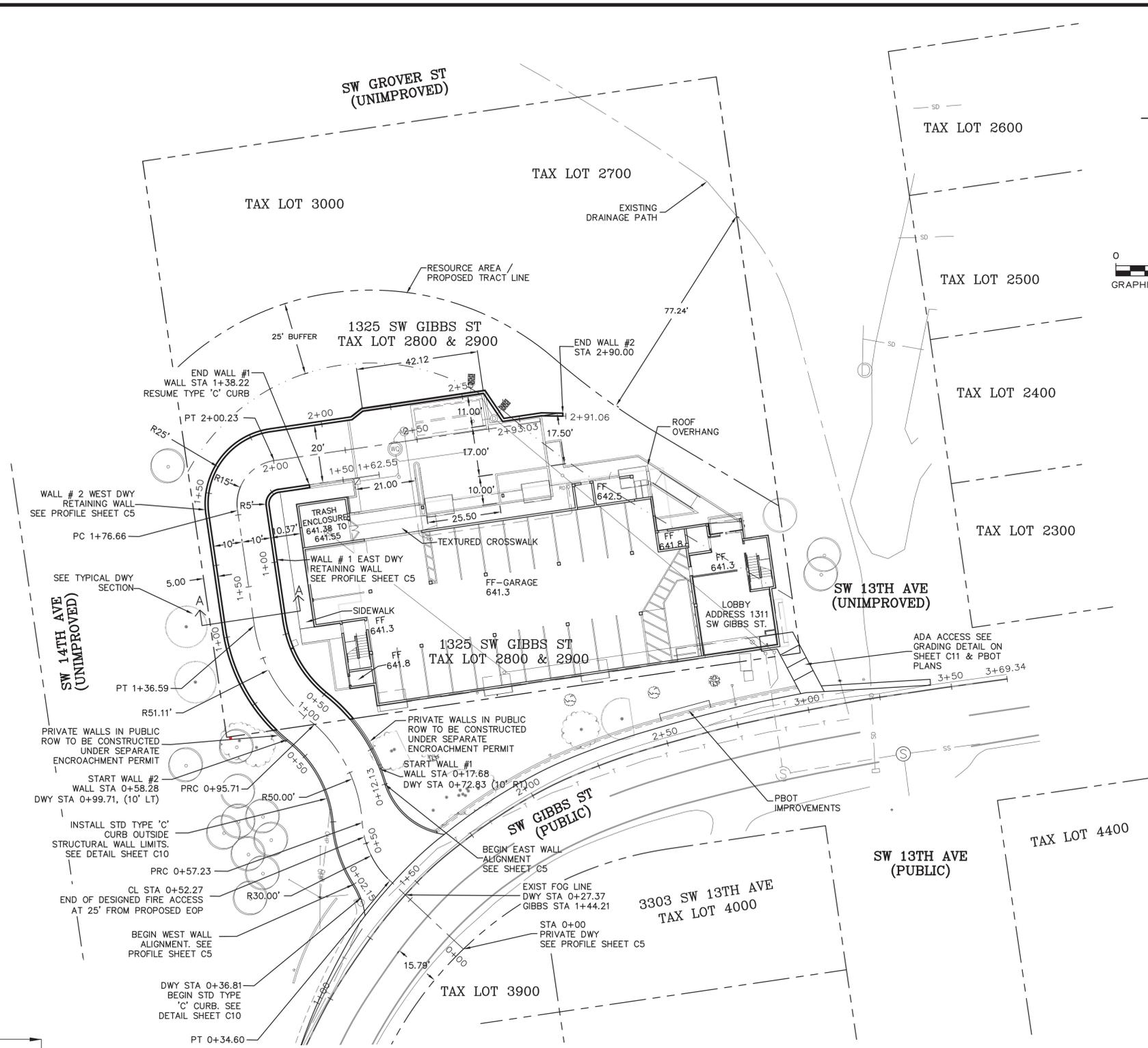
WALL NOTE: STRUCTURAL WALL DESIGNS AND SECTION TO BE PROVIDED BY OTHERS.

PAVEMENT LOAD NOTE: THE DRIVEWAYS TYPICAL ASPHALT SECTIONS FOR THE FIRST 50 FEET SHALL SUPPORT NOT LESS THAN 12,500 POUND POINT LOAD (WHEEL) AND 75,00 POUND LIVE LOAD.

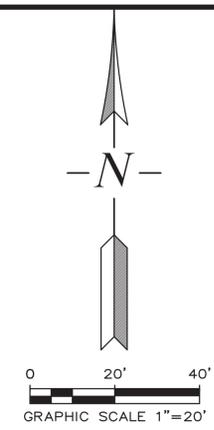
CURB NOTE: INSTALL STANDARD TYPE 'C' CURB WHERE STRUCTURAL WALL IS NOT PRESENT.



STD DWY TYPICAL SEC.
NTS



SITE PLAN
SCALE: 1" = 20'



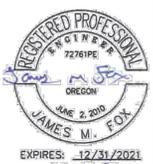
City of Portland
REVIEWED FOR
CODE COMPLIANCE
 Date: 01/03/22
 Permit #: 20-19429-00-00-00

REVISIONS	BY
PRELIM PLANS (11-16-19)	JMF
REVISED PLANS (4-21-20)	JJM
2/05/2021-1ST BDS REVIEW	JMF
4/27/2021-2nd BDS REVIEW	JMF
COP 5TH REVIEW (11-16-2021)	JMF

1325 SW GIBBS STREET
 SCOTIA WESTERN STATES HOUSING

Site Plan

SISUL ENGINEERING
 375 PORTLAND AVENUE
 GLADSTONE, OREGON 97027
 (503) 657-0188
 DRAWING: 19-029 - BDS-SITE PLAN.dwg



DATE	NOV. 2019
SCALE	1"=20'
DRAWN	JMF
JOB	SGL19-029
SHEET	C4

REVISIONS	BY
REVISED PLANS (1-31-2020)	JJM
REVISED PLANS (4-21-2020)	JJM
2/05/2021-1ST BDS REVIEW	JMF
6/07/2021-3RD BDS REVIEW	JMF
COP 4TH REVIEW (10-12-2021)	JMF
COP 5TH REVIEW (11-16-2021)	JMF

1325 SW GIBBS STREET
SCOTIA WESTERN STATES HOUSING

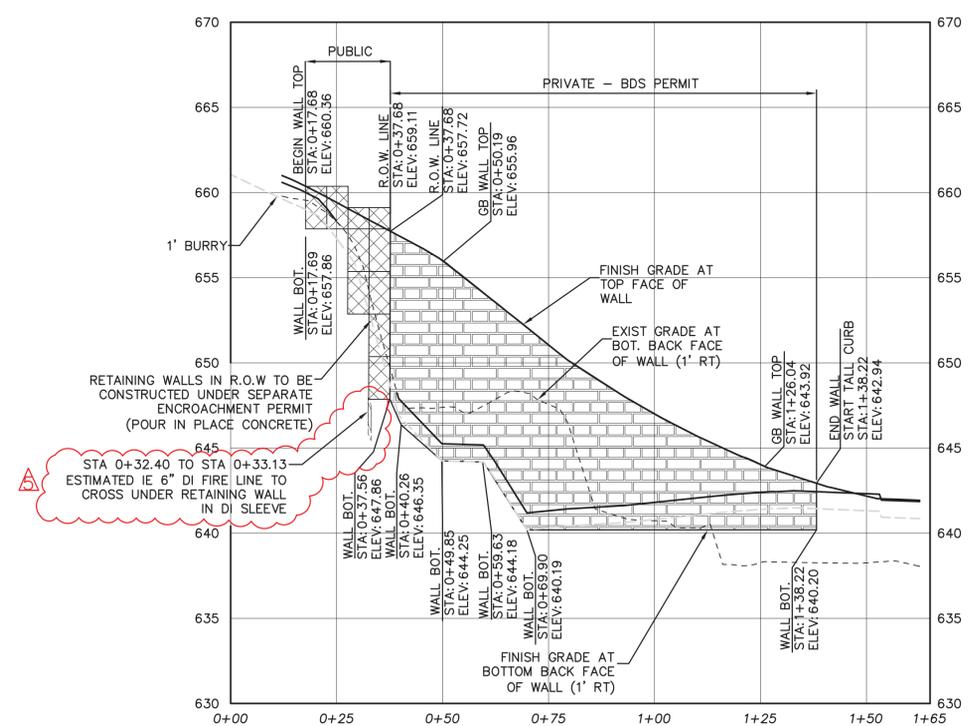
City of Portland
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Date: 01/03/22
Permit #: 20-19429-00-00-C3

SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 19-02P-PRIVATE-CON-MAN-PLAN.dwg

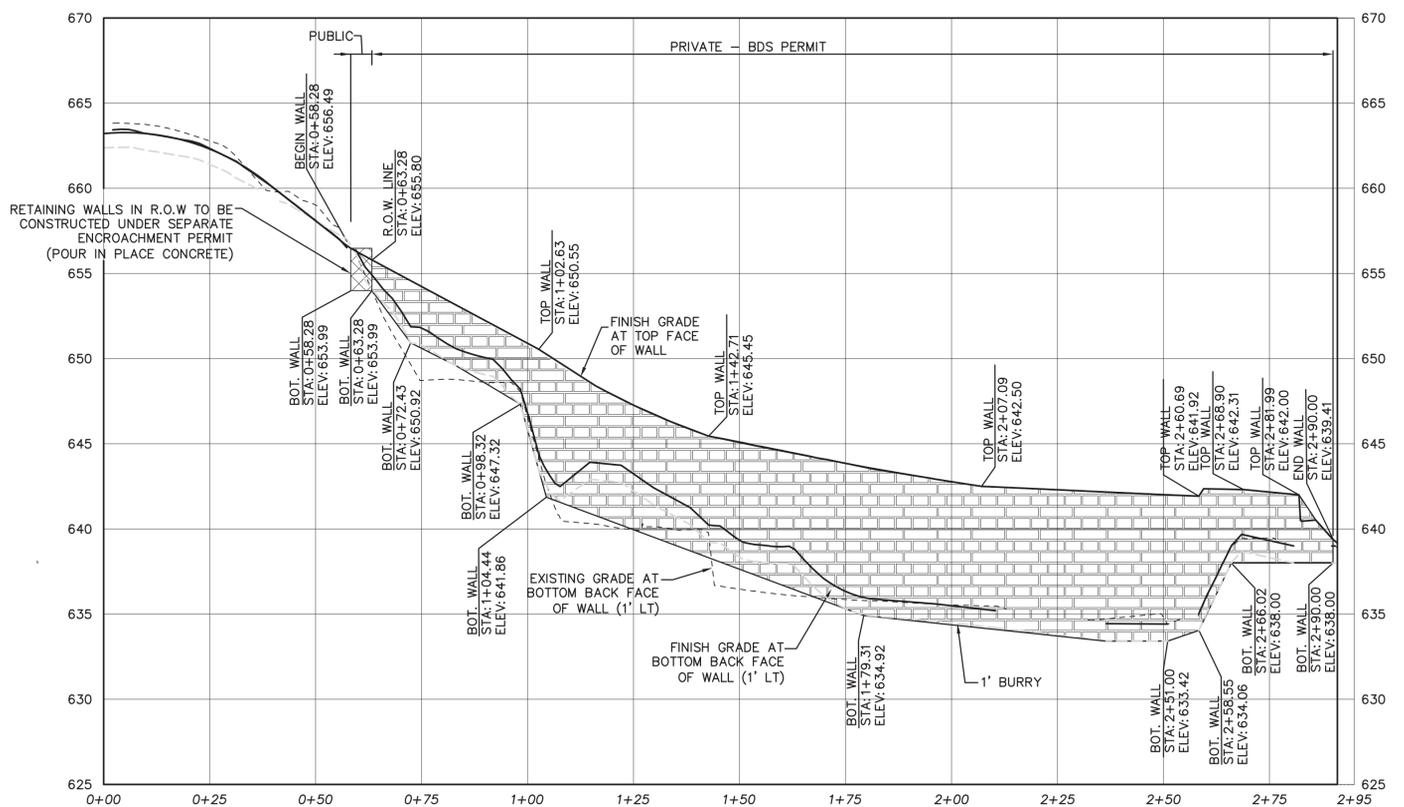
DATE	NOV. 2019
SCALE	1"=20'
DRAWN	JMF
JOB	SGL19-029
SHEET	C5



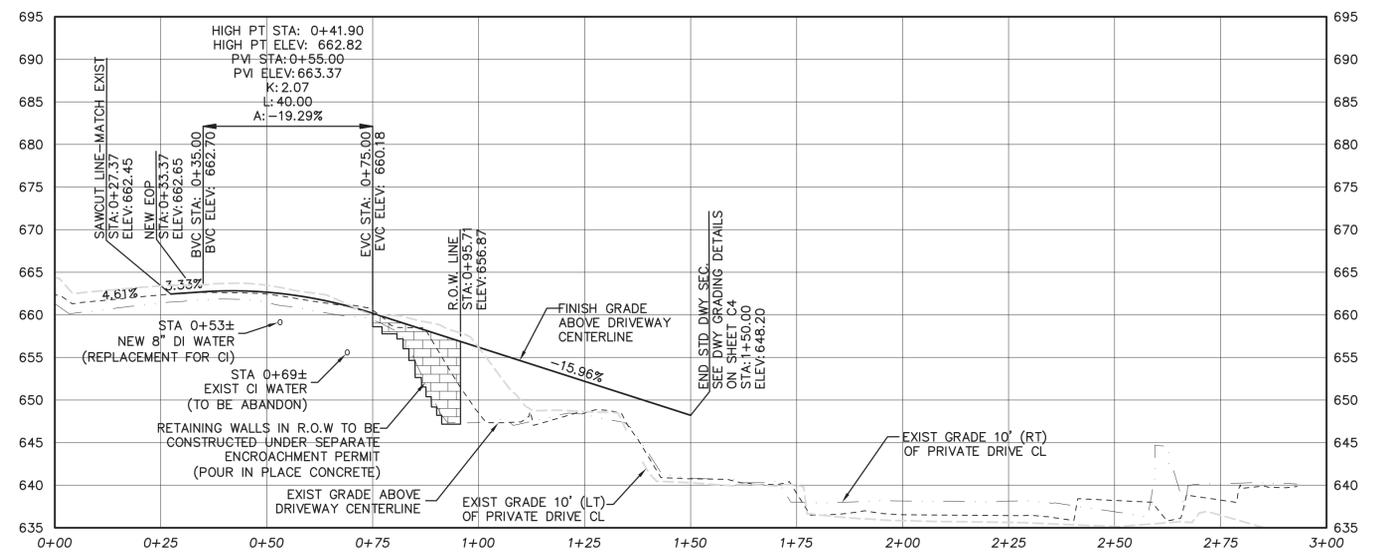
OF 11 SHEETS
LU 22 18796 APD AP
Exhibit C.7



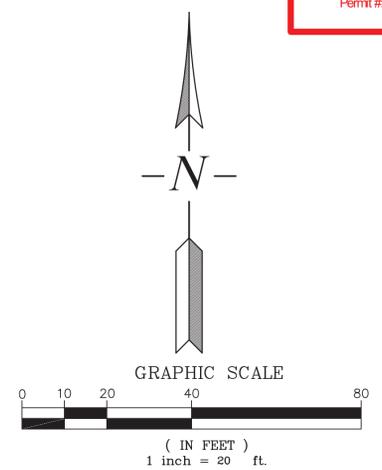
WALL # 1 - EAST DWY
HORIZONTAL SCALE: 1"=20'
VERTICAL SCALE: 1"=5'



WALL # 2 - WEST DWY
HORIZONTAL SCALE: 1"=20'
VERTICAL SCALE: 1"=5'



PRIVATE DRIVEWAY
HORIZONTAL SCALE: 1"=20'
VERTICAL SCALE: 1"=10'



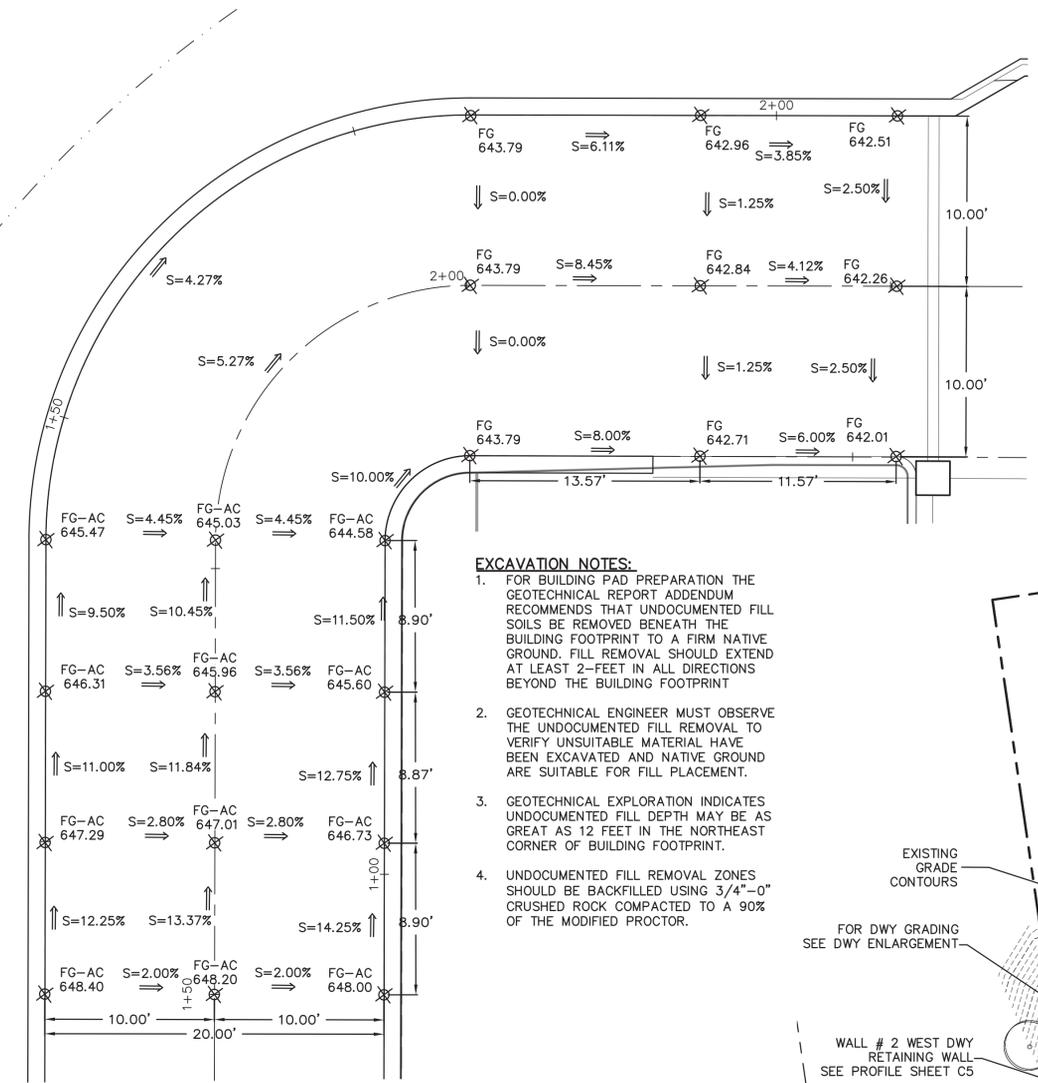
△ COP FIRE LINE
ALTERNATIVE 1
JMF

REVISIONS	BY
REVISED PLANS (1-31-2020)	JJM
REVISED PLANS (4-21-2020)	JJM
2/05/2021-1ST BDS REVIEW	JMF
4/20/2021-2ND BDS REVIEW	JMF
6/07/2021-3RD BDS REVIEW	JMF
COP 5TH REVIEW (11-16-2021)	JMF

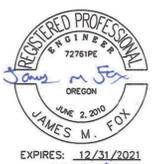
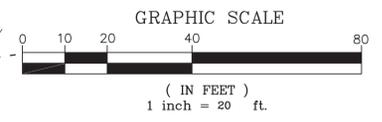
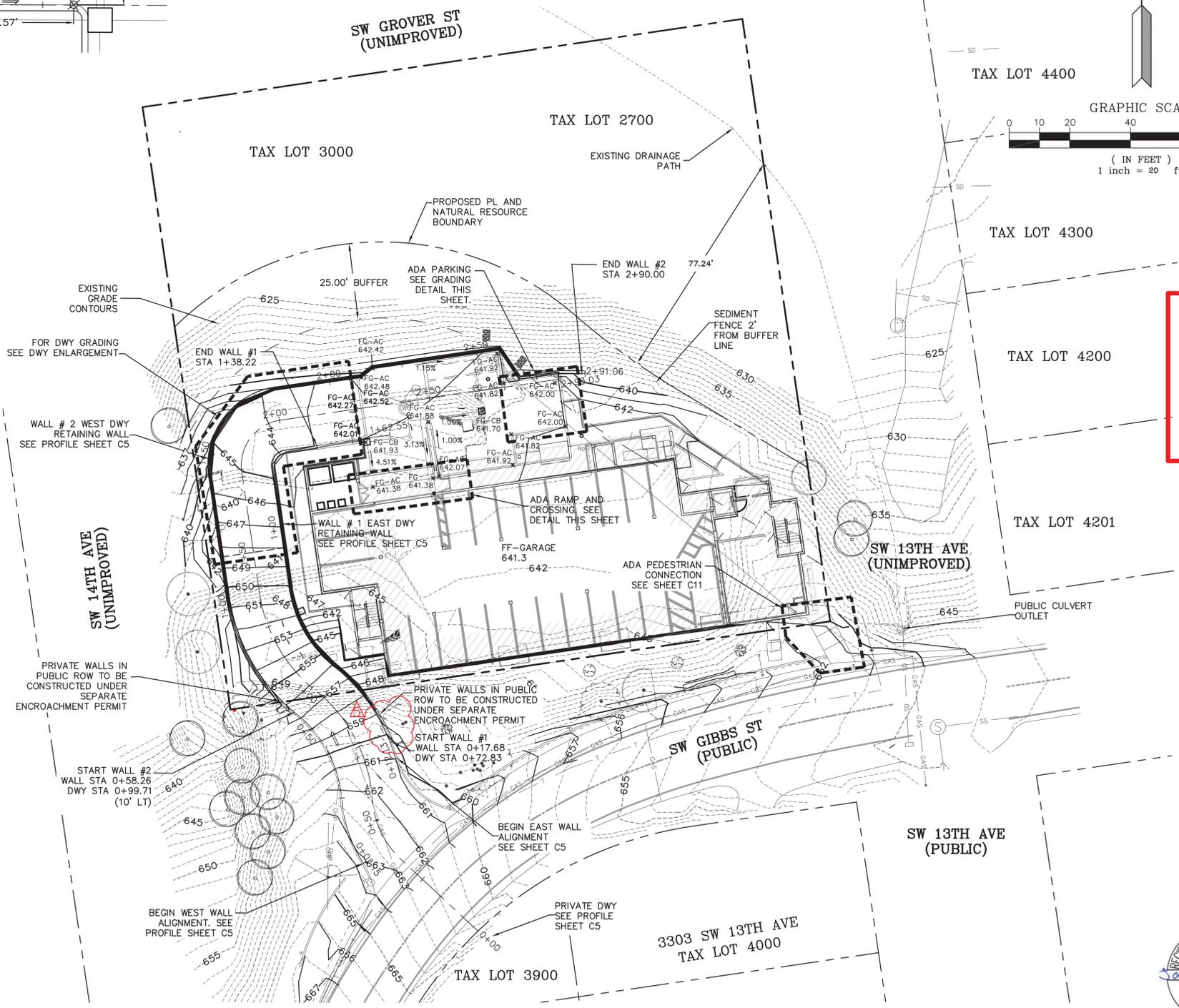
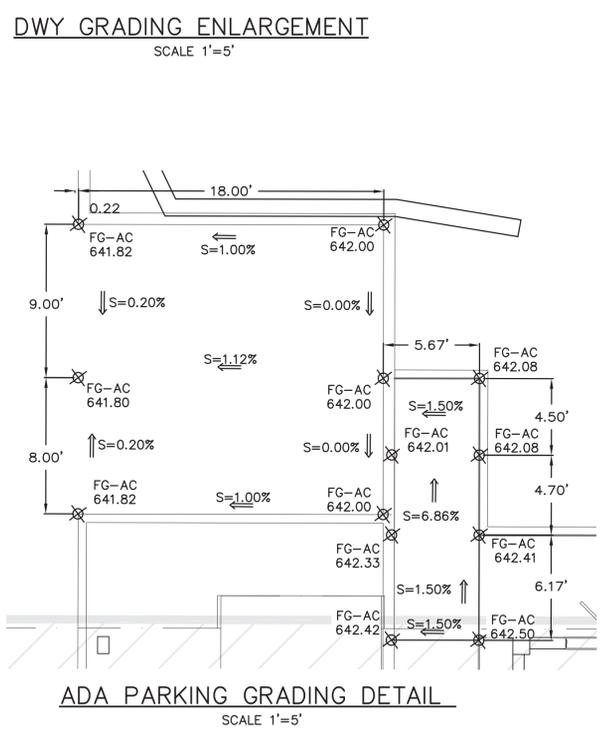
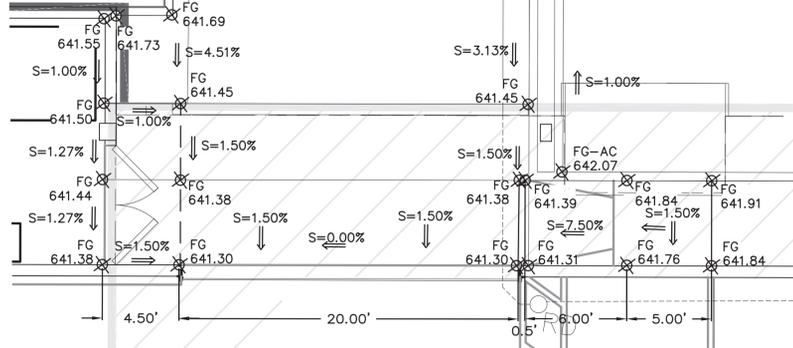
1325 SW GIBBS STREET
 SCOTIA WESTERN STATES HOUSING

City of Portland
REVIEWED FOR
CODE COMPLIANCE
 Date: 01/03/22
 Permit #: 20-19429-00-00-C

SISUL ENGINEERING
 375 PORTLAND AVENUE 97027
 GLADSTONE, OREGON (503) 657-0188
 DRAWING: 19-023-PRIVATE-CO-MAN-PLAN.dwg
 DATE NOV. 2019
 SCALE 1"=20'
 DRAWN JMF
 JOB SGL19-029
 SHEET **C6**
 OF 11 SHEETS
 EXPIRES: 12/31/2021
 11-16-21
 LU 22 187796 APD AP
 Exhibit C.7



- EXCAVATION NOTES:**
- FOR BUILDING PAD PREPARATION THE GEOTECHNICAL REPORT ADDENDUM RECOMMENDS THAT UNDOCUMENTED FILL SOILS BE REMOVED BENEATH THE BUILDING FOOTPRINT TO A FIRM NATIVE GROUND. FILL REMOVAL SHOULD EXTEND AT LEAST 2'-FEET IN ALL DIRECTIONS BEYOND THE BUILDING FOOTPRINT.
 - GEOTECHNICAL ENGINEER MUST OBSERVE THE UNDOCUMENTED FILL REMOVAL TO VERIFY UNSUITABLE MATERIAL HAVE BEEN EXCAVATED AND NATIVE GROUND ARE SUITABLE FOR FILL PLACEMENT.
 - GEOTECHNICAL EXPLORATION INDICATES UNDOCUMENTED FILL DEPTH MAY BE AS GREAT AS 12 FEET IN THE NORTHEAST CORNER OF BUILDING FOOTPRINT.
 - UNDOCUMENTED FILL REMOVAL ZONES SHOULD BE BACKFILLED USING 3/4"-0" CRUSHED ROCK COMPACTED TO A 90% OF THE MODIFIED PROCTOR.



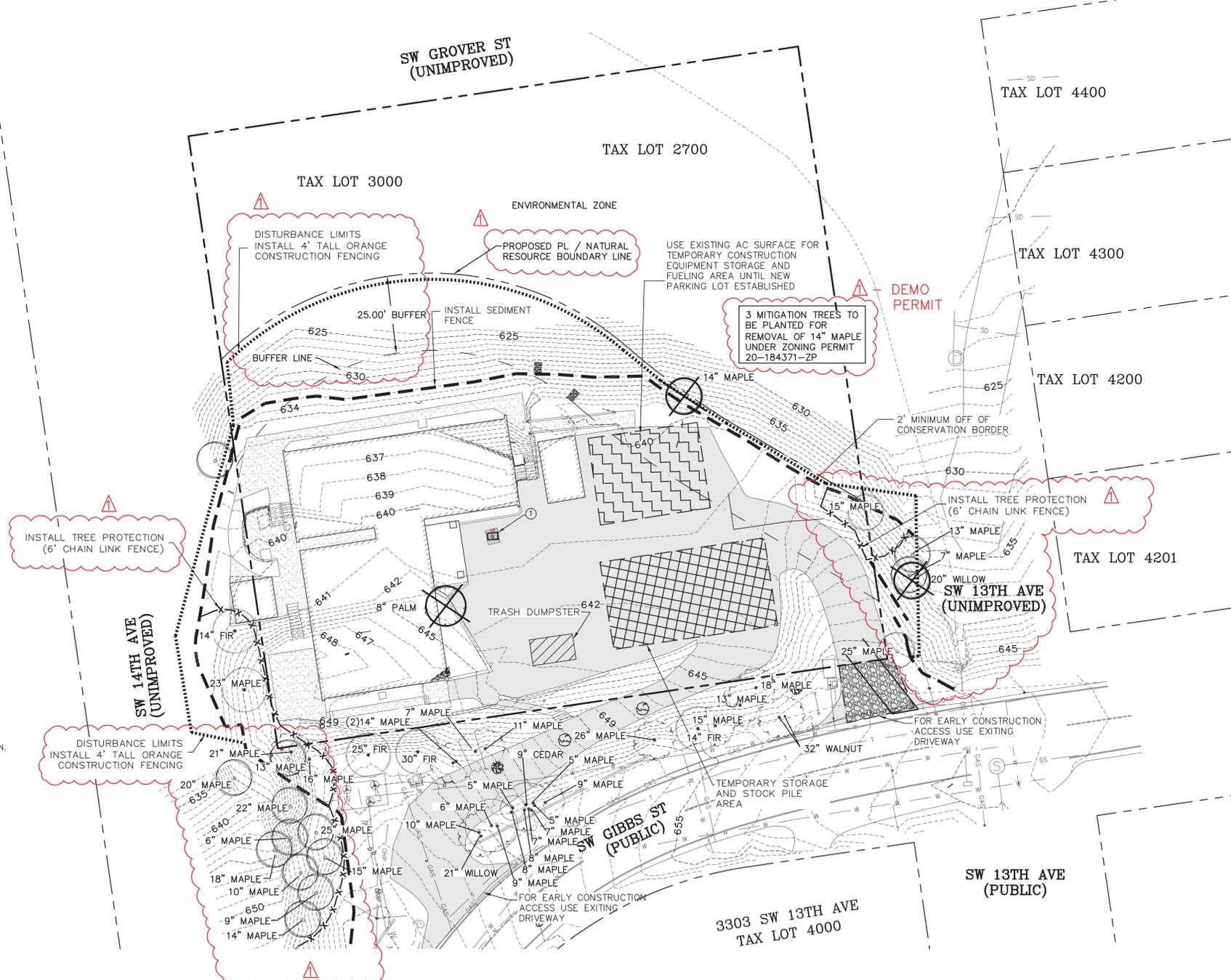
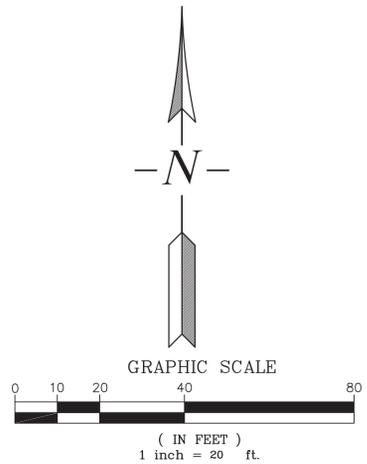
REVISIONS	BY
REVISED PLANS (1-30-2020)	JJM
REVISED PLANS (4-21-2020)	JJM
5/13/2020-PL UPDATE-DEMO	JMF
6/31/2020-CITY COMMENTS	JJM
12/11/20-BUFFER LINE	JJM
2/05/2021-1ST BDS REVIEW	JMF

1325 SW GIBBS STREET
SCOTIA WESTERN STATES HOUSING

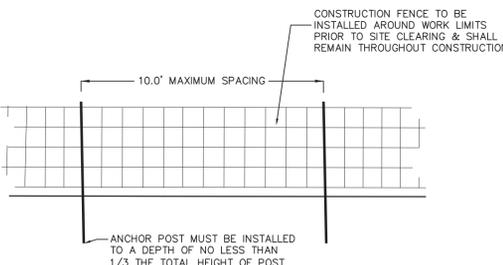
City of Portland
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Permit #: 20-19429-00-00-00

SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 19-02B-PRIVATE-CO-MAN-PLAN.dwg

DATE NOV. 2019
SCALE 1"=20'
DRAWN JMF
JOB SGL19-029
SHEET C7



EARLY CONSTRUCTION MANAGEMENT PLAN
SCALE 1"=20'



- NOTES:
1. BLAZE ORANGE MESH FENCE, "CONSTRUCTION FENCE" SHALL BE USED TO MARK WORK AREA LIMITS.
 2. WORK LIMITS SHALL BE ESTABLISHED ON SITE PRIOR TO CONSTRUCTION.
 3. DO NOT DAMAGE OR SEVERE LARGE ROOTS WHEN INSTALLING POSTS.
 4. SITE WORK LIMITS SHALL REMAIN CLEARLY MARKED THROUGHOUT CONSTRUCTION.
 5. CONSTRUCTION FENCE NOT REQUIRED WHERE SEDIMENT FENCE MARKS THE LIMITS OF CONSTRUCTION

ORANGE CONSTRUCTION FENCE WORK LIMITS DETAIL
N.T.S.

- CONSTRUCTION MANAGEMENT PLAN NOTES:
1. NO TREES SHALL BE USED AS ANCHORS FOR STABILIZING CONSTRUCTION EQUIPMENT.
 2. NATIVE SOILS THAT ARE EXCAVATED DURING CONSTRUCTION THAT ARE NOT REUSED FOR PATHWAY CONSTRUCTION SHALL BE REMOVED FROM THE SITE AND DISPOSED OF OFFSITE IN AN APPROVED LOCATION.
 3. DEVELOPMENT SHALL NOT COMMENCE UNTIL THE EROSION CONTROL MEASURES INDICATED ON THE APPROVED ESC PLAN ARE INSTALLED AND INSPECTED BY THE APPROVING AGENCY.

- TREE PROTECTION:
1. EXISTING TREES TO REMAIN SHALL BE PROTECTED FROM DAMAGE DURING CONSTRUCTION. TREES SHALL HAVE PROTECTIVE CONSTRUCTION FENCING INSTALLED ACCORDING TO THE PLANS AND PLAN DETAILS PRIOR TO SITE EXCAVATION OCCURRING.
 2. LACK OF APPROVED TREE PROTECTION IS GROUNDS FOR THE CITY OF PORTLAND TO STOP WORK ON THE PROJECT.
 3. TREES SHALL NOT BE USED AS AN ANCHOR TO STABILIZE CONSTRUCTION EQUIPMENT.
 4. TREE ROOTS SHALL BE PROTECTED FROM DAMAGE BY CUTTING OR COMPACTION TO THE EXTENT POSSIBLE. IF A ROOT OF GREATER THAN 3 INCHES OR A MASS OF SMALLER ROOTS IS ENCOUNTERED, EXCAVATION SHALL BE HALTED UNTIL THE PROJECT ARBORIST CAN VISIT THE SITE AND MAKE APPROPRIATE RECOMMENDATIONS.

- CONSTRUCTION NOTES
- ① CATCH BASIN TO BE PLUGGED AT BEGINNING OF DEMO TO PREVENT SPILLS REACHING ENVIRONMENTAL AREA.

FOR EROSION CONTROL DETAILS
SEE SHEET C9

RECEIVED 10/20/2021



CONSTRUCTION MANAGEMENT PLAN NOTES:

1. NO TREES SHALL BE USED AS ANCHORS FOR STABILIZING CONSTRUCTION EQUIPMENT.
2. NATIVE SOILS THAT ARE EXCAVATED DURING CONSTRUCTION THAT ARE NOT REUSED FOR PATHWAY CONSTRUCTION SHALL BE REMOVED FROM THE SITE AND DISPOSED OF OFFSITE IN AN APPROVED LOCATION.
3. DEVELOPMENT SHALL NOT COMMENCE UNTIL THE EROSION CONTROL MEASURES INDICATED ON THE APPROVED ESC PLAN ARE INSTALLED AND INSPECTED BY THE APPROVING AGENCY.

TREE PROTECTION:

1. EXISTING TREES TO REMAIN SHALL BE PROTECTED FROM DAMAGE DURING CONSTRUCTION. TREES SHALL HAVE PROTECTIVE CONSTRUCTION FENCING INSTALLED ACCORDING TO THE PLANS AND PLAN DETAILS PRIOR TO SITE EXCAVATION OCCURRING.
2. LACK OF APPROVED TREE PROTECTION IS GROUNDS FOR THE CITY OF PORTLAND TO STOP WORK ON THE PROJECT.
3. TREES SHALL NOT BE USED AS AN ANCHOR TO STABILIZE CONSTRUCTION EQUIPMENT.
4. TREE ROOTS SHALL BE PROTECTED FROM DAMAGE BY CUTTING OR COMPACTION TO THE EXTENT POSSIBLE. IF A ROOT OF GREATER THAN 3 INCHES OR A MASS OF SMALLER ROOTS IS ENCOUNTERED, EXCAVATION SHALL BE HALTED UNTIL THE PROJECT ARBORIST CAN VISIT THE SITE AND MAKE APPROPRIATE RECOMMENDATIONS.

NOTES:

1. LANDSCAPE OR INSTALL GROUND COVER ON ALL EXPOSED DISTURBED AREAS DURING AND AT COMPLETION OF CONSTRUCTION, FOR EROSION / SEDIMENT CONTROL.

EROSION CONTROL:

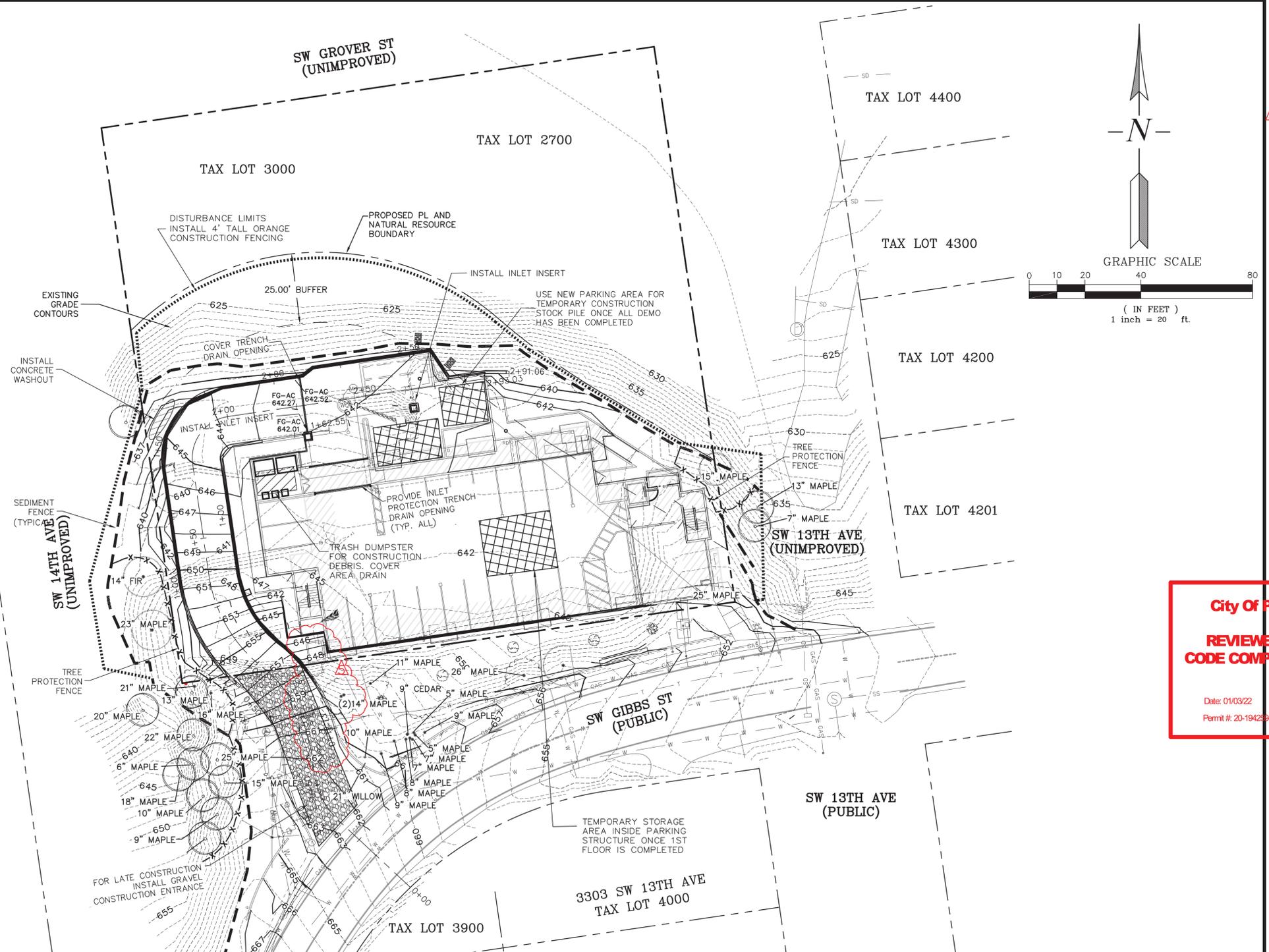
- A. APPROVAL OF THIS EROSION, SEDIMENT AND POLLUTION CONTROL PLAN (ESPCP) DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G., SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.)
- B. THE IMPLEMENTATION OF THIS ESPCP AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESPCP FACILITIES IS THE RESPONSIBILITY OF THE APPLICANT/CONTRACTOR UNTIL ALL CONSTRUCTION IS COMPLETED AND APPROVED AND VEGETATION/LANDSCAPING IS ESTABLISHED.
- C. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY FLAGGED IN THE FIELD PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE FLAGGED CLEARING LIMITS SHALL BE PERMITTED. THE FLAGGING SHALL BE MAINTAINED BY THE APPLICANT/CONTRACTOR FOR THE DURATION OF CONSTRUCTION.
- D. THE ESPCP FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES, AND IN SUCH A MANNER AS TO INSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DO NOT ENTER THE DRAINAGE SYSTEM, ROADWAYS, OR VIOLATE APPLICABLE WATER STANDARDS.
- E. THE ESPCP FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR THE ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESPCP FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND TO ENSURE THAT SEDIMENT AND SEDIMENT LADEN WATER DO NOT LEAVE THE SITE. SITE WORK MUST BE CONDUCTED IN ACCORDANCE WITH THE REQUIREMENTS OF THE CITY OF PORTLAND'S EROSION & SEDIMENT CONTROL MANUAL AND CITY OF PORTLAND TITLE 10.
- F. THE ESPCP FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/CONTRACTOR AND MAINTAINED AS NECESSARY TO ENSURE THEIR CONTINUED FUNCTIONING.
- G. THE ESPCP FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A WEEK OR WITHIN THE 24 HOURS FOLLOWING A STORM EVENT.
- H. STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES MAY BE REQUIRED TO INSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
- I. ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLES ONTO ROADWAYS OR INTO THE STORMWATER COLLECTION SYSTEM SHALL BE REMOVED OR CLEANED UP IMMEDIATELY, AND NO LATER THAN END OF THE WORK DAY. THE USE OF WATER TRUCKS TO WASH THE MATERIAL OFF THE ROADWAY IS NOT ALLOWED. WATER TRUCKS MAY BE USED IMMEDIATELY BEFORE SWEEPERS OR VACUUM SYSTEMS TO LOOSEN SEDIMENT, PROVIDED THAT DISCHARGE TO THE STORMWATER COLLECTION SYSTEM DOES NOT OCCUR.
- J. IF SEDIMENT IS TRACKED OFFSITE, ADDITIONAL BMPs SHALL BE IMPLEMENTED. THESE BMPs SHALL BE DONE ON THE ROCK PAD OR IN AN APPROVED WHEEL WASH STRUCTURE LOCATED ONSITE. THE WHEELS SHALL BE WASHED BEFORE CROSSING THE ROCK PAD TO LEAVE THE SITE.
- K. 24 HOUR EROSION CONTROL CONTACT: SEAN O' NEIL
SCOTIA WESTERN STATES HOUSING
8340 N. CAMPBELL AVENUE, #240
TUCSON, AZ 85718
(512) 362-8424

SEE EROSION CONTROL DETAILS ON SHEET C9

STORM WATER RUNOFF NOTE:

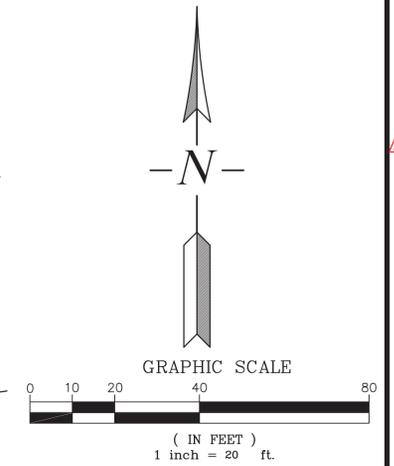
1. THE EROSION CONTROL MEASURE SHOWN ARE A MINIMUM. THE CONTRACTOR SHALL ADD ADDITIONAL EROSION CONTROL BMP'S AS NECESSARY TO PREVENT STORM WATER FROM LEAVING THE SITE IN CONCENTRATED FLOWS OR ERODING EXISTING SLOPED BANKS PRIOR TO INSTALLATION AND APPROVAL OF A PERMANENT STORMWATER FACILITY.

TO ATTENUATE FLOW THE FOLLOWING ADDITIONAL MEASURE SHOULD BE UTILIZED WHEN NECESSARY: VEGETATIVE BUFFERS, SAND BAG CHECK DAMS, WATTLES, AND FILTRATION BERMS.



LATE CONSTRUCTION MANAGEMENT PLAN

SCALE 1"=20'



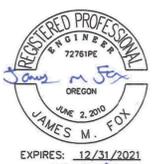
City of Portland
REVIEWED FOR
CODE COMPLIANCE
 Date: 01/03/22
 Permit #: 20-19429-00-00-00

REVISIONS	BY
REVISED PLANS (1-30-2020)	JJM
REVISED PLANS (4-21-2020)	JJM
REVISED PLANS (6-31-2020)	JJM
2/05/2021-1ST BDS REVIEW	JMF
4/20/2021-2nd BDS REVIEW	JMF
COP 5TH REVIEW (11-16-2021)	JMF

1325 SW GIBBS STREET
 SCOTIA WESTERN STATES HOUSING

Late Construction Management Plan

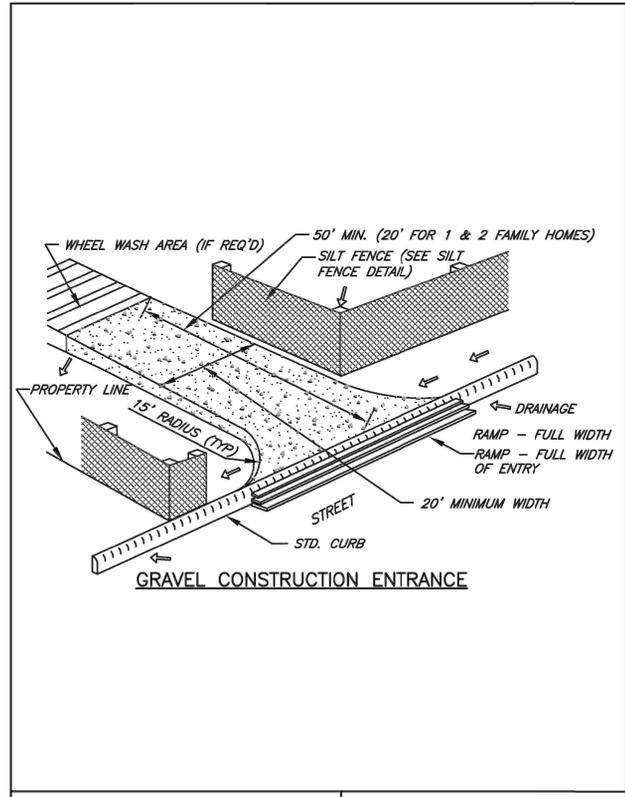
SISUL ENGINEERING
 375 PORTLAND AVENUE
 GLADSTONE, OREGON 97027
 (503) 657-0188
DRAWING: 19-022P-PRIVATE-CON-MAN-PLAN.dwg



DATE	NOV. 2019
SCALE	1"=20'
DRAWN	JMF
JOB	SGL19-029
SHEET	C8

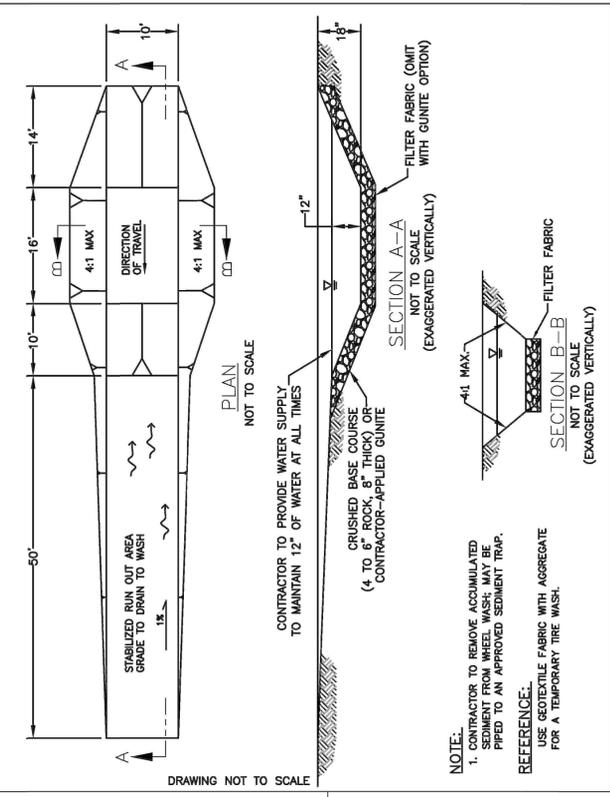
REVISIONS	BY
2/05/2021-1ST BDS REVIEW	JMF

1325 SW GIBBS STREET
SCOTIA WESTERN STATES HOUSING



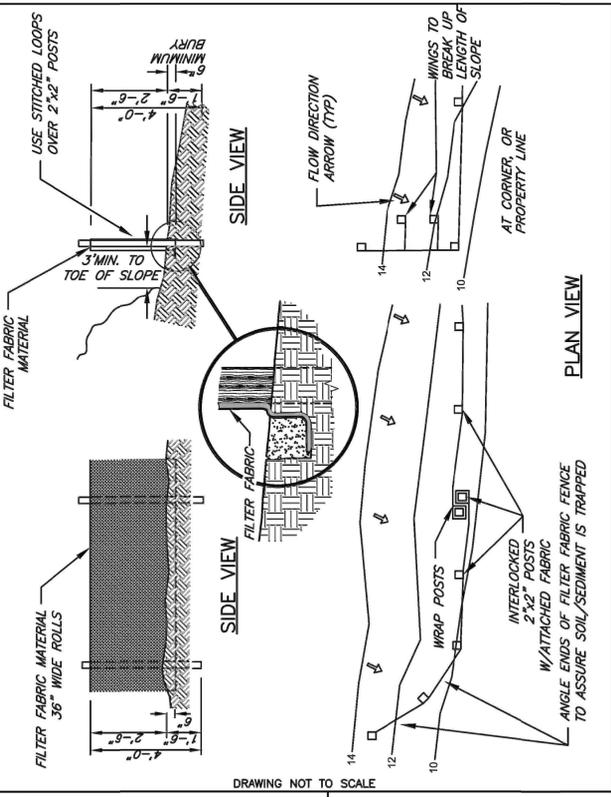
EROSION CONTROL MANUAL
GRAVEL CONSTRUCTION ENTRANCE
Detail Drawing 4.2-A

FILE DRAFT/INSPECTORS GRAPHICS DRAWING BR 331 PLOT 14



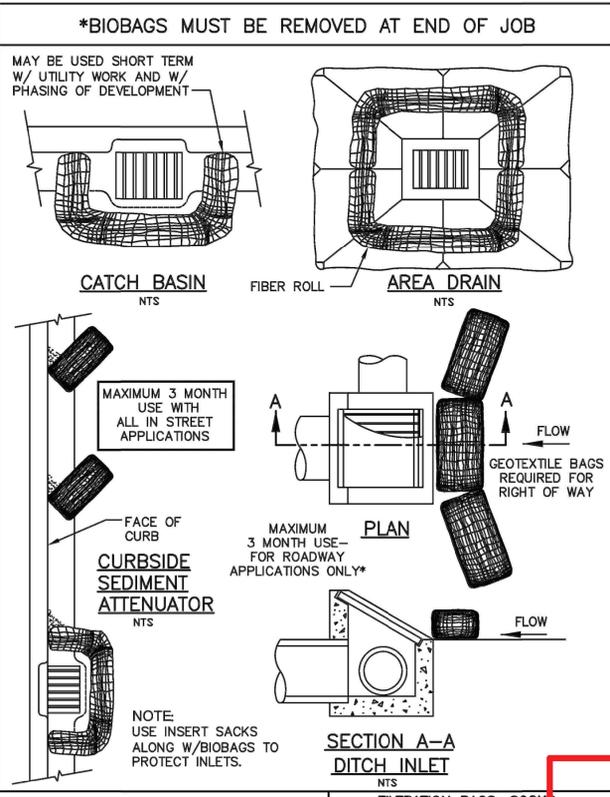
EROSION CONTROL MANUAL
WHEEL WASH STRUCTURE
Detail Drawing 4.2-B

FILE DRAFT/INSPECTORS GRAPHICS DRAWING BR 331 PLOT 14



EROSION CONTROL MANUAL
TEMPORARY SEDIMENT FENCE
Detail Drawing 4.3-A

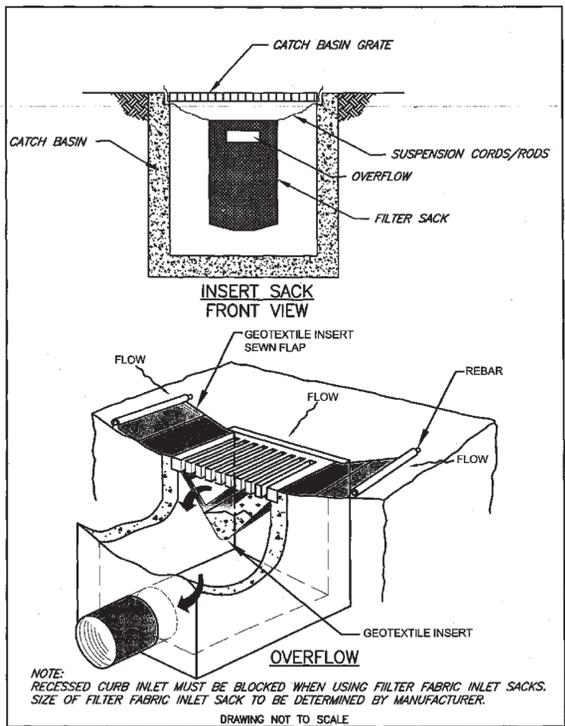
FILE DRAFT/INSPECTORS GRAPHICS DRAWING BR 331 PLOT 14



EROSION CONTROL MANUAL
FILTRATION BAGS, SOCKS & ROLLS FOR TEMPORARY INLET PROTECTION
Detail Drawing 4.3-B

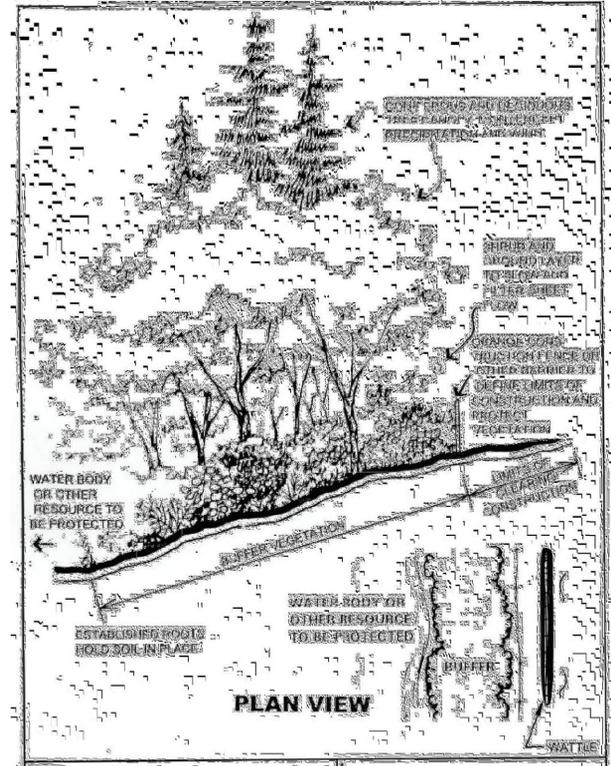
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City of Portland
REVIEWED FOR CODE COMPLIANCE
Date: 01/03/22
Permit #: 20-19429-00-00-00



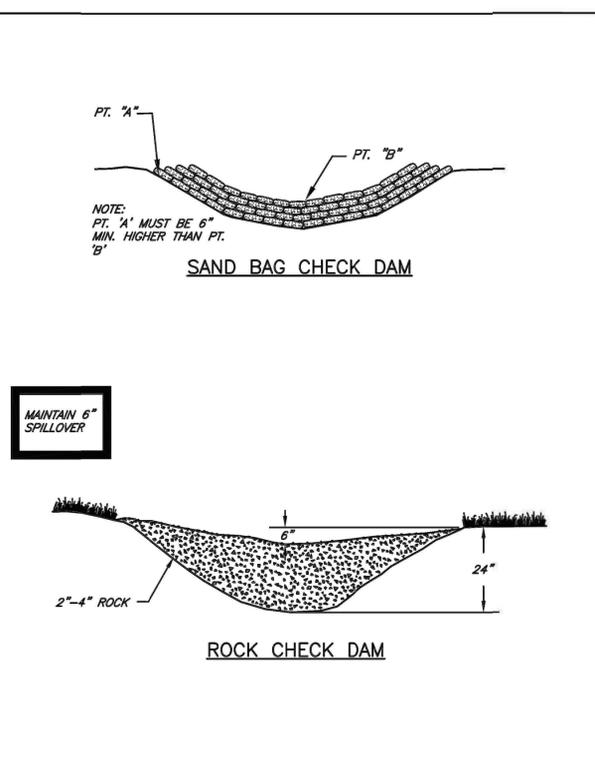
EROSION CONTROL MANUAL
INLET INSERT
Detail Drawing 4.3-C

FILE DRAFT/INSPECTORS GRAPHICS DRAWING BR 331 PLOT 14



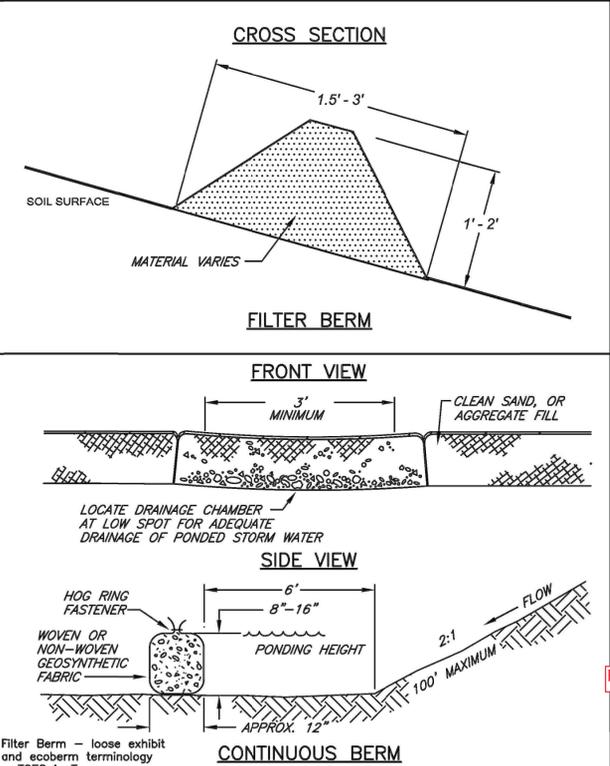
EROSION CONTROL MANUAL
VEGETATIVE BUFFER
Detail Drawing 4.3-E

FILE DRAFT/INSPECTORS GRAPHICS DRAWING BR 331 PLOT 14



EROSION CONTROL MANUAL
CHECK DAMS
Detail Drawing 4.4-B

FILE DRAFT/INSPECTORS GRAPHICS DRAWING BR 331 PLOT 14



EROSION CONTROL MANUAL
FILTRATION BERM
Detail Drawing 4.3-H

FILE DRAFT/INSPECTORS GRAPHICS DRAWING BR 331 PLOT 14

RECEIVED 10/20/2021



City of Portland
REVIEWED FOR CODE COMPLIANCE
Erosion Control Details

SISUL ENGINEERING
375 PORTLAND AVENUE
GLADSTONE, OREGON 97027
(503) 657-0188
DRAWING: 19-029-DETAILS.dwg
DATE NOV. 2019
SCALE 1"=20'
DRAWN JMF
JOB SGL19-029
SHEET 09 OF 11 SHEETS

LU 22 187796 APD AP
Exhibit C.7

REVISIONS	BY
BDS CHECKSHEETS 1ST (7-09-19)	JMF
REVISED PLANS (1-30-2020)	JJM
2/05/2021-1ST BDS REVIEW	JMF
4/27/2021-2nd BDS REVIEW	JMF

1325 SW GIBBS STREET
 SCOTIA WESTERN STATES HOUSING

Construction Details

SISUL ENGINEERING
 375 PORTLAND AVENUE
 GLADSTONE, OREGON 97027
 (503) 657-0188
 DRAWING: 19-029-DET-15.dwg

DATE NOV. 2019
 SCALE 1"=20'
 DRAWN JMF
 JOB SGL19-029
 SHEET C11
 OF C11 SHEETS
 Exhibit C.7



PROJECT INFORMATION	
ENGINEER	AVERY SCOTT
PRODUCT MANAGER	AVERY SCOTT@ADS-PIPE.COM
ADS SALES REP	JOSEPH COTTON
	304-651-2790
	JOSEPH.COTTON@ADS-PIPE.COM
PROJECT NO.	S200602

SCOTIA WESTERN STATES HOUSING

PORTLAND, OR

SC-740 STORMTECH CHAMBER SPECIFICATIONS

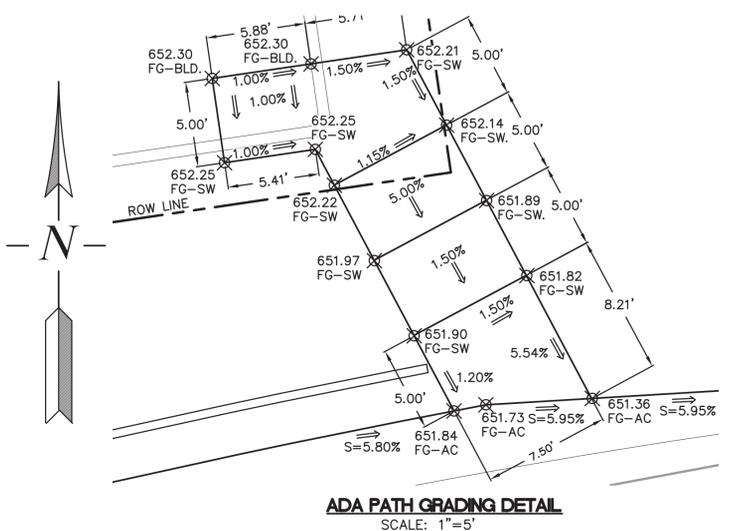
- CHAMBERS SHALL BE STORMTECH SC-740.
- CHAMBERS SHALL BE ARCH-SHAPED AND SHALL BE MANUFACTURED FROM VIRGIN, IMPACT-MODIFIED POLYPROPYLENE COPOLYMERS.
- CHAMBERS SHALL MEET THE REQUIREMENTS OF ASTM F2418-16a, "STANDARD SPECIFICATION FOR POLYPROPYLENE (PP) CORRUGATED WALL STORMWATER COLLECTION CHAMBERS".
- CHAMBER ROWS SHALL PROVIDE CONTINUOUS, UNOBSTRUCTED INTERNAL SPACE WITH NO INTERNAL SUPPORTS THAT WOULD IMPEDE FLOW OR LIMIT ACCESS FOR INSPECTION.
- THE STRUCTURAL DESIGN OF THE CHAMBERS, THE STRUCTURAL BACKFILL, AND THE INSTALLATION REQUIREMENTS SHALL ENSURE THAT THE LOAD FACTORS SPECIFIED IN THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, SECTION 12.12, ARE MET FOR: 1) LONG-DURATION DEAD LOADS AND 2) SHORT-DURATION LIVE LOADS, BASED ON THE AASHTO DESIGN TRUCK WITH CONSIDERATION FOR IMPACT AND MULTIPLE VEHICLE PRESENCES.
- CHAMBERS SHALL BE DESIGNED, TESTED AND ALLOWABLE LOAD CONFIGURATIONS DETERMINED IN ACCORDANCE WITH ASTM F2787, "STANDARD PRACTICE FOR STRUCTURAL DESIGN OF THERMOPLASTIC CORRUGATED WALL STORMWATER COLLECTION CHAMBERS". LOAD CONFIGURATIONS SHALL INCLUDE: 1) INSTANTANEOUS (<1 MIN) AASHTO DESIGN TRUCK LIVE LOAD ON MINIMUM COVER 2) MAXIMUM PERMANENT (75-YR) COVER LOAD AND 3) ALLOWABLE COVER WITH PARKED (1-WEEK) AASHTO DESIGN TRUCK.
- REQUIREMENTS FOR HANDLING AND INSTALLATION:
 - TO MAINTAIN THE WIDTH OF CHAMBERS DURING SHIPPING AND HANDLING, CHAMBERS SHALL HAVE INTEGRAL, INTERLOCKING STACKING LUGS.
 - TO ENSURE A SECURE JOINT DURING INSTALLATION AND BACKFILL, THE HEIGHT OF THE CHAMBER JOINT SHALL NOT BE LESS THAN 2".
 - TO ENSURE THE INTEGRITY OF THE ARCH SHAPE DURING INSTALLATION, a) THE ARCH STIFFNESS CONSTANT AS DEFINED IN SECTION 6.2.8 OF ASTM F2418 SHALL BE GREATER THAN OR EQUAL TO 550 LBS/IN, AND b) TO RESIST CHAMBER DEFORMATION DURING INSTALLATION AT ELEVATED TEMPERATURES (ABOVE 73° F / 23° C), CHAMBERS SHALL BE PRODUCED FROM REFLECTIVE GOLD OR YELLOW COLORS.
- ONLY CHAMBERS THAT ARE APPROVED BY THE SITE DESIGN ENGINEER WILL BE ALLOWED. UPON REQUEST BY THE SITE DESIGN ENGINEER OR OWNER, THE CHAMBER MANUFACTURER SHALL SUBMIT A STRUCTURAL EVALUATION FOR APPROVAL BEFORE DELIVERING CHAMBERS TO THE PROJECT SITE AS FOLLOWS:
 - THE STRUCTURAL EVALUATION SHALL BE SEALED BY A REGISTERED PROFESSIONAL ENGINEER.
 - THE STRUCTURAL EVALUATION SHALL DEMONSTRATE THAT THE SAFETY FACTORS ARE GREATER THAN OR EQUAL TO 1.95 FOR DEAD LOAD AND 1.75 FOR LIVE LOAD, THE MINIMUM REQUIRED BY ASTM F2787 AND BY SECTIONS 3 AND 12 OF THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS FOR THERMOPLASTIC PIPE.
 - THE TEST DERIVED CREEP MODULUS AS SPECIFIED IN ASTM F2418 SHALL BE USED FOR PERMANENT DEAD LOAD DESIGN EXCEPT THAT IT SHALL BE THE 75-YEAR MODULUS USED FOR DESIGN.
- CHAMBERS AND END CAPS SHALL BE PRODUCED AT AN ISO 9001 CERTIFIED MANUFACTURING FACILITY.

IMPORTANT - NOTES FOR THE BIDDING AND INSTALLATION OF THE SC-740 SYSTEM

- STORMTECH SC-740 CHAMBERS SHALL NOT BE INSTALLED UNTIL THE MANUFACTURER'S REPRESENTATIVE HAS COMPLETED A PRE-CONSTRUCTION MEETING WITH THE INSTALLERS.
- STORMTECH SC-740 CHAMBERS SHALL BE INSTALLED IN ACCORDANCE WITH THE "STORMTECH SC-310/SC-740/DC-780 CONSTRUCTION GUIDE".
- CHAMBERS ARE NOT TO BE BACKFILLED WITH A DOZER OR AN EXCAVATOR SITUATED OVER THE CHAMBERS. STORMTECH RECOMMENDS 3 BACKFILL METHODS:
 - STONE/SHOTTER LOCATED OFF THE CHAMBER BED.
 - BACKFILL AS ROWS ARE BUILT USING AN EXCAVATOR ON THE FOUNDATION STONE OR SUBGRADE.
 - BACKFILL FROM OUTSIDE THE EXCAVATION USING A LONG BOOM HOE OR EXCAVATOR.
- THE FOUNDATION STONE SHALL BE LEVELLED AND COMPACTED PRIOR TO PLACING CHAMBERS.
- JOINTS BETWEEN CHAMBERS SHALL BE PROPERLY SEATED PRIOR TO PLACING STONE.
- MAINTAIN MINIMUM - 6" (150 mm) SPACING BETWEEN THE CHAMBER ROWS.
- EMBEDMENT STONE SURROUNDING CHAMBERS MUST BE A CLEAN, CRUSHED, ANGULAR STONE 3/4"-2" (20-50 mm).
- THE CONTRACTOR MUST REPORT ANY DISCREPANCIES WITH CHAMBER FOUNDATION MATERIALS BEARING CAPACITIES TO THE SITE DESIGN ENGINEER.
- ADS RECOMMENDS THE USE OF FLEXSTORM CATCH IT® INSERTS DURING CONSTRUCTION FOR ALL INLETS TO PROTECT THE SUBSURFACE STORMWATER MANAGEMENT SYSTEM FROM CONSTRUCTION SITE RUNOFF.

NOTES FOR CONSTRUCTION EQUIPMENT

- STORMTECH SC-740 CHAMBERS SHALL BE INSTALLED IN ACCORDANCE WITH THE "STORMTECH SC-310/SC-740/DC-780 CONSTRUCTION GUIDE".
 - THE USE OF CONSTRUCTION EQUIPMENT OVER SC-740 CHAMBERS IS LIMITED:
 - NO EQUIPMENT IS ALLOWED ON BARE CHAMBERS.
 - NO RUBBER TIED LOADERS, DUMP TRUCKS, OR EXCAVATORS ARE ALLOWED UNTIL PROPER FILL DEPTHS ARE REACHED IN ACCORDANCE WITH THE "STORMTECH SC-310/SC-740/DC-780 CONSTRUCTION GUIDE".
 - WEIGHT LIMITS FOR CONSTRUCTION EQUIPMENT CAN BE FOUND IN THE "STORMTECH SC-310/SC-740/DC-780 CONSTRUCTION GUIDE".
 - FULL 36" (900 mm) OF STABILIZED COVER MATERIALS OVER THE CHAMBERS IS REQUIRED FOR DUMP TRUCK TRAVEL OR DUMPING.
- USE OF A DOZER TO PUSH EMBEDMENT STONE BETWEEN THE ROWS OF CHAMBERS MAY CAUSE DAMAGE TO THE CHAMBERS AND IS NOT AN ACCEPTABLE BACKFILL METHOD. ANY CHAMBERS DAMAGED BY THE "DUMP AND PUSH" METHOD ARE NOT COVERED UNDER THE STORMTECH STANDARD WARRANTY.
- CONTACT STORMTECH AT 1-888-892-2894 WITH ANY QUESTIONS ON INSTALLATION REQUIREMENTS OR WEIGHT LIMITS FOR CONSTRUCTION EQUIPMENT.



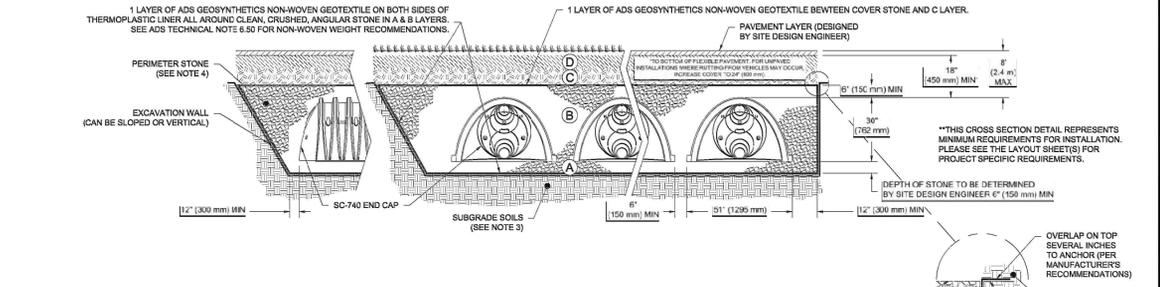
▲-UPDATED DETAILS FROM ADS
 (NOT REVIEW RELATED)

ACCEPTABLE FILL MATERIALS: STORMTECH SC-740 CHAMBER SYSTEMS

MATERIAL LOCATION	DESCRIPTION	AASHTO MATERIAL CLASSIFICATIONS	COMPACTION / DENSITY REQUIREMENT
D	FINAL FILL: FILL MATERIAL FOR LAYER 'D' STARTS FROM THE TOP OF THE 'C' LAYER TO THE BOTTOM OF FLEXIBLE PAVEMENT OR UNPAVED FINISHED GRADE ABOVE. NOTE THAT PAVEMENT SUBBASE MAY BE PART OF THE 'D' LAYER.	N/A	PREPARE PER SITE DESIGN ENGINEER'S PLANS. PAVED INSTALLATIONS MAY HAVE STRINGENT MATERIAL AND PREPARATION REQUIREMENTS.
C	INITIAL FILL: FILL MATERIAL FOR LAYER 'C' STARTS FROM THE TOP OF THE EMBEDMENT STONE ('B' LAYER) TO 1" (25 mm) ABOVE THE TOP OF THE CHAMBER. NOTE THAT PAVEMENT SUBBASE MAY BE A PART OF THE 'C' LAYER.	GRANULAR WELL-GRADED SOIL/AGGREGATE MIXTURES, <35% FINES OR PROCESSED AGGREGATE. OR MOST PAVEMENT SUBBASE MATERIALS CAN BE USED IN LIEU OF THIS LAYER.	BEGIN COMPACTATIONS AFTER 12" (300 mm) OF MATERIAL OVER THE CHAMBERS IS REACHED. COMPACT ADDITIONAL LAYERS IN 6" (150 mm) MAX LIFTS TO A MIN. 95% PROCTOR DENSITY FOR WELL GRADED MATERIAL AND 95% RELATIVE DENSITY FOR PROCESSED AGGREGATE MATERIALS. ROLLER GROSS VEHICLE WEIGHT NOT TO EXCEED 12,000 lbs (60 kN). DYNAMIC FORCE NOT TO EXCEED 20,000 lbs (89 kN).
B	EMBEDMENT STONE: FILL SURROUNDING THE CHAMBERS FROM THE FOUNDATION STONE ('A' LAYER) TO THE 'C' LAYER ABOVE.	CLEAN, CRUSHED, ANGULAR STONE AASHTO M43 ¹ 3, 357, 4, 467, 5, 56, 57, 6, 67, 68, 7, 78, 6, 89, 9, 10	NO COMPACTION REQUIRED.
A	FOUNDATION STONE: FILL BELOW CHAMBERS FROM THE SUBGRADE UP TO THE FOOT (BOTTOM) OF THE CHAMBER.	CLEAN, CRUSHED, ANGULAR STONE AASHTO M43 ¹ 3, 357, 4, 467, 5, 56, 57	PLATE COMPACT OR ROLL TO ACHIEVE A FLAT SURFACE ^{2,3}

PLEASE NOTE:

- THE LISTED AASHTO DESIGNATIONS ARE FOR GRADATIONS ONLY. THE STONE MUST ALSO BE CLEAN, CRUSHED, ANGULAR. FOR EXAMPLE, A SPECIFICATION FOR #4 STONE WOULD STATE: "CLEAN, CRUSHED, ANGULAR NO. 4 (AASHTO M43) STONE".
- STORMTECH COMPACTION REQUIREMENTS ARE MET FOR ALL LOCATION MATERIALS WHEN PLACED AND COMPACTED IN 6" (150 mm) MAX LIFTS USING TWO FULL COVERAGES WITH A VIBRATORY COMPACTOR.
- WHERE INFILTRATION SURFACES MAY BE COMPROMISED BY COMPACTION, FOR STANDARD DESIGN LOAD CONDITIONS, A FLAT SURFACE MAY BE ACHIEVED BY RAKING OR DRAGGING WITHOUT COMPACTION EQUIPMENT. FOR SPECIAL LOAD DESIGNS, CONTACT STORMTECH FOR COMPACTION REQUIREMENTS.
- ONCE LAYER 'C' IS PLACED, ANY SOIL/MATERIAL CAN BE PLACED IN LAYER 'D' UP TO THE FINISHED GRADE. MOST PAVEMENT SUBBASE SOILS CAN BE USED TO REPLACE THE MATERIAL REQUIREMENTS OF LAYER 'C' OR 'D' AT THE SITE DESIGN ENGINEER'S DISCRETION.



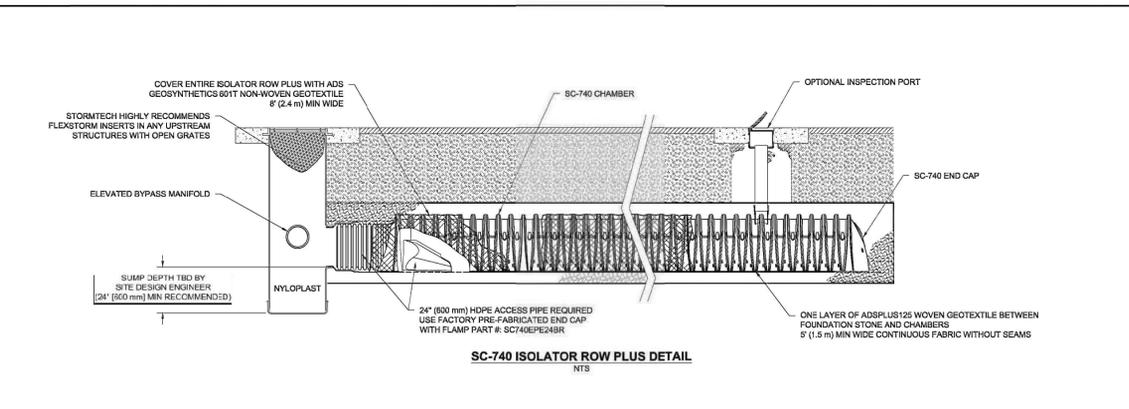
- NOTES:**
- CHAMBERS SHALL MEET THE REQUIREMENTS OF ASTM F2418-16a, "STANDARD SPECIFICATION FOR POLYPROPYLENE (PP) CORRUGATED WALL STORMWATER COLLECTION CHAMBERS".
 - SC-740 CHAMBERS SHALL BE DESIGNED IN ACCORDANCE WITH ASTM F2787 "STANDARD PRACTICE FOR STRUCTURAL DESIGN OF THERMOPLASTIC CORRUGATED WALL STORMWATER COLLECTION CHAMBERS".
 - THE SITE DESIGN ENGINEER IS RESPONSIBLE FOR ASSESSING THE BEARING RESISTANCE (ALLOWABLE BEARING CAPACITY) OF THE SUBGRADE SOILS AND THE DEPTH OF FOUNDATION STONE WITH CONSIDERATION FOR THE RANGE OF EXPECTED SOIL MOISTURE CONDITIONS.
 - PERIMETER STONE MUST BE EXTENDED HORIZONTALLY TO THE EXCAVATION WALL FOR BOTH VERTICAL AND SLOPED EXCAVATION WALLS.
 - REQUIREMENTS FOR HANDLING AND INSTALLATION:
 - TO MAINTAIN THE WIDTH OF CHAMBERS DURING SHIPPING AND HANDLING, CHAMBERS SHALL HAVE INTEGRAL, INTERLOCKING STACKING LUGS.
 - TO ENSURE A SECURE JOINT DURING INSTALLATION AND BACKFILL, THE HEIGHT OF THE CHAMBER JOINT SHALL NOT BE LESS THAN 2".
 - TO ENSURE THE INTEGRITY OF THE ARCH SHAPE DURING INSTALLATION, a) THE ARCH STIFFNESS CONSTANT AS DEFINED IN SECTION 6.2.8 OF ASTM F2418 SHALL BE GREATER THAN OR EQUAL TO 550 LBS/IN, AND b) TO RESIST CHAMBER DEFORMATION DURING INSTALLATION AT ELEVATED TEMPERATURES (ABOVE 73° F / 23° C), CHAMBERS SHALL BE PRODUCED FROM REFLECTIVE GOLD OR YELLOW COLORS.

SCOTIA WESTERN STATES HOUSING
 PORTLAND, OR
 DATE: 2/2/21 DRAWN: KJL
 PROJECT #: S200602 CHECKED: KJL

Stormtech
 4640 BUEHMAN BLVD
 HELLAND, OH 43028
 (614) 881-1111 WWW.STORMTECH.COM

ADS
 4640 BUEHMAN BLVD
 HELLAND, OH 43028
 (614) 881-1111 WWW.ADS-PIPE.COM

5 SHEET OF 8



- INSPECTION & MAINTENANCE**
- STEP 1) INSPECT ISOLATOR ROW PLUS FOR SEDIMENT
- INSPECTION PORTS (IF PRESENT)
 - REMOVE OPEN LID ON NYLOPLAST INLINE DRAIN
 - REMOVE AND CLEAN FLEXSTORM FILTER IF INSTALLED
 - USING A FLASHLIGHT AND STADIA ROD, MEASURE DEPTH OF SEDIMENT AND RECORD ON MAINTENANCE LOG
 - LOWER A CAMERA INTO ISOLATOR ROW PLUS FOR VISUAL INSPECTION OF SEDIMENT LEVELS (OPTIONAL)
 - IF SEDIMENT IS AT, OR ABOVE, 3" (80 mm) PROCEED TO STEP 2. IF NOT, PROCEED TO STEP 3.
 - ALL ISOLATOR PLUS ROWS
 - REMOVE COVER FROM STRUCTURE AT UPSTREAM END OF ISOLATOR ROW PLUS
 - USING A FLASHLIGHT, INSPECT DOWN THE ISOLATOR ROW PLUS THROUGH OUTLET PIPE
 - MIRRORS ON POLES OR CAMERAS MAY BE USED TO AVOID A CONFINED SPACE ENTRY
 - FOLLOW OSHA REGULATIONS FOR CONFINED SPACE ENTRY IF ENTERING MAN-HOLE
 - IF SEDIMENT IS AT, OR ABOVE, 3" (80 mm) PROCEED TO STEP 2. IF NOT, PROCEED TO STEP 3.
- STEP 2) CLEAN OUT ISOLATOR ROW PLUS USING THE JETVAC PROCESS
- A FREEZING CLEANING NOZZLE WITH REAR FACING SPREAD OF 45° (1.1 m) OR MORE IS PREFERRED
 - APPLY MULTIPLE PASSES OF JETVAC UNTIL BACKFLUSH WATER IS CLEAN
 - VACUUM STRUCTURE SUMP AS REQUIRED
- STEP 3) REPLACE ALL COVERS, GRATES, FILTERS, AND LIDS; RECORD OBSERVATIONS AND ACTIONS.
- STEP 4) INSPECT AND CLEAN BASINS AND MANHOLES UPSTREAM OF THE STORMTECH SYSTEM.
- NOTES**
- INSPECT EVERY 6 MONTHS DURING THE FIRST YEAR OF OPERATION. ADJUST THE INSPECTION INTERVAL BASED ON PREVIOUS OBSERVATIONS OF SEDIMENT ACCUMULATION AND HIGH WATER ELEVATIONS.
 - CONDUCT JETTING AND VACUUMING ANNUALLY OR WHEN INSPECTION SHOWS THAT MAINTENANCE IS NECESSARY.
- NOTE: INSPECTION PORTS MAY BE CONNECTED THROUGH ANY CHAMBER CORRUGATION CREST.

SCOTIA WESTERN STATES HOUSING
 DATE: 2/2/21 DRAWN: KJL
 PROJECT #: S200602 CHECKED: KJL

Stormtech
 4640 BUEHMAN BLVD
 HELLAND, OH 43028
 (614) 881-1111 WWW.STORMTECH.COM

ADS
 4640 BUEHMAN BLVD
 HELLAND, OH 43028
 (614) 881-1111 WWW.ADS-PIPE.COM

6 SHEET OF 8

City of Portland
 REVIEWED FOR
 CODE COMPLIANCE
 Date: 01/03/22
 Permit #: 20-19429-000-00

RECEIVED 10/20/2021



LU 22-187796 APD AP

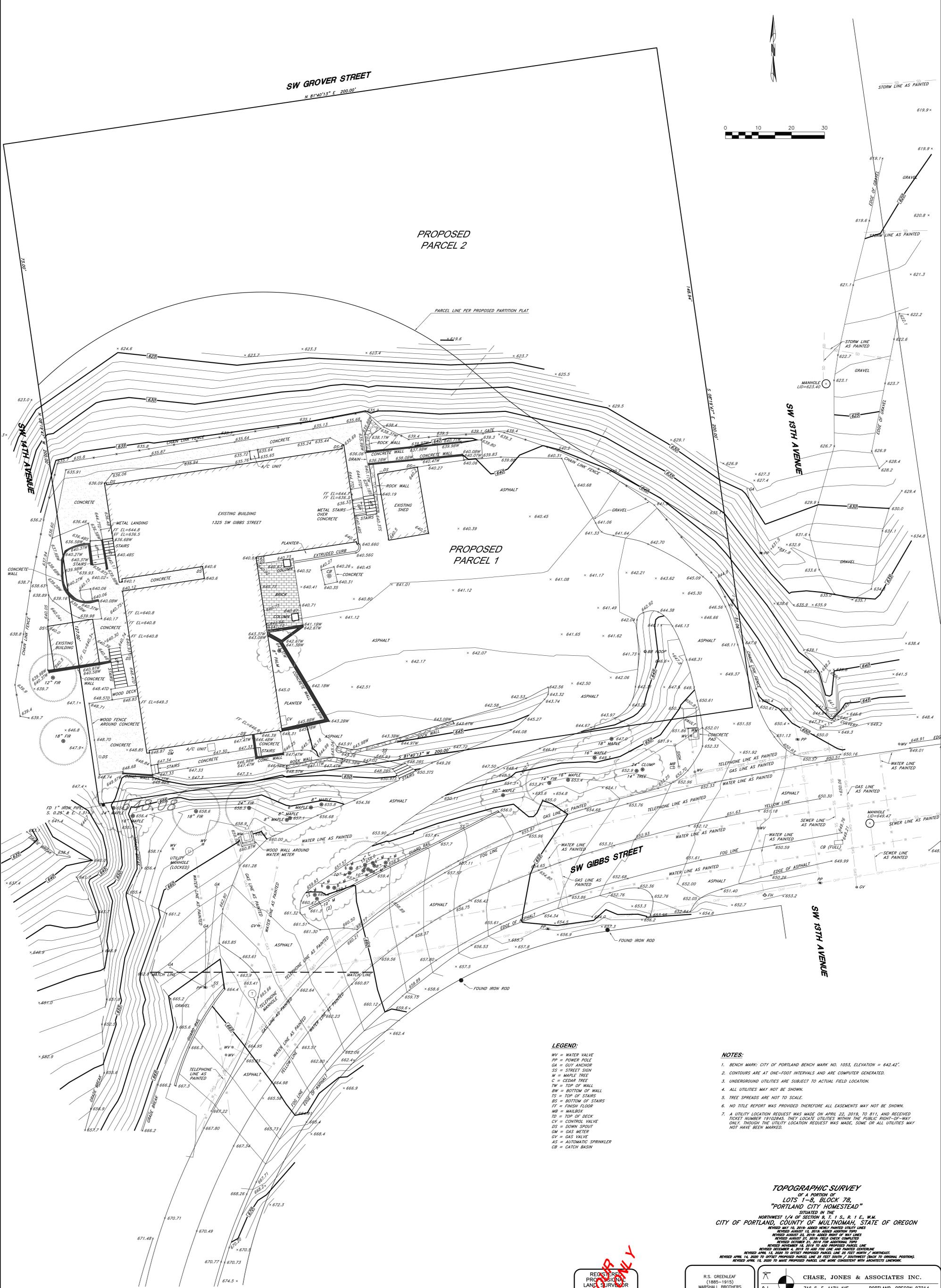
SW GROVER STREET
N 81°40'13" E 200.00'



PROPOSED
PARCEL 2

PARCEL LINE PER PROPOSED PARTITION PLAT

PROPOSED
PARCEL 1



- LEGEND:**
- WV = WATER VALVE
 - PP = POWER POLE
 - GA = GULF ANCHOR
 - SS = STREET SIGN
 - M = MAPLE TREE
 - C = CEDAR TREE
 - TW = TOP OF WALL
 - BS = BOTTOM OF STAIRS
 - FF = FINISH FLOOR
 - MB = MAILBOX
 - TD = TOP OF DECK
 - CV = CONTROL VALVE
 - DS = DOWN SPOUT
 - GM = GAS METER
 - GV = GAS VALVE
 - AS = AUTOMATIC SPRINKLER
 - CB = CATCH BASIN

NOTES:

1. BENCH MARK: CITY OF PORTLAND BENCH MARK NO. 1055, ELEVATION = 642.42'
2. CONTOURS ARE AT ONE-FOOT INTERVALS AND ARE COMPUTER GENERATED.
3. UNDERGROUND UTILITIES ARE SUBJECT TO ACTUAL FIELD LOCATION.
4. ALL UTILITIES MAY NOT BE SHOWN.
5. TREE SPREADS ARE NOT TO SCALE.
6. NO TITLE REPORT WAS PROVIDED THEREFORE ALL EASEMENTS MAY NOT BE SHOWN.
7. A UTILITY LOCATION REQUEST WAS MADE ON APRIL 22, 2019, TO B11, AND RECEIVED TICKET NUMBER 19102845. THEY LOCATE UTILITIES WITHIN THE PUBLIC RIGHT-OF-WAY ONLY. THROUGH THE UTILITY LOCATION REQUEST WAS MADE, SOME OR ALL UTILITIES MAY NOT HAVE BEEN MARKED.

TOPOGRAPHIC SURVEY
OF A PORTION OF
LOTS 1-B, BLOCK 78,
"PORTLAND CITY HOMESTEAD"
SITUATED IN THE
NORTHWEST 1/4 OF SECTION 9, T. 1 S., R. 1 E., W.M.
CITY OF PORTLAND, COUNTY OF MULTNOMAH, STATE OF OREGON

REVISIONS:
REVISED MAY 18, 2019, ADDED NEWLY MARKED UTILITY LINES
REVISED AUGUST 15, 2019, ADDED ADDITIONAL TOWNSHIP
REVISED MARCH 15, 2019, ADDED BOTTOM OF WALL LINES
REVISED AUGUST 27, 2019, FULL CHECK COMPLETED
REVISED OCTOBER 21, 2019, TOP ADDITIONAL TOWNSHIP
REVISED NOVEMBER 14, 2019, TOP ADDITIONAL PARCEL LINE
REVISED DECEMBER 4, 2019, TO ADD FOG LINE AND PAINTED CENTERLINE
REVISED APRIL 14, 2020 TO GREET PROPOSED PARCEL LINE 25 FEET NORTH / NORTHWEST
REVISED APRIL 14, 2020 TO OFFSET PROPOSED PARCEL LINE 25 FEET SOUTH / SOUTHWEST (BACK TO ORIGINAL POSITION)
REVISED APRIL 14, 2020 TO MAKE PROPOSED PARCEL LINE MORE CONSISTENT WITH ARCHITECTS LAYOUT

REGISTERED
PROFESSIONAL
LAND SURVEYOR

OREGON
1982
ERIC JONES
1996

GEN. 6-30-21

R.S. GREENLEAF (1888-1915) MARSHALL BROTHERS (1915-1957) BOOTH & WRIGHT (1957-1977) SETON, JOHNSON & ODELL (1977-1983) CHASE, JONES & ASSOCIATES, INC. (1983-)		CHASE, JONES & ASSOCIATES INC. 716 S. E. 11TH AVE. PORTLAND, OREGON 97214 PHONE: (503) 228-9844	
PROJECT NO. 15404	1/4 SECTION 3327	DATE MAY 2, 2019	SHEET 1
DRAWN BY DLW	CHECKED BY EDJ	SCALE 1" = 10'	OF 1

LU 22-187796 APD AP
Exhibit C.8



City of Portland, Oregon
Bureau of Development Services
Land Use Services
FROM CONCEPT TO CONSTRUCTION

Dan Ryan, Commissioner
Rebecca Esau, Director
Phone: (503) 823-7300
Fax: (503) 823-5630
TTY: (503) 823-6868
www.portland.gov/bds

Date: November 15, 2022

From: Leah Dawkins, Land Use Services
503-865-6734 / Leah.Dawkins@portlandoregon.gov

REQUEST FOR RESPONSE

Case File: LU 22-187796 APD AP
Pre App: PC # 22-144899

This notice is being sent to all service and technical review agencies for their input on the proposal described below. Neighborhood Associations also receive this advance notice via e-mail. Your timely response, as indicated below, will help the assigned planner determine if applicable approval criteria can be met, or what conditions might be required.

- The approval criteria are listed below. Although we are interested in any comments you may have, please consider your response in terms of these criteria.
- All agencies are encouraged to use this as an opportunity to inform the applicant of any additional requirements that may be imposed by your agency during building permit phase – especially those that would significantly affect the proposal.
- Please note in your response which requirements are specifically associated with the applicable land use review approval criteria, and which requirements you have the independent authority to impose at time of building permits.
- Neighborhood Associations are encouraged to submit comments by the deadline noted below. To comment, you may write to Leah Dawkins at 1900 SW Fourth Ave., Suite 4500, Portland, OR 97201. You can also e-mail your comments to me at my e-mail address identified above. After the staff report is published, please submit your comments to the Hearings Officer at 1900 SW Fourth Avenue, Portland, OR 97201 and fax them to 503-823-4347.

The Bureau of Development Services recommendation will be published ten days before the scheduled hearing date. You will also receive a Notice of Public Hearing for this proposal, with hearing date and time confirmed, mailed twenty days prior to the hearing.

- **Please send your response to BDS no later than: December 9, 2022** (If I receive comments after this date, I may not have enough time to include them in the staff report).
- **We must publish our report by: December 16, 2022**
- **A public hearing before the Hearings Officer is tentatively scheduled for December 28, 2022, at 2:00 pm.**

Applicant: Jesse Winterowd
Winterbrook Planning
610 SW Alder Street, #810
Portland, OR 97205
(503) 827-4422 / jesse@winterbrookplanning.com

Owner/Agent: SWS PDX LLC
15110 Boones Ferry Road, #500
Lake Oswego, OR 97035

Developer: Sean O'Neill
Scotia Western States Housing, LLC
6340 N Campbell Avenue, #240
Tucson, AZ 85718

Site Address: 1325 SW Gibbs Street

Legal Description: Partition Plat 2021-41, Lot 1, INC UND INT Tract A
Tax Account No.: R649931610
State ID No.: 1S1E09BD 02801
Quarter Section: 3327

Neighborhood: Homestead, contact at land-use@homesteadna.org
Business District: None
District Coalition: Southwest Neighborhoods Inc., contact Sylvia Bogert at 503-823-4592.
Plan District: None
Other Designations: Potential Landslide Hazard
Zoning: Residential 7,000 (R7), Residential Multi-Dwelling 2 (RM2),
Environmental Conservation Overlay (c), Constrained Sites Overlay (z)

Case Type: Planned Development Amendment (APD), Partition Amendment (AP)
Procedure: Type III, with a public hearing before the Hearings Officer. The decision of the Hearings Officer can be appealed to City Council.

Proposal:

The applicant received Land Use approval for a 5-story 27-unit multi-dwelling structure with 28 vehicle parking spaces and 30 long-term bike parking spaces through a Planned Development Review, a Land Division Partition, and Environmental Modifications submitted in 2019 (LU 19-258445 PD LDP ENM). The applicant is now proposing an amendment to the original Planned Development and Partition approvals to allow for an increase in the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces. The building envelope, façade, site access, and circulation are not proposed to change. The environmental tract created in the 2019 land use review will not change. Previously approved utility access will not change.

To better meet the outdoor area requirements and comply with new façade articulation requirements, the applicant has requested the following modifications through the Planned Development Amendment:

- Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2);
- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);

- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

This proposal is reviewed through a Type III procedure because an increase in density is considered a major change to an Approved Planned Development and the original Planned Development application was processed through a Type III procedure (33.854.500-.510). The Land Division Amendment is being processed concurrently through the Type III procedure.

Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of *Title 33*. The applicable approval criteria are found in sections:

- **33.854.310.E, Approval Criteria for Planned Developments**
- **33.854.320, Approval Criteria for Modifications of Site-Related Development Standards**
- **33.660.320.B, Approval Criteria for Other Changes to an Approved Preliminary Plan**

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was filed, provided that the application is complete at the time of filing, or complete within 180 days. This application was filed on September 28, 2022 and determined to be November 7, 2022.

It is important to submit all evidence to the Hearings Officer. City Council will not accept additional evidence if there is an appeal of this proposal.

Enclosures: Zoning Map, Site Plans, Façade Plan

Date: November 18, 2022

To: Jesse Winterowd, WINTERBROOK PLANNING
610 SW Alder St #810, Portland OR 97205
(503) 827-4422

From: Leah Dawkins

RE: LU 22-187796 APD AP

Dear Applicant:

I have received your application for a Planned Development Amendment AP at 1311 SW GIBBS ST. Your application was deemed complete on **November 7, 2022**. Your case number is given above; the hearing is scheduled for **December 28, 2022 at 2:00 pm**. I am the planner handling your case and can answer any questions you might have during the process.

The Zoning Code requires you to post notice on the site of your proposal 30 days before the hearing. The information below will help you do this. If you did not pick up poster boards from the Development Services Center when you filed your application, please contact me at least 24 hours in advance so the posting boards can be prepared for you to pick up. My phone number is (503) 865-6734. I am enclosing the notice that should be placed on the signs.

- A. You must post one of these signs every 600 feet, or fraction thereof, on each street frontage of the property. Because you have approximately 200 feet of street frontage, you should post 1 sign.
- B. These signs must be placed within 10 feet of the street frontage line, and must be visible to pedestrians and motorists. You may not post in the public right-of-way.
- C. Because the hearing for your case is scheduled for **December 28, 2022 at 2:00 pm**, you must post the notice by **November 28, 2022**, 30 days before the hearing.
- D. A certification statement is enclosed, which you must sign and return. The statement affirms that you posted the site. It also confirms your understanding that if you do not post the notice by the date above, your hearing will be automatically postponed. In addition, time limits on our processing of your case will be waived. You must return this statement to us by December 14, 2022, 14 days before the hearing.
- E. You should not remove the notice before the hearing, but it must be taken down within two weeks after the final decision is made on your request.

Encl: Posting Notice
Statement Certifying Posting

cc: Application Case File

POSTING NOTICE

CASE FILE: LU 22-187796 APD AP
PC # 22-144899
REVIEW BY: Hearings Officer
WHEN: December 28, 2022 at 2:00 pm
WHERE: 1900 SW Fourth Ave., Suite 3000
Portland, OR 97201

It is important to submit all evidence to the Hearings Officer. City Council will not accept additional evidence if there is an appeal of this proposal.

Land-Use Review: Planned Development Amendment (APD), Partition Amendment (AP)

Proposal: The applicant received Land Use approval for a 5-story 27-unit multi-dwelling structure in 2019. The applicant is now proposing an amendment to the original Planned Development and Partition approvals to increase the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces.

The following modifications have been requested through the Planned Development Amendment:

- Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2);
- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

Location: 1311 SW GIBBS ST

Legal Description: Partition Plat 2021-41, Lot 1, INC UND INT Tract A

Zoning/Designation: R7 & RM2c & z

Further information is available from the Bureau of Development Services. Please contact Leah Dawkins at (503) 865-6734 or at 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201.

The Bureau of Development Services is committed to providing equal access to information and hearings. If you need special accommodations, please call 503-823-7300 (TTY 503-823-6868).

POSTING NOTICE

CASE FILE: LU 22-187796 APD AP
PC # 22-144899
REVIEW BY: Hearings Officer
WHEN: December 28, 2022 at 2:00 pm

This land use hearing will be held remotely via the Zoom platform.

More information on how to participate is available at www.portland.gov/omf/hearings/land-use and with the Notice of Public Hearing and posted at <http://www.portlandoregon.gov/bds/35625>. You may also contact the planner at the phone number or email address at the bottom of this page.

It is important to submit all evidence to the Hearings Officer. City Council will not accept additional evidence if there is an appeal of this proposal.

Land-Use Review: Planned Development Amendment (APD), Partition Amendment (AP)

Proposal: The applicant is now proposing an amendment to 2019 Planned Development and Partition proposals to increase the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces.

The following modifications have been requested through the Planned Development Amendment:

- Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2);
- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

Location: 1311 SW GIBBS ST

Legal Description: Partition Plat 2021-41, Lot 1, INC UND INT Tract A

Zoning/Designation: R7 & RM2c & z

Further information is available from the Bureau of Development Services. Please contact Leah Dawkins at (503) 865-6734 or at leah.dawkins@portlandoregon.gov. **The Bureau of Development Services is committed to providing equal access to information and hearings. If you need special accommodations, please call 503-823-7300 (TTY 503-823-6868).**

Jesse Winterowd, WINTERBROOK PLANNING
610 SW Alder St #810
Portland OR 97205
(503) 827-4422

DATE: November 29, 2022

TO: Leah Dawkins
Bureau of Development Services
1900 SW Fourth Ave., Suite 5000
Portland, Oregon 97201

APPLICANT'S STATEMENT CERTIFYING POSTING

Case File LU 22-187796

This certifies that I have posted notice on my site as required by the Zoning Code. I understand that the hearing is scheduled for December 28, 2022 at 2:00 pm, and that I was required to post the property at least 30 days before the hearing.

The required number of poster boards, with the notices attached, were set up on November 28, 2022. These were placed within 10 feet of the street frontage line so that they were visible to pedestrians and motorists.

I understand that this form must be returned to the Bureau of Development Services no later than December 14, 2022, 14 days before the scheduled hearing. I also understand that if I do not post the notices by 30 days before the hearing, or return this form by 14 days before the hearing, my hearing will automatically be postponed. I also understand this will result in a waiver of the time limits for processing my case.

In addition, I understand that I may not remove the notices before the hearing, but am required to remove them within two weeks of the final decision on my request.



Signature

Jaime Crawford

Print Name

610 SW Alder, Suite 810

Address

Portland, OR 97205

City/State/Zip Code

	A	B	C	D	E	F
1	ENDORSEMENT	INFO1	INFO2	NAME	ADDRESS/IO ADDRESS	CITYSTATEZIP/ADDRESSEE
2	RETURN SERVICE REQUESTED		1S1E09AC 1700	MARQUAM HEIGHTS LLC	6340 N CAMPBELL AVE #240	TUCSON AZ 85718
3	RETURN SERVICE REQUESTED		1S1E09AC 5200	STARLETON JOSEPH & ROXANNE	13700 FIELDING RD	LAKE OSWEGO OR 97034
4	RETURN SERVICE REQUESTED		1S1E09AC 5300	EL GIBBS LLC	2233 NW 23RD AVE #100	PORTLAND OR 97210
5	RETURN SERVICE REQUESTED		1S1E09AC 5500	ORCINO EMMANUEL & ORCINO HELEN	1136 SW GIBBS ST	PORTLAND OR 97239
6	RETURN SERVICE REQUESTED	1S1E09AC 5600	ALIONHART ALEXANDER CHASE &	TIBBITTS GORDON C III	3316 SW 12TH AVE	PORTLAND OR 97239
7	RETURN SERVICE REQUESTED		1S1E09AC 6100	O'NEEL BRIAN D & O'NEEL AMY G	26315 NE BUTTEVILLE RD	AURORA OR 97002
8	RETURN SERVICE REQUESTED		1S1E09AC 6201	KEYASHIAN KIAN & KEYASHIAN FARANGIS	7001 SUNNE LN #501	WALNUT CREEK CA 94597-3618
9	RETURN SERVICE REQUESTED		1S1E09BD 1100	PORTLAND CITY OF	1221 SW 4TH AVE #130	PORTLAND OR 97204-1900
10	RETURN SERVICE REQUESTED		1S1E09BD 1700	LANDICA LLC	4404 SE 27TH AVE	PORTLAND OR 97202
11	RETURN SERVICE REQUESTED		1S1E09BD 1800	HILL TERRACE APARTMENTS LLC	PO BOX 467	LAKE OSWEGO OR 97034-0467
12	RETURN SERVICE REQUESTED		1S1E09BD 1900	LI GUANQUN & ZHAO CUIWEI	3203 SW 12TH AVE	PORTLAND OR 97239-3069
13	RETURN SERVICE REQUESTED		1S1E09BD 2000	HUNTER EXEMPTION TR	1840 SW 12TH AVE	PORTLAND OR 97225
14	RETURN SERVICE REQUESTED		1S1E09BD 2100	HANNA FAMILY TR	2724 NW IMPERIAL TER	PORTLAND OR 97210-3316
15	RETURN SERVICE REQUESTED		1S1E09BD 2200	BIGGERSTAFF JOINT TR	PO BOX 6837	PORTLAND OR 97228-6837
16	RETURN SERVICE REQUESTED		1S1E09BD 2300	KATAGIRI STEPHEN & KATAGIRI CLAUDIA	1233 SW GIBBS ST	PORTLAND OR 97239-3037
17	RETURN SERVICE REQUESTED		1S1E09BD 2400	QIN HUA & GU QIANG	3228 SW 13TH AVE	PORTLAND OR 97239
18	RETURN SERVICE REQUESTED		1S1E09BD 2500	CHEN QI & ZHAO XIAOLING	3216 SW 13TH AVE	PORTLAND OR 97239
19	RETURN SERVICE REQUESTED		1S1E09BD 2600	CERVANTES FLAVIA G	3206 SW 13TH AVE	PORTLAND OR 97239
20	RETURN SERVICE REQUESTED		1S1E09BD 3700	PORTLAND CITY OF	1900 SW 4TH AVE #7007	PORTLAND OR 97201
21	RETURN SERVICE REQUESTED		1S1E09BD 3800	MCALPINE WILLIAM & MCALPINE LORRIE	3329 SW 13TH AVE	PORTLAND OR 97239
22	RETURN SERVICE REQUESTED		1S1E09BD 4200	BOMALASKI MARTIN & BOMALASKI MAGGI	3326 SW 13TH AVE	PORTLAND OR 97239
23	RETURN SERVICE REQUESTED		1S1E09BD 4201	BURNS LISA M	3350 SW 13TH AVE	PORTLAND OR 97239
24	RETURN SERVICE REQUESTED		1S1E09BD 4300	HAIRANI MEHRDAD & HARATIPOUR NAZILA	3318 SW 13TH AVE	PORTLAND OR 97239
25	RETURN SERVICE REQUESTED		1S1E09BD 4400	NOSTRAND BRADBURY & NOSTRAND CATHY	604 SE UMATILLA ST	PORTLAND OR 97202
26	RETURN SERVICE REQUESTED		1S1E09BD 4500	MCALPINE MATTHEW & MCALPINE KATHRYN	1224 SW GIBBS ST	PORTLAND OR 97239
27	RETURN SERVICE REQUESTED		1S1E09BD 4700	ELLMERS KAREN L	3315 SW 12TH AVE	PORTLAND OR 97239
28	RETURN SERVICE REQUESTED	1S1E09BD 4800	STEVENSON MARILYN E TR &	STEVENSON JOHN L JR TR C/O S SWANEY	62515 STENKAMP RD	BEND OR 97701-9339
29	RETURN SERVICE REQUESTED		1S1E09BD 4900	SCHREINER PATRICIA A S	3560 SW BANCROFT CT	PORTLAND OR 97239
30	RETURN SERVICE REQUESTED		1S1E09BD 5000	L & L DUPLEX LLC	11405 SW 90TH AVE	TIGARD OR 97223-6406
31	RETURN SERVICE REQUESTED		1S1E09BD 5600	WATSON MICHAEL A	3412 SW 13TH AVE	PORTLAND OR 97239-2924
32	RETURN SERVICE REQUESTED		1S1E09BD 5700	FISCHER EDWARD & FISCHER GANEEN	3404 SW 13TH AVE	PORTLAND OR 97239-2924
33	RETURN SERVICE REQUESTED		1S1E09BD 5900	ADAMS BRUCE E & ADAMS CONSTANCE A	3411 SW 13TH AVE	PORTLAND OR 97239-2923
34	RETURN SERVICE REQUESTED		1S1E09BD 6300	MURPHY KENNETH C & MURPHY MONA C	3418 SW 14TH AVE	PORTLAND OR 97239
35	RETURN SERVICE REQUESTED		1S1E09BD 6400	MURPHY HARRY C & MURPHY SUZANN M B	4475 SW SCHOLLS FERRY RD #110	PORTLAND OR 97225
36				CURRENT RESIDENT	1229 SW GROVER ST #108	PORTLAND OR 97239
37				CURRENT RESIDENT	1229 SW GROVER ST #110	PORTLAND OR 97239
38				CURRENT RESIDENT	1238 SW GIBBS ST	PORTLAND OR 97239
39				CURRENT RESIDENT	1245 SW GROVER ST #205	PORTLAND OR 97239
40				CURRENT RESIDENT	1245 SW GROVER ST #301	PORTLAND OR 97239
41				CURRENT RESIDENT	1245 SW GROVER ST #304	PORTLAND OR 97239
42				CURRENT RESIDENT	1245 SW GROVER ST #307	PORTLAND OR 97239
43				CURRENT RESIDENT	3101 SW 13TH AVE #25	PORTLAND OR 97239
44				CURRENT RESIDENT	3101 SW 13TH AVE #26	PORTLAND OR 97239
45				CURRENT RESIDENT	3111 SW 13TH AVE #1	PORTLAND OR 97239
46				CURRENT RESIDENT	3111 SW 13TH AVE #13	PORTLAND OR 97239
47				CURRENT RESIDENT	3111 SW 13TH AVE #14	PORTLAND OR 97239
48				CURRENT RESIDENT	3115 SW 12TH AVE #211	PORTLAND OR 97239
49				CURRENT RESIDENT	3115 SW 12TH AVE #212	PORTLAND OR 97239
50				CURRENT RESIDENT	3327 SW 12TH AVE	PORTLAND OR 97239
51				CURRENT RESIDENT	3337 SW 12TH AVE	PORTLAND OR 97239
52				CURRENT RESIDENT	3423 SW 14TH AVE	PORTLAND OR 97239
53				CURRENT RESIDENT	1203 SW GIBBS ST #A	PORTLAND OR 97239
54				CURRENT RESIDENT	1245 SW GROVER ST #101	PORTLAND OR 97239
55				CURRENT RESIDENT	1245 SW GROVER ST #302	PORTLAND OR 97239
56				CURRENT RESIDENT	3101 SW 13TH AVE #19	PORTLAND OR 97239
57				CURRENT RESIDENT	3101 SW 13TH AVE #20	PORTLAND OR 97239
58				CURRENT RESIDENT	3101 SW 13TH AVE #22	PORTLAND OR 97239
59				CURRENT RESIDENT	3110 SW 13TH AVE #104	PORTLAND OR 97239
60				CURRENT RESIDENT	3110 SW 13TH AVE #105	PORTLAND OR 97239
61				CURRENT RESIDENT	3111 SW 13TH AVE #2	PORTLAND OR 97239
62				CURRENT RESIDENT	3111 SW 13TH AVE #3	PORTLAND OR 97239
63				CURRENT RESIDENT	3111 SW 13TH AVE #4	PORTLAND OR 97239
64				CURRENT RESIDENT	3111 SW 13TH AVE #5	PORTLAND OR 97239
65				CURRENT RESIDENT	3115 SW 12TH AVE #209	PORTLAND OR 97239
66				CURRENT RESIDENT	3115 SW 12TH AVE #309	PORTLAND OR 97239
67				CURRENT RESIDENT	3211 SW 12TH AVE	PORTLAND OR 97239
68				CURRENT RESIDENT	3224 SW 12TH AVE	PORTLAND OR 97239
69				CURRENT RESIDENT	3303 SW 13TH AVE #A	PORTLAND OR 97239
70				CURRENT RESIDENT	3303 SW 13TH AVE #B	PORTLAND OR 97239
71				CURRENT RESIDENT	3409 SW 12TH AVE	PORTLAND OR 97239
72				CURRENT RESIDENT	3418 SW 14TH AVE #2	PORTLAND OR 97239
73				CURRENT RESIDENT	1137 SW WHITAKER ST #A	PORTLAND OR 97239

	A	B	C	D	E	F
74				CURRENT RESIDENT	1203 SW GIBBS ST #B	PORTLAND OR 97239
75				CURRENT RESIDENT	1229 SW GROVER ST #111	PORTLAND OR 97239
76				CURRENT RESIDENT	1229 SW GROVER ST #113	PORTLAND OR 97239
77				CURRENT RESIDENT	1245 SW GROVER ST #201	PORTLAND OR 97239
78				CURRENT RESIDENT	1245 SW GROVER ST #202	PORTLAND OR 97239
79				CURRENT RESIDENT	1245 SW GROVER ST #207	PORTLAND OR 97239
80				CURRENT RESIDENT	1245 SW GROVER ST #305	PORTLAND OR 97239
81				CURRENT RESIDENT	1245 SW GROVER ST #306	PORTLAND OR 97239
82				CURRENT RESIDENT	3101 SW 13TH AVE #16	PORTLAND OR 97239
83				CURRENT RESIDENT	3101 SW 13TH AVE #17	PORTLAND OR 97239
84				CURRENT RESIDENT	3101 SW 13TH AVE #24	PORTLAND OR 97239
85				CURRENT RESIDENT	3110 SW 13TH AVE #102	PORTLAND OR 97239
86				CURRENT RESIDENT	3110 SW 13TH AVE #103	PORTLAND OR 97239
87				CURRENT RESIDENT	3111 SW 13TH AVE #10	PORTLAND OR 97239
88				CURRENT RESIDENT	3111 SW 13TH AVE #6	PORTLAND OR 97239
89				CURRENT RESIDENT	3111 SW 13TH AVE #7	PORTLAND OR 97239
90				CURRENT RESIDENT	3115 SW 12TH AVE #208	PORTLAND OR 97239
91				CURRENT RESIDENT	3115 SW 12TH AVE #210	PORTLAND OR 97239
92				CURRENT RESIDENT	3340 SW 12TH AVE #A	PORTLAND OR 97239
93				CURRENT RESIDENT	3340 SW 14TH AVE	PORTLAND OR 97239
94				CURRENT RESIDENT	3405 SW 12TH AVE	PORTLAND OR 97239
95				CURRENT RESIDENT	1206 SW GIBBS ST	PORTLAND OR 97239
96				CURRENT RESIDENT	1229 SW GROVER ST #109	PORTLAND OR 97239
97				CURRENT RESIDENT	1229 SW GROVER ST #112	PORTLAND OR 97239
98				CURRENT RESIDENT	1245 SW GROVER ST #203	PORTLAND OR 97239
99				CURRENT RESIDENT	1245 SW GROVER ST #204	PORTLAND OR 97239
100				CURRENT RESIDENT	1245 SW GROVER ST #206	PORTLAND OR 97239
101				CURRENT RESIDENT	1245 SW GROVER ST #303	PORTLAND OR 97239
102				CURRENT RESIDENT	3101 SW 13TH AVE #18	PORTLAND OR 97239
103				CURRENT RESIDENT	3101 SW 13TH AVE #21	PORTLAND OR 97239
104				CURRENT RESIDENT	3101 SW 13TH AVE #23	PORTLAND OR 97239
105				CURRENT RESIDENT	3110 SW 13TH AVE #106	PORTLAND OR 97239
106				CURRENT RESIDENT	3110 SW 13TH AVE #107	PORTLAND OR 97239
107				CURRENT RESIDENT	3111 SW 13TH AVE #11	PORTLAND OR 97239
108				CURRENT RESIDENT	3111 SW 13TH AVE #12	PORTLAND OR 97239
109				CURRENT RESIDENT	3111 SW 13TH AVE #15	PORTLAND OR 97239
110				CURRENT RESIDENT	3111 SW 13TH AVE #8	PORTLAND OR 97239
111				CURRENT RESIDENT	3111 SW 13TH AVE #9	PORTLAND OR 97239
112				CURRENT RESIDENT	3115 SW 12TH AVE #213	PORTLAND OR 97239
113				CURRENT RESIDENT	3115 SW 12TH AVE #308	PORTLAND OR 97239
114				CURRENT RESIDENT	3115 SW 12TH AVE #310	PORTLAND OR 97239
115				CURRENT RESIDENT	3115 SW 12TH AVE #311	PORTLAND OR 97239
116				CURRENT RESIDENT	3115 SW 12TH AVE #312	PORTLAND OR 97239
117				CURRENT RESIDENT	3115 SW 12TH AVE #313	PORTLAND OR 97239
118				CURRENT RESIDENT	3225 SW 12TH AVE	PORTLAND OR 97239
119				CURRENT RESIDENT	3303 SW 13TH AVE #C	PORTLAND OR 97239
120				CURRENT RESIDENT	3405 SW 13TH AVE	PORTLAND OR 97239
121				CURRENT RESIDENT	3418 SW 14TH AVE #1	PORTLAND OR 97239
122	RETURN SERVICE REQUESTED	OWNER	1S1E09BD 2801	SWS PDX LLC	15110 BOONES FERRY RD #500	LAKE OSWEGO OR 97035
123	RETURN SERVICE REQUESTED	APPLICANT	WINTERBROOK PLANNING	WINTEROWD JESSE	610 SW ALDER ST #810	PORTLAND OR 97205
124	RETURN SERVICE REQUESTED	OWNERS AGENT	SCOTIA WESTERN STATES HOUSING LLC	ONEILL SEAN	15962 BOONES FERRY RD #202	LAKE OSWEGO OR 97035
125	RETURN SERVICE REQUESTED		LAND USE CONTACT	SOUTHWEST HILLS RESIDENTIAL LEAGUE	SW2257 NW RALEIGH ST	PORTLAND OR 97210
126	RETURN SERVICE REQUESTED		LAND USE CONTACT	OFFICE OF COMMUNITY & CIVIC LIFE	1120 SW 5TH AVE #114	PORTLAND OR 97204
127	RETURN SERVICE REQUESTED		HOMESTEAD NEIGHBORHOOD ASSOCIATION	JONES MILT	7805 SW 40TH AVE #80191	PORTLAND OR 97219
128	RETURN SERVICE REQUESTED		LAND USE CONTACT	FRIENDS OF TERWILLIGER	430 SW HAMILTON ST	PORTLAND OR 97239
129	RETURN SERVICE REQUESTED		LAND USE CONTACT	WASHINGTON CO - TRANSPORTATION	1400 SW WALNUT ST	HILLSBORO OR 97123
130	RETURN SERVICE REQUESTED			JUDY PETERS	6916 NE 40TH ST	VANCOUVER WA 98661
131	RETURN SERVICE REQUESTED			KARLA MOORE-LOVE (CITY HALL)	1221 SW 4TH AVE #130	PORTLAND OR 97204
132	RETURN SERVICE REQUESTED		RISK & LAND DEPARTMENT	NW NATURAL	250 SW TAYLOR ST	PORTLAND OR 97204-3038
133	RETURN SERVICE REQUESTED		LAND USE CONTACT	PACIFIC POWER & LIGHT	7544 NE 33RD DR	PORTLAND OR 97211
134	RETURN SERVICE REQUESTED		PORTLAND METRO REGIONAL SOLUTIONS	C/O DLCD REGIONAL REPRESENTATIVE	1600 SW FOURTH AVE #109	PORTLAND OR 97201
135	RETURN SERVICE REQUESTED		LAND USE CONTACT	PORT OF PORTLAND PLANNING	PO BOX 3529	PORTLAND OR 97208
136	RETURN SERVICE REQUESTED		LAND USE CONTACT	TRANSIT DEVELOPMENT	1800 SW FIRST AVE #300	PORTLAND OR 97201
137	RETURN SERVICE REQUESTED		PORTLAND SCHOOL DISTRICT	LAND USE NOTICE CONTACT	501 N DIXON	PORTLAND OR 97227
138				LAND USE CONTACT	PROSPER PORTLAND	129/PROSPER
139				PORTLAND PARK TRAIL	TATE WHITE	B106/R1302
140					HEARINGS CLERK	299/3100
141					DAWN KRANTZ	B299/R5000

Date: December 5, 2022
To: Interested Person
From: Leah Dawkins, Land Use Services
503-865-6734 / Leah.Dawkins@portlandoregon.gov

**NOTICE OF A PUBLIC HEARING ON A PROPOSAL
IN YOUR NEIGHBORHOOD**

CASE FILE: LU 22-187796 APD AP
PC # 22-144899
REVIEW BY: Hearings Officer
WHEN: December 28, 2022, at 2:00 pm

This land use hearing will have two options for participation:

1. Remote via the Zoom platform, or
2. In person at 1900 SW 4th Avenue, Room 3000 (3rd Floor), Portland, OR 97201.

See the instructions on how to participate attached to this notice.

It is important to submit all evidence to the Hearings Officer. City Council will not accept additional evidence if there is an appeal of this proposal.

Development has been proposed in your neighborhood requiring a land use review. The proposal, review process, and information on how to respond to this notice are described below. A copy of the site plan and zoning map are attached. I am the staff person handling the case. Please call me if you have questions regarding this proposal. Please contact the applicant if you have questions regarding any future development on the site.

Applicant: Jesse Winterowd, Winterbrook Planning
610 SW Alder Street, #810 / Portland, OR 97205
(503) 827-4422 / jesse@winterbrookplanning.com

Owner: SWS PDX LLC
15110 Boones Ferry Road, #500 / Lake Oswego, OR 97035

Owner's Agent: Sean O'Neill, Scotia Western States Housing LLC
15962 Boones Ferry Rd, Ste 202 / Lake Oswego, OR 97035

Site Address: 1311 SW Gibbs Street

Legal Description: Partition Plat 2021-41, Lot 1, INC UND INT Tract A
Tax Account No.: R649931610
State ID No.: 1S1E09BD 02801
Quarter Section: 3327

Neighborhood: Homestead, contact Milt Jones at land-use@homesteadna.org
Business District: None
District Coalition: Office of Community & Civic Life, contact at CivicLife@portlandoregon.gov

Plan District: None
Other Designations: Potential Landslide Hazard
Zoning: Residential 7,000 (R7), Residential Multi-Dwelling 2 (RM2), Environmental Conservation Overlay (c), Constrained Sites Overlay (z)

Case Type: Planned Development Amendment (APD) Review, Partition Amendment (AP) Review
Procedure: Type III, with a public hearing before the Hearings Officer. The decision of the Hearings Officer can be appealed to City Council.

Proposal:

The applicant received Land Use approval for a 5-story 27-unit multi-dwelling structure with 28 vehicle parking spaces and 30 long-term bike parking spaces through a Planned Development Review, a Land Division Partition, and Environmental Modifications submitted in 2019 (LU 19-258445 PD LDP ENM). The applicant is now proposing an amendment to the original Planned Development and Partition approvals to allow for an increase in the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces. The building envelope, façade, site access, and circulation are not proposed to change. The environmental tract created in the 2019 land use review will not change. Previously approved utility access will not change.

To better meet the outdoor area requirements and comply with new façade articulation requirements, the applicant has requested the following modifications through the Planned Development Amendment:

- Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2);
- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

This proposal is reviewed through a Type III procedure because an increase in density is considered a major change to an Approved Planned Development and the original Planned Development application was processed through a Type III procedure (33.854.500-.510). The Land Division Amendment is being processed concurrently through the Type III procedure.

Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of *Title 33*. The applicable approval criteria are found in sections:

- **33.660.320.B, Approval Criteria for Other Changes to an Approved Preliminary Plan**
- **33.854.310.E, Approval Criteria for Planned Developments**
- **33.854.320, Approval Criteria for Modifications of Site-Related Development Standards**

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was filed, provided that the application is complete at the time of filing, or complete within 180 days. This application was filed on September 28, 2022 and determined to be November 7, 2022.

DECISION MAKING PROCESS

The Bureau of Development Services will be making a recommendation on this proposal; our report and recommendation will be available 10 days before the hearing. The Staff report will be posted on the Bureau of Development Services website at <http://www.portlandoregon.gov/bds/35625>. Land use review notices are listed by the District Coalition shown at the beginning of this document. If you are interested in viewing information in the file, please contact the planner listed on the front of this notice. The planner can email you documents from the file. A fee would be required for all requests for paper copies of file documents. Additional information about the City of Portland, and city bureaus is available online at <https://www.portland.gov>. A digital copy of the Portland Zoning Code is available online at <https://www.portlandoregon.gov/zoningcode>.

The neighborhood association, listed on the first page of this notice, may take a position on this application and may have scheduled an open meeting prior to making their recommendation to the Bureau of Development Services. Please contact the person listed as the neighborhood contact to determine the time and date of this meeting.

We are seeking your comments on this proposal. The evidentiary proceeding before the Hearings Officer will be the only opportunity for the parties to submit oral and written evidence in this matter. Any appeal to the City Council will be limited to legal arguments concerning the Hearings Officer decision and the evidence in the record compiled by the Hearings Officer in support of that decision.

To comment, you may write to the Land Use Hearings Officer, 1900 SW Fourth Ave., Suite 3100, Portland, OR 97201, FAX your comments to 503-823-4347, or e-mail your comments to HearingsOfficeClerks@portlandoregon.gov, or testify during the hearing. **Please see the enclosed insert for further information on how to testify.** In your comments, you should address the approval criteria. Please refer to the file number when seeking information or submitting testimony. Written comments must be received by the end of the hearing. Please note that all correspondence and testimony received will become part of the public record. The applicant and Bureau of Development Services staff will be available during the hearing to answer questions and respond to comments. The general order of appearance for oral testimony at the hearing is as follows: Bureau of Development Services staff report, applicant testimony, testimony of interested parties who wish to ask questions or testify, staff response and closing comments, and applicant's closing comments. Hearings Officer will make a decision about this proposal within 17 days of the close of the record.

The applicant and proponents have the burden of proof to show that each and every element of the approval criteria are satisfied. In order to prevail, the opponents must persuade the Hearings Officer to find that the applicant has not carried the burden of proof with regard to one or more of the approval criteria. The opponents may also explain to the Hearings Officer how or why the facts asserted by the applicant are not supported by evidence in the record. Opponents may wish to recommend conditions of approval which will make the proposal more acceptable, if approved.

Prior to the conclusion of the hearing, any participant may request an opportunity to present additional evidence or testimony regarding the application. If such a request is made, the record will be held open for seven days to receive the new evidence and the record shall be held open for at least an additional seven days to provide the other parties an opportunity to respond to that new evidence.

ORS 227.178 states the City must issue a final decision on land use review applications within 120 days of the application being deemed complete. The 120-day review period may be extended at the request of the applicant.

APPEAL PROCESS

You can appeal the Hearings Officer decision to City Council. However, the appeal to City Council will be conducted as an on-the-record review of the Hearings Officer decision. The City Council will consider legal arguments (for example arguments pointing out ways the Hearings Officer decision improperly interprets or applies relevant approval criteria). The City Council will also consider arguments that the Hearings Officer findings are not supported by the evidence submitted to the Hearings Officer. However, the City Council will not accept or consider new evidence that was not submitted to the Hearings Officer.

A fee is charged for appeals. Recognized neighborhood associations may qualify for an appeal fee waiver. City Council's decision may be appealed to the Oregon Land Use Board of Appeals (LUBA).

Failure to raise an issue (in writing or during the hearing) by the close of the record by the Hearings Officer in this case may preclude an appeal to LUBA on that issue. Also, if you do not provide enough detailed information to the Hearings Officer, they may not be able to respond to the issue you are trying to raise. In such a situation, an appeal to LUBA on that issue may not be allowed.

HEARING CANCELLATION

This public hearing will be cancelled due the inclement weather or other similar emergency.

- Hearings Officer: This public hearing will be cancelled if the City of Portland is closed. Check local television or the City of Portland website (www.portlandoregon.gov) for closures. Contact the Hearings Office at 503-823-7307, for immediate information regarding cancellations or rescheduling.
- Design Commission, Historic Landmarks Commission or Adjustment Committee: This public hearing will be cancelled if Portland Public Schools close due to inclement weather or other similar emergency. Check local television and radio reports for school closures. Please call the Bureau of Development Services at 503-823-7617, for information regarding cancellations or rescheduling.

The hearing will be rescheduled for the earliest possible date. A renotification notice will not be sent.

To attend the hearing, public transportation is available. Tri-Met buses stop near the BDS building at SW Fifth or Sixth Ave. at SW Hall St. and SW Harrison St. Call Tri-Met at 503-238-7433 (or www.trimet.org) for routes and times. Hourly rated public parking is available a half block south of the building on Fourth Ave.

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

Enclosure:

Zoning Map
Site Plan
Enlarged Plan
Façade Plan
Land Use Hearing Participation Information



Hearings Office

City of Portland

1900 SW 4th Avenue, Room 3100, Portland, OR 97201 phone: 503.823.7307

www.portland.gov/omf/hearings

email: HearingsOfficeClerks@portlandoregon.gov

fax: 503.823.4347



Land Use Hearing Participation Information

Case Number: LU 22-187796 APD AP (Hearings Office Case Number 4220018)

Site Address: 1311 SW Gibbs Street

Hearing Date: December 28, 2022

Hearing Time: 2:00 PM

The above-referenced land use hearing will take place via the Zoom platform.

Zoom Participation Options and Instructions

Zoom Option

- You will need an internet connection and a personal computer or other internet capable device.
- You can listen, view the City Planner’s PowerPoint presentation, and you have the opportunity to provide oral testimony.
- Instructions:

Go to:

<https://us06web.zoom.us/j/88936098415>

Telephone Option

- You will need a telephone.
- You will be able to listen and testify but you will not be able to view documents and presentations shared on the Zoom platform (if any).
- Instructions:

Dial: +1 669 900 6833

Meeting ID: 889 3609 8415#

Written Testimony

- Prior to the Hearing and until the close of evidence, you may submit written testimony. In other words, written testimony can be provided up to the close of the record, which will be announced at the hearing.
- *Note: A document’s timeliness is determined by the date/time when it is received by the Hearings Office Clerks.*

Portland Hearings Office
Case #4220018
Bureau Case #LU 22-187796 APD AP
Exhibit #2
Page 1 of 2

- Please reference the case number in any documents you wish to submit.
- Instructions/Options:

Online Case Management System

<https://www.portland.gov/omf/hearings/land-use>



Email: HearingsOfficeClerks@portlandoregon.gov

Fax: 503-823-4347

Mail: Hearings Office, 1900 SW 4th Avenue, Room 3100,
Portland, OR 97201

View File Contents

We use a fully electronic case file. Your online viewing options are available at our website: <https://www.portland.gov/omf/hearings/land-use>

If you do not have online access, you may make a written request for a hard copy of any exhibit to be mailed to you.

Meaningful Access

The City of Portland is committed to providing meaningful access.

For accommodations, modifications, translation, interpretation, or other services, please call 503-823-7307, the TTY at 503-823-6868 or the Oregon Relay Service: 711. Traducción e interpretación | Chuyển Ngữ hoặc Phiên Dịch | 翻译或传译

Письменный или устный перевод | 翻訳または通訳 | Traducere sau

Interpretare 번역 및 통역 | Письмовий або усний переклад | Turjumida ama Fasiraadda

Questions or Concerns

You may contact the Hearings Clerks via email or by leaving a voicemail with the office: HearingsOfficeClerks@portlandoregon.gov; 503-823-7307.

Portland Hearings Office
Case #4220018
Bureau Case #LU 22-187796 APD AP
Exhibit #2
Page 2 of 2

LU 22-187796 APD AP
Exhibit D.7



City of Portland, Oregon
Bureau of Development Services
Land Use Services
FROM CONCEPT TO CONSTRUCTION

Dan Ryan, Commissioner
Rebecca Esau, Director
Phone: (503) 823-7300
Fax: (503) 823-5630
TTY: (503) 823-6868
www.portland.gov/bds

Date: December 12, 2022
To: Interested Person
From: Leah Dawkins, Land Use Services
503-865-6734 / Leah.Dawkins@portlandoregon.gov

**NOTICE OF A PUBLIC HEARING ON A PROPOSAL
IN YOUR NEIGHBORHOOD**

**THIS IS A REVISED NOTICE. THE HEARING DATE, TIME, AND PROPOSAL
HAVE NOT CHANGED. PARTICIPATION INSTRUCTIONS FOR THE HEARING
HAVE BEEN UPDATED.**

CASE FILE: LU 22-187796 APD AP
PC # 22-144899
REVIEW BY: Hearings Officer
WHEN: December 28, 2022, at 2:00 pm

**This land use hearing will be held remotely via the Zoom platform.
See the instructions on how to participate attached to this notice.**

It is important to submit all evidence to the Hearings Officer. City Council will not accept additional evidence if there is an appeal of this proposal.

Development has been proposed in your neighborhood requiring a land use review. The proposal, review process, and information on how to respond to this notice are described below. A copy of the site plan and zoning map are attached. I am the staff person handling the case. Please call me if you have questions regarding this proposal. Please contact the applicant if you have questions regarding any future development on the site.

Applicant: Jesse Winterowd, Winterbrook Planning
610 SW Alder Street, #810 / Portland, OR 97205
(503) 827-4422 / jesse@winterbrookplanning.com

Owner/Agent: SWS PDX LLC
15110 Boones Ferry Road, #500
Lake Oswego, OR 97035

Developer: Sean O'Neill, Scotia Western States Housing, LLC
15962 Boones Ferry Rd, #202 / Lake Oswego, OR 97035

Site Address: 1311 SW Gibbs Street

Legal Description: Partition Plat 2021-41, Lot 1, INC UND INT Tract A
Tax Account No.: R649931610

State ID No.: 1S1E09BD 02801
Quarter Section: 3327

Neighborhood: Homestead, contact Milt Jones at land-use@homesteadna.org
Business District: None
District Coalition: Office of Community and Civic Life, contact at CivicLife@portlandoregon.gov

Plan District: None
Other Designations: Potential Landslide Hazard
Zoning: Residential 7,000 (R7), Residential Multi-Dwelling 2 (RM2), Environmental Conservation Overlay (c), Constrained Sites Overlay (z)

Case Type: Planned Development Amendment (APD), Partition Amendment (AP) Review
Procedure: Type III, with a public hearing before the Hearings Officer. The decision of the Hearings Officer can be appealed to City Council.

Proposal:

The applicant received Land Use approval for a 5-story 27-unit multi-dwelling structure with 28 vehicle parking spaces and 30 long-term bike parking spaces through a Planned Development Review, a Land Division Partition, and Environmental Modifications submitted in 2019 (LU 19-258445 PD LDP ENM). The applicant is now proposing an amendment to the original Planned Development and Partition approvals to allow for an increase in the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces. The building envelope, façade, site access, and circulation are not proposed to change. The environmental tract created in the 2019 land use review will not change. Previously approved utility access will not change.

To better meet the outdoor area requirements and comply with new façade articulation requirements, the applicant has requested the following modifications through the Planned Development Amendment:

- Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2);
- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

This proposal is reviewed through a Type III procedure because an increase in density is considered a major change to an Approved Planned Development and the original Planned Development application was processed through a Type III procedure (33.854.500-.510). The Land Division Amendment is being processed concurrently through the Type III procedure.

Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of *Title 33*. The applicable approval criteria are found in sections:

- **33.660.320.B, Approval Criteria for Other Changes to an Approved Preliminary Plan**
- **33.854.310.E, Approval Criteria for Planned Developments**
- **33.854.320, Approval Criteria for Modifications of Site-Related Development Standards**

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was filed, provided that the application is complete

at the time of filing, or complete within 180 days. This application was filed on September 28, 2022 and determined to be complete on November 7, 2022.

DECISION MAKING PROCESS

The Bureau of Development Services will be making a recommendation on this proposal; our report and recommendation will be available 10 days before the hearing. The Staff report will be posted on the Bureau of Development Services website at <http://www.portlandoregon.gov/bds/35625>. Land use review notices are listed by the District Coalition shown at the beginning of this document. If you are interested in viewing information in the file, please contact the planner listed on the front of this notice. The planner can email you documents from the file. A fee would be required for all requests for paper copies of file documents. Additional information about the City of Portland, and city bureaus is available online at <https://www.portland.gov>. A digital copy of the Portland Zoning Code is available online at <https://www.portlandoregon.gov/zoningcode>.

The neighborhood association, listed on the first page of this notice, may take a position on this application and may have scheduled an open meeting prior to making their recommendation to the Bureau of Development Services. Please contact the person listed as the neighborhood contact to determine the time and date of this meeting.

We are seeking your comments on this proposal. The evidentiary proceeding before the Hearings Officer will be the only opportunity for the parties to submit oral and written evidence in this matter. Any appeal to the City Council will be limited to legal arguments concerning the Hearings Officer decision and the evidence in the record compiled by the Hearings Officer in support of that decision.

To comment, you may write to the Land Use Hearings Officer, 1900 SW Fourth Ave., Suite 3100, Portland, OR 97201, FAX your comments to 503-823-4347, or e-mail your comments to HearingsOfficeClerks@portlandoregon.gov, or testify during the hearing. **Please see the enclosed insert for further information on how to testify.** In your comments, you should address the approval criteria. Please refer to the file number when seeking information or submitting testimony. Written comments must be received by the end of the hearing. Please note that all correspondence and testimony received will become part of the public record. The applicant and Bureau of Development Services staff will be available during the hearing to answer questions and respond to comments. The general order of appearance for oral testimony at the hearing is as follows: Bureau of Development Services staff report, applicant testimony, testimony of interested parties who wish to ask questions or testify, staff response and closing comments, and applicant's closing comments. Hearings Officer will make a decision about this proposal within 17 days of the close of the record.

The applicant and proponents have the burden of proof to show that each and every element of the approval criteria are satisfied. In order to prevail, the opponents must persuade the Hearings Officer to find that the applicant has not carried the burden of proof with regard to one or more of the approval criteria. The opponents may also explain to the Hearings Officer how or why the facts asserted by the applicant are not supported by evidence in the record. Opponents may wish to recommend conditions of approval which will make the proposal more acceptable, if approved.

Prior to the conclusion of the hearing, any participant may request an opportunity to present additional evidence or testimony regarding the application. If such a request is made, the record will be held open for seven days to receive the new evidence and the record shall be held open for at least an additional seven days to provide the other parties an opportunity to respond to that new evidence.

ORS 227.178 states the City must issue a final decision on land use review applications within 120 days of the application being deemed complete. The 120-day review period may be extended at the request of the applicant.

APPEAL PROCESS

You can appeal the Hearings Officer decision to City Council. However, the appeal to City Council will be conducted as an on-the-record review of the Hearings Officer decision. The City Council will consider legal arguments (for example arguments pointing out ways the Hearings Officer decision improperly interprets or applies relevant approval criteria). The City Council will also consider arguments that the Hearings Officer findings are not supported by the evidence submitted to the Hearings Officer. However, the City Council will not accept or consider new evidence that was not submitted to the Hearings Officer.

A fee is charged for appeals. Recognized neighborhood associations may qualify for an appeal fee waiver. City Council's decision may be appealed to the Oregon Land Use Board of Appeals (LUBA).

Failure to raise an issue (in writing or during the hearing) by the close of the record by the Hearings Officer in this case may preclude an appeal to LUBA on that issue. Also, if you do not provide enough detailed information to the Hearings Officer, they may not be able to respond to the issue you are trying to raise. In such a situation, an appeal to LUBA on that issue may not be allowed.

HEARING CANCELLATION

This public hearing will be cancelled due the inclement weather or other similar emergency.

- Hearings Officer: This public hearing will be cancelled if the City of Portland is closed. Check local television or the City of Portland website (www.portlandoregon.gov) for closures. Contact the Hearings Office at 503-823-7307, for immediate information regarding cancellations or rescheduling.
- Design Commission, Historic Landmarks Commission or Adjustment Committee: This public hearing will be cancelled if Portland Public Schools close due to inclement weather or other similar emergency. Check local television and radio reports for school closures. Please call the Bureau of Development Services at 503-823-7617, for information regarding cancellations or rescheduling.

The hearing will be rescheduled for the earliest possible date. A renotification notice will not be sent.

To attend the hearing, public transportation is available. Tri-Met buses stop near the BDS building at SW Fifth or Sixth Ave. at SW Hall St. and SW Harrison St. Call Tri-Met at 503-238-7433 (or www.trimet.org) for routes and times. Hourly rated public parking is available a half block south of the building on Fourth Ave.

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

Enclosure:

Zoning Map
Site Plans
Building Elevations
Land Use Hearing Participation Information

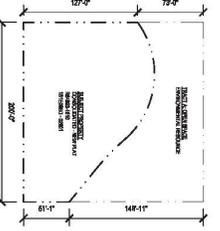
ZONING SUMMARY:

PROPOSED ZONING:	R7 / RM2	PROPOSED:	RESIDENTIAL
PROPOSED USE:	3 STORY 42 UNIT APARTMENTS OVER 4 ADDITIONAL AT-RISK UNITS & 4 ADDITIONAL AT-RISK TRACT A2	REQUIREMENTS:	SEE A1.11 AND A1.21 FOR LIFE SAFETY SUMMARY
LAND USE:	REFERS TO NARRATIVE		
LOT AREA (V) =	20,000 SF (14,000 SF W/ ENVIRONMENTAL RESOURCE (MGN OUT))		
LOT AREA (M) =	20,000 SF (8,000 SF W/ ENVIRONMENTAL RESOURCE (MGN OUT))		
FAR PROPOSED (MGN):	CONTRIBUTE	NON-CONTRIBUTE (PARKING)	4.883
FLOOR 1:	5,280		
FLOOR 2:	5,497		
FLOOR 3:	5,497		
FLOOR 4:	5,497		
TTL:	21,771	4.883	
TTL FAR PROPOSED IN RM2:	27,228 SF (1:1)		

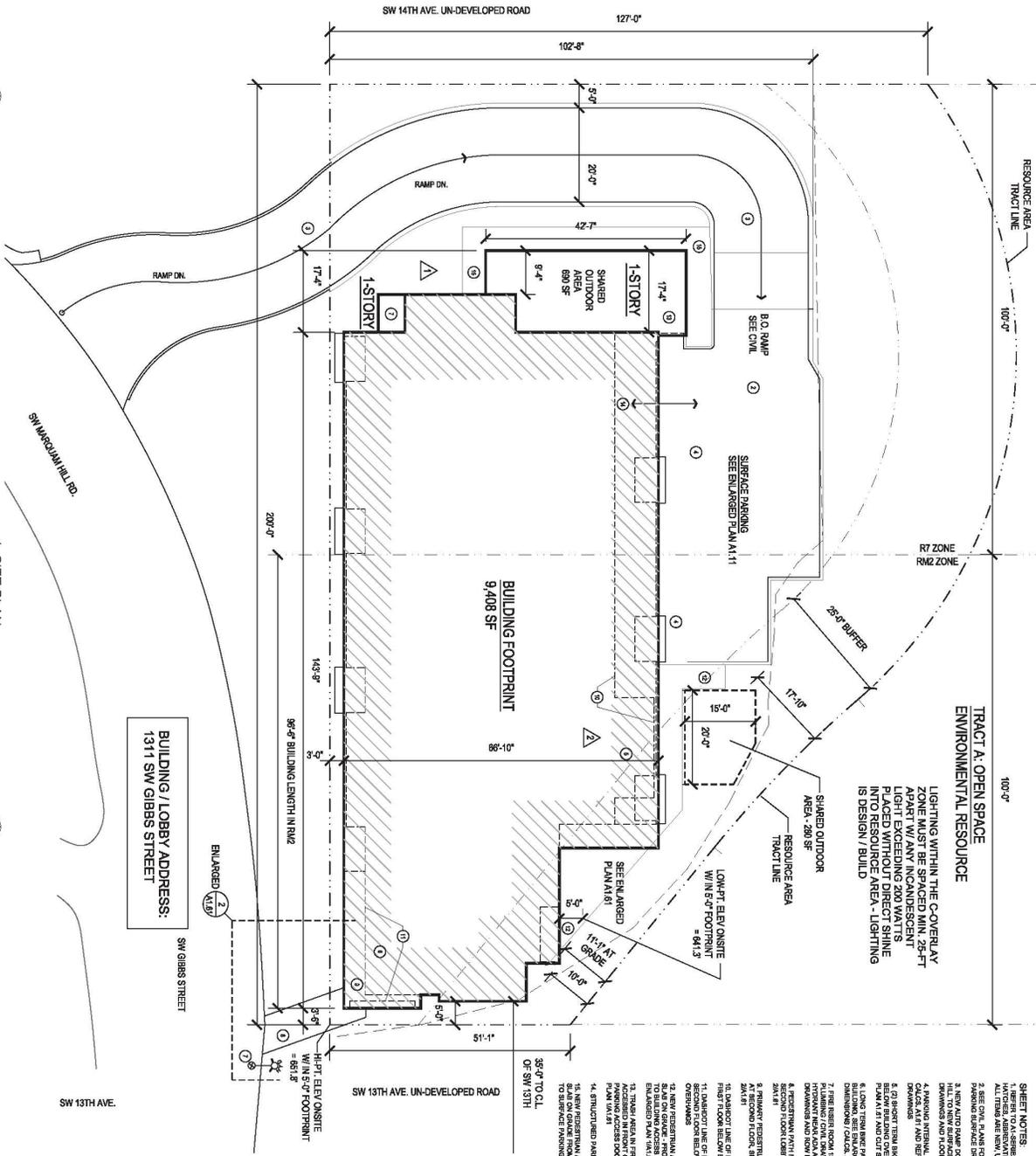
PLANNING AND ZONING DEVELOPMENT STANDARDS

REQUIRED / ALLOWABLE (PER D1)	PROPOSED
HEIGHT (FEET MAX)	48 FT
HEIGHT (FEET MIN)	48 FT
FRONT SETBACK	48 FT MAX WEST OF SITE
REAR SETBACK	0 FT (CONSERVATION ZONING)
SIDE SETBACK	1 FT (SIDE) / 2 FT (REAR)
PERCENT PARKING	4.1 (LONG TERM) / 5.0 (SHORT TERM)
PERCENT OPEN SPACE	8

SEE A1.01 FOR FIRE DEPT. RELATED ITEMS SEE A1.11 AND A1.21 FOR LIFE SAFETY SUMMARY



2. PROPERTY LINE PLAN
SCALE: 1/8" = 1'-0" ON 11x17'



1. SITE PLAN
SCALE: 3/32" = 1'-0" ON 22x34

LU 22-187796 APP, AP

A1.00

LAND USE SET
09/01/2023

SITE PLAN: COVERAGE + ZONING SUMMARY

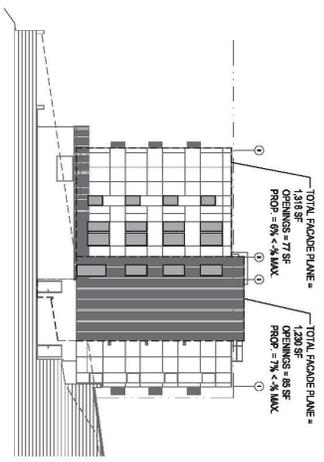
GIBBS DEVELOPMENT
1325 SW GIBBS STREET PORTLAND, OR 97239



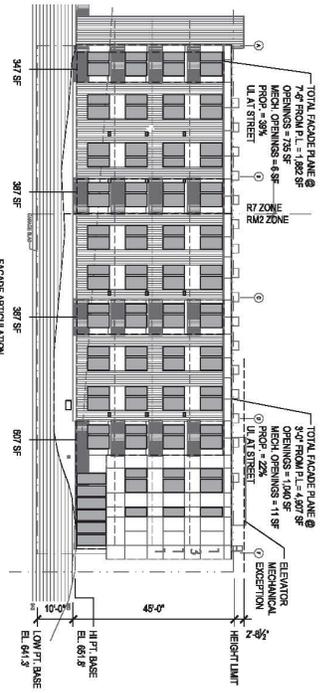
STEELHEAD ARCHITECTURE
5210 SE 26TH AVE. PORTLAND OREGON 97202

- SHEET NOTES:**
1. REFER TO ALL SHEETS FOR SYMBOLS, DIMENSIONS, AND DIMENSIONS. ALL DIMENSIONS ARE IN FEET UNLESS NOTED OTHERWISE.
 2. SEE CIVIL, PLUMBING, MECHANICAL AND ELECTRICAL DRAWINGS FOR DETAILS AND DIMENSIONS.
 3. NEW AUTO RAMP DOWN OFF SW MARQUAM RD. SEE CIVIL, PLUMBING, MECHANICAL AND ELECTRICAL DRAWINGS AND FLOOR PLANS.
 4. MARKED INTERNAL LANDSCAPE AREA SEE CIVIL, PLUMBING, MECHANICAL AND ELECTRICAL DRAWINGS AND FLOOR PLANS.
 5. TO SHORT TERM USE PARKING SPACES BELOW BUILDING OVERLAP. SEE ENLARGED PLAN A1.10 AND CIVIL SHEET A1.10.
 6. LONG TERM USE PARKING SPACES BELOW BUILDING OVERLAP. SEE ENLARGED PLAN A1.11 AND CIVIL SHEET A1.11.
 7. THE REAR ROOM 1ST FLOOR - SEE ENLARGED PLAN A1.11 FOR DIMENSIONS, COLORS AND CUT SHEETS FOR FINISHES AND FLOOR FINISHES. SEE CIVIL DRAWINGS AND NOW PREPARED.
 8. PROTECTIVE WALL FROM SW GIBBS TO SW 13TH STREET. SEE ENLARGED PLAN A1.11.
 9. PRIMARY RESIDENTIAL ENTRY TO BUILDING FROM SW GIBBS STREET. SEE ENLARGED PLAN A1.11.
 10. DASHDOT LINE OF BUILDING OUTLINE AT FIRST FLOOR BELOW BUILDING OVERLAP. SEE ENLARGED PLAN A1.11 AND CIVIL.
 11. DASHDOT LINE OF BUILDING OUTLINE AT SECOND FLOOR BELOW BUILDING OVERLAP. SEE ENLARGED PLAN A1.11 AND CIVIL.
 12. NEW RESIDENTIAL ACCESS FROM SW GIBBS STREET TO BUILDING OVERLAP. SEE ENLARGED PLAN A1.11 AND CIVIL.
 13. TRACT A OPEN SPACE WITHIN THE COVERED APART W/ ANY INCANDESCENT LIGHT EXCEEDING 200 WATTS PLACED WITHOUT DIRECT SHINE INTO RESOURCE AREA - LIGHTING IS DESIGN / BUILD.
 14. ENLARGED PARKING GARAGE ACCESS FROM SW GIBBS STREET TO BUILDING OVERLAP. SEE ENLARGED PLAN A1.11 AND CIVIL.
 15. NEW RESIDENTIAL ACCESS FROM SW GIBBS STREET TO BUILDING OVERLAP. SEE ENLARGED PLAN A1.11 AND CIVIL.
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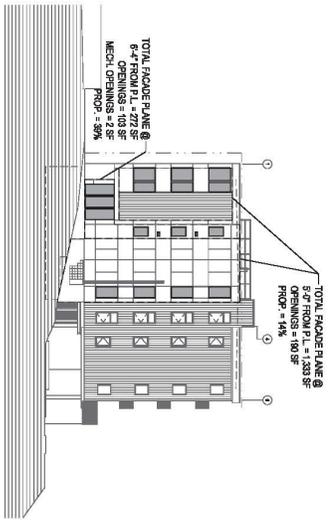
SHEET 1 (NO. 15)
 1. REFER TO AGENSIES FOR APPROVAL.
 ALL ITEMS ARE NEW UNLESS NOTED OTHERWISE.



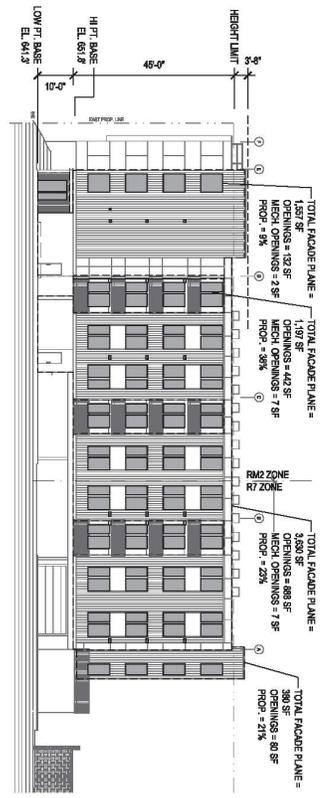
3. HEIGHT + EAST FACADE PLANE
 SCALE: 1/16" = 1'-0" ON Z2X34 1/32" = 1'-0" ON 11x17



1. HEIGHT + SOUTH FACADE PLANE
 SCALE: 1/16" = 1'-0" ON Z2X34 1/32" = 1'-0" ON 11x17



4. HEIGHT + WEST FACADE PLANE
 SCALE: 1/16" = 1'-0" ON Z2X34 1/32" = 1'-0" ON 11x17



2. HEIGHT + NORTH FACADE PLANE
 SCALE: 1/16" = 1'-0" ON Z2X34 1/32" = 1'-0" ON 11x17



STEELHEAD ARCHITECTURE

5210 SE 26TH AVE. PORTLAND OREGON 97202

BUILDING HEIGHT + FACADE PLANE /
 OPENING CALCULATIONS

GIBBS DEVELOPMENT
 1325 SW GIBBS STREET PORTLAND, OR 97239

LAND USE SET
 09/07/2022

A1.02

LU 22-187796 APD, AP



Hearings Office

City of Portland

1900 SW 4th Avenue, Room 3100, Portland, OR 97201 phone: 503.823.7307

www.portland.gov/omf/hearings

email: HearingsOfficeClerks@portlandoregon.gov

fax: 503.823.4347



Land Use Hearing Participation Information

Case Number: LU 22-187796 APD AP (Hearings Office Case Number 4220018)

Site Address: 1311 SW Gibbs Street

Hearing Date: December 28, 2022

Hearing Time: 2:00 PM

The above-referenced land use hearing will take place via the Zoom platform.

Zoom Participation Options and Instructions

Zoom Option

- You will need an internet connection and a personal computer or other internet capable device.
- You can listen, view the City Planner’s PowerPoint presentation, and you have the opportunity to provide oral testimony.
- Instructions:

Go to:

<https://us06web.zoom.us/j/88936098415>

Telephone Option

- You will need a telephone.
- You will be able to listen and testify but you will not be able to view documents and presentations shared on the Zoom platform (if any).
- Instructions:

Dial: +1 669 900 6833
Meeting ID: 889 3609 8415#

Written Testimony

- Prior to the Hearing and until the close of evidence, you may submit written testimony. In other words, written testimony can be provided up to the close of the record, which will be announced at the hearing.
- *Note: A document’s timeliness is determined by the date/time when it is received by the Hearings Office Clerks.*

- Please reference the case number in any documents you wish to submit.
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Portland, OR 97201

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Interpretare 번역 및 통역 | Письмовий або усний переклад | Turjumida ama Fasiraadda|

Questions or Concerns

You may contact the Hearings Clerks via email or by leaving a voicemail with the office: HearingsOfficeClerks@portlandoregon.gov; 503-823-7307.



CITY OF PORTLAND ENVIRONMENTAL SERVICES



1120 SW Fifth Avenue, Suite 613, Portland, Oregon 97204 ■ Mingus Mapps, Commissioner ■ Michael Jordan, Director

Land Use Response

Date: December 6, 2022

To: Leah Dawkins, BDS Land Use Services
503-865-6734, Leah.Dawkins@portlandoregon.gov

From: Ella Indarta, BES Systems Development
503-823-8068, Ella.Indarta@portlandoregon.gov

Case File: LU 22-187796

Location: 1311 SW GIBBS ST

R#: R712893

Proposal: HO HEARING: The applicant received Land Use approval for a 5-story 27-unit multi-dwelling structure with 28 vehicle parking spaces and 30 long-term bike parking spaces through a Planned Development Review, a Land Division Partition, and Environmental Modifications submitted in 2019 (LU 19-258445 PD LDP ENM). The applicant is now proposing an amendment to the original Planned Development and Partition approvals to allow for an increase in the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces. The building envelope, façade, site access, and circulation are not proposed to change. The environmental tract created in the 2019 land use review will not change. Previously approved utility access will not change. To better meet the outdoor area requirements and comply with new façade articulation requirements, the applicant has requested the following modifications through the Planned Development Amendment: Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2); Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a); Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a); Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a). This proposal is reviewed through a Type III procedure because an increase in density is considered a major change to an Approved Planned Development and the original Planned Development application was processed through a Type III procedure (33.854.500-.510). The Land Division Amendment is being processed concurrently through the Type III procedure.

The following comments are based on the land use review plans and documents provided to the Bureau of Environmental Services (BES). Some references to Portland City Code (PCC) are included below; the applicant may also refer to the Auditor's Office [Online Charter and Code page](#).

A. RESPONSE SUMMARY

BES does not object to approval of the Amendment to Planned Development and Amendment to Partition application.

B. SANITARY SERVICE

For the preliminary land division plan to be approved, the applicant must show that the proposal complies with approval criterion 33.652, Sanitary Sewer Disposal Service. The comments below are in response to this criterion.

1. **Existing Sanitary Infrastructure:** According to available GIS data, the following sewer infrastructure is located in the vicinity of the project site:

Ph: 503-823-7740 Fax: 503-823-6995 ■ www.portlandoregon.gov/bes ■ Using recycled paper ■ An Equal Opportunity Employer

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LU 22-187796 APD AP
Exhibit E.1

- a. Public 8-inch CSP sanitary sewer in SW 13th Ave (BES as-built # 3172). This main is within the conservation zone.
 - b. Public 12-inch CSP combined sewer in SW Gibbs St east of the property (BES as-built # 1182). This is a newly extended main built through EP555.
 - c. Public 8-inch CIP combined sewer in the right-of-way north of the property and within a public easement north of the property (BES as-built # 2234).
2. *Service Availability:* Sanitary connections from private property that are to be permitted according to PCC 17.32.090 must be separately conveyed to the property line and connected through individual laterals to a City sanitary or combined sewer. All discharge must be connected via a route of service approved by the BES Chief Engineer.
 - a. *Proposed Development:* The new development will be served by a new connection to the combined sewer in SW Gibbs within its frontage. The requested amendments do not appear to have an impact on the availability of sanitary service for this project.
 3. *Connection Requirements:* Connections to the City sewer system must meet the standards of the City of Portland's [Sewer and Drainage Facilities Design Manual](#), [PCC 17.32.090](#), administrative rules [ENB-4.07](#) and [ENB-4.17](#), and all other relevant City codes and rules. Sanitary sewage from private property must be separately conveyed to the property line and connected through individual laterals for discharge to the City separate sanitary or combined sewer. Per [ENB-4.07](#), sewer connection permits are required to make new connections to City mains and laterals, relocate or upsize existing laterals, and repair sewers in City right-of-way. The permittee is responsible for verifying the location, depth and size of an existing sewer lateral and for ensuring the lateral is clear of obstructions prior to connection.

Staff finds the applicant's proposed sanitary sewer service acceptable for the purpose of reviewing the Amendment to Planned Development and Amendment to Partition application against the sanitary sewer disposal approval criterion.

C. STORMWATER MANAGEMENT

For the preliminary land division plan to be approved, the applicant must show that the proposal complies with approval criterion 33.653, Stormwater Management. The comments below are in response to this criterion.

1. *Existing Stormwater Infrastructure:* According to available GIS data, the following stormwater infrastructure is located in the vicinity of the project site:
 - a. There are no public storm-only sewers available to this property.
2. *General Stormwater Management Requirements:* Development and redevelopment sites that include any of the triggers listed in PCC 17.38.040 are subject to the policies and standards of PCC 17.38.035, Portland's [Stormwater Management Manual](#) (SWMM) and [Source Control Manual](#) (SCM). Projects must comply with the current adopted version of the SWMM as of the permit application date. A fundamental evaluation factor in the SWMM is the Stormwater Infiltration and Discharge Hierarchy (Section 1.3.3), which sets the framework that will be used to determine when a project's stormwater runoff must be infiltrated onsite and when offsite discharge will be permitted, and the parameters that must be met for either scenario. If tested infiltration rates on a property are greater than or equal to 2 inches per hour, onsite infiltration will be required unless the site qualifies for the ecoroof exception per Section 3.2.1 or infiltration is determined infeasible based on site conditions described in Chapter 2 of the SWMM. Note that maximum building coverage allowed by the zoning code, including below grade development, does not exempt the applicant from stormwater requirements. Pollution reduction and flow control requirements must be met using vegetated facilities to the maximum extent feasible, though roof runoff and some paved impervious surfaces are exempt when discharging directly to a UIC (refer to Sections 1.3.2, 1.3.4, 3.2.4 and 4.2.2 of the SWMM).

3. *Private Property Stormwater Management:* Stormwater runoff from this project must comply with all applicable standards of the SWMM and be conveyed to a discharge point along a route of service approved by the BES Director or the Director's designee.
 - a. From the submitted site plans, it appears that the proposed Amendment to Planned Development and Amendment to Partition application does include any changes to the previously approved stormwater management plan. This proposal does not include an increase in impervious area beyond what has been approved under LU 19-258445 PD LDP and 20-194259 CO. The stormwater management system proposed is consistent with the plan approved under 20-194259 CO.
4. *Public Right-of-Way Stormwater Management:* Stormwater runoff from public right-of-way improvements as required by the City of Portland Bureau of Transportation (PBOT) must be managed according to the standards of the SWMM and the Sewer and Drainage Facilities Design Manual.
 - a. PBOT requires frontage improvements to include pavement widening along the frontage of the property. Public stormwater management improvements per the standards of the SWMM and the Sewer and Drainage Facilities Design Manual are therefore required. BES understands that given the elevation of the street, the lack of room in the ROW, and the landslide hazard area, stormwater management for the ROW improvements are challenging. BES has approved the Special Circumstance application allowing the applicant to pay an offsite management fee. BES Development Engineering approved the Concept Development plans (i.e. 30% design) for the right-of-way stormwater improvements on 4/23/20; therefore, BES finds that public stormwater infrastructure can be constructed as shown on the applicant's Preliminary Site Utility Plan.

Staff finds the applicant's proposed stormwater management plan acceptable for the purpose of reviewing the Amendment to Planned Development and Amendment to Partition application against the stormwater management approval criterion.

D. CONDITIONS OF APPROVAL

BES has no recommended conditions of approval.

E. PERMIT INFORMATION

At the time of permit review the applicant should be aware of the following:

1. *Connection Fees:* Sewer system development charges and connection fees are assessed at the time of building plan review and change every fiscal year on July 1st. For additional information on these fees use the [BDS Online Fee Estimator](#) or call the BES Development Review Team at 503-823-7761, option 2.
2. *Building Plans:* Building plans for this project must include a detailed site utility plan which shows proposed and existing sanitary connections, as well as stormwater management that meets the requirements of the version of the SWMM that is in effect at the time permit applications are submitted.
3. *Source Control Requirements:* Source control requirements from the [Source Control Manual](#) (SCM), [Portland City Code \(PCC\) Title 17](#), and [BES Administrative Rules](#) that may be applicable to this project are listed below with the corresponding chapter, section, code, and/or rule. For specific questions on the following, please contact BES Source Control at 503-823-7122.
 - a. *Site Use and Activity-Based Source Control Requirements (SCM Chapter 6):* BES recommends the applicant review the following SCM sections to understand the structural, treatment, and operational BMP requirements that may impact the project design.

- 1) *Waste and Recycling Storage (SCM Section 6.1)*
- b. *Grease Management Program (PCC 17.34, ENB 4.26)*: The City requires grease management (GM) devices in all food service establishments and for any business/industry that may introduce fats, oils, or grease (FOG) into the public sewer. A monitoring access structure (MAS) may also be required. Please refer to the MAS discussion below regarding MAS requirements. For additional information, go to the [Cut Through the FOG](#) webpage.
- c. *Extra Strength Sewer Charge Program (PCC 17.34, PCC 17.36, ENB 4.25)*: Food service establishments and some other industries incur a surcharge on their sewer bills. A monitoring access structure (MAS) may also be required. Please refer to the MAS discussion below regarding MAS requirements. Establishments that employ best management practices can apply for a rate reduction. Call 503-823-7093 for more information.

G. ADMINISTRATIVE REVIEW

The applicant may request a modification of a decision presented in this response, as applicable, via an administrative review as outlined in PCC sections 17.06.050, 17.32.150, 17.33.100, 17.34.115, 17.36.110, 17.38.060 and 17.39.120 and in those sections' associated administrative rules. Some portions of this response are not decisions, but guidance related to requirements that this proposal may be subject to during City review of other processes, such as a building permit or public works permit review. While these are not decisions that are ripe to be considered through an administrative review, if the outcome of a future administrative review needs to be anticipated at this time in order to inform the land use action, the administrative review process may be utilized. Some items, such as technical standards, are not reviewable. For guidance on whether a modification can be requested and whether the land use process is the proper time to request it, consult with the BES staff identified above prior to submitting a request.

There is no fee charged for an administrative review, and all BES penalties and late fees will be stayed pending the outcome of the review process, as applicable. To request an administrative review, the applicant must complete the Administrative Review Request Form (located here: www.portlandoregon.gov/bes/68285) and submit it to the Systems Development staff listed above within 20 business days of the mailing date of this response. The applicant should coordinate with the BDS planner to determine whether applying for an administrative review would have an impact on state-mandated land use timelines.



PORTLAND BUREAU OF TRANSPORTATION

1900 SW Fourth Ave., Suite 5000 Portland, OR 97201 503-823-5185
Fax 503-823-7576 TTY 503-823-6868 www.portlandoregon.gov/transportation

Jo Ann Hardesty Commissioner Chris Warner Director

**RESPONSE TO THE BUREAU OF DEVELOPMENT SERVICES
LAND USE REVIEW REQUEST**

**Portland Transportation
Development Review
Bureau of Transportation Engineering & Development**

LU: 22-187796-000-00-LU Date: December 6, 2022
To: Leah Dawkins, Bureau of Development Services, B299/R5000
From: Michael Pina, B106/800, 503-823-4249
Applicant: Jesse Winterowd*
WINTERBROOK PLANNING
610 SW ALDER ST #810
PORTLAND OR 97205
Location: 1311 SW GIBBS ST
TYPE OF REQUEST: Type 3 procedure APD - Planned Development Amendment

DESCRIPTION OF PROJECT

HO HEARING: The applicant received Land Use approval for a 5-story 27-unit multi-dwelling structure with 28 vehicle parking spaces and 30 long-term bike parking spaces through a Planned Development Review, a Land Division Partition, and Environmental Modifications submitted in 2019 (LU 19-258445 PD LDP ENM). The applicant is now proposing an amendment to the original Planned Development and Partition approvals to allow for an increase in the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces. The building envelope, façade, site access, and circulation are not proposed to change. The environmental tract created in the 2019 land use review will not change. Previously approved utility access will not change. To better meet the outdoor area requirements and comply with new façade articulation requirements, the applicant has requested the following modifications through the Planned Development Amendment: 1) Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2); 2) Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a); 3) Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a); 4) Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a). This proposal is reviewed through a Type III procedure because an increase in density is considered a major change to an Approved Planned Development and the original Planned Development application was processed through a Type III procedure (33.854.500-.510). The Land Division Amendment is being processed concurrently through the Type III procedure.

RESPONSE

Transportation Impacts approval Criterion (33.641.020)

For the previous Planned Development land-use review (19-258445 PD), the applicant submitted a Transportation Impact Study (TIS), professionally prepared by Lancaster Mobley, to support the transportation-related approval criteria, in which PBOT reviewed and agreed with the conclusions that the transportation-related approval criteria are satisfied.

With the proposed amendment, the applicant then submitted an addendum to the previous TIS, accounting for increase in trip generation to then site based on the addition of units from 27 to 43, which changes the trip generation increase from 7 PM Peak Hour trips to 12 PM Peak Hour trips. This is still a minimal increase in trips and will not degrade the Level of Service (LOS) below the acceptable LOS threshold of 'E' at all nearby stop-controlled intersections. Therefore, PBOT approves the updated TIS and the same comments as previously identified still apply. Therefore, based on the evidence included in the record, the applicant has demonstrated to PBOT's satisfaction that the transportation system is capable of supporting the proposed use in addition to the existing uses in the area

Design of Rights-of-Way Approval Criterion (33.654.120)

For the required frontage improvements, the applicant submitted Public Works permit 19-193047 WT | TH0903 which has received 30% concept approval and bond payment, ensuring that vehicle, bicycle, and pedestrian circulation system is designed to be safe, efficient, and to city required standards.

RECOMMENDATION

PBOT has no objections to the Planned Development Amendment and associated Modifications.



Mingus Mapps, Commissioner
Gabriel Solmer, Administrator

1120 SW Fifth Avenue, Suite 405
Portland, Oregon 97204-1926
Information: 503-823-7404
portlandoregon.gov/water



Water Bureau

DATE: December 6, 2022

TO: Leah Dawkins
Bureau of Development Services

FROM: Michael Puckett
Development Review and Services

SUBJECT: Review of **22-187796 APD AP**

The Water Bureau has reviewed the proposed action and has the following comments:

No issue with the Planned Development review.

The 3" meter approved under 20-194259-CO will have substantial unused capacity. At the time of permit submission and review, a new W-4 form will be required for review.

Conditions of Approval: None

Please call me if you have any questions or comments. My phone number is 503-865-6374.



PORTLAND FIRE & RESCUE



Jo Ann Hardesty, Commissioner
Sara Boone, Fire Chief
AJ Jackson, Fire Marshal
Prevention Division
1300 SE Gideon Street
Portland, OR, 97202
Phone: (503) 823-3770

LAND USE REVIEW RESPONSE

TO: Leah Dawkins, City of Portland, Land Use Review
FROM: Dawn Krantz, Portland Fire Bureau 503-823-3718
DATE: December 8, 2022
SUBJECT: LU 22-187796 APD
SITE LOCATION : 1311 SW GIBBS ST

The following conditions of approval and informational comments are based on the land use review information provided to the Fire Bureau. Fire Bureau requirements are generated from the 2021 Portland Fire Code. All current Fire Code requirements apply and are required to be met. If these conditions cannot be met, an appeal providing an alternative method is an option for the applicant. If the applicant chooses to appeal a requirement, the appeal must be listed as a condition in the decision. Fire Code Appeals can be obtained at the Fire Bureau web page, www.portlandonline.com.

The Fire Bureau has no concerns with this proposal based on the provided information that no changes to the building envelope or access to the site is being proposed. The applicant will be required to meet all applicable Fire Code requirements at the time of permit review and development.



City of Portland, Oregon
Bureau of Development Services
Site Development
FROM CONCEPT TO CONSTRUCTION

Dan Ryan, Commissioner
Rebecca Esau, Director
Phone: (503) 823-6892
Fax: (503) 823-5433
TTY: (503) 823-6868
www.portland.gov/bds

Land Use Review Response
Site Development Section, BDS

To: Leah Dawkins, LUR Division
From: Jed Stoken, Site Development (503-823-7579)

Location/Legal:

Land Use Review: LU 22-187796

Proposal: Revisions to previously-approved land use decision (LU #19-258445 PD, ENM, LDP) to increase the dwelling unit count from 27 to 43 units (convert some of the previously proposed two-bedroom apartments into one-bedroom and studio apartments). The proposal will increase both bicycle parking (from 32 to 51 spaces) and required outdoor areas (from 1,296 sq. ft. to 2,266 sq. ft). There are no proposed changes to the building envelope, façade, or site access and circulation. And the protected environmental tract, which was created in a 2019 land division, will remain unchanged. Four modifications are requested:- Façade Articulation (33.120.230.C.2)- Upper Common Deck Dimensions (33.120.240.C.2.a)- Lower Common Lawn Dimensions (33.120.240.C.2.a)- Lower Common Lawn Location (33.120.240.C.2.a)

Quarter Sec. Map: 3327

Date: December 6, 2022

Site Development has reviewed the plans and documents provided to the Bureau of Development Services (BDS) for the revision to the previously-approved land use decision (LU #19-258445 PD, ENM, LDP) and takes no exceptions to the approval of this planned development amendment and does not request any conditions of approval.



Urban Forestry

Land Use Review Response

Date: December 07, 2022
From: Mariano Masolo
503-823-4560, Mariano.Masolo@portlandoregon.gov

Case File: 22-187796-000-00-LU
Location: 1311 SW GIBBS ST

Proposal: HO HEARING: The applicant received Land Use approval for a 5-story 27-unit multi-dwelling structure with 28 vehicle parking spaces and 30 long-term bike parking spaces through a Planned Development Review, a Land Division Partition, and Environmental Modifications submitted in 2019 (LU 19-258445 PD LDP ENM). The applicant is now proposing an amendment to the original Planned Development and Partition approvals to allow for an increase in the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces. The building envelope, façade, site access, and circulation are not proposed to change. The environmental tract created in the 2019 land use review will not change. Previously approved utility access will not change.

To better meet the outdoor area requirements and comply with new façade articulation requirements, the applicant has requested the following modifications through the Planned Development Amendment:

- Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2);
- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

This proposal is reviewed through a Type III procedure because an increase in density is considered a major change to an Approved Planned Development and the original Planned Development application was processed through a Type III procedure (33.854.500-.510). The Land Division Amendment is being processed concurrently through the Type III procedure.

Urban Forestry has reviewed the proposal for its impact on existing city trees, street trees and heritage trees, street tree planting requirements and related mitigation in accordance with Title 11, Trees and for potential impacts upon urban tree canopy. It is the applicant's responsibility to disclose all aspects of their land use proposal that may impact required street tree plantings and existing street trees during the land use review process.

UNLESS EXPLICITLY STATED HEREIN, THIS REVIEW DOES NOT APPROVE STREET TREE REMOVALS AND DOES NOT PROVIDE ANY EXEMPTIONS TO TITLE 11 REQUIRMENTS.

Permits required after land use approval are subject to all applicable development standards and all provisions of the City Code, including Title 11. Title 11 regulations will be applied during the permit review process.



PLEASE NOTE THERE MAY BE OTHER APPLICABLE TREE REQUIREMENTS AS PER TITLE 33 PLANNING & ZONING.

A. Response

Urban Forestry does not object to approval of the land use proposal. The proposed development will be subject to Title 11 regulations during the permit review process.

All street tree permitting is deferred to Public Works Permit TH0903





City of Portland, Oregon - Bureau of Development Services

1900 SW Fourth Avenue • Portland, Oregon 97201 | 503-823-7300 | www.portland.gov/bds



To: Leah Dawkins
From: Chanel Horn, Life Safety Plans Examiner
Date: December 6, 2022
RE: 1311 SW GIBBS ST, 22-187796-LU

LIFE SAFETY PLAN REVIEW RESPONSE

The following comments are based on the plans and documents provided to the Life Safety Plan reviewer. They are intended to provide the applicant with preliminary Building Code information that could affect the Land Use Review, Public Records request and/or future Building Permit reviews. The comments may not identify all conflicts between the Land Use proposal and the Building Codes. A complete Life Safety plan review will be provided at the time of Building Permit submittal at which time any additional Building Code issues will be noted. The comments are based on the Oregon Structural Specialty Code (OSSC), the International Existing Building Code (IEBC), the Oregon Mechanical Specialty Code (OMSC), or the Oregon Residential Specialty Code (ORSC).

RESPONSE SUMMARY

- Life Safety Plan Review does not object to the approval of this proposal. The applicant should be aware that several building code requirements may impact the final design of this building. For information regarding future compliance, see the **GENERAL LIFE SAFETY COMMENTS** below.
- Life Safety Plan Review does not object to the approval of this proposal. This approval is conditional on the finalization of the property line adjustment approved through this LUR/PR. If this public record is not finalized, a Covenant Not to Sell the Properties Separately must be established for this project. For information regarding future compliance, see the **GENERAL LIFE SAFETY COMMENTS** below.
- Life Safety Plan Review does not object to the approval of this proposal. Prior to Life Safety approval of the final plat or Land Use proposal, the applicant must address the Building Code issues listed as part of the **GENERAL LIFE SAFETY COMMENTS** below.
- Life Safety Plan Review cannot support approval of the current Land Use proposal. Prior to Life Safety approval of the final plat, the applicant must address the Building Code issues listed as part of the **GENERAL LIFE SAFETY COMMENTS** below.

Item #	GENERAL LIFE SAFETY COMMENTS
1	Building Permit Issued - A Building Permit has been applied for and has been issued. A Life Safety reviewer has been in contact with the applicant. Please refer to correspondence from the assigned Life Safety reviewer for building code-related comments. All questions regarding Building Code issues should be directed to the assigned Life Safety reviewer.
2	Building Permit Revision Required - A separate Building Permit Revision is required for the work proposed and the proposal must be designed to meet all applicable building codes and ordinances. Information about submitting a permit revision request is available online at https://www.portland.gov/bds/permit-review-process/permit-revision-request .

From: mjones.miltjones.com
To: [Dawkins, Leah](#)
Subject: 1325 SW Gibbs Project Proposal Review (Case File LU 22-187796 APD AP)
Date: Tuesday, December 6, 2022 12:15:35 PM

The adequacy of pedestrian transportation should be part of this proposal review.

Pedestrian transportation infrastructure on this part of SW Gibbs, especially in the vicinity of the SW 13th right-of-way, is inadequate. In fact, it is not safe. The City needs to provide sidewalks on SW Gibbs, or at least an improved and safer pedestrian corridor.

Thank you,

Milt Jones
425 SW Bancroft
Portland, Oregon 97239



Homestead Neighborhood Association

P.O. Box 80191
Portland, OR 97280
December 8, 2022

Leah Dawkins, Land Use Services
1900 SW Fourth Avenue
Suite 4500
Portland, Oregon 97201

Re: Proposed Amendments for Development at 1311 (a.k.a. 1325) SW Gibbs
Case File: LU22-187796 APD AP, PC#22-144899

Please accept these comments from the Homestead Neighborhood Association concerning proposed changes and authorizations for development at 1311 SW Gibbs.

The applicant proposes amendments to the original approvals to allow for an increase in the number of units from 27 to 43 units through alteration of interior floor plans. The proposal would also increase the number of bicycle parking spaces from 32 to 51.

In the main, we consider the proposed changes to be acceptable. The developer has worked cooperatively with the Neighborhood Association throughout this project and we share a common goal of providing "walk to work" housing for our residents. We have met with the developer several times regarding improvements of pedestrian safety at this location. The developer is willing to improve pedestrian access and safety but at this time the City has been uncooperative in this endeavor. Specifically, the City has called for improvements of the Gibbs St. shoulder immediately in front (south) of the development (which will not be regularly used by pedestrian residents of the development), but has not addressed the main and significant safety bottleneck along Gibbs St. across the 13th Ave. right-of-way immediately east of the development. At that location, which must be traversed by all pedestrians and bicyclists to and from the development, there is inadequate width for pedestrians to walk without straying onto the travelled way of Gibbs St. which has high motorized traffic volumes during commute times.

We believe that the proposed changes result in a development that is a good match with the employee demographic of our large Marquam Hill hospital employers and that the changes will further the City's own stated goal of creating "climate friendly walkable neighborhoods" so long as safe pedestrian accommodation along Gibbs across the 13th Ave. right-of-way are included.

Unfortunately, the City thus far has neglected the "walkable" part of the equation. Between the project at 1311 SW Gibbs and the employment centers of the university, hospitals, medical offices and even the only bus stop in walking distance there is no good pedestrian (or bicycle) connection. Briefly stated, the current pedestrian and bicycle transportation system is not

capable of safely supporting the increased number of pedestrians and bicycles clearly contemplated by the changes proposed for this development.

We do not believe that this proposal should be approved without addressing the currently inadequate and dangerous pedestrian transportation conditions on SW Gibbs.

The City has long been aware of the inadequate and unsafe conditions on SW Gibbs between this project and the Marquam Hill medical facilities; particularly where SW Gibbs crosses the SW 13th Ave right-of-way. At this point there is not only no sidewalk, but the roadside shoulder is crumbled, narrow and overgrown (this is on City property) with invasive Himalayan blackberries. The City and the community have consistently identified SW Gibbs as a priority for receiving pedestrian improvements. As a part of the 4T regional trail system and a residential and commuter walkway, SW Gibbs from SW 11th to Marquam Hill Road is listed on both the City of Portland's System Development Charge list (project 90049.2) and the SW in Motion (SWIM) Tier 1 project list. Earlier this year an article appeared in the "BikePortland" newsletter describing the need for improvements on Gibbs Street. See the following link:

<https://bikeportland.org/2022/09/09/a-dispatch-from-sw-gibb-where-development-doesnt-come-with-sidewalks-363145>

Also, below this letter are pictures of the unsafe pedestrian conditions on Gibbs at 13th Avenue.

The developer of this project will be making improvements on SW Gibbs immediately uphill from SW 13th Ave. The City should make or require improvements for pedestrian travel across the unused 13th Ave right of way and further downhill as far as necessary for pedestrian walk-to-work safety.

Neglect of pedestrian infrastructure in this neighborhood as it is infilled with walk-to-work residential projects is inexcusable. OHSU, a major pedestrian destination employer, and neighborhood multi-unit residential and mixed-use commercial projects are contributing very significant sums to the City in the form of development charges for transportation and transportation safety improvement, among other things. We believe those funds should be used in the first instance to address the local public infrastructure and safety needs associated with the projects paying those fees.

In the absence of pedestrian safety improvements on SW Gibbs, we find ourselves having to oppose any decision allowing the proposed changes to this project. In the event that the Bureau of Developmental Services (BDS) approves the changes without the necessary improvements to pedestrian safety, the Homestead Neighborhood Association intends to appeal that decision. We hope that BDS does not sidestep this issue and point to another Bureau (Transportation) as the responsible office for a solution (a symptom of the disfunction of Portland city government that caused Portland citizens to this year elect a change in government structure). Instead, we hope that BDS can work with PBOT to solve this issue of safe walkability when approving needed developments in our neighborhoods.

Thank you for considering these comments.

Sincerely,

Ed Fischer, Chairperson
Homestead Neighborhood Association



SW Gibbs St, looking west



SW Gibbs St. looking east



City of Portland, Oregon - Bureau of Development Services

1900 SW Fourth Avenue • Portland, Oregon 97201 | 503-823-7300 | www.portland.gov/bds



Land Use Review Application

File Number: _____

FOR INTAKE, STAFF USE ONLY

Date Rec _____ by _____

Type I Type Ix Type II Type Iix Type III Type IV ELD

LU Reviews _____

[Y] [N] Unincorporated MC

[Y] [N] Flood Hazard Area (LD & PD only)

[Y] [N] Potential Landslide Hazard Area (LD & PD only)

[Y] [N] 100-year Flood Plain [Y] [N] DOGAMI

Qtr Sec Map(s) _____ Zoning _____

Plan District _____

Historic and/or Design District _____

Neighborhood _____

District Coalition _____

Business Assoc _____

Related File # _____

**APPLICANT: Complete all sections below that apply to the proposal. Please print legibly.
Email this application and supporting documents to: LandUseIntake@portlandoregon.gov**

Development Site

Address or Location 1311 SW Gibbs Street

Cross Street SW 13th Avenue Sq. ft./Acreage 40,000 sq. ft.

Site tax account number(s)

R 712893 R _____ R _____

R _____ R _____ R _____

Describe project (attach additional page if necessary)

Describe proposed stormwater disposal methods

All stormwater will be conveyed to a 12" outfall into an existing drainage way located in the SW 13th Ave. right-of-way.

Identify requested land use reviews

Type III Planned Development Review, & Type Ix Partition Amendment

• **Design & Historic Reviews** - For **new development**, provide project valuation.

For **renovation**, provide exterior alteration value.

AND provide total project valuation.

\$ N/A

\$ N/A

\$ N/A

N/A

• **Land Divisions** - Identify number of lots (include lots for existing development).

New street (public or private)?

yes no

yes no N/A

• **Affordable Housing** - For buildings containing five or more dwelling units, will 50% or more of the units be affordable to households with incomes equal to or less than 60% of the median family income for the county or state, whichever is greater?

continued / over
LU 22-187796 APD AP
Exhibit G.1

Applicant Information

- Identify the primary contact person, applicant, property owner and contract purchaser. Include any person that has an interest in your property or anyone you want to be notified. Information provided, including telephone numbers and e-mail addresses, will be included in public notices.
- For all reviews, the applicant must sign the Responsibility Statement.
- For land divisions, all property owners must sign the application.

PRIMARY CONTACT:

Typed Full Name Jesse Winterowd I acknowledge this typed name as my signature

Company/Organization Winterbrook Planning

Mailing Address 610 SW Alder Street, Suite 810

City Portland State Oregon Zip Code 97205

Day Phone 503-827-4422 FAX _____ email jesse@winterbrookplanning.com

Check all that apply Applicant Owner Other

Typed Full Name Sean O'Neill I acknowledge this typed name as my signature

Company/Organization Scotia Western States Housing, LLC.

Mailing Address 15962 Boones Ferry Road, Suite 202

City Lake Oswego State Oregon Zip Code 97035

Day Phone 503-956-9307 FAX _____ email sean@scotiawsh.com

Check all that apply Applicant Owner Other

Typed Full Name _____ I acknowledge this typed name as my signature

Company/Organization _____

Mailing Address _____

City _____ State _____ Zip Code _____

Day Phone _____ FAX _____ email _____

Check all that apply Applicant Owner Other

Typed Full Name _____ I acknowledge this typed name as my signature

Company/Organization _____

Mailing Address _____

City _____ State _____ Zip Code _____

Day Phone _____ FAX _____ email _____

Check all that apply Applicant Owner Other

Responsibility Statement As the applicant submitting this application for a land use review, I am responsible for the accuracy of the information submitted. The information being submitted includes a description of the site conditions. I am also responsible for gaining the permission of the owner(s) of the property listed above in order to apply for this review and for reviewing the responsibility statement with them. If the proposal is approved, the decision and any conditions of the approval must be recorded in the County Deed Records for the property. The City of Portland is not liable if any of these actions are taken without the consent of the owner(s) of the property. In order to process this review, City staff may visit the site, photograph the property, or otherwise document the site as part of the review. I understand that the completeness of this application is determined by the Director. By my signature, I indicate my understanding and agreement to the Responsibility Statement.

Name of person submitting this application agrees to the above Responsibility Statement and acknowledges typed name as signature:

Jesse Winterowd Date: September 19, 2022

Phone number: 503-827-4422

Email this application and supporting documents to LandUseIntake@portlandoregon.gov | Submittal of locked or password protected documents will delay intake of your application. **2**

LU 22-18798-APD-AP EXHIBIT G.1

October 19, 2022

Jesse Winterowd, Winterbrook Planning
610 SW Alder St #810
Portland OR 97205

Re: Land Use Review LU 22-187796 APD AP

Dear Jesse Winterowd:

The Bureau of Development Services received your application for a Planned Development Amendment located at 1311 SW GIBBS ST on September 28, 2022. Your case has been assigned to me, Leah Dawkins. In order to continue to review your application, additional information is needed. Once you submit this information, your application will be considered complete, and I will proceed with a full review of your proposal. Up to this point, your application has been reviewed only to determine if all required information has been submitted. The application has not been fully reviewed to determine if it meets the relevant approval criteria, however some issues you may want to consider are identified in Section II below.

I. Information Necessary to Complete Application

The following information must be submitted before your proposal can be evaluated:

1. **Planned Development Review Modification:**

- **Common Areas:** The requested modification for the Lower Common Lawn requires additional information. Please provide details and an elevation drawing of the Lower Common Lawn, including any amenities provided and the landscape wall provided to separate the structured outdoor space from the slope to the north. An elevation drawing of this portion of the site will provide more detail showing the accessibility of the space from the structure and the usability of the space for building tenants. Provide detail on any outdoor furnishings, amenities, or way-finding signage proposed.
- Provide photos of the site in its current state, including the area proposed for the new Lower Common Lawn in order to contextualize the location of the common area within the environmental zone boundary as well as against the structure.

2. **Bureau of Environmental Services** has requested the following plan revisions:

- Revised plans must include the location of existing and proposed connections to the sewer for all lots.
- Revised plans and narrative must confirm that the proposed stormwater system is consistent with the approved stormwater management plan in CO 20-194259 or submit an updated stormwater management plan if necessary.

3. **Site Development** has requested the following information:

- The revision to the previously approved land use decision (LU #19-258445 PD, ENM, LDP) includes changes to the disturbed area of the site by increasing the Lower Common Lawn Dimensions and Location. Because the site is within the

City's Regulatory Landslide Hazard Area, these changes must be assessed at it relates to the Landslide Hazard Area Approval Criterion (PCC 33.632.100).

An addendum to the Landslide Hazard Study performed under LU #19-258445 must be submitted that evaluates whether the proposed revisions to the previous approved land use decision meets the Landslide Hazard Approval Criterion.

All Service Bureau completeness review responses are included with this letter.

II. Time to Complete Application

The Portland Zoning Code allows you up to 180 days to complete your application. Since the 180-day period began on the day we received the application, the deadline to make your application complete is **Monday March 27, 2023**.

III. Determination of a Complete Application

The application will be determined complete when you have submitted:

1. All of the requested information included in Section I, above. If you cannot provide all of the requested information at one time and intend to submit additional information, please include a written statement with each separate submittal indicating that you still intend to provide the additional missing information by the **Monday March 27, 2023** deadline, **or**
2. Some of the requested information included in Section I, above, and a written statement that no additional information will be provided; **or**
3. A written statement that none of the requested information included in Section I, above, will be provided.

Please be aware that not submitting the requested information may result in your application being denied. The information is needed to demonstrate the approval criteria are met. Once the application is deemed complete, review of your application can proceed using the information you have provided.

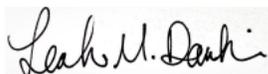
Your application will be approved if it meets the relevant land use review approval criteria. It is your responsibility to document how the approval criteria are met. The items listed above will help provide that documentation.

Voiding of Application

If your application is not complete by **Monday March 27, 2023**, it will be voided, and the application fee will not be refunded. The City's land use review procedures are outlined in Chapter 33.730 of the Portland Zoning Code.

Please contact me if you have any questions about this letter. My telephone number is **503-865-6734**, and my e-mail address is Leah.Dawkins@portlandoregon.gov. Requested information noted above should be emailed to me. Please e-mail me for file dropbox instructions if document or drawing file sizes are greater than 5MB. Please label all correspondence and materials you submit with the case number LU 22-187796.

Sincerely,



Leah Dawkins, Planner
Land Use Services Division

cc: Owner, Scotia Western States Housing
Application Case File



CITY OF PORTLAND ENVIRONMENTAL SERVICES



1120 SW Fifth Avenue, Suite 613, Portland, Oregon 97204 ■ Mingus Mapps, Commissioner ■ Michael Jordan, Director

Completeness Response

Date: October 10, 2022

To: Leah Dawkins, BDS Land Use Services
503-865-6734, Leah.Dawkins@portlandoregon.gov

From: Ella Indarta, BES Systems Development
503-823-8068, Ella.Indarta@portlandoregon.gov

Case File: LU 22-187796

Location: 1311 SW GIBBS ST

Proposal: Revisions to previously-approved land use decision (LU #19-258445 PD, ENM, LDP) to increase the dwelling unit count from 27 to 43 units (convert some of the previously proposed two-bedroom apartments into one-bedroom and studio apartments). The proposal will increase both bicycle parking (from 32 to 51 spaces) and required outdoor areas (from 1,296 sq. ft. to 2,266 sq. ft.). There are no proposed changes to the building envelope, façade, or site access and circulation. And the protected environmental tract, which was created in a 2019 land division, will remain unchanged. Four modifications are requested:- Façade Articulation (33.120.230.C.2)- Upper Common Deck Dimensions (33.120.240.C.2.a)- Lower Common Lawn Dimensions (33.120.240.C.2.a)- Lower Common Lawn Location (33.120.240.C.2.a)

BES provides the following comments in response to materials received for the purpose of determining completeness of the above-referenced Land Use application. Items requested in this memo should not be considered final, as staff reserves the right to request additional materials during the formal review period.

1. **Plans:** Submit a revised set of land use plans to reflect all changes requested by BES staff below, as well as changes requested by other agency reviewers.
 - a. Revised plans must include the location of existing and proposed connections to the sewer for all lots.
 - b. Revised plans and narrative must confirm that the proposed stormwater system is consistent with the approved stormwater management plan in CO 20-194259 or submit an updated stormwater management plan if necessary.

Review For Completeness Response

To: Leah Dawkins, 503-865-6734, LUR Division
From: Jed Stoken, Site Development 503-823-7579

Location/Legal: 1311 SW GIBBS ST
Land Use Review: LU 22-187796
Proposal: Revisions to previously-approved land use decision (LU #19-258445 PD, ENM, LDP) to increase the dwelling unit count from 27 to 43 units (convert some of the previously proposed two-bedroom apartments into one-bedroom and studio apartments). The proposal will increase both bicycle parking (from 32 to 51 spaces) and required outdoor areas (from 1,296 sq. ft. to 2,266 sq. ft). There are no proposed changes to the building envelope, façade, or site access and circulation. And the protected environmental tract, which was created in a 2019 land division, will remain unchanged. Four modifications are requested:- Façade Articulation (33.120.230.C.2)- Upper Common Deck Dimensions (33.120.240.C.2.a)- Lower Common Lawn Dimensions (33.120.240.C.2.a)- Lower Common Lawn Location (33.120.240.C.2.a)

Quarter Sec. Map: 3327
Date: October 11, 2022

Land Use Review Submittal Requirements

The revision to the previously approved land use decision (LU #19-258445 PD, ENM, LDP) includes changes to the disturbed area of the site by increasing the Lower Common Lawn Dimensions and Location. Because the site is within the City's Regulatory Landslide Hazard Area, these changes must be assessed at it relates to the Landslide Hazard Area Approval Criterion (PCC 33.632.100).

An addendum to the Landslide Hazard Study performed under LU #19-258445 must be submitted that evaluates whether the proposed revisions to the previous approved land use decision meets the Landslide Hazard Approval Criterion.



PORTLAND BUREAU OF TRANSPORTATION

1900 SW Fourth Ave., Suite 5000 Portland, OR 97201 503-823-5185
Fax 503-823-7576 TTY 503-823-6868 www.portlandoregon.gov/transportation

Jo Ann Hardesty Commissioner Chris Warner Director

**RESPONSE TO THE BUREAU OF DEVELOPMENT SERVICES
LAND USE REVIEW REQUEST**

**Portland Transportation
Development Review
Bureau of Transportation Engineering & Development**

LU: 22-187796--000-00 LU Date: October 112022
To: Leah Dawkins, Bureau of Development Services, B299/R5000
From: Michael Pina, B106/800, 503-823-4249
Applicant: Jesse Winterowd, WINTERBROOK PLANNING
610 SW ALDER ST #810
PORTLAND OR 97205
Location: 1311 SW GIBBS ST
TYPE OF REQUEST: Type 3 procedure APD - Planned Development Amendment

DESCRIPTION OF PROJECT

Revisions to previously-approved land use decision (LU #19-258445 PD, ENM, LDP) to increase the dwelling unit count from 27 to 43 units (convert some of the previously proposed two-bedroom apartments into one-bedroom and studio apartments). The proposal will increase both bicycle parking (from 32 to 51 spaces) and required outdoor areas (from 1,296 sq. ft. to 2,266 sq. ft). There are no proposed changes to the building envelope, façade, or site access and circulation. And the protected environmental tract, which was created in a 2019 land division, will remain unchanged. Four modifications are requested:- Façade Articulation (33.120.230.C.2)- Upper Common Deck Dimensions (33.120.240.C.2.a)- Lower Common Lawn Dimensions (33.120.240.C.2.a)- Lower Common Lawn Location (33.120.240.C.2.a)

RESPONSE

Portland Transportation/Development Review (PBOT) has reviewed the application for completeness and offers the following comments.

Approval Criteria for Planned Developments in All Zones (33.854.310.B)

- The applicant has provided an addendum to the project’s previous Transportation Impact Study (TIS) 19-267999 TR, prepared by Lancaster Engineering, addressing above referenced approval criteria and the increase in trip generation to the site. The addendum has been received by PBOT but has not been reviewed at time of this response.

For the required frontage improvements, the applicant has initiated Public Works permit 19-193047 WT | TH0903 and has received 30% concept approval. Performance Guarantee (bond) must be paid prior to issuance of the building permit.

Therefore, PBOT recommends the application be deemed complete for review purposes.

Land Use Hearing Intake Form Template

- Hearings Office Case No.: 4220018
- BDS Assigned Case Number: LU 22-187796 ADP AP
- Applicant/Owner: Jesse Winterowd
- Appellant: _____
- Site Address: 1311 SW Gibbs Street
- Planner: Leah Dawkins
- Planner's Phone Number: (503) 865-6734
- Deemed complete date: November 07, 2022
- Length of time needed: 1 hour
- Level of complexity: Medium
- Level of controversy: Low
- Type: III
- Is the HO Hearings Room 3000 large enough? (Capacity: 49)
Yes
- Is this case subject to 100 day clock?: No
- Current date of 120th-day (re-calculate if necessary): _____
- What date is the 51st day?: December 28, 2022
- Do you have a signed waiver or extension: No
- Preferred hearing dates (include 3): December 28, 2022, January 04, 2023, January 09, 2023, (Monday or Wednesday priority days)



Hearings Office

City of Portland

1900 SW 4th Avenue, Room 3100, Portland, OR 97201 phone: 503.823.7307
www.portland.gov/omf/hearings fax: 503.823.4347
email: HearingsOfficeClerks@portlandoregon.gov



Land Use Hearing Participation Information

Case Number: LU 22-187796 ADP AP (Hearings Office Case Number 4220018)

Site Address: 1311 SW Gibbs Street

Hearing Date: December 28, 2022

Hearing Time: 2:00 PM

The above-referenced land use hearing will take place via the Zoom platform.

Zoom Participation Options and Instructions

Zoom Option

- You will need an internet connection and a personal computer or other internet capable device.
- You can listen, view the City Planner’s PowerPoint presentation, and you have the opportunity to provide oral testimony.
- Instructions:

Go to:

<https://us06web.zoom.us/j/88936098415>

Telephone Option

- You will need a telephone.
- You will be able to listen and testify but you will not be able to view documents and presentations shared on the Zoom platform (if any).
- Instructions:

Dial: +1 669 900 6833

Meeting ID: 889 3609 8415#

Written Testimony

- Prior to the Hearing and until the close of evidence, you may submit written testimony. In other words, written testimony can be provided up to the close of the record, which will be announced at the hearing.
- *Note: A document’s timeliness is determined by the date/time when it is received by the Hearings Office Clerks.*

- Please reference the case number in any documents you wish to submit.
- Instructions/Options:

Online Case Management System

<https://www.portland.gov/omf/hearings/land-use>



Email: HearingsOfficeClerks@portlandoregon.gov

Fax: 503-823-4347

Mail: Hearings Office, 1900 SW 4th Avenue, Room 3100,
Portland, OR 97201

View File Contents

We use a fully electronic case file. Your online viewing options are available at our website: <https://www.portland.gov/omf/hearings/land-use>

If you do not have online access, you may make a written request for a hard copy of any exhibit to be mailed to you.

Meaningful Access

The City of Portland is committed to providing meaningful access.

For accommodations, modifications, translation, interpretation, or other services, please call 503-823-7307, the TTY at 503-823-6868 or the Oregon Relay Service: 711. Traducción e interpretación | Chuyển Ngữ hoặc Phiên Dịch | 翻译或

传译

Письменный или устный перевод | 翻訳または通訳 | Traducere sau

Interpretare 번역 및 통역 | Письмовий або усний переклад | Turjumida ama Fasiraadda|

Questions or Concerns

You may contact the Hearings Clerks via email or by leaving a voicemail with the office: HearingsOfficeClerks@portlandoregon.gov; 503-823-7307.



Hearing Office

City of Portland

1900 SW 4th Avenue, Room 3100, Portland, OR 97201

www.portlandoregon.gov/hearings

email: HearingsOfficeClerks@portlandoregon.gov

phone: 503.823.7307

fax: 503.823.4347



Land Use Hearing scheduled

A public hearing in land use case no. 4220018 has been scheduled for 2:00 PM, December 28 2022. The Hearings Office Land Use Administrative Rules require that the Bureau of Development Services make available a copy of its staff report and recommendation in a Type III proceeding at least 10 days before the date of the hearing, and the administrative decision and appeal in a Type II proceeding at least 7 days before the date of the hearing.

In the discretion of the Hearings Officer, failure of the Bureau of Development Services to make available the report or administrative decision and appeal within the time required by the Code may constitute grounds for continuing the hearing or holding the record open. In so determining, the Hearings Officer will consider the particular circumstances of the case, the possible prejudice to the persons failing to receive a copy of the material or to any other party, and the reason for the failure to comply.



City of Portland, Oregon
Bureau of Development Services
Land Use Services
 FROM CONCEPT TO CONSTRUCTION

Dan Ryan, Commissioner
 Rebecca Esau, Director
 Phone: (503) 823-7300
 Fax: (503) 823-5630
 TTY: (503) 823-6868
www.portland.gov/bds

STAFF REPORT AND RECOMMENDATION TO THE HEARINGS OFFICER

CASE FILE: LU 22-187796 APD AP
 Hearings Office Case #4220018
 PC # 22-144899
 REVIEW BY: Hearings Officer
 WHEN: December 28, 2022 at 2:00 pm

This land use hearing will be held remotely via the Zoom platform.

More information on how to participate is available at www.portland.gov/omf/hearings/land-use and with the Notice of Public Hearing posted at <https://www.portlandoregon.gov/bds/article/801313>.

It is important to submit all evidence to the Hearings Officer. City Council will not accept additional evidence if there is an appeal of this proposal.

BUREAU OF DEVELOPMENT SERVICES STAFF: LEAH DAWKINS / LEAH.DAWKINS@PORTLANDOREGON.GOV

GENERAL INFORMATION

Applicant: Jesse Winterowd, Winterbrook Planning
 610 SW Alder Street, #810
 Portland, OR 97205
 (503) 827-4422 / jesse@winterbrookplanning.com

Owner/Agent: SWS PDX LLC
 15110 Boones Ferry Road, #500
 Lake Oswego, OR 97035

Developer: Sean O'Neill, Scotia Western States Housing, LLC
 6340 N Campbell Avenue, #240
 Tucson, AZ 85718

Site Address: 1325 SW Gibbs Street

Legal Description: Partition Plat 2021-41, Lot 1, INC UND INT Tract A
Tax Account No.: R649931610
State ID No.: 1S1E09BD 02801
Quarter Section: 3327

Neighborhood: Homestead, contact at land-use@homesteadna.org
Business District: None
District Coalition: Southwest Neighborhoods Inc., contact Sylvia Bogert at 503-823-4592.
Plan District: None
Other Designations: Potential Landslide Hazard

Zoning: Residential 7,000 (R7), Residential Multi-Dwelling 2 (RM2), Environmental Conservation Overlay (c), Constrained Sites Overlay (z)

Case Type: Planned Development Amendment (APD), Partition Amendment (AP)

Procedure: Type III, with a public hearing before the Hearings Officer. The decision of the Hearings Officer can be appealed to City Council.

Proposal:

The applicant received Land Use approval for a 5-story 27-unit multi-dwelling structure with 28 vehicle parking spaces and 30 long-term bike parking spaces through a Planned Development Review, a Land Division Partition, and Environmental Modifications submitted in 2019 (LU 19-258445 PD LDP ENM). The applicant is now proposing Amendments to the original Planned Development and Partition approvals to allow for an increase in the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces. The building envelope, façade, site access, and circulation are not proposed to change. The environmental tract created in the 2019 land use review will not change. Previously approved utility access will not change.

To better meet the outdoor area requirements and comply with new façade articulation requirements, the applicant has requested the following modifications through the Planned Development Amendment:

- Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2);
- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

This proposal is reviewed through a Type III procedure because an increase in density is considered a major change to an Approved Planned Development and the original Planned Development application was processed through a Type III procedure (33.854.500-.510). The Land Division Amendment is being processed concurrently through the Type III procedure.

Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of *Title 33*. The applicable approval criteria are found in sections:

- **33.660.320.B, Approval Criteria for Other Changes to an Approved Preliminary Plan**
- **33.854.520,, Approval Criteria for Changes to an Approved Planned Development**
- **33.854.320, Approval Criteria for Modifications of Site-Related Development Standards**

ANALYSIS

Site and Vicinity: The site is located on the north side of SW Gibbs Street as it turns into SW Marquam Hill Road. New development is currently under construction on the site as approved through LU19-258445 PD LDP ENM. The site slopes moderately to steeply down to the north and west beyond the developed portion and is densely forested with a canopy that connects to the adjacent nature park.

Properties immediately east and south of the site are developed with single family homes (R5 zone). Multiple apartment buildings are located immediately north of the site. Marquam Nature Park (OS zone) is located directly west of the site. The OHSU campus is approximately 1,000-feet to the east.

Infrastructure:

- **Streets** – The site is a full city block with approximately 200-feet of frontage on SW Gibbs Street, which turns into SW Marquam Hill Road, SW 13th Avenue, SW Grover Street, and SW 14th Avenue. There are two driveways entering the site from SW Gibbs Street that serve the existing development. At this location, SW 13th Avenue, SW Grover Street, and SW 14th Avenue are classified as Local Service Streets for all modes in the Transportation System Plan (TSP). SW Gibbs Street/Marquam Hill Road is classified as Local Service for transit and freight, Neighborhood Collector, City Bikeway, City Walkway, Major Emergency Response, and Community Collector in the TSP. A Public Recreational Trail designation (SW Trail #1) exists within the SW Gibbs Street right-of-way adjacent to the site. Tri-Met provides transit service approximately 520-feet east of the site at the intersection of SW Gibbs Street and SW 11th Avenue via Bus #8.

At this location, SW 13th Avenue, SW Grover Street, and SW 14th Avenue are all 60-foot wide unimproved rights-of-way. SW Gibbs Street/Marquam Hill Road is improved with a 24-foot paved roadway surface (fog line to fog line) within a 60-170-foot wide right-of-way. There are no curbs, planter strips, or sidewalks.

- **Water Service** – There is an existing 8-inch CI water main in SW Gibbs Street.
- **Sanitary Service** - According to available GIS data, the following sewer infrastructure is located in the vicinity of the project site:
 - Public 8-inch CSP sanitary sewer in SW 13th (BES as-built # 3172). This main is within the conservation zone.
 - Public 12-inch CSP combined sewer in SW Gibbs St east of the property (BES as-built# 1182). This is a newly extended main built through EP555.
 - Public 8-inch CIP combined sewer in the right-of-way north of the property (SW Grover Street) and within a public easement north of the property (BES as-built # 2234).
- **Stormwater Disposal** – There is no public storm-only sewer currently available to this property.

Zoning: Residential 7,000 – The single-dwelling zones are intended to preserve land for housing and to provide housing opportunities for individual households. The zones implement the comprehensive plan policies and designations for single-dwelling housing and provide options for infill housing that is compatible with the scale of the single-dwelling neighborhood.

Residential Multi-Dwelling 2 – The RM2 zone is a medium-scale multi-dwelling zone that is generally applied in and around a variety of centers and corridors that are well-served by transit. Allowed housing is characterized by buildings of up to three or four stories with a higher percentage of building coverage than in the RM1 zone, while still providing opportunities for landscaping and outdoor spaces that integrate with residential neighborhood characteristics. The major types of new housing development will be a diverse range of multi-dwelling structures and other compact housing that contribute to the intended urban scale of centers and corridors, while providing transitions in scale and characteristics to lower-scale residential neighborhoods.

Environmental Zone (33.430) – Environmental Conservation Overlay: The “c” overlay is intended to conserve important environmental features and resources while still allowing compatible development. Land divisions and planned developments must meet environmental standards or go through environmental review. No disturbance area is proposed within the environmental overlay zones as a result of this amendment review.

Environmental Resources: The application of the environmental overlay zones is based on detailed studies that have been carried out within separate areas throughout the City. Environmental resources and functional values present in environmental zones are described in environmental inventory reports for these respective study areas.

The project site is mapped within the Southwest Hills Resource Protection Plan as Site No. 113: Marquam Hill Ravine. Resources and functional values of concern on the project site include wildlife habitat, groundwater recharge, scenic, recreation, perennial and seasonal creeks, and forested wetlands.

Land Use History: City records indicate the following prior land use reviews for this site:

- **CU 075-76:** Approval of a Conditional Use Review for a convalescent home.
- **CU 020-79:** Approval of a Conditional Use review for a welfare institution with variances to minimum site area and minimum front yard requirements.
- **LUR 96-00236:** Approval of an expansion of a conditional use for a group living use in a residential zone at 1325 SW Gibbs, in order to create an enclosed connection between the two existing buildings and to add a vertical lift.
- **LU 19-258445:** Approval of a 1-parcel partition resulting in one parcel and an Environmental Tract; approval of a Planned Development Review for a multi-dwelling structure in an R1 through R2.5 zone, that consists of a 5-story, 27-unit apartment building; and approval of several Environmental Review Modifications.

This proposal is an amendment to the Planned Development Review and Land Division Partition portions of LU 19-258445 PD LDP ENM.

Agency Review: Several Bureaus have responded to this proposal and relevant comments are addressed under the applicable approval criteria. Exhibits “E” contain the complete responses.

Neighborhood Review: A Notice of Public Hearing on a Proposal in Your Neighborhood was mailed on December 5, 2022 and revised notice was mailed on December 12, 2022 to clarify that the hearing will be remote only. Two written responses were received from a notified property owner and the neighborhood association in response to the proposal. Comments expressed support for the overall proposal to add additional units to the approved development, but expressed continued concern about the lack of pedestrian and bicycle improvements within the public right-of-way at the intersection of SW Gibbs and 13th Avenue in addition to drawing attention to a general lack of pedestrian infrastructure within this neighborhood.

This proposal has been reviewed by the Bureau of Transportation for transportation impacts and right-of-way improvements. Within the context of this proposal, Staff found that off-site improvements to the adjacent intersection are not warranted by the impacts of this development. See additional information discussed below under 33.660.320.B.2. The applicant has been working with concerned neighbors and the Bureau of Transportation to make improvements to the adjacent right-of-way in addition to what was required through the previously approved land use review. The applicant has indicated that they will continue to work with the neighborhood outside of the requirements of this and the prior land use review.

ZONING CODE APPROVAL CRITERIA

APPROVAL CRITERIA FOR CHANGES TO AN APPROVED PRELIMINARY PLAN IN OPEN SPACE AND RESIDENTIAL ZONES

33.660.320.B, Approval Criteria for Other Changes to an Approved Preliminary Plan The following criteria apply to other changes to an Approved Preliminary Plan

1. **The proposed changes are not substantial enough, singly or in combination to warrant a new review of the entire preliminary plan; and**

Findings: The applicant is proposing to increase the number of dwelling units within the building envelope approved through **LU 19-258445**. Because the partition was approved concurrent with a Planned Development (PD) and subject the requirement to build the specific development proposed through the PD, an amendment to the approved partition is required to approve the increase in units. However, the proposed change will not substantially change the approved preliminary plan. There will be no change to the lot or environmental resource tract. Criteria K and L, Transportation Impacts and Services and utilities could be impacted by the change to the number of units proposed through the PD. These two approval criteria are addressed below.

2. **The approval criteria addressed by the approval of the Preliminary Plan can still be met, with appropriate conditions of approval.**

Findings: Staff has determined that the following approval criteria are applicable to the proposed change to the approved Preliminary Plan, which involves the increase in the number of units from 27 to 43, because they could be impacted by the increased number of units:

K. Transportation Impacts. The approval criteria of Chapter 33.641, Transportation Impacts, must be met; and

Findings: The transportation system must be capable of supporting the proposed development in addition to the existing uses in the area.

Evaluation factors include: safety, street capacity, level of service, connectivity, transit availability, availability of pedestrian and bicycle networks, on-street parking impacts, access restrictions, neighborhood impacts, impacts on pedestrian, bicycle, and transit circulation. Evaluation factors may be balanced and measures to mitigate impacts may be necessary.

The Development Review Section of the Portland Bureau of Transportation (PBOT) has reviewed the application against the evaluation factors and has provided the following findings (see Exhibit E.2):

For the previous Planned Development land-use review (19-258445 PD), the applicant submitted a Transportation Impact Study (TIS), professionally prepared by Lancaster Mobley, to support the transportation-related approval criteria, in which PBOT reviewed and agreed with the conclusions that the transportation-related approval criteria are satisfied.

With the proposed amendment, the applicant then submitted an addendum to the previous TIS, accounting for increase in trip generation to then site based on the addition of units from 27 to 43, which changes the trip generation increase from 7 PM Peak Hour trips to 12 PM Peak Hour trips [compared to the trips estimated for the prior use at the site]. This is still a minimal increase in trips and will not degrade the Level of Service (LOS) below the acceptable LOS threshold of 'E' at all nearby stop-controlled intersections. Therefore, PBOT approves the updated TIS and the same comments as previously identified still apply. Therefore, based on the evidence included in the record, the applicant has demonstrated to PBOT's satisfaction that the transportation system is capable of supporting the proposed use in addition to the existing uses in the area.

PBOT has reviewed and concurs with the information supplied and available evidence. Given the limited increase in trips, PBOT found that transportation improvements beyond those already required are not warranted (see additional discussion under Criterion L below). These criteria are met.

L. Services and utilities.

Findings: Chapters 33.651 through 33.654 address water service standards, sanitary sewer disposal standards, stormwater management, utilities and rights of way. The criteria and standards are met as shown in the following table:

<p>33.651 Water Service standard – See Exhibit E.3</p> <p>The Water Bureau has indicated that service is available to the site from the 8-inch CI water main in SW Gibbs Street, as noted on page 3 of this report. The water service standards of 33.651 have been verified. Therefore, this criterion is met.</p>
<p>33.652 Sanitary Sewer Disposal Service standards – See Exhibit E.1</p> <p>The Bureau of Environmental Services has indicated that service is available to the site, as noted on page 3 of this report. The sanitary sewer service standards of 33.652 have been verified.</p>
<p>33.653.020 & .030 Stormwater Management criteria and standards – See Exhibit E.1</p> <p>No stormwater tract is proposed or required. Therefore, criterion A is not applicable. The applicant submitted an updated Stormwater Report to address this criterion. BES reviewed the proposed amendments and utility plans and the response is summarized below:</p> <p>Private Property Stormwater Management: Stormwater runoff from this project must comply with all applicable standards of the SWMM and be conveyed to a discharge point along a route of service approved by the BES Director or the Director’s designee.</p> <p>From the submitted site plans, it appears that the proposed Amendment to Planned Development and Amendment to Partition application does not include any changes to the previously approved stormwater management plan. This proposal does not include an increase in impervious area beyond what has been approved under LU 19-258445 PD LDP and 20-194259 CO. The stormwater management system proposed is consistent with the plan approved under 20-194259 CO.</p> <p>Public Right-of-Way Stormwater Management: Stormwater runoff from public right-of-way improvements as required by the City of Portland Bureau of Transportation (PBOT) must be managed according to the standards of the SWMM and the Sewer and Drainage Facilities Design Manual.</p> <p>PBOT requires frontage improvements to include pavement widening along the frontage of the property. Public stormwater management improvements per the standards of the SWMM and the Sewer and Drainage Facilities Design Manual are therefore required. BES understands that given the elevation of the street, the lack of room in the ROW, and the landslide hazard area, stormwater management for the ROW improvements are challenging. BES has approved the Special Circumstance application allowing the applicant to pay an offsite management fee. BES Development Engineering approved the Concept Development plans (i.e. 30% design) for the right-of- way stormwater improvements on 4/23/20; therefore, BES finds that public stormwater infrastructure can be constructed as shown on the applicant’s Preliminary Site Utility Plan.</p> <p>The Bureau of Environmental Services (BES) has determined that the applicant’s stormwater management plan is acceptable for the purpose of reviewing the Amendment to Planned Development and Amendment to Partition application against stormwater management approval criterion. This criterion is met.</p>
<p>33.654.110.B.1 Through streets and pedestrian connections</p> <p>Generally, through streets should be provided no more than 530 feet apart and pedestrian</p>

connections should be provided no more than 330 feet apart. Through streets and pedestrian connections should generally be at least 200 feet apart.

There are several “paper” platted streets surrounding the site, SW 13th Avenue, SW Grover Street, and SW 14th Avenue, which meet the spacing standards. However, PBOT determined in the previous land use review (LU 19-258445) of this site that given the challenging terrain, Environmental overlays, Open Space zoning, landslide hazards, tree canopy, and water features, it is not practical or appropriate to provide street improvements to extend either a public street or pedestrian connection through the subject site or in the existing unimproved rights-of-way in order to further the City’s connectivity goals. The environmental constraints noted above have not changed and PBOT has indicated in their review of the current proposal that no new or additional transportation improvements are required for the current land use review.

For the reasons described above, this criterion is met.

33.654.120.B & C Width & elements of the right-of-way – See Exhibit E.2

33.644.120.H Street Trees – See Exhibit E.6

At this location, SW 13th Avenue, SW Grover Street, and SW 14th Avenue are all 60-foot wide unimproved rights-of-way. SW Gibbs Street/Marquam Hill Road is improved with a 24-foot paved roadway surface (fog line to fog line) within a 60-170-foot wide right-of-way. There are no curbs, planter strips, or sidewalks.

In reviewing this land division, PBOT relies on accepted civil and traffic engineering standards and specifications to determine if existing street improvements for motor vehicles, pedestrians and bicyclists can safely and efficiently serve the proposed new development. As previously noted, SW 13th Avenue, SW Grover Street, and SW 14th Avenue will not be required to be improved in relation to the proposed development due to numerous environmental constraints.

The following frontage improvements along SW Gibbs Street were required through LU 19-258445:

- The applicant shall provide a minimum 6-ft wide paved shoulder widening.
- On-street parking must be removed.
- The existing guardrail must be replaced with current AASHTO-compliant guardrail.

For the required frontage improvements, the applicant submitted Public Works permit 19 193047 WT | TH0903 which has received 30% concept approval and bond payment, ensuring that vehicle, bicycle, and pedestrian circulation system is designed to be safe, efficient, and to city required standards.

Urban Forestry evaluated the provision of street trees and planting areas for the public right-of-way and does not object to this proposal. All street tree permitting is deferred to Public Works Permit TH0903.

No dedication or additional conditions of approval are necessary to provide required right-of-way elements. This criterion is met.

33.654.130.A - Utilities (defined as telephone, cable, natural gas, electric, etc.)

At the time of the final plat for FP 19-258445, time no specific utility easements adjacent to the right-of-way were identified as being necessary. Therefore, this criterion is met.

APPROVAL CRITERIA FOR CHANGES TO AN APPROVED PLANNED DEVELOPMENT**33.854.520 Approval Criteria**

The approval criteria for changes to a Planned Development are those used for approval of the original planned development application. The original planned development application, LU 19-258445 PD ENM LDP, was reviewed under the approval criteria of 33.854.310 Approval Criteria for Planned Developments in All Zones.

33.854.010 Purpose

These regulations assign Planned Development Reviews to an appropriate procedure type. The approval criteria ensure that innovative and creative development is encouraged when it is well designed and integrated into the neighborhood.

33.854.310 Approval Criteria for Planned Developments in All Zones

Criteria A through E apply to proposals for additional height or FAR in the CM2, CM3, CE, and CX zones that are taking advantage of 33.270.100.I. If the Planned Development is not proposing additional height or FAR as allowed by 33.270.100.I, then only criteria E through H apply.

Applicant findings provided below have been modified by staff to provide correct references to exhibit numbers. Applicant findings that have been modified by staff are in **bold**.

Findings: The applicant is proposing an Amendment to a Planned Development review to allow a multi-dwelling structure within an RF-R5 zone, which will consist of a 5-story, 43-unit multi-dwelling structure that will include 28 vehicle parking spaces and 51 long-term bike parking spaces. Therefore, only criteria E through H apply.

- E.** Configure the site and development to visually integrate both the natural and built features of the site and the natural and built features of the surrounding area. Aspects to be considered include:
 1. Orienting the site and development to the public realm, while limiting less active uses of the site such as parking and storage areas along the public realm;

Findings: The applicant provided the following findings:

The site is located on a sloping bend on the ROW of SE Gibbs Street, approximately 15 feet below the grade. The building has been designed to have the main entrance on the second floor, with a short pathway leading from the main entrance to the public ROW of SW Gibbs Street. The site's driveway has been reoriented to provide vehicle access from only the southwest corner of the site (**Sheet C1 of Exhibit C.7**). With commercial and employment centers to the east of the site, this reorientation will ensure that pedestrian flow between the main entrance and these destinations will remain unobstructed. The driveway will circle to the back of the building connecting to vehicle parking on the ground floor. Both the second floor (main lobby) and ground floor (lower lobby) will have secure bicycle parking rooms. All other storage areas are planned on the ground floor, below the grade of the existing roadway and public realm.

The building is setback 3-feet from the front property line and is the prominent feature of the site as viewed from the public realm. The structured parking at the base of the building is imperceptible from the right-of-way based on noted grade differences. A bike storage room and trash enclosure are also located in this area where they are out of sight. The development is oriented to the public realm and less active uses of the site are limited. Therefore, this criterion is met.

- 2. Preservation of natural features on the site, such as stands of trees, water features or topographical elements;

Findings: The applicant provided the following findings:

The site contains a flat, developed area near SW Gibbs Street before dropping steeply downhill to the north. An environmental conservation overlay zone exists on the northern portion of the site which contains a dense forested area. As part of the previously approved PD, a land division was approved to create a protected environmental resource tract. This tract preserves the existing stands of trees and topographical elements on site. No identified continuously flowing water features or wetlands exist on the site. A drainage area is located within the environmental tract as shown on the Vicinity Map (Exhibit A.15). The nearest disturbance area will be approximately 80 feet away, as shown on **Sheet C4 of Exhibit C.7**.

Tree removals onsite have been minimized to the extent possible. Since approval of LU #19-258445 PD, ENM, LDP, additional tree removals within the ROW have been found by PBOT to be necessary. These additional tree removals are addressed in Public Works permit 19-193047 WT | TH0903, which has received 30% approval. All onsite tree preservation proposed and approved on **Sheets L4 and L5 of Exhibit C.6** are proposed to continue to remain preserved. Mitigation plantings are proposed for on-site tree removals.

Natural features are present on the northern portion of the site and include steep slopes with a heavily treed closed canopy area characterized by big leaf maple and Douglas fir trees. As previously noted, this area was placed within an environmental resource tract through LU 19-258445 and will continue to be permanently preserved. The natural features of the site have been preserved to the extent practical. Therefore, this criterion is met.

3. Inclusion of architectural features that complement positive characteristics of surrounding development, such as similar building scale and style, building materials, setbacks, and landscaping;

Findings: The applicant provided the following findings:

The surrounding development includes a mix of single-family dwelling, multi-dwelling structures, and some commercial uses. At times, businesses and residences are carved into the hillsides, using the natural topography to create daylight basements and second floor main entrances like the one proposed in this application. This strategy creates the appearance of smaller scale structures that blend with the range of sizes of single-family residences.

The proposed design was also approved in 2020 (LU #19-258445 PD, ENM, LDP) and the building façade and outdoor features are not proposed to deviate from this previous approval. Since the development was approved, two additional apartment complexes have been approved in this neighborhood. Both apartments did not have to adhere to RM2 standards. The proposed architectural features that mirror the surrounding development include:

- Personal balconies and common decks
- Neutral tones
- Vertical natural wood paneling
- Flat roofs
- 4-stories from grade

The proposed design includes more modern development designs that will match the apartments currently under construction along SW 12th Ave.

The proposed development has incorporated positive characteristics of surrounding development with consideration to scale and style, building materials, setbacks, and landscaping and will not deviate from what was approved in 2019. Staff finds the scale and setbacks of the proposed building to be similar to that of development within 2 to 3 blocks east of the site. Multi-dwelling

structures addressed 3223 SW 11th Avenue, 1033 SW Gibbs Street, and 3211 SW 10th Avenue are between 3 and 4 stories in height. Therefore, this criterion is met.

4. Mitigation of differences in appearance through means such as setbacks, screening, landscaping, and other design features;

Findings: The applicant provided the following findings:

Directly opposite of the site across SW Gibbs Street is a single-family dwelling that sits approximately 5 -10 feet above grade. The home is screened with thick shrubs with a lower story entrance and driveway parking onto SW Gibbs St. and an upper story entrance and driveway parking on SW 13th Street. The site is surrounded by heavily vegetative unimproved ROWs to the east, west, and north.

- Beyond the unimproved ROW to the west is Marquam Nature Park – a densely forested park with mature trees reaching 40' and beyond in height.
- Beyond the unimproved ROW to the east are a series of four older one- and two-story single-family dwellings.
- Beyond the unimproved ROW to the north is the Forest View Apartments, a recently remodeled apartment complex built in 1961 and containing 25 units.

The vegetative unimproved ROWs, the proposed landscaping surrounding the building, and the protected environmental tract along the north and west side of the site (see LU #19-258445 PD, ENM, LDP) will screen the building from these adjacent uses.

The primary difference in appearance is the presence of a multi-dwelling structure within a single-dwelling zone. The proposed development adequately mitigates for this difference in appearance through incorporating existing topography, natural areas, and landscaping. Therefore, this criterion is met.

5. Minimizing potential negative effects on surrounding residential uses; and

Findings: The applicant provided the following findings:

The potential negative effects of the proposed development on surrounding residential uses are typical of any increase in residential units: traffic, noise, and light. These impacts are addressed as follows:

- Traffic: Transportation impact studies are provided in **Exhibit A.5**. As indicated in the TIS, the use will generally match the residential traffic characteristics of the surrounding area and travel patterns will not significantly alter from what is typical for the neighborhood. Impacts to the neighborhood will be minimal. Vehicle types and travel patterns will be similar to existing neighborhood conditions. The proposed 43 units will add an additional 144 weekday total trips to the surrounding transportation system. These new site trips are not expected to significantly alter the operation or safety of the existing transportation facilities, beyond those previously approved in LU #19-258445 PD, ENM, LDP. Further, off-site parking will not be impacted because the proposal will provide 4 additional spaces than required by PZC 33.266. For 43 units, 22 spaces are required; 28 spaces are proposed.
- Noise: The site is buffered by forested vegetation on three sides provided by the terrain and unimproved conditions of the adjacent rights-of-way. Vehicle parking and maneuvering areas will be under and inside the structure, while the small outdoor parking area is located on the far side of the proposed building from adjacent residential property to the east and below the street grade from the adjacent residence to the south. These features will provide a buffer from automobile noise generated on site.

- Light: Sources of light impacts will come from automobile headlights and interior and exterior lighting from the structure and pedestrian walkways. A screen of natural, mature vegetation exists on three sides of the property, and the small outdoor parking area is below the street grade and on the opposite side of the proposed structure from adjacent residential development. Landscaping as shown on Sheet L4, will provide additional screening from headlights in the parking area. Impacts from interior and exterior lights will be mitigated due to the location of the structure below the street grade and within a buffer of mature vegetation.

Potential negative effects to surrounding residential uses are primarily minimized by utilizing the differences in grade to present a less impactful development and preserving approximately half the site in forested open area. As noted above, traffic, noise, and light are minimized to the extent practicable. Therefore, this criterion is met.

6. Preservation of any City-designated scenic resources; and

Findings: There are no City-designated scenic resources on the site. Therefore, this criterion does not apply.

F. Open Area. On sites zoned RF through R2.5

1. Where proposed development includes attached houses, duplexes, triplexes, fourplexes, multi-dwelling structures, or multi-dwelling development, adequate open area to accommodate the proposed development must be provided. Open area does not include vehicle areas.
2. Where multi-dwelling development with detached single dwelling units is proposed, 50 percent of the total number of dwelling units on the site must be oriented around a common outdoor area.

Findings: The applicant provided the following findings:

The proposed multi-dwelling structure is located on property zoned both R7 and RM2, (**see Site Plan-Exhibit C.1**). With 43 units, 2,064 square feet of outdoor area is required. 27 units will have personal balconies, totaling 1,296 square feet. An additional 970 square feet of common outdoor areas are proposed. A total of 2,266 square feet of outdoor areas are proposed, 202 square feet more than the required amount. This is enough additional square footage to accommodate another 4 units in addition to the 43 units proposed.

As for quality, outdoor areas are proposed to be surfaced with either decking materials or grass. The lower common lawn is planned along the main pedestrian pathway between the surface parking/maneuvering areas and the lower lobby entrance. This location will allow for more frequent and natural-feeling use of the space, rather than if it were planned on the opposite side of the site.

The upper deck will be accessed from a main entrance on the second floor of the building, and it will look out towards the forested slopes of Marquam Park. The scenic views and large dimensions will be inviting to visitors and residents. Neither the upper deck nor the lower lawn are proposed to be roofed or enclosed, but both could accommodate tables and seating for users.

The applicant has requested modifications to the common outdoor area standards. The modification approval criteria are addressed later in this report. With the modifications, adequate open area can be provided with the proposed development. Therefore, criterion F.1. can be met. The applicant is not proposing any detached single dwelling units, so criterion F.2. does not apply.

- G.** Accessible connections. Provide one or more accessible routes that connect all buildings on the site to adjacent streets, common open areas, and parking areas. Use landscaping and site furnishings to ensure the accessible route provides a pleasant user experience.

Findings: Accessible routes have been provided throughout the site, connecting each building entrance to one another and to SW Gibbs, the common open areas, and the parking area. A new path is being provided to connect the lower lawn common area to the rear main entrance of the building. Wayfinding signs will be installed to direct users to the lower lawn open space. Additional landscaping and furnishings are proposed to enhance the new lower lawn open space. With the condition that the site paths are provided as shown on the architectural, landscaping, and engineering plans (Exhibits C.1-C.7), this criterion is met.

- H.** Garbage and recycling areas. Garbage and recycling collection areas must be adequate in size to accommodate the proposed development, designed to encourage recycling, and located to facilitate pick-up service. Screening and buffering of garbage and recycling areas must be provided to maintain a clean and attractive development.

Findings: The applicant has proposed to provide a garbage and recycling collection area that will be beneath the upper common area deck adjacent to the parking area on the first floor of the building. The garbage and recycling area is accessible for collection from the driveway adjacent to the building accessing the parking garage. The garbage and recycling collection area is enclosed within the parking garage and is screened from public view. This criterion is met.

APPROVAL CRITERIA FOR MODIFICATIONS OF SITE-RELATED DEVELOPMENT STANDARDS THROUGH A PLANNED DEVELOPMENT

33.854.320 Additional Approval Criteria for Modifications of Site-Related Development Standards

The following criteria apply to modifications of site-related development standards, including parking standards. These modifications are done as part of a Planned Development review and do not have to go through the adjustment process.

Modification to Façade Articulation Standards

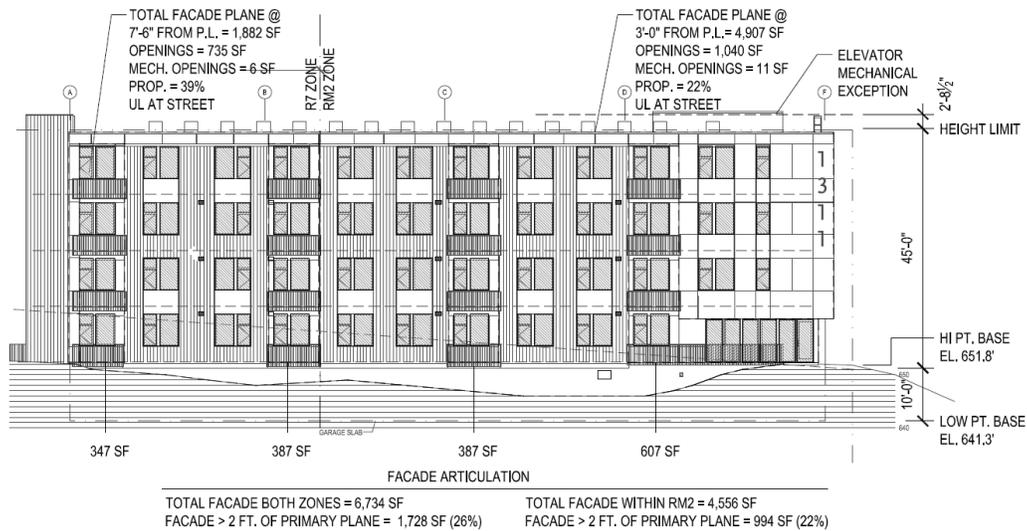
The applicant is requesting a modification to reduce the area of façade articulation from 25 percent to 22 percent of the total street-facing surface area as required by 33.120.230.C.2. This modification is triggered by a change in the Zoning Code since the original land use review was approved. The modification will be approved if the following approval criteria are met:

- A. Better meets approval criteria. The resulting development will better meet the approval criteria of Section 33.854.310, above; and**

Findings: The applicant has provided the following findings:

This standard applies to the development proposal because the building is setback within 20-feet of the front street property line. The building is planned roughly 3 feet from the front lot line because of the presence of the environmental overlays on the northern (rear) and eastern (right) areas of the site. The façade articulation standard is a new standard that did not apply to the original building design and approval. The proposal is for interior changes to floor plans that increase housing units within the original design approval. The original design was determined by City of Portland planning staff and the Hearings Officer to meet planned development approval criteria for site orientation, preservation of natural features, neighborhood compatibility, accessibility and garbage/recycling areas – the same criteria applicable to this modification. Since the design has not changed, these elements continue to be met, as discussed below.

Granting the modification would waive a requirement for an additional 145 square feet of façade articulation. On the 45-foot-tall building sitting 15-feet below the street grade, this amounts to one 3.2-foot-wide strip of additional articulation. This strip width would be virtually unnoticeable to any pedestrian, bicyclist, or motorist.



A portion of the front façade is below the grade of the road (see Figure 8). This small portion – above the garage slab – will not be visible to pedestrians or bicyclists/motorists. Removing this portion of the façade from the total calculations, 25% of the front façade in RM2 will be offset. This meets the minimum façade articulation standard. Further, when we consider the entire front façade in both R7 and RM2, the total façade articulation will be 26%. This total articulation exceeds the minimum standard in RM2.

The proposal better meets applicable PD approval criteria (PZC 33.854.310.E through H) for the following reasons:

- E.1 Orienting the site and development to the public realm, while limiting less active uses of the site such as parking and storage areas along the public realm: The building will be 3-feet from the front lot line and SW Gibbs street. The requested modification is triggered because of the close proximity of the building to the front setback. The modification supports the siting of the building near the ROW, oriented toward the public realm. This approval criterion is positively impacted through the proposed modification.
- E.2 Preservation of natural features on the site, such as stands of trees, water features or topographical elements: As with public realm orientation, siting the building near the front lot line will result in better preservation of natural resource areas. The location of the building allows for the protection of the resource area of the environmental overlays. This criterion will be better met with the proposed modification.
- E.3 Inclusion of architectural features that complement positive characteristics of surrounding development, such as similar building scale and style, building materials, setbacks, and landscaping: The proposed design was previously determined by City of Portland planning staff and the Hearings Officer to meet this standard. Existing multi-family development nearby the site does not include façade articulation elements enough to meet the 25% standard. The proposed development will be the first required to meet the 25% standard. The request to reduce the façade articulation area from 25 to 22% will result in a minimal reduction in façade offset areas. Further, the proposed development will include other architectural elements that are seen in the surrounding development patterns – balconies, neutral tones, vertical wood paneling, modern/contemporary smooth accent facades (Hardie shingle siding and fiber cement panels), flat roofs, and common deck space. This criterion will not be impacted from this modification request.

- E.4 Mitigation of differences in appearance through means such as setbacks, screening, landscaping, and other design features; & E.5 Minimizing potential negative effects on surrounding residential uses; &
- F.1 Where proposed development includes attached houses, duplexes, triplexes, fourplexes, multi-dwelling structures, or multi-dwelling development, adequate open area to accommodate the proposed development must be provided. Open area does not include vehicle areas: These criteria will not be negatively or positively impacted through the requested modification because a loss of 145 square feet of offset area will not be noticeable from the street on a building with a front façade totaling 11,290 square feet. Considering the entire façade in both R7 and RM2, this modification request will reduce façade articulation areas only by 1.2%. These criteria will remain unimpacted.
- G. Accessible connections. Provide one or more accessible routes that connect all buildings on the site to adjacent streets, common open areas, and parking areas. Use landscaping and site furnishings to ensure the accessible route provides a pleasant user experience; & H. Garbage and recycling areas. Garbage and recycling collection areas must be adequate in size to accommodate the proposed development, designed to encourage recycling, and located to facilitate pick-up service. Screening and buffering of garbage and recycling areas must be provided to maintain a clean and attractive development: Façade articulation percentages will not negatively or positively impact site circulation or the placement of garbage and recycling areas. These criteria will not be impacted.

The requested modification to façade articulation positively impacts the site design by allowing the building to be located closer to the street which better orients the site to the public realm and better protects natural resources on the site. Allowing the modification does not impact the original approved site design or existing built features of the building while allowing for additional density in the building permitted through the new RM2 zoning. As the applicant notes, the total façade articulation across the entire building is 26 percent, meeting the intent of the regulation. Façade articulation does not impact open areas, accessible connections, or garbage and recycling areas. This criterion is met.

B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standards for which a modification is requested.

Findings: The purpose of 33.120.230 Building Length and Façade Articulation is as follows:

Purpose. These standards, along with the height and setback standard, limit the bulk of buildings close to the street. These standards help ensure that large buildings will be divided into smaller components that relate to the scale and development patterns of Portland's multi-dwelling residential areas and add visual interest and variety to the street environment.

The applicant has provided the following findings:

As described above, the request for a 3% reduction in façade articulation area totaling 145 square feet is so minimal that it will be indistinguishable for passersby. Other design features, like site topography, front entrance design to utilize the second floor, the proportion of windows, and front landscaping lend more to help limit the bulk of the building so near the street. The requested modification will remain consistent with the purpose of the façade articulation standards.

The overall façade of the building provides 26% façade articulation, which meets the purpose of the standard by breaking up the building into smaller components and adding visual interest to the building for users of the street. The proposal is consistent with the purpose of the façade articulation standards. This criterion is met.

Modifications to Required Outdoor and Common Outdoor Area Requirements

The applicant has requested the following modifications to 33.120.240 Required Outdoor and Common Areas:

- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

The modifications will be approved if the following approval criteria are met:

A. Better meets approval criteria. The resulting development will better meet the approval criteria of Section 33.854.310, above; and

Findings: The applicant has provided the following findings:

The proposal better meets applicable PD approval criteria (PZC 33.854.310.E through H) for the following reasons:

- **E.1 Orienting the site and development to the public realm, while limiting less active uses of the site such as parking and storage areas along the public realm:** The upper common deck is proposed over the trash/recycling areas and three parking spaces. The requested dimension reduction is to allow for this deck to cover these areas on the ground floor. Figure 3 shows the deck and proposed driveway leading to the back of the site. Placing the deck here allows for more site area to plan driveway access to the rear, outside of the public realm. If the upper deck was designed to meet the 20-foot by 20-foot requirement, the outdoor area would need to be placed elsewhere on the site, making site planning more challenging to fit all elements, and most likely resulting in the reduction of the driveway area preventing access from the rear.

The lower common lawn is proposed near the northeast corner of the site, outside of the protected environmental resource areas. This area is behind the building and along a pedestrian pathway linking the lower lobby entrance to the surface parking areas. The request to reduce dimensional requirements will have no impacts on site orientation and layout. The request to locate the open space area farther away from the lower lobby main entrance will not impact the site orientation and relationship with the public realm.

- **E.2 Preservation of natural features on the site, such as stands of trees, water features or topographical elements:** As discussed above, proposing the upper deck over passive use areas (storage and parking) allows for a more compact site design. The requested dimensional modification allows for the building to be set further away from the environmentally sensitive areas resulting in greater protection of natural features. Placing the deck over parking and storage also results in less anticipated grading activities that would have resulted from leveling the steep site topography to include a useable outdoor open space area.

The dimensional modifications are requested in-part to limit disturbance to natural features. The site is steeply sloped both to the north and to the east. Expanding this area or placing this area elsewhere on the site to measure 20 feet by 20 feet and at least 500 square feet will require additional grading and impacts to trees. As it stands, placing the lower common lawn as near as possible to the pedestrian walkway and parking areas will allow for the full protection of natural resource areas and limit impacts associated with grading and fill. The proposal will positively impact preservation efforts.

Placing the lower common lawn nearest to the surface parking area allows for the better protection of natural features. The lower lobby main entrance is planned near the eastern edge of the of the building. The entrance is approximately 10 feet away from the natural resource areas of the protected environmental tract. If the proposed lower common lawn was moved nearer to the lower lobby main entrance, additional impacts from fill and grading activities would result. Locating the proposed common area further away for a

building entrance will allow for the full protection of natural resource areas and limit impacts associated with grading and fill. The proposal will positively impact preservation efforts.

- **E.3** *Inclusion of architectural features that complement positive characteristics of surrounding development, such as similar building scale and style, building materials, setbacks, and landscaping:* The requests do not positively or negatively impact architectural features. A roughly 2.5-foot reduction in width (approximately 107 square feet) to the upper deck will be unnoticeable to users. The reduction in size of lower common lawn dimensions and the location of the lower lawn do not positively or negatively impact architectural features.
- **E.4** Mitigation of differences in appearance through means such as setbacks, screening, landscaping, and other design features; &

E.5 *Minimizing potential negative effects on surrounding residential uses:* The upper deck is planned to be accessed from the second floor and will face the forested natural area of Marquam Park. Placing the largest common open space area here will reduce impacts to the adjacent residential uses to the north, east, and south. All noise expected from active and passive recreational use in this area will be buffered by the proposed building and large, heavily vegetative environmental resource track. The dimensional reduction modification to place the outdoor common area over the parking/storage areas will positively improve mitigation measures for surrounding residential uses.

Reducing the dimensional requirements for the lower common lawn area will not positively or negatively impact surrounding uses or contribute to mitigation measures to blend with existing development.

Increasing the distance between the proposed open space area and the building entrance will not positively or negatively impact surrounding uses or contribute to mitigation measures to blend with existing development.

- **F.1** *Where proposed development includes attached houses, duplexes, triplexes, fourplexes, multi-dwelling structures, or multi-dwelling development, adequate open area to accommodate the proposed development must be provided. Open area does not include vehicle areas;* Although the request is for a dimension reduction from 20 to roughly 17.5 feet wide, the upper deck will still remain a useable common open area. The deck will be 690 square feet with a roughly 10-foot by 10-foot “notch” on the southern portion of the deck to offset the dimension reduction (see figure 9). The deck will be open and surfaced with decking materials. The deck will remain wide enough to accommodate passive uses. This standard is not positively or negatively impacted by the requested modification.

Although the request is for a dimension reduction from 20 to roughly 11.5 feet wide, and a reduction from 500 to 280 square feet, overall combined outdoor area requirements will be met. Further, by allowing an alternative dimensional design for the creation of this open space area will result in an excess of 202 square feet more than what is required. This criterion will be positively impacted through the proposed modifications.

Although the request is for an increase in distance from a main entrance, by creating this open space area the proposal will exceed the open space minimum requirement by 202 square feet. With additional outdoor open space areas, this criterion will be positively impacted through the proposed modifications.

- **G.** *Accessible connections. Provide one or more accessible routes that connect all buildings on the site to adjacent streets, common open areas, and parking areas. Use landscaping and site furnishings to ensure the accessible route provides a pleasant user experience:* Site circulation will not be impacted through this dimensional modification request (for the upper common deck).

The proposed lower common lawn will be adjacent to the surface parking and pedestrian path connecting the parking areas and the lower lobby main entrance. The dimensional modification request will allow for the open space to be planned in a central location, directly adjacent to the main pedestrian pathway. If the open space area is required to meet dimensional minimums, the outdoor area would have to be moved to less active areas of the site, with lower accessibility opportunities. The resulting modifications will have a positive impact to this criterion.

The proposed lower common lawn will be adjacent to the surface parking and pedestrian path connecting the parking areas and the lower lobby main entrance (see Figure 10). The locational modification allows the open space area to be placed in a central location, along the main pathway between the lower lobby entrance and the surface parking areas. The common open space area will be accessible to all along this pathway. The resulting modifications will have a positive impact to this criterion.

- **H. Garbage and recycling areas.** *Garbage and recycling collection areas must be adequate in size to accommodate the proposed development, designed to encourage recycling, and located to facilitate pick-up service. Screening and buffering of garbage and recycling areas must be provided to maintain a clean and attractive development:* The ability to place the deck over the garbage/recycling areas and some parking spaces will allow these passive uses to be screened from public view. This criterion will be positively impacted by the requested modifications.

There will be no impacts to proposed garbage and recycling areas with the granting of the lower common lawn modifications.

The applicant has provided findings indicating that the proposed modifications will better meet the Planned Development approval criteria. The requested modifications will either have a positive impact on the approval criteria, such as improving natural features and hiding the garbage and recycling areas, or will have no impact on the approval criteria. With the condition that the common outdoor areas are constructed as shown on the architectural, landscaping, and engineering plans (Exhibits C.1-C.7), this criterion is met for each of the three requested modifications.

B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standards for which a modification is requested.

Findings: The purpose of 33.120.240 Required Outdoor and Common Areas is follows

Purpose. The required outdoor and common area standards ensure opportunities for outdoor relaxation or recreation. The standards work with the building coverage and minimum landscaped areas standards to ensure that some of the land not covered by buildings is of adequate size, shape, and location to be usable for outdoor recreation or relaxation. The standards also ensure that outdoor areas are located so that residents have convenient access. Required outdoor areas are an important aspect in addressing the livability of a residential property by providing outdoor living opportunities, some options for outdoor privacy, and a healthy environment. These standards also allow for common area requirements to be met by indoor community facilities because they also provide opportunities for recreation and gathering.

The applicant has provided the following findings for each of the three requested modifications:

The request to reduce the upper deck dimension by roughly 2.5 feet will not impact the overall size or location of the upper deck. The dimension reduction request is for roughly 2.5 feet. The deck is proposed to be approximately 17.5 feet wide and 43 feet long. These dimensions still allow for both active and passive

uses to occur with ample room for multiple simultaneous users. Providing this modification would not negatively impact useable open space availability.

The dimension reduction request for the lower common lawn is for roughly 10 feet in both directions and 220 square feet. However, by creating this lower common lawn the proposal will exceed outdoor open space requirements by 202 square feet. Allowing the dimensional reductions will essentially ensure an over provision of outdoor open space areas, improving the livability of both residents and visitors.

The request to exceed the distance from a main door by 25 feet – from 20 to 45 feet – is requested to allow the creation of a second common open space area on a main floor. Without impacting environmental resources, the creation of the lower lawn in this area will allow the development proposal to exceed outdoor open space requirements by 202 square feet. Allowing the locational modification will essentially ensure an over provision of outdoor open space areas, improving the livability of both residents and visitors.

The applicant has requested modifications to the size and location of common outdoor areas in order to add an additional outdoor area, providing additional opportunities for recreation and relaxation for building residents. The proposed outdoor areas will continue to be of adequate size and shape and will still be conveniently located to provide useable space for recreation and relaxation. Signage from interior lobby areas will be provided to guide residents to the lower common lawn, mitigating its distance from a main entrance. With the condition that this signage is provided, this criterion is met for each of the three requested modifications.

DEVELOPMENT STANDARDS

Development standards that are not relevant to the land division review, have not been addressed in the review. Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 11 can be met, and those of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

Maximum density – This is a split zoned site located in the R7 and RM2 zone. Each zoned portion of the site is 20,000 square feet in area. As explained in 33.270.100.M, a PD review allows density to be transferred within a split zoned site. In this case, the total number of dwelling units permitted is determined by calculating the number of dwelling units allowed in the R7 portion of the site and adding it to the number of dwelling units allowed in the RM2 portion of the site. The number of units allowed in the RM2 portion of the site is calculated in terms of floor area; the floor area calculation is converted to dwelling units at the rate of 1 dwelling unit per 1,000 square feet of floor area. The base floor area allowance in the RM2 portion of the site is 1.5 to 1, or 30 units. However, the applicant is providing inclusionary housing and obtained a floor area bonus of .75 to 1. The total floor area permitted in the RM2 portion of the site is 2.25 to 1, or 45 units. The total number of dwelling units allowed on this site is 47 unit. The applicant is proposing a total of 43 units, meeting the maximum permitted density on this site.

OTHER TECHNICAL REQUIREMENTS

Technical decisions have been made as part of this review process. These decisions have been made based on other City Titles, adopted technical manuals, and the technical expertise of appropriate service agencies. These related technical decisions are not considered land use actions. If future technical decisions result in changes that bring the project out of conformance with this land use decision, a new land use review may be required. The following is a summary of technical service standards applicable to this preliminary partition proposal.

Bureau	Code Authority and Topic
Development Services/503-823-7300 www.portlandonline.com/bds	Title 24 – Building Code, Flood plain Title 10 – Erosion Control, Site Development Administrative Rules for Private Rights-of-Way
Environmental Services/503-823-7740 www.portlandonline.com/bes	Title 17 – Sewer Improvements 2008 Stormwater Management Manual
Fire Bureau/503-823-3700 www.portlandonline.com/fire	Title 31 Policy B-1 – Emergency Access
Transportation/503-823-5185 www.portlandonline.com/transportation	Title 17 – Public Right-of-Way Improvements Transportation System Plan
Urban Forestry (Parks)/503-823-4489 www.portlandonline.com/parks	Title 11 – Trees
Water Bureau/503-823-7404 www.portlandonline.com/water	Title 21 – Water availability

As authorized in Section 33.800.070 of the Zoning Code conditions of approval related to these technical standards may have been included in the decision on this proposal.

CONCLUSIONS

The applicant has proposed amendments to prior Planned Development and Partition approvals to allow for an increase in the number of units from 27 to 43 units through the alteration of interior floor plans. The increased density is allowed under the current zoning on the site. The proposal will increase the number of bicycle parking spaces from 32 to 51 spaces.

The applicant has requested the following modifications through the Planned Development Amendment to address the increased outdoor area requirements from the additional units and thenew façade articulation requirements under the current zoning, :

- Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230.C.2);
- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.140.C.2.a);
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240.C.2.a);
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240.C.2.a).

As discussed in this report, the relevant standards and approval criteria associated with the Land Division Partition Amendment, Planned Development Amendment, and Modifications have been met, or can be met with conditions. Therefore, Staff finds this proposal should be approved.

TENTATIVE STAFF RECOMMENDATION

(May be revised upon receipt of new information at any time prior to the Hearings Officer decision)

Approval of an Amendment for Changes to an Approved Preliminary Plan; and

Approval of an Amendment to a Planned Development Review to increase the approved number of dwelling units in a multi-dwelling structure from 27 to 43 units; and

Approval of the following Planned Development Review Modifications:

1. Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230)

2. Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.240)
3. Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240)
4. Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240)

These approvals are illustrated with Exhibits C.1-C.7 and are subject to the following conditions:

- A. Development on Parcel 1 must be in substantial conformance with Exhibits C.1-C.4. As part of the building permit revision submittal, all required site plans, building elevations, landscape plans, and any additional drawings must reflect the information and designs approved by this land use review. The sheets on which this information appears must be labeled, "Proposal and design as approved in Case File #LU 22-187796 APD AP. No field changes allowed." Specifically, the permit revision must include the following elements:
 - Increased bike parking spaces
 - Added common outdoor areas, with accessible pathways and related directional signage
 - Additional landscaping as shown on Exhibit C.3
- B. Conditions C.1 – C.6 of LU 19-258445 continue to apply to the site, with the exception that the design approved through the PD is updated to that approved under this review as stated in Condition A above.

Procedural Information. The application for this land use review was submitted on September 28, 2022 and was determined to be complete on November 7, 2022.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on September 28, 2022.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period. Unless further extended by the applicant, **the 120 days will expire on: March 7, 2023.**

Some of the information contained in this report was provided by the applicant.

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the recommendation of the Bureau of Development Services with input from other City and public agencies.

Conditions of Approval. If approved, this project may be subject to a number of specific conditions, listed above. Compliance with the applicable conditions of approval must be documented in all related permit applications. Plans and drawings submitted during the permitting process must illustrate how applicable conditions of approval are met. Any project elements that are specifically required by conditions of approval must be shown on the plans, and labeled as such.

These conditions of approval run with the land, unless modified by future land use reviews. As used in the conditions, the term “applicant” includes the applicant for this land use review, any person undertaking development pursuant to this land use review, the proprietor of the use or development approved by this land use review, and the current owner and future owners of the property subject to this land use review.

This report is not a decision. The review body for this proposal is the Hearings Officer who will make the decision on this case. This report is a recommendation to the Hearings Officer by the Bureau of Development Services. The review body may adopt, modify, or reject this recommendation. The Hearings Officer will make a decision about this proposal within 17 days of the close of the record. Your comments to the Hearings Officer can be mailed c/o the Hearings Officer, 1900 SW Fourth Ave., Suite 3100, Portland, OR 97201 or faxed to 503-823-4347.

You will receive mailed notice of the decision if you write a letter received before the hearing or testify at the hearing, or if you are the property owner or applicant. This Staff Report will be posted on the Bureau of Development Services website at <http://www.portlandoregon.gov/bds/35625>. Land use review notices are listed on the website by the District Coalition in which the site is located; the District Coalition for this site is identified at the beginning of this staff report. You may review the file on this case at the Development Services Building at 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201.

Appeal of the decision. The decision of the Hearings Officer may be appealed to City Council, who will hold a public hearing. If you or anyone else appeals the decision of the Hearings Officer, only evidence previously presented to the Hearings Officer will be considered by the City Council.

Who can appeal: You may appeal the decision only if you write a letter which is received before the close of the record for the hearing, if you testify at the hearing, or if you are the property owner/applicant. Appeals must be filed within 14 days of the decision. **Appeals must be filed within 14 days of the decision. An appeal fee of \$3,398.00 will be charged (one-half of the BDS LUS application fee, up to a maximum of \$5,000).**

Appeal Fee Waivers: Neighborhood associations recognized by the Office of Neighborhood Involvement may qualify for a waiver of the appeal fee provided that the association has standing to appeal. The appeal must contain the signature of the Chair person or other person authorized by the association, confirming the vote to appeal was done in accordance with the organization’s bylaws.

Neighborhood associations, who wish to qualify for a fee waiver, must complete the Type III Appeal Fee Waiver Request for Organizations Form and submit it prior to the appeal deadline. The Type III Appeal Fee Waiver Request for Organizations Form contains instructions on how to apply for a fee waiver, including the required vote to appeal.

For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this planned development review amendment approval. This approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun. If the approved project includes more than one component, such as multiple buildings, or multiple phases that will be broken into separate building permit applications, at least one permit must be obtained within 3 years of the date of this decision, and all permits must be obtained within ten years of the date of this decision. After that ten-year period, a new land use review will be required before permits will be issued for any remaining project components that have not yet been permitted, subject to the Zoning Code in effect at that time.

Applying for your permits. A building permit, occupancy permit, or development permit may be required before carrying out an approved project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed herein;
- All applicable development standards, unless specifically exempted as part of this land use review;
- All requirements of the building code; and
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the City.

Planner's Name: Leah Dawkins

Date: December 14, 2022

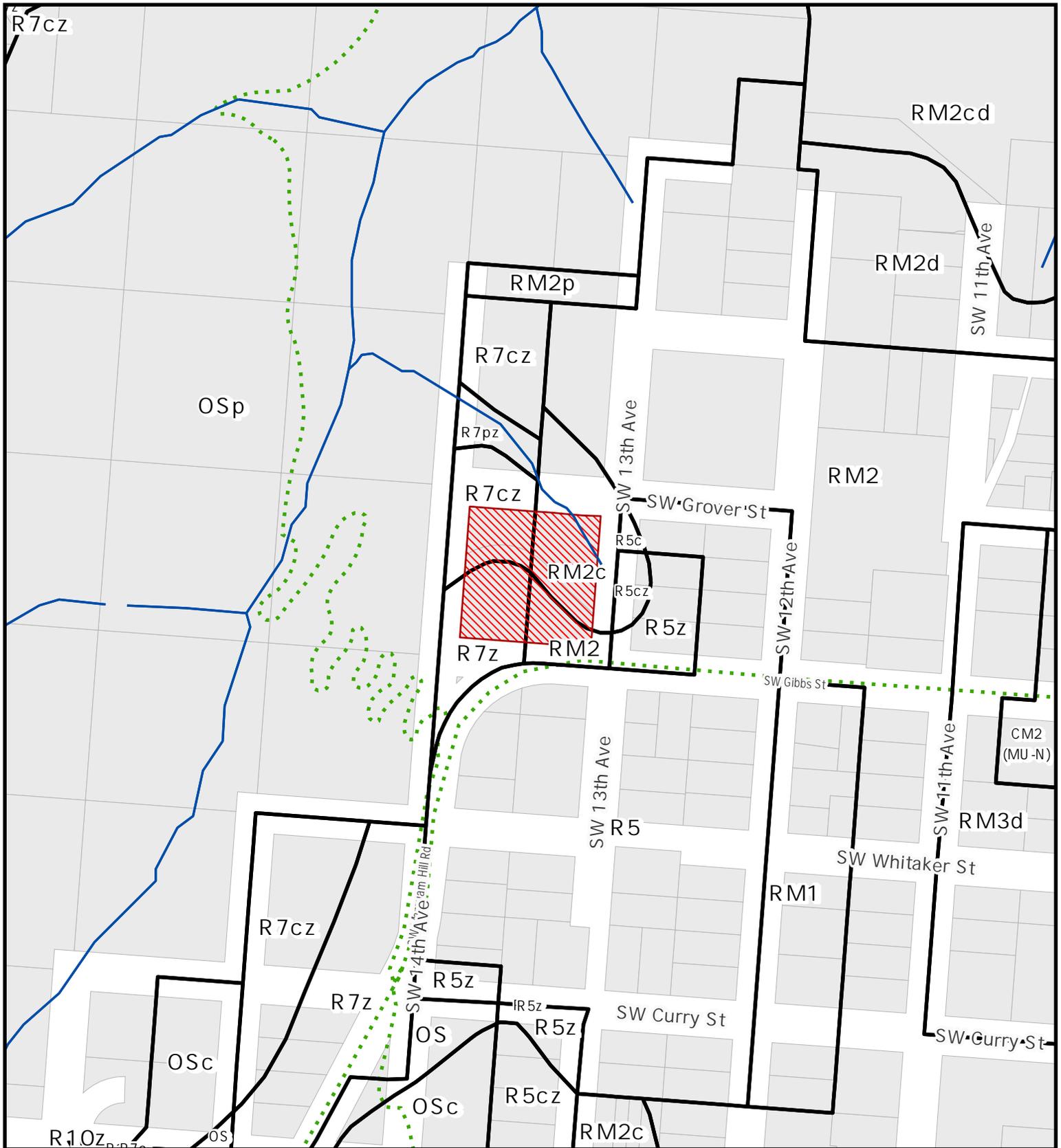
EXHIBITS

NOT ATTACHED UNLESS INDICATED

- A. Applicant's Statement:
 1. Applicant Narrative
 2. Resubmittal Response- 11/07/22
 3. Neighborhood Outreach Documentation
 4. Stormwater Management Report
 5. Transportation Impact Studies
 6. Arborist Report and Tree Protection Plan- July 2020
 7. Surrounding Neighborhood Photographs
 8. Inclusionary Housing Intake Form
 9. Inclusionary Housing Calculation Sheet
 10. Common Area Furniture Product Sheet
 11. Common Area Context Site Photos and Grading
 12. BES Plan Confirmation
 13. Site Development Grading Plan Confirmation
 14. Site Development Completeness Email
 15. Vicinity Map
 16. Early Assistance Meeting Notes (EA 22-144899)
- B. Zoning Map (attached):
 1. Existing Zoning
 2. Proposed Zoning
- C. Plans & Drawings:
 1. Site Plan (attached)
 2. Lower Common Lawn Plan Enlarged (attached)
 3. Lower Lawn Landscaping Plan (attached)
 4. Façade Elevation Plan (attached)
 5. Land Use and Architectural Plan Set (15 sheets)
 6. Landscape Plan Set (5 sheets)
 7. Engineering Plan Set (12 sheets)
 8. Topographic Survey
- D. Notification information:
 1. Request for Response
 2. Posting Letter sent to applicant
 3. Notice to be Posted
 4. Revised Notice to be Posted

5. Applicant's statement certifying posting
 6. Mailing list
 7. Mailed Notice
 8. Revised Mailed Notice
- E. Agency Responses:
1. Bureau of Environmental Services
 2. Bureau of Transportation Engineering and Development Review
 3. Water Bureau
 4. Fire Bureau
 5. Site Development Review Section of Bureau of Development Services
 6. Bureau of Parks, Forestry Division
 7. Life Safety Section of Bureau of Development Services
- F. Letters:
1. Milt Jones, 12/06/22, Pedestrian infrastructure concerns
 2. Homestead Neighborhood Association, 12/08/22, Pedestrian infrastructure concerns
- G. Other:
1. Original LUR Application
 2. Incomplete Letter
- H.

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).



For Zoning Code in Effect July 1, 2022 - Sept. 30, 2022

ZONING F

- Site
- Stream
- Recreational Trails

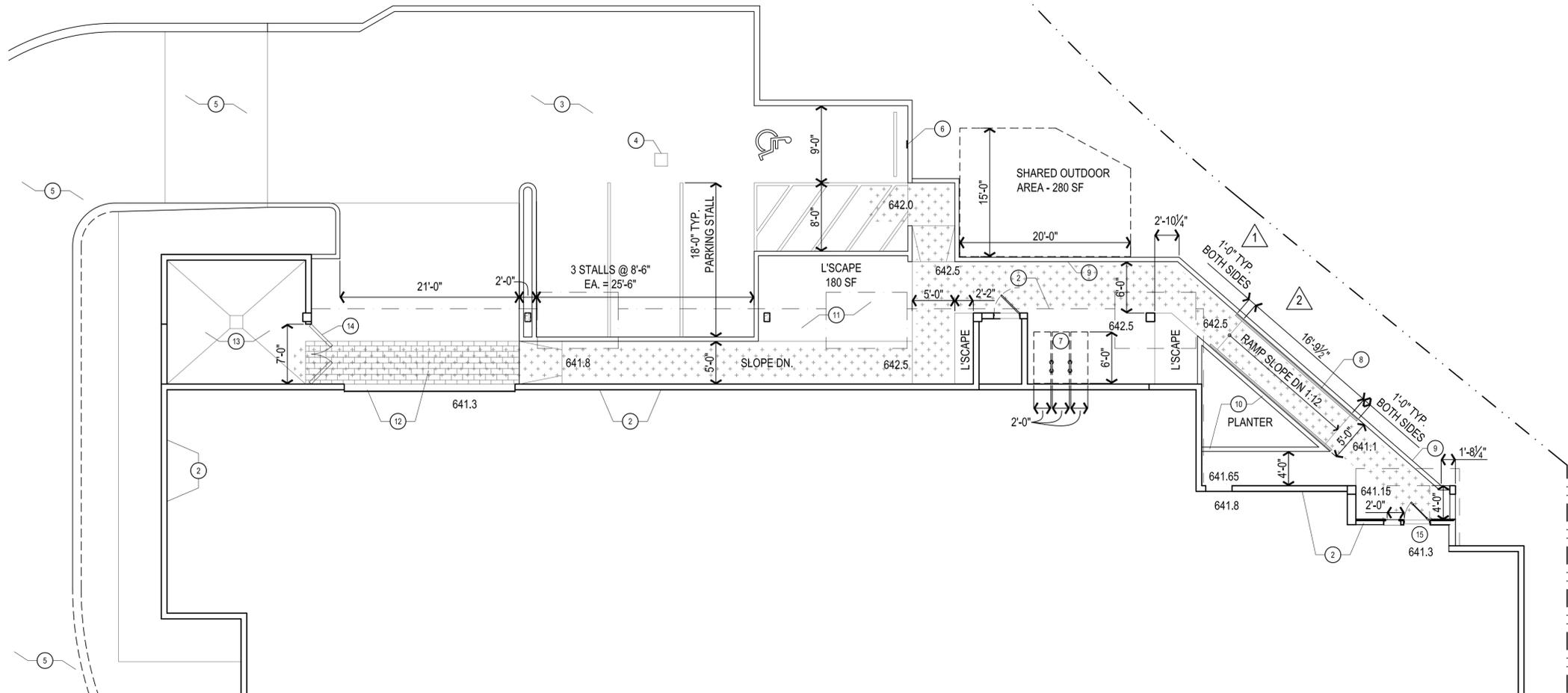
File No. LU 22 - 187796 APD, AP

1/4 Section 3327

Scale 1 inch = 200 feet

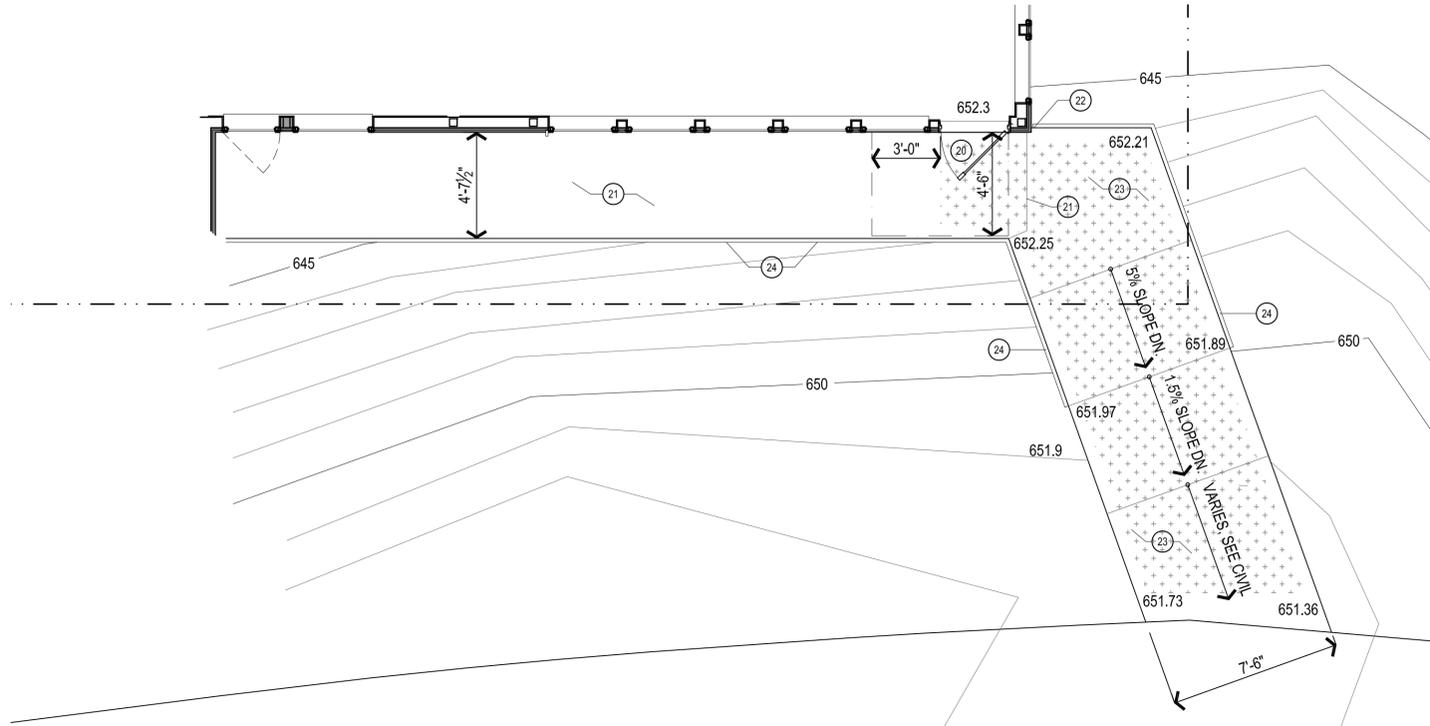
State ID 1S1E09BD 2801

Portland Hearings Office
 Case #42 Exhibit B Nov 10, 2022
 Bureau Case #LU 22-187796 APD AP
 Exhibit #4
 Page 24 of 28



1. ENLARGED PARKING LOT + PED ACCESS TO LOWER LOBBY
 SCALE: 1/4" = 1'-0" ON 22x34 1/8" = 1'-0" ON 11x17

- SHEET NOTES:**
- REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 - OUTLINE OF FIRST FLOOR BUILDING, SEE A2.12 PLAN
 - SURFACE PARKING LOT, SEE CIVIL DRAWINGS FOR SLOPES
 - PARKING LOT AREA DRAIN TO STORM WATER CONTAINMENT / TREATMENT, SEE CIVIL DRAWINGS
 - AUTO RAMP UP, SEE CIVIL DRAWINGS AND STRUCT. DRAWINGS
 - ACCESSIBLE PARKING SIGNAGE, SEE 2/A1.62
 - SHORT TERM BICYCLE PARKING, SEE 5/A1.62 - TYP. OF (3)
 - RAMP HANDRAIL, TYP. EA SIDE - SEE 3/A1.62
 - CONC. CURB, MIN. 6" ABOVE ADJACENT GRADE
 - TRIANGULAR CONC. PLANTER, CURB 6" ABOVE HI-SIDE OF RAMP - FLAT TOP FOR APPROX. 26" HEIGHT ABOVE LOW SIDE OF WALKWAY
 - SURFACE PARKING INTERNAL LANDSCAPE AREA, SEE LANDSCAPE DRAWINGS
 CALCS AS FOLLOWS:
 REQ. = 45 SF / STALL (x4) = 180 SF
 PROVIDED = 180 SF
 - TEXTURED CONCRETE PEDESTRIAN PATH TO CONTRAST ADJACENT PAVING - CROSSING AT STRUCTURED PARKING GARAGE ENTRY
 - ENCLOSED / COVERED TRASH AREA W/ HYDRONICALLY ISOLATED SLAB SLOPED MIN. 1/8" PER FT. TO DRAIN, SEE PLUMBING DRAWINGS
 - DOUBLE 42" WIDE GATES, SEE ELEVATIONS
 - LOWER LEVEL LOBBY ENTRY W/ LATCH SIDE APPROACH DOOR CLEARANCE, SEE 7/A1.11
 - 16 - 19. NOT USED
 - UPPER LEVEL LOBBY ENTRY W/ FORWARD APPROACH DOOR CLEARANCE, SEE 6/A1.11
 - LINE OF PT SLAB BELOW - EXPOSED PT SLAB SLOPED 1/4" PER FT. AWAY FROM BUILDING
 - ALIGN CONC. PEDESTRIAN PATH W/ CORNER OF CONC. WALLS
 - CONC. SLAB ON GRADE W/ TURNED DOWN EDGES, S.S.D. - SEE SPOT ELEVATIONS AND TOPO LINES PLUS CIVIL DRAWING C11 FOR MULTI-SLOPE AND CONNECTION POINT AND ELEVATION OF PATH TO ROAD IN PUBLIC RIGHT OF WAY
 - STEEL GUARDRAIL AT CONC. EDGE, SEE 3/A9.1 - TERMINATES AT END OF 5% SLOPE - HANDRAILS NOT ALLOWED IN ROW PER PBOT



2. ENLARGED PED ACCESS TO UPPER LOBBY
 SCALE: 3/32" = 1'-0" ON 22x34 3/64" = 1'-0" ON 11x17

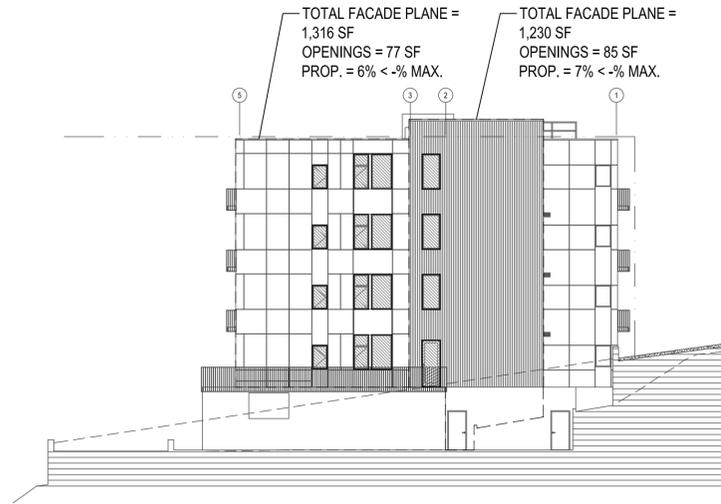
DENOTES PEDESTRIAN ACCESS PATH



RECEIVED 04/22/21

LAND USE SET
 09/01/2022

SHEET NOTES:
 1. REFER TO A1-SERIES FOR SYMBOLS, HATCHES, ABBREVIATIONS AND SCHEDULES. ALL ITEMS ARE NEW, U.O.N.
 2.

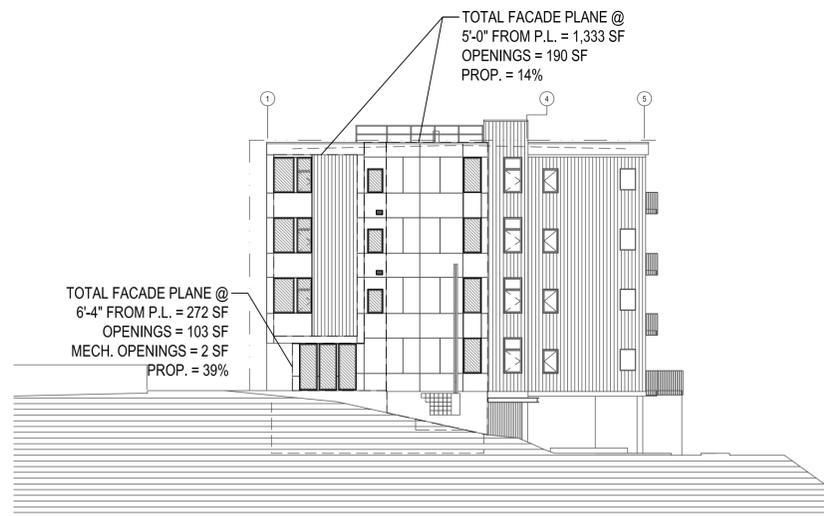


3. HEIGHT + EAST FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



1. HEIGHT + SOUTH FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17

TOTAL FACADE BOTH ZONES = 6,734 SF
 FACADE > 2 FT. OF PRIMARY PLANE = 1,728 SF (26%)
 TOTAL FACADE WITHIN RM2 = 4,556 SF
 FACADE > 2 FT. OF PRIMARY PLANE = 994 SF (22%)



4. HEIGHT + WEST FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



2. HEIGHT + NORTH FACADE PLANE
 SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



From: mjones.miltjones.com
To: [Hearings Office Clerks](#)
Subject: Case Number LU 22-187796 APD AP (Hearings Office Case #4220018)
Date: Monday, December 26, 2022 4:28:52 PM

Please accept these comments for the record in this case concerning development at 1311 SW Gibbs Street (also known as 1325 SW Gibbs). They are in addition to the comments I submitted on December 6.

It is the position of the Portland Bureau of Transportation (PBOT) in the Project Report that the road shoulder on SW Gibbs allows pedestrians there to walk safely and comfortably. I disagree.

I live in the neighborhood and walk this route fairly often. It is a primary access point for Portland's Trail #1 and pedestrian access from there to Council Crest Park. Walking on the shoulder of SW Gibbs in the vicinity of SW 13th Street is neither comfortable nor safe.

In recent days, it appears that local neighbors and the project developer have taken on the task of improving road shoulder conditions for pedestrians on the City right-of-way by removing encroaching blackberry vines and laying down some gravel for an improved path. Their efforts are to be applauded. But there is still more for the City to do.

The Hearings Officer should not take the PBOT representations of safety as true without examination. After all, despite the City's much advertised "Vision Zero" safety program, pedestrian fatalities in Portland have climbed to record levels this year. The Hearings Office should make a site visit to the SW Gibbs shoulder pathway near 13th Street to assess its safety condition personally; at night.

Why at night? Because the development in question here is targeted to provide "walk to work" housing for employees of the medical institutions just down the road on Marquam Hill. These are 7 days per week, 24 hours per day workplaces with employees coming and going at all hours of the day and night. The pedestrian walkway should be safe when the pedestrians use it.

When you visit the walkway at night, you should take a flashlight. And you should not try to walk on the shoulder side away from traffic. There is a street light on the opposite side of SW Gibbs that provides fair illumination of the pathway. But without a flashlight, and if you move over to avoid traffic, you may find yourself stumbling into the sinkhole/eroded shoulder embankment as you cross SW 13th Street and find yourself headed over the edge and toward the creek more than six feet below.

It is sort of a mystery to me why PBOT seems to be going to some lengths to avoid fixing the pedestrian safety issues here. The area in question is completely within the SW Gibbs and SW 13th Street rights-of-way on City property. The City has very publicly made pedestrian safety an issue of concern. Improving the pathway safety

does not look like it would take significant work. The lack of safety is a serious potential liability for the City. And the development projects in the nearby area (residences and medical institutions) have provided substantial development funds for public infrastructure improvements.

The City needs to be told to make this pathway safe.

Thank you for considering these comments.

Milton Jones
425 SW Bancroft
Portland, Oregon 97239

Self-INTRO

3 Points of this testimony:

1. Concern about PBOT Transportation Impact review process
2. Request PBOT enact changes in TIS review process and policy
3. Request specific allowance for developer in this case to use (or be credited) Transportation System Charges for off-site pedestrian safety improvements

1. PBOT Transportation Impact Review

Traffic Impact Study (TIS)

“Impacts” only involve vehicle trips generated

No estimation of pedestrian trips

No acknowledgement of pedestrian improvements needed and identified as projects on Portland’s SDC & TIP (project 90049.2) and SWIM (project BP-07)

TIS authors likely did not visit site on the ground

Examples:

1. (p.13 of 37): When addressing Pedestrian Safety: *“When sidewalks are not available along local streets, roadway speeds (posted & statutory speeds of 20 & 25 mph) and traffic volumes are generally lower allowing pedestrians the ability to safely and comfortably walk along roadway shoulders when necessary”*

NOTE: TIS contains no actual speed data to verify “lower speed”, the statement about “local” roads is not relevant for Gibbs which is classified as a collector, and “safely walking along shoulders” is a judgement statement simply not supported by actual conditions. --SHOW PICTURES

2. (p. 15 of 37): When addressing Connectivity: *“The project site is located within a neighborhood where the transportation grid system is reasonably complete, providing multiple redundant routes to/from the site via motor vehicle and other modes of travel.”*

NOTE: With the exception of 11th St., streets west of OHSU off of Gibbs are mostly dead-end. Hundreds of houses west of OHSU have no choice but to use Gibbs on every trip they make. This is not a “reasonably complete grid system” -- exemplifies lack of awareness of conditions.

PBOT Review of TIS

Makes no requirement to evaluate pedestrian impact

Apparently reviewed and approved TIS without any further evaluation or on-ground field review. (I have to think that if anyone had visited the site and looked at how anyone would walk from the site to anywhere else, they surely would have noticed uncomfortable and unsafe conditions)

There is apparently no consideration of the use of Transportation System Development Charges (TSDC) for partial improvements. NOTE: Again, a good field review might have determined that only a short distance of shoulder widening and improvement would have been beneficial – and not an entire 2 block sidewalk as was costed in the TIP.

2. Request PBOT Director call for a multi-office PBOT review of best practices for Transportation Impact Statements in light of current goals of making climate friendly walkable neighborhoods and enhancing active transportation.

Improvements to process that should be considered:

Require trip generation to include estimated pedestrian and bicycle traffic. (These volumes could affect typical mitigation measures beyond just sidewalks, including crosswalk warrants, signal warrants, bicycle signal warrants, etc.)

Require reviewers to visit the proposed site on the ground and examine first-hand facilities and access issues within a reasonable area of interest around the site, looking at likely destinations to and from the proposed development site.

Require documentation of actual traffic volumes and speeds and include considerations within the evaluation of project impacts.

Require review of recent, planned or proposed system improvements within a reasonable impact area around project site.

Require (within the TIS) consideration of the use of Project Development Charges for needed

3. Finally, I request that PBOT meet with the Developer and the Neighborhood Association on the ground to discuss a short term improvement and the possibility of using Transportation System Development Charges to address improvements.

City of Portland

Bureau of Development Services

Staff Presentation to the Hearings Officer

LU 22-187796 APD AP (1311 SW Gibbs Street)

Summary of Proposal

Prior Approval:

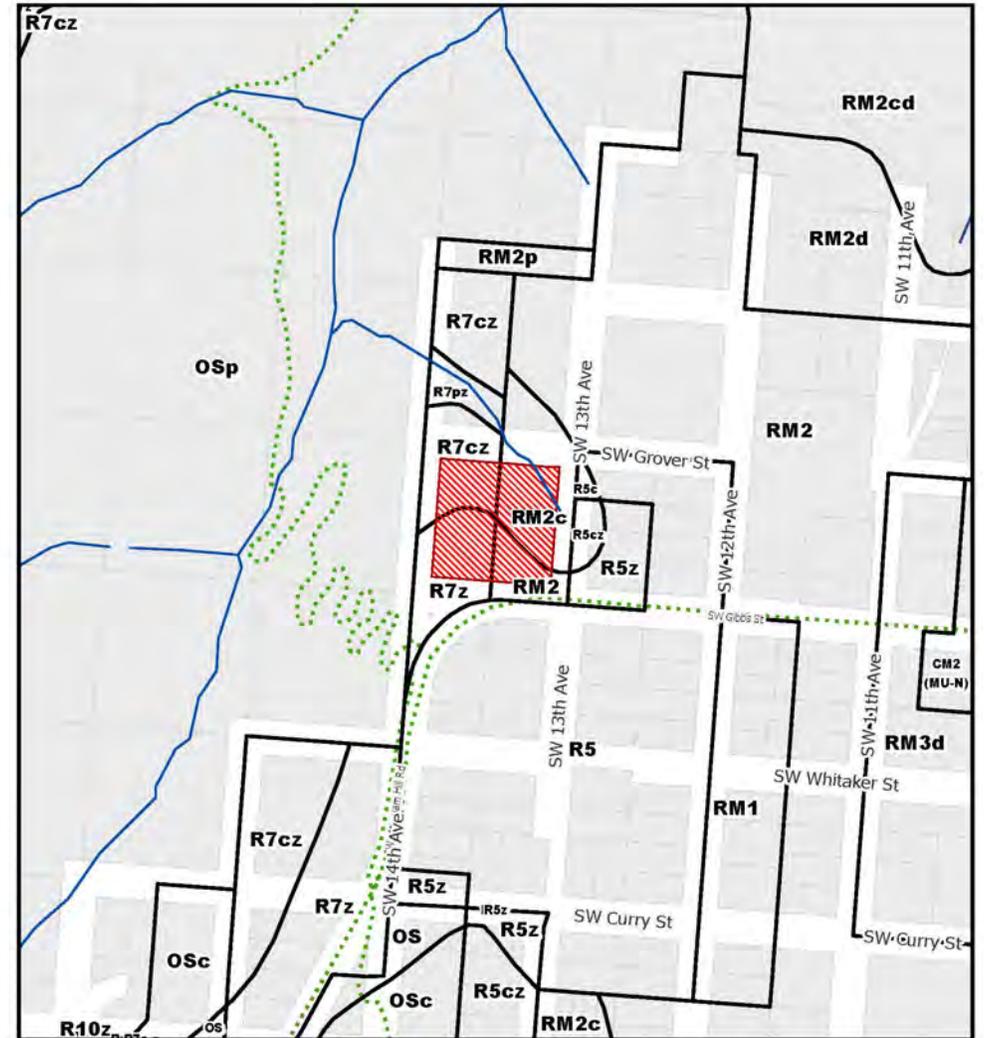
- LU 19-258445 PD LDP ENM: 2019 Land Use approval for a 5-story 27-unit multi-dwelling structure with 28 vehicle parking spaces and 30 long-term bike parking spaces through a Planned Development Review, a Land Division Partition, and Environmental Modifications

Current Proposal:

- Amendment to approved Planned Development and Land Division Partition to increase number of units from 27 to 43 through the alteration of interior floor plans. Increased density is permitted through the current zoning (RM2) on the site.
- Planned Development Review Modifications:
 - Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area
 - Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-foot by 20-foot ;
 - Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-foot by 10-foot and reduce the minimum area from 500 square feet to 280 square feet
 - Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet

Zoning Map

- Residential 7,000 (R7)
- Residential Multi-Dwelling 2 (RM2)
- Environmental Conservation (c) Overlay
- Potential Landslide Hazard



For Zoning Code in Effect July 1, 2022 - Sept. 30, 2022

ZONING ↑

- Site
- Stream
- Recreational Trails

File No. LU 22 - 187796 APD, AP
 3327
 1/4 Section
 Scale 1 inch = 200 feet
 State ID 1S1E09BD 2801

Portland Hearings Office
 Case #4220018 Exhibit B Nov 10, 2022
 Bureau Case #LU 22-187796 ADP AP
 Exhibit #7
 Page 3 of 24

Aerial Photo











800.350.6464

Hammer
Brodie

HANSON
HANSON
HANSON

Portland Hearing Office
Case #4220018
Bureau Case #14-22-10178-001-001
Exhibit #7
Page 10 of 10





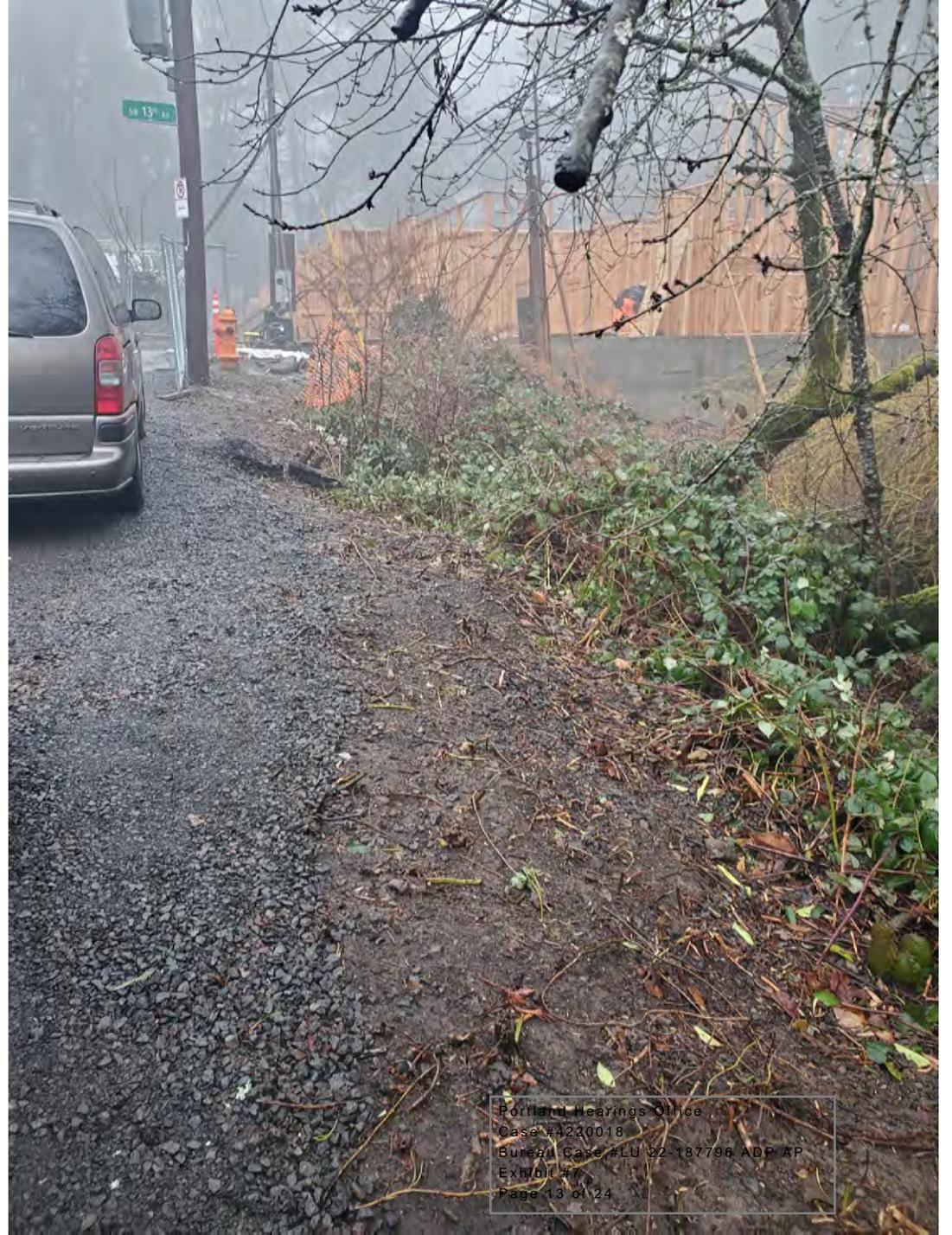
Builders & More
503-812-0175
www.buildersandmore.com

DBEC



SW Gibbs 3100 51

DEAD
END







SW 14TH AV

2 HOUR
VISITOR
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EXCEPT BY
ZONE C
PERMIT

SWTrails
1
← →

Approval Criteria

- **Relevant Approval Criteria:** In order to be approved, this proposal must comply with the approval criteria of *Title 33*. The relevant criteria are found in:
 - **33.660.320.B, Approval Criteria for Other Changes to an Approved Preliminary Plan;**
 - **33.854.520, Approval Criteria for Changes to an Approved Planned Development;**
 - **33.854.320, Approval Criteria for Modifications of Site-Related Development Standards**

Transportation Impacts 33.660.120.K

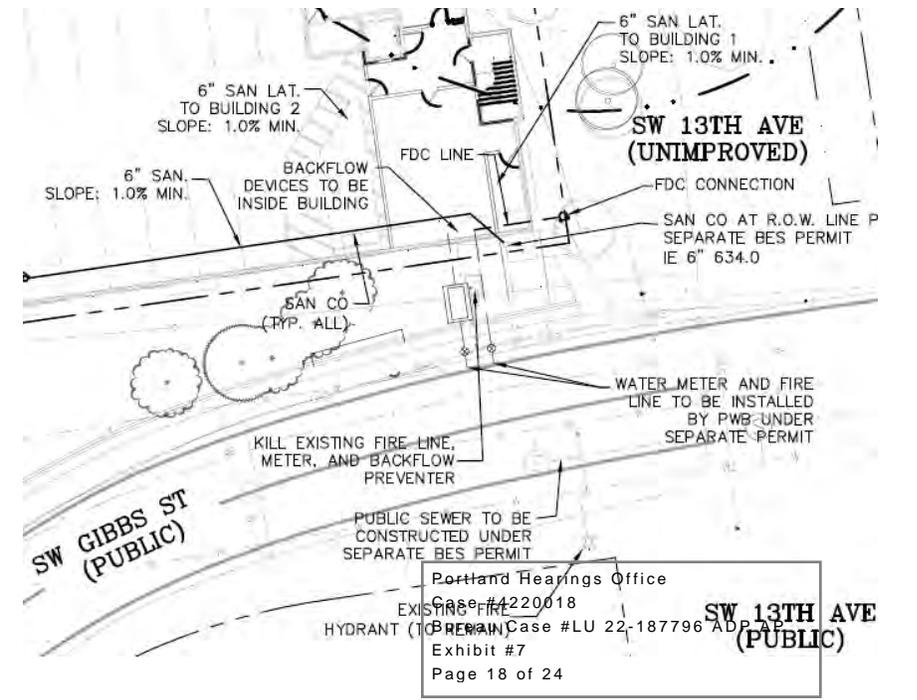
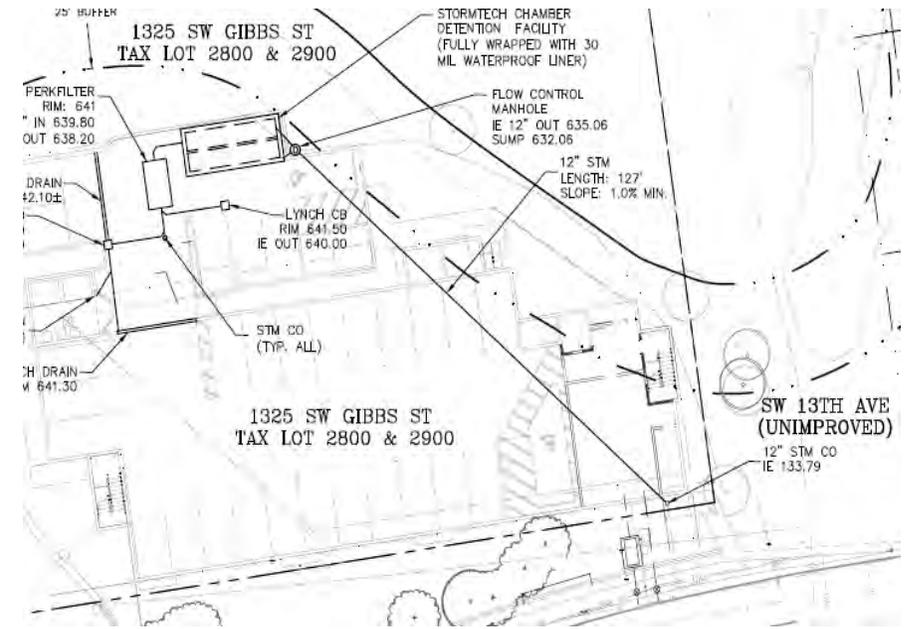
- Updated TIS submitted to address this criterion
- Transportation system capable of supporting proposed additional units
- Prior requirements from LU 19-258445 remain



Services and Utilities

33.660.120.L

- **Water**
 - Water available in SW Gibbs
- **Sanitary Sewer**
 - Extend combo main in SW Gibbs
- **Stormwater Management**
 - Private: runoff discharged to combo main in SW Gibbs
 - Public: Special Circumstances approved
- **Services remain unchanged from prior review**



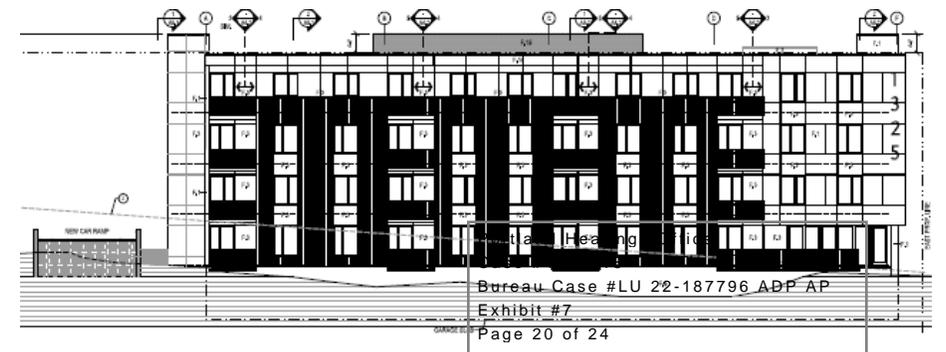
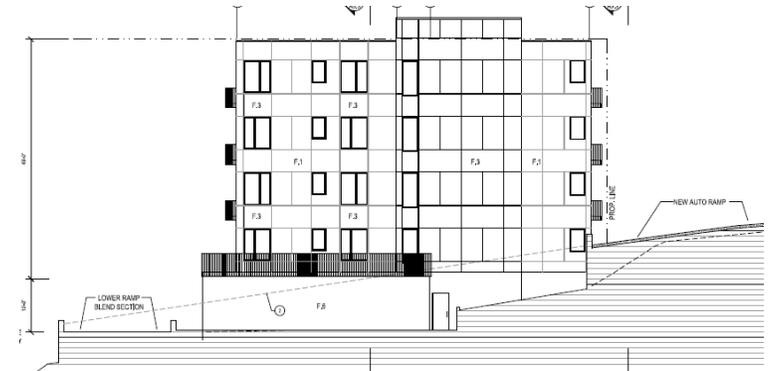
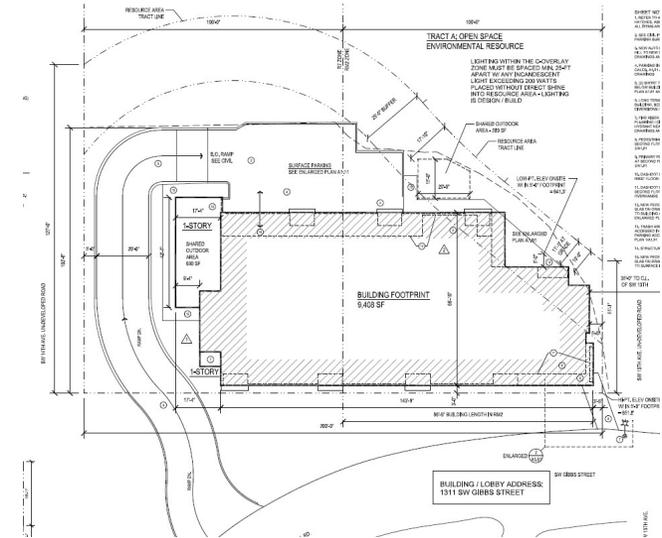
Rights-of-Way 33.654

- Existing ROW's (SW 13th, SW Grover, SW 14th) meet spacing standards but impractical to improve
- SW Gibbs:
 - Min 6-ft wide paved shoulder
 - Remove on-street parking
 - Replace guardrail



Amendment to the Planned Development 33.854.520

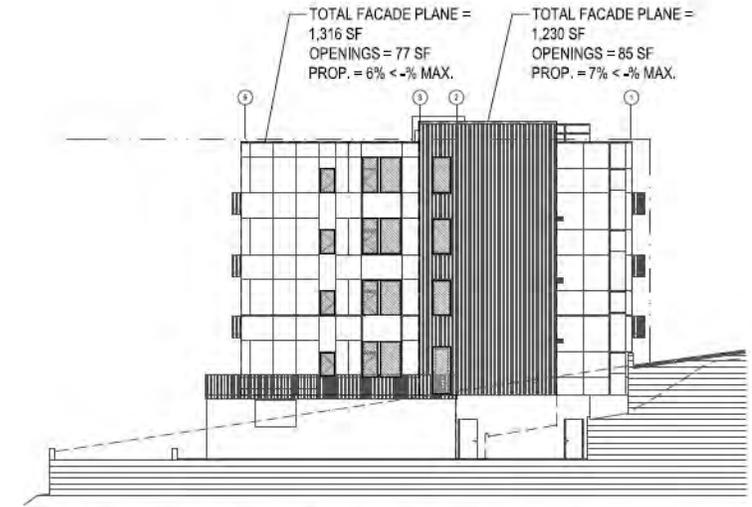
- Planned Development for multi-dwelling structure in RF-R2.5 zone (5-story, 43-unit apartment)
 - Building oriented to public realm; parking/storage minimized
 - EN Tract continues to preserve natural features
 - Incorporates positive characteristics of surrounding development
 - Similar scale as nearby apartments
 - Mitigation for differences in appearance via topography, natural areas, & landscaping
 - Potential negative effects adequately minimized. Added trips from additional units not expected to alter the safety of existing transportation facilities.
 - New outdoor area added
 - New connections through the site added
 - Garbage and recycling area unchanged



Modifications to the Planned Development 33.854.320

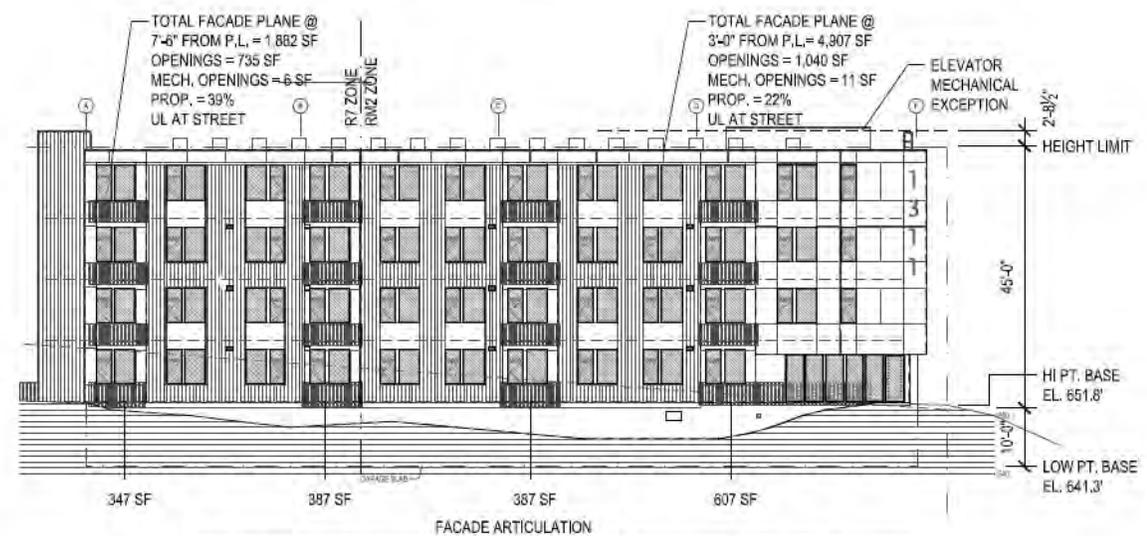
Façade Articulation

- **Modification to reduce façade articulation from 25% to 22% in RM2 portion of site.**
 - Triggered by change in zoning code
 - Façade unchanged from prior review
 - Meets PD approval criteria
 - Meets purpose of façade standard by limiting building bulk close to the street and breaking up façade into smaller areas.
 - Overall façade articulation is 26%



3. HEIGHT + EAST FAÇADE PLANE

SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17



TOTAL FAÇADE BOTH ZONES = 6,734 SF
 FAÇADE > 2 FT, OF PRIMARY PLANE = 1,728 SF (26%)
 TOTAL FAÇADE WITHIN RM2 = 4,556 SF
 FAÇADE > 2 FT, OF PRIMARY PLANE = 894 SF (22%)

1. HEIGHT + SOUTH FAÇADE PLANE

SCALE: 1/16" = 1'-0" ON 22x34 1/32" = 1'-0" ON 11x17

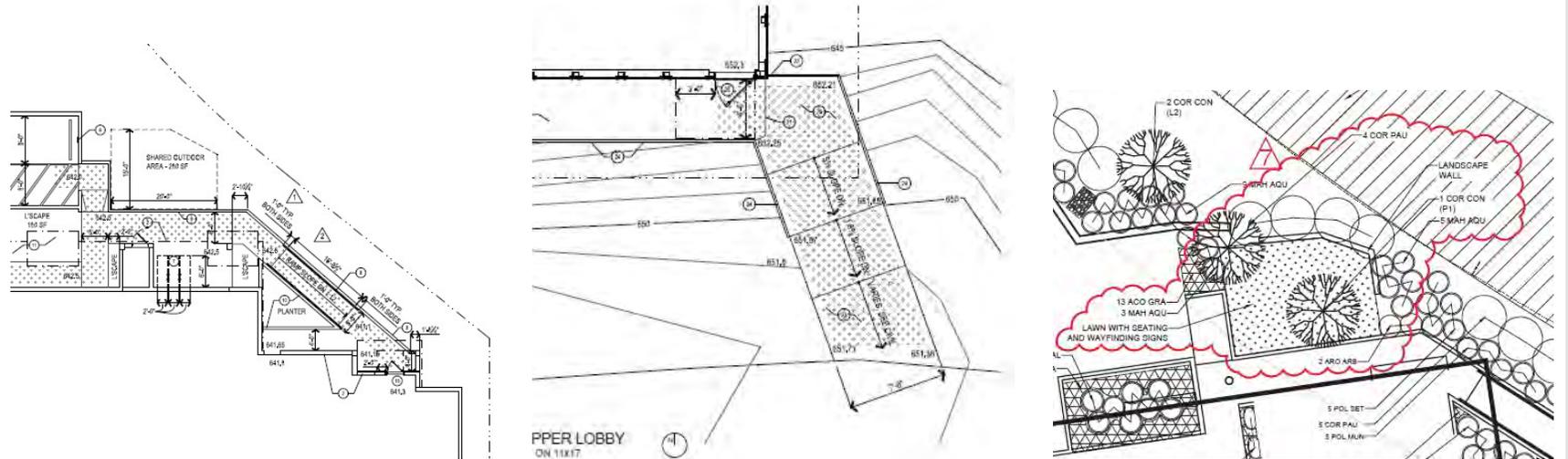
Portland Hearings Office
 Case #4220018
 Bureau Case #LU 22-187796 ADP AP
 Exhibit #7
 Page 21 of 24

Modifications to the Planned Development 33.854.320

Outdoor and Common Area

- Modifications to reduce area of upper common deck, reduce area of lower common lawn, and increase distance between lower common area and building entrance.

- Upper common area unchanged from prior review, mod. triggered by zoning code change
- New lower common area provided to accommodate additional units
- Modifications meet purpose of the standard by providing opportunities for outdoor relaxation and passive recreation
- Reduction in sizes will not impact overall usefulness of outdoor areas
- Increase in distance from entry will be mitigated through signage and additional site connectivity.



Neighborhood Review

Neighborhood Comments

- Continued concern about lack of pedestrian and bicycle improvements within the public right-of-way at the intersection of SW Gibbs and 13th Avenue immediately south of the site
- General lack of pedestrian infrastructure within the neighborhood

Staff Response

- While staff recognizes the concerns of the neighborhood, staff found that within the context of the proposal to add units to the building under construction, off-site improvements to the adjacent intersection are not warranted by the impacts of this development



Staff Recommendation

Approval of an Amendment for Changes to an Approved Preliminary Plan;

Approval of an Amendment to the Planned Development Review to increase the approved number of dwelling units in a multi-dwelling structure from 27 to 43 units; and

Approval of the following Planned Development Review Modifications:

- Reduce the area of street facing façade articulation from 25% to 22% of the total street facing surface area (33.120.230)
- Reduce the required common outdoor area dimensions for an upper common deck from a 20-foot by 20-foot square to dimensions of 17.5-feet by 20-feet (33.120.240)
- Reduce the required common outdoor area dimensions for a new lower common outdoor area from a 20-foot by 20-foot square to dimensions of 11.5-feet by 10-feet and reduce the minimum area from 500 square feet to 280 square feet (33.120.240)
- Increase the maximum distance between an outdoor common area and a building entrance from 20 feet to 45 feet (33.120.240)



Portland Hearings Office
Case #4220018
Bureau Case #LU 22-187796 ADP AP
Exhibit #8
Page 1 of 4









Hearings Office

City of Portland

1900 SW 4th Avenue, Room 3100, Portland, OR 97201

www.portlandoregon.gov/hearings

phone: 503.823.7307

fax: 503.823.4347



RECORD CLOSING INFORMATION

LU CASE # 22-187796 ADP AP

Hearings Officer: William Guzman

HO CASE # 4220018

Hearing began at 2:02 p.m. on December 28, 2022, and closed at 3:30 p.m.

THE RECORD WAS CLOSED AT THAT TIME.

Hearing is continued to _____ a.m. p.m. on _____ because:

Record was held open until:

4:30 p.m. January 5, 2023 for new evidence from anyone

4:30 p.m. January 12, 2023 for rebuttal to information in record

4:30 p.m. January 19, 2023 for Applicant's final rebuttal

4:00 p.m. _____ for _____

X The record will close at 4:30 p.m. on the latest date shown. If, in the Applicant's final rebuttal, the Hearings Officer receives a request from the Applicant to close the record early, the Hearings Officer has discretion to grant or deny the request.

The applicant waived applicant's rights granted by ORS 197.763 (6)(e), if any, to an additional seven day time period to submit written rebuttal into the record.

Internal Use Only

From: [Ed Fischer](#)
To: edfischer8@gmail.com
Subject: FW: SW Gibbs: 11th to Marquam Hill Rd.
Date: Tuesday, January 3, 2023 11:02:18 PM
Attachments: [image001.png](#)

From: Ed Fischer <edfischer8@gmail.com>
Sent: Thursday, December 22, 2022 7:17 PM
To: 'Falbo, Nick' <Nick.Falbo@portlandoregon.gov>
Cc: 'Harrison, Michael' <harmicha@ohsu.edu>; 'Milt Jones' <mjones@miltjones.com>; 'Aaron Clemons' <clemonsa@ohsu.edu>; 'Sean O'Neill' <sean@scotiawsh.com>
Subject: RE: SW Gibbs: 11th to Marquam Hill Rd.

Nick,

Thank-you for that thorough and informative reply. I wish we could have thought to ask for this information when we first heard that a proposed development on Gibbs immediately west of (and adjacent to) SW 13th was planning to include about 17 or 18 new apartments without off-street parking. That is going to increase pedestrian traffic on Gibbs by a corresponding amount. The plan is for 43 apartment units with about 26 or 27 off-street parking spots.

Do you know whether PBOT ever evaluates the impacts (and corresponding needs) of pedestrian traffic from new developments, the way most agencies evaluate the impact of vehicle traffic generated by new developments? If not, shouldn't they be? And if so, why wasn't that looked at for the 1311 (aka 1325) SW Gibbs 43 unit apartment project? (On an aside, do you know if ITE or anyone has developed pedestrian trip-generation factors for apartments within close proximity to institutions?)

I would like to believe that had the PBOT reviewer visited this site in person and tried to walk (which will be the main form of transportation here) from this site to the nearest anything (bus-stop, shop, food, classes, hospital...) they would have noticed the lack of safe walking surface. The fact that this stretch of Gibbs had been identified on the System Development Charge list (project 90049.2) and listed in SWIM as a Tier 1 project should have raised a flag for someone during the review process. If ever there was a case for using some of the development charges paid by the developer on a project to address a safety issue that is exacerbated by the development, this surely is it. Especially when you note that the most severe "pinch-point" that needs attention is within the public right-of-way adjacent to the development (13th Avenue & Gibbs).

The developer has been very cooperative with the neighborhood association and has listened to and accommodated our concerns. They have been quite willing to provide some improvement to the walking path but to correct the worst spot to an acceptable standard will be expensive and it seems unfair that they should have to pay for all of that in addition to paying many thousands of dollars in development fees. It seems to me PBOT missed an opportunity to get a valuable safety project completed more efficiently and in a time when it is needed by not checking into this situation earlier and with an eye beyond just the frontage of the project. It would be nice to see if there was a better

way to assure future developments are getting the kind of comprehensive review they deserve. (If I could find Chris Warner's email address I would have included him with this note.)

Thanks again, Nick, for your reply. Stay safe.

---Ed

From: Falbo, Nick <Nick.Falbo@portlandoregon.gov>
Sent: Thursday, December 22, 2022 3:38 PM
To: Ed Fischer <edfischer8@gmail.com>
Cc: Harrison, Michael <harmicha@ohsu.edu>; Milt Jones <mjones@miltjones.com>; 'Aaron Clemons' <clemonsa@ohsu.edu>; 'Sean O'Neill' <sean@scotiawsh.com>
Subject: RE: SW Gibbs: 11th to Marquam Hill Rd.

Hi Ed,

thanks for the call and for reaching out. I'm happy to provide some planning context for you on this block, perhaps it can be helpful in your advocacy.

Guiding Southwest in Motion Recommendations is our [Transportation System Plan](#) street classification system, which designates different types of desired user accommodations and priorities. this section of SW Gibbs St has the following classifications:

Pedestrian classification: Neighborhood Walkway

Bicycle classification: City Bikeway

Transit classification: Local Service Transit Street

Freight classification: Local Service Truck Street

Design classification: Community Corridor

Emergency classification: Major Emergency Response

Traffic classification: Neighborhood Collector Street

I've highlighted the classifications that are most notable on this block, where some higher level of service should be provided or maintained. SW Gibbs St is *not* a local street, it is classified as neighborhood collector street. On neighborhood collectors we would prefer to see some sort of separated walkway provided.

The need for a sidewalk was identified in [PedPDX](#) our citywide pedestrian master plan. A project to build sidewalks is identified in Southwest in Motion as BP-07, and also in the TSP as project no. 90049.2. As you noted, this project is eligible for use of SDC funds, but those funds alone are not enough to construct the project.

I see two relevant [traffic counts](#) in our system. Travel pattern changes since COVID may influence how accurate these numbers are for understanding traffic today.

- 2018 count up the hill on SW Marquam Hill Rd saw an Average Daily Traffic(ADT) of 3,253 motor vehicles
- 2014 count on SW Gibbs east of 10th saw an Average Daily Traffic(ADT) of 4,006 motor vehicles

Neither of these are a perfect match for the segment in question, but they do indicate a traffic volume of between 3 and 4,000 vehicles per day. This is definitely in the range of traffic where we'd like to see a separated walkway to support pedestrian travel.

I hope this information is helpful in your work.

It's worth noting that there are many reasons a developer may not be required to build out the full standard frontage, and while I don't know the details I suspect there are some critical issues at play on this site that make construction infeasible.

-Nick

.....

Nick Falbo | Senior Transportation Planner

Pronouns: He/Him

Portland Bureau of Transportation

1120 SW 5th Avenue, Suite 1301

Portland, OR 97204

Phone: 503.823.6152

nick.falbo@portlandoregon.gov

<https://www.portland.gov/transportation>

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From: Ed Fischer <edfischer8@gmail.com>

Sent: Tuesday, December 20, 2022 10:26 AM

To: Falbo, Nick <Nick.Falbo@portlandoregon.gov>

Cc: Harrison, Michael <harmicha@ohsu.edu>; Milt Jones <mjones@miltjones.com>; 'Aaron Clemons' <clemonsa@ohsu.edu>; 'Sean O'Neill' <sean@scotiawsh.com>

Subject: SW Gibbs: 11th to Marquam Hill Rd.

Hello Nick,

I left a phone message this morning. I wonder if you can help the Homestead Neighborhood address

a pedestrian safety issue on SW Gibbs related to a development under construction now that will result in residents of 43 new housing units to walk along a rather precarious stretch of SW Gibbs road. We believe the City is passing up an opportunity for a much needed improvement to pedestrian safety by not requiring or even allowing the developer to use PDC funds to provide a safe width & surface for pedestrians along Gibbs across the 13th Avenue right-of-way.

Do you have access to any traffic counts (pre-covid would be best) of Gibbs Street near 13th Avenue. AM & PM peak hour volumes would be best.

Was there any testimony, evidence, or analysis of conditions along Gibbs that contributed to the decision to add the Gibbs project to the SWIM projects list?

Do you have any suggestions on what might be good information to share at a public hearing on December 28th reviewing the application for an additional 16 units to the 1311 (aka 1325) SW Gibbs that might help persuade PBOT to change its position that no off-site improvements are necessary because... ***“Sidewalks are partially complete along nearby area roadways. Where sidewalks are not available along local streets, roadway speeds and traffic volumes are generally lower, allowing pedestrians the ability to safely and comfortably walk along roadway shoulders when necessary.”***

Is Gibbs considered a “Local” street? It seems like it should be classed as a collector. It has a high volume of commuter traffic being the only way to access OHSU and Marquam Hill institutions from the near west (via Fairmount, Patton, Hillsdale, etc.).

Any information or suggestions would be appreciated. Thank-you.

Ed Fischer, Chair
Homestead NA

Memorandum



To: William Guzman, Hearings Officer for City of Portland
Leah Dawkins, City Planner II, City of Portland Bureau of
Development Services

From: Sean O'Neill, Vice President, Scotia Western States
Housing, LLC
Jesse Winterowd, AICP, PMP

Date: January 9, 2023

Re: **LU 22-187796 ADP AP (Hearings Office Case
Number 4220018)**

Dear Mr. William Guzman,

Thank you for your time and effort in leading the Type III Land Use hearing for the amendment to our approved planned development (PD).

Prior to responding to the neighborhood's concerns, I would like to recap a couple of facts about the case that are pertinent to the discussion that the neighborhood has brought up.

1) The original PD was required because of density reallocation. The original site was zoned half R1 and half R7. Together they allowed for 27 units. The original PD spread the 27 units across the entire site, and away from the sensitive environmental zones. The PD was not requested because of an increase to the number of units allowed under the zoning code. The density remained the same as planned by the City.

2) At our initial application submission, the Better Housing by Design (BHBD) changes to the zoning code were being codified, and as such we were not able to utilize the higher allowed unit count. If BHBD was in place at the time of the initial PD we would have had the current unit count and configuration from the beginning. Now that BHBD is in place, we can make this modification under this program.

3) This is the 3rd project we have worked on in the Homestead Neighborhood. Through our work we have learned that the market demands for 1-bedroom and studio units are higher than they are for 2- and 3-bedroom units. The overall purpose of our change is to modify the unit types so that they better meet market demands.

As mentioned in the hearing, we are not changing the square footage of the building. In essence, we are taking larger apartment units and dividing them into smaller units. More specifically, we

Winterbrook Planning
610 SW Alder St., Suite 810
Portland, OR 97205
503.827.4422 ■ 503.827.4350 (fax)
jesse@winterbrookplanning.com

Portland Hearings Office Case #4220018 Bureau Case #LU 22-187796 ADP AP Exhibit #11 Page 1 of 2

are taking (16) 2-and 3-bedroom units and reconfiguring them into 1-bedroom and studio units. The total unit count will rise from 27 to 43, but the number of bedrooms will reduce by four.

We do not believe there is any merit to the argument that the additional units will aggravate any of the existing pedestrian or traffic conditions . Therefore, the previously approved building footprint, elevations and dimensions (which is well under construction) is proposed to remain unchanged. This fact must be front and center.

While it may seem like relations between us and the neighborhood are contentious since they have spoken strongly at meetings, this is far from the case. I think it is very important to reiterate that in Ed Fischer's opening statement he stated that he did not want our request to be denied and that he wanted to see it move forward. As we mentioned in the hearing, we have voluntarily agreed to facilitate the improvements of an approximate 6' to 8' wide gravel pathway left in place from the eastern edge of our property for approximately 60' to the east of our property (it is mostly installed at this point, but there will be some slight modifications to the path with additional gravel and flattening). We are making this path in good faith to address neighborhood concerns, and we believe this will benefit to our project and future residents as well.

To conclude, the additional 16 units will not affect the pedestrian system, particularly from our development to the major employment center of OHSU. Further, the proposed gravel shoulder markedly improves pedestrian safety along SW Gibbs St. over the previous conditions prior to our development. We view our development as a net positive for the neighborhood and we understand that neighborhood concerns are not directed solely at our project, but rather expressed frustrations are focused on the systemic practices and policies of the City.

Thank You,

Sean O'Neill