

Development Services

From Concept to Construction

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More Contact Info (<http://www.portlandoregon.gov/bds/article/519984>)



APPEAL SUMMARY

Status: Mixed Decision. Items 1a and 1b: Decision Rendered. Item 2: Hold for Additional Information - Held over from ID 20745 (8/14/19) for additional information

Appeal ID: 20798	Project Address: 3547-3561 SE Milwaukie Ave
Hearing Date: 8/28/19	Appellant Name: Chris Chilberg
Case No.: B-008	Appellant Phone: 5036384245
Appeal Type: Building	Plans Examiner/Inspector: Stephanie Hansen, Peter Drake, Steve Mortensen
Project Type: commercial	Stories: 2 Occupancy: R-2 Construction Type: V-B
Building/Business Name: Mesa Properties AZ LLLP	Fire Sprinklers: No
Appeal Involves: Alteration of an existing structure, Reconsideration of appeal	LUR or Permit Application No.: 19-171148-CO
Plan Submitted Option: pdf [File 1] [File 2] [File 3]	Proposed use: Multi Family (8 unit apt)

APPEAL INFORMATION SHEET

Appeal item 1

Code Section	OSSC 1009.7.2
Requires	Riser height 7 inch maximum and rectangular tread depth 11 inch minimum.
Proposed Design	We propose the stairs to the upper landing will have a tread length of 11 inches, with a minimum of 10 inches horizontal dimension from nose to nose of tread. The risers shall not exceed 7-1/2 inches (the actual dimension will be slightly over 7" - 7.01 inches per riser). Each stairway shall have 17 risers (119-1/4" total height from the lower walkway to the balcony).
Reason for alternative	if the stair tread length is increased to 11" horizontal from nose to nose as required by the current code, the bottom of the stairs will extend beyond their current termination point, intruding into the egress pathways for the lower floor occupants. This would result in a major revision to the existing egress pathways for the lower floor. The existing stair construction was in a state of decay, and replacement was required. The original stairs for this building were as described above (we have provided a copy of the original stair design for reference). The intent is to match exactly the existing stair tread length and riser height. Replacing the stairs in a manner that matches the existing conditions maintains the current level of safety.
	Reconsideration Text
	See attached requested documents

Appeal item 2

Code Section	OSSC 1012.6 and OSSC 1012.1
Requires	Handrails and handrail extensions.
Proposed Design	We propose the original guardrail be re-used, without adding a new handrail (no handrail existed on the existing stairs). The existing guardrail and its attachments will be upgraded as required for structural purposes (to withstand code level loading requirements), as well as safety requirements (additional elements added to prevent the passage of a 4" diameter sphere).
Reason for alternative	<p>The addition of a hand rail will require a major revision to the existing egress pathways. Per current code requirements, the top of a handrail must be horizontal, and extend 12" beyond the top riser. Furthermore, a handrail must extend beyond the bottom riser by at least a tread length. This would result in revisions to the framing of the upper floor landings, mid height landing (at south stair), and the existing egress pathways for the lower floor. As noted previously, the intent is maintain or increase the level of safety, while repairing the degraded elements of the balcony. The proposed design significantly increases the level of safety (guardrail and attachments to meet current code standards).</p> <p>Reconsideration Text</p> <p>See attached requested documents</p>

APPEAL DECISION

1a. Existing stair tread length to remain: Granted as proposed.

1b. Existing stair riser height to remain: Granted as proposed.

2. Omission of handrail extensions with re-use of original handrail: Hold for additional information.

Appellant may contact John Butler (503 823-7339) with questions.

For Items 1a, 1b: The Administrative Appeal Board finds that the information submitted by the appellant demonstrates that the approved modifications or alternate methods are consistent with the intent of the code; do not lessen health, safety, accessibility, life, fire safety or structural requirements; and that special conditions unique to this project make strict application of those code sections impractical.

Pursuant to City Code Chapter 24.10, you may appeal this decision to the Building Code Board of Appeal within 90 calendar days of the date this decision is published. For information on the appeals process, go to www.portlandoregon.gov/bds/appealsinfo, call (503) 823-7300 or come in to the Development Services Center.

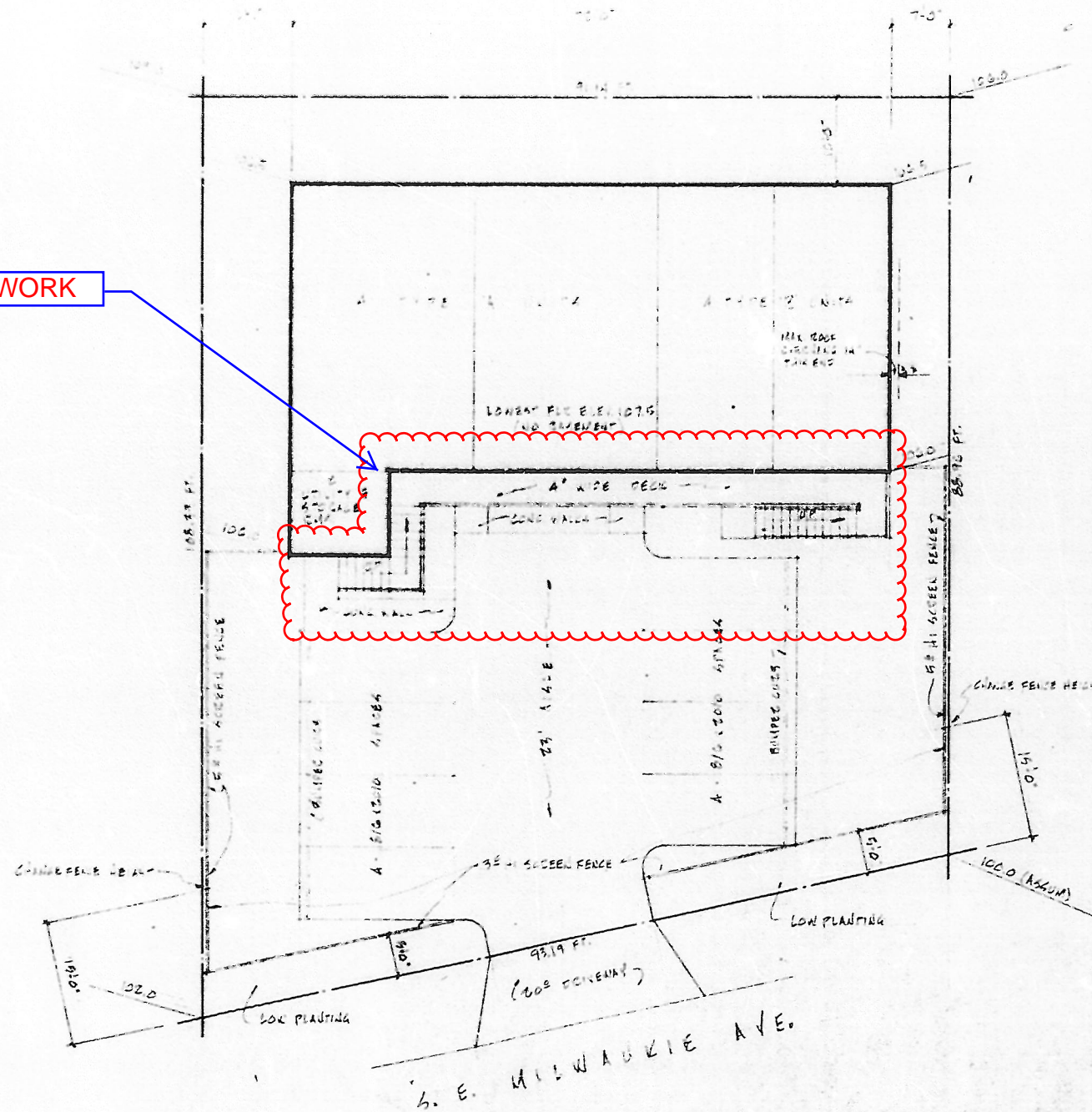
For Item 2: Note: Additional information is submitted as a no fee reconsideration, following the same submittal process and using the same appeals form as the original appeal. Indicate at the beginning of the appeal form that you are filing a reconsideration and include the original assigned Appeal ID number.

Include the original appeal language, with the new information in a separate paragraph clearly identified as "Reconsideration Text". No additional fee is required.

2620
11-3-76

Original Site Plan for Information Only

AREA OF WORK



UNIT APARTMENT DEVELOPMENT -
THE ROBERT CANDALL CO.
100.0' 100.0'

GENERAL DESCRIPTION -
A. TYPE A UNIT - 700 SQ. FT. 2 BEDROOM
A. TYPE B UNIT - 475 SQ. FT. 2 BEDROOM
C. STUDENT APARTMENTS 1/2 40 FT.
B. BIG 1000 SQUARE FEET

LEGAL DESCRIPTION -
LOTS 6 & 7 BLOCK 3
DUNEDIN ADDN
S. E. 1/4 SEC 11 T14 R1E W1/2
MAP NO. 5451

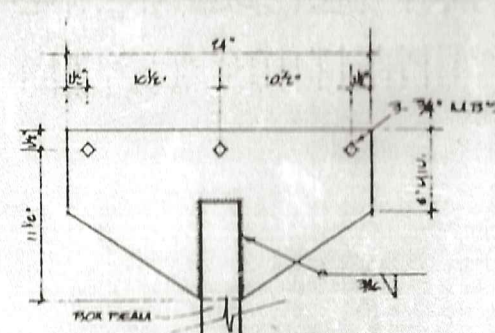
AREA -

AREA OF LAND	9000	40 FT
LOT COVERAGE	1908	40 FT 2070
AREA OF FACING	1902	40 FT 2790
AREA OF LANDSCAPING	2070	40 FT 4470

NORTH

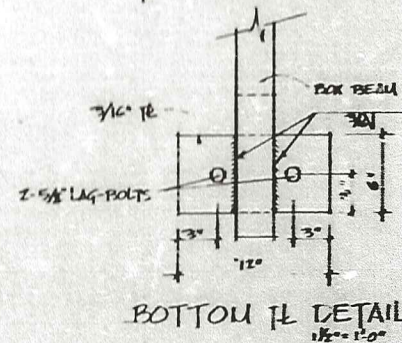
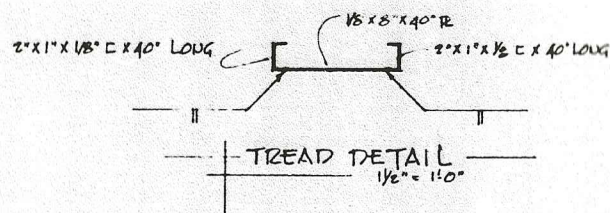
THE ROBERT CANDALL CO.	
OWNER THE ROBERT CANDALL CO.	DATE JULY 10, 1976
JOB SITE PLAN	AREA 100.0' 100.0'
DATE JULY 10, 1976	BY DALE B. HALLER

Original Site Plan for Information Only

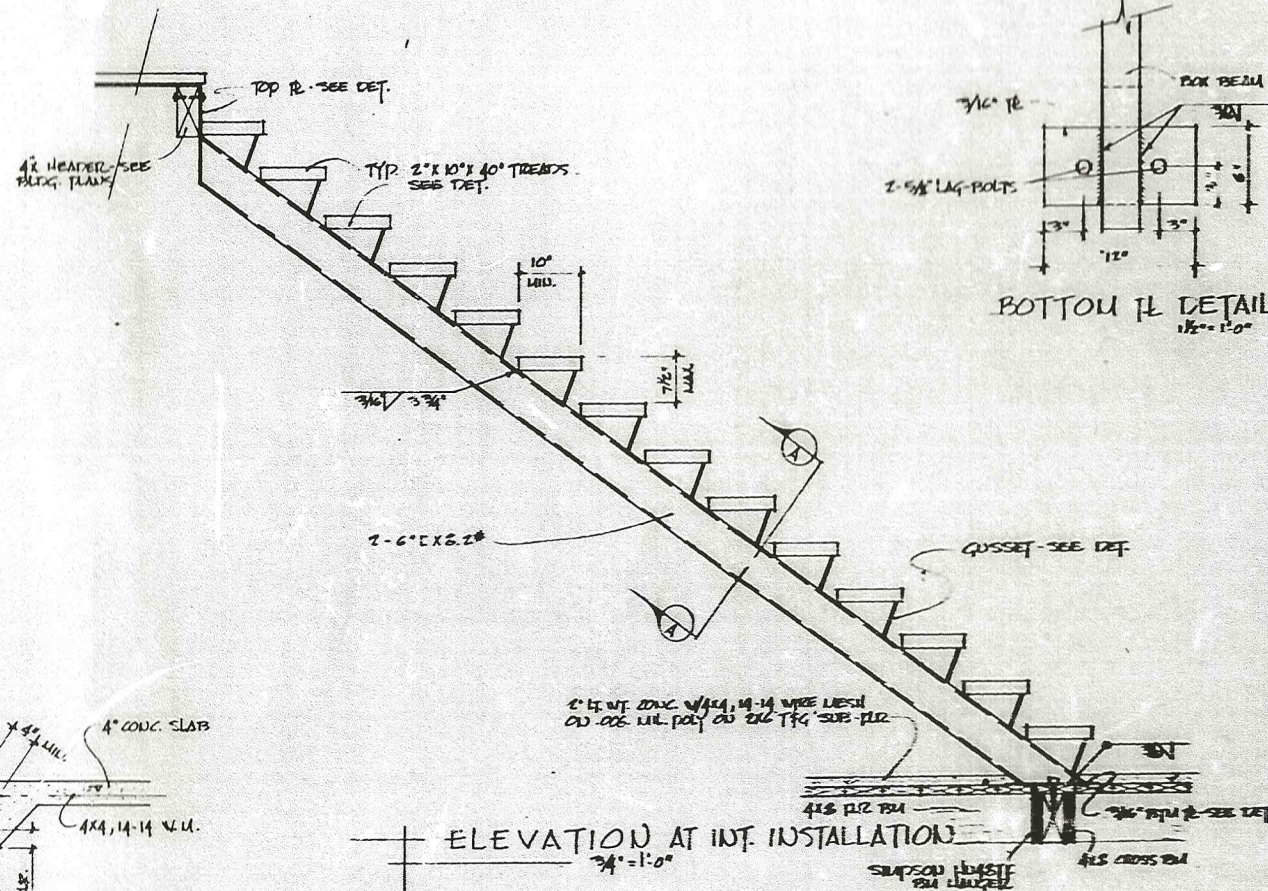


GUSSET DETAIL
1/2" = 1'-0"

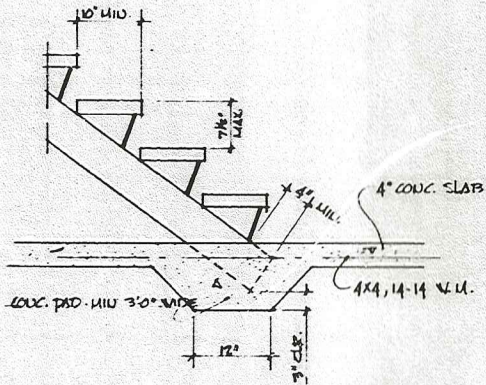
TOP DE DETAIL
1 1/2" = 110"




BOTTOM FL DETAIL
1/2" = 1'-0"



ELEVATION AT INT. INSTALLATION
34° = 1:0°



ELEVATION AT EXT. INSTALLATION
3/4" = 1' 0"

ZANDALL CONSTRUCTION CO	
PLANS BY DALE B. HALLER, R.E.	
JOB STAIR DECK & INT. ELEV. SUSPENSION	