



# City of Portland Design Commission

## Design Advice Request

### SUMMARY MEMO

**Date:** May 9, 2022  
**To:** Brian Varricchione, Mackenzie  
**From:** Benjamin Nielsen, BDS Design & Historic Review  
(503) 865-6519, benjamin.nielsen@portlandoregon.gov  
**Re:** EA 22-119814 DA – NE 7<sup>th</sup> Ave Apartments  
Design Advice Request Commission Summary Memo – April 21, 2022

Thank you for taking advantage of the opportunity to hold a Design Advice Request regarding your project. I hope you find it informative and valuable as you continue with your project development. Following, is a summary of the comments provided by the Design Commission at the April 21, 2022, Design Advice Request. This summary was generated from notes taken at the public meeting and a subsequent review of the public meeting recordings. To review those recordings, please visit: <https://efiles.portlandoregon.gov/Record/14964927/>.

These Design Commission comments are intended to guide you in further design exploration of your project. These comments may also inform City staff when giving guidance over the course of future related land use reviews. It should be understood that these comments address the project as presented on April 21, 2022. As the project design evolves, the comments, too, may evolve or may no longer be pertinent.

Design Advice Requests are not intended to substitute for other Code-required land use or legislative procedures. Please keep in mind that the formal Type III land use review process [which includes a land use review application, public notification and a Final Decision] must be followed once the Design Advice Request meetings are complete, if formal approval for specific elements of your project is desired.

Please continue to coordinate with me as you prepare your Type III Land Use Review Application.

*Encl:*  
Summary Memo

*Cc:* Design Commission  
Respondents

**Executive Summary.** Commissioners thought the proposed building was generally contextual, cohesive, and well thought out in its overall design while encouraging further development and revisions at the ground level, particularly with weather protection and the design and programming at the south elevation, to create a more engaging, integrated, and contextually-responsive public realm.

**Commissioners Present.** *Brian McCarter, Chandra Robinson, Sam Rodriguez, Zari Santner, and Don Vallaster.*

**Summary of Comments.** Following is a general summary of Commission comments by design tenet.

## CONTEXT

1. Overall Lloyd District context.
  - a. Commissioners agreed that the overall massing moves used on the building—shifting volumes rather than a pure extrusion of the site—respond well to the Lloyd District context.
  - b. However, some commissioners were also concerned that the building may be too similar to other buildings in the Lloyd District and pushed for some additional differentiation. The idea that came up most frequently was adding balconies (recessed rather than attached) to help the building look more residential in character as opposed to looking more like a commercial office building. When integrating balconies, consider one commissioner's caution that they could detract from the building's overall coherency if not done right.
2. Green Loop.
  - a. Commissioners agreed that ground floor uses along NE 7<sup>th</sup> need to engage the Green Loop visually if not physically. They were intrigued by the idea of having recreation areas along the ground floor of the Green Loop. They noted that it is very interesting to be able to see into the basketball court at the L.A. Fitness in the Pearl District, as an example, and that being able to see into the proposed recreation areas along NE 7<sup>th</sup> Ave would both have relevancy to, and help to activate, the Green Loop.
3. Commissioners agreed that the proposal was lacking in greenery and integrated landscaping and noting that buildings responding to the Green Loop should be green. One commissioner noted that "creative approaches would set a great example." Another suggested the idea of pursuing a continuous planted zone in the right-of-way instead of standard tree wells – revising right-of-way standards would necessitate extensive coordination with PBOT staff prior to any land use review submittal.

## PUBLIC REALM

1. Canopies and Weather Protection.
  - a. Commissioners agreed that canopies should be extended further over the sidewalk to provide useful weather protection.
  - b. While one commissioner said the fold-down canopy along the southern portion of the east elevation, wrapping around to the south elevation, is an interesting idea that is not seen much, she also raised concerns that the fold down fin may be a problem where it touches down and imposes on the sidewalk. Another commissioner said that the awnings at the north and south ends of the building are not speaking the same language, and that a more consistent design should be used.



- c. One commissioner noted that the canopy over the entry should be more prominent.
2. Commissioners also agreed that the inactive, blank wall area around the garage entrance should be reduced and made to be not as dominant an exterior form. One commissioner called it “too forbidding and pedestrian-unfriendly.” Another commented that the transition from glazing to “massive masonry” was too abrupt a change. Most commissioners believe that additional active use area should be proposed on at least one side of the garage entry, with one noting that the current proposal has inactive, blank wall along more than half the façade on the south elevation.
3. Commissioners discussed the proposed glazing at the pool and sport court areas along NE 7<sup>th</sup> Ave. Rather than obscuring visibility in this area with translucent film, the Commission suggested working with RACC to have artists create translucency (for reference, see the glazing in the installation at Tanner Springs Park). This could also be used to meet Guideline A5-3: *Incorporate Works of Art*.
4. One commissioner noted that the ground floor at the northeast corner of the building is relatively squat and that both the canopy and ground floor height should be taller. He also noted that there is a sweet spot for canopy coverage, with a height of approximately 10 feet and depth of 5 feet over the sidewalk providing the best weather protection.

## QUALITY & PERMANENCE

1. Commissioners generally agreed that the proposed design was cohesive in its middle and top portions but that there was less coherency at the building's base. One noted that it would be helpful to see a birds-eye view of the roof and how it is working in 3D. This is particularly important with regard to the penthouse “frame” on the west elevation, with which commissioners struggled somewhat at the end of the hearing.
2. Regarding materials, commissioners thought the materials proposed were generally of high quality.
  - a. Regarding the proposed Equitone, commissioners said that using concealed fasteners at the lower levels of the building would lead to the highest quality building, similar to how they are used at The Rodney in the Pearl District, though they were not sure as a group where to draw the line between where concealed fasteners are needed and where the material can switch to using the color-matched rivet fasteners at higher floors.
  - b. Commissioners challenged the architects to pay attention to the color of the masonry at the ground level to ensure that it helps to integrate into a cohesive design for the building. One noted the current color was jarring.
  - c. Commissioners generally believe the proposed vertical fins were a good addition to the design and that they help to articulate the façade and emphasize the building's verticality. Though acknowledging this, one commissioner questioned why horizontal sun shades were not proposed on the south elevation rather than vertical fins to provide appropriate sun shading on that elevation.
  - d. Commissioners noted that wood on the ground floor and other areas needs to be well protected from rain.
3. One commissioner also pointed out a discrepancy with the stem walls on the ground level and how in one portion of the building they stepped down with the grade, and in another they sloped following the grade and said that they should follow the same language. He also tied this comment to the canopies, noting that canopies on buildings on sloped sites generally step down with the grade.

## Modification

1. Commissioners said they believe the Modification to parking space width is supportable.

## Exhibit List

- A. Applicant's Submittals
  1. Original Drawing Set
  2. Revised Drawing Set, received and dated 03/24/2022
  3. Revised Drawing Set, received and dated 04/07/2022
- B. Zoning Map
- C. Drawings
  1. Title Page
  2. Contents + Project Team
  3. Project Description
  4. Zoning Summary
  5. Urban Context
  6. Existing Site + Photos
  7. Context Precedents
  8. Project Evolution
  9. Project Evolution
  10. Proposed Site Plan
  11. Floor Plans (P1 & Level 1)
  12. Floor Plans (Levels 2-14, Level Penthouse, Roof)
  13. Outdoor Amenities (Roof & Level 1)
  14. North + South Elevation
  15. West Elevation
  16. East Elevation
  17. Materials
  18. Massing at SE Corner (NE Irving St + NE 7<sup>th</sup> Ave)
  19. Massing with Site Context
  20. Modification
  21. Back Page
- D. Notification
  1. Mailing list
  2. Mailed notice
  3. Posting instructions sent to applicant
  4. Posting notice as sent to applicant
  5. Applicant's statement certifying posting
- E. Service Bureau Comments
  1. Bureau of Environmental Services
  2. Bureau of Transportation
- E. Public Comments
  1. Dasha Peterman, 04/11/2022, comment noting that Verizon business (MCI) has facilities in the area
- F. Other
  1. Application form
  2. Email from staff re: Modification in Exhibit A.2, 03/29/2022
  3. Staff comments re: proposal, 04/01/2022
  4. Staff memo to Design Commission, 04/14/2022

5. Written comments from Commissioner Brian McCarter, 04/19/2022
6. Staff presentation to Design Commission, 04/21/2022



CONTENTS

TABLE OF CONTENTS	C.1
PROJECT DESCRIPTION	C.2
ZONING SUMMARY	C.3
PROPOSED SITE PLAN	C.4
MASSING AT SE CORNER	C.5

PROJECT TEAM

MASTER DEVELOPMENT, LLC	Applicant
MACKENZIE	Architecture   Interiors   Structural   Civil   Landscape   Planning
SAZAN GROUP	Mechanical   Electrical   Plumbing

PROJECT SUMMARY

Address	701 + 777 NE 7th Avenue Portland, OR 97232
Property Tax IDs	R182216 (701 NE 7th Ave) + R182217 (777 NE 7th Ave)
Tax Lots	1N1E35BC2200 (701 NE 7th Ave) + 1N1E35BC2100 (777 NE 7th Ave)
Size	20,000 SF

PROJECT NARRATIVE

The proposed development is for a 14-story multi-family apartment building located at the southwest corner of NE 7th Avenue and NE Oregon Street in the Lloyd District. Tax lot 1N1E35BC2200 is currently occupied by an office building, Tax lot 1N1E35BC2100 is currently occupied by a fenced and vacant building that appears to most recently be used as a food and beverage establishment.

The Portland Streetcar (A Loop) runs along the site’s NE Oregon Street frontage.

The proposed building use includes 13 stories of apartment units with 20 units on each floor (260 total units). The ground floor of the building will provide resident amenities and an amenity deck and ecoroof will occupy the roof.

Below grade parking is included. Vehicle access to the below-grade garage will be provided from NE Irving Street.

Pedestrian entrances are provided along NE Oregon Street and NE 7th Avenue with each entrance set back and covered to provide weather protection.

The site is located in the Central Commercial (CX) zone, with a d (design) overlay, and is within the Central City Plan District (Lloyd District design subdistrict).

With the site located along the Portland Streetcar and Green Loop, careful consideration has been given to the pedestrian friendly street scape.





ZONING SUMMARY

Base Zone	Central Commercial (CX)
Overlay	d (Design)
District	Central City (CC)
Subdistrict	Lloyd District
Base Zone FAR	4:1
Base Zone Height	75 feet
Central City FAR	9:1
Central City Height	250 feet
Inclusionary Housing FAR Bonus	3:1
Inclusionary Housing Height Bonus	75 feet
FAR Total	12:1
Height Total	325 feet
Minimum Setbacks	0 feet
Maximum Setbacks	Building must extend to within 12 feet of street lot line for 75% of lot line
Maximum Lot Coverage	100% of Site Area
Minimum Landscape Area	0%
Central City Parking Sector	Lloyd District Subdistrict, 2
Minimum Parking	None
Maximum Parking	1.2 spaces per dwelling unit
Distance from Transit Stop	Approximately 415 feet - Streetcar Approximately 800 feet - Bus Approximately 750 feet - MAX
Loading	One Standard A or Two Standard B

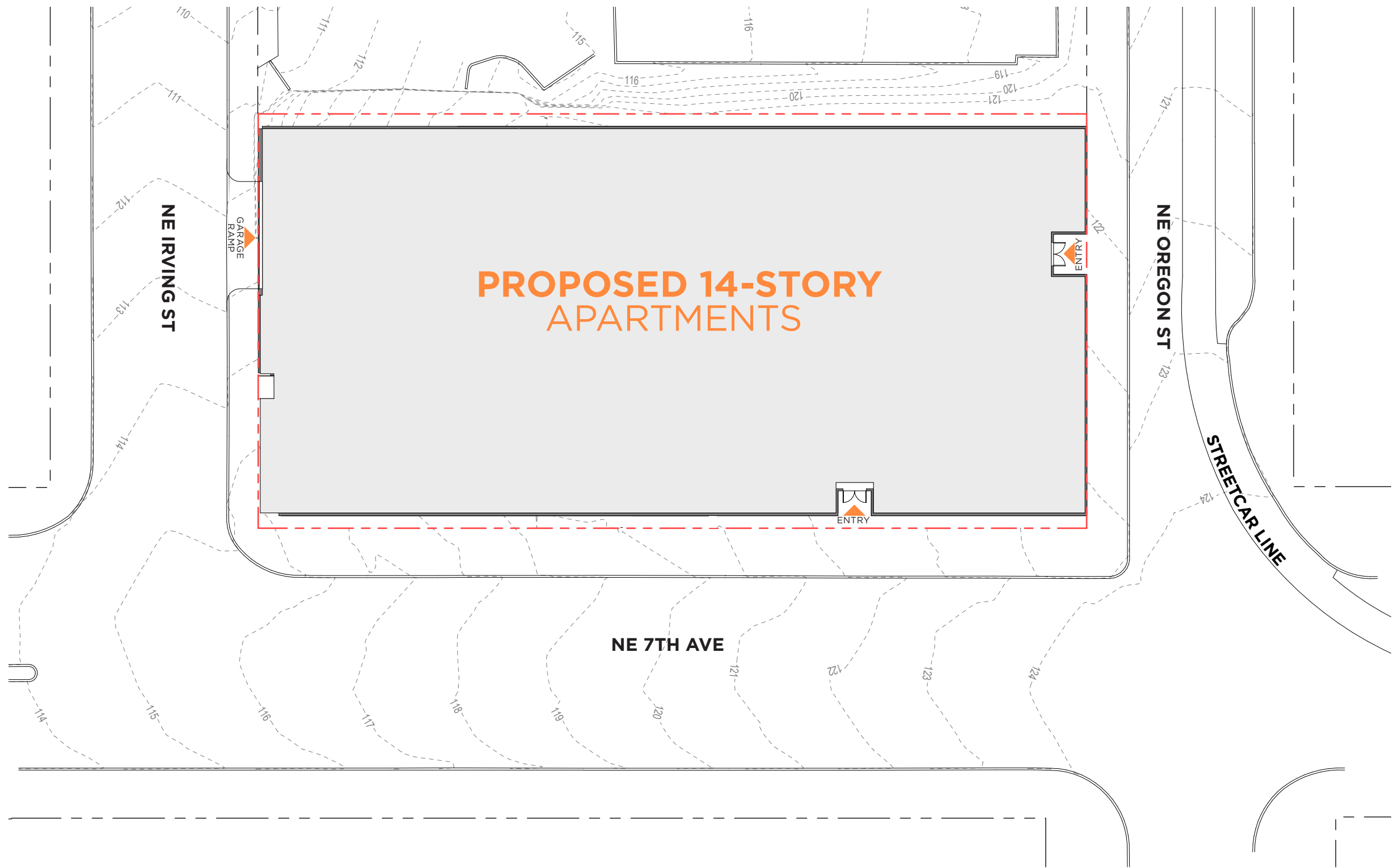
PARKING SUMMARY

ZONING CODE REQUIREMENTS

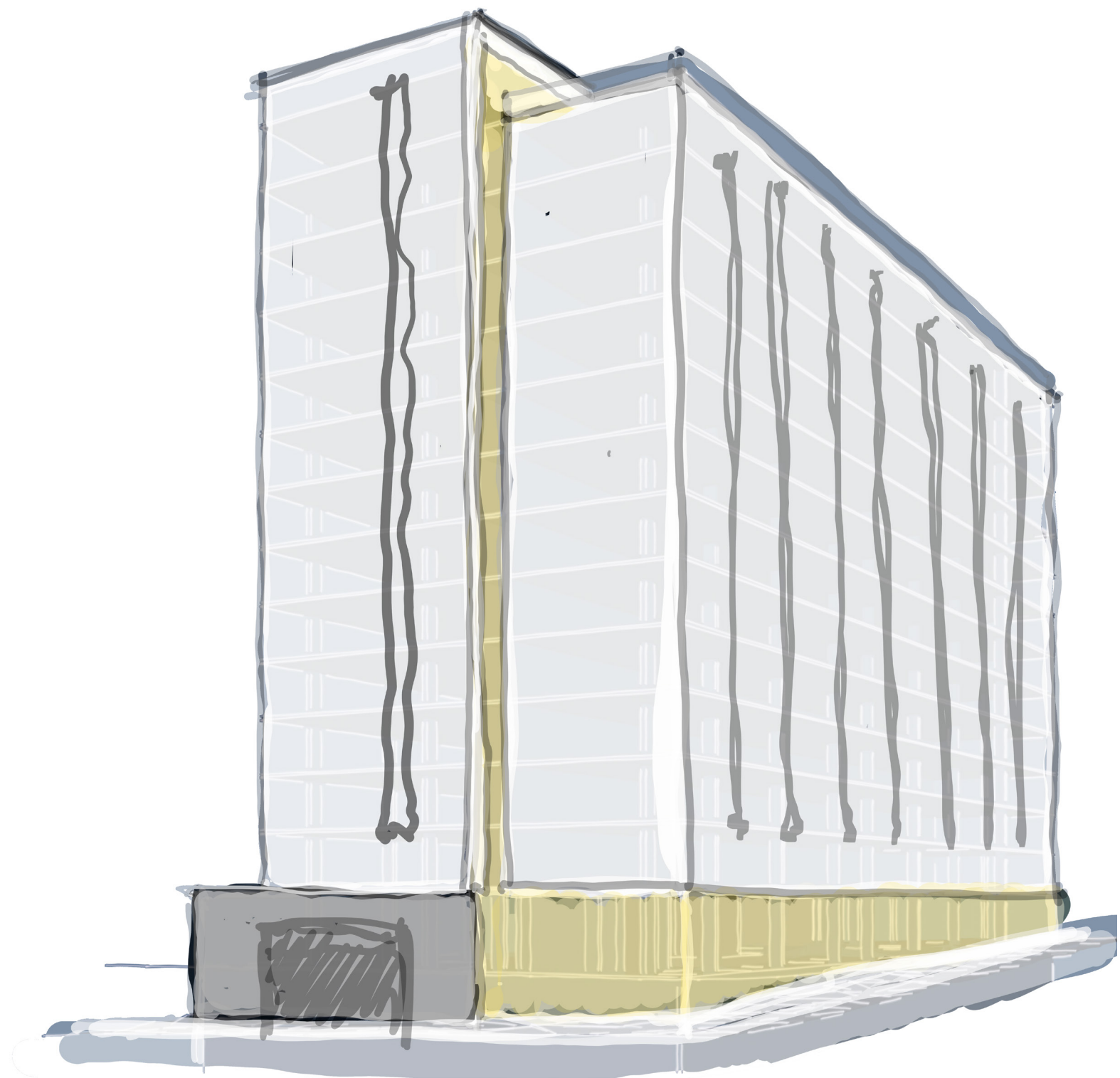
Ecoroof	Required
Outdoor Area	Not Required
Building Length + Facade Articulation	Not Required
Ground Floor Active Use	Not Required
Ground Floor Window Standard	40% of wall area facing streets, measured from 2 feet to 10 feet above the finished grade
Upper Floor Window Standard	15% of wall area facing streets, measured from 10 feet above the finished grade
Pedestrian Standards	Connections to abutting streets apply on all frontages unless at least 50% of facade is within 10 feet of street
Transit Street Main Entrance	Transit street standards apply on NE Oregon Street Need entrance facing the street, or at up to 45 degree angle, or face courtyard within 60 feet of transit street
Bird-Safe Exterior Glazing	Required
Low-Carbon Building	Required
Inclusionary Zoning	Required
Design Review	Required
Applicable Design Guidelines	Central City Fundamental Design Guidelines Lloyd District Design Guidelines

ZONING MODIFICATIONS

The list of potential zoning modifications is still under development and will be included in future submittals







# MACKENZIE.

503.224.9560 | [mcknze.com](https://mcknze.com)

Architecture · Interiors · Structural Engineering · Civil Engineering  
Land Use Planning · Transportation Planning · Landscape Architecture

Copyright © 2022 Mackenzie, All Rights Reserved.

**Master Development**  
**NE 7th Ave Apartments**  
Design Advice Request | 03.24.2022

CONTENTS

CONTENTS + PROJECT TEAM	C.2
PROJECT DESCRIPTION	C.3
ZONING SUMMARY	C.4
URBAN CONTEXT	C.5
EXISTING SITE + PHOTOS	C.6
PROJECT EVOLUTION	C.7
PROPOSED SITE PLAN	C.9
FLOOR PLANS	C.10
OUTDOOR AMENITIES	C.12
BUILDING ELEVATIONS	C.13
MATERIALS	C.16
MASSING	C.17
MODIFICATIONS	C.19

PROJECT TEAM

MASTER DEVELOPMENT, LLC	Applicant
MACKENZIE	Architecture   Interiors   Structural   Civil   Landscape   Planning   Traffic
SAZAN GROUP	Mechanical   Electrical   Plumbing



PROJECT SUMMARY

Address	701 + 777 NE 7th Avenue Portland, OR 97232
Property Tax IDs	R182216 (701 NE 7th Ave) + R182217 (777 NE 7th Ave)
Tax Lots	1N1E35BC2200 (701 NE 7th Ave) + 1N1E35BC2100 (777 NE 7th Ave)
Size	20,000 SF

PROJECT NARRATIVE

The proposed development is for a 14-story multi-family apartment building located at the southwest corner of NE 7th Avenue and NE Oregon Street in the Lloyd District. Tax lot 1N1E35BC2200 is currently occupied by an office building, tax lot 1N1E35BC2100 is currently occupied by a fenced and vacant building that appears to most recently be used as a food and beverage establishment.

The site is located in the Central Commercial (CX) zone, with a d (design) overlay, and is within the Central City Plan District (Lloyd District design subdistrict).

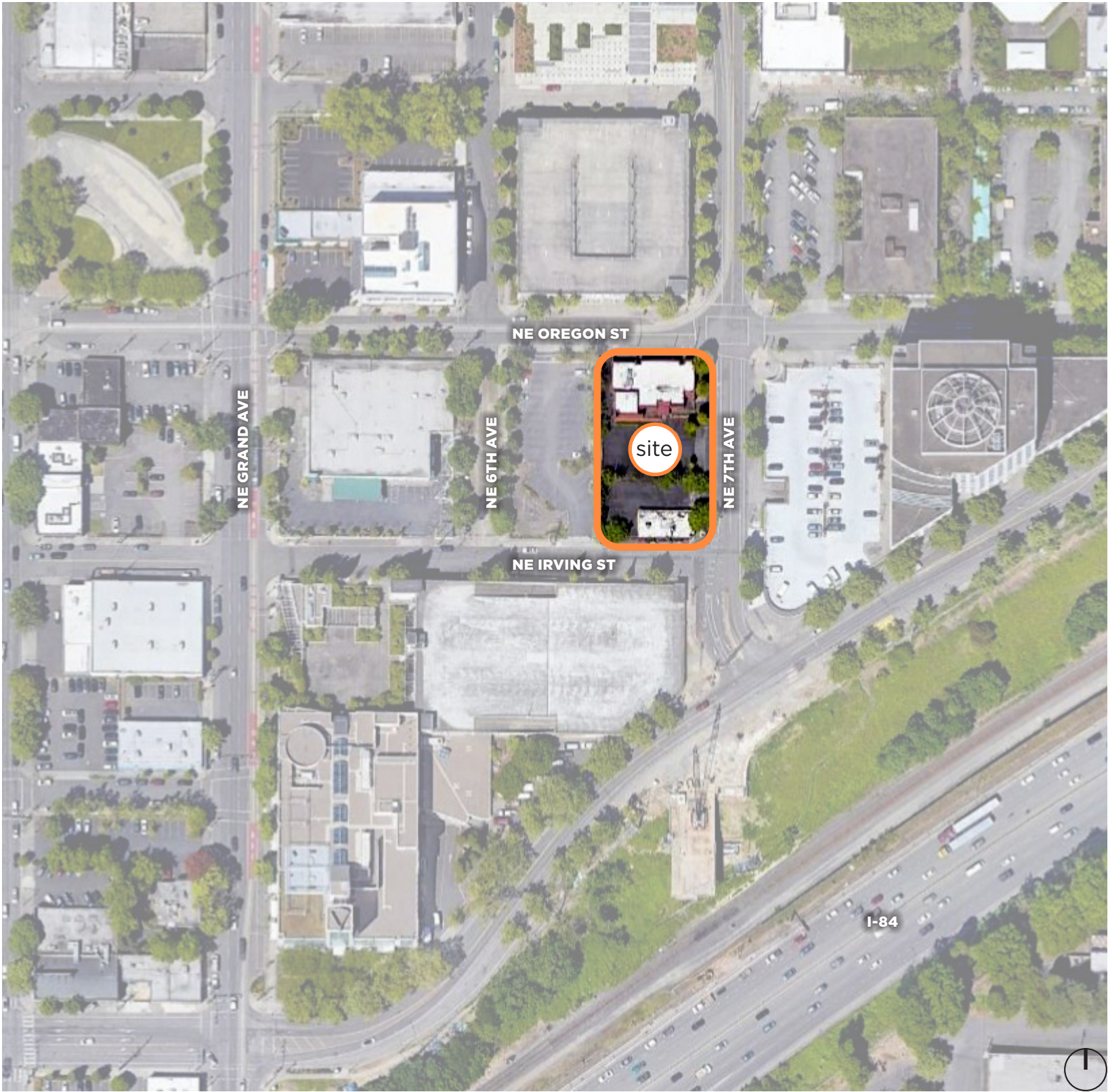
The proposed building use includes 13 stories of apartment units with 20 units on each floor (260 total units). The ground floor of the building will provide resident amenities while an amenity deck and ecoroof will occupy the roof.

Below grade parking is included. Vehicle access to the below-grade garage will be provided from NE Irving Street. While the exact number of parking spaces is still under development the provided parking will be less than 1.2 spaces per unit.

Pedestrian entrances are provided along NE Oregon Street and NE 7th Avenue with each entrance set back and covered to provide weather protection.

The Portland Streetcar (A Loop) runs along the site’s NE Oregon Street frontage.

With the site located along the Portland Streetcar and Green Loop, careful consideration has been given to the pedestrian friendly streetscape.



ZONING SUMMARY

Base Zone	Central Commercial (CX)
Overlay	d (Design)
District	Central City (CC)
Subdistrict	Lloyd District
Base Zone FAR	4:1
Base Zone Height	75 feet
Central City FAR	9:1
Central City Height	250 feet
Inclusionary Housing FAR Bonus	3:1
Inclusionary Housing Height Bonus	75 feet
FAR Total	12:1
Height Total	325 feet
Minimum Setbacks	0 feet
Maximum Setbacks	Building must extend to within 12 feet of street lot line for 75% of lot line
Maximum Lot Coverage	100% of Site Area
Minimum Landscape Area	0%

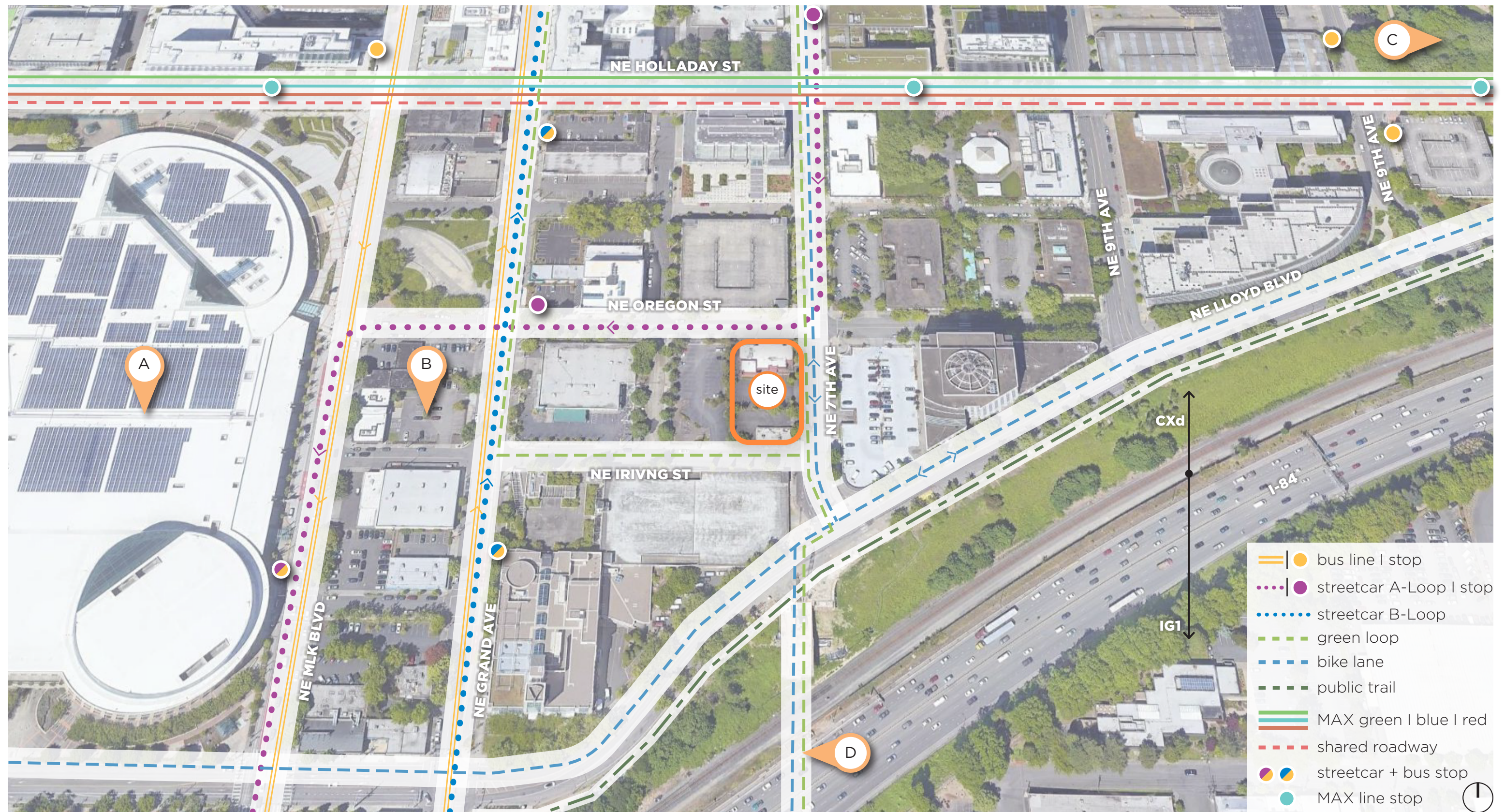
PARKING SUMMARY

Central City Parking Sector	Lloyd District Subdistrict, 2
Minimum Parking	None
Maximum Parking	1.2 spaces per dwelling unit
Distance from Transit Stop	Approximately 415 feet - Streetcar Approximately 800 feet - Bus Approximately 750 feet - MAX
Loading	One Standard A or Two Standard B

ZONING CODE REQUIREMENTS

Ecoroof	Required
Outdoor Area	Not Required
Building Length + Facade Articulation	Not Required
Ground Floor Active Use	Not Required
Ground Floor Window Standard	40% of wall area facing streets, measured from 2 feet to 10 feet above the finished grade
Upper Floor Window Standard	15% of wall area facing streets, measured from 10 feet above the finished grade
Pedestrian Standards	Connections to abutting streets apply on all frontages unless at least 50% of facade is within 10 feet of street
Transit Street Main Entrance	Transit street standards apply on NE Oregon Street Need entrance facing the street, or at up to 45 degree angle, or face courtyard within 60 feet of transit street
Bird-Safe Exterior Glazing	Required
Low-Carbon Building	Required
Inclusionary Zoning	Required
Design Review	Required
Applicable Design Guidelines	Central City Fundamental Design Guidelines Lloyd District Design Guidelines





- A convention center
- B convention center plaza
- C holladay park
- D Earl Blumenauer pedestrian + bike bridge (under construction)

## Master Development | NE 7th Ave Apartments

03.24.2022

Urban Context  
© 2022 Mackenzie | 2210364.00

**MACKENZIE.**  
EA 22-119814 DA | C.5  
EA 22-119814 DA - Exhibit A.2





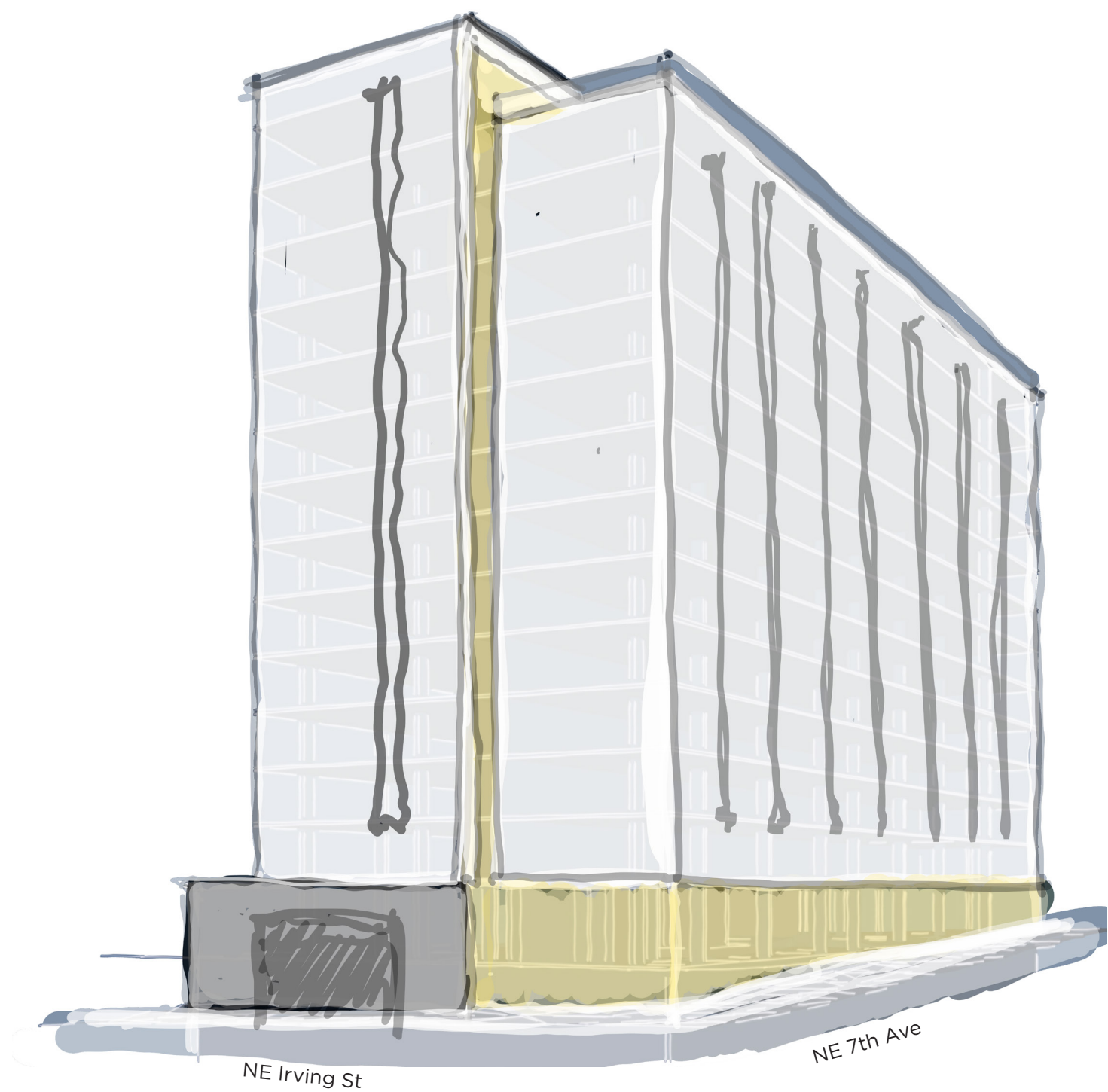
EXISTING SITE



EXISTING SITE CONTEXT







Preliminary design sketch



Progression design model

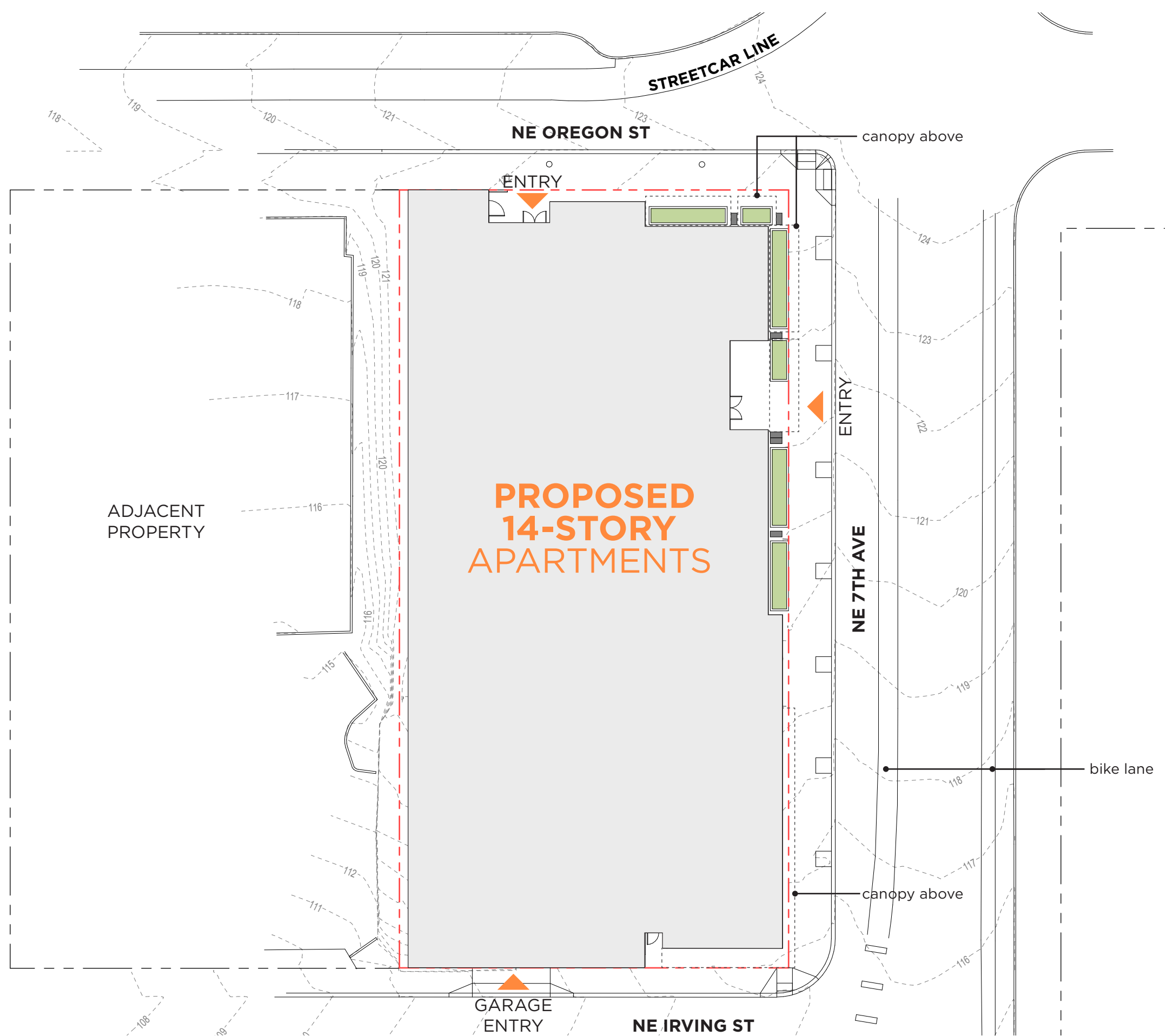


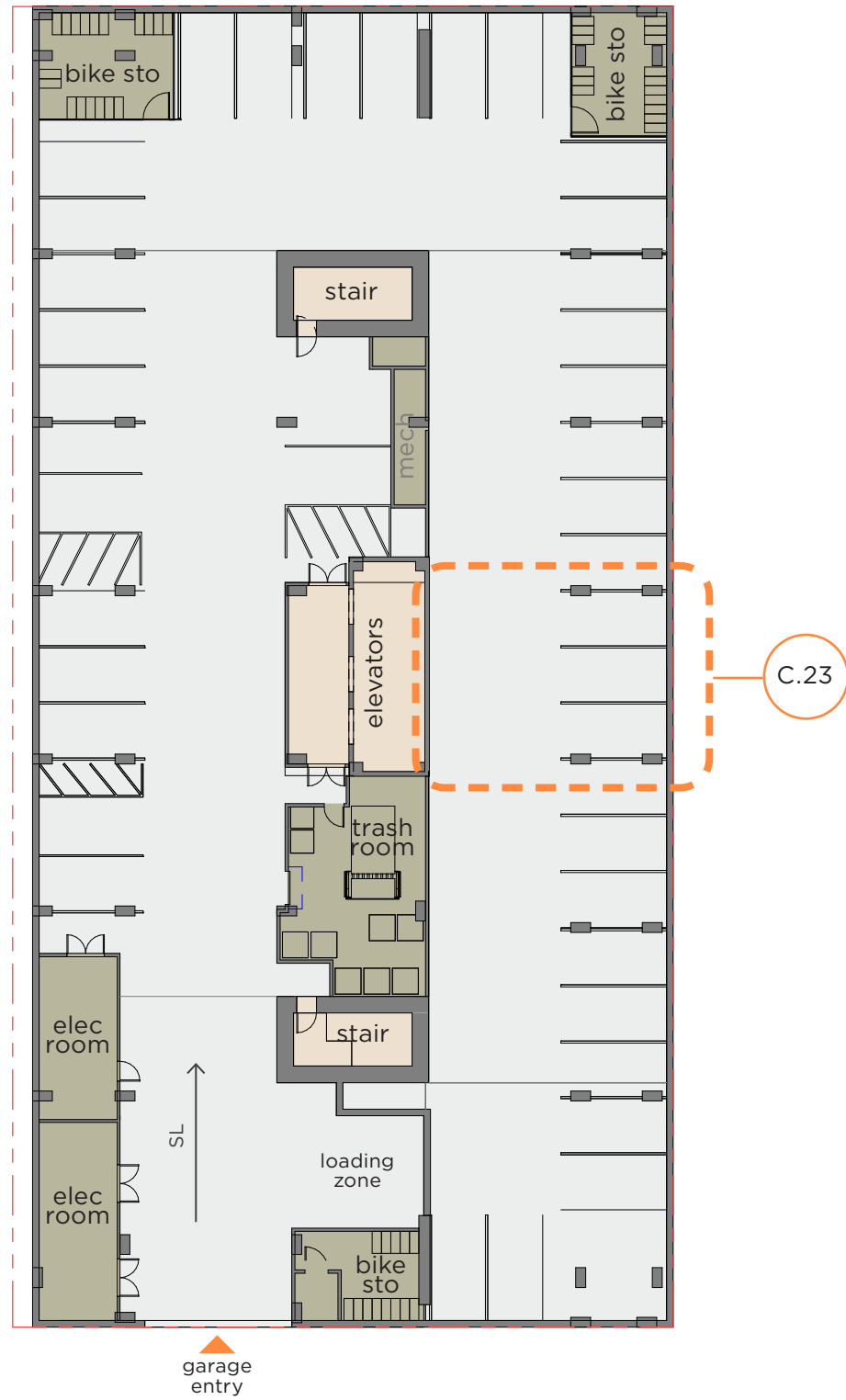


Progression design model



Proposed design model



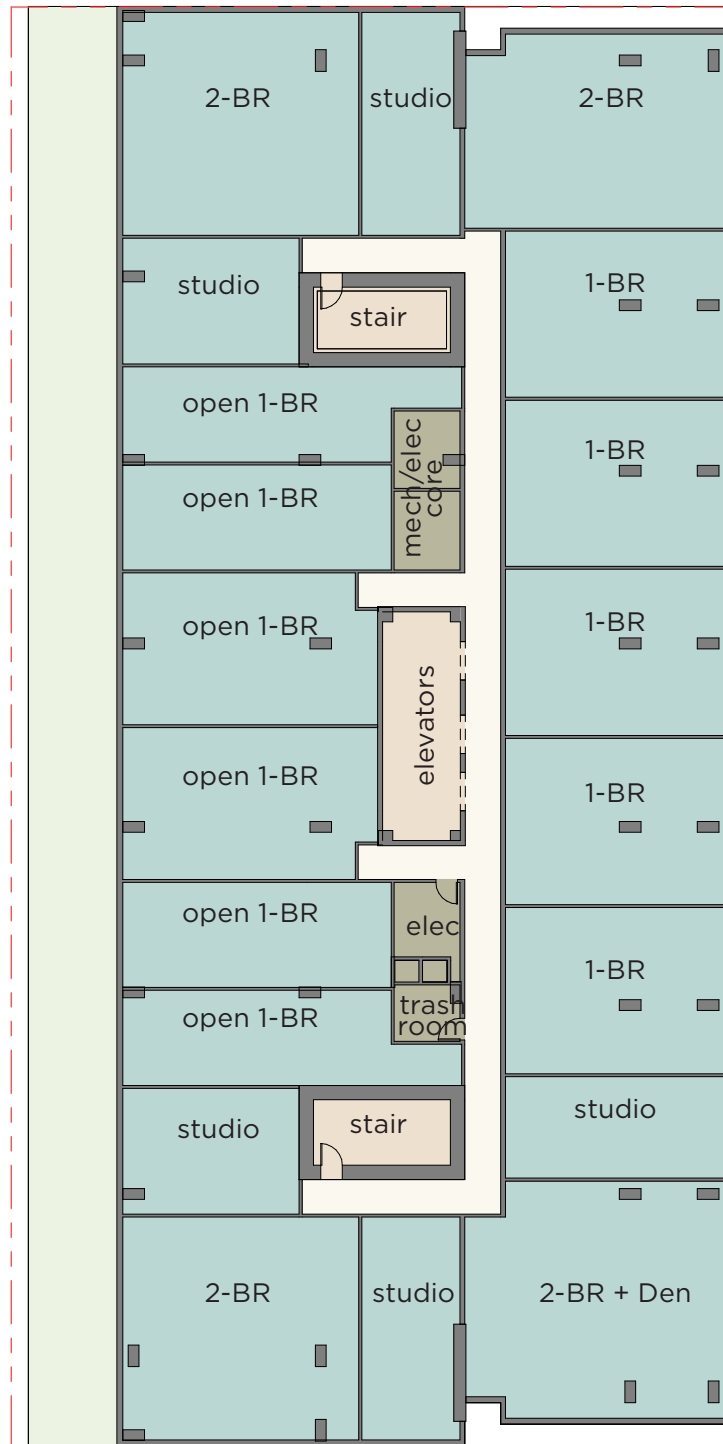


Floor Plan Level P1

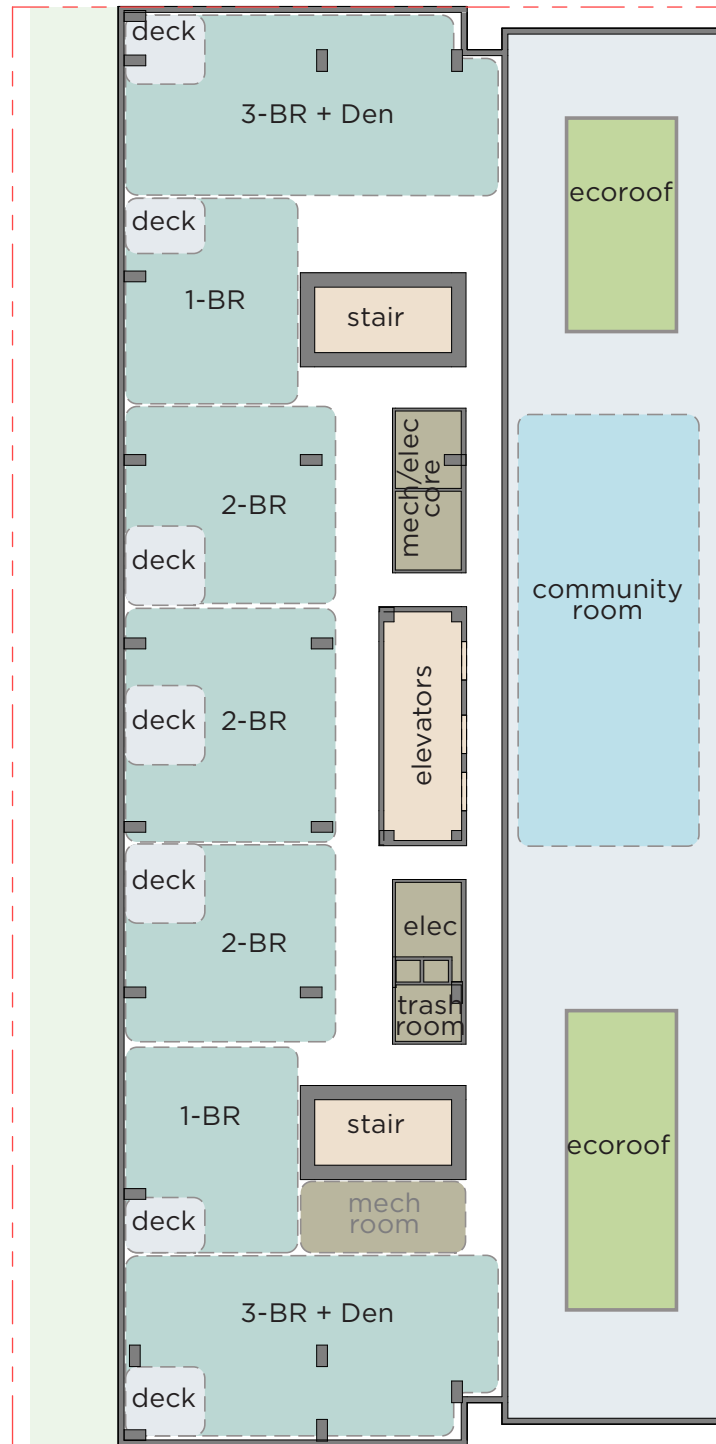


Floor Plan Level 1

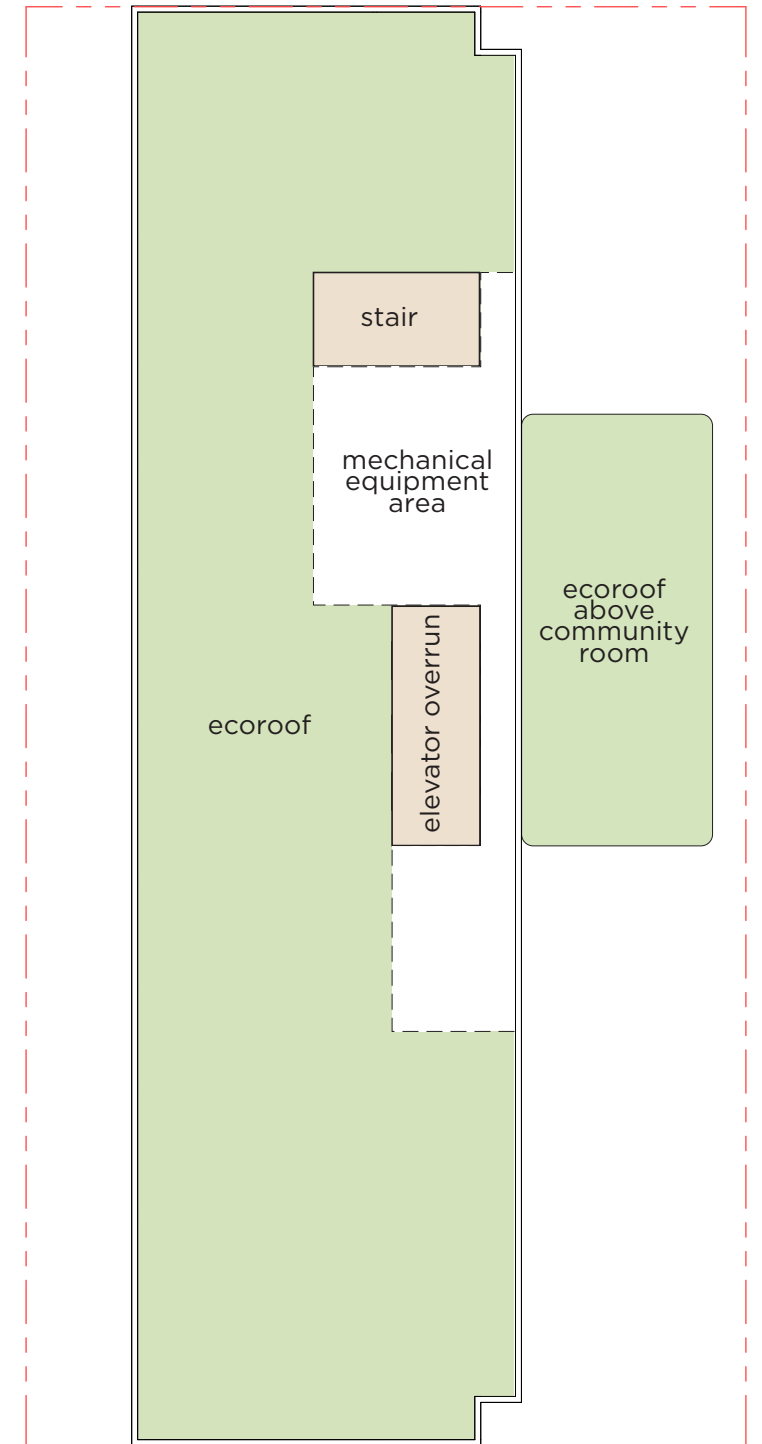




Floor Plan Levels 2-14

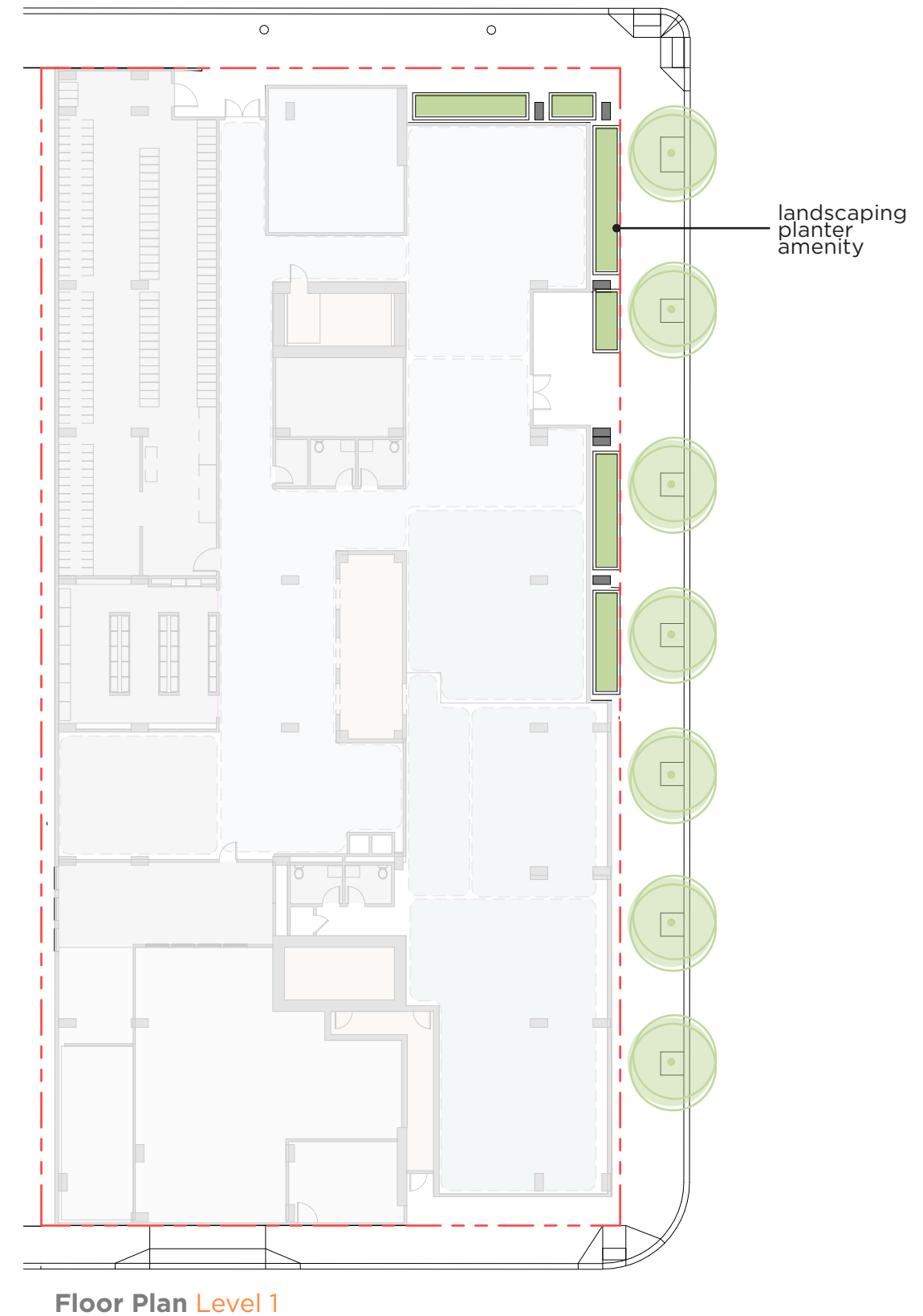
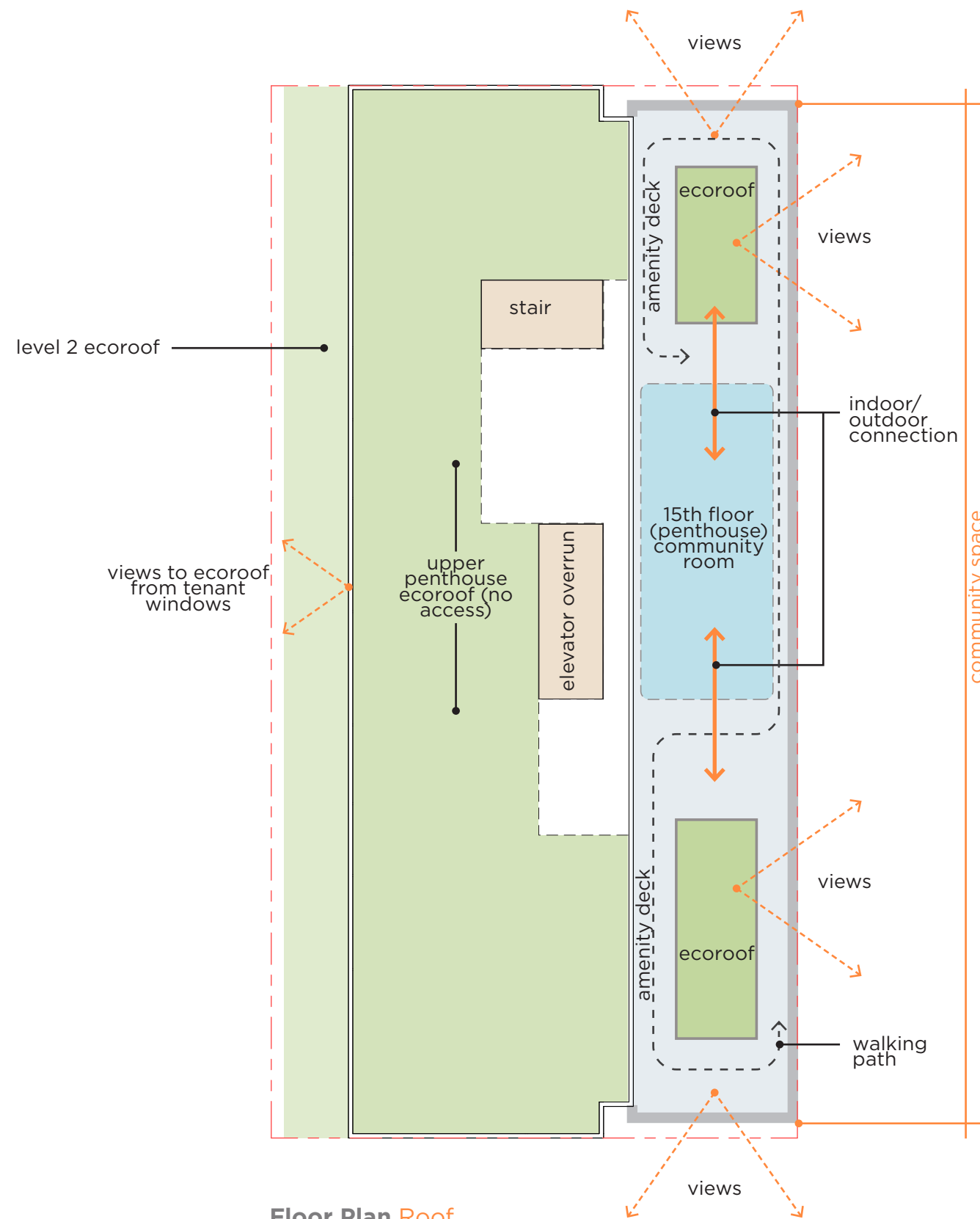


Floor Plan Level Penthouse

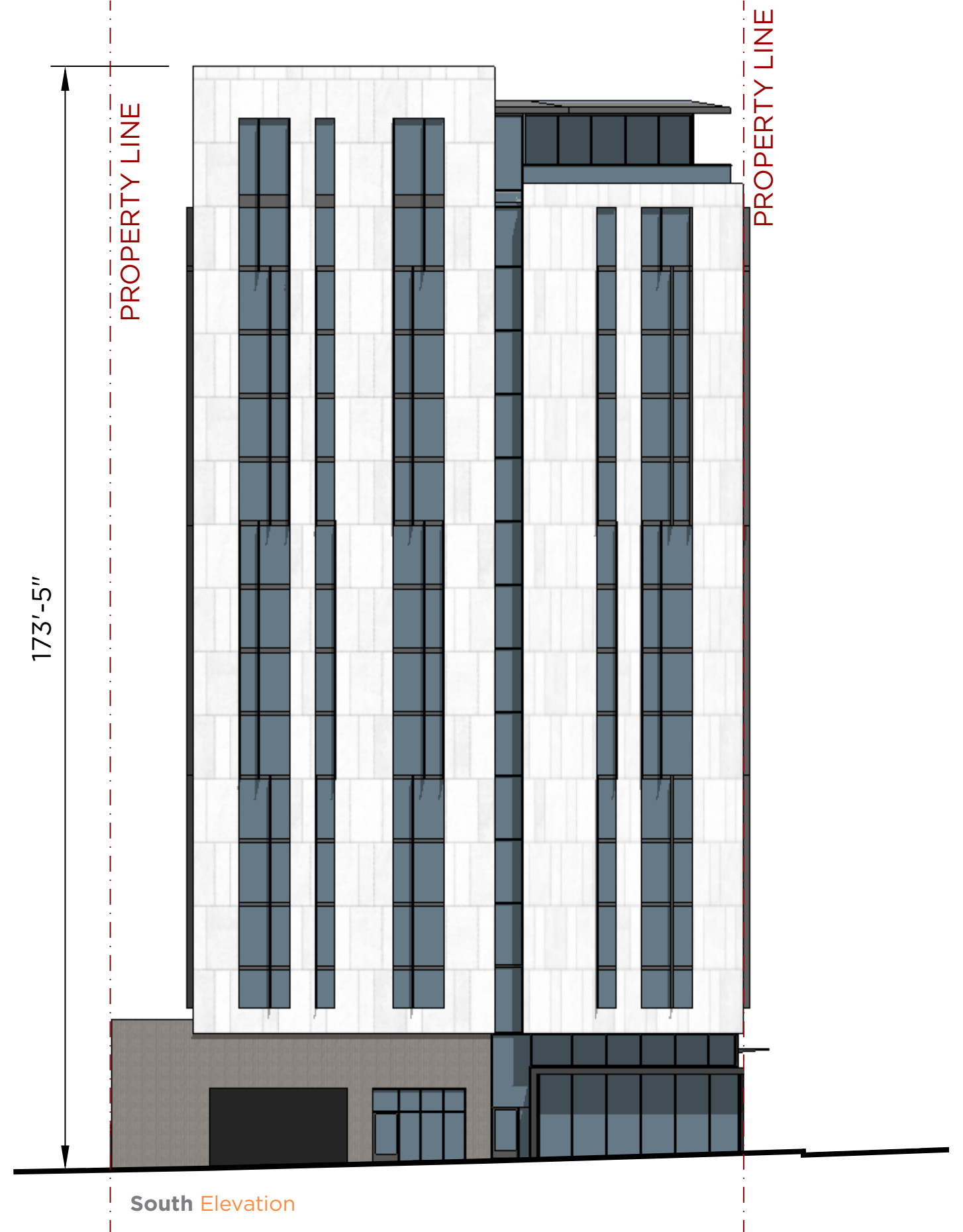
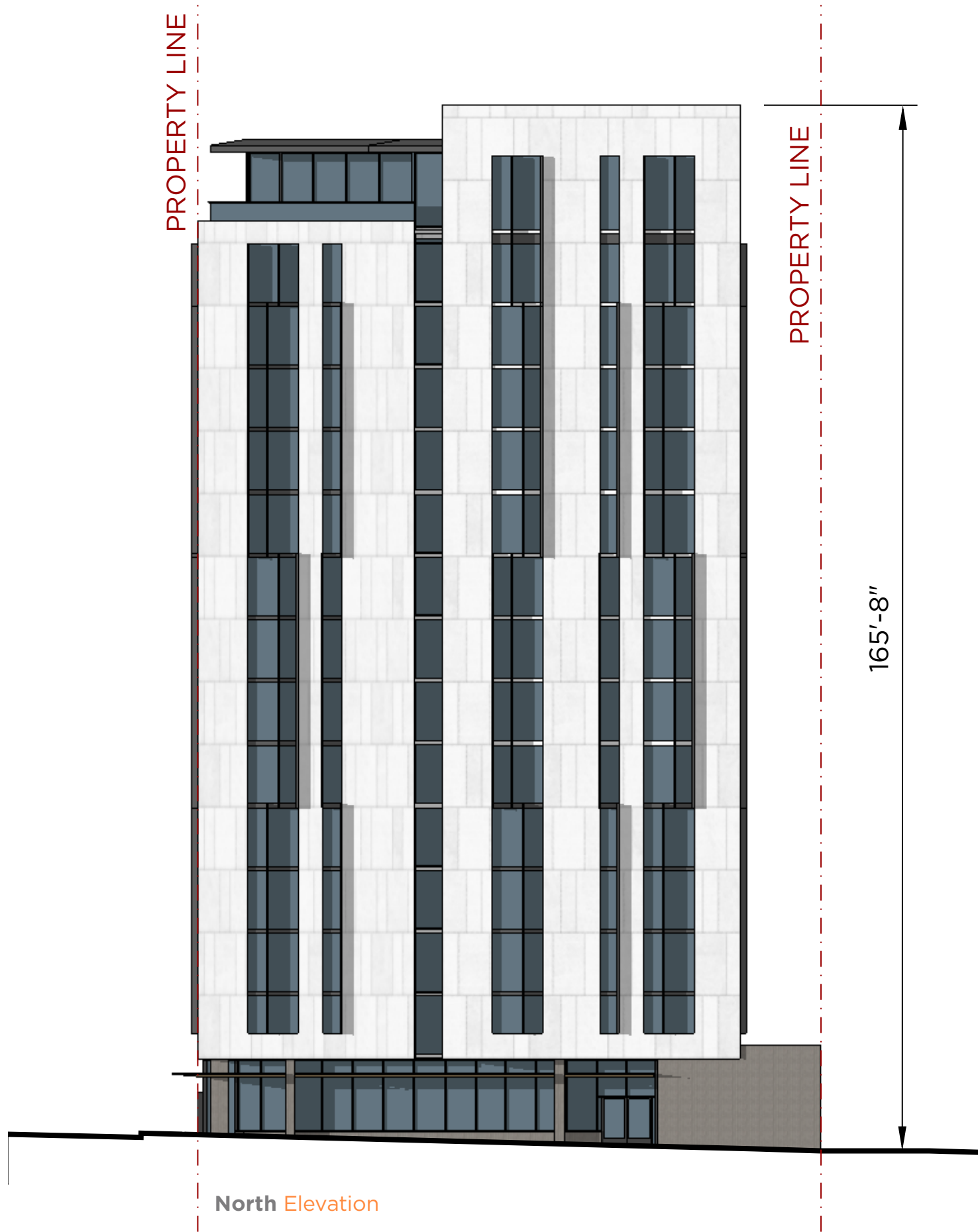


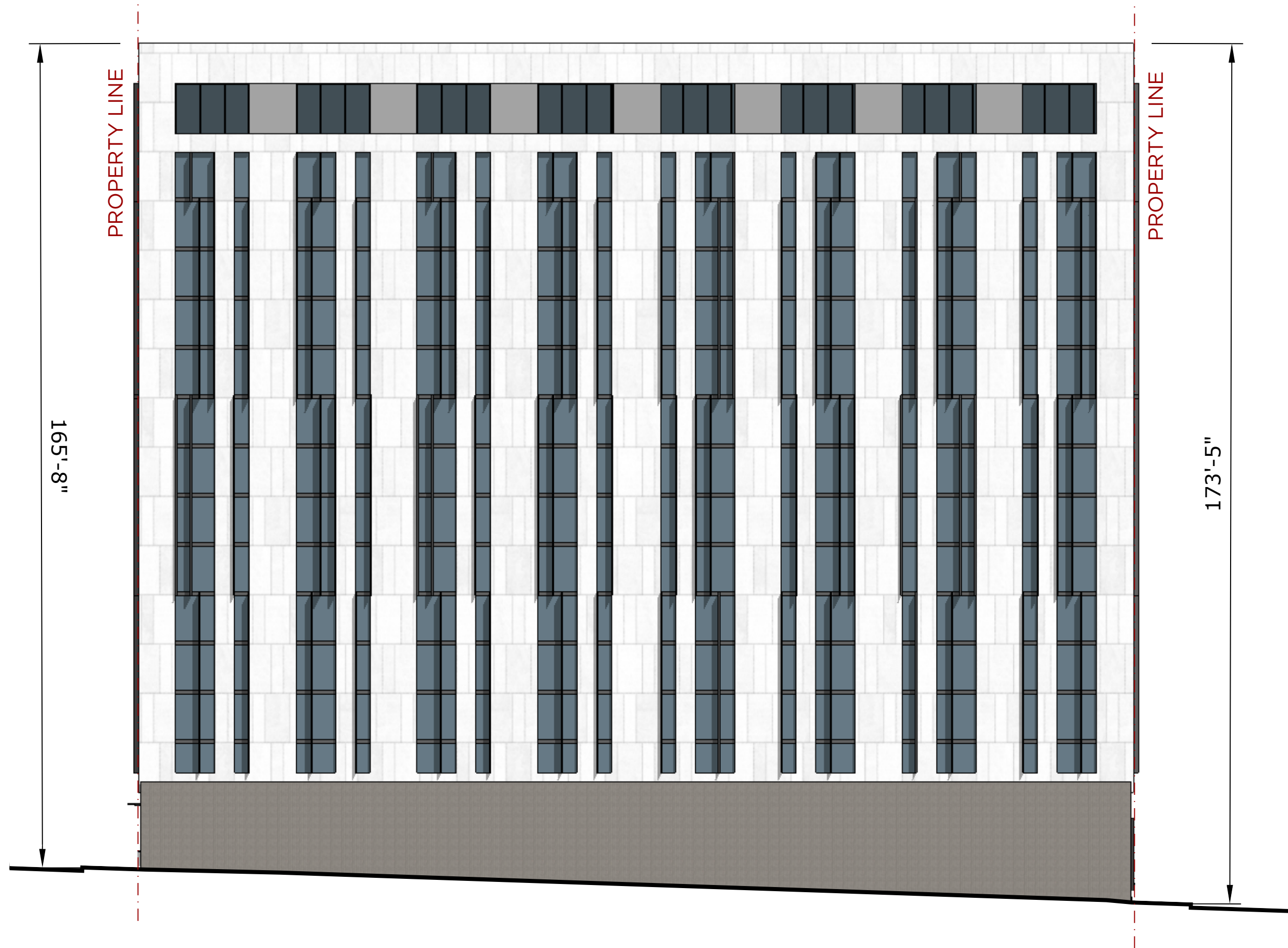
Floor Plan Roof



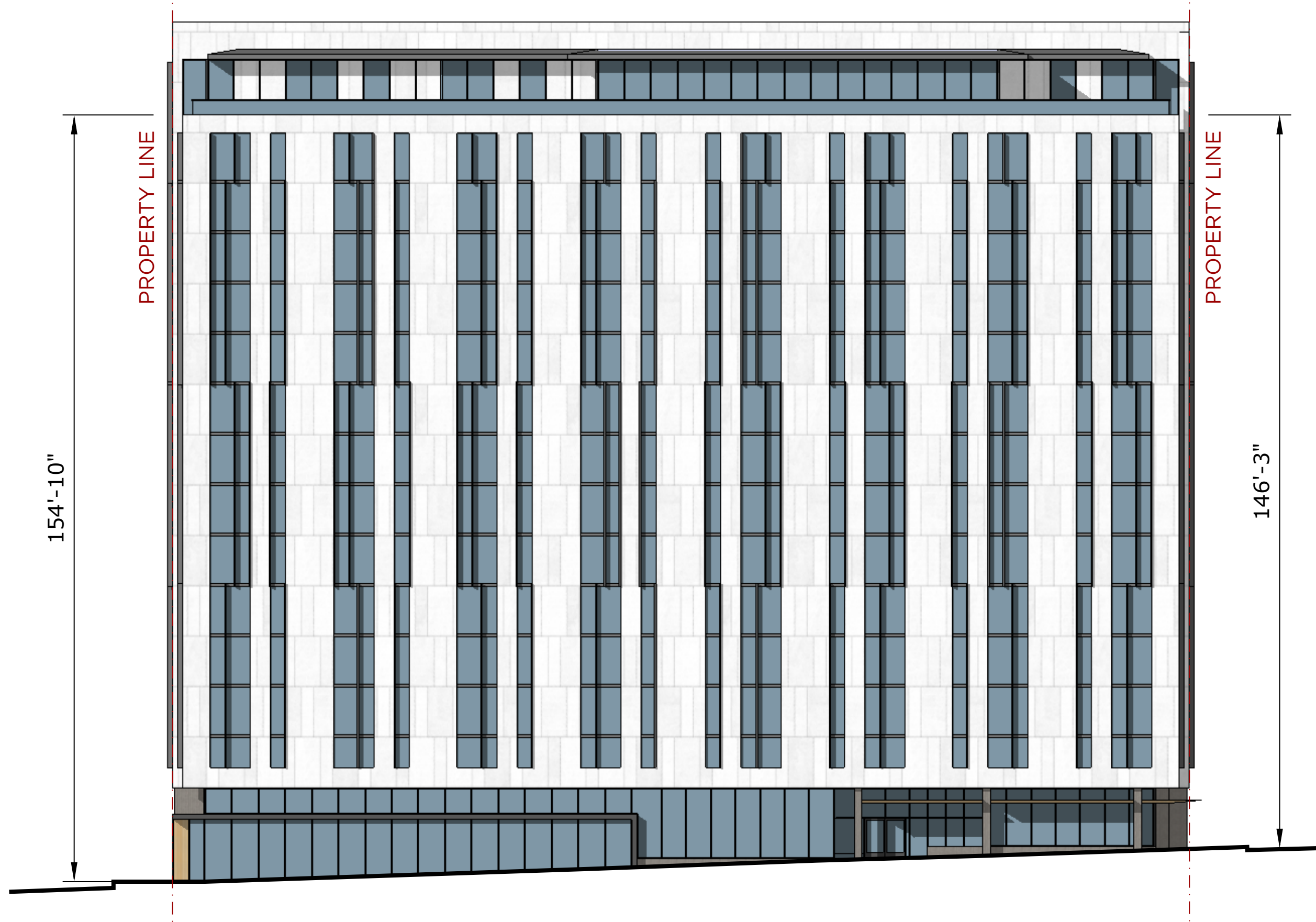




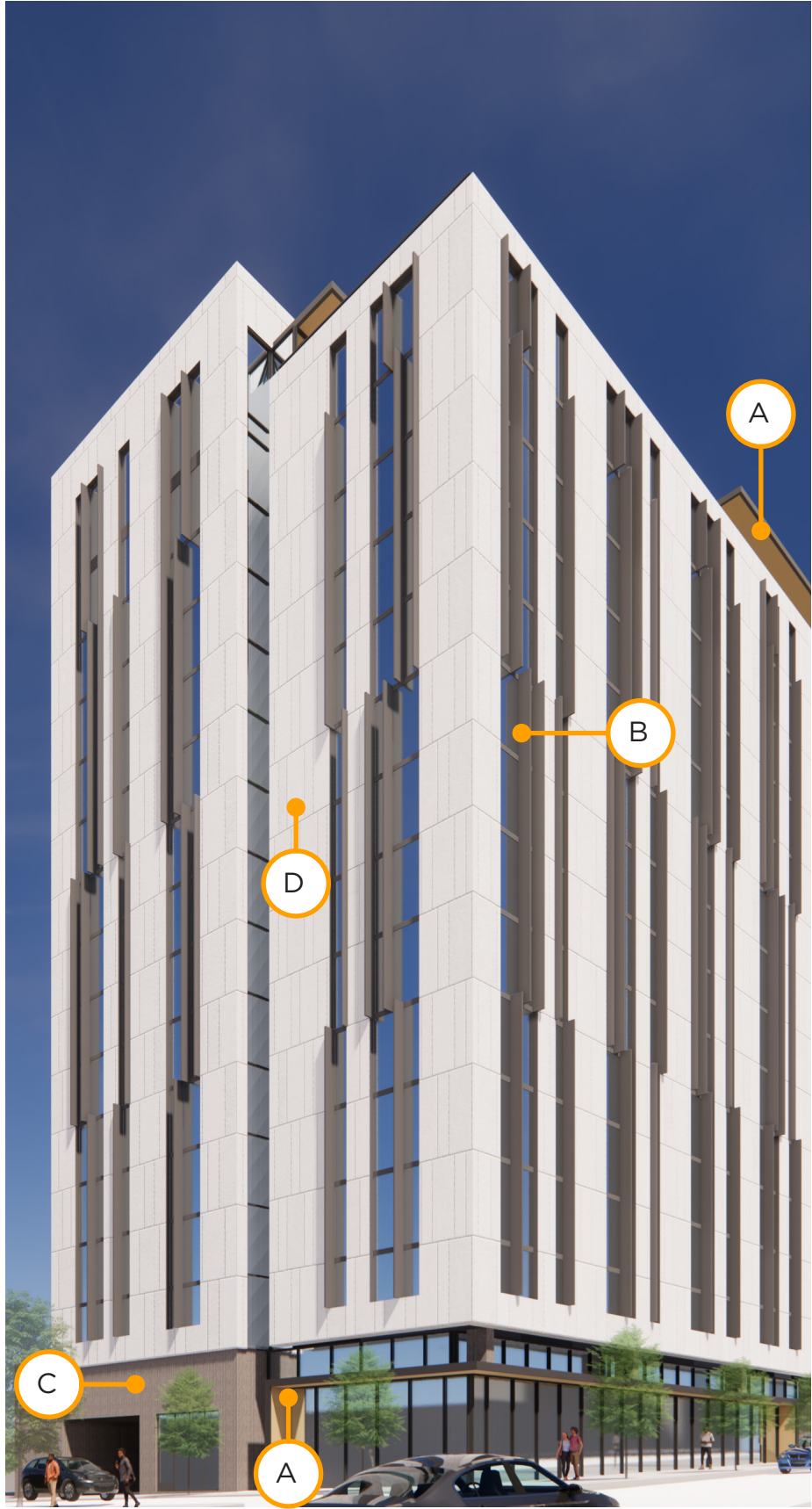








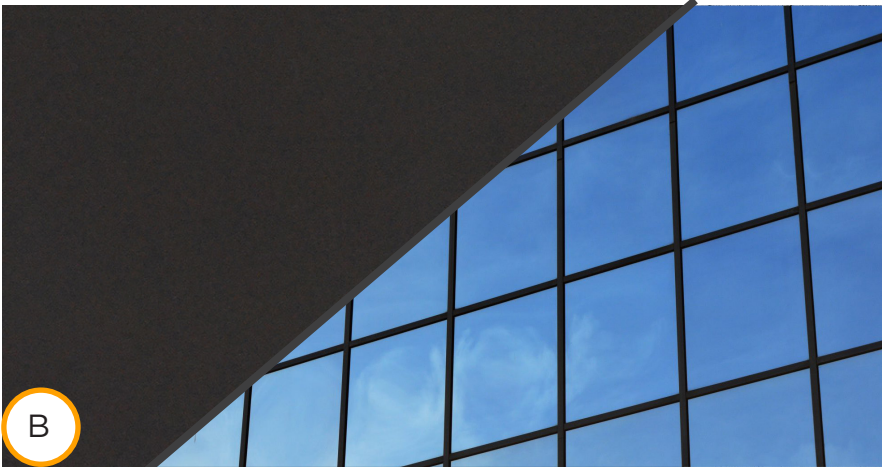




Natural Wood



Textured Fiber Cement or Textured Metal Panel



Dark Bronze Metal Accents + Window Mullions + Glazing



Dark Masonry



Textured Fiber Cement or Textured Metal Panel



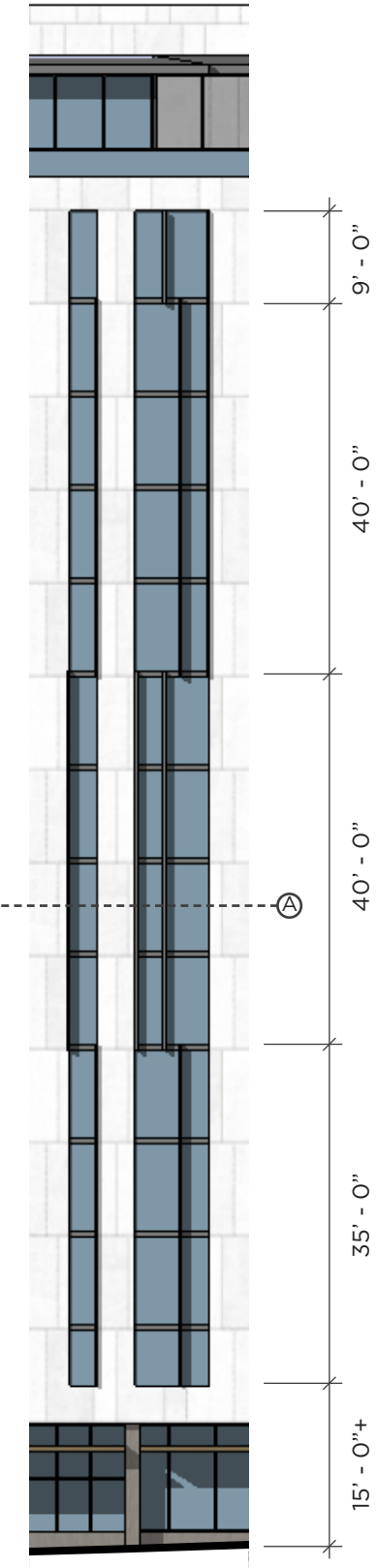
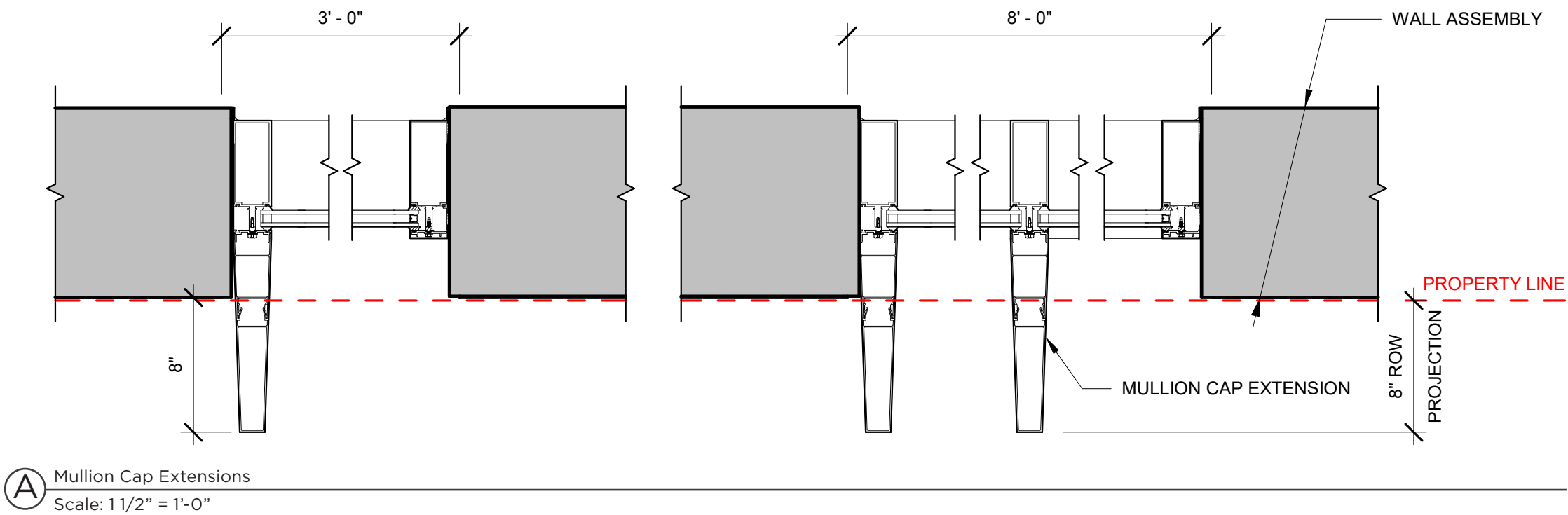






MODIFICATION 01 | PROTRUSION INTO RIGHT-OF-WAY

Proposal seeks a modification to allow mullion cap extension to project 8” into the right-of-way starting at a minimum of 12’ above grade. Modification will better meet the standard by enhancing the verticality of the project and creating added visual interest along the facades.



MODIFICATION 02 | PARKING SPACE DIMENSION

Proposed modification:

Proposal seeks a modification to the 8’-6” x 16’-0” parking space requirement in those spaces with structural columns, to allow encroachment of columns into the parking space width.

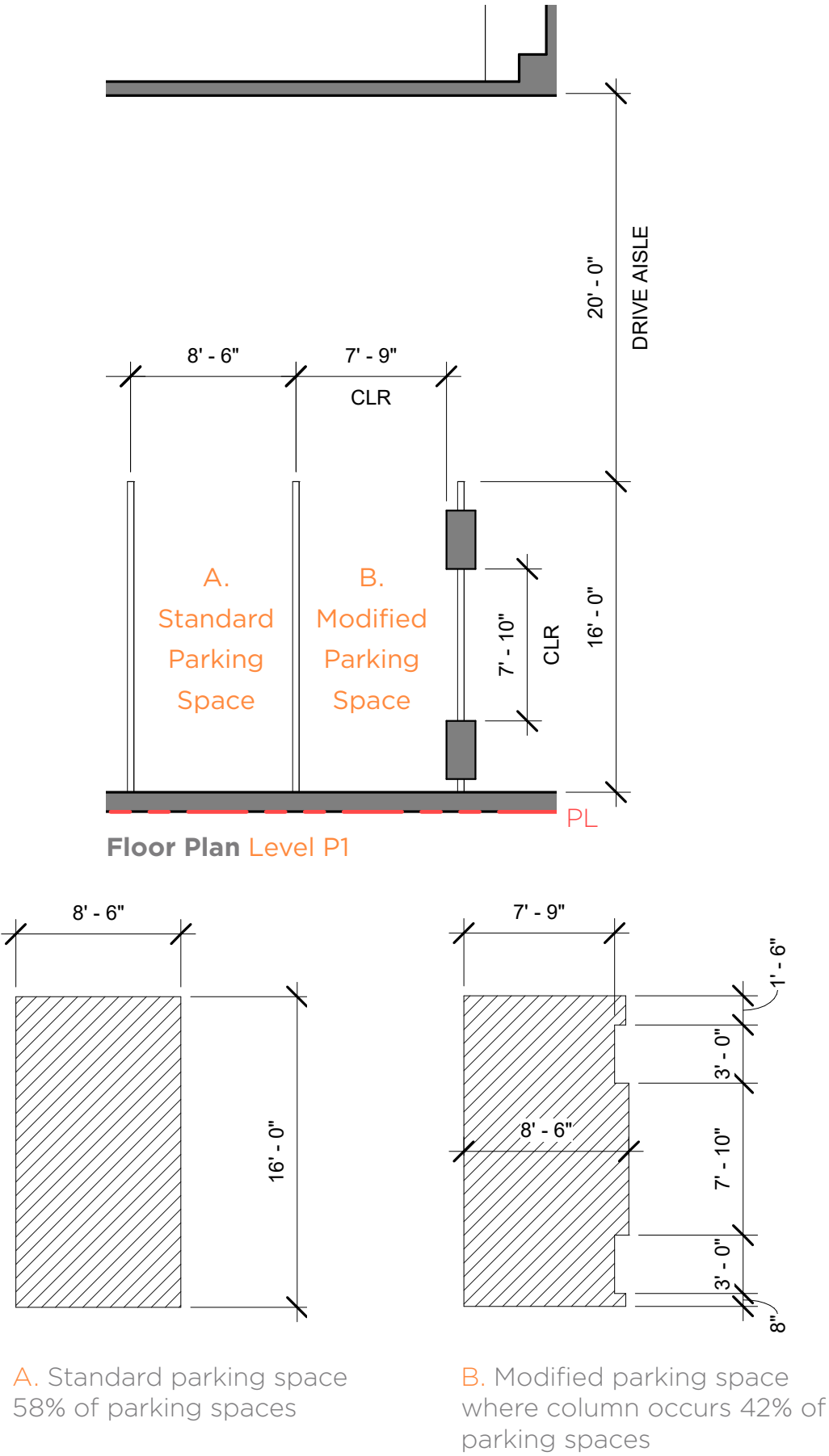
Parking columns occur at 22 spaces per typical floor, which is 42% of parking spaces.

Note that all parking spaces will meet the 16’-0” minimum length requirement, and all parking spaces will have stripes located at an 8’-6” spacing width.

Certain parking spaces will have one-half of a column width encroaching into one side of the space. As the columns are 1’-6” wide, that results in a 9” reduction of the parking space width where the column occurs.

Meeting the Design Guidelines:

- 1. Arranging the structural columns requires attention to how it will impact the plan layouts of the many levels of the building: residential units, parking levels, ground floor, and the associated exterior walls of the ground floor. The columns have been arranged to the greatest benefit of the ground floor to have an inset of the exterior wall on the ground floor. This results in having a column inset into the parking space stripe by that same distance. This provides more area between the building and the NE 7th Avenue Right of Way. This meets Design Guideline A8, Contribute to a Vibrant Streetscape, by integrating building setbacks with adjacent sidewalks.
- 2. This modification meets the purpose of the parking space width standard, as it provides the required 8’-6” width at the central portion of the parking space, where there are car door swings.
- 3. The impact resulting from this modification has been mitigated as much as possible by thoughtfully locating the columns to be clear of the car door opening area.



# MACKENZIE.

503.224.9560 | [mcknze.com](https://mcknze.com)

Architecture · Interiors · Structural Engineering · Civil Engineering  
Land Use Planning · Transportation Planning · Landscape Architecture

Copyright © 2022 Mackenzie, All Rights Reserved.

**Master Development**  
**NE 7th Ave Apartments**  
Design Advice Request | 04.07.2022



CONTENTS

CONTENTS + PROJECT TEAM	C.2
PROJECT DESCRIPTION	C.3
ZONING SUMMARY	C.4
URBAN CONTEXT	C.5
EXISTING SITE + PHOTOS	C.6
CONTEXT PRECEDENTS	C.7
PROJECT EVOLUTION	C.8
PROPOSED SITE PLAN	C.10
FLOOR PLANS	C.11
OUTDOOR AMENITIES	C.13
BUILDING ELEVATIONS	C.14
MATERIALS	C.17
MASSING	C.18
MODIFICATION	C.20

PROJECT TEAM

MASTER DEVELOPMENT, LLC	Applicant
MACKENZIE	Architecture   Interiors   Structural   Civil   Landscape   Planning   Traffic
SAZAN GROUP	Mechanical   Electrical   Plumbing

PROJECT SUMMARY

Address	701 + 777 NE 7th Avenue Portland, OR 97232
Property Tax IDs	R182216 (701 NE 7th Ave) + R182217 (777 NE 7th Ave)
Tax Lots	1N1E35BC2200 (701 NE 7th Ave) + 1N1E35BC2100 (777 NE 7th Ave)
Size	20,000 SF

PROJECT NARRATIVE

The proposed development is for a 15-story multi-family apartment building located at the southwest corner of NE 7th Avenue and NE Oregon Street in the Lloyd District. Tax lot 1N1E35BC2200 is currently occupied by an office building, tax lot 1N1E35BC2100 is currently occupied by a fenced and vacant building that appears to most recently be used as a food and beverage establishment.

The site is located in the Central Commercial (CX) zone, with a d (design) overlay, and is within the Central City Plan District (Lloyd District design subdistrict).

The proposed building use includes 13 stories of apartment units with 20 units on each floor and 5 units on the 15th floor (265 total units). The ground floor of the building will provide resident amenities while community room, an amenity deck and ecoroof will occupy the roof.

Below grade parking is included. Vehicle access to the below-grade garage will be provided from NE Irving Street. While the exact number of parking spaces is still under development the provided parking will be less than 1.2 spaces per unit.

Pedestrian entrances are provided along NE Oregon Street and NE 7th Avenue with each entrance set back and covered to provide weather protection.

The Portland Streetcar (A Loop) runs along the site’s NE Oregon Street frontage.

With the site located along the Portland Streetcar and Green Loop, careful consideration has been given to the pedestrian friendly streetscape.





ZONING SUMMARY

Base Zone	Central Commercial (CX)
Overlay	d (Design)
District	Central City (CC)
Subdistrict	Lloyd District
Base Zone FAR	4:1
Base Zone Height	75 feet
Central City FAR	9:1
Central City Height	250 feet
Inclusionary Housing FAR Bonus	3:1
Inclusionary Housing Height Bonus	75 feet
FAR Total	12:1
Height Total	325 feet
Minimum Setbacks	0 feet
Maximum Setbacks	Building must extend to within 12 feet of street lot line for 75% of lot line
Maximum Lot Coverage	100% of Site Area
Minimum Landscape Area	0%

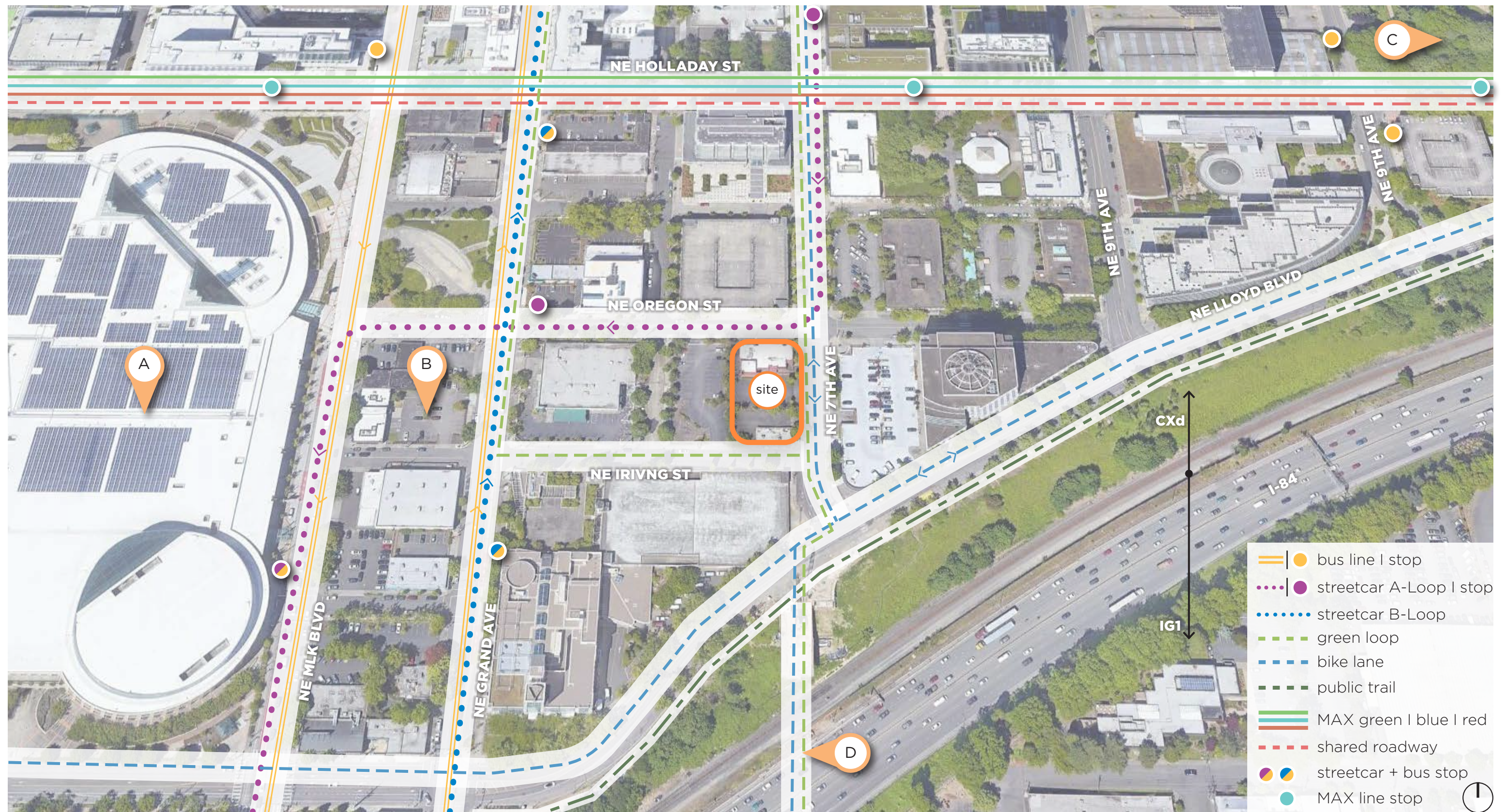
PARKING SUMMARY

Central City Parking Sector	Lloyd District Subdistrict, 2
Minimum Parking	None
Maximum Parking	1.2 spaces per dwelling unit
Distance from Transit Stop	Approximately 415 feet - Streetcar Approximately 800 feet - Bus Approximately 750 feet - MAX
Loading	One Standard A or Two Standard B

ZONING CODE REQUIREMENTS

Ecoroof	Required
Outdoor Area	Not Required
Building Length + Facade Articulation	Not Required
Ground Floor Active Use	Not Required
Ground Floor Window Standard	40% of wall area facing streets, measured from 2 feet to 10 feet above the finished grade
Upper Floor Window Standard	15% of wall area facing streets, measured from 10 feet above the finished grade
Pedestrian Standards	Connections to abutting streets apply on all frontages unless at least 50% of facade is within 10 feet of street
Transit Street Main Entrance	Transit street standards apply on NE Oregon Street Need entrance facing the street, or at up to 45 degree angle, or face courtyard within 60 feet of transit street
Bird-Safe Exterior Glazing	Required
Low-Carbon Building	Required
Inclusionary Zoning	Required
Design Review	Required
Applicable Design Guidelines	Central City Fundamental Design Guidelines Lloyd District Design Guidelines





- A convention center
- B convention center plaza
- C holladay park
- D Earl Blumenauer pedestrian + bike bridge (under construction)





EXISTING SITE



EXISTING SITE CONTEXT







block 103



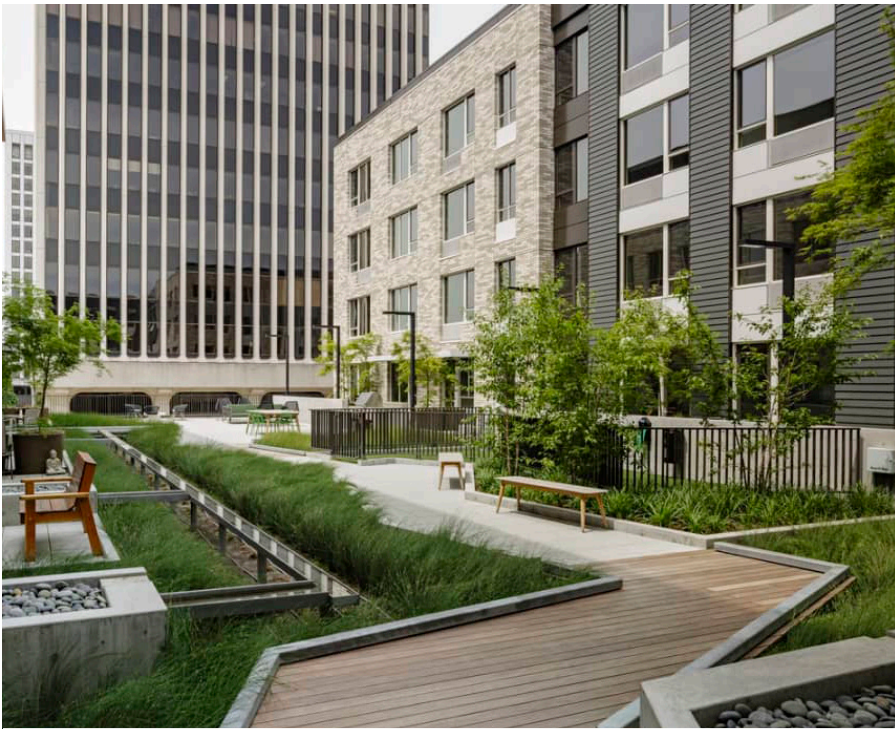
block 90



aster tower



velomor

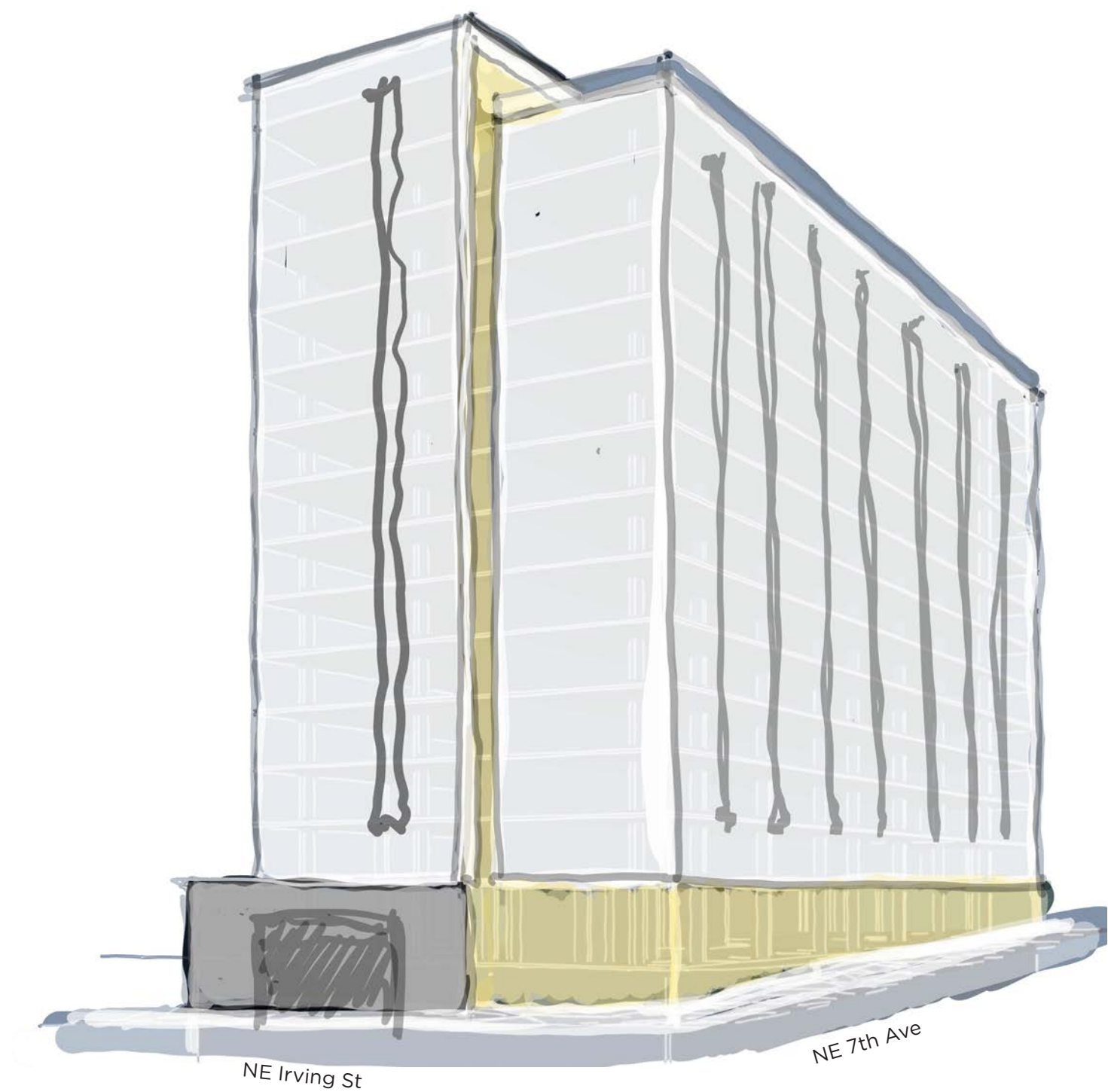


elwood



north vista





Preliminary design sketch



Progression design model

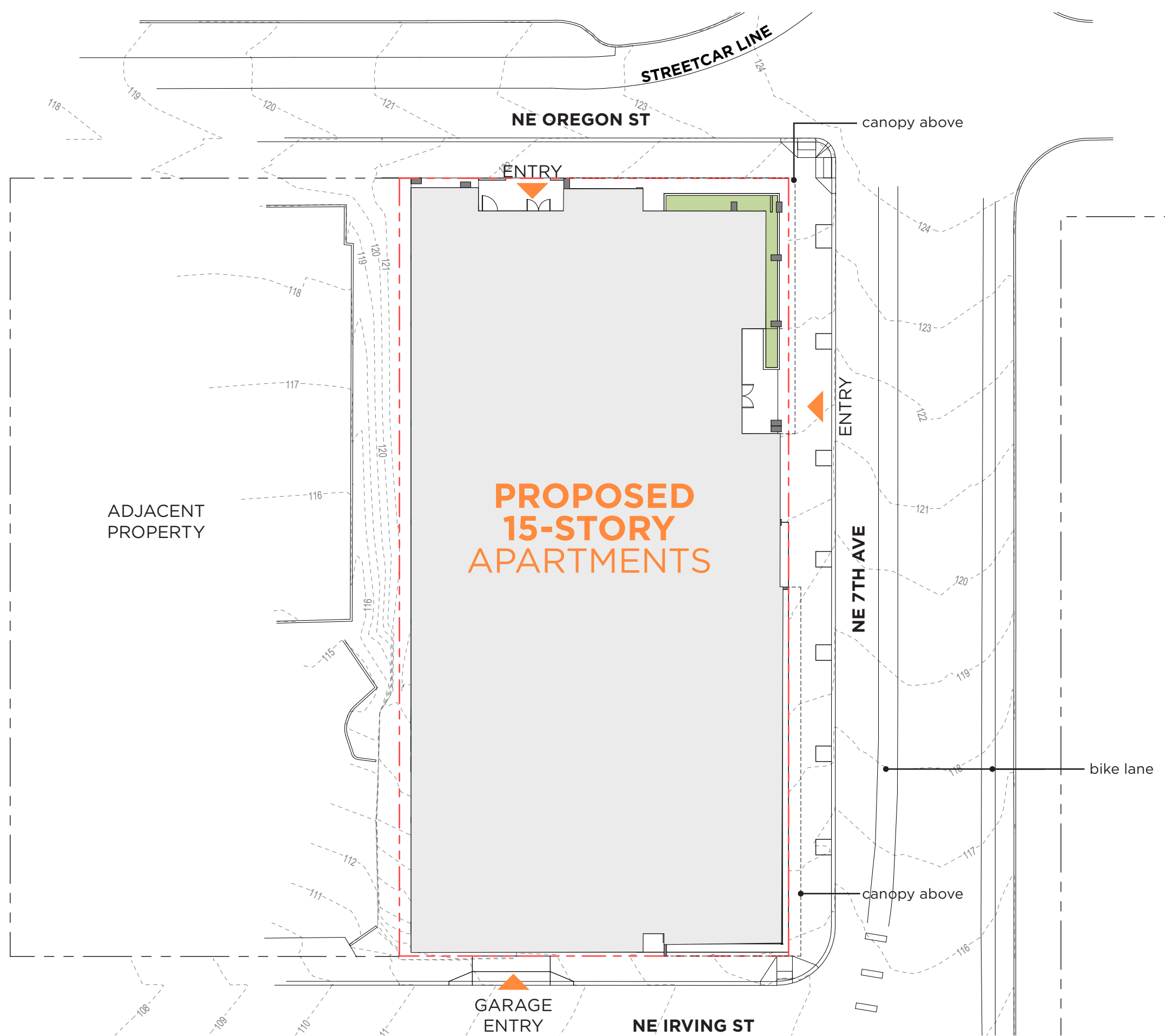




Progression design model



Proposed design model



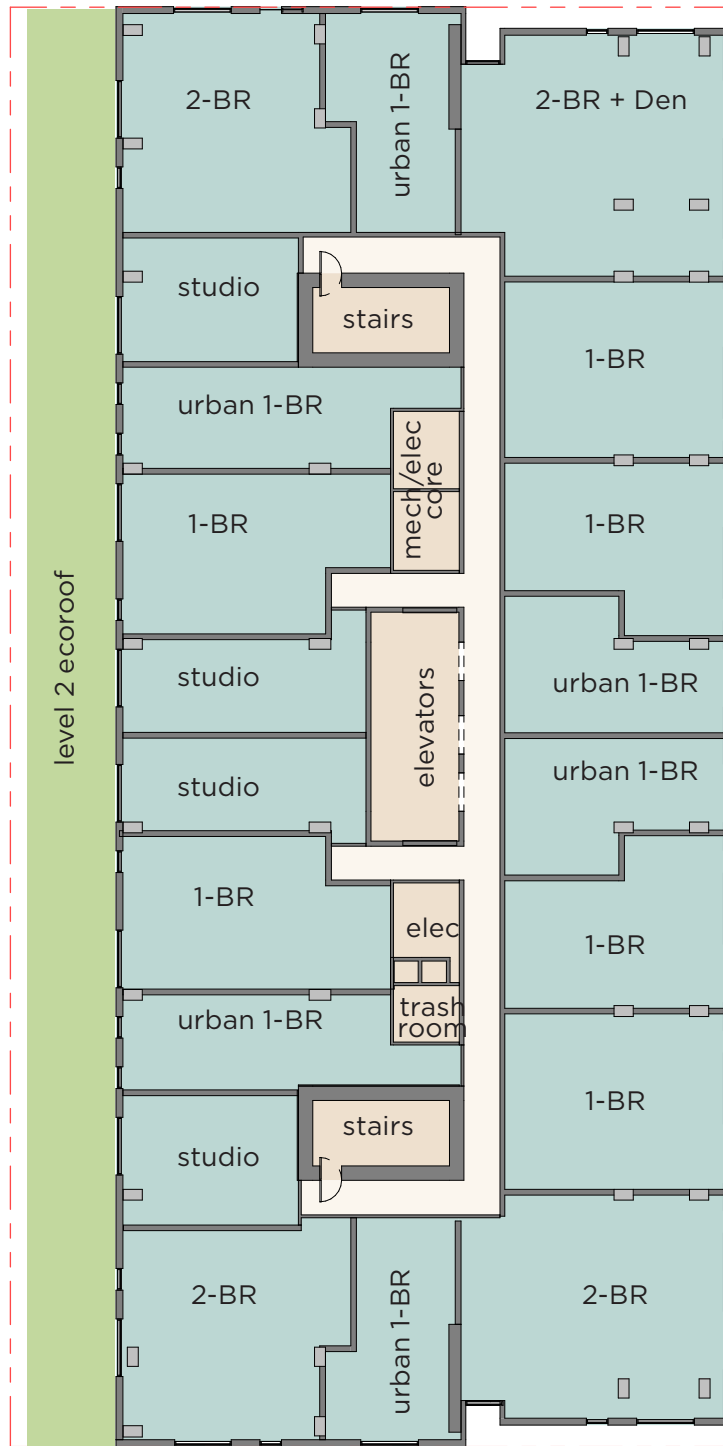


Floor Plan Level P1

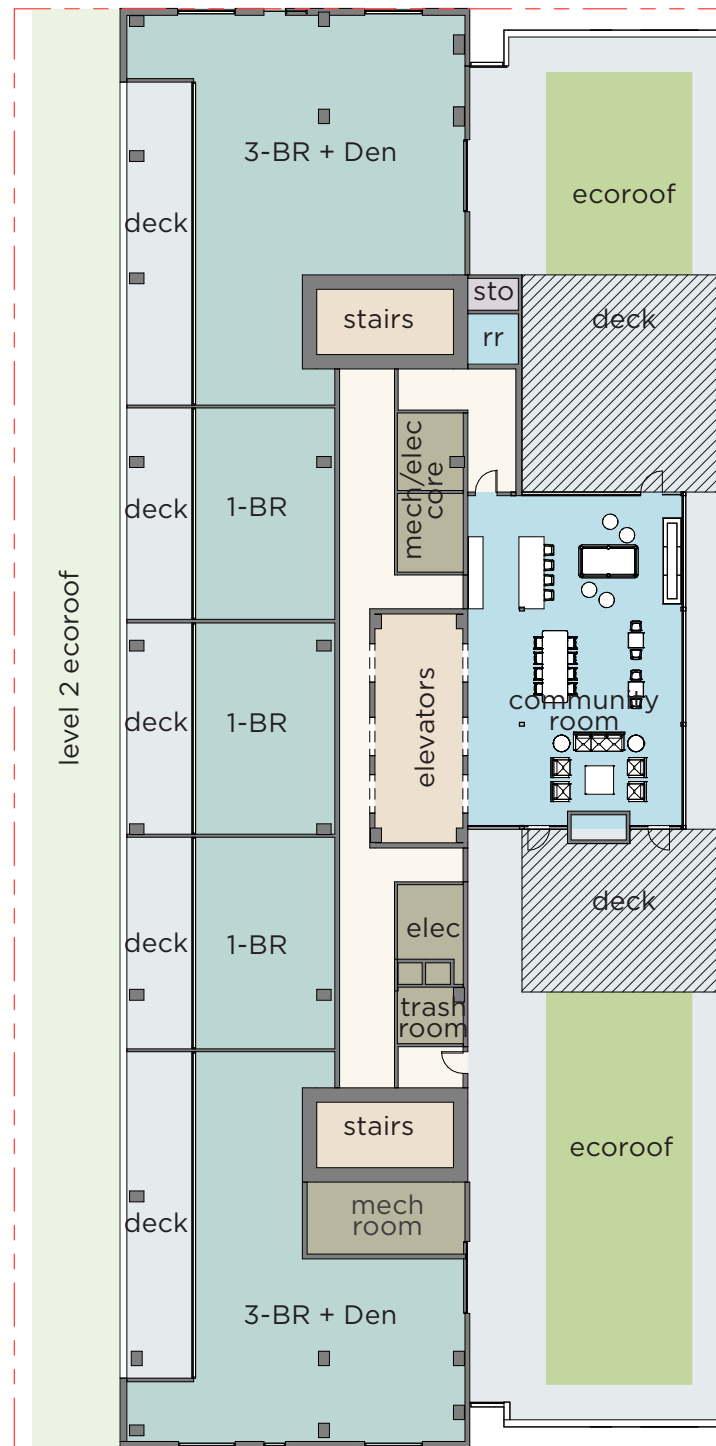


Floor Plan Level 1

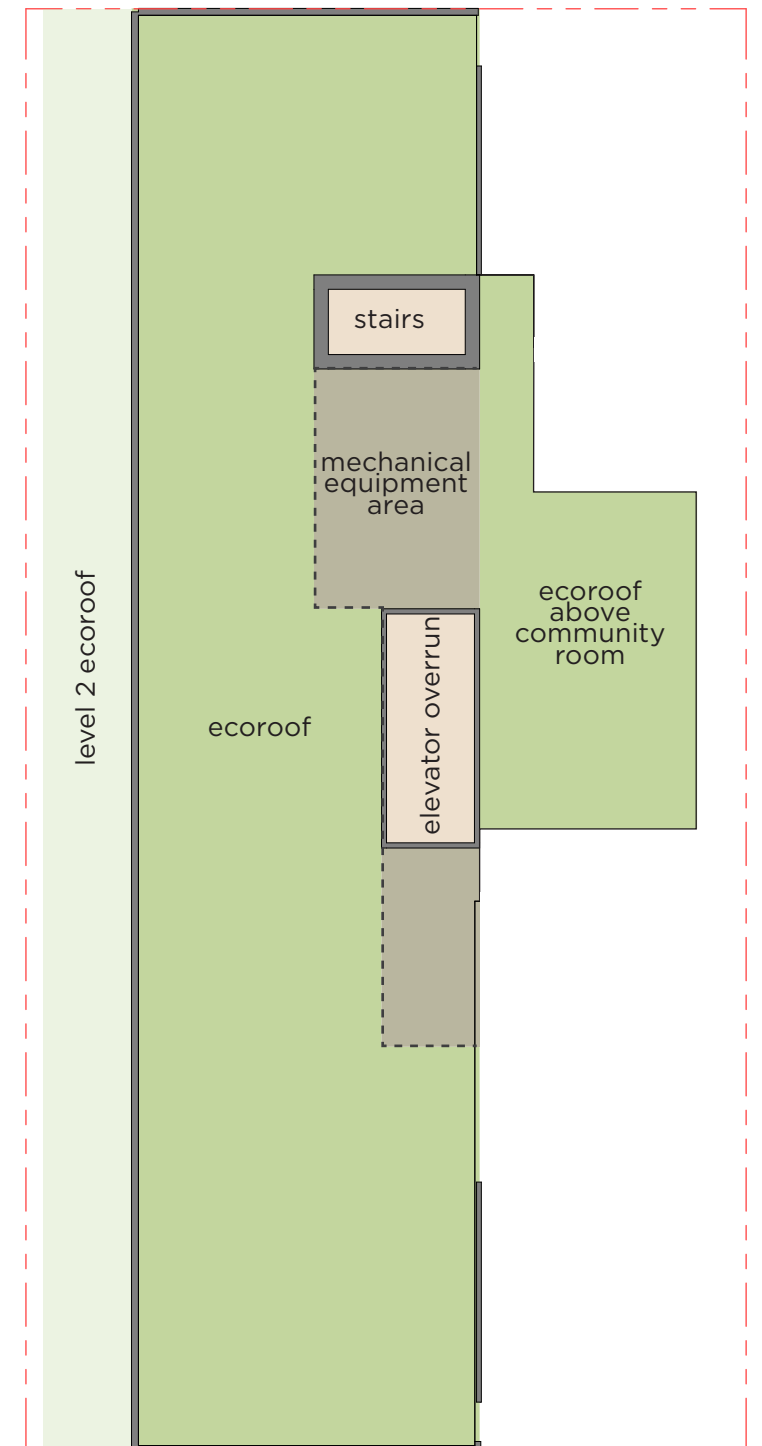




Floor Plan Levels 2-14



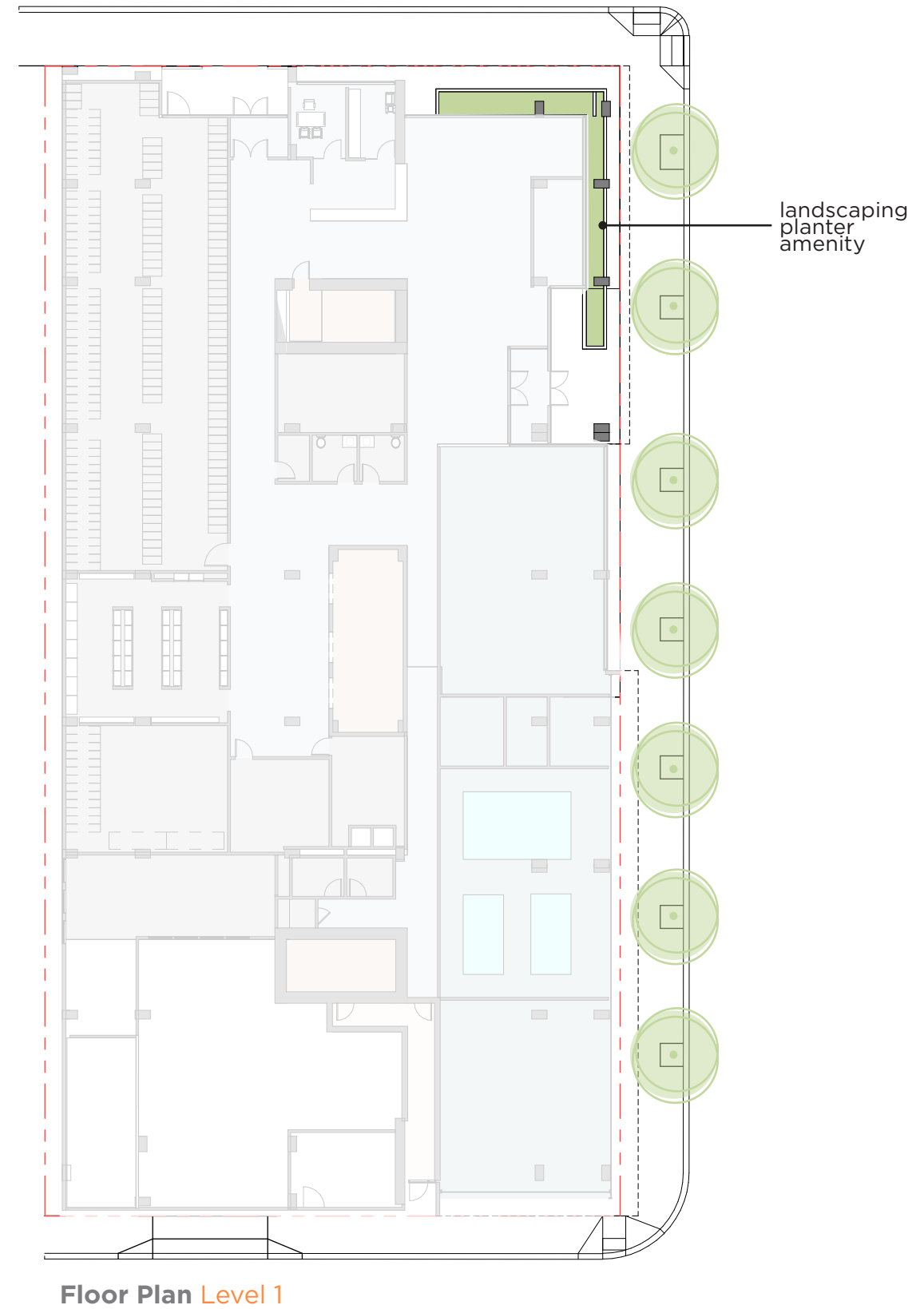
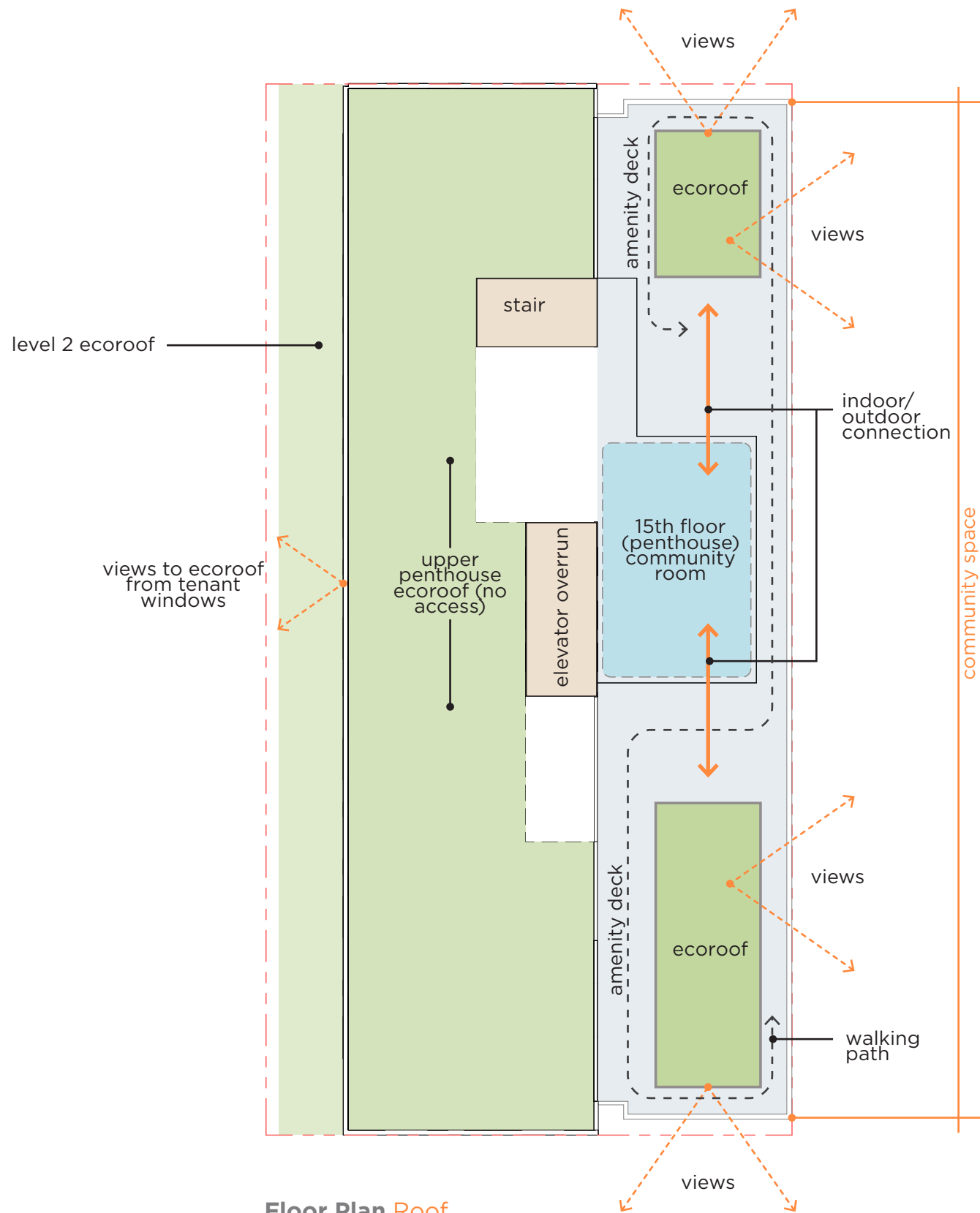
Floor Plan Level Penthouse

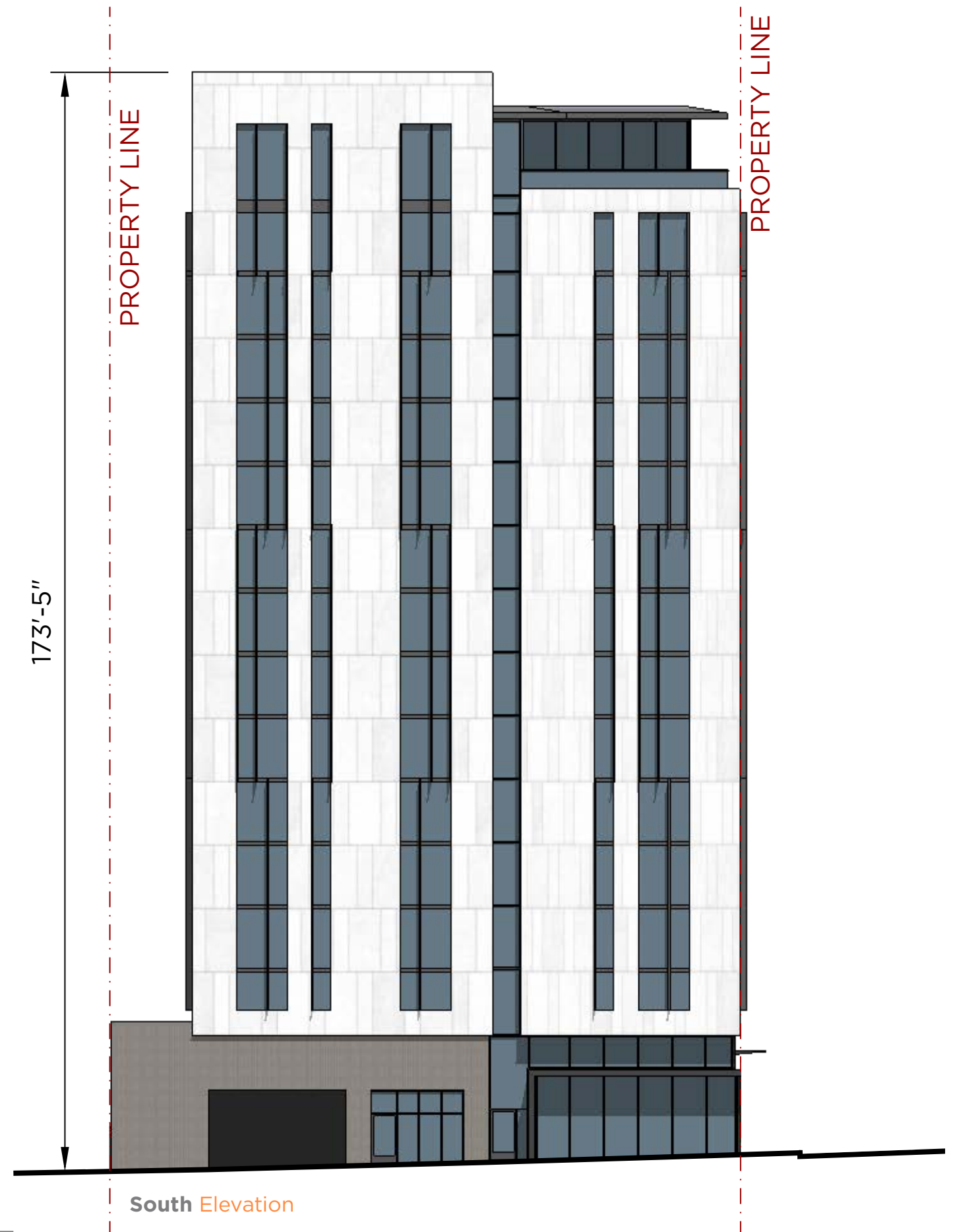


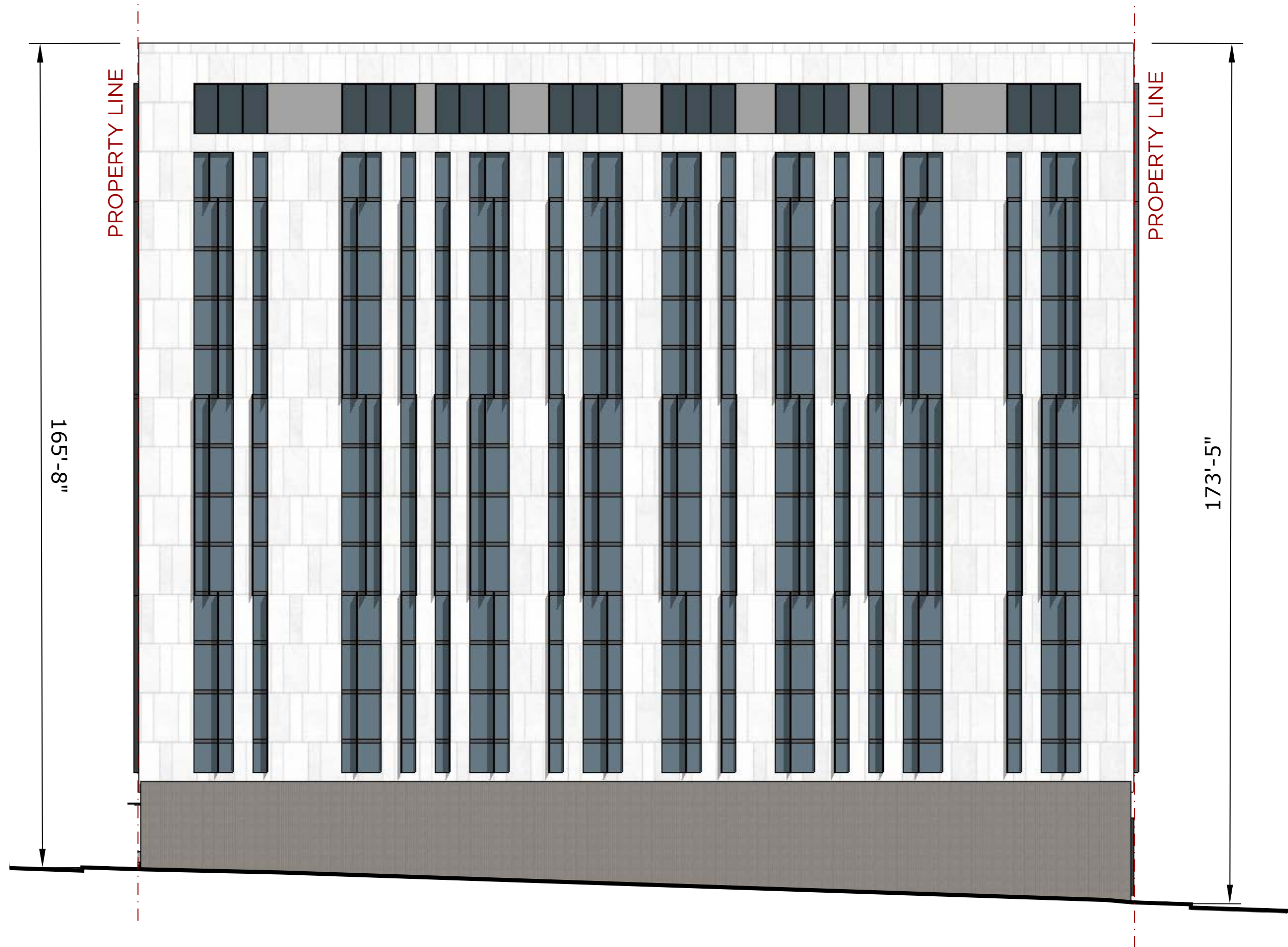
Floor Plan Roof

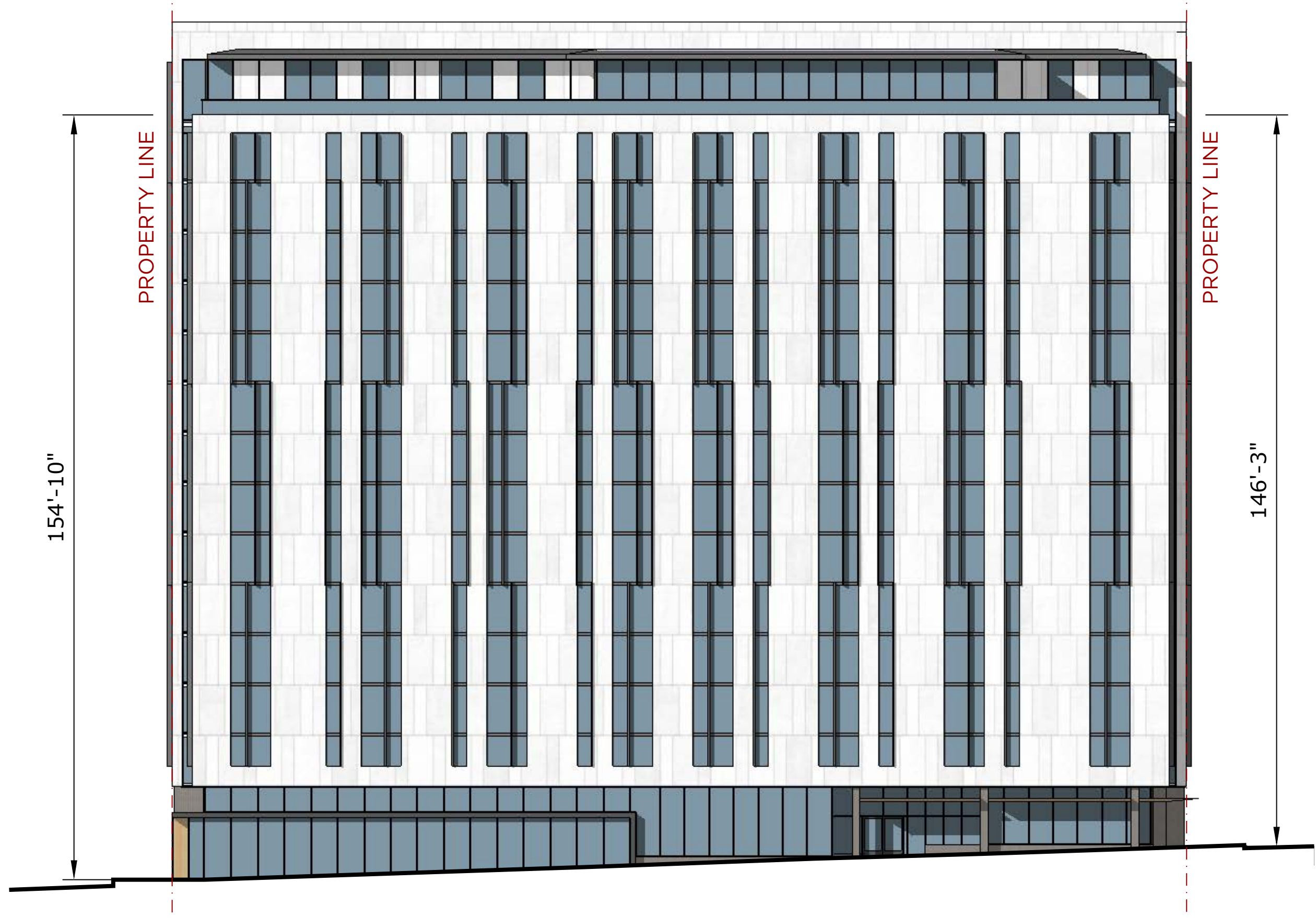








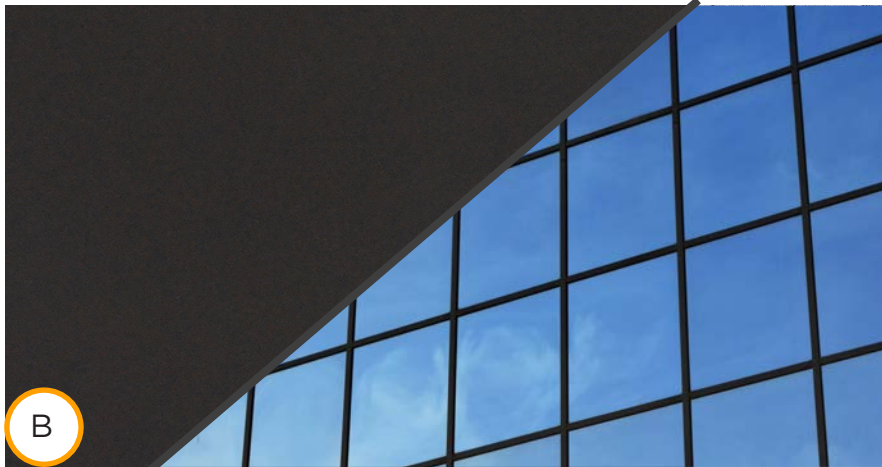








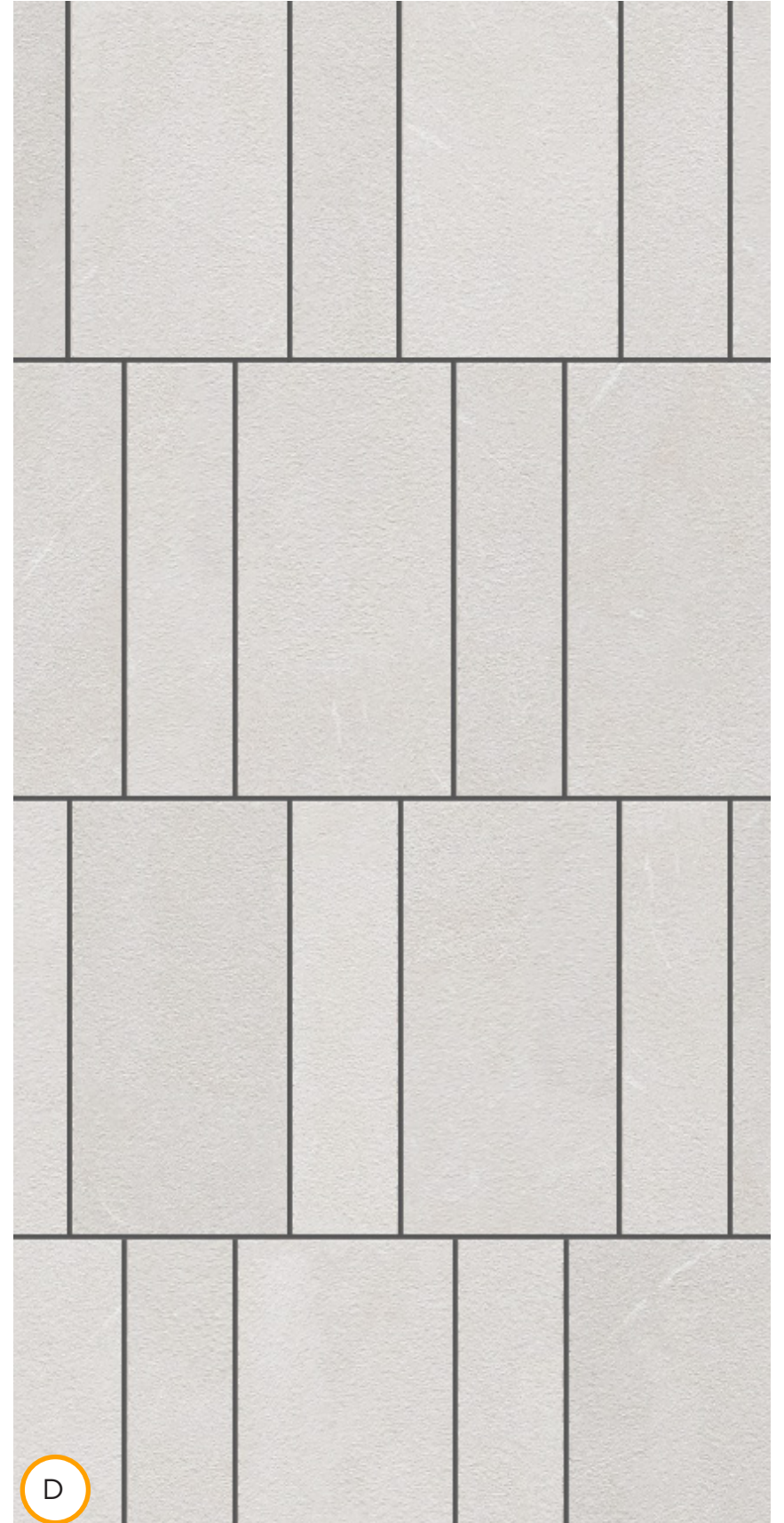
A  
Natural Wood



B  
Dark Bronze Metal Accents + Window Mullions + Glazing



C  
Dark Masonry



D  
Architectural Fiber Cement Panels  
(Basis of Design: Equitone)









MODIFICATION I PARKING SPACE DIMENSION

Proposed modification:

Proposal seeks a modification to the 8’-6” x 16’-0” parking space requirement in those spaces with structural columns, to allow encroachment of columns into the parking space width.

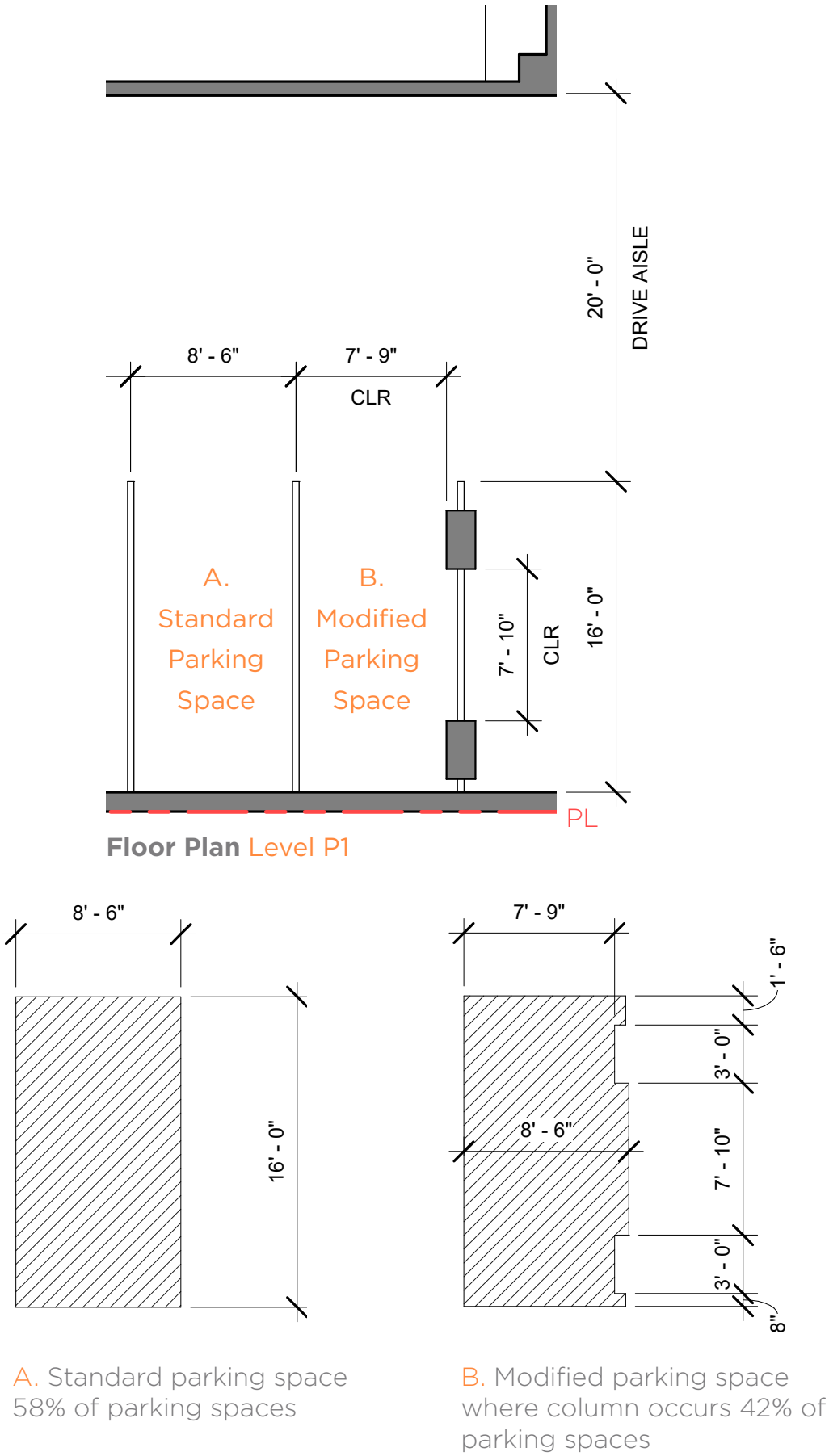
Parking columns occur at 22 spaces per typical floor, which is 42% of parking spaces.

Note that all parking spaces will meet the 16’-0” minimum length requirement, and all parking spaces will have stripes located at an 8’-6” spacing width.

Certain parking spaces will have one-half of a column width encroaching into one side of the space. As the columns are 1’-6” wide, that results in a 9” reduction of the parking space width where the column occurs.

Meeting the Design Guidelines:

- 1. Arranging the structural columns requires attention to how it will impact the plan layouts of the many levels of the building: residential units, parking levels, ground floor, and the associated exterior walls of the ground floor. The columns have been arranged to the greatest benefit of the ground floor to have an inset of the exterior wall on the ground floor. This results in having a column inset into the parking space stripe by that same distance. This provides more area between the building and the NE 7th Avenue Right of Way. This meets Design Guideline A8, Contribute to a Vibrant Streetscape, by integrating building setbacks with adjacent sidewalks.
- 2. This modification meets the purpose of the parking space width standard, as it provides the required 8’-6” width at the central portion of the parking space, where there are car door swings.
- 3. The impact resulting from this modification has been mitigated as much as possible by thoughtfully locating the columns to be clear of the car door opening area.



# MACKENZIE.

503.224.9560 | [mcknze.com](https://mcknze.com)

Architecture · Interiors · Structural Engineering · Civil Engineering  
Land Use Planning · Transportation Planning · Landscape Architecture

Copyright © 2022 Mackenzie, All Rights Reserved.



# ZONING



Site



Recreational Trails

For Zoning Code in effect Post August 1, 2021

CENTRAL CITY PLAN DISTRICT  
LLOYD SUB DISTRICT

File No.	EA 22 - 119814 DA
1/4 Section	2931
Scale	1 inch = 200 feet
State ID	1N1E35BC 2200
Exhibit	B Mar 08, 2022



**Master Development**  
**NE 7th Ave Apartments**  
Design Advice Request | 04.07.2022

CONTENTS

CONTENTS + PROJECT TEAM	C.2
PROJECT DESCRIPTION	C.3
ZONING SUMMARY	C.4
URBAN CONTEXT	C.5
EXISTING SITE + PHOTOS	C.6
CONTEXT PRECEDENTS	C.7
PROJECT EVOLUTION	C.8
PROPOSED SITE PLAN	C.10
FLOOR PLANS	C.11
OUTDOOR AMENITIES	C.13
BUILDING ELEVATIONS	C.14
MATERIALS	C.17
MASSING	C.18
MODIFICATION	C.20

PROJECT TEAM

MASTER DEVELOPMENT, LLC	Applicant
MACKENZIE	Architecture   Interiors   Structural   Civil   Landscape   Planning   Traffic
SAZAN GROUP	Mechanical   Electrical   Plumbing

PROJECT SUMMARY

Address	701 + 777 NE 7th Avenue Portland, OR 97232
Property Tax IDs	R182216 (701 NE 7th Ave) + R182217 (777 NE 7th Ave)
Tax Lots	1N1E35BC2200 (701 NE 7th Ave) + 1N1E35BC2100 (777 NE 7th Ave)
Size	20,000 SF

PROJECT NARRATIVE

The proposed development is for a 15-story multi-family apartment building located at the southwest corner of NE 7th Avenue and NE Oregon Street in the Lloyd District. Tax lot 1N1E35BC2200 is currently occupied by an office building, tax lot 1N1E35BC2100 is currently occupied by a fenced and vacant building that appears to most recently be used as a food and beverage establishment.

The site is located in the Central Commercial (CX) zone, with a d (design) overlay, and is within the Central City Plan District (Lloyd District design subdistrict).

The proposed building use includes 13 stories of apartment units with 20 units on each floor and 5 units on the 15th floor (265 total units). The ground floor of the building will provide resident amenities while community room, an amenity deck and ecoroof will occupy the roof.

Below grade parking is included. Vehicle access to the below-grade garage will be provided from NE Irving Street. While the exact number of parking spaces is still under development the provided parking will be less than 1.2 spaces per unit.

Pedestrian entrances are provided along NE Oregon Street and NE 7th Avenue with each entrance set back and covered to provide weather protection.

The Portland Streetcar (A Loop) runs along the site’s NE Oregon Street frontage.

With the site located along the Portland Streetcar and Green Loop, careful consideration has been given to the pedestrian friendly streetscape.





ZONING SUMMARY

Base Zone	Central Commercial (CX)
Overlay	d (Design)
District	Central City (CC)
Subdistrict	Lloyd District
Base Zone FAR	4:1
Base Zone Height	75 feet
Central City FAR	9:1
Central City Height	250 feet
Inclusionary Housing FAR Bonus	3:1
Inclusionary Housing Height Bonus	75 feet
FAR Total	12:1
Height Total	325 feet
Minimum Setbacks	0 feet
Maximum Setbacks	Building must extend to within 12 feet of street lot line for 75% of lot line
Maximum Lot Coverage	100% of Site Area
Minimum Landscape Area	0%

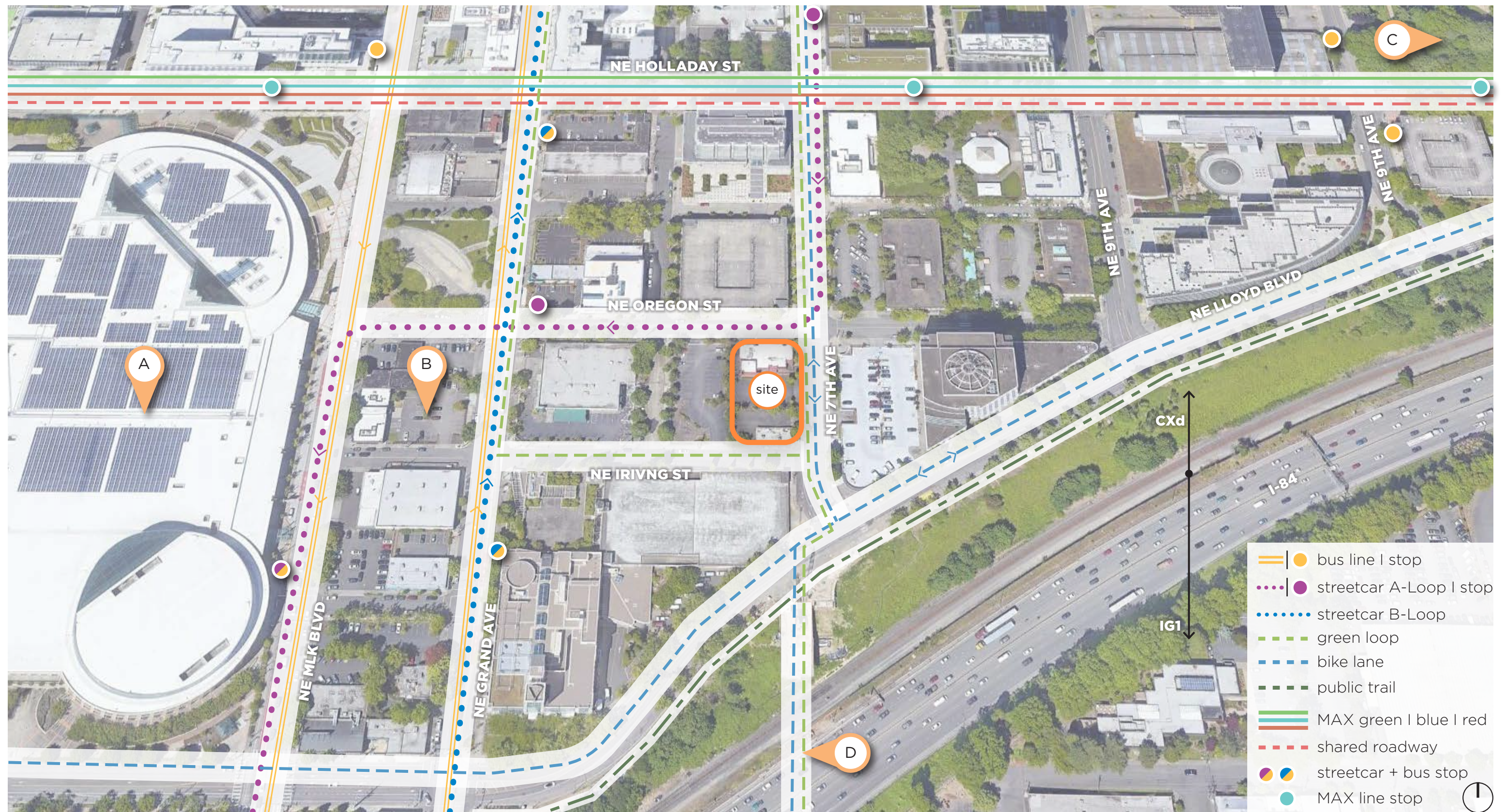
PARKING SUMMARY

Central City Parking Sector	Lloyd District Subdistrict, 2
Minimum Parking	None
Maximum Parking	1.2 spaces per dwelling unit
Distance from Transit Stop	Approximately 415 feet - Streetcar Approximately 800 feet - Bus Approximately 750 feet - MAX
Loading	One Standard A or Two Standard B

ZONING CODE REQUIREMENTS

Ecoroof	Required
Outdoor Area	Not Required
Building Length + Facade Articulation	Not Required
Ground Floor Active Use	Not Required
Ground Floor Window Standard	40% of wall area facing streets, measured from 2 feet to 10 feet above the finished grade
Upper Floor Window Standard	15% of wall area facing streets, measured from 10 feet above the finished grade
Pedestrian Standards	Connections to abutting streets apply on all frontages unless at least 50% of facade is within 10 feet of street
Transit Street Main Entrance	Transit street standards apply on NE Oregon Street Need entrance facing the street, or at up to 45 degree angle, or face courtyard within 60 feet of transit street
Bird-Safe Exterior Glazing	Required
Low-Carbon Building	Required
Inclusionary Zoning	Required
Design Review	Required
Applicable Design Guidelines	Central City Fundamental Design Guidelines Lloyd District Design Guidelines





- A convention center
- B convention center plaza
- C holladay park
- D Earl Blumenauer pedestrian + bike bridge (under construction)

## Master Development | NE 7th Ave Apartments

04.07.2022

Urban Context  
© 2022 Mackenzie | 2210364.00

**MACKENZIE.**  
EA 22-119814 DA | C.5  
EA 22-119814 DA - Exhibit C.5





EXISTING SITE



EXISTING SITE CONTEXT







block 103



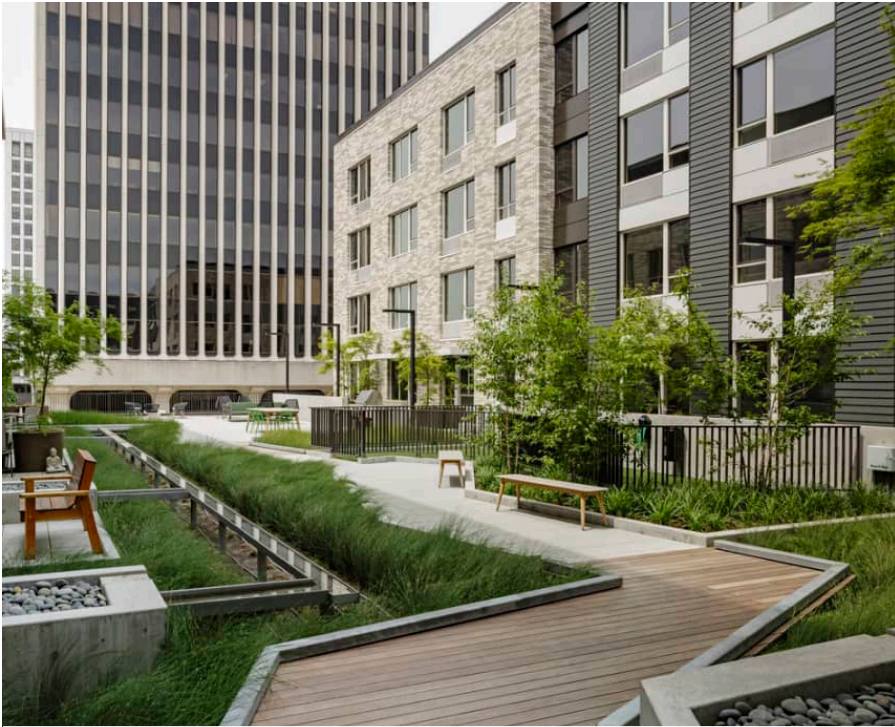
block 90



aster tower



velomor

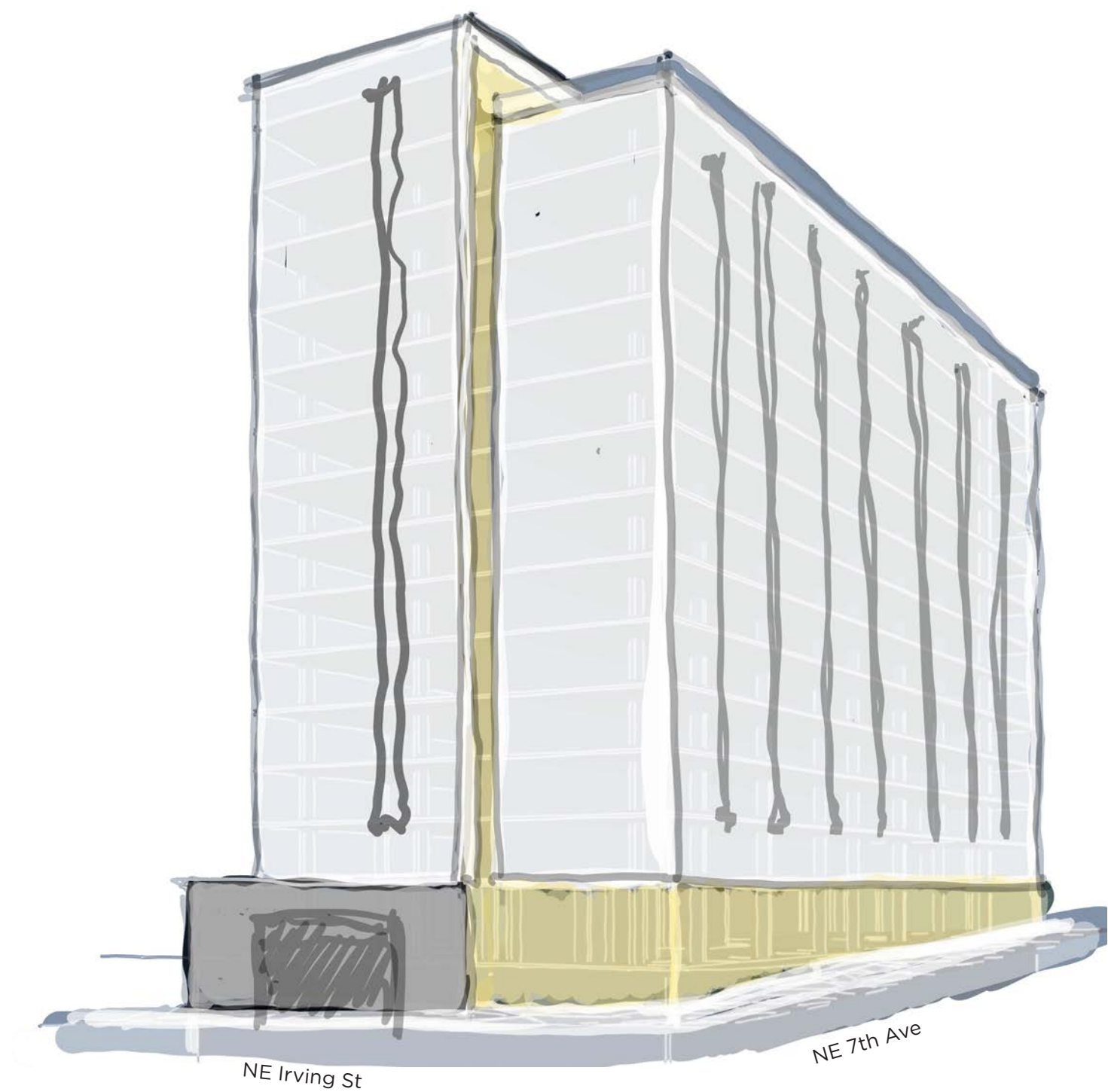


elwood



north vista





Preliminary design sketch



Progression design model

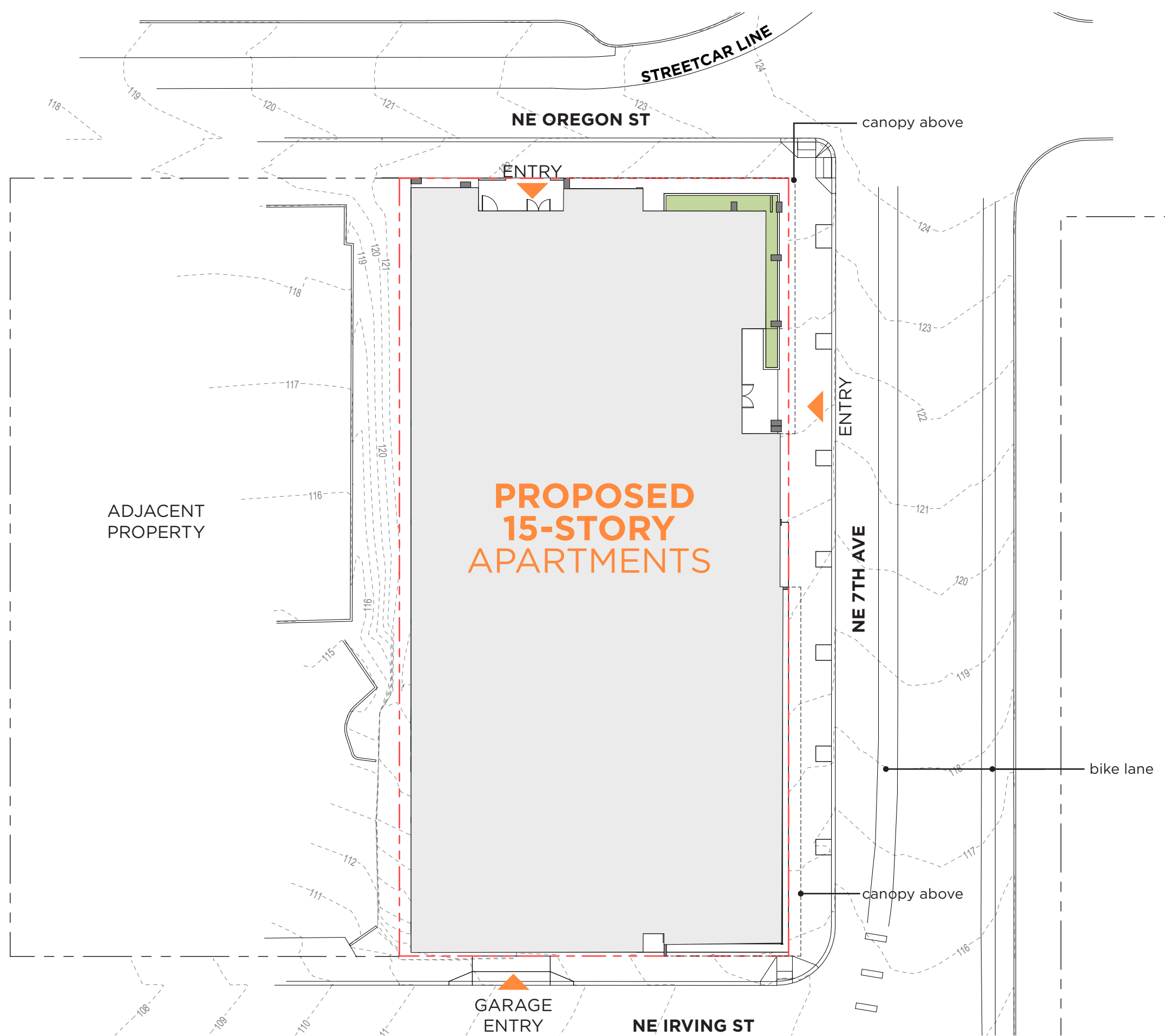




Progression design model



Proposed design model





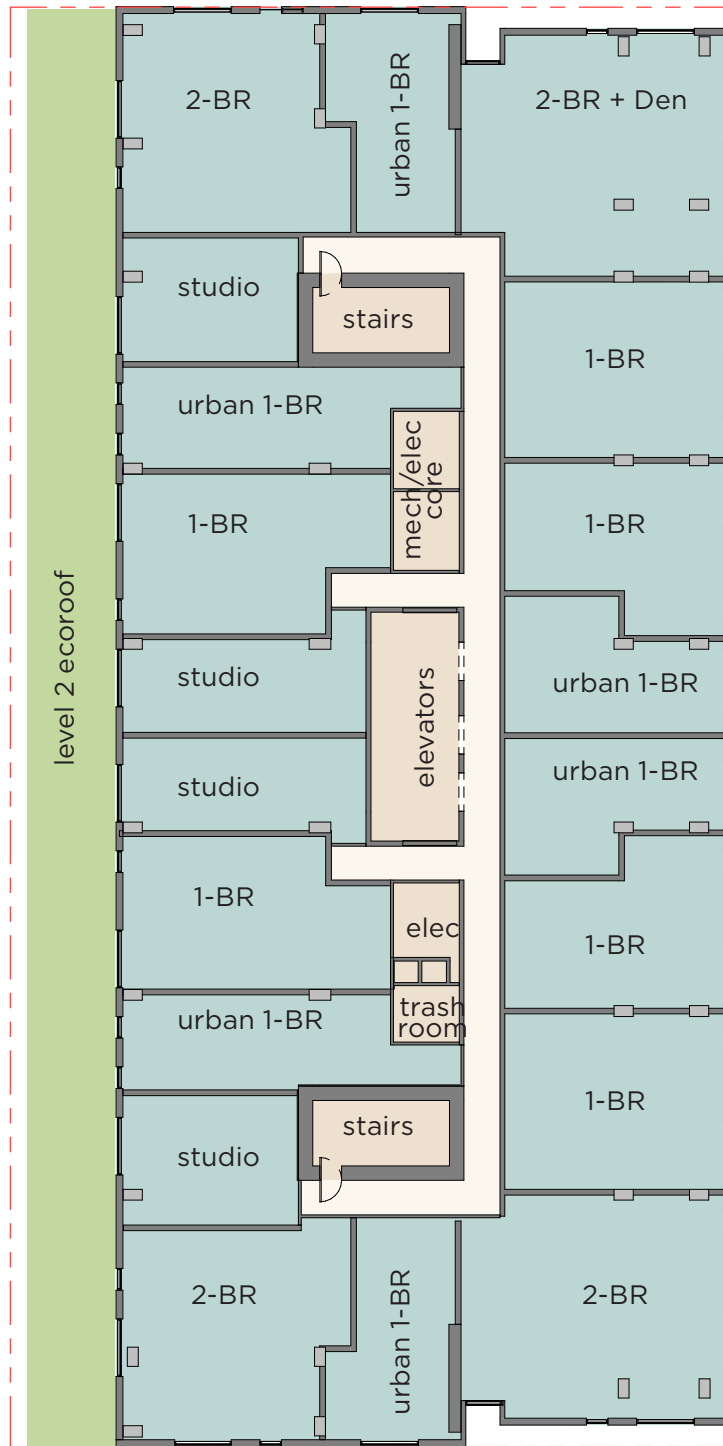


Floor Plan Level P1

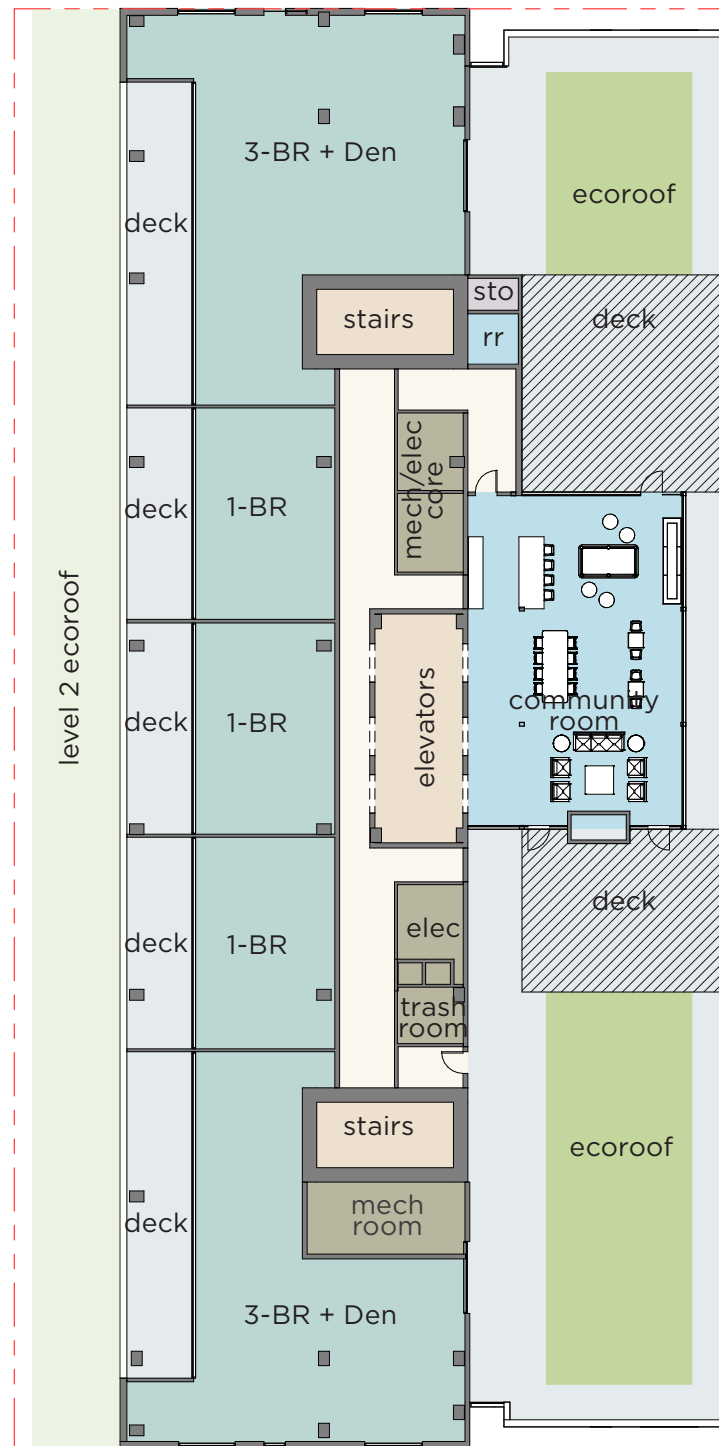


Floor Plan Level 1

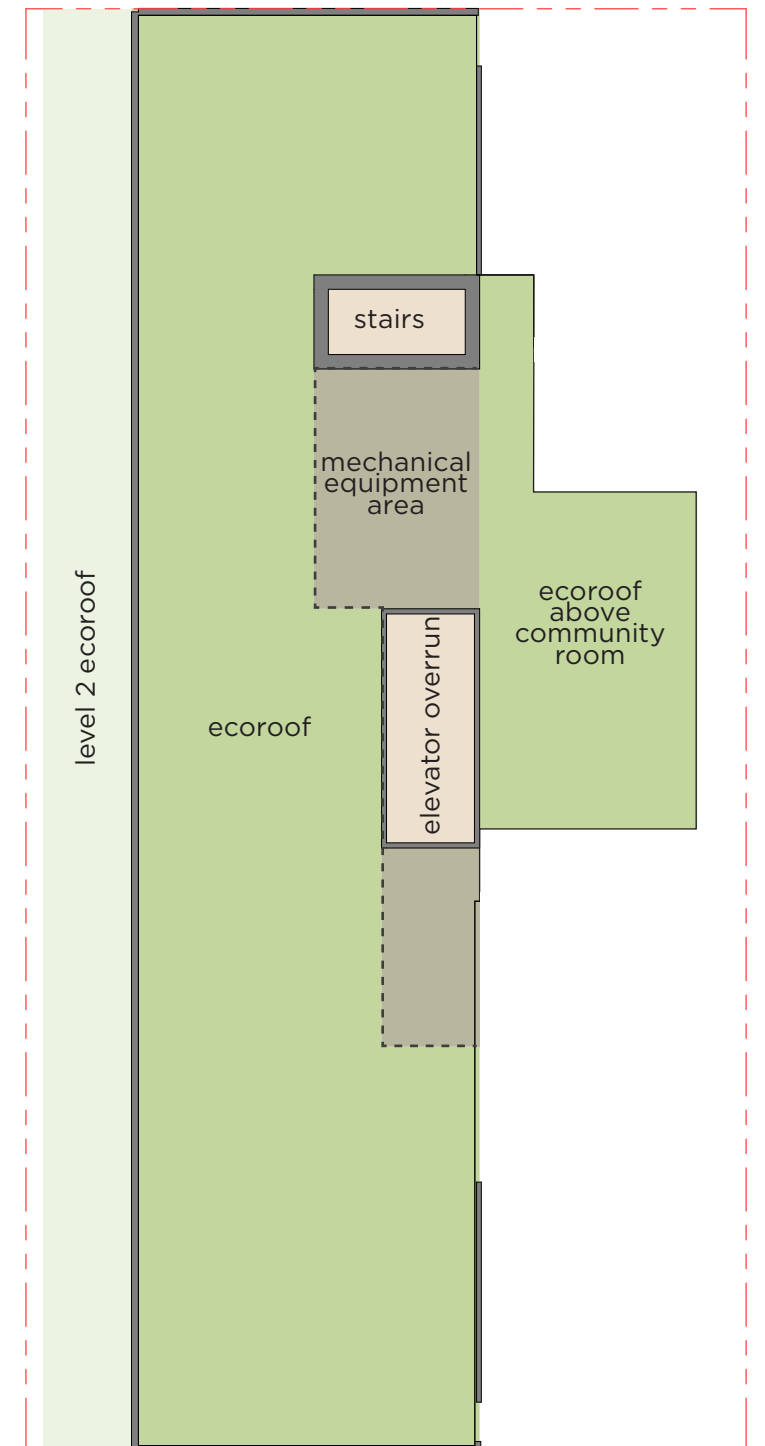




Floor Plan Levels 2-14



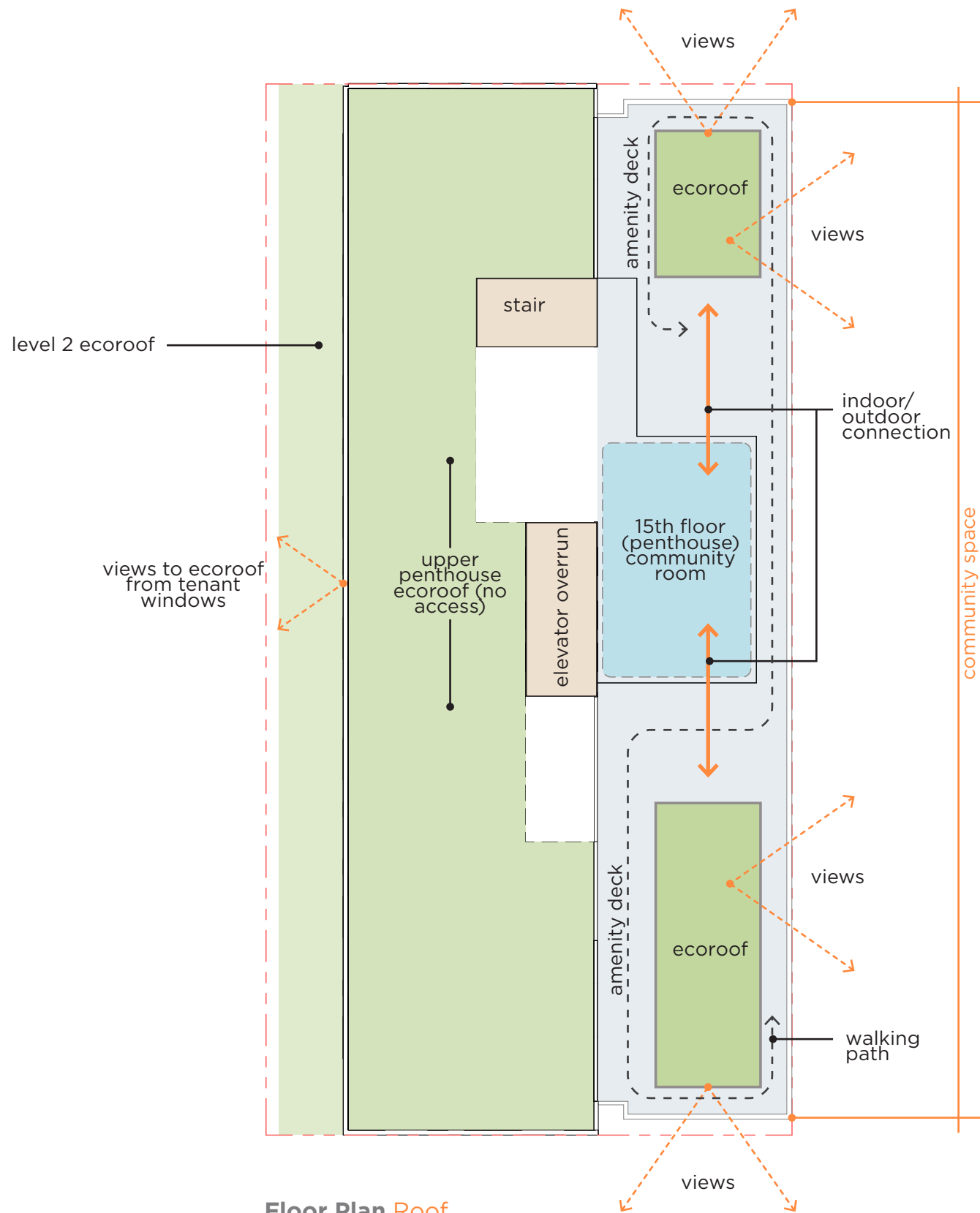
Floor Plan Level Penthouse



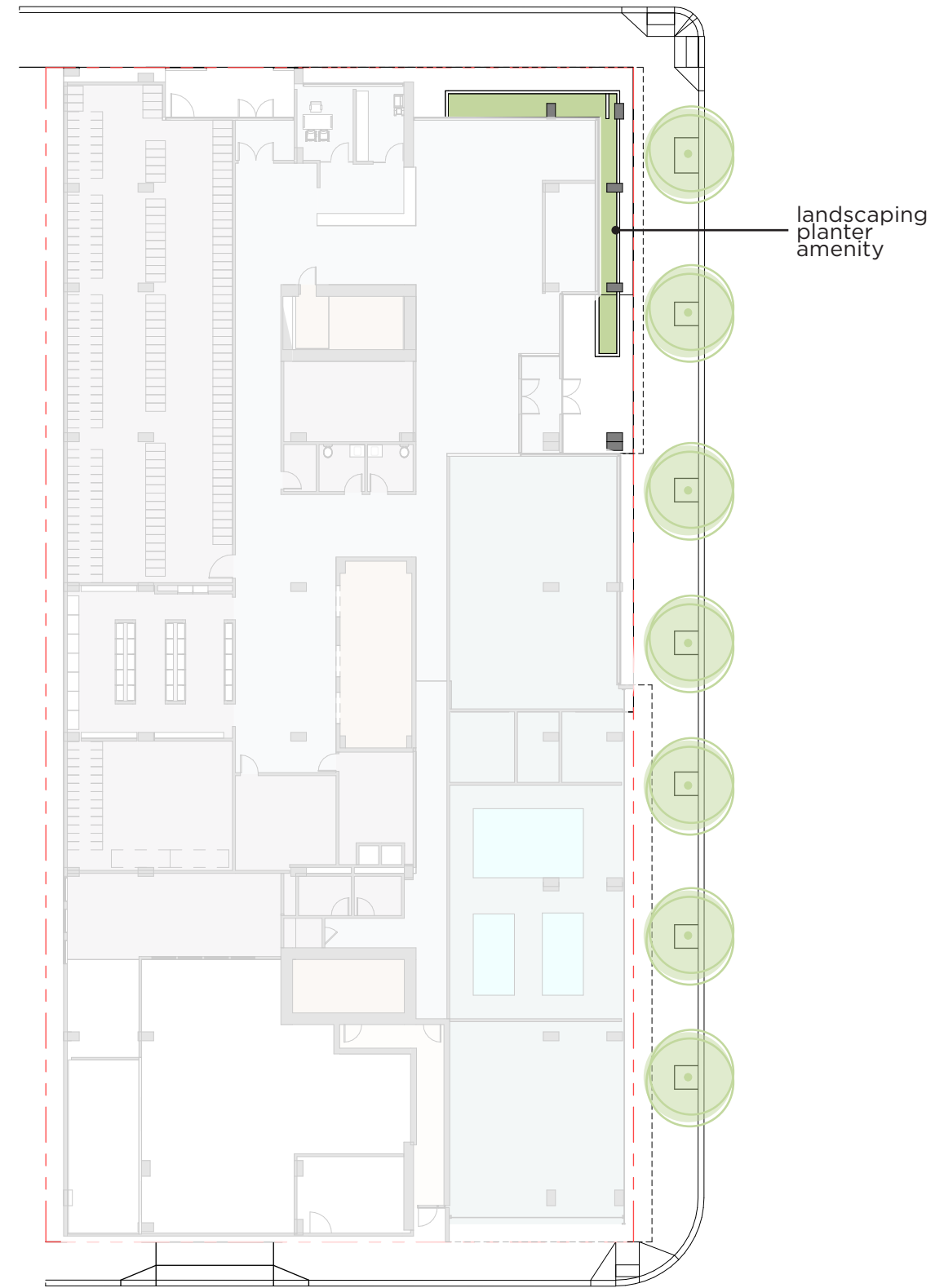
Floor Plan Roof







Floor Plan Roof

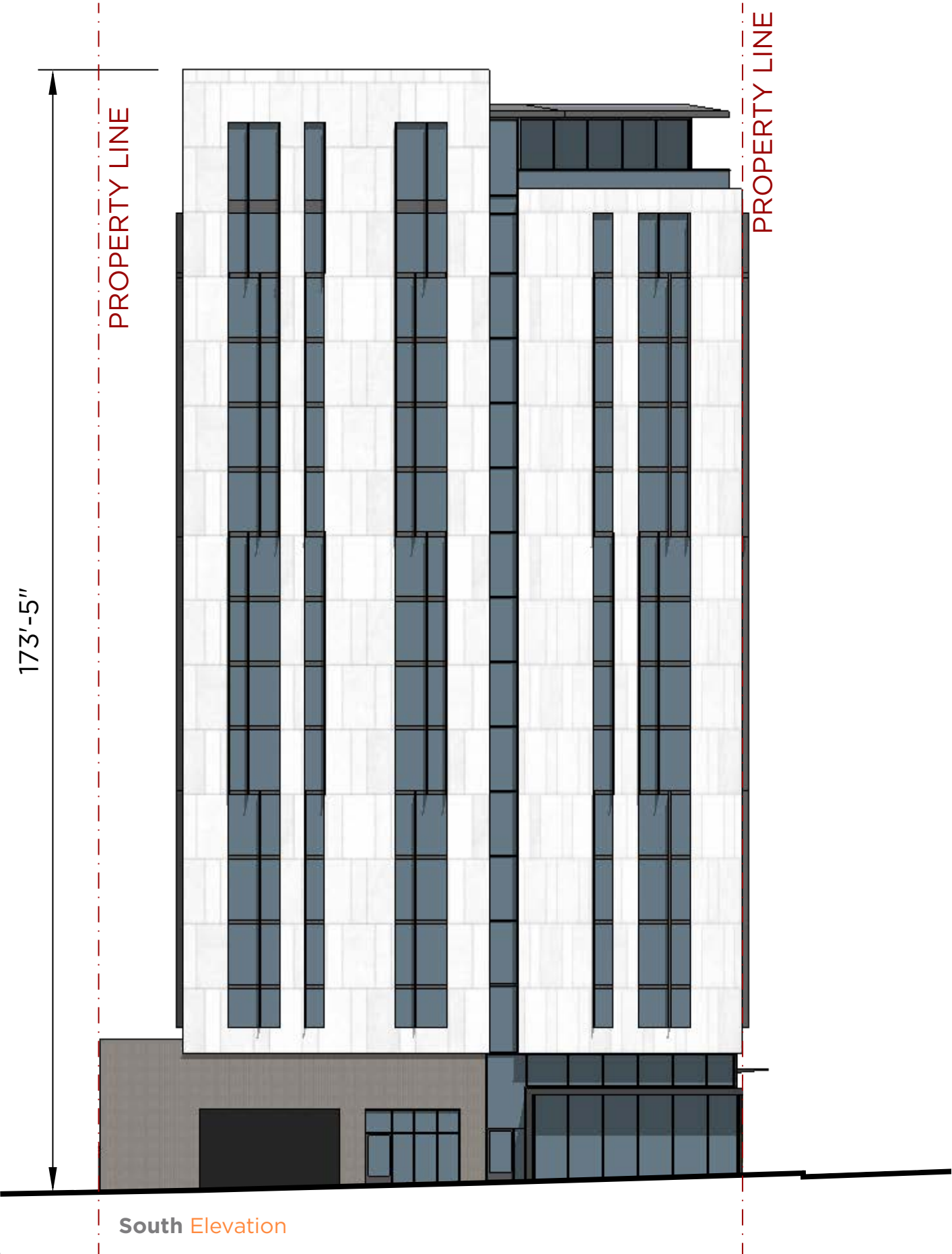


Floor Plan Level 1



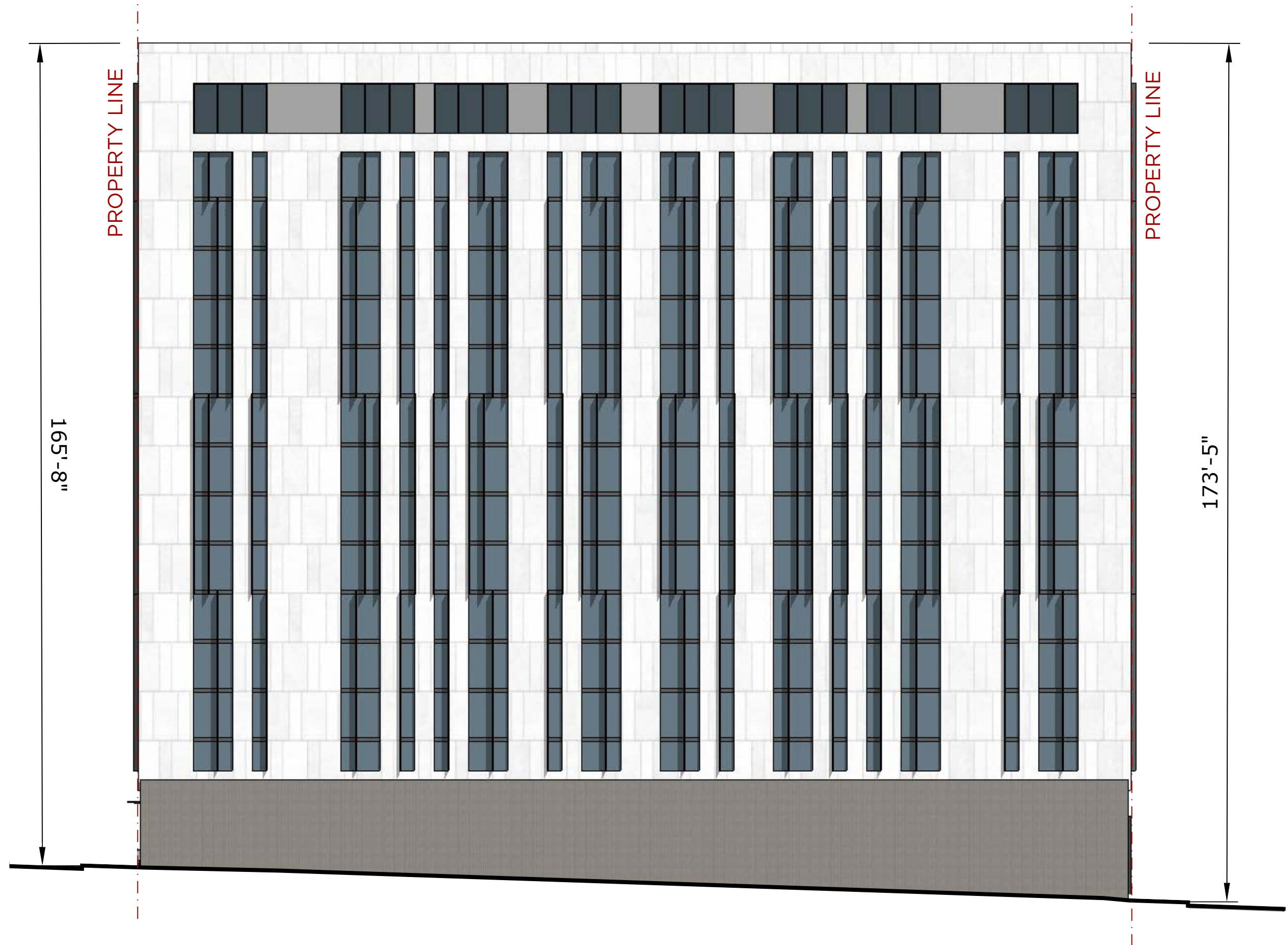


North Elevation



South Elevation



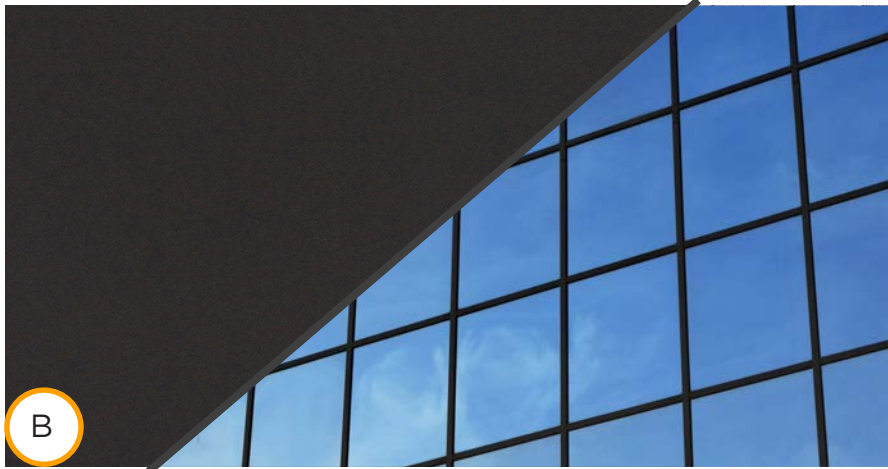








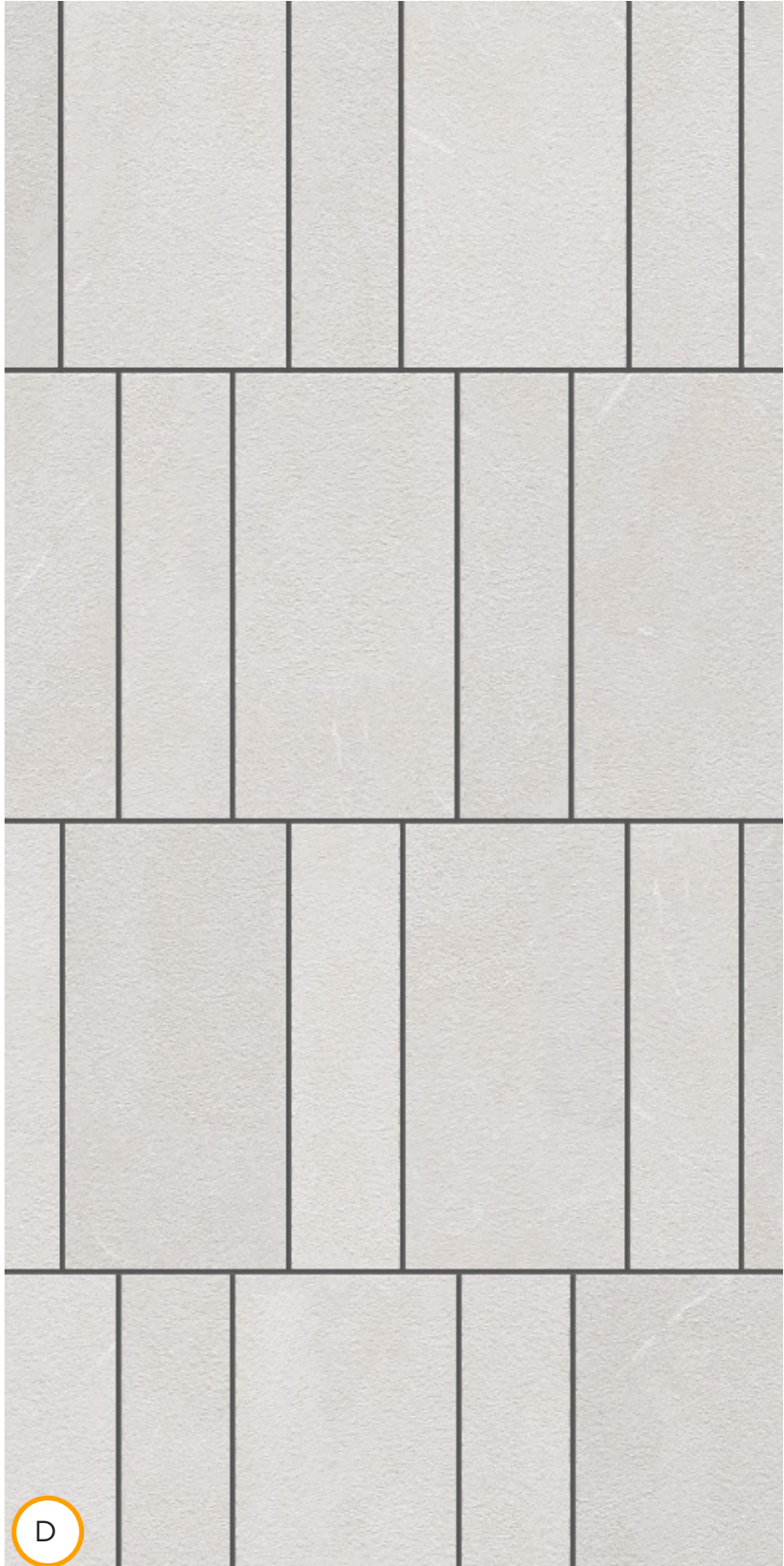
A  
Natural Wood



B  
Dark Bronze Metal Accents + Window Mullions + Glazing



C  
Dark Masonry



D  
Architectural Fiber Cement Panels  
(Basis of Design: Equitone)









MODIFICATION I PARKING SPACE DIMENSION

Proposed modification:

Proposal seeks a modification to the 8’-6” x 16’-0” parking space requirement in those spaces with structural columns, to allow encroachment of columns into the parking space width.

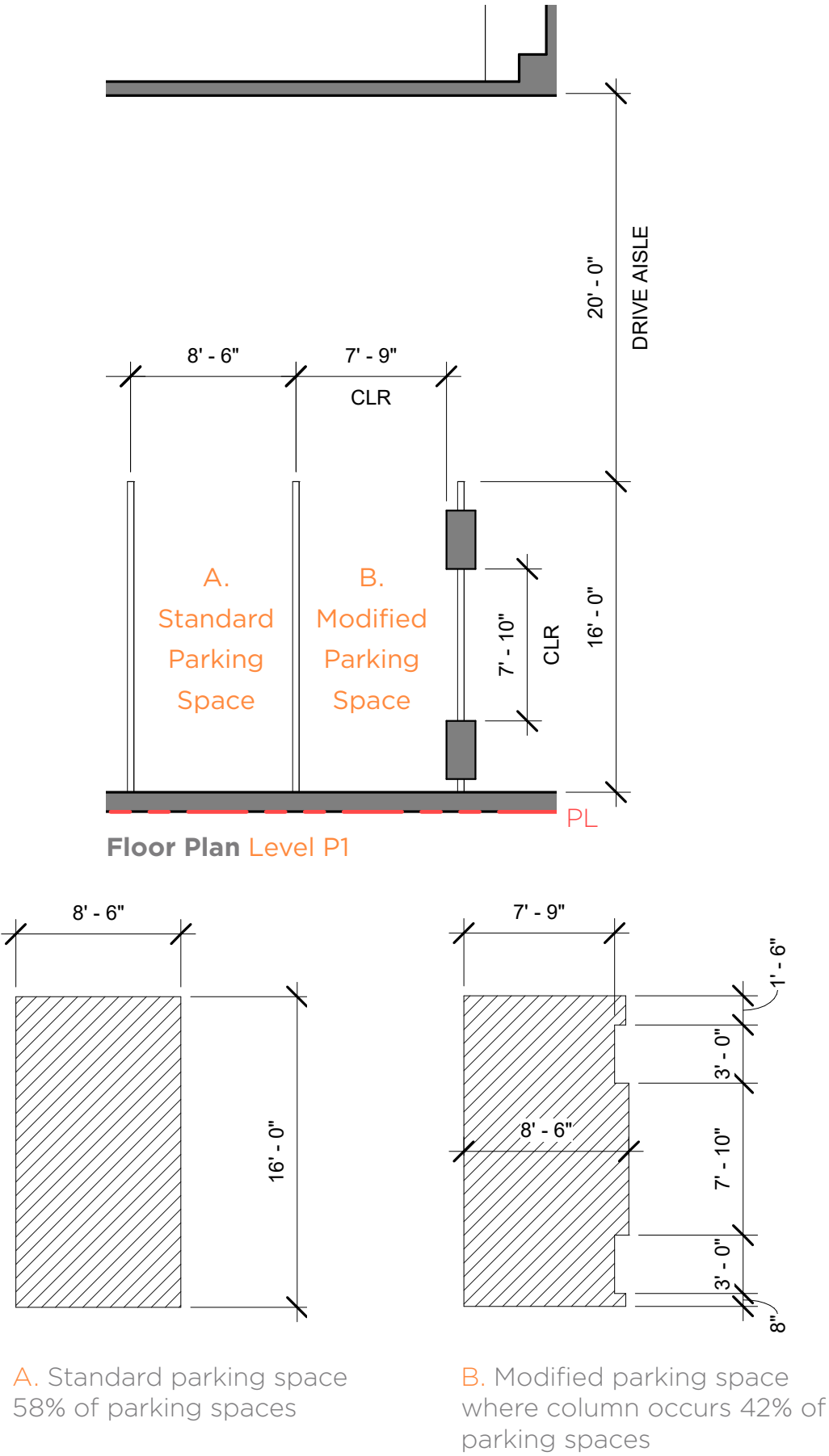
Parking columns occur at 22 spaces per typical floor, which is 42% of parking spaces.

Note that all parking spaces will meet the 16’-0” minimum length requirement, and all parking spaces will have stripes located at an 8’-6” spacing width.

Certain parking spaces will have one-half of a column width encroaching into one side of the space. As the columns are 1’-6” wide, that results in a 9” reduction of the parking space width where the column occurs.

Meeting the Design Guidelines:

- 1. Arranging the structural columns requires attention to how it will impact the plan layouts of the many levels of the building: residential units, parking levels, ground floor, and the associated exterior walls of the ground floor. The columns have been arranged to the greatest benefit of the ground floor to have an inset of the exterior wall on the ground floor. This results in having a column inset into the parking space stripe by that same distance. This provides more area between the building and the NE 7th Avenue Right of Way. This meets Design Guideline A8, Contribute to a Vibrant Streetscape, by integrating building setbacks with adjacent sidewalks.
- 2. This modification meets the purpose of the parking space width standard, as it provides the required 8’-6” width at the central portion of the parking space, where there are car door swings.
- 3. The impact resulting from this modification has been mitigated as much as possible by thoughtfully locating the columns to be clear of the car door opening area.





# MACKENZIE.

503.224.9560 | [mcknze.com](https://mcknze.com)

Architecture · Interiors · Structural Engineering · Civil Engineering  
Land Use Planning · Transportation Planning · Landscape Architecture

Copyright © 2022 Mackenzie, All Rights Reserved.

	A	B	C	D	E	F
1	ENDORSEMENT	INFO1	INFO2	NAME	ADDRESS/IO ADDRESS	CITYSTATEZIP/ADDRESSEE
2	RETURN SERVICE REQUESTED	1N1E35BC 1000	FORUM BUILDING LLC	SEIU LOCAL 503	PO BOX 12159	SALEM OR 97309
3	RETURN SERVICE REQUESTED		1N1E35BC 1200	FEDERAL GOVERNMENT	809 NE 6TH AVE	PORTLAND OR 97232
4	RETURN SERVICE REQUESTED	1N1E35BC 1200	EAST-WEST COLLEGE OF THE	HEALING ARTS INC	1515 SW 5TH AVE STE 602	PORTLAND OR 97201
5	RETURN SERVICE REQUESTED		1N1E35BC 1200	CITY OF PORTLAND	1120 SW 5TH AVE STE 1204	PORTLAND OR 97204
6	RETURN SERVICE REQUESTED		1N1E35BC 1200	RISE PARTNERSHIP INC	525 NE OREGON ST STE 525	PORTLAND OR 97232
7	RETURN SERVICE REQUESTED		1N1E35BC 1300	600 HOLLADAY LIMITED PARTNERSHIP	1211 SW 5TH AVE STE 2230	PORTLAND OR 97204
8	RETURN SERVICE REQUESTED		1N1E35BC 1300	CAFE TODAY NORTH LLC	1288 STATE ST	SALEM OR 97301
9	RETURN SERVICE REQUESTED		1N1E35BC 1500	AAT LLOYD DISTRICT LLC	3420 CARMEL MOUNTAIN RD #100	SAN DIEGO CA 92121
10	RETURN SERVICE REQUESTED		1N1E35BC 1600	BLUMENAUER EARL	729 NE OREGON ST RM 115	PORTLAND OR 97232
11	RETURN SERVICE REQUESTED		1N1E35BC 1600	COLUMBIA RIVER INTERTRIBAL FISH COMMI	729 NE OREGON ST #200	PORTLAND OR 97232
12	RETURN SERVICE REQUESTED	1N1E35BC 1900	OR STATE OF DEPT OF ADMIN SERVICES	ATTN STATE PARKING	1100 AIRPORT RD SE	SALEM OR 97301-6082
13	RETURN SERVICE REQUESTED		1N1E35BC 2000	STATE OF OREGON	800 NE OREGON ST #16	PORTLAND OR 97232
14	RETURN SERVICE REQUESTED		1N1E35BC 2100	OREGON AUTOMOBILE DEALERS ASSN INC	777 NE 7TH AVE	PORTLAND OR 97232
15	RETURN SERVICE REQUESTED		1N1E35BC 2300	CONSOLIDATED FEDERAL CREDIT UNION	1033 NE 6TH AVE	PORTLAND OR 97232
16	RETURN SERVICE REQUESTED	1N1E35BC 2400	BOYLAND PROPERTIES PORTLAND LLC	C/O VIERCK & RAKOSKI CPAS PC	606 SE 117TH AVE STE 100	VANCOUVER WA 98683
17	RETURN SERVICE REQUESTED		1N1E35BC 2400	PARKING NW LLC	PO BOX 6916	PORTLAND OR 97228
18	RETURN SERVICE REQUESTED		1N1E35BC 400	FIRST TECHNOLOGY FEDERAL CREDIT UNION	PO BOX 2100	BEAVERTON OR 97075
19	RETURN SERVICE REQUESTED	1N1E35BC 400	LIBERTY MUTUAL INS CO #0066	PT COMPLIANCE SERVICES	PO BOX 80615	INDIANAPOLIS IN 46280
20	RETURN SERVICE REQUESTED		1N1E35BC 400	600 HOLLADAY LIMITED PARTNERSHIP	720 SW WASHINGTON ST STE 640	PORTLAND OR 97205
21	RETURN SERVICE REQUESTED	1N1E35BC 400	CARRINGTON MORTGAGE SERVICES	ATTN ELIZABETH JUNGERS	25 ENTERPRISE STE 400	ALISO VIEJO CA 92656
22	RETURN SERVICE REQUESTED		1N1E35BC 400	CHICAGO TITLE COMPANY OF OREGON	601 RIVERSIDE AVE T-5 TAX DEPT	JACKSONVILLE FL 32204
23	RETURN SERVICE REQUESTED		1N1E35BC 400	KINDERCCARE EDUCATION AT WORK LLC	PO BOX 6760	PORTLAND OR 97228
24	RETURN SERVICE REQUESTED		1N1E35BC 400	REGUS MANAGEMENT GROUP LLC	3000 KELLWAY DR STE 140	CARROLLTON TX 75006-3355
25	RETURN SERVICE REQUESTED		1N1E35BC 400	ASHFORTH PACIFIC INC	707 SUMMER ST 4TH FLR	STAMFORD CT 06901
26	RETURN SERVICE REQUESTED		1N1E35BC 400	TICOR TITLE COMPANY OF OREGON	601 RIVERSIDE AVE 5TH FLR	JACKSONVILLE FL 32204
27	RETURN SERVICE REQUESTED		1N1E35BC 400	RMKUS CONSULTING GROUP INC	12140 WINCHESTER LN STE 300	HOUSTON TX 77079
28	RETURN SERVICE REQUESTED	1N1E35BC 600	LLOYD DISTRICT PROPERTIES	LIMITED PARTNERSHIP	720 SW WASHINGTON ST #640	PORTLAND OR 97205-3545
29				CURRENT RESIDENT	720 NE GRAND AVE	PORTLAND OR 97232
30				CURRENT RESIDENT	729 NE OREGON ST #125	PORTLAND OR 97232
31				CURRENT RESIDENT	800 NE OREGON ST #225	PORTLAND OR 97232
32				CURRENT RESIDENT	800 NE OREGON ST #370	PORTLAND OR 97232
33				CURRENT RESIDENT	800 NE OREGON ST #425	PORTLAND OR 97232
34				CURRENT RESIDENT	800 NE OREGON ST #611	PORTLAND OR 97232
35				CURRENT RESIDENT	809 NE 6TH AVE #350	PORTLAND OR 97232
36				CURRENT RESIDENT	830 NE HOLLADAY ST #100	PORTLAND OR 97232
37				CURRENT RESIDENT	830 NE HOLLADAY ST #105	PORTLAND OR 97232
38				CURRENT RESIDENT	525 NE OREGON ST #190	PORTLAND OR 97232
39				CURRENT RESIDENT	525 NE OREGON ST #200	PORTLAND OR 97232
40				CURRENT RESIDENT	622 NE GRAND AVE	PORTLAND OR 97232
41				CURRENT RESIDENT	650 NE HOLLADAY ST #125	PORTLAND OR 97232
42				CURRENT RESIDENT	650 NE HOLLADAY ST #150	PORTLAND OR 97232
43				CURRENT RESIDENT	729 NE OREGON ST #190	PORTLAND OR 97232
44				CURRENT RESIDENT	800 NE OREGON ST #1010	PORTLAND OR 97232
45				CURRENT RESIDENT	800 NE OREGON ST #1105	PORTLAND OR 97232
46				CURRENT RESIDENT	800 NE OREGON ST #1145	PORTLAND OR 97232
47				CURRENT RESIDENT	800 NE OREGON ST #310	PORTLAND OR 97232
48				CURRENT RESIDENT	800 NE OREGON ST #772	PORTLAND OR 97232
49				CURRENT RESIDENT	800 NE OREGON ST #805	PORTLAND OR 97232
50				CURRENT RESIDENT	800 NE OREGON ST #825	PORTLAND OR 97232
51				CURRENT RESIDENT	800 NE OREGON ST #850	PORTLAND OR 97232
52				CURRENT RESIDENT	830 NE HOLLADAY ST #140	PORTLAND OR 97232
53				CURRENT RESIDENT	830 NE HOLLADAY ST #145	PORTLAND OR 97232
54				CURRENT RESIDENT	830 NE HOLLADAY ST #150	PORTLAND OR 97232
55				CURRENT RESIDENT	650 NE HOLLADAY ST #160	PORTLAND OR 97232
56				CURRENT RESIDENT	729 NE OREGON ST #115	PORTLAND OR 97232
57				CURRENT RESIDENT	729 NE OREGON ST #150	PORTLAND OR 97232
58				CURRENT RESIDENT	800 NE OREGON ST #1045	PORTLAND OR 97232
59				CURRENT RESIDENT	800 NE OREGON ST #15	PORTLAND OR 97232
60				CURRENT RESIDENT	800 NE OREGON ST #150	PORTLAND OR 97232
61				CURRENT RESIDENT	800 NE OREGON ST #407	PORTLAND OR 97232
62				CURRENT RESIDENT	800 NE OREGON ST #430	PORTLAND OR 97232
63				CURRENT RESIDENT	800 NE OREGON ST #505	PORTLAND OR 97232
64				CURRENT RESIDENT	800 NE OREGON ST #607	PORTLAND OR 97232
65				CURRENT RESIDENT	816 NE GRAND AVE	PORTLAND OR 97232
66				CURRENT RESIDENT	525 NE OREGON ST #500	PORTLAND OR 97232



	A		B		C		D		E		F	
67							CURRENT RESIDENT		525 NE OREGON ST #525		PORTLAND OR 97232	
68							CURRENT RESIDENT		650 NE HOLLADAY ST #110		PORTLAND OR 97232	
69							CURRENT RESIDENT		701 NE 7TH AVE		PORTLAND OR 97232	
70							CURRENT RESIDENT		729 NE OREGON ST #100		PORTLAND OR 97232	
71							CURRENT RESIDENT		800 NE OREGON ST #1005		PORTLAND OR 97232	
72							CURRENT RESIDENT		800 NE OREGON ST #177		PORTLAND OR 97232	
73							CURRENT RESIDENT		800 NE OREGON ST #260		PORTLAND OR 97232	
74							CURRENT RESIDENT		800 NE OREGON ST #465		PORTLAND OR 97232	
75							CURRENT RESIDENT		800 NE OREGON ST #537		PORTLAND OR 97232	
76							CURRENT RESIDENT		800 NE OREGON ST #640		PORTLAND OR 97232	
77							CURRENT RESIDENT		800 NE OREGON ST #730		PORTLAND OR 97232	
78							CURRENT RESIDENT		800 NE OREGON ST #930		PORTLAND OR 97232	
79							CURRENT RESIDENT		800 NE OREGON ST #965		PORTLAND OR 97232	
80							CURRENT RESIDENT		916 NE GRAND AVE		PORTLAND OR 97232	
81	RETURN SERVICE REQUESTED		OWNER/APPLICANT	1N1E35BC 2100		MASTER DEVELOPMENT LLC		MASTER STEVE		520 CONGER ST		EUGENE OR 97402
82	RETURN SERVICE REQUESTED		CONSULTANT			MACKENZIE		VARRICCHIONE BRIAN		1515 SE WATER AVE #100		PORTLAND OR 97214
83	RETURN SERVICE REQUESTED					LAND USE CONTACT		LLOYD BA		PO BOX 6762		PORTLAND OR 97228
84	RETURN SERVICE REQUESTED					LAND USE CONTACT		LLOYD NA		PO BOX 6762		PORTLAND OR 97228
85	RETURN SERVICE REQUESTED					LAND USE CONTACT		NECN		4815 NE 7TH AVE		PORTLAND OR 97211
86	RETURN SERVICE REQUESTED					LAND USE CONTACT		CENTRAL CITY CONCERN		232 NW 6TH AVE		PORTLAND OR 97209
87	RETURN SERVICE REQUESTED					LAND USE CONTACT		PLAN AMENDMENT SPECIALIST		635 CAPITAL ST NE #150		SALEM OR 97301
88	RETURN SERVICE REQUESTED					LAND USE CONTACT		TRANSIT DEVELOPMENT		1800 SW FIRST AVE SUITE 300		PORTLAND OR 97201
89										PROSPER PORTLAND		129/PROSPER
90										DAWN KRANTZ		B299/R5000
91	RETURN SERVICE REQUESTED				22-119814	DAR	4-1-22	CASE FILE NIELSEN		1900 SW 4TH AVE #5000		PORTLAND OR 97201

# Design Advice Request

## NE 7<sup>TH</sup> AVE APARTMENTS

CASE FILE	EA 22-119814 DA		
WHEN	Thursday, April 21, 2022 @ 1:30 PM <i>(This is the hearing start time –see Commission agenda for estimated project start time.)</i>		
WHERE	ONLINE: Meeting link will be listed on the agenda available at <a href="http://www.portland.gov/bds/design-commission">www.portland.gov/bds/design-commission</a>		
HOW	TO COMMENT: Follow instructions on the Design Commission agenda <u>or</u> email the planner at Benjamin.Nielsen@portlandoregon.gov		
REVIEW BY	Design Commission		
PROCESS	A <b>Design Advice Request</b> is a voluntary review process that allows the Commission to provide early feedback on a development proposal, prior to the required land use review		
PROPOSAL	Design Advice Request for a proposed 15-story multifamily apartment building with 260 dwelling units on a half-block site in the Lloyd Subdistrict of the Central City Plan District. The ground floor of the building will provide resident amenities. The rooftop will have an amenity deck, amenity room, and an ecoroof. Below grade parking is proposed with access from NE Irving St. One Modification to allow some parking spaces to be 7'-9" wide may be proposed.		
REVIEW APPROVAL CRITERIA	Central City Fundamental Design Guidelines Lloyd District Design Guidelines		
SITE ADDRESS	701 & 777 NE 7 <sup>th</sup> Avenue		
ZONING/ DESIGNATION	CXD – Central Commercial with Design Overlay Lloyd Subdistrict of the Central City Plan District		
APPLICANT(S)	Steve Master Master Development LLC	OWNER(S)	Steve Master Master Development LLC
QUESTIONS? BDS CONTACT	Benjamin Nielsen, City Planner (503) 865-6513 / Benjamin.Nielsen@PortlandOregon.gov Bureau of Development Services, 1900 SW 4 <sup>th</sup> Ave, Suite 5000, Portland, OR 97201		

Traducción e interpretación | Chuyển Ngữ hoặc Phiên Dịch | 翻译或传译 | Turjumida ama Fasiraadda | 翻訳または通訳 | ການແປພາສາ ຫຼື ການອະທິບາຍ  
Письменный или устный перевод | Traducere sau Interpretare | 번역및통역 | الترجمة التحريرية أو الشفوية | Письмовий або усний переклад



503-823-7300



BDS@PortlandOregon.gov



[www.PortlandOregon.gov/bds/translated](http://www.PortlandOregon.gov/bds/translated)

TTY: 503-823-6868  
Relay Service: 711





**City of Portland, Oregon**  
**Bureau of Development Services**  
**Land Use Services**  
**FROM CONCEPT TO CONSTRUCTION**

Dan Ryan, Commissioner  
Rebecca Esau, Director  
Phone: (503) 823-7300  
Fax: (503) 823-5630  
TTY: (503) 823-6868  
[www.portland.gov/bds](http://www.portland.gov/bds)

## **DESIGN ADVICE REQUEST PROCESS OVERVIEW**

### **Purpose**

Design Advice Requests (DARs) are a form of early assistance intended to provide a public forum for the preliminary discussion and exchange of information between the applicant, BDS staff, the public, and the representative Commission. The feedback that results from a DAR is advisory and preliminary in nature. The DAR is not a land use review and decisions are not made in the DAR process.

An applicant may request advice from the Design Commission or Historic Landmarks Commission prior to submitting a land use review. In some cases, a DAR may be required by a provision of the Zoning Code. These requests do not substitute for required prescribed regulatory or legislative processes.

### **Public Participation**

The public meeting with the Commission will provide an opportunity for parties to submit oral and written comment. The Commission relies on Portlanders to bring their perspective on their community. Portland has a strong design legacy that continues through this process. The public's early input on significant projects helps to make sure we get this right. Continued participation through the land use review is necessary for public comments to be part of the land use review record.

### **Meeting Order**

The order of appearance for those attending the meeting is as follows:

- BDS Staff Introduction
- Applicant Presentation
- City Staff Discussion Topics
- Public Comments
- Commission and Applicant Discussion

### **Guiding Criteria**

Design Guidelines are used to guide the conversation during the DAR because they are the approval criteria used in the subsequent land use review. All feedback should relate to the concept's response to the Guidelines. Copies of the Design Guidelines are available online at [portlandoregon.gov/designguidelines](http://portlandoregon.gov/designguidelines).

### **Outside DAR Scope**

The Commissions only have the authority to influence elements of a project that relate to the approval criteria. For example, guidelines do not address private views. Here other resources for questions on issues that the Commissions cannot address:

#### **Bureau of Planning and Sustainability (BPS)**

503-823-7700 | [portlandoregon.gov/bps](http://portlandoregon.gov/bps)

- Off-Street Parking Requirements
- Upcoming Legislative Projects on Zoning
- Zoning Allowances

#### **Bureau of Transportation (PBOT) | 503-823-**

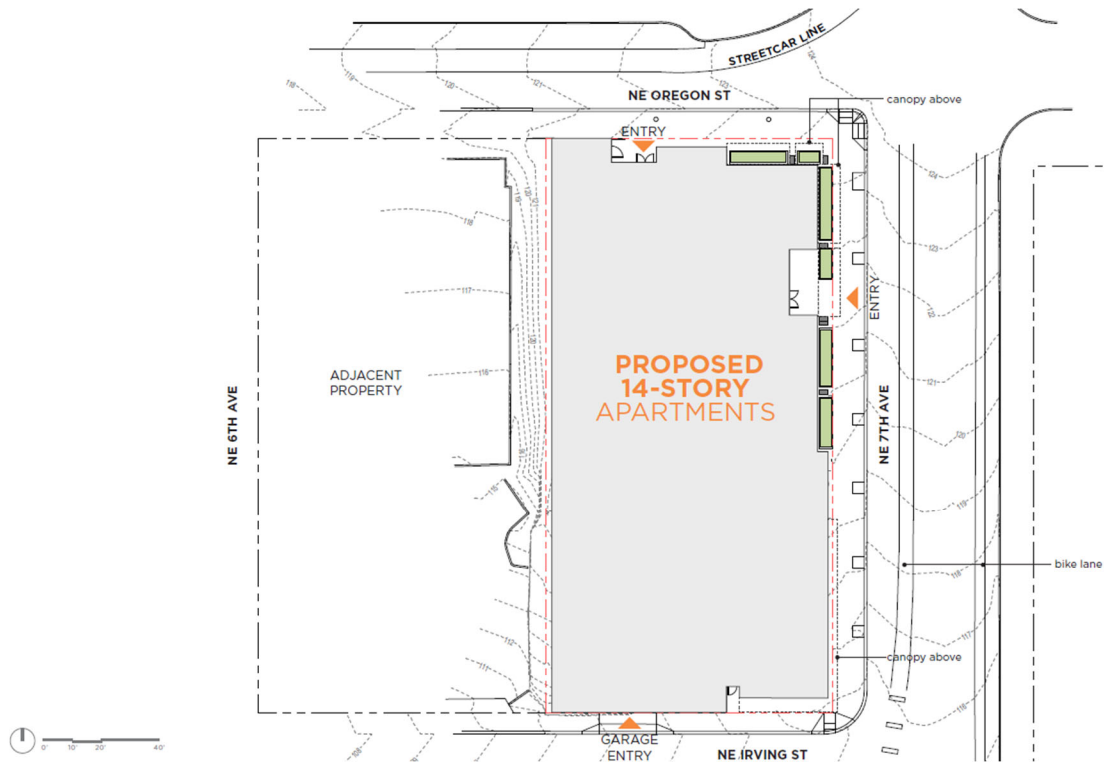
5185 | [portlandoregon.gov/transportation](http://portlandoregon.gov/transportation)

- On-Street Parking
- Construction Impacts on Streets & Sidewalks

#### **Office of Community & Civic Life**

503-823-4519 | [portlandoregon.gov/civic](http://portlandoregon.gov/civic)

- Neighborhood Association Information
- Crime Prevention
- Noise Control Program
- Neighborhood Mediation







**City of Portland, Oregon**  
**Bureau of Development Services**  
**Land Use Services**  
**FROM CONCEPT TO CONSTRUCTION**

Dan Ryan, Commissioner  
Rebecca Esau, Director  
Phone: (503) 823-7300  
Fax: (503) 823-5630  
TTY: (503) 823-6868  
[www.portland.gov/bds](http://www.portland.gov/bds)

Date: March 29, 2022

To: Brian Varricchione, Mackenzie

From: Benjamin Nielsen, Land Use Services, [Benjamin.Nielsen@portlandoregon.gov](mailto:Benjamin.Nielsen@portlandoregon.gov)

RE: Design Advice Request posting for EA 22-119814 DA

Dear Brian:

I have received your application for a Design Advice Request (DA) at 701 & 777 NE 7<sup>th</sup> Ave. Your case number is given above. The first meeting with the Design Commission is scheduled for **April 21, 2022**. I am the planner handling your case, and can answer any questions you might have during the process.

You are required to post notice on the site of your proposal 20 days before the hearing. The information below will help you do this. I am also enclosing instructions for making the required posting boards and the notice that should be placed on the signs.

- A. You must post one of these signs adjacent to each street frontage on the site.
- B. These signs must be placed within 10 feet of the street frontage line, and must be visible to pedestrians and motorists. You may not post in the public right-of-way.
- C. Because the meeting with the Design Commission for your case is scheduled for April 21, 2022, you must post the notices by April 1, 2022, 20 days before the hearing.
- D. A certification statement is enclosed, which you must sign and return. The statement affirms that you posted the site. It also confirms your understanding that if you do not post the notice by the date above, your hearing will be automatically postponed. You must return this statement to us by April 7, 2022, 14 days before the hearing.
- E. You should not remove the notice before the meeting, but it must be taken down within two weeks after the meeting. You may want to save the posting boards to use for the required site posting during the Type III land use review.

Encls: Posting Notice  
Statement Certifying Posting  
Additional Instructions for Posting Notice Signs

cc: Application Case File

# Design Advice Request

## NE 7<sup>TH</sup> AVE APARTMENTS

CASE FILE	EA 22-119814 DA		
WHEN	Thursday, April 21, 2022 @ 1:30 PM <i>(This is the hearing start time –see Commission agenda for estimated project start time.)</i>		
WHERE	ONLINE: Meeting link will be listed on the agenda available at <a href="http://www.portland.gov/bds/design-commission">www.portland.gov/bds/design-commission</a>		
HOW	TO COMMENT: Follow instructions on the Design Commission agenda <u>or</u> email the planner at Benjamin.Nielsen@portlandoregon.gov		
REVIEW BY	Design Commission		
PROCESS	A <b>Design Advice Request</b> is a voluntary review process that allows the Commission to provide early feedback on a development proposal, prior to the required land use review		
PROPOSAL	Design Advice Request for a proposed 15-story multifamily apartment building with 260 dwelling units on a half-block site in the Lloyd Subdistrict of the Central City Plan District. The ground floor of the building will provide resident amenities. The rooftop will have an amenity deck, amenity room, and an ecoroof. Below grade parking is proposed with access from NE Irving St. One Modification to allow some parking spaces to be 7'-9" wide may be proposed.		
REVIEW APPROVAL CRITERIA	Central City Fundamental Design Guidelines Lloyd District Design Guidelines		
SITE ADDRESS	701 & 777 NE 7 <sup>th</sup> Avenue		
ZONING/ DESIGNATION	CXD – Central Commercial with Design Overlay Lloyd Subdistrict of the Central City Plan District		
APPLICANT(S)	Brian Varricchione Mackenzie	OWNER(S)	Steve Master Master Development LLC
QUESTIONS? BDS CONTACT	Benjamin Nielsen, City Planner (503) 865-6513 / Benjamin.Nielsen@PortlandOregon.gov Bureau of Development Services, 1900 SW 4 <sup>th</sup> Ave, Suite 5000, Portland, OR 97201		

Traducción e interpretación | Chuyển Ngữ hoặc Phiên Dịch | 翻译或传译 | Turjumida ama Fasiraadda | 翻訳または通訳 | ການແປພາສາ ຫຼື ການອະທິບາຍ  
Письменный или устный перевод | Traducere sau Interpretare | 번역및통역 | الترجمة التحريرية أو الشفوية | Письмовий або усний переклад



503-823-7300



BDS@PortlandOregon.gov



[www.PortlandOregon.gov/bds/translated](http://www.PortlandOregon.gov/bds/translated)

TTY: 503-823-6868  
Relay Service: 711



Brian Varricchione, Mackenzie  
1515 SE Water Ave #100  
Portland, OR 97214

DATE: \_\_\_\_\_

TO: Benjamin Nielsen / Benjamin.Nielsen@portlandoregon.gov  
Bureau of Development Services  
1900 SW Fourth Ave., Suite 5000  
Portland, Oregon 97201

**APPLICANT'S STATEMENT CERTIFYING DESIGN ADVICE REQUEST POSTING**

**Case File EA 22-119814 DA**

This certifies that I have posted notice on my site. I understand that the meeting with the Design Commission is scheduled for **April 21, 2022** at 1:30PM, and that I was required to post the property at least 20 days before the hearing.

The required number of poster boards, with the notices attached, were set up on \_\_\_\_\_(date). These were placed adjacent to each street frontage so that they were visible to pedestrians and motorists.

I understand that this form must be returned to the Bureau of Development Services no later than April 7, 2022, 14 days before the scheduled meeting. I also understand that if I do not post the notices by April 1, 2022, or return this form by April 7, 2022, my meeting will automatically be postponed.

In addition, I understand that I may not remove the notices before the meeting, but am required to remove them within two weeks of the meeting.

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Print Name

\_\_\_\_\_  
Address

\_\_\_\_\_  
City/State/Zip Code

## **Additional Instructions for Posting Notice Signs**

### Layout:

The overall board must be printed at its full 18-inch by 24-inch size. Templates for the 18-inch by 24-inch board are provided in Adobe Illustrator, Adobe InDesign, and PDF formats.

Place an image of your proposal, preferably a rendering or an elevation, on the left side of the 18-by-24-inch sign board. Make the image as large as possible without covering any of the blue background of the board and leaving enough room for the posting notice text. Place the posting notice text provided by the city planner on the right side of the board. The posting notice text is provided in PDF format by the city planner for easy insertion/attachment to the sign template; it should be inserted at its full 8.5-inch by 11-inch size. See the example image on the following page for reference.

### Materials:

Your local sign manufacturer will have a variety of options available, but for environmental reasons we recommend corrugated plastic because it can be recycled. Corrugated plastic is sold under many trade names. It is an extruded twin wall plastic-sheet product produced from high-impact polypropylene resin with a similar make-up to corrugated cardboard and is ideally suited for outdoor signage. It can be direct printed or will accept pressure sensitive adhesive graphics. The sign must be printed in color.

### Installation:

Signs may be attached to an existing building or structure, or mounted on posts, stakes, a fence, or other reasonable and sturdy structure that is fully accessible to the public.

### Mounting Height:

To ensure that your sign is displayed at the correct height, the top of the sign when mounted should be no higher than 60" inches above grade and free from any obstruction. The bottom of the sign must be at least 24" above the ground.

An example image of the posting board layout is provided below:





## Nielsen, Benjamin

---

**From:** Sid Hariharan <SHariharan@mcknze.com>  
**Sent:** Friday, April 1, 2022 8:59 AM  
**To:** Nielsen, Benjamin  
**Cc:** Brian Varricchione; Thomas Peck; Rachel A. Hedlof; Julie Bradshaw  
**Subject:** RE: NE 7th Ave Apartments DAR Sign Posting  
**Attachments:** DAR Sign Posting Certification -220401(1.1).pdf

Hi Benjamin,

Please see the attached Statement Certifying Posting. The signs were posted on March 31, 2022.

Additionally, do you have additional comments about our DAR package at this time?

Thank You,

Sid Hariharan | he/him/his  
Land Use Planning  
D 971.346.3700 E [shariharan@mcknze.com](mailto:shariharan@mcknze.com)

MACKENZIE.

---

**From:** Nielsen, Benjamin <Benjamin.Nielsen@portlandoregon.gov>  
**Sent:** Tuesday, March 29, 2022 3:50 PM  
**To:** Sid Hariharan <SHariharan@mcknze.com>  
**Cc:** Brian Varricchione <BVarricchione@mcknze.com>; Thomas Peck <TPeck@mcknze.com>; Rachel A. Hedlof <RHedlof@mcknze.com>; Julie Bradshaw <JBradshaw@mcknze.com>  
**Subject:** RE: NE 7th Ave Apartments Design Advice Request - Draft Drawings

Hi Sid:

Yes. Please see attached posting notice and instructions and the template files for printing the boards.

Regarding your submittal last week, too, the proposed mullion cap extensions into the ROW will not need a Modification, since there is no zoning code development standard for these. Instead, you will need to verify that they meet the PBOT [encroachment guide](#) (and related building code requirements) and secure any encroachment permits that may be required with your permit review. Since this is not a Modification, it is not mentioned on the posting notice.

Thanks,

**Benjamin Nielsen** – Senior Planner  
p: 503.865.6519

---

**From:** Sid Hariharan <[SHariharan@mcknze.com](mailto:SHariharan@mcknze.com)>  
**Sent:** Tuesday, March 29, 2022 2:55 PM  
**To:** Nielsen, Benjamin <[Benjamin.Nielsen@portlandoregon.gov](mailto:Benjamin.Nielsen@portlandoregon.gov)>  
**Cc:** Brian Varricchione <[BVarricchione@mcknze.com](mailto:BVarricchione@mcknze.com)>; Thomas Peck <[TPeck@mcknze.com](mailto:TPeck@mcknze.com)>; Rachel A. Hedlof



<[RHedlof@mcknze.com](mailto:RHedlof@mcknze.com)>; Julie Bradshaw <[JBradshaw@mcknze.com](mailto:JBradshaw@mcknze.com)>

**Subject:** RE: NE 7th Ave Apartments Design Advice Request - Draft Drawings

Hi Benjamin,

Checking in to see whether you've been able to generate the posting notice for the site so we can print and post by the 1<sup>st</sup>?

Thanks,

Sid Hariharan | he/him/his

Land Use Planning

D 971.346.3700 E [shariharan@mcknze.com](mailto:shariharan@mcknze.com)

**MACKENZIE.**

---

**From:** Nielsen, Benjamin <[Benjamin.Nielsen@portlandoregon.gov](mailto:Benjamin.Nielsen@portlandoregon.gov)>

**Sent:** Friday, March 25, 2022 6:35 PM

**To:** Sid Hariharan <[SHariharan@mcknze.com](mailto:SHariharan@mcknze.com)>

**Cc:** Brian Varricchione <[BVarricchione@mcknze.com](mailto:BVarricchione@mcknze.com)>; Thomas Peck <[TPeck@mcknze.com](mailto:TPeck@mcknze.com)>; Rachel A. Hedlof <[RHedlof@mcknze.com](mailto:RHedlof@mcknze.com)>; Julie Bradshaw <[JBradshaw@mcknze.com](mailto:JBradshaw@mcknze.com)>

**Subject:** RE: NE 7th Ave Apartments Design Advice Request - Draft Drawings

Thanks, Sid. I'll review these and get comments to you next week.

I will also send the posting notice early next week so that you may post the site by 4/1.

Thanks,

**Benjamin Nielsen** – Senior Planner

p: 503.865.6519

---

**From:** Sid Hariharan <[SHariharan@mcknze.com](mailto:SHariharan@mcknze.com)>

**Sent:** Thursday, March 24, 2022 1:46 PM

**To:** Nielsen, Benjamin <[Benjamin.Nielsen@portlandoregon.gov](mailto:Benjamin.Nielsen@portlandoregon.gov)>

**Cc:** Brian Varricchione <[BVarricchione@mcknze.com](mailto:BVarricchione@mcknze.com)>; Thomas Peck <[TPeck@mcknze.com](mailto:TPeck@mcknze.com)>; Rachel A. Hedlof <[RHedlof@mcknze.com](mailto:RHedlof@mcknze.com)>; Julie Bradshaw <[JBradshaw@mcknze.com](mailto:JBradshaw@mcknze.com)>

**Subject:** NE 7th Ave Apartments Design Advice Request - Draft Drawings

Good Afternoon, Benjamin,

Attached is the DAR Draft Drawings package for the proposed NE 7<sup>th</sup> Ave Apartments (701 & 777 NE 7<sup>th</sup> Ave) in the Lloyd District (EA 22-119814). Please let me know if you have any questions, or if the material does not open properly. We look forward to discussing this project.

Thank You,

Sid Hariharan | he/him/his

Land Use Planning

D 971.346.3700 E [shariharan@mcknze.com](mailto:shariharan@mcknze.com)

**MACKENZIE.**

ARCHITECTURE ■ INTERIORS ■ STRUCTURAL, CIVIL, AND TRAFFIC ENGINEERING  
LAND USE AND TRANSPORTATION PLANNING ■ LANDSCAPE ARCHITECTURE

<https://mackenzie.inc> | [Portland, OR](#) | [Vancouver, WA](#) | [Seattle, WA](#)

Mackenzie Email [Disclaimer](#)



Brian Varricchione, Mackenzie  
1515 SE Water Ave #100  
Portland, OR 97214

DATE: April 1, 2022

TO: Benjamin Nielsen / Benjamin.Nielsen@portlandoregon.gov  
Bureau of Development Services  
1900 SW Fourth Ave., Suite 5000  
Portland, Oregon 97201

**APPLICANT'S STATEMENT CERTIFYING DESIGN ADVICE REQUEST POSTING**

**Case File EA 22-119814 DA**

This certifies that I have posted notice on my site. I understand that the meeting with the Design Commission is scheduled for **April 21, 2022** at 1:30PM, and that I was required to post the property at least 20 days before the hearing.

The required number of poster boards, with the notices attached, were set up on March 31, 2022 (date). These were placed adjacent to each street frontage so that they were visible to pedestrians and motorists.

I understand that this form must be returned to the Bureau of Development Services no later than April 7, 2022, 14 days before the scheduled meeting. I also understand that if I do not post the notices by April 1, 2022, or return this form by April 7, 2022, my meeting will automatically be postponed.

In addition, I understand that I may not remove the notices before the meeting, but am required to remove them within two weeks of the meeting.

\_\_\_\_\_  
Signature

Sid Hariharan

\_\_\_\_\_  
Print Name

1515 SE Water Ave, Suite 100  
Address

Portland OR, 97214  
City/State/Zip Code





Calytera AMANDA 7

https://abo.portlandoregon.gov/backoffice/core/home.zhtml

General Folder 820 SE ALDER ST [EA] General Folder 701 & 777 NE 7TH AV...

Folder | Property(2) | People(3) | Info(17) | Fee(2) | **Process(14)** | Document(1) | Comment(4) | Attachment(8) | Correspondence(11)

2022 119814 000 00 EA Early Assistance Pending - EA

Attempt [+Attempt](#) [Checklist](#) [Info](#) [Options](#)

**PBOT Dev Rev - EA Response (69017) Closed** 701 NE 7TH AVE , PORTLAND, Oregon, 97232, USA

Team	Assigned User	Baseline Start Date	Baseline End Date
Team	de Freitas,Fabio	Mar 10, 2022 00:00:00	Apr 07, 2022 17:07:16
To Start	To End	Sign Off User	Discipline
Mar 10, 2022 00:00:00	Apr 07, 2022 17:07:16	de Freitas,Fabio	
Started	Ended	Created By	
Apr 18, 2022 08:04:43	Apr 18, 2022 08:04:43		
Reference	People	Last Modified By	
Reference	People	de Freitas,Fabio	
Display Order	Inspection Number	Last Modified	
900	Inspection Number	Apr 18, 2022 08:05:06	

Comments

The applicant should refer to the response prepared by PBOT for the associated Pre-application Conference (EA 21-117524) for transportation-related issues/requirements.

Days Elapsed  
39

Top

## Nielsen, Benjamin

---

**From:** Dasha Peterman <DPeterman@asginc.us>  
**Sent:** Monday, April 11, 2022 1:10 PM  
**To:** info@mwdevelopment.net  
**Cc:** Nielsen, Benjamin  
**Subject:** RE: 4/1/2022 Land Use Notifications Investigations Ticket#4672-2022  
**Attachments:** 4672-2022 email.pdf; 4672-2022 map.PNG; 3-GIS Legend.pdf

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

Good Afternoon,

Investigations Ticket#4672-2022

Verizon business (MCI) does have facilities in the area.

Please see attached.

Call Before You Dig!

**Kind Regards,**  
**Dasha Peterman**  
*Project Coordinator BL*

**ASG**

3301 Matrix Drive  
Richardson, TX, 75082

**Plan, Design, Build, Service.**

---

**From:** BDS LUS Notification <BDSLUS@portlandoregon.gov>  
**Sent:** Friday, April 1, 2022 8:42 AM  
**To:** BDS LUS Notification <BDSLUS@portlandoregon.gov>  
**Subject:** 4/1/2022 Land Use Notifications

**PROPOSALS**



<p>NWNW          Pearl District          Pearl District   04-01-22   1325 NW Kearney St   Design Review with Modification Request - REQUEST FOR RESPONSE   LU 22-109692 DZM          Pull date: April 21, 2022 by 5 p.m.</p>
<p>NECN          Lloyd District          Lloyd District   04-01-22   701 &amp; 777 NE 7th Ave   Design Advice Review - DESIGN ADVICE REQUEST   EA 22-119814 DA          Pull date: April 21, 2022 by 5 p.m.</p>
<p>CIVIC LIFE          South Portland          South Portland   04-01-22   Property bounded by SW Bond, SW Abernethy, SW River Pkwy and SW Lowell   Design Advice Review - DESIGN ADVICE REQUEST   EA 22-117306 DA          Pull date: April 21, 2022 by 5 p.m.</p>
<p>None          Columbia Corridor          None   04-01-22   7005 NE Marine Dr   Environmental Review - PROPOSAL   LU 22-116532 EN          Pull date: May 2, 2022 by 5 p.m.</p>
<p>NWNW          None          Southwest Hills Residential League   04-01-22   3640 SW Dosch Rd   Adjustment Review - PROPOSAL   LU 22-104373 AD          Pull date: April 22, 2022 by 5 p.m.</p>
<p>NWNW          Nob Hill &amp; Pearl District          Northwest District   04-01-22   1618 NW Northrup St   Adjustment Review - PROPOSAL   LU 22-118343 AD          Pull date: April 22, 2022 by 5 p.m.</p>

## DECISION

<p>NWNW          Nob Hill          Northwest District   04-01-22   2357 NW Kearney St   Historic Resource Review - DECISION   LU 22-108394 HR</p>
<p>SEUL          None          North Tabor   04-01-22   371 NE 52nd Ave   Adjustment Review - DECISION   LU 22-102658 AD</p>

Those who do not have A7 you may view the above listed Notices at the following link.  
<https://www.portlandoregon.gov/bds/36652>

**For Proposals:** Click **Public Notices** and then choose the District Coalition.

**For Decisions:** Click **Land Use Decisions** and then choose the District Coalition.

**For Request for Response:** Click **Public Notices** and then choose the District Coalition.

**DO NOT REPLY TO THIS EMAIL**

**Reply Directly to the Case Planner.**

Land Use Services Notification e-mail  
City of Portland  
Bureau of Development Services



[www.asginc.us](http://www.asginc.us)

'This message (including any attachments) may contain confidential, proprietary, privileged and/or private information and is intended to be for the use of the intended recipient. If you are not the intended recipient of this message, please notify the sender immediately, and delete the message and any attachments. Any disclosure, reproduction, distribution or other use of this message or any attachments by an individual or entity other than the intended recipient is prohibited.'; and fall back to action Ignore if the disclaimer can't be inserted.



# Design Advice Request

## NE 7<sup>TH</sup> AVE APARTMENTS

CASE FILE	EA 22-119814 DA		
WHEN	Thursday, April 21, 2022 @ 1:30 PM <i>(This is the hearing start time –see Commission agenda for estimated project start time.)</i>		
WHERE	ONLINE: Meeting link will be listed on the agenda available at <a href="http://www.portland.gov/bds/design-commission">www.portland.gov/bds/design-commission</a>		
HOW	TO COMMENT: Follow instructions on the Design Commission agenda <u>or</u> email the planner at Benjamin.Nielsen@portlandoregon.gov		
REVIEW BY	Design Commission		
PROCESS	A <b>Design Advice Request</b> is a voluntary review process that allows the Commission to provide early feedback on a development proposal, prior to the required land use review		
PROPOSAL	Design Advice Request for a proposed 15-story multifamily apartment building with 260 dwelling units on a half-block site in the Lloyd Subdistrict of the Central City Plan District. The ground floor of the building will provide resident amenities. The rooftop will have an amenity deck, amenity room, and an ecoroof. Below grade parking is proposed with access from NE Irving St. One Modification to allow some parking spaces to be 7'-9" wide may be proposed.		
REVIEW APPROVAL CRITERIA	Central City Fundamental Design Guidelines Lloyd District Design Guidelines		
SITE ADDRESS	701 & 777 NE 7 <sup>th</sup> Avenue		
ZONING/ DESIGNATION	CXD – Central Commercial with Design Overlay Lloyd Subdistrict of the Central City Plan District		
APPLICANT(S)	Steve Master Master Development LLC	OWNER(S)	Steve Master Master Development LLC
QUESTIONS? BDS CONTACT	Benjamin Nielsen, City Planner (503) 865-6513 / Benjamin.Nielsen@PortlandOregon.gov Bureau of Development Services, 1900 SW 4 <sup>th</sup> Ave, Suite 5000, Portland, OR 97201		

Traducción e interpretación | Chuyển Ngữ hoặc Phiên Dịch | 翻译或传译 | Turjumida ama Fasiraadda | 翻訳または通訳 | ຫຼັກການອະທິບາຍ  
Письменный или устный перевод | Traducere sau Interpretare | 번역및통역 | الترجمة التحريرية أو الشفوية | Письмовий або усний переклад



503-823-7300



BDS@PortlandOregon.gov



[www.PortlandOregon.gov/bds/translated](http://www.PortlandOregon.gov/bds/translated)

TTY: 503-823-6868

Relay Service: 711



**City of Portland, Oregon**  
**Bureau of Development Services**  
**Land Use Services**  
**FROM CONCEPT TO CONSTRUCTION**

Dan Ryan, Commissioner  
Rebecca Esau, Director  
Phone: (503) 823-7300  
Fax: (503) 823-5630  
TTY: (503) 823-6868  
[www.portland.gov/bds](http://www.portland.gov/bds)

## **DESIGN ADVICE REQUEST PROCESS OVERVIEW**

### **Purpose**

Design Advice Requests (DARs) are a form of early assistance intended to provide a public forum for the preliminary discussion and exchange of information between the applicant, BDS staff, the public, and the representative Commission. The feedback that results from a DAR is advisory and preliminary in nature. The DAR is not a land use review and decisions are not made in the DAR process.

An applicant may request advice from the Design Commission or Historic Landmarks Commission prior to submitting a land use review. In some cases, a DAR may be required by a provision of the Zoning Code. These requests do not substitute for required prescribed regulatory or legislative processes.

### **Public Participation**

The public meeting with the Commission will provide an opportunity for parties to submit oral and written comment. The Commission relies on Portlanders to bring their perspective on their community. Portland has a strong design legacy that continues through this process. The public's early input on significant projects helps to make sure we get this right. Continued participation through the land use review is necessary for public comments to be part of the land use review record.

### **Meeting Order**

The order of appearance for those attending the meeting is as follows:

- BDS Staff Introduction
- Applicant Presentation
- City Staff Discussion Topics
- Public Comments
- Commission and Applicant Discussion

### **Guiding Criteria**

Design Guidelines are used to guide the conversation during the DAR because they are the approval criteria used in the subsequent land use review. All feedback should relate to the concept's response to the Guidelines. Copies of the Design Guidelines are available online at [portlandoregon.gov/designguidelines](http://portlandoregon.gov/designguidelines).

### **Outside DAR Scope**

The Commissions only have the authority to influence elements of a project that relate to the approval criteria. For example, guidelines do not address private views. Here other resources for questions on issues that the Commissions cannot address:

#### **Bureau of Planning and Sustainability (BPS)**

503-823-7700 | [portlandoregon.gov/bps](http://portlandoregon.gov/bps)

- Off-Street Parking Requirements
- Upcoming Legislative Projects on Zoning
- Zoning Allowances

#### **Bureau of Transportation (PBOT) | 503-823-**

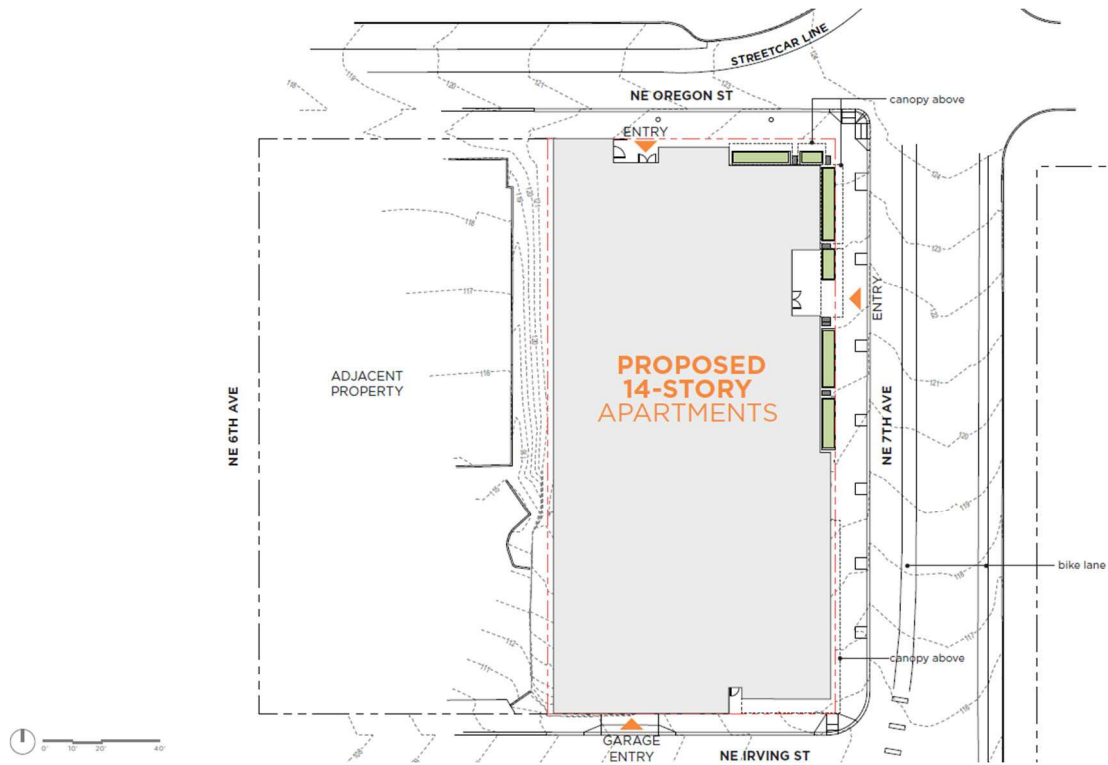
5185 | [portlandoregon.gov/transportation](http://portlandoregon.gov/transportation)

- On-Street Parking
- Construction Impacts on Streets & Sidewalks

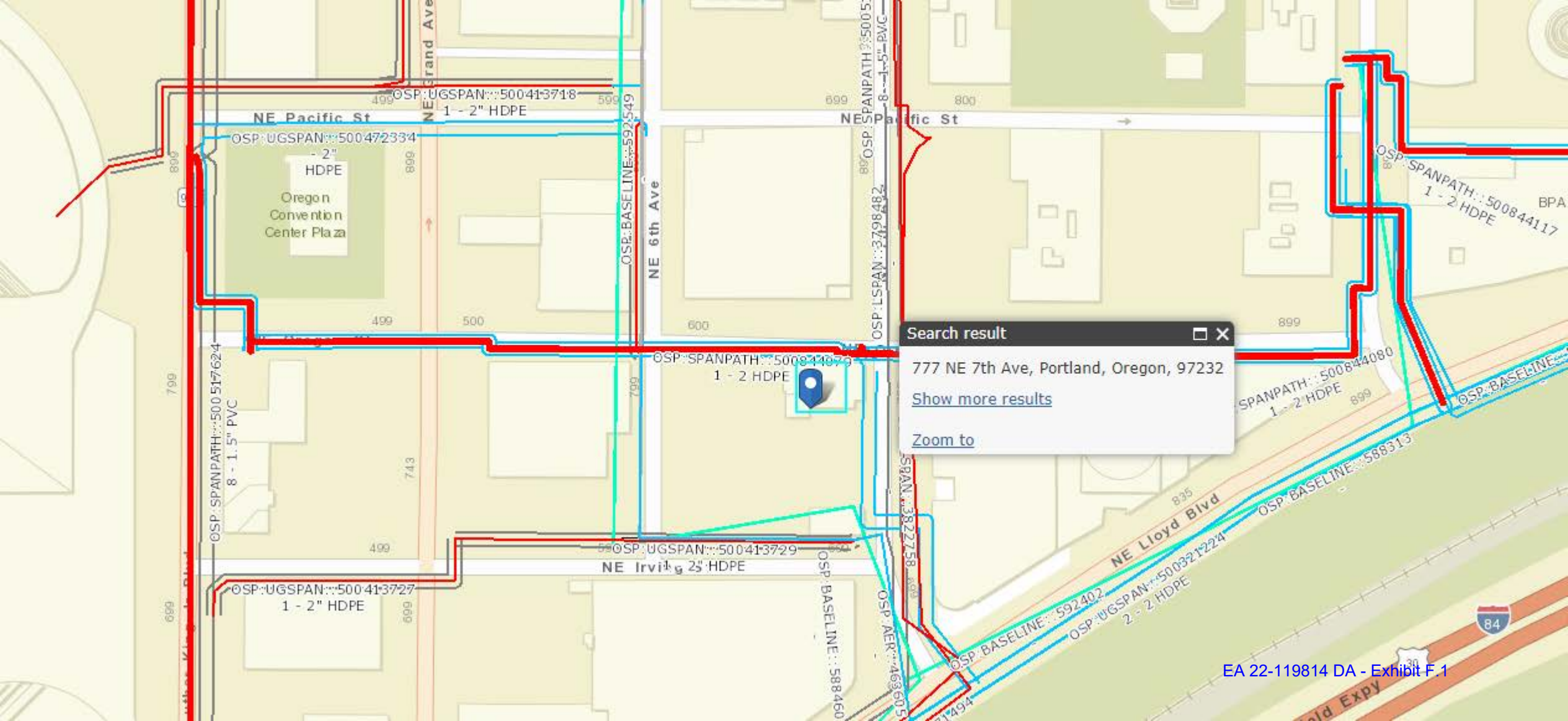
#### **Office of Community & Civic Life**

503-823-4519 | [portlandoregon.gov/civic](http://portlandoregon.gov/civic)

- Neighborhood Association Information
- Crime Prevention
- Noise Control Program
- Neighborhood Mediation







Search result

777 NE 7th Ave, Portland, Oregon, 97232

[Show more results](#)

[Zoom to](#)



### 3-GIS LEGEND

#### VZB BURIED CABLE

Existing



Proposed (In Progress Construction)



Abandoned

ABN-ABN-ABN-ABN-ABN-

Other Cable (Verizon Owned)



Existing Span (Verizon Owned)



#### VZB DIRECT BURIED CABLE

Existing



Proposed (In Progress Construction)



Abandoned

ABN-ABN-ABN-ABN-ABN-

#### VZB AERIAL CABLE

Existing



Proposed (In Progress Construction)



Abandoned

ABN-ABN-ABN-ABN-ABN-

#### VZB FSRV (UNVERIFIED)

Aerial



Buried



#### VZB BURIED CONDUIT (SPAN)

Existing



Proposed (In Progress Construction)



Abandoned



#### VZB SUBMARINE CABLE

Existing



Proposed (In Progress Construction)



Abandoned



#### VZB Not Protected Leased

Span Leased



Proposed Leased



Proposed Existing Leased





# City of Portland, Oregon - Bureau of Development Services

1900 SW Fourth Avenue • Portland, Oregon 97201 | 503-823-7300 | [www.portland.gov/bds](http://www.portland.gov/bds)



## Early Assistance Application

### FOR INTAKE, STAFF USE ONLY

Date Rec \_\_\_\_\_ by \_\_\_\_\_

LU Reviews Expected \_\_\_\_\_

☐ Required ☐ Optional Note PC 21-117524

Y N Unincorporated MC  
Y N Flood Hazard Area (LD & PD only)  
Y N Potential Landslide Hazard Area (LD & PD only)  
Y N 100-year Flood Plain  
Y N DOGAMI

File Number: \_\_\_\_\_

Appt Date/Time: \_\_\_\_\_

Qtr Sec Map(s) \_\_\_\_\_ Zoning \_\_\_\_\_

Plan District \_\_\_\_\_

Historic and/or Design District \_\_\_\_\_

Neighborhood \_\_\_\_\_

District Coalition \_\_\_\_\_

Business Assoc \_\_\_\_\_

Neighborhood within 400/1000 ft \_\_\_\_\_

**APPLICANT: Complete all sections below that apply to the proposal. Please print legibly.  
Email this application and supporting documents to: [LandUseIntake@portlandoregon.gov](mailto:LandUseIntake@portlandoregon.gov)**

### Development Site

Address \_\_\_\_\_ Cross Street \_\_\_\_\_ Site Size/Area \_\_\_\_\_

Tax account number(s) R \_\_\_\_\_ R \_\_\_\_\_ R \_\_\_\_\_ R \_\_\_\_\_

### Short Project Description:

include proposed stormwater disposal methods. Attach additional sheets for a more detailed description, if needed.

**Design & Historic Review** (New development: give project valuation. Renovation: give exterior alteration value) \$ \_\_\_\_\_

**APPLICANT:** Select an Early Assistance Type and check boxes for desired meeting/written notes options:

Early Assistance Type	City Reviewers	On-line MS Teams meeting & written notes provided <sup>1</sup>	No meeting, written notes provided
<input type="checkbox"/> <b>Pre-application Conference<sup>2</sup></b> required for Type III and IV land use reviews	BDS Land Use Services, Transportation, Environmental Services, Water, Parks, others as needed	<input type="checkbox"/>	
<input type="checkbox"/> <b>Design Advice Request<sup>2</sup></b> Public Zoom meeting or written notes only with Design Commission or Historic Landmarks Commission	BDS Land Use Services and Design Commission or Historic Landmarks Commission		
<input type="checkbox"/> <b>Early Assistance - Zoning and Infrastructure Bureaus</b> (including initial bureau responses for street vacations)	BDS Land Use Services, Transportation, Environmental Services, Water, Parks	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> <b>Early Assistance - Zoning Only</b>	BDS Land Use Services	<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/> <b>Pre-Permit Zoning Plan Check</b> <input type="checkbox"/> 1-2 housing units <input type="checkbox"/> All other development	BDS Land Use Services		<input type="checkbox"/>
<input type="checkbox"/> <b>Public Works Inquiry for 1-2 housing units</b> No land use review or property line adjustment expected	Transportation, Environmental Services, Water		<input type="checkbox"/>

<sup>1</sup>Where a meeting is optional, an additional fee applies for the meeting in addition to written notes. Please see the Land Use Services fee schedule for detailed fee information: [www.portland.gov/bds/documents/land-use-services-fees-schedule](http://www.portland.gov/bds/documents/land-use-services-fees-schedule).

<sup>2</sup>Public notice (email and internet posting) provided for Pre-application conferences and Design Advice Requests.



**Applicant Information** For Early Assistance options that include a meeting, indicate who should be invited by BDS staff. A legible email address must be provided. Include separate sheet for additional names if needed.

**PRIMARY CONTACT, check all that apply** ☐ Applicant ☐ Owner ☐ Other \_\_\_\_\_  
Invite to MS Teams Meeting?: ☐ Yes ☐ No  
Name \_\_\_\_\_ Company \_\_\_\_\_  
Mailing Address \_\_\_\_\_  
City \_\_\_\_\_ State \_\_\_\_\_ Zip Code \_\_\_\_\_  
Day Phone \_\_\_\_\_ email \_\_\_\_\_

---

**Check all that apply** ☐ Applicant ☐ Owner ☐ Other \_\_\_\_\_  
Invite to MS Teams Meeting?: ☐ Yes ☐ No  
Name \_\_\_\_\_ Company \_\_\_\_\_  
Mailing Address \_\_\_\_\_  
City \_\_\_\_\_ State \_\_\_\_\_ Zip Code \_\_\_\_\_  
Day Phone \_\_\_\_\_ email \_\_\_\_\_

---

**Check all that apply** ☐ Applicant ☐ Owner ☐ Other \_\_\_\_\_  
Invite to MS Teams Meeting?: ☐ Yes ☐ No  
Name \_\_\_\_\_ Company \_\_\_\_\_  
Mailing Address \_\_\_\_\_  
City \_\_\_\_\_ State \_\_\_\_\_ Zip Code \_\_\_\_\_  
Day Phone \_\_\_\_\_ email \_\_\_\_\_

---

**Please submit this application via email with the following materials to  
LandUseIntake@portlandoregon.gov:**

- ☐ Written project description
- ☐ Site plans drawn to a measurable scale, with scale and scale bar identified
- ☐ Building elevations drawn to a measurable scale (if appropriate), with scale and scale bar identified

Once the application is received, staff will contact you regarding payment and scheduling a date and time for your meeting.

**Questions to be discussed:**

Please include on a separate sheet of paper all questions you wish to be addressed.

**Note:**

1. Only material submitted with the original application will be addressed by City staff; we are unable to address any additional material that is submitted after the application is received.
2. For some proposals, such as those using the Community Design Standards, you will receive more detailed information if you provide full-sized plans.
3. Estimates for System Development Charges (SDCs) are not be provided at Early Assistance Meetings. Refer to SDC information on the BDS website.
4. Plans examiners do not participate in Early Assistance meetings and they do not provide written comments. For life/safety and building code questions, consult with a plans examiner in the Permit Center or schedule a Life Safety Preliminary Meeting ([www.portland.gov/bds/documents/life-safety-preliminary-meeting-request-packet](http://www.portland.gov/bds/documents/life-safety-preliminary-meeting-request-packet)).

Following a pre-application conference, the summary report with web links to forms and handouts will be e-mailed to you. If you prefer to receive paper copies, please check this box. ☐

## Nielsen, Benjamin

---

**From:** Nielsen, Benjamin  
**Sent:** Tuesday, March 29, 2022 3:50 PM  
**To:** Sid Hariharan  
**Cc:** Brian Varricchione; Thomas Peck; Rachel A. Hedlof; Julie Bradshaw  
**Subject:** RE: NE 7th Ave Apartments Design Advice Request - Draft Drawings  
**Attachments:** EA 22-119814 DA - Posting Notice.pdf; 18x24 Sign Board Templates for Applicant.zip

Hi Sid:

Yes. Please see attached posting notice and instructions and the template files for printing the boards.

Regarding your submittal last week, too, the proposed mullion cap extensions into the ROW will not need a Modification, since there is no zoning code development standard for these. Instead, you will need to verify that they meet the PBOT [encroachment guide](#) (and related building code requirements) and secure any encroachment permits that may be required with your permit review. Since this is not a Modification, it is not mentioned on the posting notice.

Thanks,

**Benjamin Nielsen** – Senior Planner

p: 503.865.6519

---

**From:** Sid Hariharan <SHariharan@mcknze.com>  
**Sent:** Tuesday, March 29, 2022 2:55 PM  
**To:** Nielsen, Benjamin <Benjamin.Nielsen@portlandoregon.gov>  
**Cc:** Brian Varricchione <BVarricchione@mcknze.com>; Thomas Peck <TPeck@mcknze.com>; Rachel A. Hedlof <RHedlof@mcknze.com>; Julie Bradshaw <JBradshaw@mcknze.com>  
**Subject:** RE: NE 7th Ave Apartments Design Advice Request - Draft Drawings

Hi Benjamin,

Checking in to see whether you've been able to generate the posting notice for the site so we can print and post by the 1<sup>st</sup>?

Thanks,

Sid Hariharan | he/him/his  
Land Use Planning  
D 971.346.3700 E [shariharan@mcknze.com](mailto:shariharan@mcknze.com)

**MACKENZIE.**

---

**From:** Nielsen, Benjamin <[Benjamin.Nielsen@portlandoregon.gov](mailto:Benjamin.Nielsen@portlandoregon.gov)>  
**Sent:** Friday, March 25, 2022 6:35 PM  
**To:** Sid Hariharan <[SHariharan@mcknze.com](mailto:SHariharan@mcknze.com)>  
**Cc:** Brian Varricchione <[BVarricchione@mcknze.com](mailto:BVarricchione@mcknze.com)>; Thomas Peck <[TPeck@mcknze.com](mailto:TPeck@mcknze.com)>; Rachel A. Hedlof

<[RHedlof@mcknze.com](mailto:RHedlof@mcknze.com)>; Julie Bradshaw <[JBradshaw@mcknze.com](mailto:JBradshaw@mcknze.com)>

**Subject:** RE: NE 7th Ave Apartments Design Advice Request - Draft Drawings

Thanks, Sid. I'll review these and get comments to you next week.

I will also send the posting notice early next week so that you may post the site by 4/1.

Thanks,

**Benjamin Nielsen** – Senior Planner

p: 503.865.6519

---

**From:** Sid Hariharan <[SHariharan@mcknze.com](mailto:SHariharan@mcknze.com)>

**Sent:** Thursday, March 24, 2022 1:46 PM

**To:** Nielsen, Benjamin <[Benjamin.Nielsen@portlandoregon.gov](mailto:Benjamin.Nielsen@portlandoregon.gov)>

**Cc:** Brian Varricchione <[BVarricchione@mcknze.com](mailto:BVarricchione@mcknze.com)>; Thomas Peck <[TPeck@mcknze.com](mailto:TPeck@mcknze.com)>; Rachel A. Hedlof <[RHedlof@mcknze.com](mailto:RHedlof@mcknze.com)>; Julie Bradshaw <[JBradshaw@mcknze.com](mailto:JBradshaw@mcknze.com)>

**Subject:** NE 7th Ave Apartments Design Advice Request - Draft Drawings

Good Afternoon, Benjamin,

Attached is the DAR Draft Drawings package for the proposed NE 7<sup>th</sup> Ave Apartments (701 & 777 NE 7<sup>th</sup> Ave) in the Lloyd District (EA 22-119814). Please let me know if you have any questions, or if the material does not open properly. We look forward to discussing this project.

Thank You,

Sid Hariharan | he/him/his

Land Use Planning

📞 971.346.3700 ✉ [shariharan@mcknze.com](mailto:shariharan@mcknze.com)

**MACKENZIE.**

ARCHITECTURE ▪ INTERIORS ▪ STRUCTURAL, CIVIL, AND TRAFFIC ENGINEERING

LAND USE AND TRANSPORTATION PLANNING ▪ LANDSCAPE ARCHITECTURE

<https://mackenzie.inc> | [Portland, OR](#) | [Vancouver, WA](#) | [Seattle, WA](#)

Mackenzie Email [Disclaimer](#)





CENTRAL CITY FDG (2003) + LLOYD SDG (1991)		PROJECT NAME: NE 7th Ave Apartments		CASE NUMBER: EA 22-119814 DA
DATE: 04/01/2022		PROJECT ARCHITECT: Mackenzie		
CONTEXT		MET?	Comments	
A1: Integrate the River		Y	While this guideline could be better met by orienting the rooftop terrace to the west for views to the river, development on the vacant lot next door may obscure these views in the future. Upper story windows and views from the north and south of the roof terrace will provide views to the river.	
	A1-1: Connect Public Facilities to the River	N/A	This site is not adjacent to the river.	
A2: Emphasize Portland Themes		?		
A3: Respect the Portland Block Structures		Y		
	A3-1: Support a Convenient Pedestrian Linkage Through the Superblocks Between Convention Center and Lloyd Center	N/A	This site is not a superblock.	
	A3-2: Make Superblock Plazas Inviting and Easily Accessible from Holladay Street	N/A	This site is not a superblock.	
A5: Enhance, Embellish & Identify Areas		Y/N	The proposed development is generally contextual to the Lloyd District in its scale, massing, and proportions. Masonry materials should be used.	
	A5-1: Develop Identifying Features	?	Specific identifying features are not articulated yet. The site's adjacency to the Green Loop suggests that some relationship with this new urban design element should be included in the development.	
	A5-2: Accommodate or Incorporate Underground Utility Service	?	Utility services are not indicated yet.	
	A5-3: Incorporate Works of Art	?	No works of art are indicated.	
	A5-4: Incorporate Water Features	?	No water features are indicated. Water features may not be needed since there is no outdoor public space on the site.	
	A5-5: Use Public Right-of-Way Design Criteria Established for the Lloyd District	Y	Will be met through public works permitting.	
	A5-6: Incorporate Landscaping as an Integral Element of Design	?	Landscape planters are indicated along the sidewalk on the east and north frontages, though these are under canopies and the building above.	
	A5-7: Integrate the Civic Campus into the Lloyd District	N/A		
	A5-8: Integrate the Lloyd Shopping Center into the Lloyd District	N/A		
A6: Re-use, Rehabilitate, Restore Buildings		N/A		
A9: Strengthen Gateways		N/A	This site is not at a gateway location.	
	A9-1: Provide a Distinct Sense of Entry and Exit	?	The site is within eyeshot of the new 7th Ave ped/bike bridge over I-84. The southeast corner of the building has extra prominence in that respect.	

C1: Enhance View Opportunities		Y	Windows on the upper stories and the proposed penthouse amenity space and deck will afford views for residents to surrounding points of interest.
		?	Transparency of the windows is not yet clear.
		N	Balconies should be included on the upper stories.
	C1-1: Integrate Parking	Y	Underground structured parking is proposed, which will help to keep parking areas out of the public realm.
		N	A large portion of the ground level of the south façade is taken up by structured parking and other back-of-house uses.
	C1-2: Integrate Signs	?	
	C2-1: Maximize View Opportunities	Y/N	See C1.
C4: Complement the Context of Existing Buildings		Y/N	See A5.
	C10-1: Use Masonry Materials	Y	Masonry materials are proposed at the ground level.
		N	Fiber cement or metal panels are proposed for the upper stories of the building.
	C10-2: Design Exterior Building Walls That Are Transparent in Glazed Areas and Sculptural in Surface	Y	Texture is added to the building's facades with proposed mullion extensions. The building has a somewhat sculptural massing with a simple division along the north-south axis.
		?	Transparency of the windows is not yet clear.
	C10-3: Use Light Colors	Y	The primary building material is lightly colored. Darker colors are reserved for the ground floor, recesses on the upper stories, and accents.
PUBLIC REALM			
		MET?	Comments
A4: Use Unifying Elements		Y/N	See A5 and A5-5.
A7: Establish and Maintain a Sense of Urban Enclosure		Y	The scale of development is appropriate for the district. Canopies, where proposed, help to enclose and articulate the sidewalk edge. Setback areas and planters at the ground floor along portions of NE 7th and NE Oregon provide additional articulation.
		N	Additional and deeper canopies should be provided along all three street frontages.
A8: Contribute to a Vibrant Streetscape		Y	The ground floor along NE 7th and most of NE Oregon St has active interior spaces, such as the building's lobby, lounge, coworking space, spa, and fitness room.
		N	See A8-1.
	A8-1: Incorporate Active Ground Level Uses in Parking Structures	N	No active ground level uses are proposed along the south façade of the parking structure.
B1: Reinforce and Enhance the Pedestrian System		?	Zones of the sidewalk are not clear yet.
	B1-1: Protect Pedestrian Areas from Mechanical Exhaust	?	
	B1-2: Incorporate Additional Lighting	?	
	B1-3: Design Projects to Attract Pedestrians to the Broadway/Weidler Corridor	N/A	
B2: Protect the Pedestrian		?	
B3: Bridge Pedestrian Obstacles		?	Will likely be met through public works permit.
	B3-1: Provide Pedestrian Crossings Spaced at Traditional One-Block Intervals	?	Will likely be met through public works permit.

	B3-2: Improve Pedestrian Crossings on NE Broadway	N/A	
B4: Provide Stopping and Viewing Places		Y	A recessed area is shown at the main lobby entrance, providing room for residents and visitors to stop and socialize outside the building and off the public sidewalk.
B5: Make Plazas, Parks & Open Space Successful		?	The Green Loop--a linear park and bikeway--passes the site. It is unclear how this development will help to make it successful.
B6: Develop Weather Protection		N	No weather protection is proposed along the south façade or the southern portion of the east façade. The extent of weather protection along the north façade and northern portion of the east façade is unclear.
	B6-1: Provide Pedestrian Rain Protection	N	See B6.
B7: Integrate Barrier-Free Design		Y	
C3: Respect Architectural Integrity		N/A	
	C3-1: Design to Enhance Existing Themes in the Broadway/ Weidler Corridor	N/A	
	C3-2: Orient Development Along the Lloyd District's Eastern Edge Toward Adjacent Neighborhoods	N/A	
C6: Develop Transitions Between Buildings & Public Spaces		Y	See B4.
	C6-1: Step-Back Upper Building Floors Along Holladay Street	N/A	
C7: Design Corners that Build Active Intersections		Y	Stairs and elevators are located away from the building's corners.
		?	There is no special emphasis at either corner that acknowledges the context of the Green Loop crossing to the south or the turning of the streetcar line on the north.
C8: Differentiate the Sidewalk Level of Buildings		Y	The sidewalk level of the building is clearly differentiated from the upper stories with storefront windows and other changes in material.
C9: Develop Flexible Sidewalk Level Spaces		?	Ground floor height is not clear. The site's slope may complicate future efforts to divide the ground level space for other uses.
C10: Integrate Encroachments		?	
C11: Integrate Roofs and Use Rooftops		Y	Rooftop amenity deck, ecoroofs, and mechanical units.
		?	While the size of mechanical units is not yet clear, there is no apparent integrated screening for the units indicated yet.
C12: Integrate Exterior Lighting		?	
C13: Integrate Signs		?	
QUALITY AND PERMANENCE			
		MET?	Comments
C2: Promote Permanence & Quality in Design		?	Specific materials aren't proposed yet. Fiber cement panels may not be of high enough quality for a
C5: Design for Coherency		Y	Overall design appears to follow a clear concept.
		N	The large blank wall area on the ground floor of the south elevation does not integrate well with the overall design of the building.





**City of Portland, Oregon**  
**Bureau of Development Services**  
**Land Use Services**  
FROM CONCEPT TO CONSTRUCTION

Dan Ryan, Commissioner  
Rebecca Esau, Director  
Phone: (503) 823-7300  
Fax: (503) 823-5630  
TTY: (503) 823-6868  
[www.portland.gov/bds](http://www.portland.gov/bds)

## Design Advice Request

### DISCUSSION MEMO

**Date:** April 14, 2022  
**To:** Portland Design Commission  
**From:** Benjamin Nielsen, Design & Historic Review Team  
(503) 865-6519, [benjamin.nielsen@portlandoregon.gov](mailto:benjamin.nielsen@portlandoregon.gov)  
**Re:** EA 22-119814 DA – NE 7<sup>th</sup> Ave Apartments  
Design Advice Request Memo – April 21, 2022

This memo is regarding the upcoming DAR on April 21, 2022, for the NE 7<sup>th</sup> Ave Apartments project. The following supporting documents are available as follows:

- Drawings – <https://efiles.portlandoregon.gov/Record/14964927/>. Note, Commissioners who requested hard copies will receive the drawing set by courier.
- Guideline matrix attached.

#### I. PROGRAM OVERVIEW

Design Advice Request for a proposed 15-story multifamily apartment building with 260 dwelling units on a half-block site in the Lloyd Subdistrict of the Central City Plan District. The ground floor of the building will provide resident amenities. The rooftop will have an amenity deck, amenity room, and an ecoroof. Below grade parking is proposed with access from NE Irving St. One Modification to allow some parking spaces to be 7'-9" wide may be proposed.

#### II. DEVELOPMENT TEAM BIO

<b>Architect</b>	Brian Varricchione, Mackenzie
<b>Owner's Representative</b>	Steve Master, Master Development LLC
<b>Project Valuation</b>	\$ 45,000,000

#### III. FUTURE DESIGN REVIEW APPROVAL CRITERIA: Central City Fundamental Design Guidelines and the Special Design Guidelines for the Design Zone of the Lloyd District of the Central City Plan (see attached matrix)

#### IV. POTENTIAL MODIFICATION

Subject to the following approval criteria:

- A. Better meets design guidelines. The resulting development will better meet the applicable design guidelines; and

- B. Purpose of the standard. On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested; and
- C. Mitigation of impacts. Any impacts resulting from the modifications are mitigated to the extent practical.

Following Modification may be requested:

1. 33.266.130.F.2, Parking Space and Aisle Dimensions. Reduce the width of parking spaces near columns to 7'-9" from the required 8'-6" in width.

## V. STAFF ANALYSIS & RECOMMENDED DAR DISCUSSION TOPICS

Staff advise you consider the following among your discussion items on April 21, 2022:

### CONTEXT

1. **Policy.** The following summarizes key policy context as it applies to the subject site.

- a. **Plan – 2035 Comprehensive Plan / Central City 2035 Plan.**

- The most relevant policy from CC2035 for development on this site is the Green Loop (Policy 5.12), which will run along NE 7<sup>th</sup> Ave past this site and which will connect to the Central Eastside via the new Blumenauer Bridge one block to the south. There are currently no Green Loop-specific standards for development in the Lloyd District (unlike for development in Downtown or the Pearl District). However, since the Green Loop is described as a linear park or open space (i.e., more than just a bike path or route) connecting all districts in the Central City, the Commission should consider how well the proposed development will support this new linear park concept using CCFD Guidelines *A4 – Use Unifying Elements*, *A5 – Enhance, Embellish, and Identify Areas*, *B1 – Reinforce and Enhance the Pedestrian System*, and *B5 – Make Plazas, Parks, and Open Space Successful*.

- b. **Development Standards – CX – Central Commercial base zone / Central City Plan District – Lloyd Subdistrict.**

- Though early in the design process, the proposal appears generally compliant with zoning code standards (allowed use, maximum setbacks, parking, loading, landscaping, windows, etc.). One anticipated Modification to parking space widths is noted above. Staff believes this Modification may be supportable as long as it does not impact pedestrian or vehicular movement outside the parking structure.

- c. **Streets – TSP Designations.**

- NE Oregon St is classified as a Major Transit Priority Street and Major City Walkway and as local service for all other modes.
- NE 7<sup>th</sup> Ave is classified as a Major City Bikeway and Major City Walkway and as local service for all other modes.
- NE Irving St is classified as local service for all modes.
- The subject site is located within the Lloyd Bicycle District and the Central City Pedestrian District.

2. **Natural.**

- a. The site is developed with existing single story buildings and surface parking lots. There are some existing trees along the periphery of the parking lots, but no other natural features on the site.

- b. The site slopes up from the southwest corner towards the northeast corner by about 10 feet.
- c. The predominant natural feature in the area is Sullivan's Gulch, which is one block south of the subject site.
- d. Views towards various parts of the Cascade Mountains, West Hills, and the Willamette River may be possible currently from the upper stories of the building, though these may be blocked by future development.

### 3. Built.

- a. The site lies at the southern edge of the Lloyd District and within close proximity to I-84. The Oregon Square Blocks, for which the Design Commission approved a new two-building office complex in 2021, lies kitty-corner to the northeast of the subject site.
- b. The new Earl Blumenauer bike/pedestrian bridge crossing Sullivan's Gulch and I-84 is under construction one block to the south of the subject site.
- c. The Portland Streetcar A Loop line runs south along NE 7<sup>th</sup> Ave and turns west at NE Oregon St, passing the subject site. The closest stop is 1.5 blocks to the west of the site.
- d. A parking garage structure for the Metro government building lies to the immediate south of the site. Another parking garage for the 600 Holladay building lies to the immediate north of the site. A third parking structure, this one for the State of Oregon office building, lies to the immediate east of the site. A surface parking lot currently abuts the subject site to the immediate west.

### 4. Context Guidelines that are not fully met.

- A2 – Emphasize Portland Themes. No information is provided on how this guideline is met.
- C1 – Enhance View Opportunities and C2-1 Maximize View Opportunities.
  - The full text for C1 reads: "Orient windows, entrances, balconies, and other building elements to surrounding points of interest and activity. Size and place new buildings to protect existing views and view corridors. Develop building facades that create visual connections to adjacent public spaces." The full text for C2-1 is the same as the title.
  - The transparency and reflectivity of the ground floor windows is not yet clear. Balconies should be included on the upper stories.

### 5. Lloyd District-specific context guidelines that apply to this site include:

- A5-1 – Develop Identifying Features. The full guideline reads: "Encourage the inclusion of features in the design of projects that give projects identity and a sense of place or significance within the District." Specific identifying features are not readily apparent or articulated yet. The site's adjacency to the Green Loop suggests that some significant relationship with this new urban design element should be included in the development.
- A5-2 – Accommodate or Incorporate Underground Utility Service. The full guideline reads: "Accommodate or Incorporate underground utility service to development projects." Underground services are expected.
- A5-3 – Incorporate Works of Art. The full guideline reads: "Incorporate works of art into development projects." No works of art are yet indicated, nor is there an indication of where they may be incorporated into the design through a RACC process.
- A5-5 – Use Public Right-of-Way Design Criteria Established for the Lloyd District. This guideline will be met through the public works permitting process, which will require that



the sidewalks be reconstructed to current Lloyd District standards. Such standard improvements are also exempt from Design Review.

- A5-6 – Incorporate Landscaping as an Integral Element of Design. The full guideline reads: “Incorporate landscaping as an integral element of design which is supportive of both the built and natural environment.” Landscape planters are indicated along the sidewalk on the east and north frontages, though these are under canopies and the building above. The planters proposed do not seem to be “an integral element” of the design, nor do they provide much in the way of support for the natural environment.
- A9-1 – Provide a Distinct Sense of Entry and Exit. The full guideline reads: “Design and develop gateways into and within the Lloyd District that are appropriate and relate to the district’s and subdistricts’ emerging characteristics.” The site is within eyeshot of the new 7th Ave ped/bike bridge over I-84, which is a new gateway into the Lloyd District. The southeast corner of the building has extra prominence in that respect and should respond to that condition.
- C1-1 – Integrate Parking. The full guideline reads: “Integrate parking in a manner that is attractive and complementary to the site and its surroundings. Design parking garage exterior to visually respect and integrate with adjacent buildings and environment.” A large portion of the ground level of the south façade is taken up by structured parking and other back-of-house uses. Particularly considering the surrounding context of parking garages, this pattern should not be continued.
- C10-1 – Use Masonry Materials. The full guideline reads: “Except for window glazing, use masonry types of materials as the predominant exterior material for building walls. Use modular stone or masonry materials on the building base or first floor of buildings whenever possible.” Masonry materials are proposed at the ground level; however fiber cement panels are proposed for the upper stories of the building. (Note that Equitone panels are indicated as the basis of design.) Fiber cement panels are not a masonry material and, depending upon the product, may not be of sufficient quality to meet Guideline C2 – Promote Quality and Permanence in Development either.
- C10-2 – Design Exterior Building Walls That Area Transparent in Glazed Areas and Sculptural in Surface. The full guideline reads: “Design exterior building walls that are transparent or translucent in the glazed areas and which are textural, sculptural, and articulated in surface character.” Texture is added to the building’s facades with proposed mullion extensions. The building has a somewhat sculptural massing with a simple division along the north-south axis. Transparency of the windows is not yet clear.
- C10-3 – Use Light Colors. The full guideline reads: “The use of light color values is preferred for the predominant exterior building materials. Darker value materials should be used to accent or articulate the design.” The primary building material is lightly colored. Darker colors are reserved for the ground floor, recesses on the upper stories, and accents.
- Staff believes that Guideline A5-5 – Incorporate Water Features does not apply to this proposal. Note that the full guideline here reads: “Enhance the quality of major public spaces by incorporating water features.” There are no major public spaces on or near this site.

## PUBLIC REALM

### 1. Site organization.

- a. The ground floor of the building includes most of the building's common spaces, including the building's lobby, a coworking space, leasing offices, fitness room, pool, sport court, mail rooms, and bicycle parking. These spaces, with generally large areas of windows facing NE 7<sup>th</sup> Ave and NE Oregon St, help to activate these streets and create the kind of vibrancy desired by CCFD *Guidelines A7 – Establish and Maintain a Sense of Urban Enclosure* and *A8 – Contribute to a Vibrant Streetscape*.
- b. A lower ground floor (due to the site's slope) along NE Irving is occupied primarily by the parking garage entry, bike storage, and utility rooms. Consequently, activation and vibrancy along NE Irving St is lacking.

### 2. Circulation System.

- a. The main lobby entry is located at the northeast corner of the building, helping to reinforce connections to the streetcar line and the Green Loop. A secondary entry to the bike storage room is provided
- b. Parking and loading are accessed from the southwest corner—at the low point of the site—off NE Irving St.

### 3. Public Realm Guidelines that are not fully met.

- a. A7 – Establish and Maintain a Sense of Urban Enclosure. Additional and deeper canopies should be provided along all three street frontages.
- b. A8 – Contribute to a Vibrant Streetscape and A8-1 – Incorporate Active Ground Level Uses in Parking Structures. No active ground level uses are proposed along the south façade at the parking structure, which occupies a large area of the south façade. Additional active ground level uses should be extended along this frontage. Similarly, additional ground floor windows into the bike storage area (even if they are translucent rather than transparent) would help to create a more vibrant ground level on the north elevation.
- c. B5 – Make Plazas, Parks, & Open Space Successful.
  - The full text of the guideline reads: “Orient building elements such as main entries, lobbies, windows, and balconies to face public parks, plazas, and open spaces. Where provided, integrate water features and/or public art to enhance the public open space. Develop locally-oriented pocket parks that incorporate amenities for nearby patrons.”
  - The Green Loop—a linear park and bikeway—passes the site. Although the building's main entry and lobby and ground floor and upper story windows are oriented towards NE 7<sup>th</sup> Ave, more could be done to help make the Green Loop successful. Balconies on the east elevation would help to better meet this guideline, as would public art and possibly even a water feature. Creation of a locally-oriented pocket park or other amenities would also help to support the Green Loop vision.
- d. B6 – Develop Weather Protection and B6-1 – Provide Pedestrian Rain Protection. Much of the proposed canopy ribbon that wraps the south and southern portion of the east façade and the canopies shown at the northern portion of the east façade do not extend far enough over the public sidewalk to provide meaningful weather protection. The extent of weather protection along the north façade is unclear.
- e. C7 – Design Corners that Build Active Intersections.
  - The full text of the guideline reads: “Use design elements including, but not limited to, varying building heights, changes in facade plane, large windows, awnings, canopies,

marquees, signs, and pedestrian entrances to highlight building corners. Locate flexible sidewalk-level retail opportunities at building corners. Locate stairs, elevators, and other upper floor building access points toward the middle of the block.”

- There is no special emphasis at either corner that acknowledges the context of the Green Loop crossing to the south or the turning of the streetcar line on the north, though this may not be as desired considering the general level of cohesiveness of the design. A better response to Guideline B5 may help here as well.
- f. C9 – Develop Flexible Sidewalk Level Spaces. Ground floor height is not clear since elevations are not scaled. A tall ground floor level is needed to allow for successful future tenant conversions. The site's slope may complicate future efforts to divide the ground level space for other uses.

## QUALITY & PERMANENCE

### 1. Exterior materials & resilience.

- a. As noted above, Equitone is proposed as the basis of design for the primary fiber cement cladding of the building. While this has been used successfully in at least one other similarly-scaled building in recent years (The Rodney at NW Glisan and 14<sup>th</sup>), it is not a masonry material as required by *Guideline C10-1*. If ultimately found to be acceptable in meeting *C10-1*, detailing of Equitone panels is also an important consideration. The Design Commission required panels affixed from only the rear on the lower levels of The Rodney, as opposed to the usual rivet fasteners to increase the overall quality of the material. Additionally, if other fiber cement materials are proposed to be substituted for Equitone, those often do not have the same density/strength as Equitone, nor do they have the same integral color, and the Commission has found on past projects that such qualities are particularly necessary to meet *Guideline C2 – Promote Quality and Permanence in Development*.
- b. Proposed masonry materials at the ground level are likely to be of sufficient quality, dependent upon ultimate material selection and detailing.
- c. Storefront windows are proposed along a large portion of the ground level façade and are likely to be of sufficient quality, dependent upon ultimate material selection and detailing. Clear glazing should be indicated at the ground level as well to meet public realm-related guidelines.
- d. The proposed upper story window type is not yet indicated in the drawings, though they are shown in a dark bronze color. An earlier drawing set appeared to indicate that these would be an aluminum window system with mullion cap extensions providing additional texture and articulation to the building.

### 2. Coherency.

- a. The overall design of the building appears to follow a clear concept, having a distinct base, middle, and top expression with a regularized placement of windows and articulating fins across the upper stories of the building.
- b. The ground floor, however, includes a large blank wall area on the ground floor of the south elevation (and to a lesser extent on the north elevation as well) that does not integrate as well with the overall design of the building. The wrap-around ribbon/canopy at the southeast corner of the building also seems foreign to the design.
- c. The top floor of the west elevation breaks from the pattern used at the top floor on the north and south elevations due to the proposed roof decks for the penthouse units. The resolution of the design of the top of the tower on the west elevation would benefit from



being more consistent with the north and south elevations rather than introducing a third type of design response for the top floor of the building.



CENTRAL CITY FDG (2003) + LLOYD SDG (1991)		PROJECT NAME: NE 7th Ave Apartments	CASE NUMBER: EA 22-119814 DA
DATE: 04/14/2022		PROJECT ARCHITECT: Mackenzie	
CONTEXT		MET?	Comments
A1: Integrate the River		Y	While this guideline could be better met by orienting the rooftop terrace to the west for views to the river, development on the vacant lot next door may obscure these views in the future. Upper story windows and views from the north and south of the roof terrace will provide views to the river.
	A1-1: Connect Public Facilities to the River	N/A	This site is not adjacent to the river.
A2: Emphasize Portland Themes		?	No information is provided on how this guideline is met.
A3: Respect the Portland Block Structures		Y	
	A3-1: Support a Convenient Pedestrian Linkage Through the Superblocks Between Convention Center and Lloyd Center	N/A	This site is not a superblock.
	A3-2: Make Superblock Plazas Inviting and Easily Accessible from Holladay Street	N/A	This site is not a superblock.
A5: Enhance, Embellish & Identify Areas		Y/N	The proposed development is generally contextual to the Lloyd District in its scale, massing, and proportions. Masonry materials should be used.
	A5-1: Develop Identifying Features	?	Specific identifying features are not articulated yet. The site's adjacency to the Green Loop suggests that some significant relationship with this new urban design element should be included in the development.
	A5-2: Accommodate or Incorporate Underground Utility Service	?	Utility services are not indicated yet but are expected.
	A5-3: Incorporate Works of Art	?	No works of art are yet indicated, nor is there an indication of where they may be incorporated into the design through a RACC process.
	A5-4: Incorporate Water Features	?	No water features are indicated. Water features may not be needed since there is no outdoor public space on the site.
	A5-5: Use Public Right-of-Way Design Criteria Established for the Lloyd District	Y	Will be met through public works permitting.
	A5-6: Incorporate Landscaping as an Integral Element of Design	?	Landscape planters are indicated along the sidewalk on the east and north frontages, though these are under canopies and the building above. The planters proposed do not seem to be "an integral element" of the design, nor do they provide much in the way of support for the natural environment.
	A5-7: Integrate the Civic Campus into the Lloyd District	N/A	
	A5-8: Integrate the Lloyd Shopping Center into the Lloyd District	N/A	
A6: Re-use, Rehabilitate, Restore Buildings		N/A	
A9: Strengthen Gateways		N/A	This site is not at a gateway location.

	A9-1: Provide a Distinct Sense of Entry and Exit	?	The site is within eyeshot of the new 7th Ave ped/bike bridge over I-84, which is a new gateway into the Lloyd District. The southeast corner of the building has extra prominence in that respect and should respond to that condition.
C1: Enhance View Opportunities		Y	Windows on the upper stories and the proposed penthouse amenity space and deck will afford views for residents to surrounding points of interest.
		?	The transparency and reflectivity of the ground floor windows is not yet clear.
		N	Balconies should be included on the upper stories.
	C1-1: Integrate Parking	Y	Underground structured parking is proposed, which will help to keep parking areas out of the public realm.
		N	A large portion of the ground level of the south façade is taken up by structured parking and other back-of-house uses. Particularly considering the surrounding context of parking garages, this pattern should not be continued.
	C1-2: Integrate Signs	?	
	C2-1: Maximize View Opportunities	Y/N	See C1.
C4: Complement the Context of Existing Buildings		Y/N	See A5.
	C10-1: Use Masonry Materials	Y	Masonry materials are proposed at the ground level.
		N	Fiber cement panels are proposed for the upper stories of the building. (Note that Equitone panels are indicated as the basis of design.) Fiber cement panels are not a masonry material and, depending upon the product, may not be of sufficient quality to meet Guideline C2 – Promote Quality and Permanence in Development either.
	C10-2: Design Exterior Building Walls That Are Transparent in Glazed Areas and Sculptural in Surface	Y	Texture is added to the building's facades with proposed mullion extensions. The building has a somewhat sculptural massing with a simple division along the north-south axis.
		?	Transparency of the windows is not yet clear.
	C10-3: Use Light Colors	Y	The primary building material is lightly colored. Darker colors are reserved for the ground floor, recesses on the upper stories, and accents.
PUBLIC REALM			
		MET?	Comments
A4: Use Unifying Elements		Y/N	See A5 and A5-5.
A7: Establish and Maintain a Sense of Urban Enclosure		Y	The scale of development is appropriate for the district. Canopies, where proposed, help to enclose and articulate the sidewalk edge. Setback areas and planters at the ground floor along portions of NE 7th and NE Oregon provide additional articulation.
		N	Additional and deeper canopies should be provided along all three street frontages.
A8: Contribute to a Vibrant Streetscape		Y	The ground floor along NE 7th and most of NE Oregon St has active interior spaces, such as the building's lobby, lounge, coworking space, spa, and fitness room.
		N	See A8-1.
	A8-1: Incorporate Active Ground Level Uses in Parking Structures	N	No active ground level uses are proposed along the south façade at the parking structure, which occupies a large area of the south façade. Additional active ground level uses should be extended along this frontage. Similarly, additional ground floor windows into the bike storage area (even if they are translucent rather than transparent) would help to create a more vibrant ground level on the north elevation.
B1: Reinforce and Enhance the Pedestrian System		?	Zones of the sidewalk are not clear yet.
	B1-1: Protect Pedestrian Areas from Mechanical Exhaust	?	
	B1-2: Incorporate Additional Lighting	?	



	B1-3: Design Projects to Attract Pedestrians to the Broadway/Weidler Corridor	N/A	
B2: Protect the Pedestrian		?	
B3: Bridge Pedestrian Obstacles		?	Will likely be met through public works permit.
	B3-1: Provide Pedestrian Crossings Spaced at Traditional One-Block Intervals	?	Will likely be met through public works permit.
	B3-2: Improve Pedestrian Crossings on NE Broadway	N/A	
B4: Provide Stopping and Viewing Places		Y	A recessed area is shown at the main lobby entrance, providing room for residents and visitors to stop and socialize outside the building and off the public sidewalk.
B5: Make Plazas, Parks & Open Space Successful		?	The Green Loop—a linear park and bikeway—passes the site. Although the building's main entry and lobby and ground floor and upper story windows are oriented towards NE 7th Ave, more could be done to help make the Green Loop successful. Balconies on the east elevation would help to better meet this guideline, as would public art and possibly even a water feature. Creation of a locally-oriented pocket park or other amenities would also help to support the Green Loop vision.
B6: Develop Weather Protection		N	Much of the proposed canopy ribbon that wraps the south and southern portion of the east façade and the canopies shown at the northern portion of the east façade do not extend far enough over the public sidewalk to provide meaningful weather protection. The extent of weather protection along the north façade is unclear.
	B6-1: Provide Pedestrian Rain Protection	N	See B6.
B7: Integrate Barrier-Free Design		Y	
C3: Respect Architectural Integrity		N/A	
	C3-1: Design to Enhance Existing Themes in the Broadway/ Weidler Corridor	N/A	
	C3-2: Orient Development Along the Lloyd District's Eastern Edge Toward Adjacent Neighborhoods	N/A	
C6: Develop Transitions Between Buildings & Public Spaces		Y	See B4.
	C6-1: Step-Back Upper Building Floors Along Holladay Street	N/A	
C7: Design Corners that Build Active Intersections		Y	Stairs and elevators are located away from the building's corners.
		?	There is no special emphasis at either corner that acknowledges the context of the Green Loop crossing to the south or the turning of the streetcar line on the north, though this may not be as desired considering the general level of cohesiveness of the design. A better response to Guideline B5 may help here as well.
C8: Differentiate the Sidewalk Level of Buildings		Y	The sidewalk level of the building is clearly differentiated from the upper stories with storefront windows and other changes in material.
C9: Develop Flexible Sidewalk Level Spaces		?	Ground floor height is not clear since elevations are not scaled A tall ground floor level is needed to allow for successful future tenant conversions. The site's slope may complicate future efforts to divide the ground level space for other uses.
C12: Integrate Exterior Lighting		?	
C13: Integrate Signs		?	

QUALITY AND PERMANENCE		
	MET?	Comments
C2: Promote Permanence & Quality in Design	?	<i>Fiber cement panels may not be of high enough quality for a tower of this scale.</i>
C5: Design for Coherency	Y	<i>The overall design of the building appears to follow a clear concept, having a distinct base, middle, and top expression with a regularized placement of windows and articulating fins across the upper stories of the building.</i>
	N	<i>The ground floor, however, includes a large blank wall area on the ground floor of the south elevation (and to a lesser extent on the north elevation as well) that does not integrate as well with the overall design of the building. The wrap-around ribbon/canopy at the southeast corner of the building also seems foreign to the design.</i>  <i>The top floor of the west elevation breaks from the pattern used at the top floor on the north and south elevations due to the proposed roof decks for the penthouse units. The resolution of the design of the top of the tower on the west elevation would benefit from being more consistent with the north and south elevations rather than introducing a third type of design response for the top floor of the building.</i>
C10: Integrate Encroachments	?	
C11: Integrate Roofs and Use Rooftops	Y	<i>Rooftop amenity deck, ecoroofs, and mechanical units.</i>
	?	<i>While the size of mechanical units is not yet clear, there is no apparent integrated screening for the units indicated yet.</i>

## Comments on NE 7<sup>th</sup> Apts DAR

### CONTEXT:

- Context of the area is variable and being defined block by block by redevelopment in the last 20 years. The proposed building height and massing are generally compatible at a course level.
- Building orientation is basically appropriate for pedestrian and vehicular entrances.
- A5-1 Identifying Features: not enough information to decide. I agree with staff that this building could really take advantage of the Green Loop amenity on NE 7<sup>th</sup>. The applicant should work with PBOT to determine the cross section design and really compliment it with ground floor uses, ground floor height, main entrance and canopy design.
- A5-6 Landscaping as an Integral Element and A9-1 Distinct Sense of Entry: The Green Loop and Blumenauer Bridge are great public realm features to amplify in the building and site design.
- A5-3 Art: a great opportunity to highlight Green Loop. Look at what Indianapolis Cultural Trail has done with existing and new development.

### PUBLIC REALM

- A7: Canopy extent and depth needs more work. NE 7<sup>th</sup> become a primary pedestrian routes with the Green Loop and Blumenauer Bridge.
- A8 Vibrant Streetscape and A8-1 Active Ground Floor Uses in Parking Garages: Corners are not differentiated but it may be that the proposed use or flexibility to convert in the future at the SE corner on 7<sup>th</sup>/Irving needs more work. The tall windows and clerestory window above the canopy offer great transparency out to the future Green Loop and Blumenauer Bridge. The 'sports court' (what – a racquetball court with two players?) seems a missed opportunity. Agree with staff, more work is needed on the remainder of the south façade to minimize blank wall. Stems walls that extend horizontally at the north section of the east elevation, then change to sloping glass to the sloping grade at mid-block seems not well integrated. Combine with comments below about the discontinuity of the canopy system are re-work before our next hearing.
- B6 Weather Protection and B6-1 Rain Protection: increase the depth of cover. The ribbon design of the canopy that turns down at mid block, leaving a blank zone, then a different canopy expression at the NE corner are well integrated as a whole canopy system. And it makes the main entrance to the lobby not as prominent as it should be. Develop a system that gracefully steps down the street grade from north to south.
- While the elevations of vertical windows and sun fins have a high degree of continuity, the building doesn't express much life of its inhabitants on its primary east elevation. What about balconies – projected, recessed, Juliette, etc?
- Sun fins – what I have seen in building with serious solar control is vertical fins used in east and west elevations because the sun angle is lower and horizontal eyebrow fins over windows in the south elevation. Question – are the vertical fins on the south elevation just visual elements or real solar control elements?



#### QUALITY AND PERMANENCE

- Overall design continuity and clarity: I agree with staff about base, middle, top simplicity. See comments in Public Realm about window stem walls and canopy design continuity.
- The penthouse with outdoor amenity spaces appears to be a good concept and allow residents to take advantage of great views to the SE, S and SW of the City, river, and West Hills. However, we need a 3D birdseye view to better understand how the top of the building is actually designed.
- Equitone panels: I defer to my architect colleagues on this topic.

Commissioner Brian McCarter



City of Portland  
**Design Commission**

## Design Advice Request

EA 22-119814 DA

### **NE 7<sup>th</sup> Ave Apartments**

April 21, 2022

Staff Presentation

## **Context**

Location

Zoning

Approval Criteria

Context

Program Overview

## **Applicant Presentation**

## **Discussion Topics**

Context

Public Realm

Quality & Permanence

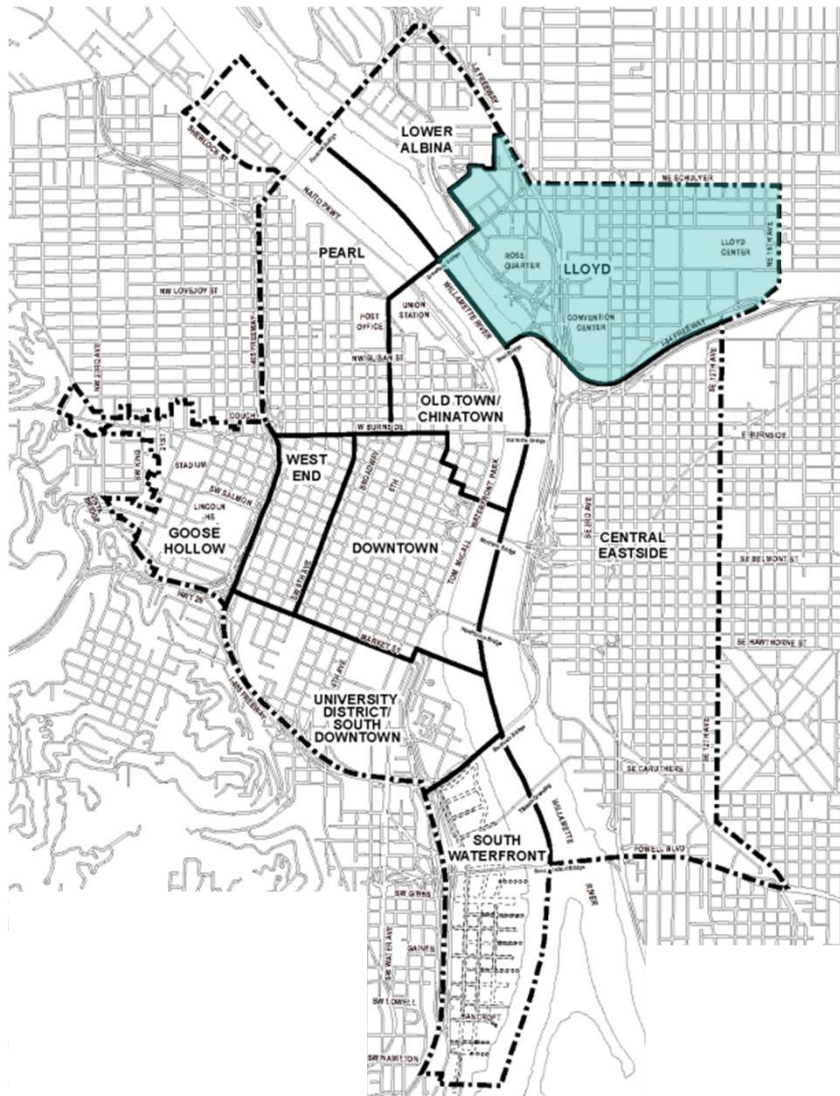
Modifications

## **Public Comments**

## **Commission Discussion**



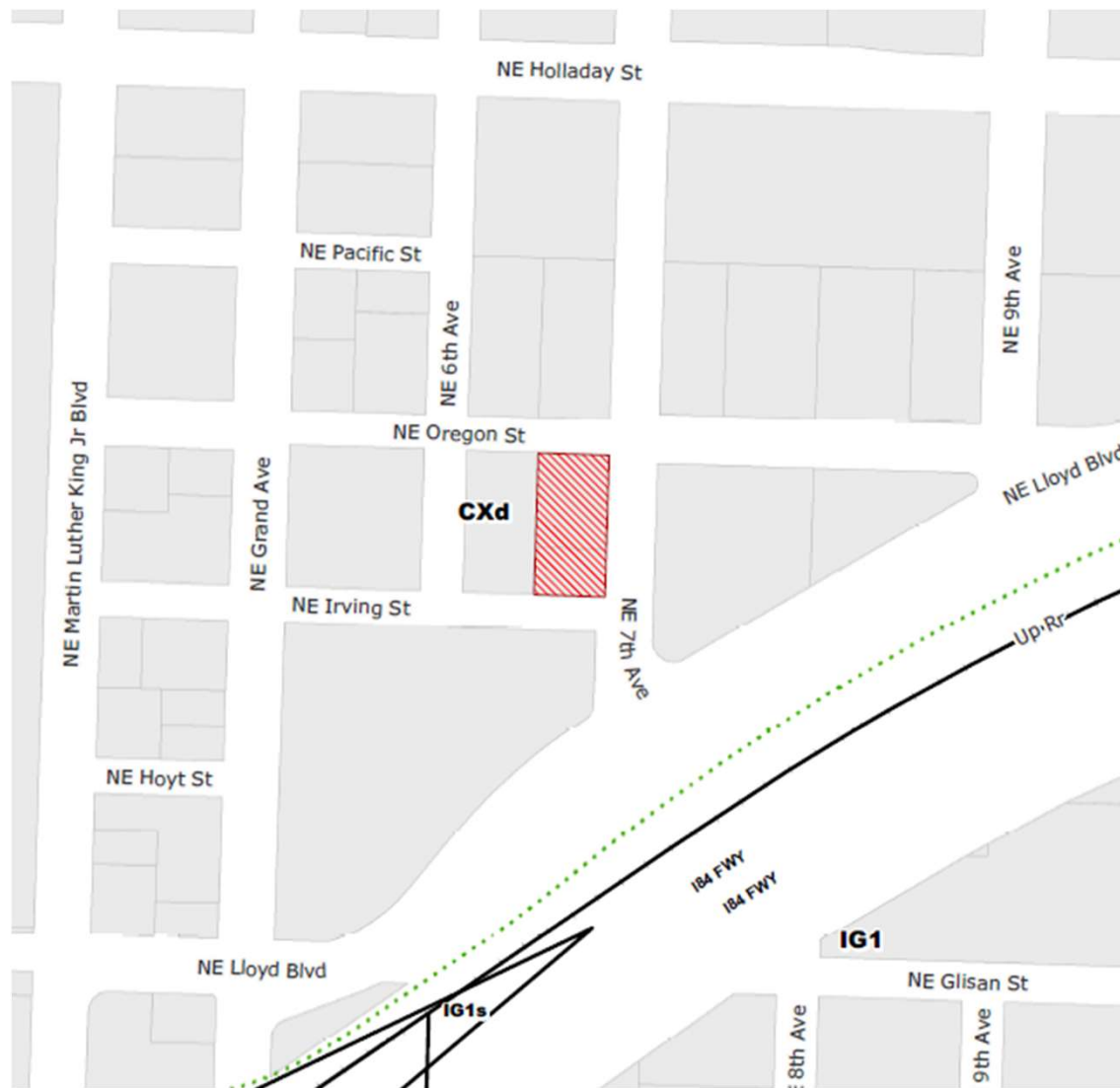
# Context



# Location

Central City Plan District

Lloyd Subdistrict



# Zoning

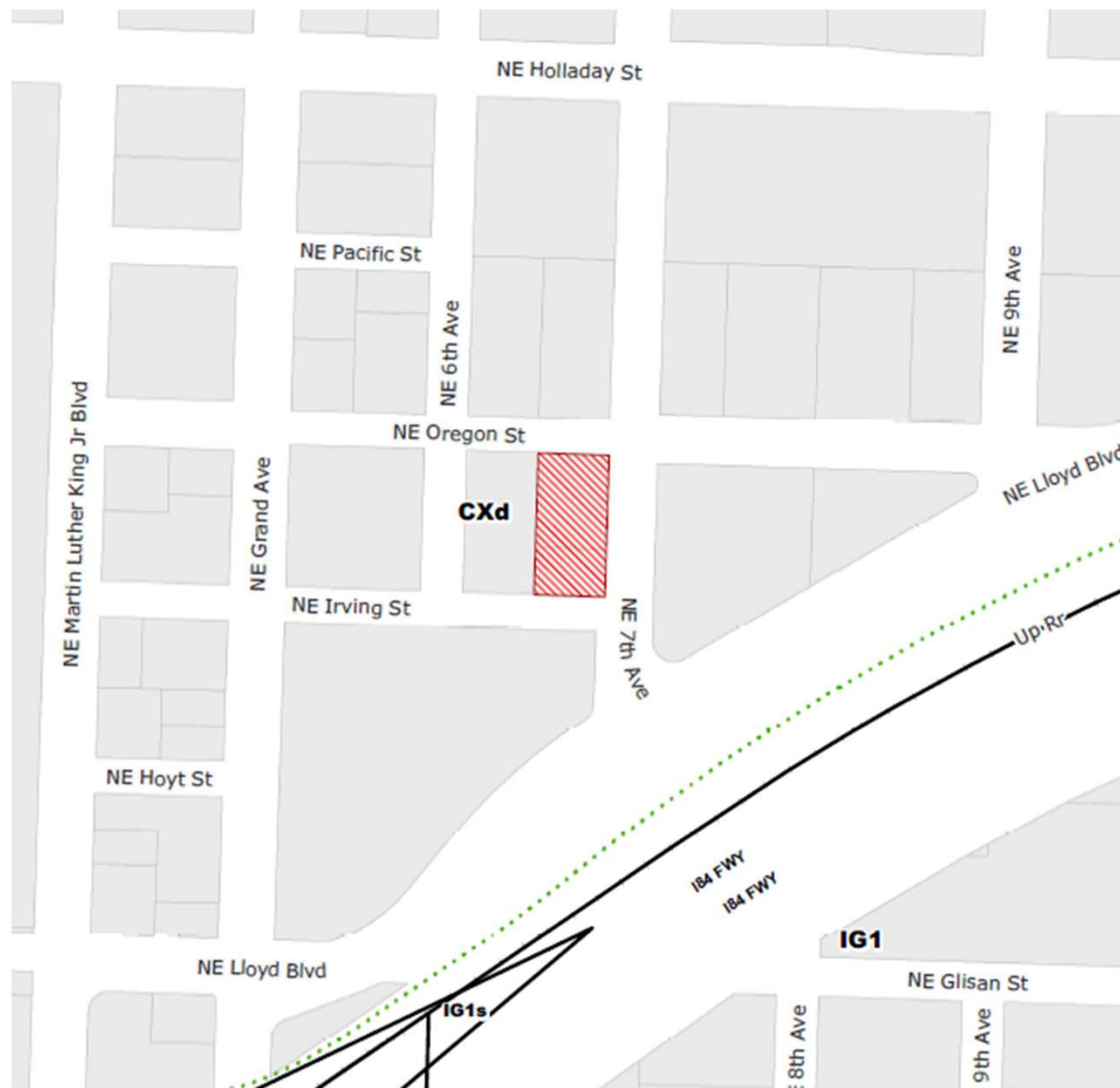
**Base Zone:**  
CX, Central Commercial

**Overlay:**  
Design Overlay

**Floor Area Ratio:**  
9:1 base + 3:1 bonus  
unlimited transfers allowed  
after bonus

**Height:**  
250' max base  
325' max with bonus





# Approval Criteria

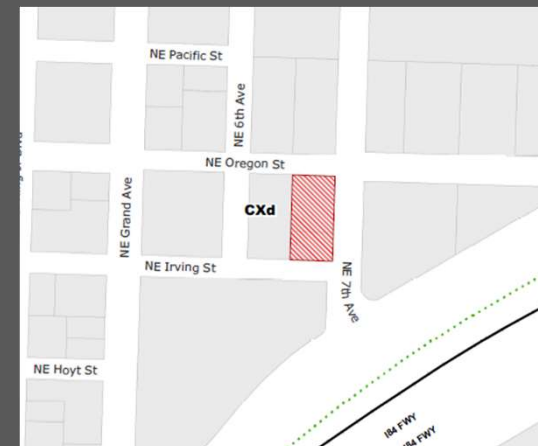
Central City Fundamental Design Guidelines

Special Guidelines for the Design Zone of the Lloyd District of the Central City Plan

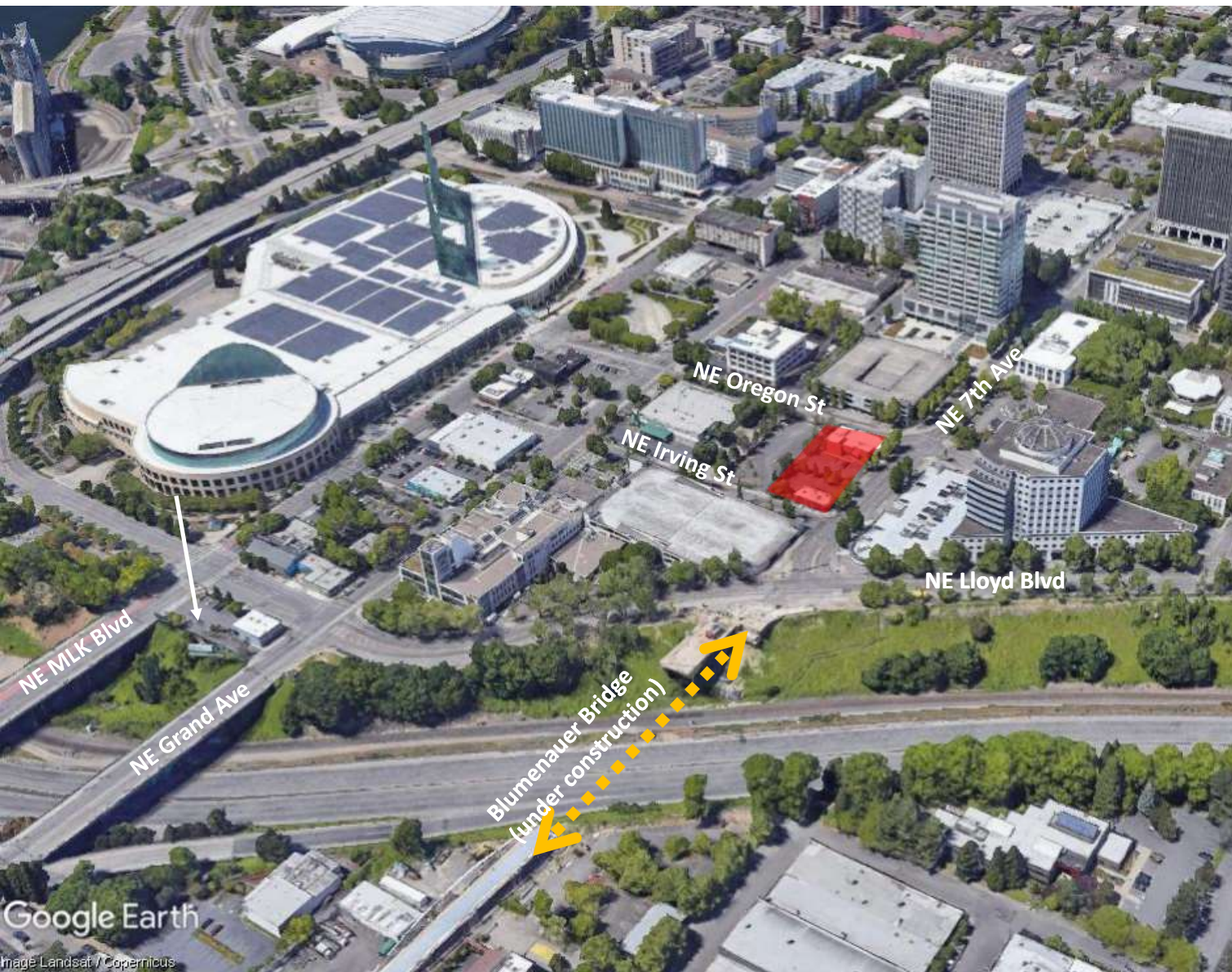


## Context

- Southern edge of Lloyd District
- Near future Blumenauer Bridge / Green Loop
- Streetcar line turns at NE corner of site, stop 1-1/2 blocks to the west

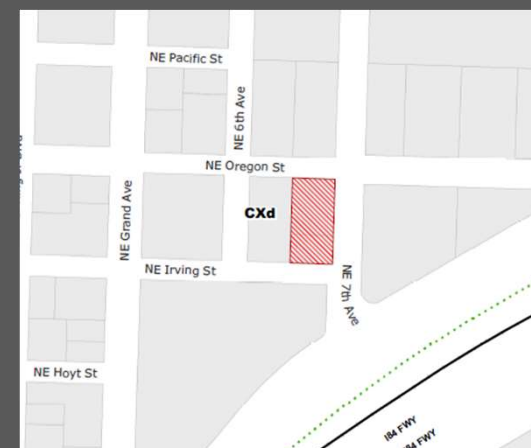






## Context

- Southern edge of Lloyd District
- Near future Blumenauer Bridge / Green Loop
- Streetcar line turns at NE corner of site, stop 1-1/2 blocks to the west

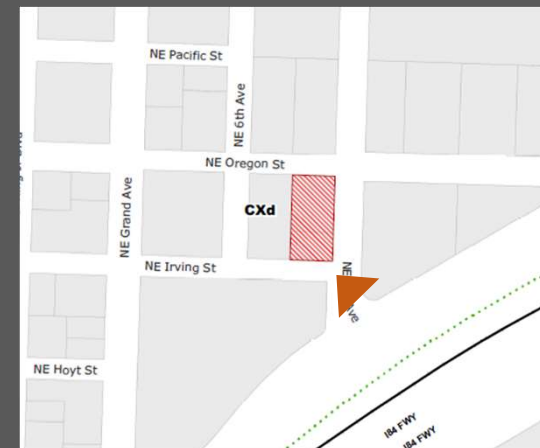






## Context

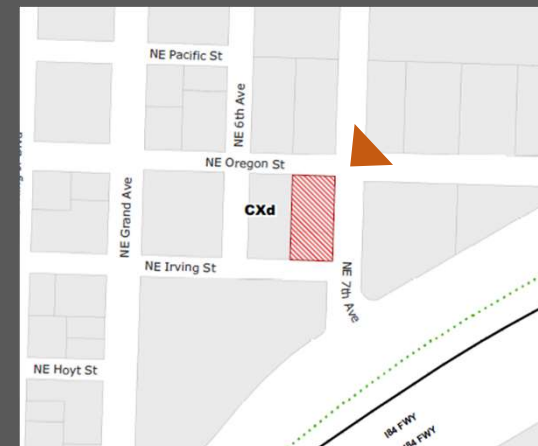
- SE corner of site





## Context

- NE corner of site



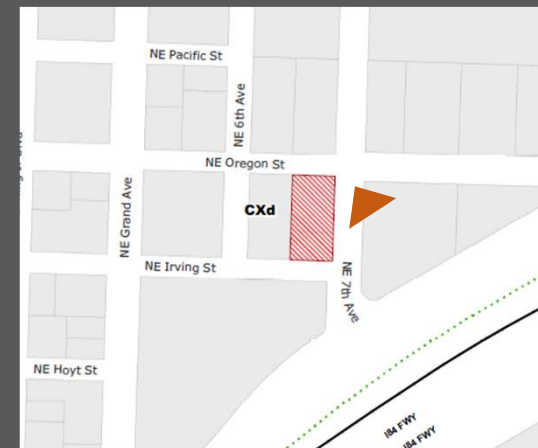




Google

## Context

- Parking garage to the north of the site across NE Oregon St

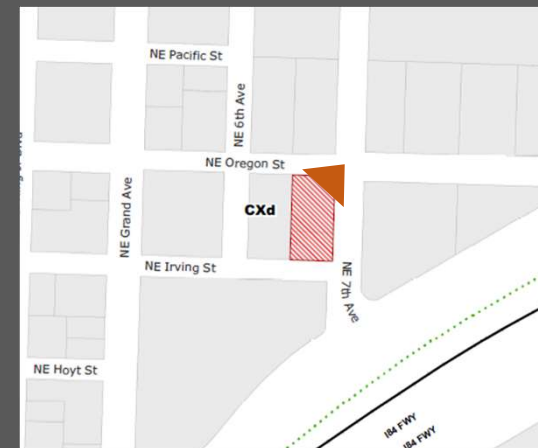






## Context

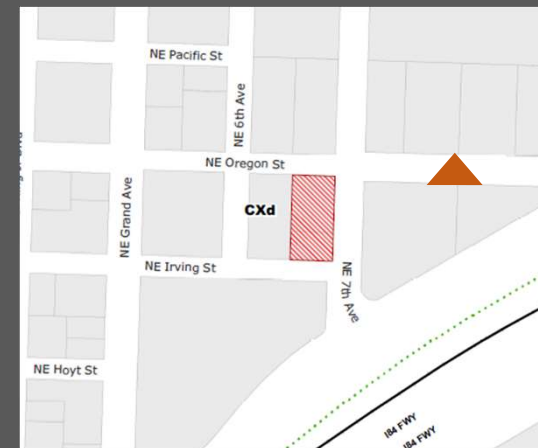
- Existing development at Oregon Square to the NE of the site





## Context

- Oregon Square redevelopment proposal approved on 10/1/2020 and expires 1/1/2024

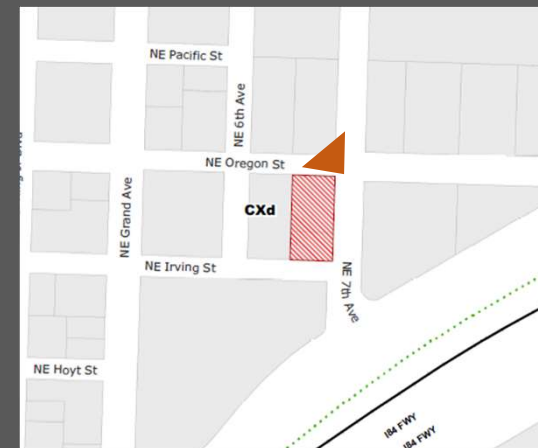






## Context

- Parking garage to the east of the site across NE 7<sup>th</sup> Ave

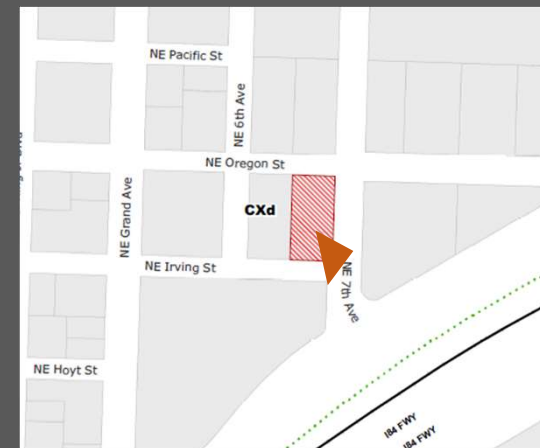






## Context

- Parking garage to the east of the site across NE 7<sup>th</sup> Ave

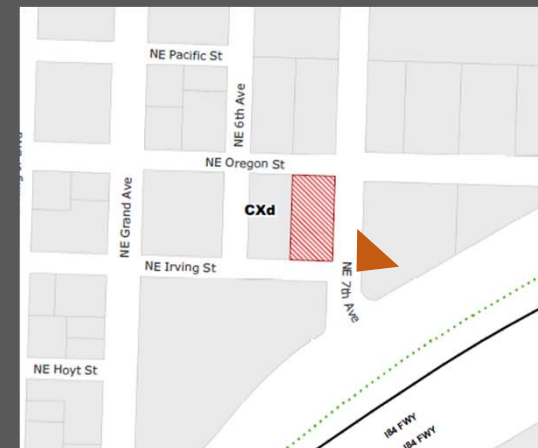






## Context

- Parking garage to the south of the site across NE Irving St

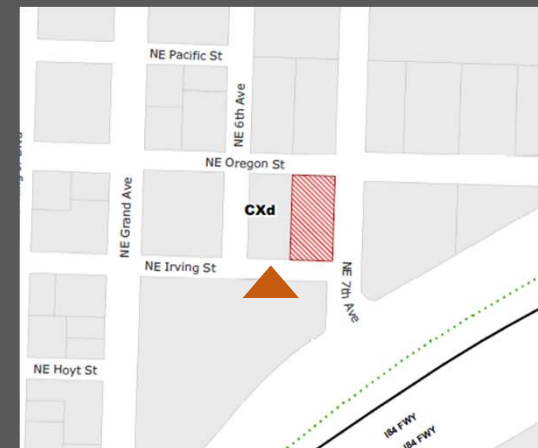






## Context

- Parking lot on the site immediately west of the subject site

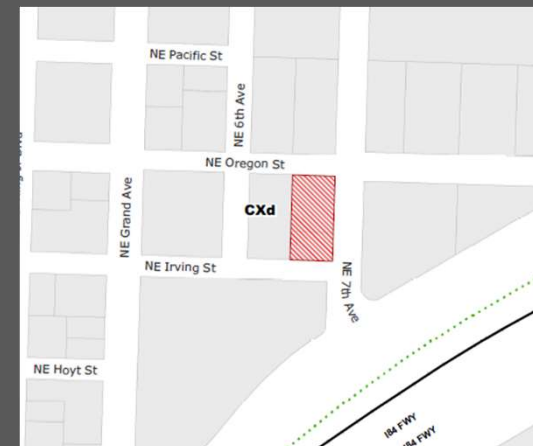






## Program Overview

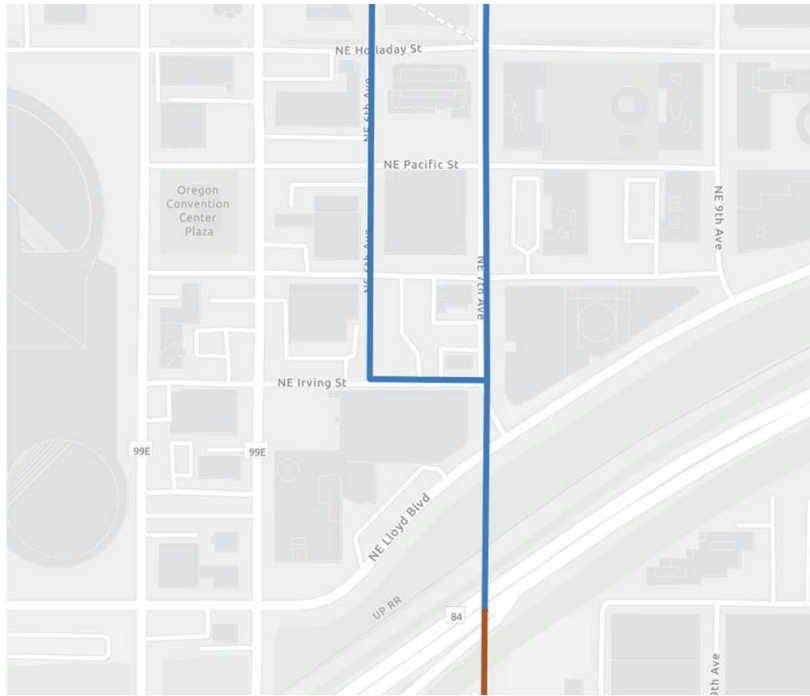
- 265 dwelling units
- Ground floor amenities for residents
- Structured parking that is mostly below grade
- Amenity deck and room on Level 15



# Applicant Presentation

# Discussion Topics



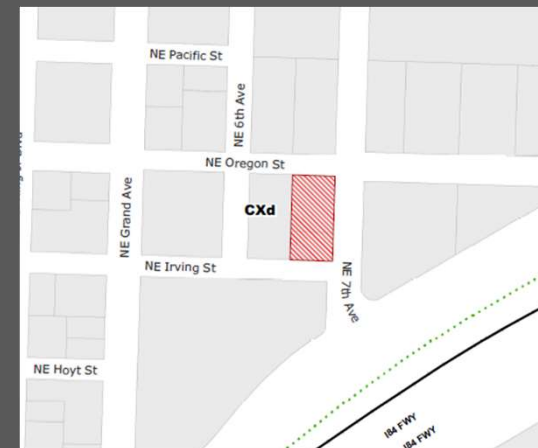


## Green Loop

The most relevant policy from CC2035 for development on this site is the Green Loop (Policy 5.12), which will run along NE 7<sup>th</sup> Ave past this site, and which will connect to the Central Eastside via the new Blumenauer Bridge one block to the south. There are currently no Green Loop-specific standards for development in the Lloyd District (unlike for development in Downtown or the Pearl District).

## Context

1. Green Loop
2. View Opportunities
3. Lloyd District-specific Guidelines



## HOW DO YOU KNOW YOU'RE ON THE GREEN LOOP?



## Green Loop

However, since the Green Loop is described as a linear park or open space (i.e., more than just a bike path or route) connecting all districts in the Central City, the Commission should consider how well the proposed development will support this new linear park concept using CCFD Guidelines A4 – *Use Unifying Elements*, A5 – *Enhance, Embellish, and Identify Areas*, B1 – *Reinforce and Enhance the Pedestrian System*, and B5 – *Make Plazas, Parks, and Open Space Successful*.

### THE GREEN LOOP

## DESIGN PRINCIPLES

#### ① Building Orientation

New development will be encouraged to orient its storefronts or building lobbies toward the Green Loop. New ground floor activity will provide greater visibility to the loop and create a safe and more vibrant environment.

#### ② Multi-Use Path

Paths that can accommodate a variety of different active uses including walking, jogging, and biking will be a defining feature of the Green Loop. Depending on the context these uses can be clustered together or separated by greenery or other features.

#### ③ Physical Separation

The Green Loop concept includes physically separated paths to minimize conflicts between cyclists, pedestrians, and vehicles. These separated corridors will create safer, more intuitive pathways through the Central City for walkers, bikers and joggers.

#### ④ Connected Canopy

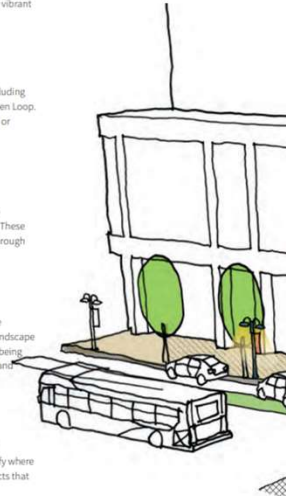
A key wayfinding element of the Green Loop will be a distinctive approach to trees and other green features. The character of landscape plantings will vary along different segments of the Green Loop, being responsive to adjacent needs while helping to clarify the route and improve environmental performance.

#### ⑤ Branding/Identity

The paths and adjacent properties will feature wayfinding and environmental design tools to help residents and visitors identify where they are while reflecting the local character of the various districts that the loop moves through.

#### ⑥ Unique Street Furnishings

Street furnishings along the Green Loop will help distinguish the path, emphasizing its linear park environment and supporting activity nodes. The specific amenities and their locations will vary with right-of-way width and the adjacent ground floor uses.

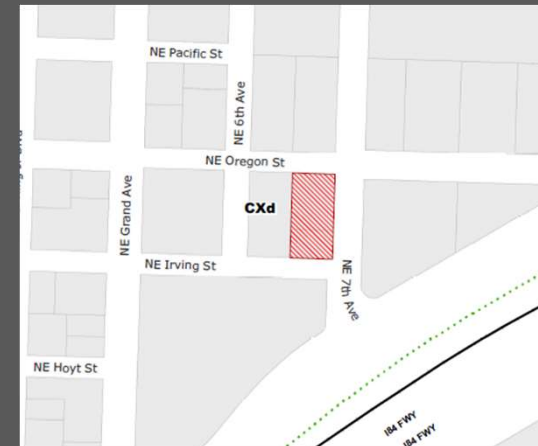


## Context

### 1. Green Loop

### 2. View Opportunities

### 3. Lloyd District-specific Guidelines



### 1 Building Orientation

New development will be encouraged to orient its storefronts or building lobbies toward the Green Loop. New ground floor activity will provide greater visibility to the loop and create a safe and more vibrant environment.

### 2 Multi-Use Path

Paths that can accommodate a variety of different active uses including walking, jogging, and biking will be a defining feature of the Green Loop. Depending on the context these uses can be clustered together or separated by greenery or other features.

### 3 Physical Separation

The Green Loop concept includes physically separated paths to minimize conflicts between cyclists, pedestrians, and vehicles. These separated corridors will create safer, more intuitive pathways through the Central City for walkers, bikers and joggers.

### 4 Connected Canopy

A key wayfinding element of the Green Loop will be a distinctive approach to trees and other green features. The character of landscape plantings will vary along different segments of the Green Loop, being responsive to adjacent needs while helping to clarify the route and improve environmental performance.

### 5 Branding/Identity

The paths and adjacent properties will feature wayfinding and environmental design tools to help residents and visitors identify where they are while reflecting the local character of the various districts that the loop moves through.

### 6 Unique Street Furnishings

Street furnishings along the Green Loop will help distinguish the path, emphasizing its linear park environment and supporting activity nodes. The specific amenities and their locations will vary with right-of-way width and the adjacent ground floor uses.

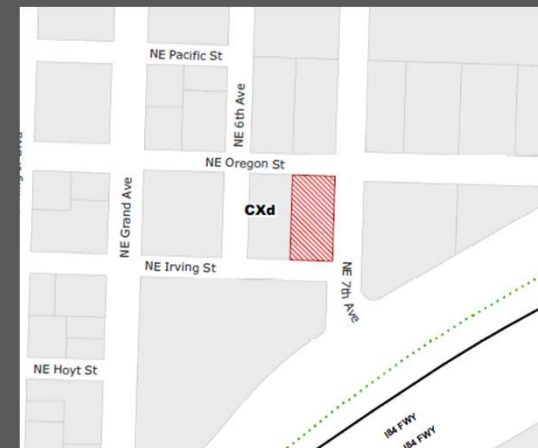


## Context

### 1. Green Loop

### 2. View Opportunities

### 3. Lloyd District-specific Guidelines







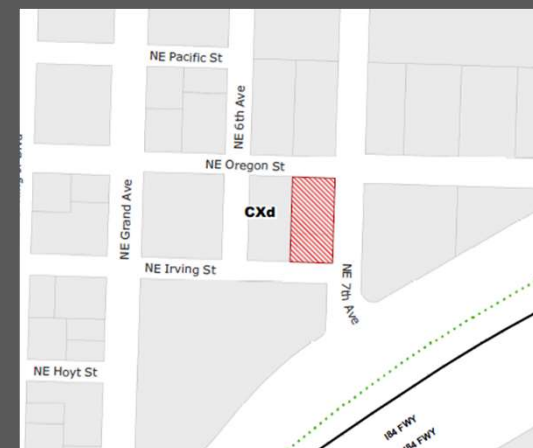
## View Opportunities

The transparency and reflectivity of the ground floor windows is not yet clear. Balconies should be included on the upper stories.

*C1 – Enhance View Opportunities and C2-1 Maximize View Opportunities*

## Context

1. Green Loop
2. View Opportunities
3. Lloyd District-specific Guidelines





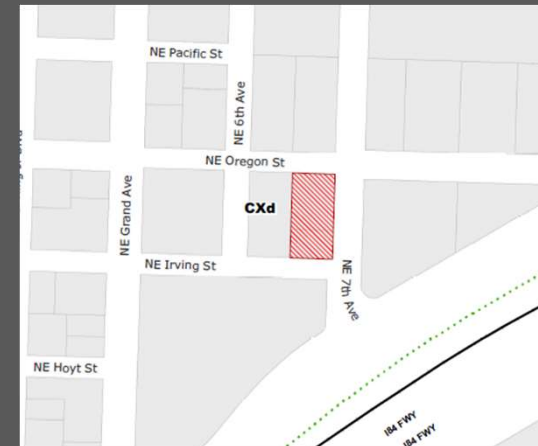
## Identifying Features, Portland Themes, Entry/Exit

Specific identifying features are not readily apparent or articulated yet. The site's adjacency to the Green Loop suggests that some significant relationship with this new urban design element should be included in the development.

*A2 Emphasize Portland Themes and A5-1 Develop Identifying Features*

## Context

1. Green Loop
2. View Opportunities
3. Lloyd District-specific Guidelines





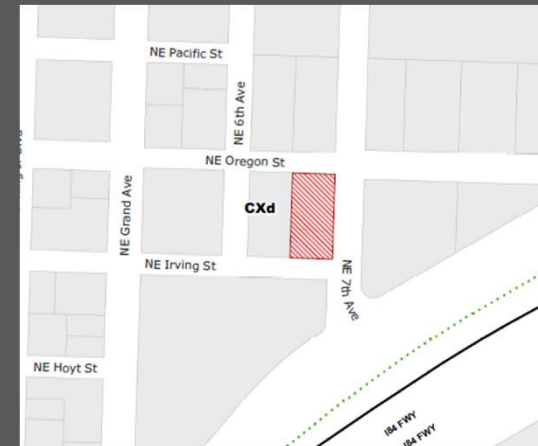
## Identifying Features, Portland Themes, Entry/Exit

The site is within eyeshot of the new 7th Ave ped/bike bridge over I-84, which is a new gateway into the Lloyd District. The southeast corner of the building has extra prominence in that respect and should respond to that condition.

*A9-1 Provide a Distinct Sense of Entry and Exit*

## Context

1. Green Loop
2. View Opportunities
3. Lloyd District-specific Guidelines







## Context

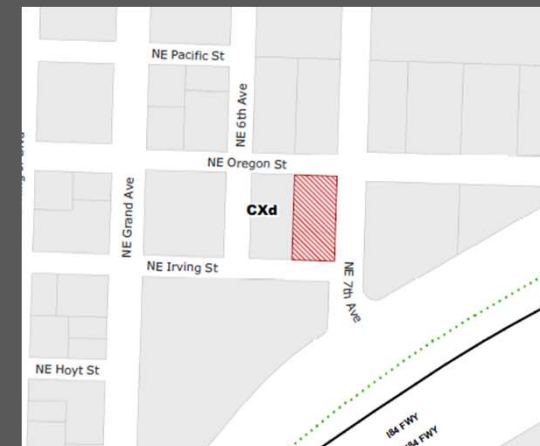
1. Green Loop
2. View Opportunities
3. Lloyd District-specific Guidelines

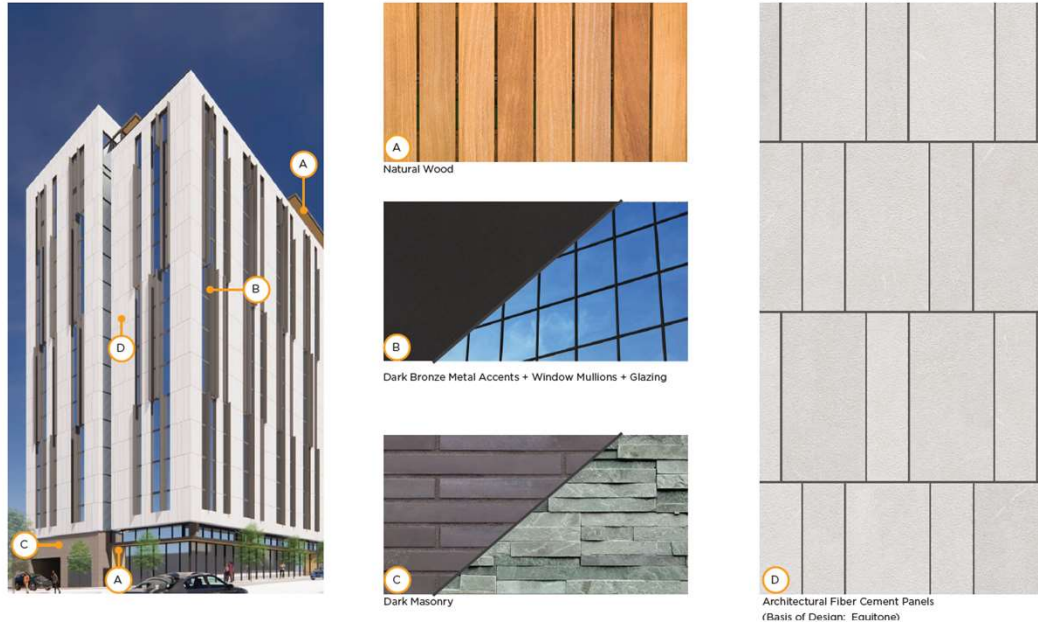
## Works of Art & Incorporate Landscaping

No works of art are yet indicated, nor is there an indication of where they may be incorporated into the design through a RACC process.

Landscape planters are indicated along the sidewalk on the east and north frontages, though these are under canopies and the building above. The planters proposed do not seem to be “an integral element” of the design, nor do they provide much in the way of support for the natural environment.

*A5-3 Incorporate Works of Art and A5-6 Incorporate Landscaping as an Integral Element of Design*





## Masonry, Transparency & Sculpture, & Light Colors

Masonry materials are proposed at the ground level; however, fiber cement panels are proposed for the upper stories of the building.

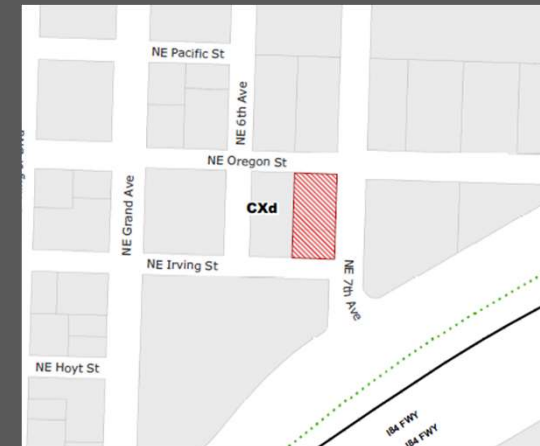
Texture is added to the building's facades with proposed mullion extensions. The building has a somewhat sculptural massing with a simple division along the north-south axis. Transparency of the windows is not yet clear.

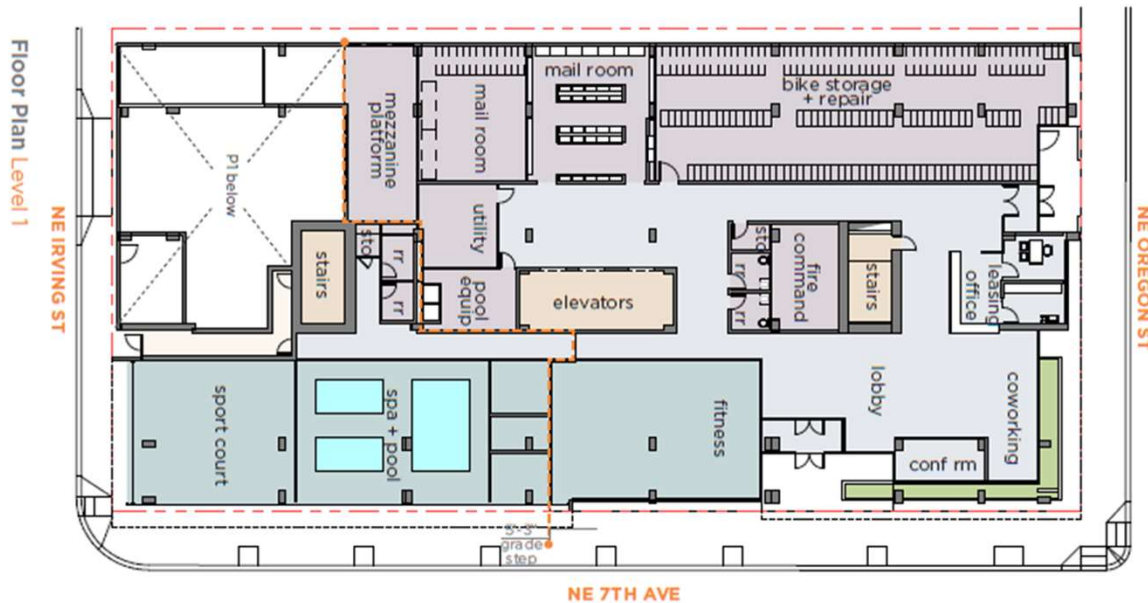
The primary building material is lightly colored. Darker colors are reserved for the ground floor, recesses on the upper stories, and accents.

*C10-1 Use Masonry Materials, C10-2 Design Exterior Building Walls That Are Transparent in Glazed Areas and Sculptural in Surface, and C10-3 Use Light Colors*

## Context

1. Green Loop
2. View Opportunities
3. Lloyd District-specific Guidelines





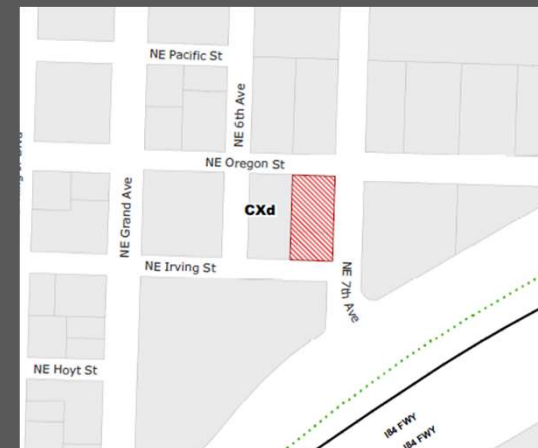
## Ground Floor

The ground floor of the building includes most of the building's common spaces, including the building's lobby, a coworking space, leasing offices, fitness room, pool, sport court, mail rooms, and bicycle parking. These spaces, with generally large areas of windows facing NE 7<sup>th</sup> Ave and NE Oregon St, help to activate these streets and create vibrancy on the street.

*A8 Contribute to a Vibrant Streetscape*

## Public Realm

1. Ground Floor
2. Parking Garage
3. Green Loop
4. Weather Protection
5. Corners







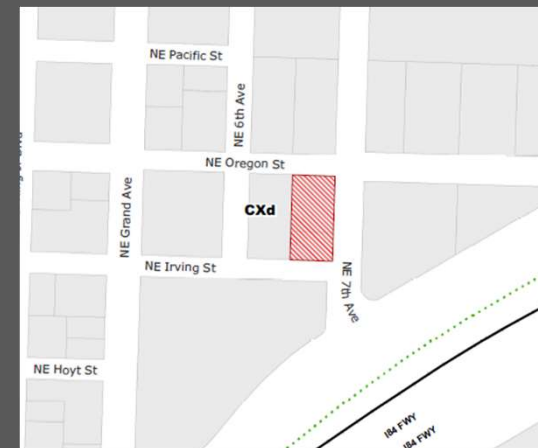
## Ground Floor

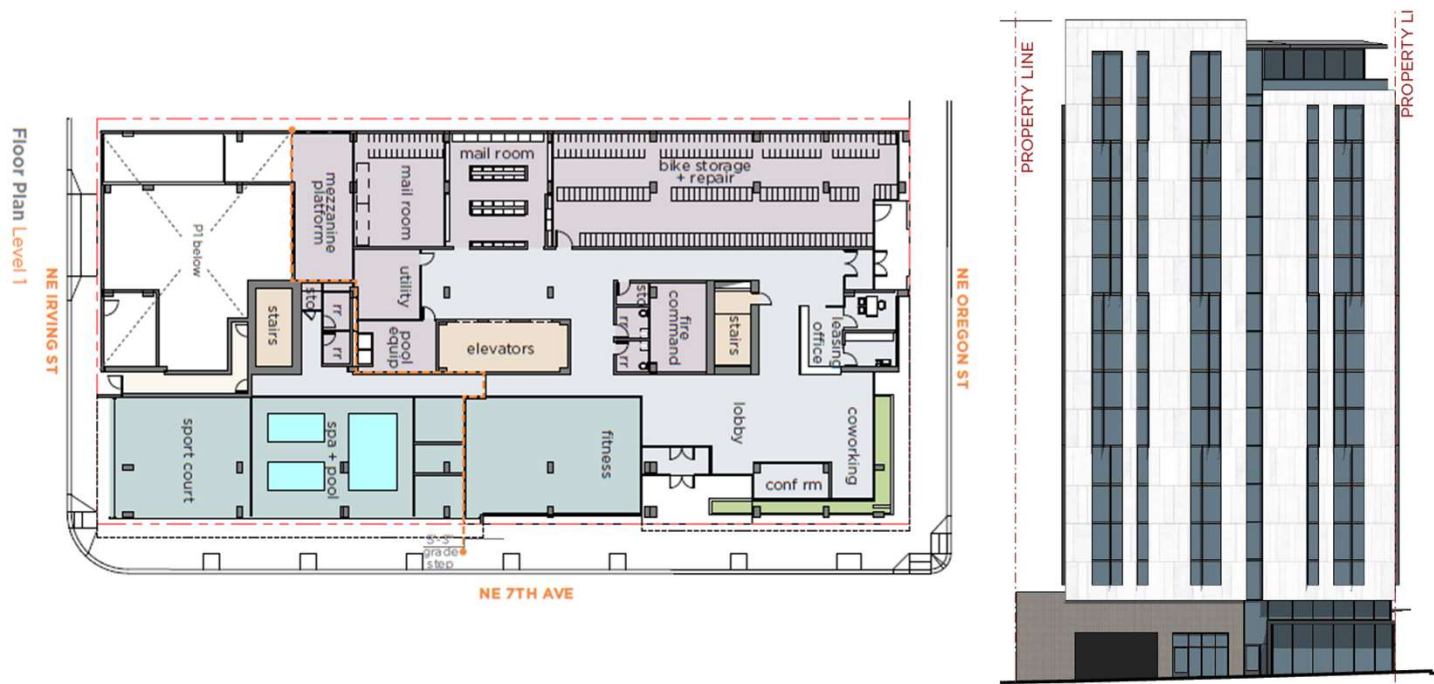
Ground floor height is not clear since elevations are not scaled. A tall ground floor level is needed to allow for successful future tenant conversions. The site's slope may complicate future efforts to divide the ground level space for other uses.

*C9 Develop Flexible Sidewalk Level Spaces*

## Public Realm

1. Ground Floor
2. Parking Garage
3. Green Loop
4. Weather Protection
5. Corners





## Parking Garage & Bike Parking

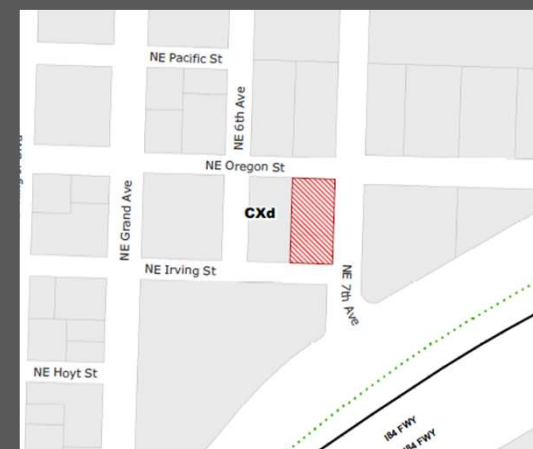
No active ground level uses are proposed along the south façade at the parking structure, which occupies a large area of the south façade. Additional active ground level uses should be extended along this frontage. Similarly, additional ground floor windows into the bike storage area (even if they are translucent rather than transparent) would help to create a more vibrant ground level on the north elevation.

The pattern of large parking garage frontages in the district should not be continued.

*A8-1 Incorporate Active Ground Level Uses in Parking Structures and C1-1 Integrate Parking*

## Public Realm

1. Ground Floor
2. Parking Garage
3. Green Loop
4. Weather Protection
5. Corners



HOW DO YOU KNOW YOU'RE ON  
THE GREEN LOOP?



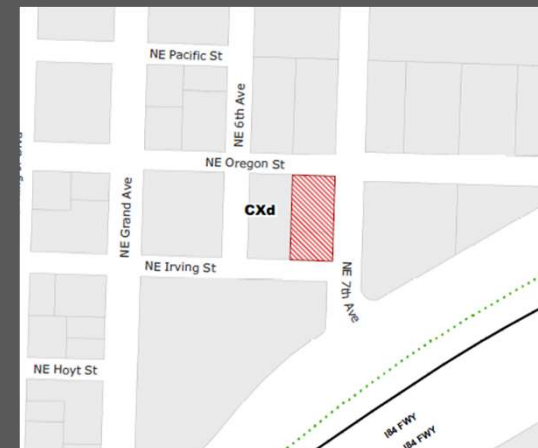
## Green Loop

Although the building's main entry and lobby and ground floor and upper story windows are oriented towards NE 7<sup>th</sup> Ave, more could be done to help make the Green Loop successful. Balconies on the east elevation would help to better meet this guideline, as would public art and possibly even a water feature. Creation of a locally-oriented pocket park, integral landscaping, and other amenities would also help to support the Green Loop vision.

*B5 Make Plazas, Parks, & Open Space Successful*

## Public Realm

1. Ground Floor
2. Parking Garage
- 3. Green Loop**
4. Weather Protection
5. Corners







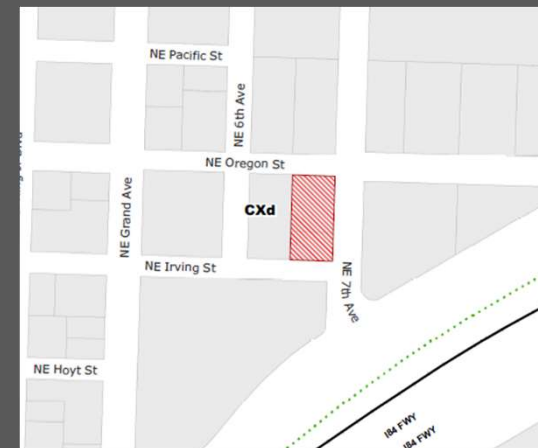
## Weather Protection

Much of the proposed canopy ribbon that wraps the south and southern portion of the east façade and the canopies shown at the northern portion of the east façade do not extend far enough over the public sidewalk to provide meaningful weather protection. The extent of weather protection along the north façade is unclear.

*B6 Develop Weather Protection and B6-1 Provide Pedestrian Rain Protection*

## Public Realm

1. Ground Floor
2. Parking Garage
3. Green Loop
- 4. Weather Protection**
5. Corners





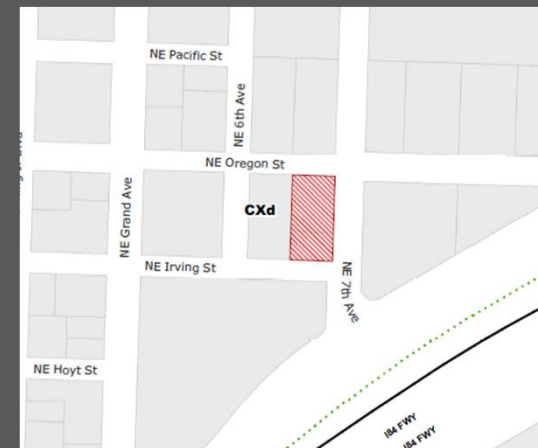
## Corners

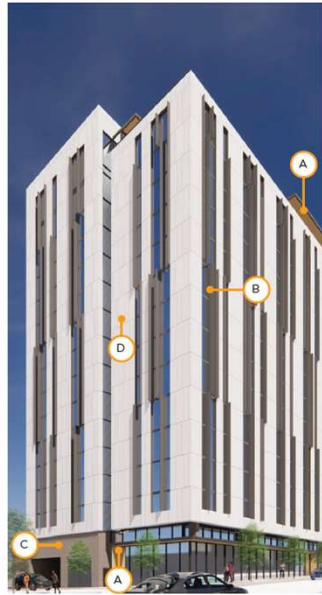
There is no special emphasis at either corner that acknowledges the context of the Green Loop crossing to the south or the turning of the streetcar line on the north, though this may not be as desired considering the general level of cohesiveness of the design. A better response to Guideline B5 may help here as well.

*C7 Design Corners that Build Active Intersections*

## Public Realm

1. Ground Floor
2. Parking Garage
3. Green Loop
4. Weather Protection
- 5. Corners**





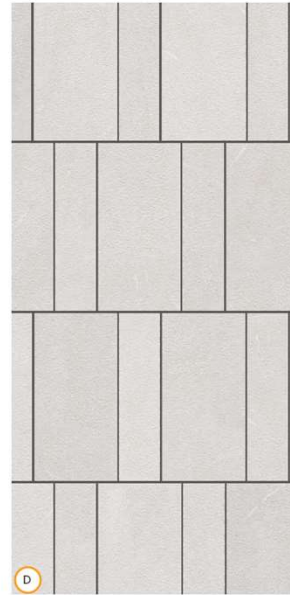
A  
Natural Wood



B  
Dark Bronze Metal Accents + Window Mullions + Glazing



C  
Dark Masonry



D  
Architectural Fiber Cement Panels  
(Basis of Design: Equitone)

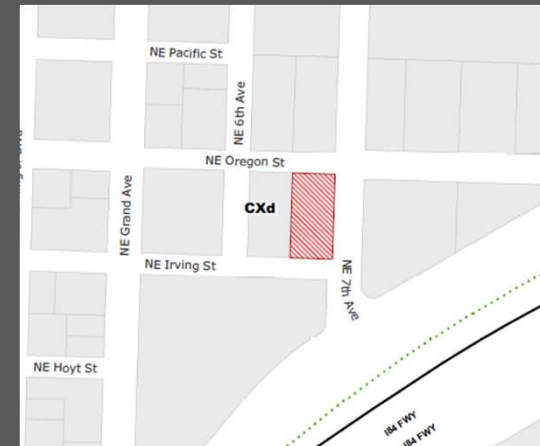
## Equitone / Fiber Cement

Equitone is proposed as the basis of design for the primary fiber cement cladding of the building. While this has been used successfully in at least one other similarly-scaled building in recent years (The Rodney at NW Glisan and 14<sup>th</sup>), it is not a masonry material as required by *Guideline C10-1*. If ultimately found to be acceptable in meeting *C10-1*, detailing of Equitone panels is also an important consideration.

## Quality & Permanence

### 1. Equitone / Fiber Cement

### 2. Coherency





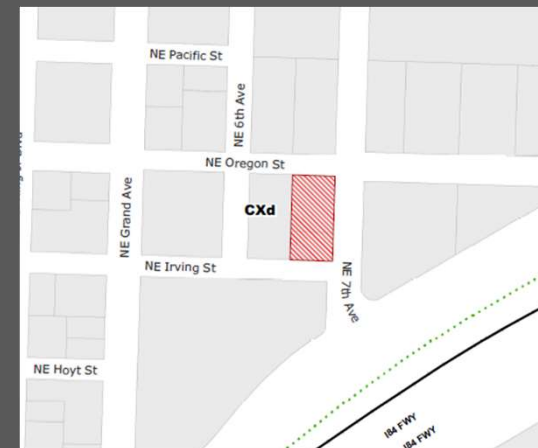


## Equitone / Fiber Cement

The Design Commission required panels affixed from only the rear on the lower levels of The Rodney, as opposed to the usual rivet fasteners to increase the overall quality of the material. Additionally, if other fiber cement materials are proposed to be substituted for Equitone, those often do not have the same density/strength as Equitone, nor do they have the same integral color, and the Commission has found on past projects that such qualities are particularly necessary to meet *Guideline C2 – Promote Quality and Permanence in Development*.

## Quality & Permanence

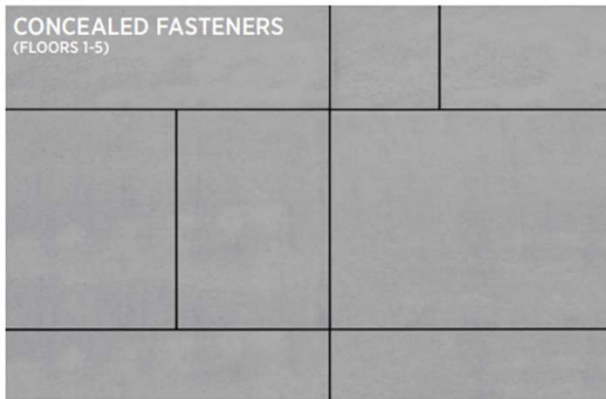
1. Equitone / Fiber Cement
2. Coherency



## EXPOSED FASTENERS (FLOORS 6-15)



## CONCEALED FASTENERS (FLOORS 1-5)



### EDITONE Astro Rivet for EDITONE [tectiva]

The stainless steel (quality A2, AISI 304) EDITONE blind rivet has a concealed head to match the panel and built-in spacer (spigots). The EDITONE stainless steel cylinder maintains a consistent gap between the panel and the metal frame and allows total free movement of the panel. An uncapped rivet is also available. Failure to use this rivet invalidates the product's warranty.

#### Rivet sleeve

Rivet sleeves are used with the rivets to form the fixed points when fixing the panels. The sleeve slides over the rivet and fits the hole in the panel.



### Tergo Secret Fix

Tergo is a system for secret fixing EDITONE panels to aluminum supporting frames. The panels have factory drilled undercut fastener holes in the back of the panel. Hanging brackets are attached to the panel with other special rivets and spacers or undercut bolts and washers.

The spacers of the aluminum supporting frame will provide the necessary static calculations required to position these undercut holes. They also control the height and position of the hanging brackets.

#### Suppliers

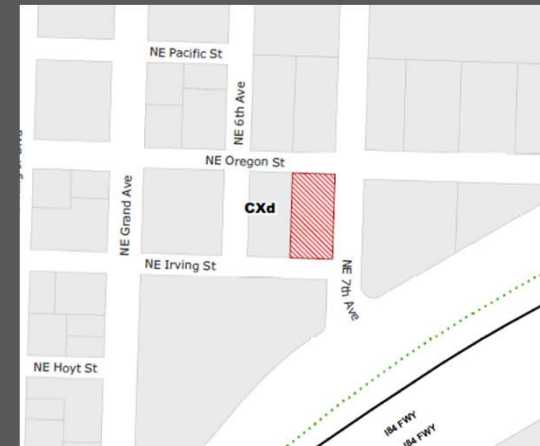
The rivet suppliers have been developed by Fischer. The undercut bolt system was developed by Rail.



# Quality & Permanence

## 1. Equitone / Fiber Cement

## 2. Coherency



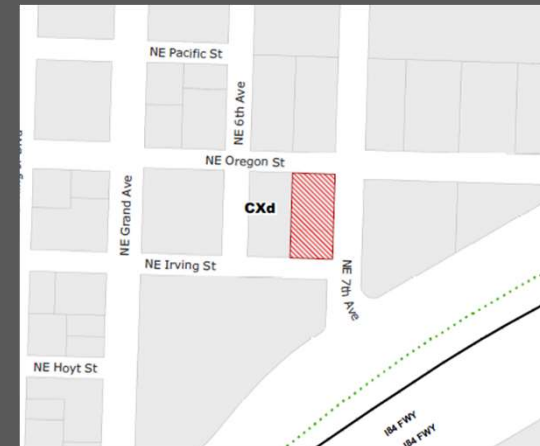


## Overall Design

The overall design of the building appears to follow a clear concept, having a distinct base, middle, and top expression with a regularized placement of windows and articulating fins across the upper stories of the building.

## Quality & Permanence

1. Equitone / Fiber Cement
2. Coherency





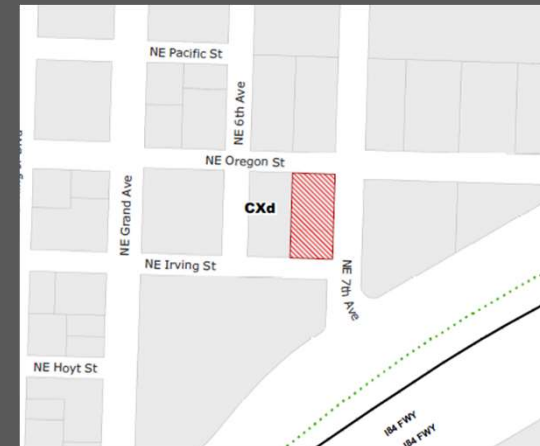


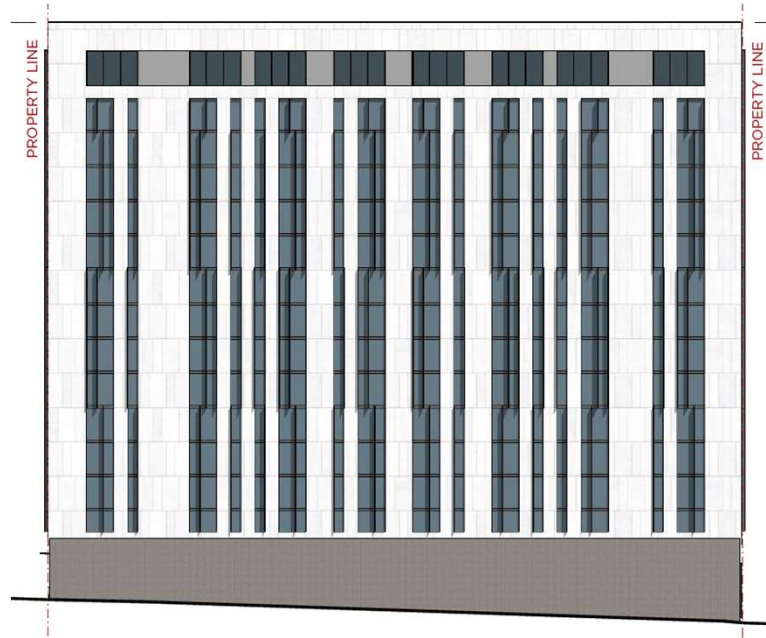
## Overall Design

The ground floor, however, includes a large blank wall area on the ground floor of the south elevation (and to a lesser extent on the north elevation as well) that does not integrate as well with the overall design of the building. The wrap-around ribbon/canopy at the southeast corner of the building also seems foreign to the design.

## Quality & Permanence

1. Equitone / Fiber Cement
2. Coherency



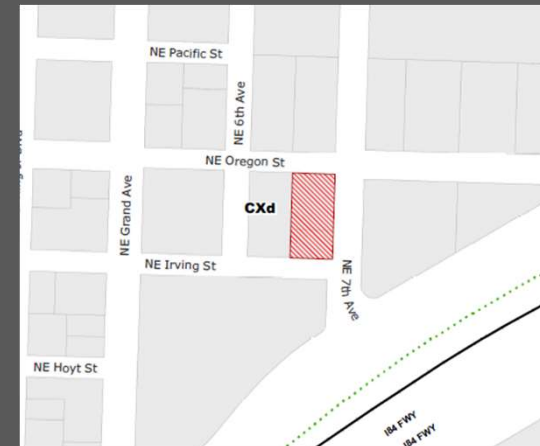


## Overall Design

The top floor of the west elevation breaks from the pattern used at the top floor on the north and south elevations due to the proposed roof decks for the penthouse units. The resolution of the design of the top of the tower on the west elevation would benefit from being more consistent with the north and south elevations rather than introducing a third type of design response for the top floor of the building.

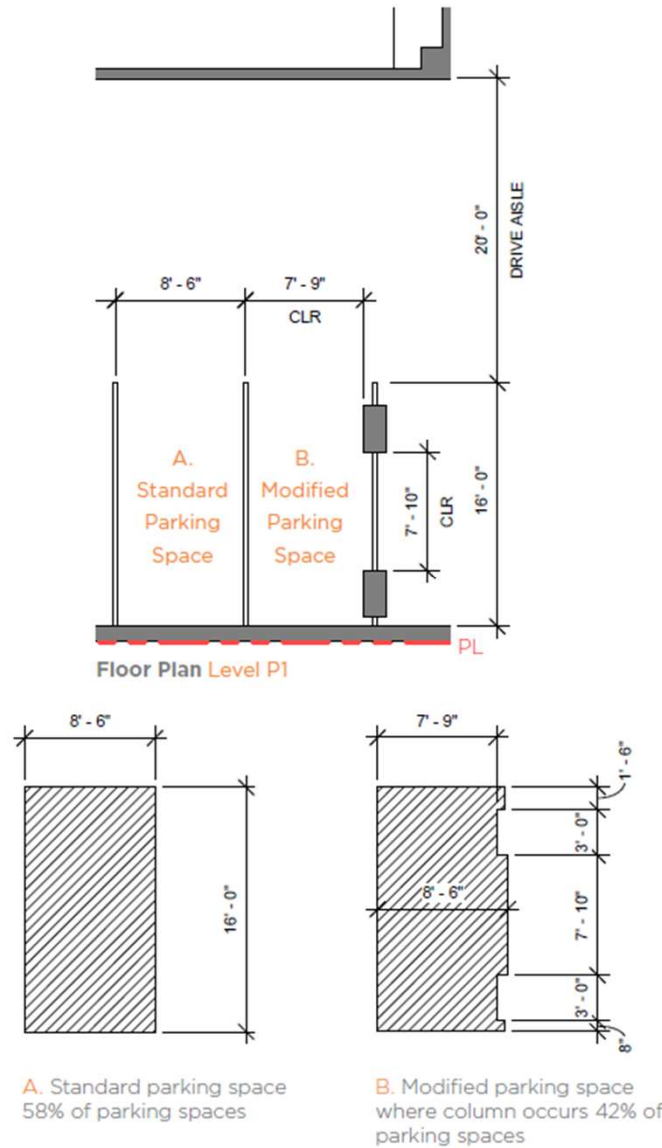
## Quality & Permanence

1. Equitone / Fiber Cement
2. Coherency



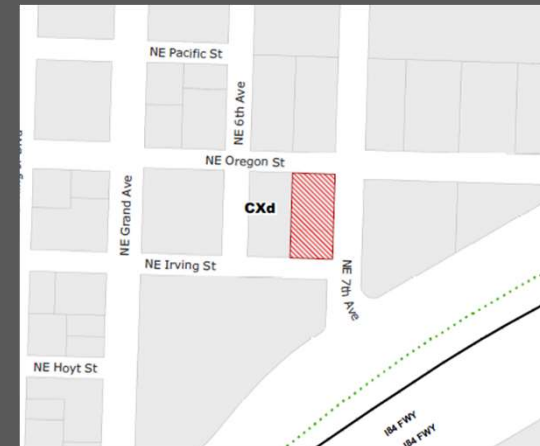
# Parking space size

PZC 33.266.130.F.2 requires perpendicular parking spaces to be at least 8'-6" wide with a 20'-0" drive aisle.



## Modification

### 1. Parking space size





# Questions