



Portland Planning & Sustainability Commission

Department of Community Services
Transportation Division
July 13, 2021

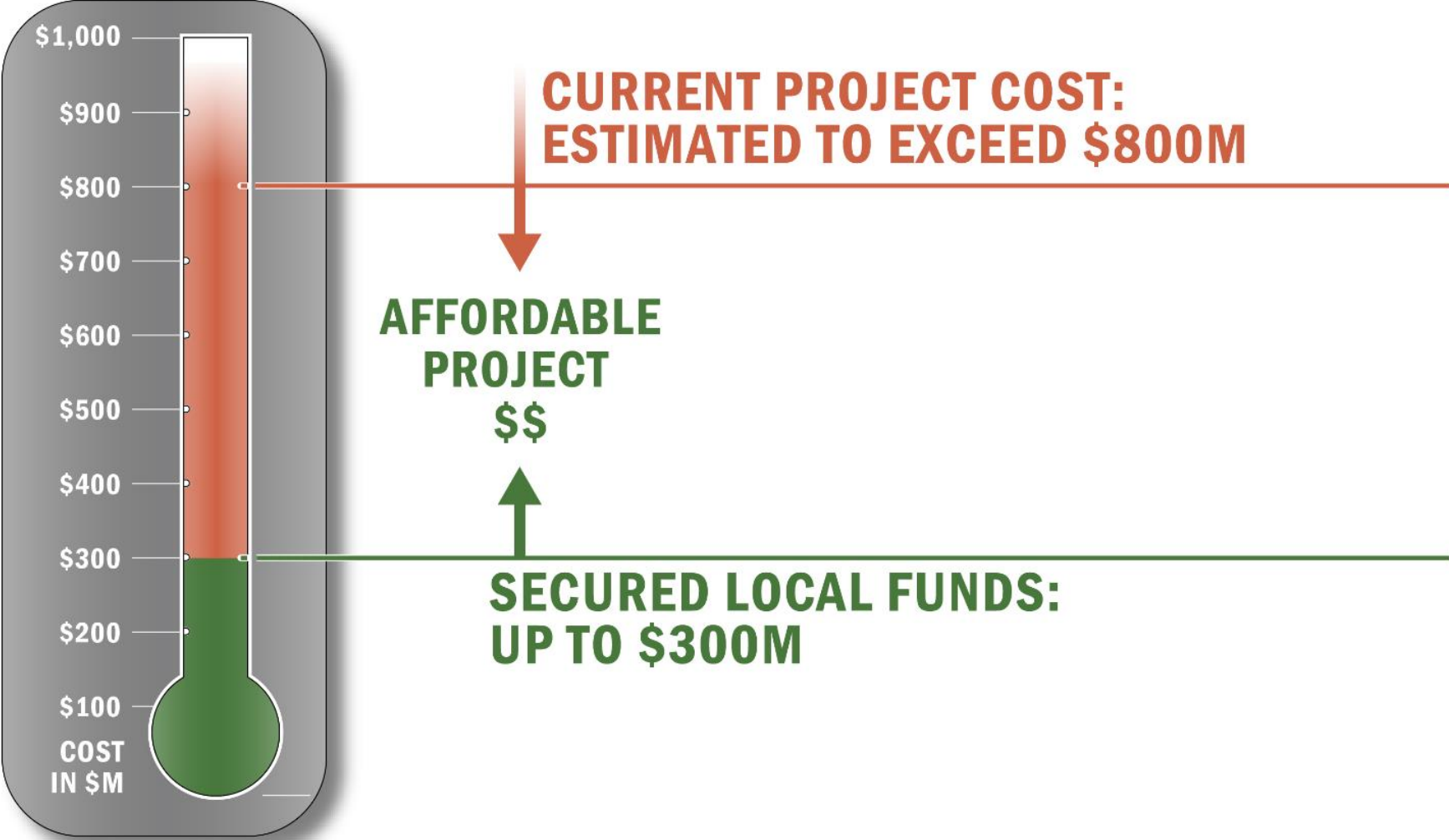


Project Update



Funding Context

Must achieve an affordable Project to be viable



Note: City of Portland and other local cities agreed to forego VRF revenue to provide financial support of the project.

Project Update

Funding Opportunities and Approaches

Funding Opportunities

- Federal Transportation & Infrastructure Package
- Federal RAISE Grant
- Potential Future Regional Transportation Bond Measure
- Multnomah County Vehicle Registration Fee (secured)



Approaches

- Cost reductions
- Establishing a cost cap





Cost Saving Measures Under Analysis



Guiding Principles

- Moving forward with recommended Long Span Replacement Alternative
- Ensure the Purpose and Need is met
 - Seismic resiliency
 - Emergency response and regional recovery
 - Long term transportation needs
- Maintain County's equity lens
- Fiscal responsibility



Cost Saving Measures

Range of Cost Saving Options being Considered

1. Bridge-specific Changes
 - 1a. Bridge Types
 - 1b. Bridge Width
 - 1c. Approach Span Lengths
2. Property Impacts / ROW Acquisition
3. Connections to Skidmore MAX, Eastbank Esplanade
4. Aesthetic Enhancements
5. Delivery Method



Cost Saving Measures NOT Pursued

Things we considered but chose NOT to pursue

The Project will not:

- Reduce seismic design criteria
- Eliminate potential for future Streetcar
- Reduce to three vehicular lanes
- Eliminate capacity for oversized and specialized heavy haul vehicles
- Reduce bike/ped width to less than 14-feet
- Remove the crash worthy barrier between vehicular lanes and bike/ped space



West Approach Bridge Type

Girder Type with Two Supports in Waterfront Park

Existing condition



West Approach Bridge Type

Girder Type with Two Supports in Waterfront Park

**\$5 - \$10M
Savings**



(UNDER ANALYSIS)

Design Advice Provided to Project Team:



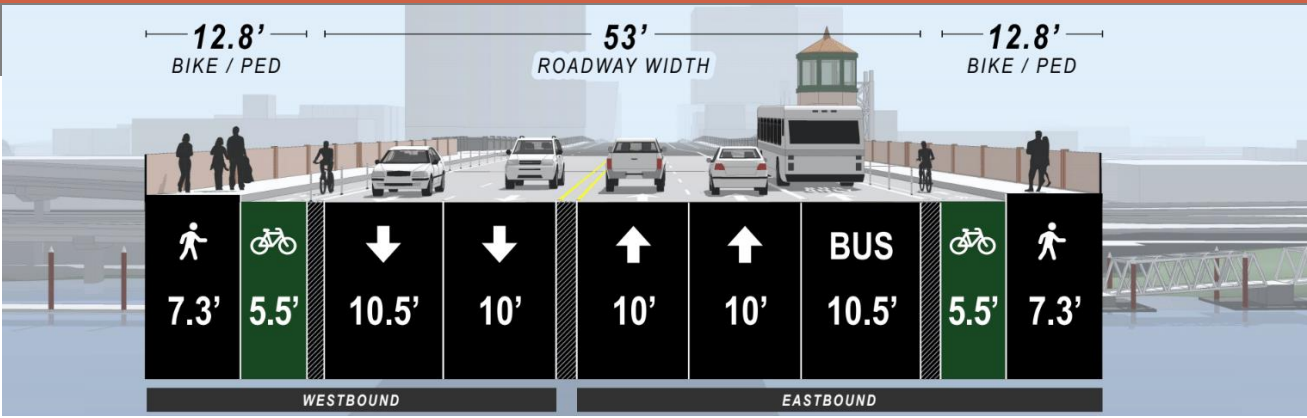
- Due to visual impacts to historic districts, Girder-styled west approach option best meets zoning code and historic guidelines
- Bascule movable bridge option minimizes impacts to views
- Cable Supported option offers similar scale and visual cohesion to east side building heights
- Cable Supported option offers more transparency
- Preference for “observable asymmetry” due to distinct differences in urban fabric on west and east sides



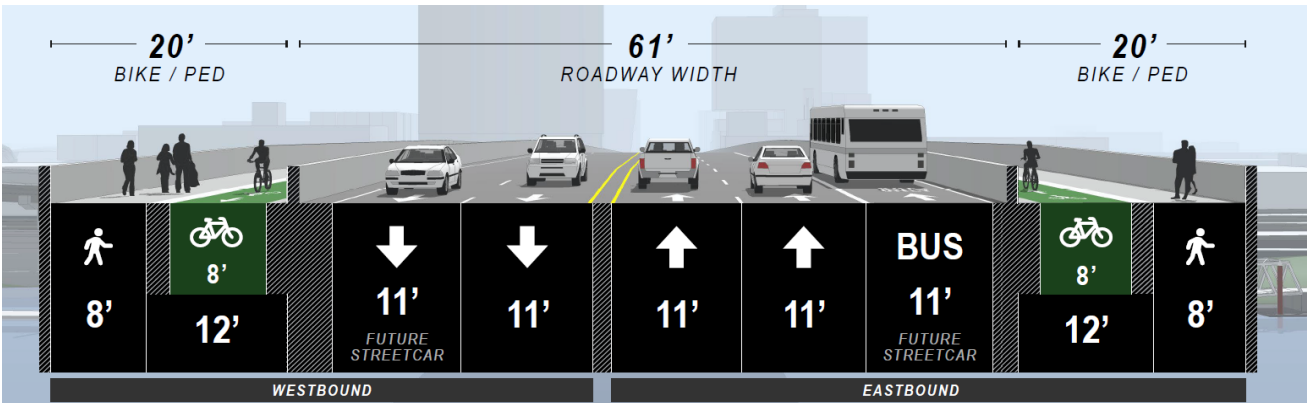
Bridge Cross Section

Narrower Bridge

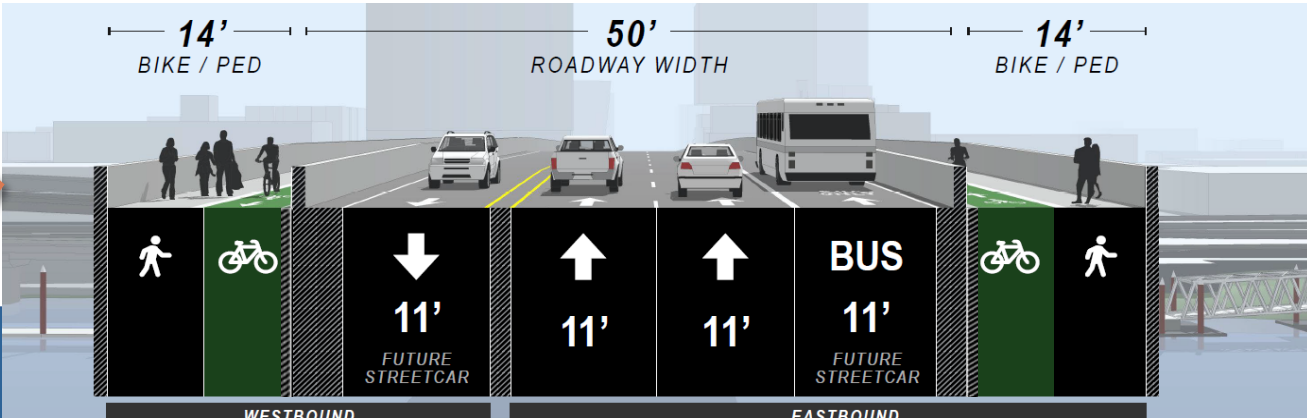
Existing Cross Section



DEIS Cross Section



Refined Cross Section Under Analysis

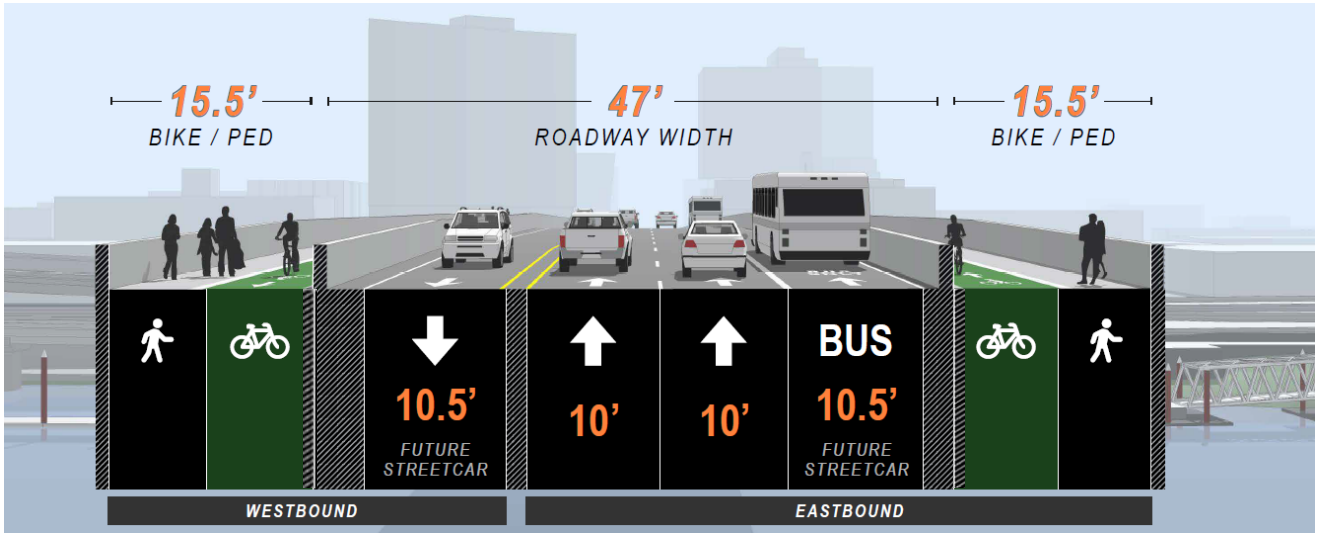
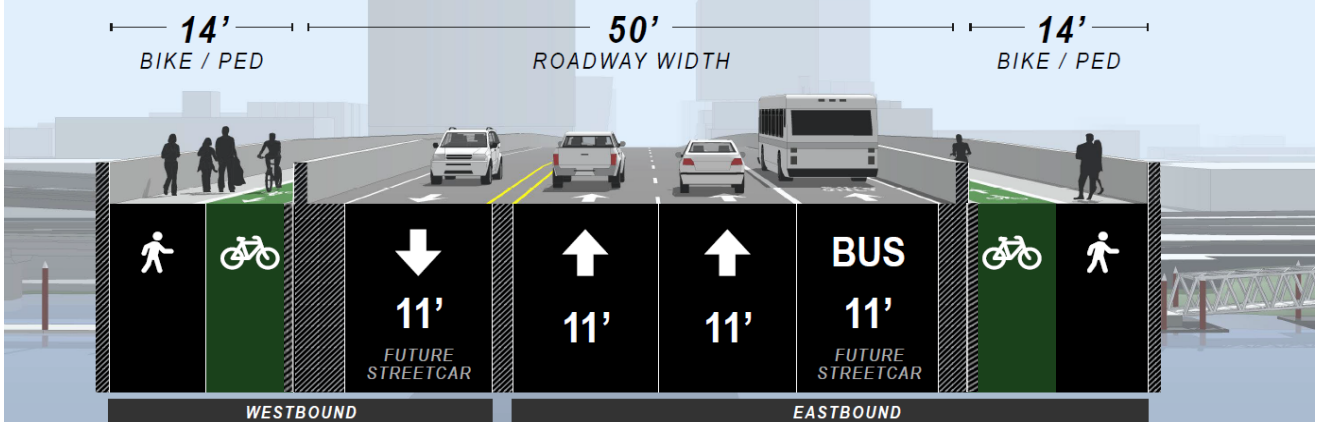


\$140 - \$165M Savings

Bridge Cross Section

Narrower Bridge: Space Allocation Options

Project team will study various ways space could be allocated



* Note: Buffer between bike / pedestrian spaces not shown



(UNDER ANALYSIS)

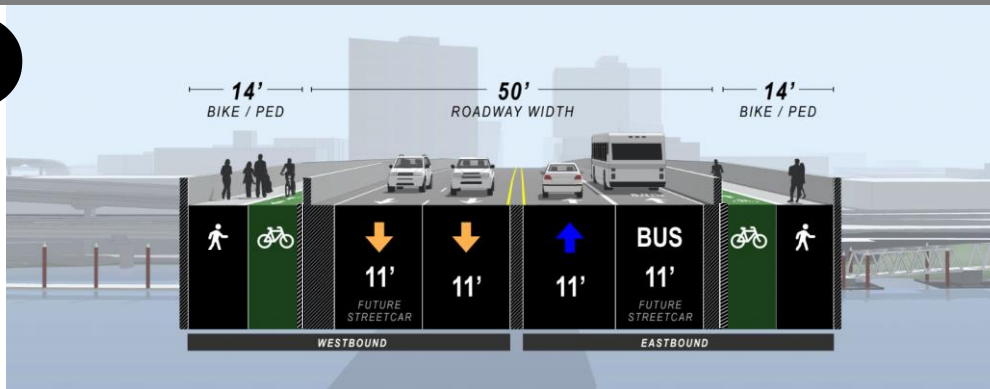
Traffic Lane Configurations

Three Study Options

Option 1 (Balanced):

2 WB General Purpose + 2 EB (1 General Purpose and 1 Bus lane)

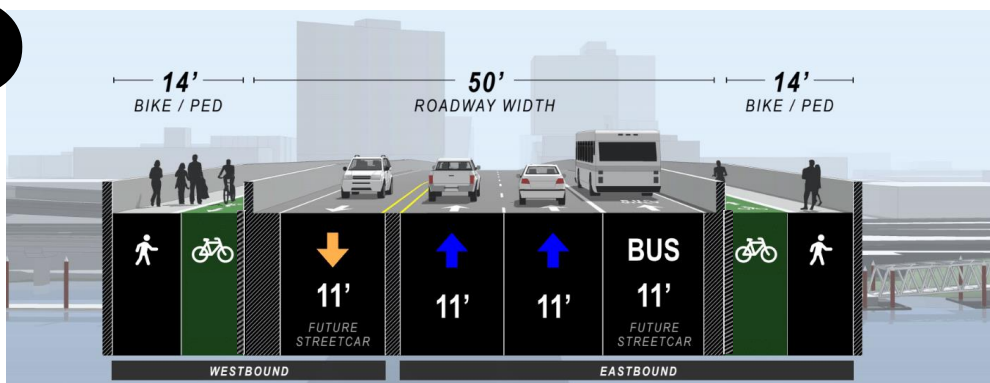
1



Option 2 (EB Focus):

1 WB General Purpose + 3 EB (2 General Purpose and 1 EB Bus lane)

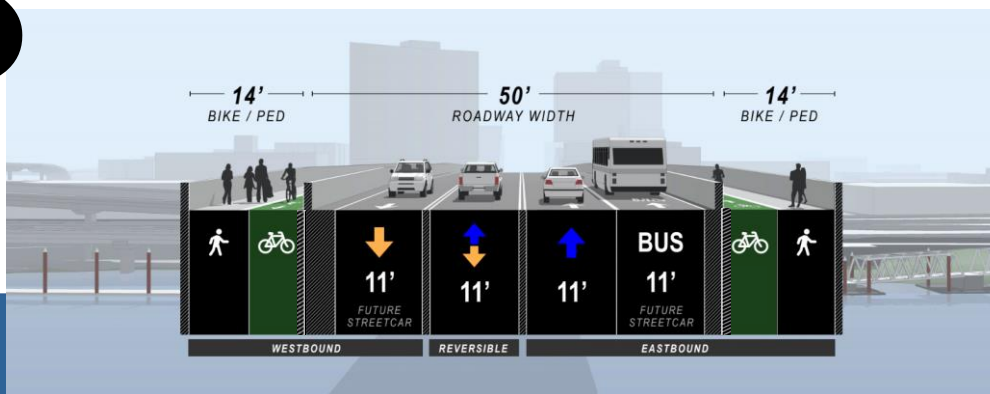
2



Option 3 (Reversible Lane):

1 WB + 1 Reversible Lane + 2 EB (1 GP and 1 Bus lane)

3

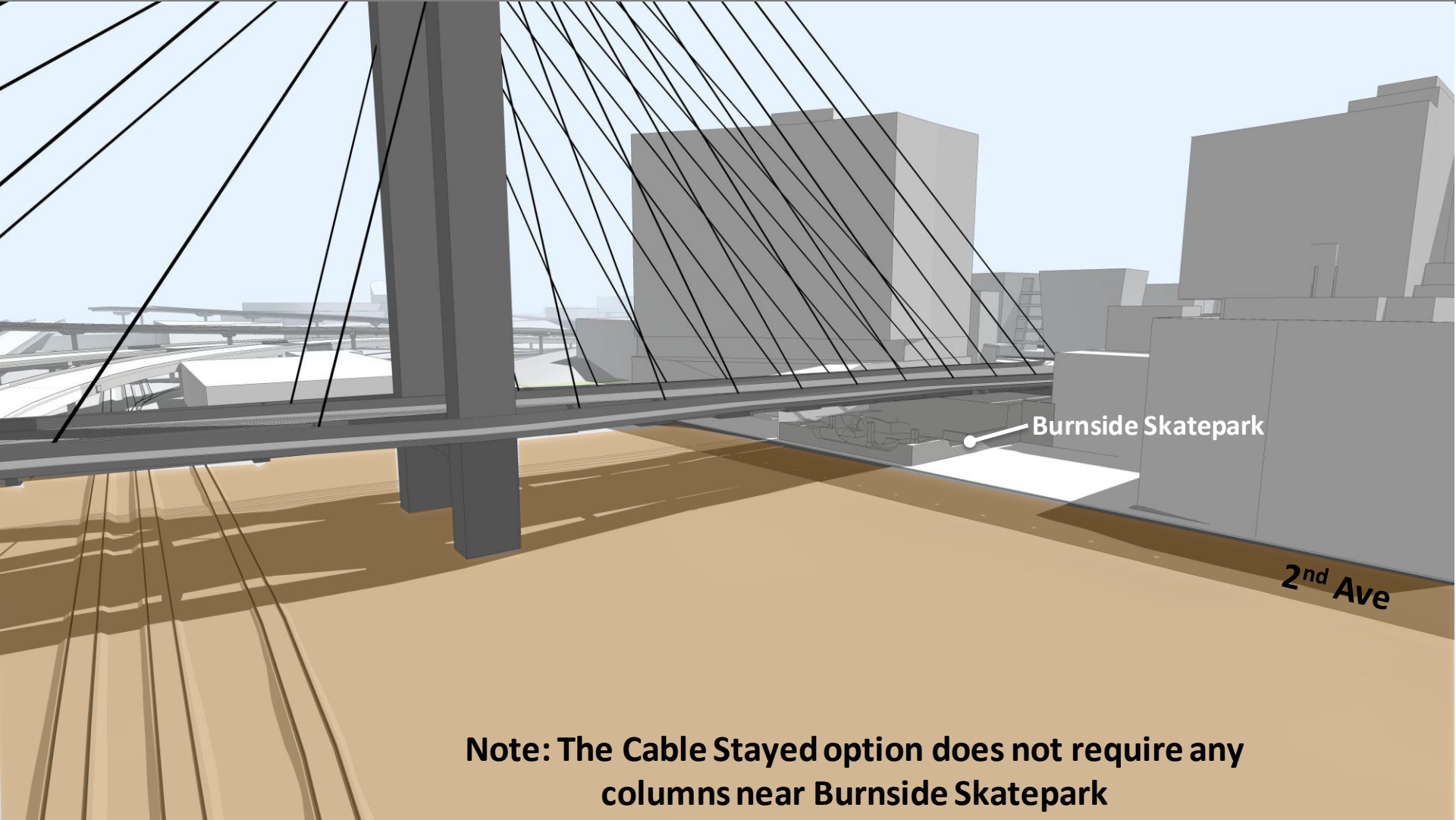


(UNDER ANALYSIS)

East Approach: Near Skatepark



Cable Stayed DEIS Alternative



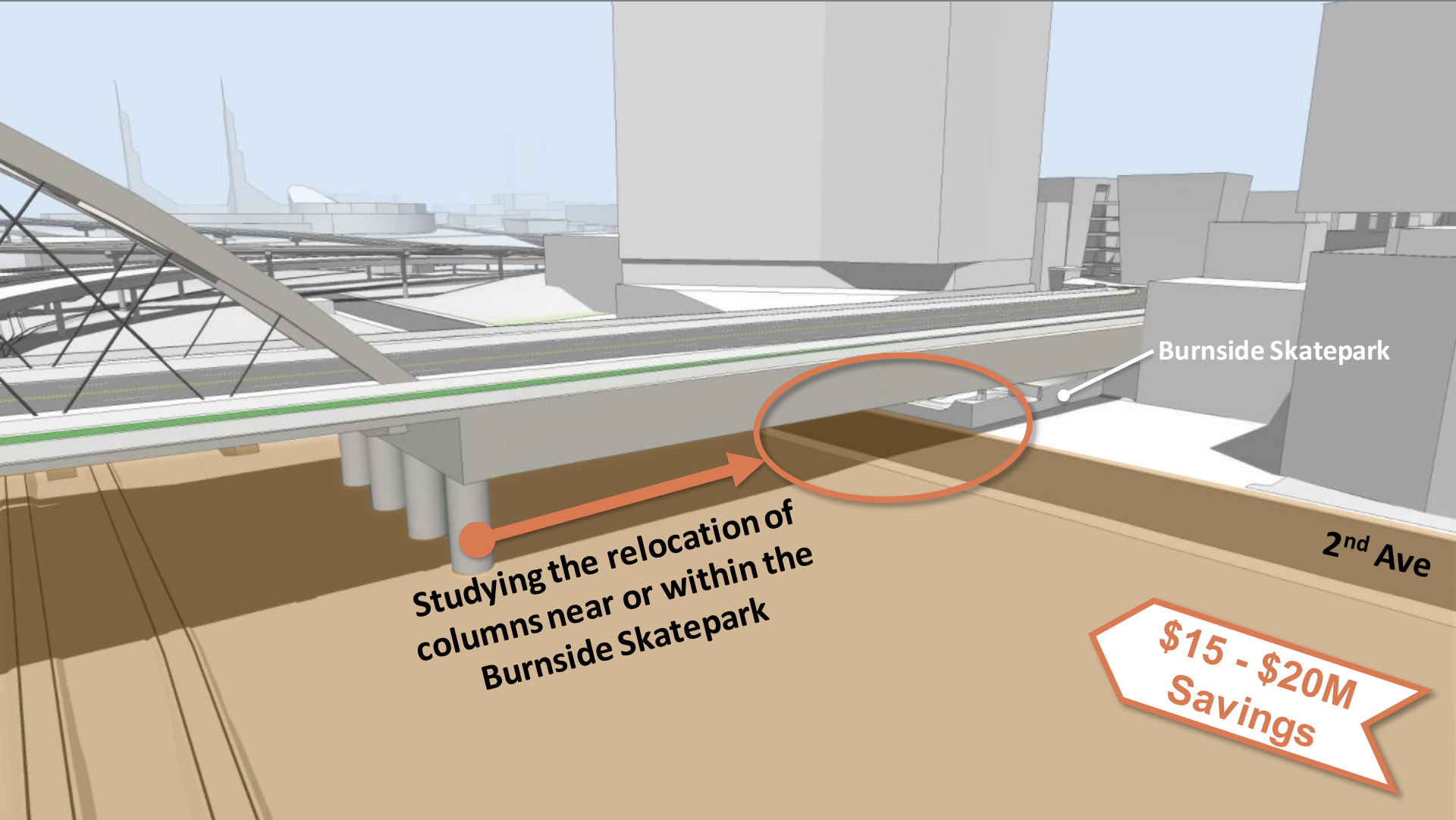
Note: The Cable Stayed option does not require any columns near Burnside Skatepark



East Approach: Near Skatepark



Tied Arch DEIS Alternative



Studying the relocation of columns near or within the Burnside Skatepark

\$15 - \$20M Savings

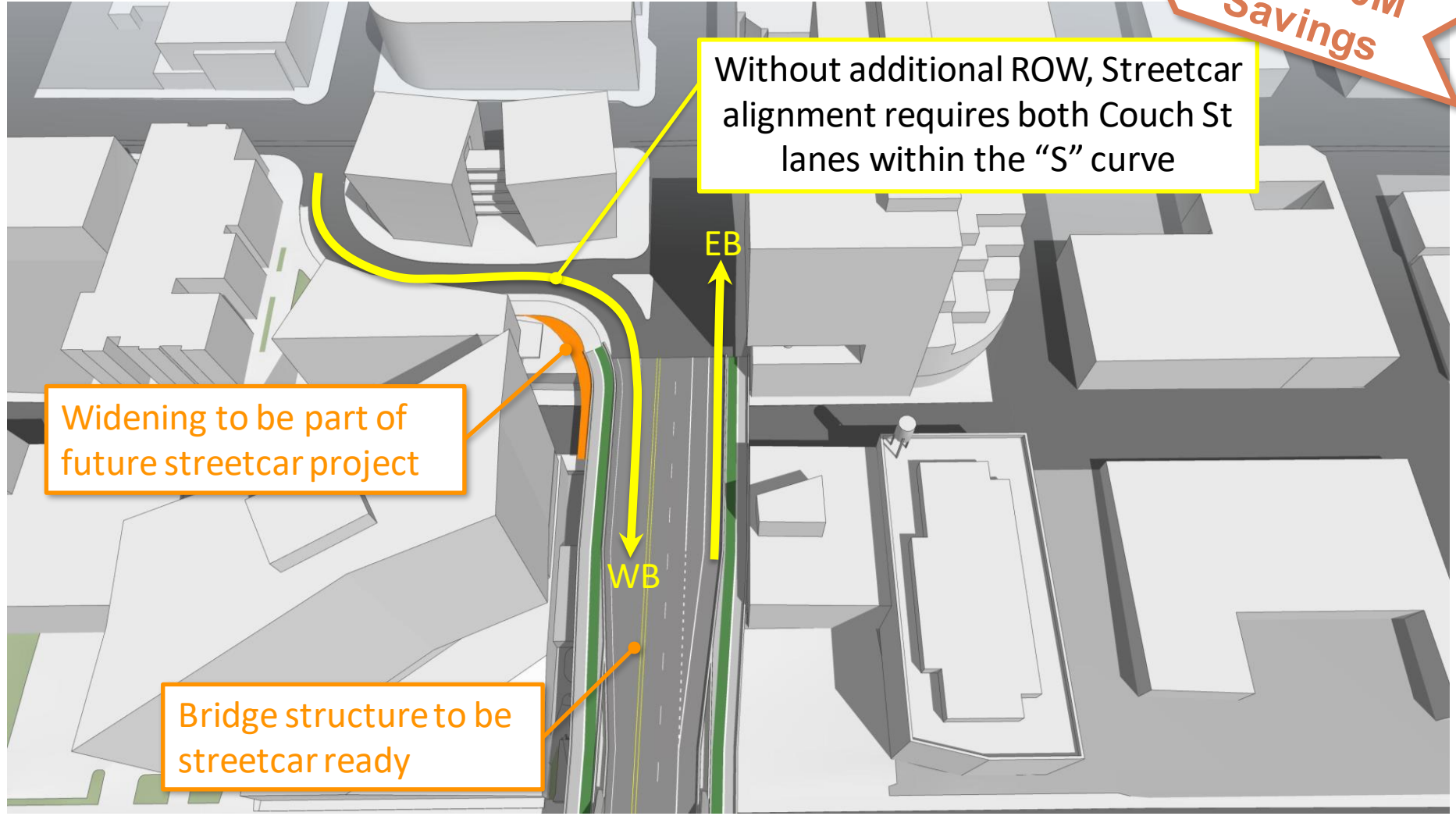


Does not apply to Cable Stayed bridge type

Property Impact / Right of Way

No Permanent ROW Impacts for Streetcar

\$5 - \$10M Savings



(UNDER ANALYSIS)

Connections to MAX / Esplanade

Initial Assumptions



(UNDER ANALYSIS)

Connections to MAX / Esplanade

County to fund connections to facilities below

**North & South Stairs to
Skidmore Max Station**



Owner: Multnomah County

**South Stairs to
Eastbank Esplanade**



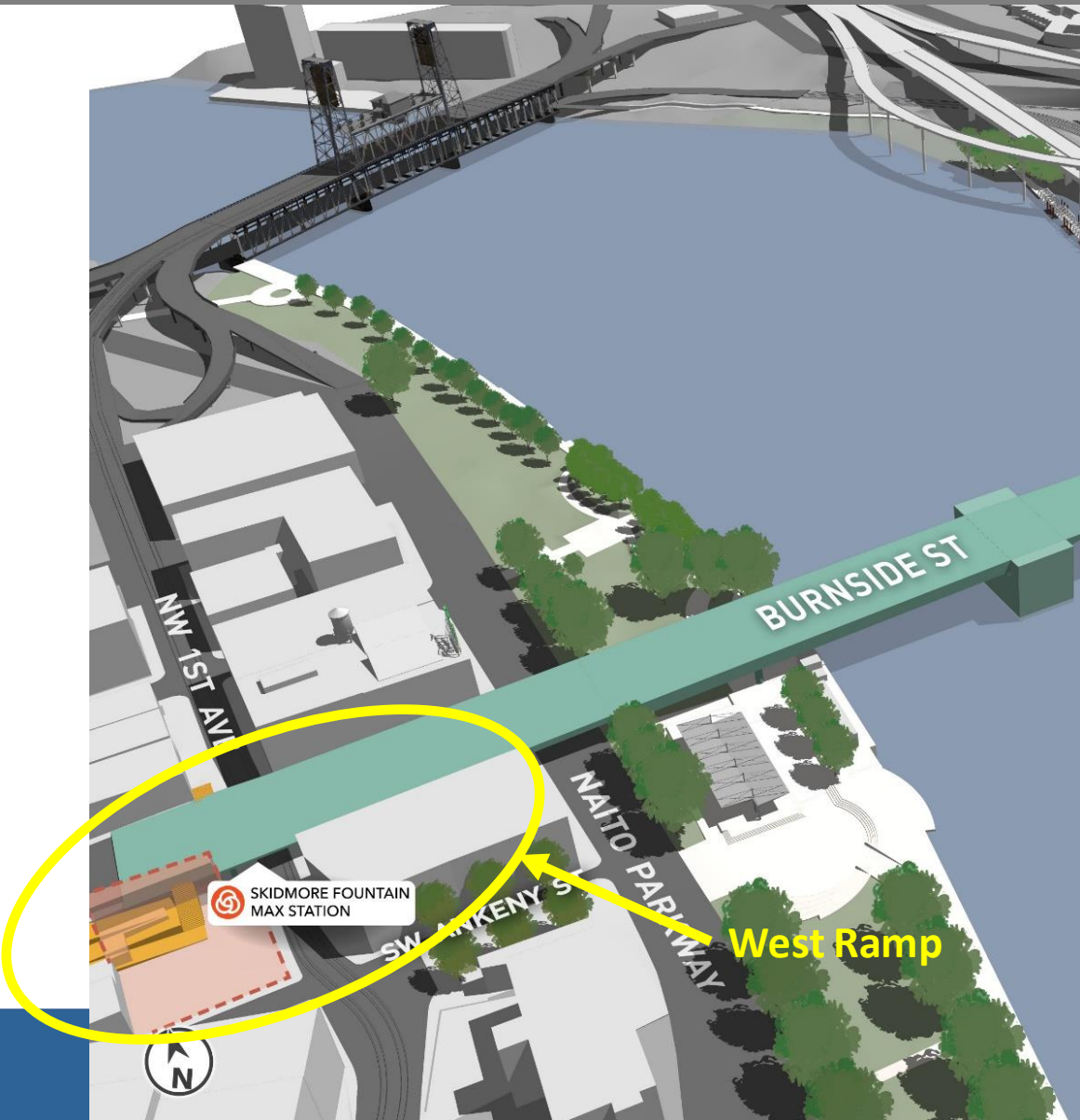
Owner: City of Portland



(UNDER ANALYSIS)

Connection to MAX Station

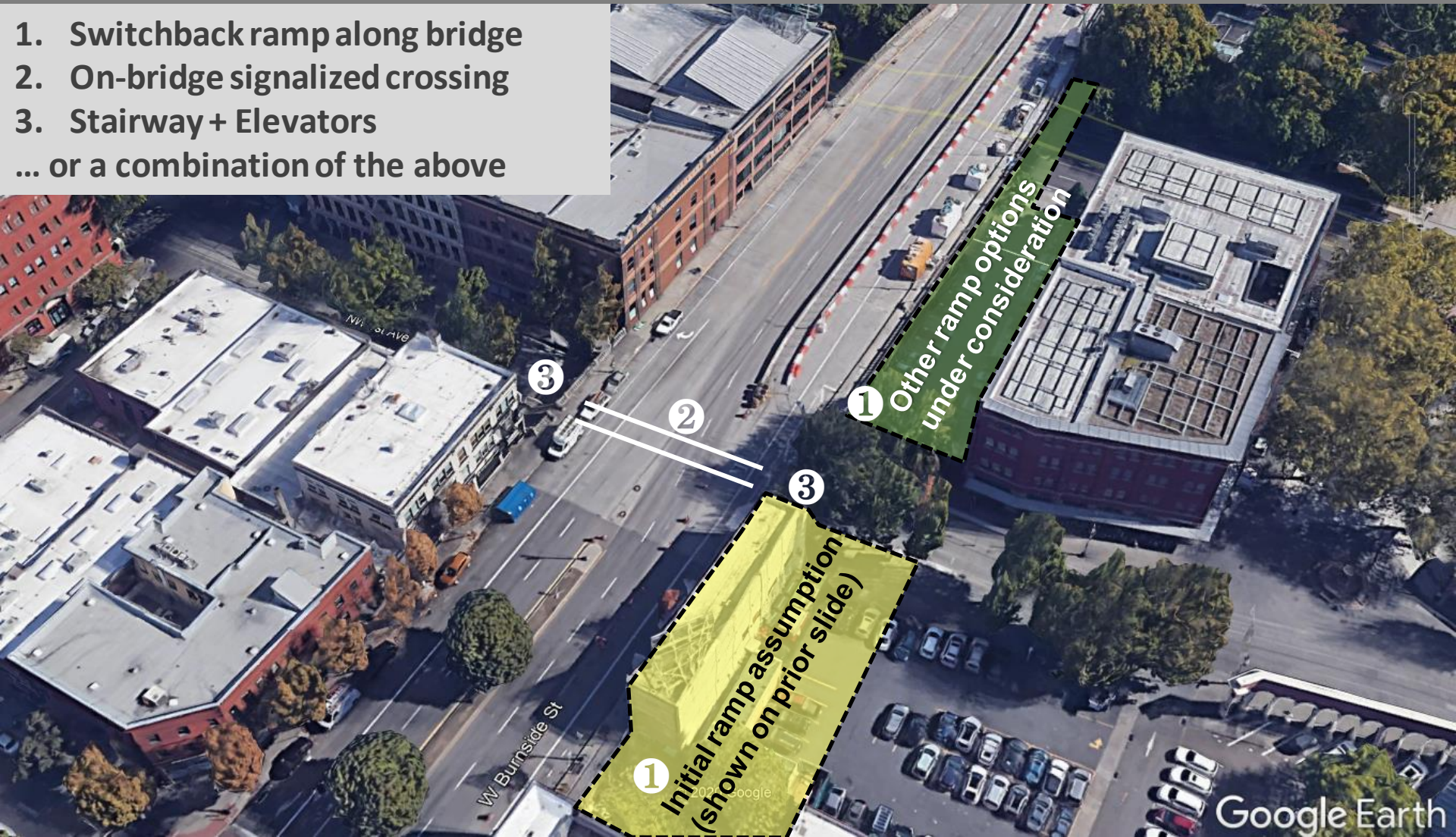
Location Overview



Connection to MAX Station

Initial options under consideration

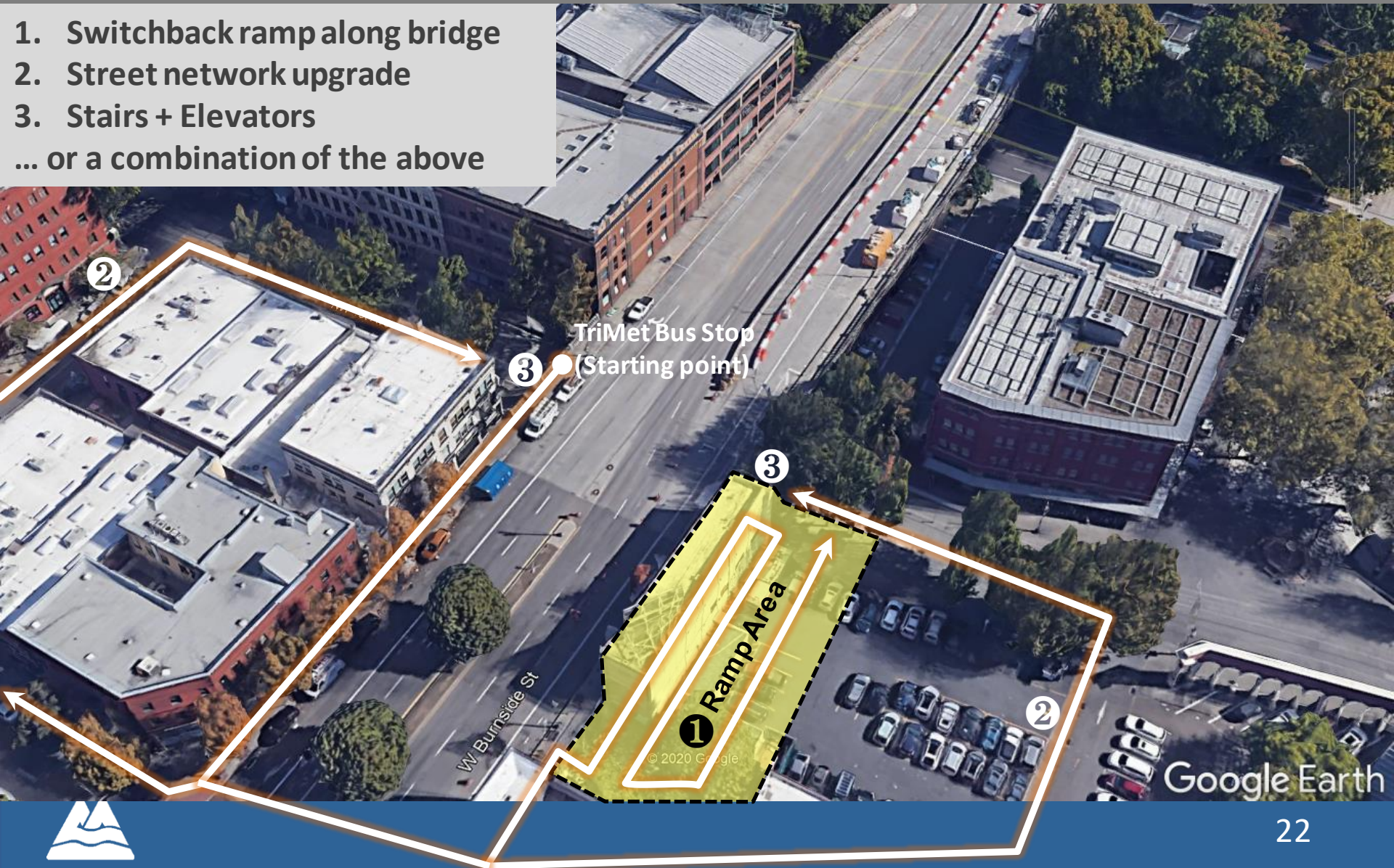
- 1. Switchback ramp along bridge
- 2. On-bridge signalized crossing
- 3. Stairway + Elevators
- ... or a combination of the above



Connection to MAX Station

Refined options under consideration

- 1. Switchback ramp along bridge
- 2. Street network upgrade
- 3. Stairs + Elevators
- ... or a combination of the above



Connection to MAX Station

North Street Network Routing: Bus Stop to Westbound & Eastbound MAX



Connection to MAX Station

South Street Network Routing: Bus Stop to Westbound & Eastbound MAX



Connection to MAX Station

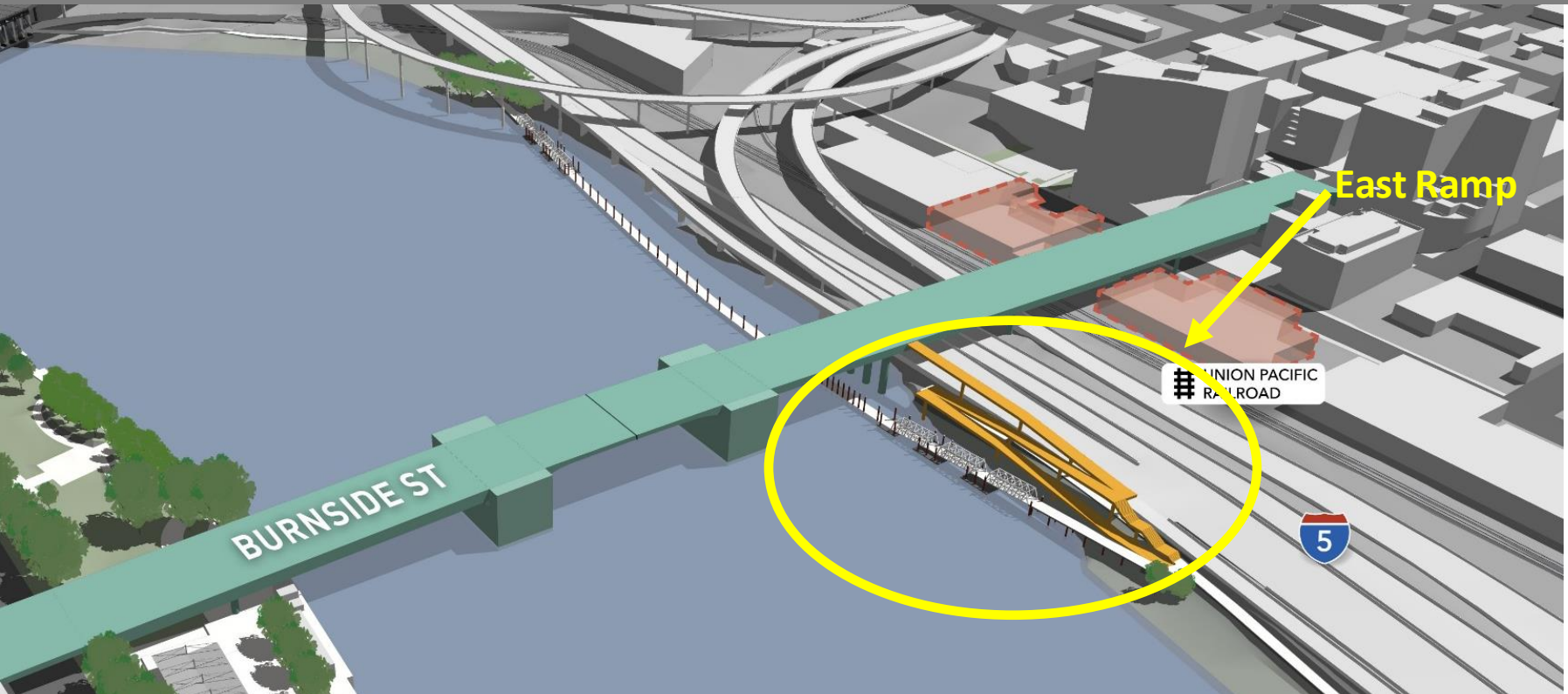
Ramp Network Routing: Bus Stop to Westbound & Eastbound MAX



Connection to Eastbank Esplanade



Location Overview



Connection to Eastbank Esplanade

Initial options under consideration

1. Ramp from bridge
 2. On-bridge signalized crossing or under bridge crossing
 3. Stairway + Elevator
- ... or a combinations of the above



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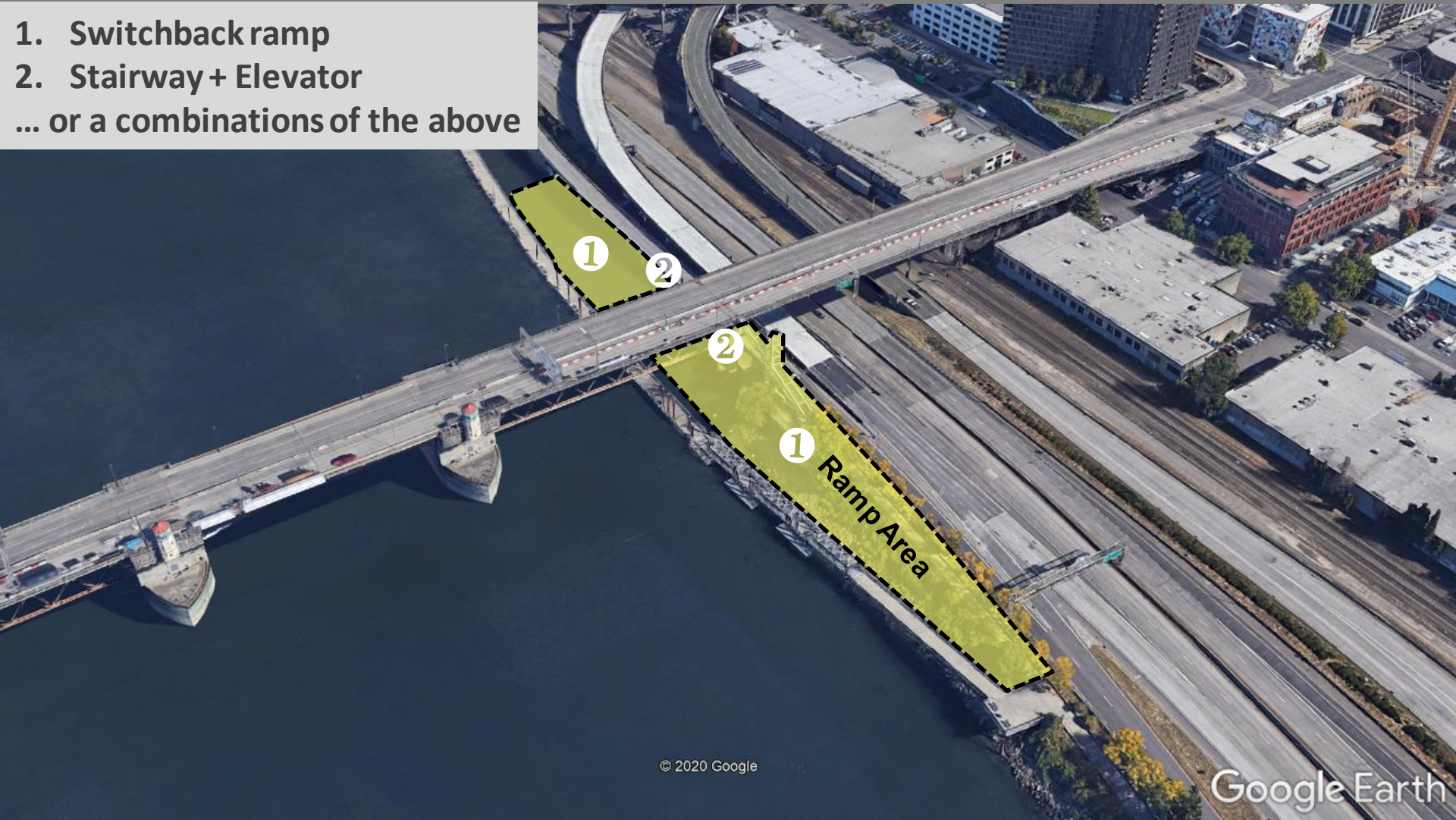
Google Earth



Bridge Connection – East Side

Options under consideration

- 1. Switchback ramp
- 2. Stairway + Elevator
- ... or a combinations of the above



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Google Earth



Cost Saving Measures

Range of Cost Saving Options being Considered

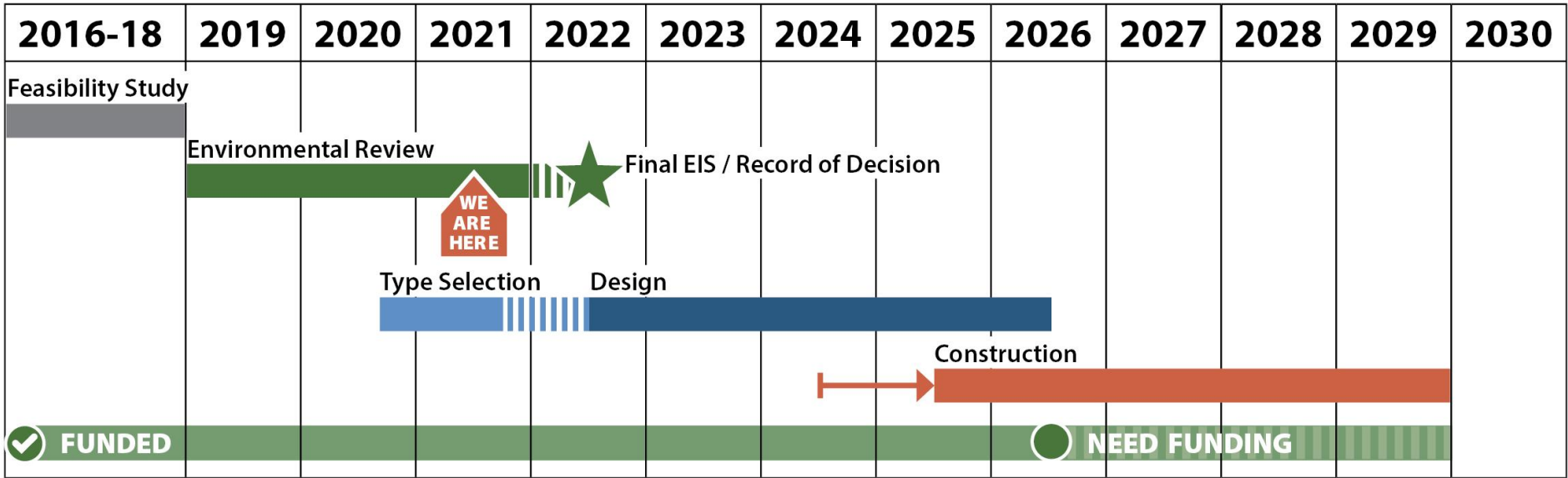
Topic Buckets	Cost Savings Item	Preliminary Cost Savings Range
1a. Bridge Specific	Girder vs Long Span (on West Approach)	\$5M to \$10M
	Cable Stayed vs Tied Arch	(Pending Type Sel.)
	Lift vs Bascule	(Pending Type Sel.)
1b. Bridge Width	Roadway reduced from 5 to 4 vehicle lanes	\$85M to \$100M
	Sidewalks / Bike lanes reduced from 20' to 14'	\$55M to \$65M
1c. Tied Arch Approach Span Lengths	Additional columns (i.e., Burnside Skatepark)	\$15M to \$20M
2. Property Impacts / ROW Acquisition	No ROW Acquisition on Couch Couplet for Streetcar	\$5M to \$10M
3. Connections to MAX / Esplanade	County to fund connections to facilities below	TBD
4. Aesthetic Enhancements	Limit Aesthetics / Lighting / Urban Design/ Landscaping	\$5M to \$10M
5. Delivery Method	“Best Value” Bid vs CM/GC Delivery	TBD
Cost Savings Range:		\$175M - \$220M





Workplan Update

Project Timeline



Key Schedule Changes

- Final EIS moved out six months to accommodate additional analysis
- Construction start moved to reflect when we think we'll have funding by



- **Spring / Summer 2021 – Technical Analysis**
- **Fall 2021 – CTF Meetings**
 - Review analysis findings, county cost cap decision and cost saving recommendations
 - Seek CTF concurrence on recommendations
- **Fall / Winter 2021 – Continued Outreach to Community Stakeholders**
- **February 2022 – Community Outreach with Publication of Supplemental Draft EIS**
- **Spring 2022 – Finalize Type Selection Recommendation**
- **Summer 2022 – Final EIS and Record of Decision**





Open Discussion



Thank you!

