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# MINNESOTA PLACES

DESIGN ADVICE REQUEST 20-184491-EA OCTOBER 15TH, 2020

PDX SIGNATURE PROPERTIES NATIVE LAND DEVELOPMENT WDY CIVIL ENGINEERS WRIGHT ARCHITECTURE

wright  
architecture

# PROJECT NARRATIVE

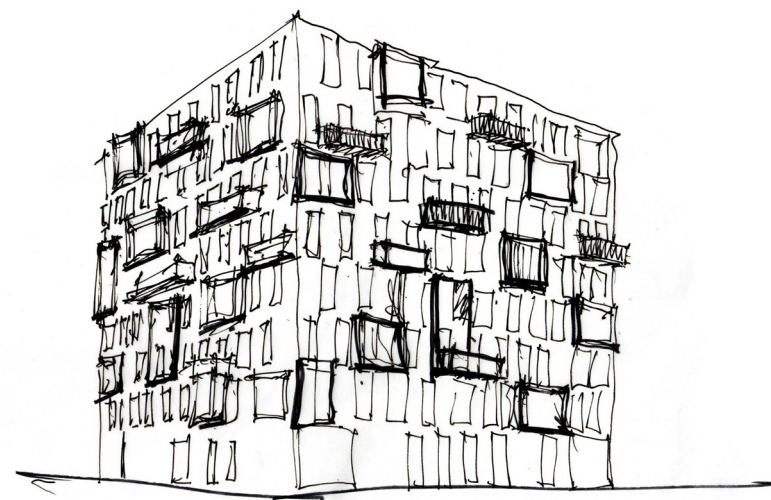


Proposed redevelopment of a 1/4 block, 10,000 SF site adjacent to the I-5 Freeway in the North Interstate Plan District. The proposal includes approximately 72 residential units in a new 54,000 SF, 8-story building. 100% of the units will be offered at 60% Average Median Income (AMI), and are proposed as a mix of three-, two- and one-bedroom apartments. The site currently contains a single-family house built in 1942 and a duplex from 1978. All stormwater to be disposed of on site through the use of on-site swales and drywells.

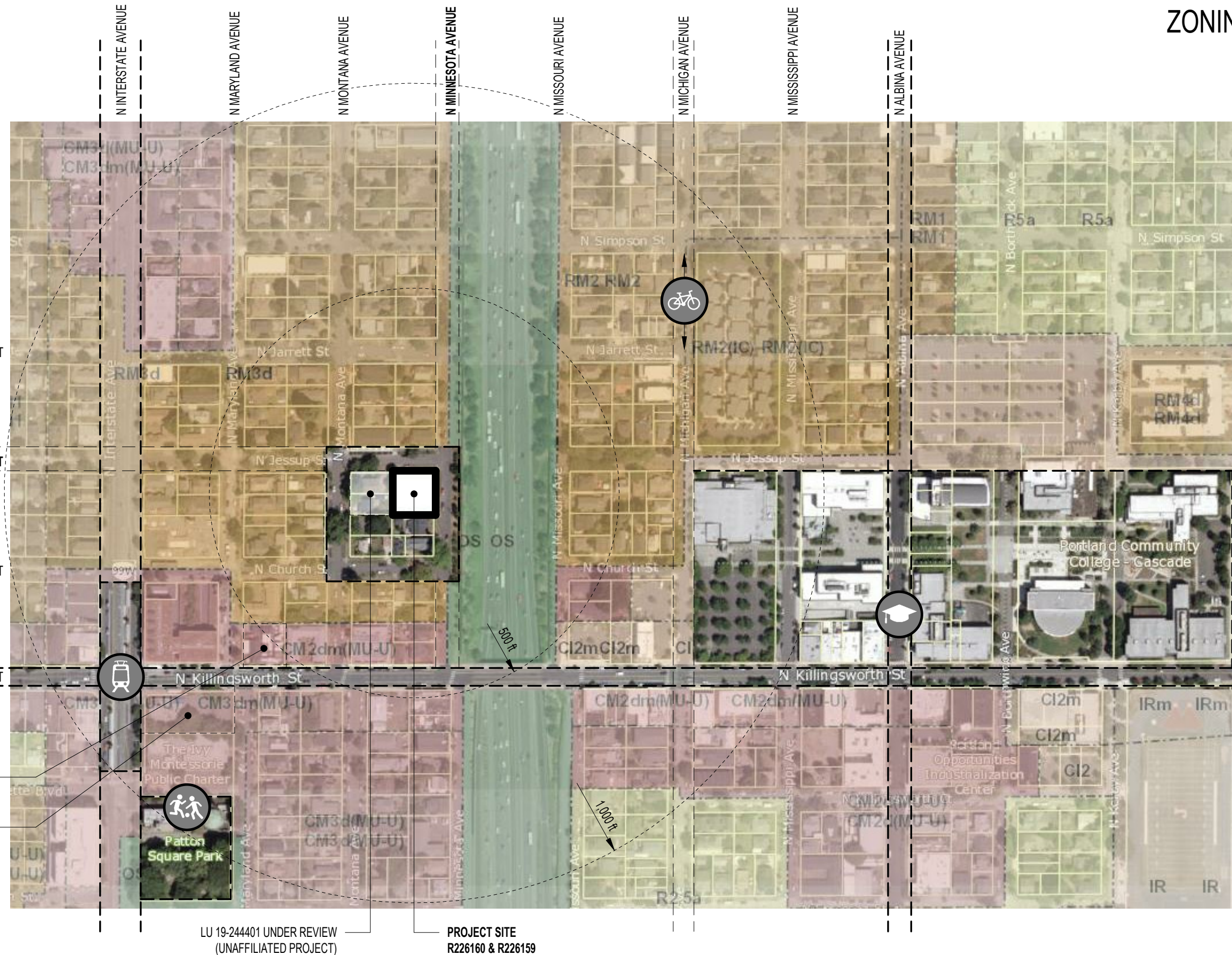
The project team is dedicated to the creation of new, permanent affordable housing units that are close to existing high-capacity transit and other urban amenities. The project is receiving OHCS LIFT Rental NOFA funding (2020 LIFT Rental NOFA Winner), 4% Low-Income Tax Credits, Metro Transit Oriented Development Grant, and Oregon Multifamily Energy Program funding.

Per the Community Design Guidelines, this site offers a unique opportunity for large-scale development along the freeway edge, and is within close proximity to the Killingsworth light rail station and Portland Community College. The new building will have two street frontages each having distinctive character; the North being a typical residential street with a mix of building types, while the East faces a sound wall and the I-5 trench. The ground floor is oriented to match the two street conditions with the main entrance, common areas and office facing N Jessup, whereas utilities, the loading dock and bicycle parking will be located along N Minnesota.

The building is being designed as a Type IV C mass timber structure utilizing mass plywood panels (MPP). The project will also be built using modular construction, minimizing neighborhood impacts by decreasing on-site disruption.



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**C2.1 ZONING SUMMARY**

BASE ZONE	RM3
OVERLAY	d (Design)
COMP PLAN	MD-U
DISTRICT	North Interstate
URBAN RENEWAL	Interstate
HISTORIC	None
MAXIMUM F.A.R.	4:1 (5:1 w/ bonus)
BONUS F.A.R.	1:1 (maximum)
MAXIMUM HEIGHT	125 ft (85 ft)
STREET SETBACKS	0 ft
SIDE & REAR	10 ft
LANDSCAPING	1,500 SF
LOADING	1 TYPE B
OUTDOOR AREA	2,592 SF
AUTO PARKING	0
BICYCLE PARKING	108 L.T. / 6 S.T.
	[ Long Term: 76 vert. / 32 horiz. (6 large) ]

EA 20-129522 UNDER REVIEW  
(UNAFFILIATED PROJECT)

CO 20-146519 UNDER REVIEW  
(UNAFFILIATED PROJECT)

LU 19-244401 UNDER REVIEW  
(UNAFFILIATED PROJECT)

**PROJECT SITE**  
R226160 & R226159

**BUILDING HEIGHT & MASS**

33.561.210 Maximum Building Height

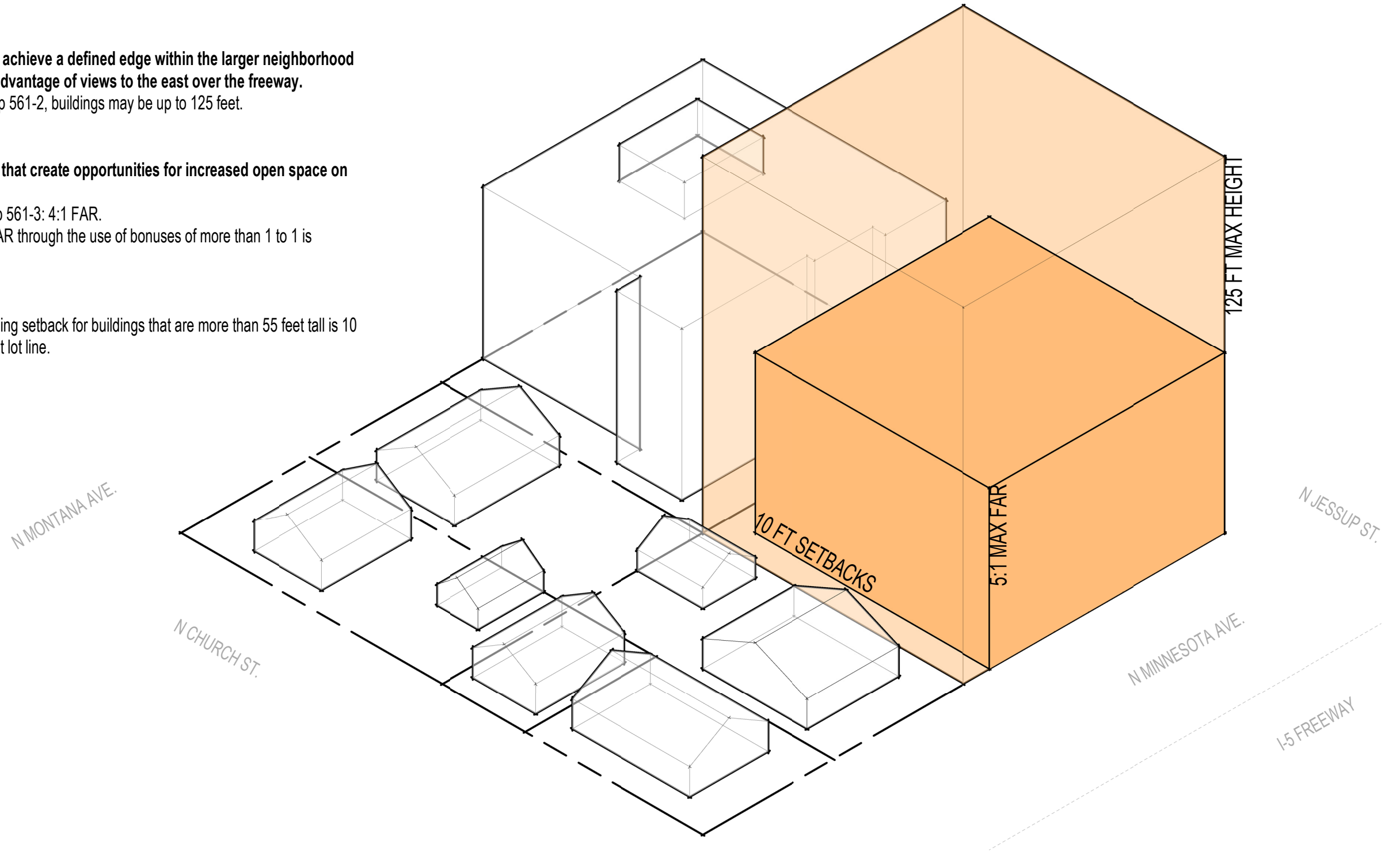
- A. Allow taller buildings along Interstate 5 to achieve a defined edge within the larger neighborhood context and allow buildings to take greater advantage of views to the east over the freeway.
- B. In the height opportunity areas shown on Map 561-2, buildings may be up to 125 feet.

33.561.220 Floor Area Ratios

- A. Allow design flexibility for taller buildings that create opportunities for increased open space on the site and visually prominent architecture.
- C. Maximum floor area ratios are shown on Map 561-3: 4:1 FAR.
- D. Maximum increase in FAR. An increase in FAR through the use of bonuses of more than 1 to 1 is prohibited.

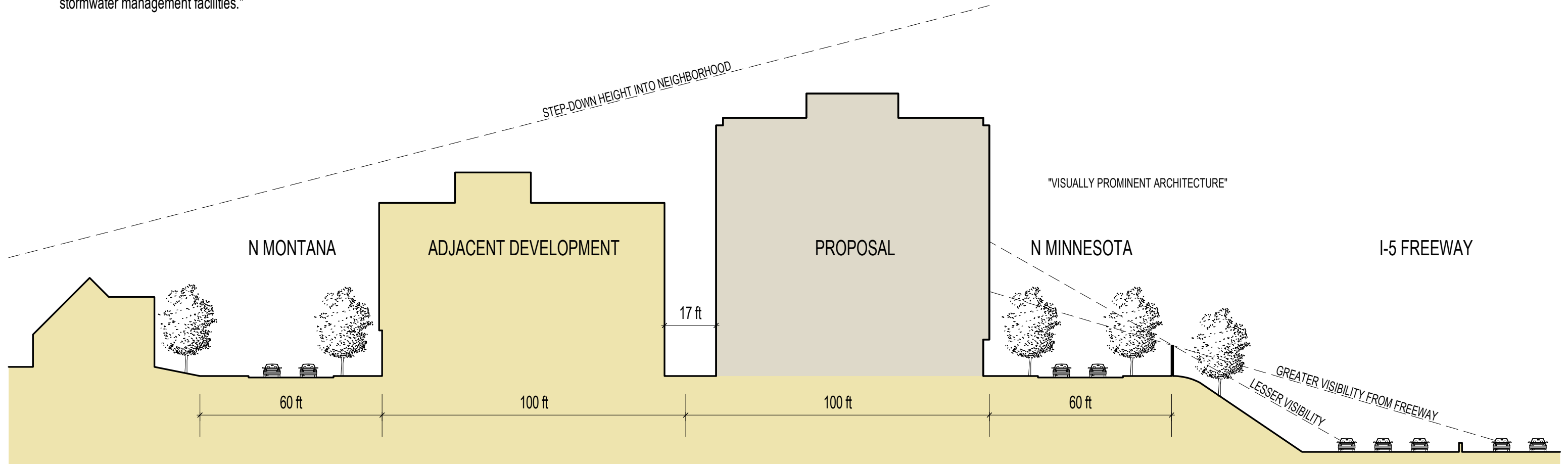
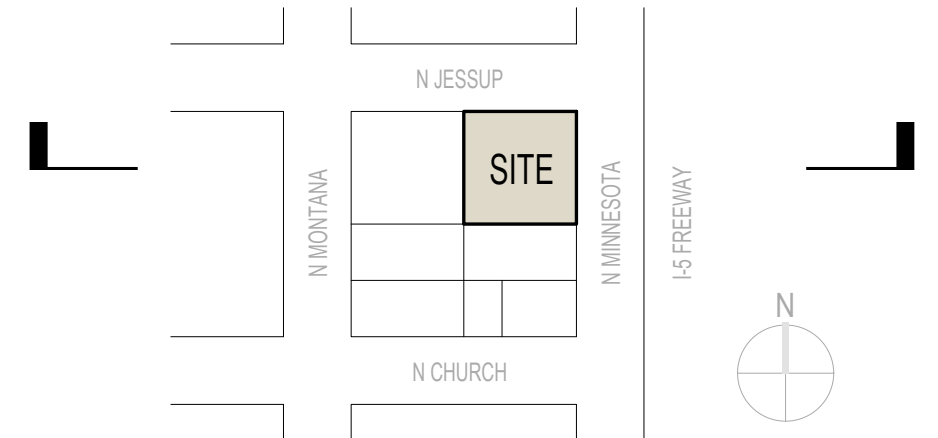
33.120.220 Setbacks

- B.1.b The required minimum side and rear building setback for buildings that are more than 55 feet tall is 10 feet from a side or rear lot line that is not a street lot line.



**NORTH INTERSTATE PLAN DISTRICT'S INTENT FOR THE SITE**

- "Allow taller buildings along Interstate 5 to achieve a **defined edge** within the larger neighborhood context and allow buildings to take greater **advantage of views** to the east over the freeway."
- "Allow **design flexibility for taller buildings** that create opportunities for increased open space on the site and **visually prominent architecture.**"
- "Creating a **special developed edge along the Interstate 5 Freeway.**"
- "Creating a **buffer for nearby residences and buildings.**"
- "Minnesota Avenue exists sporadically along the western edge of the freeway/sound wall, and offers a **unique opportunity for access to parking areas, building loading, other service functions, and/or stormwater management facilities.**"





Stacked logs Western Oregon



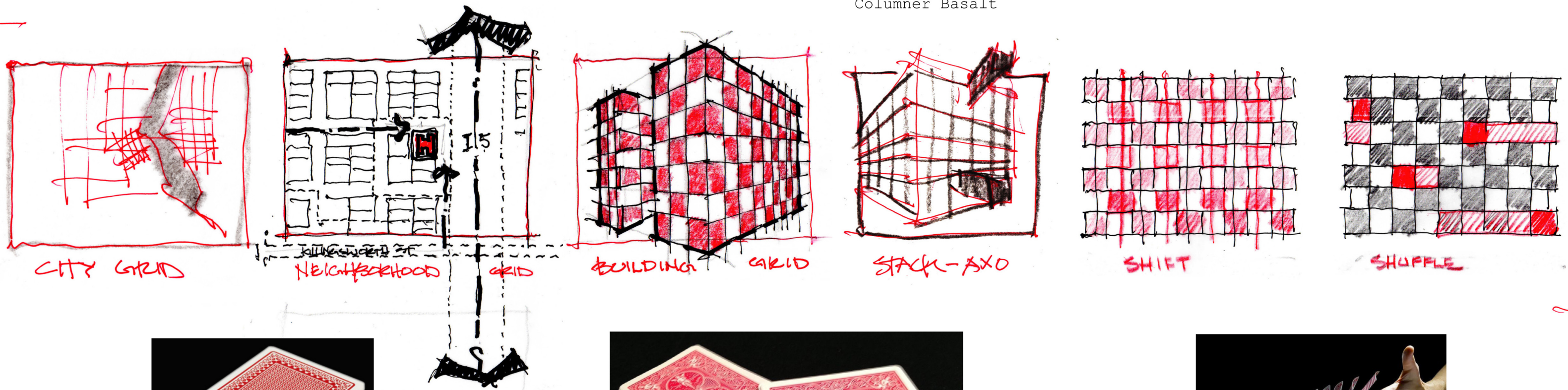
Train trestle bridge constructed of stacked logs



Columnar Basalt



Kiln dried stacked lumber



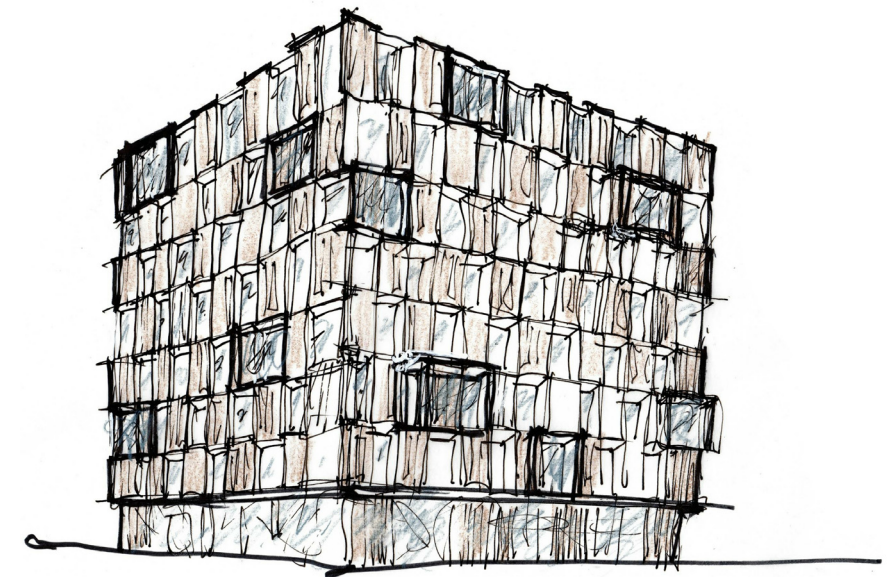
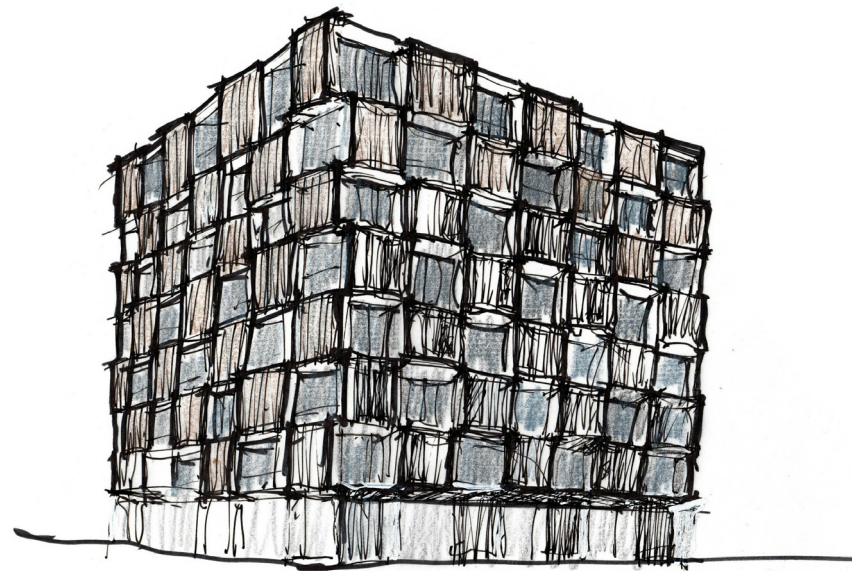
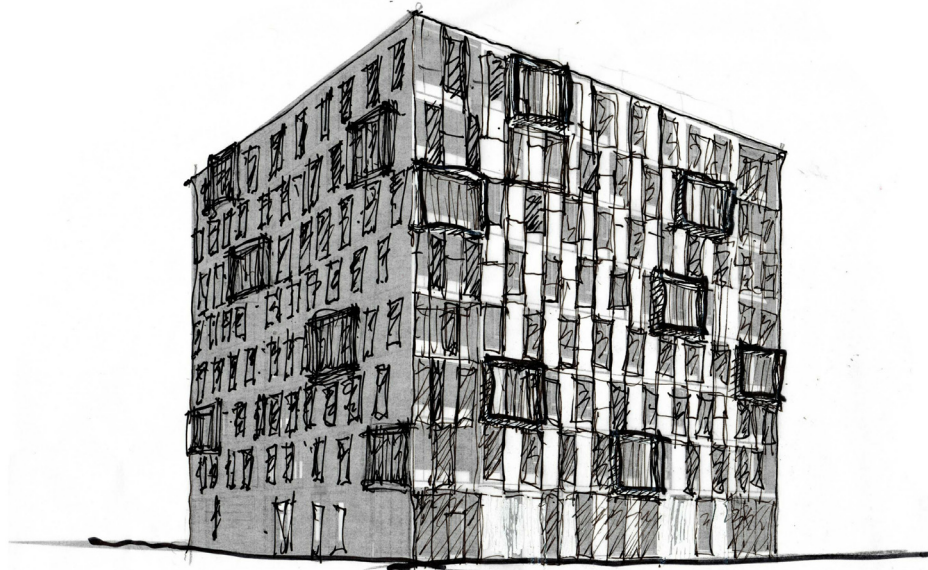
Stack



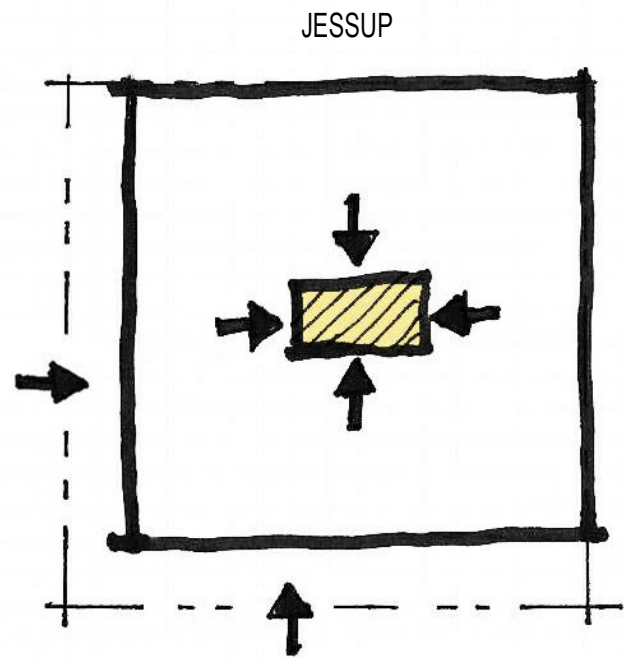
Shift



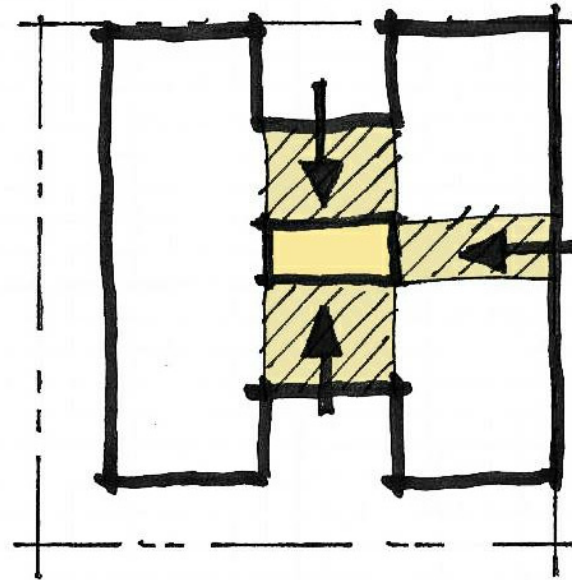
Shuffle



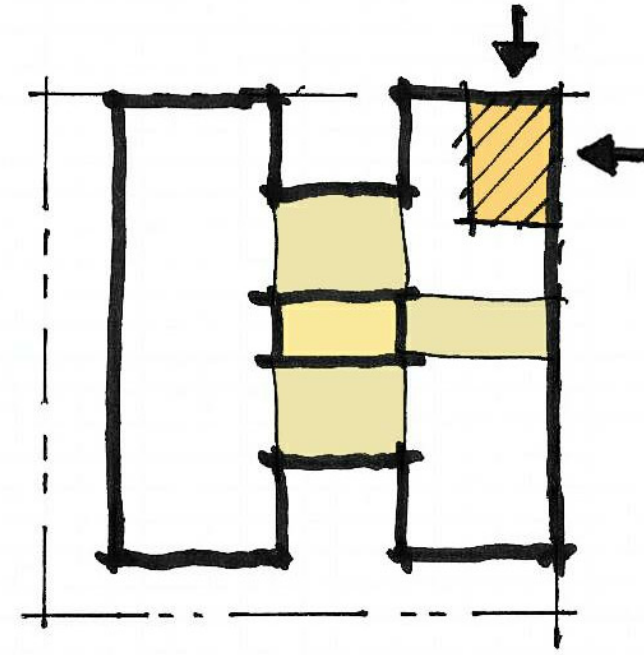




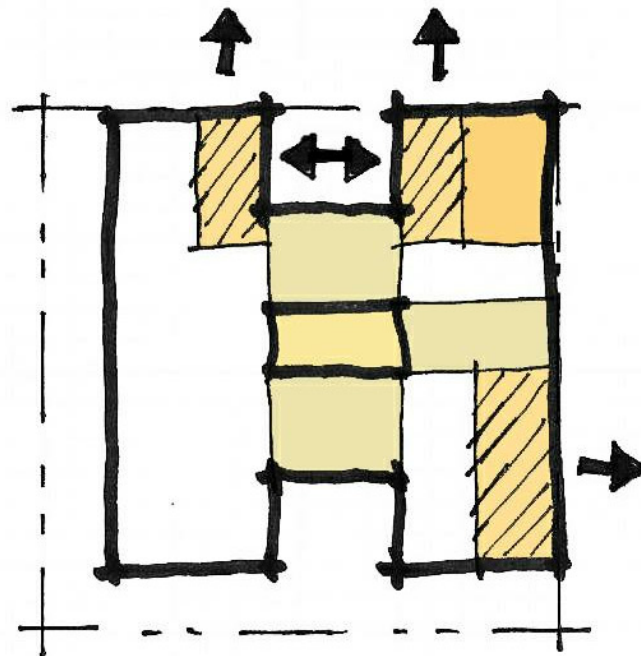
1. SET BACK FROM NIGHBORS & LOCATE CORE



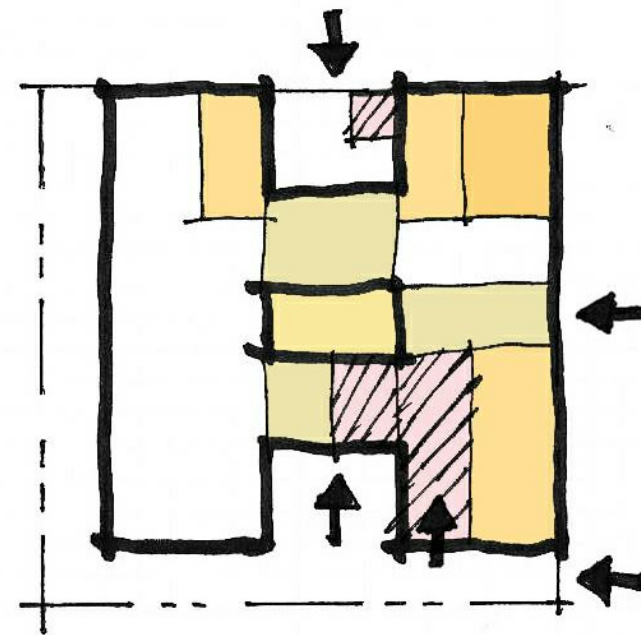
2. LOCATE ENTRANCES & ERODE FORM



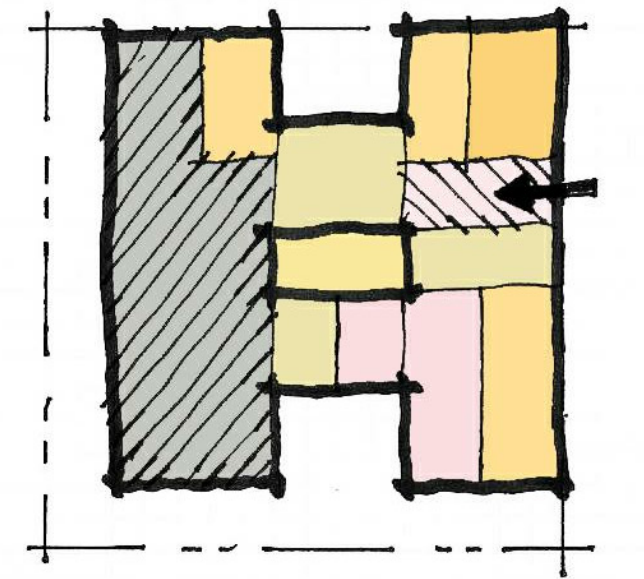
3. ACTIVATE CORNER



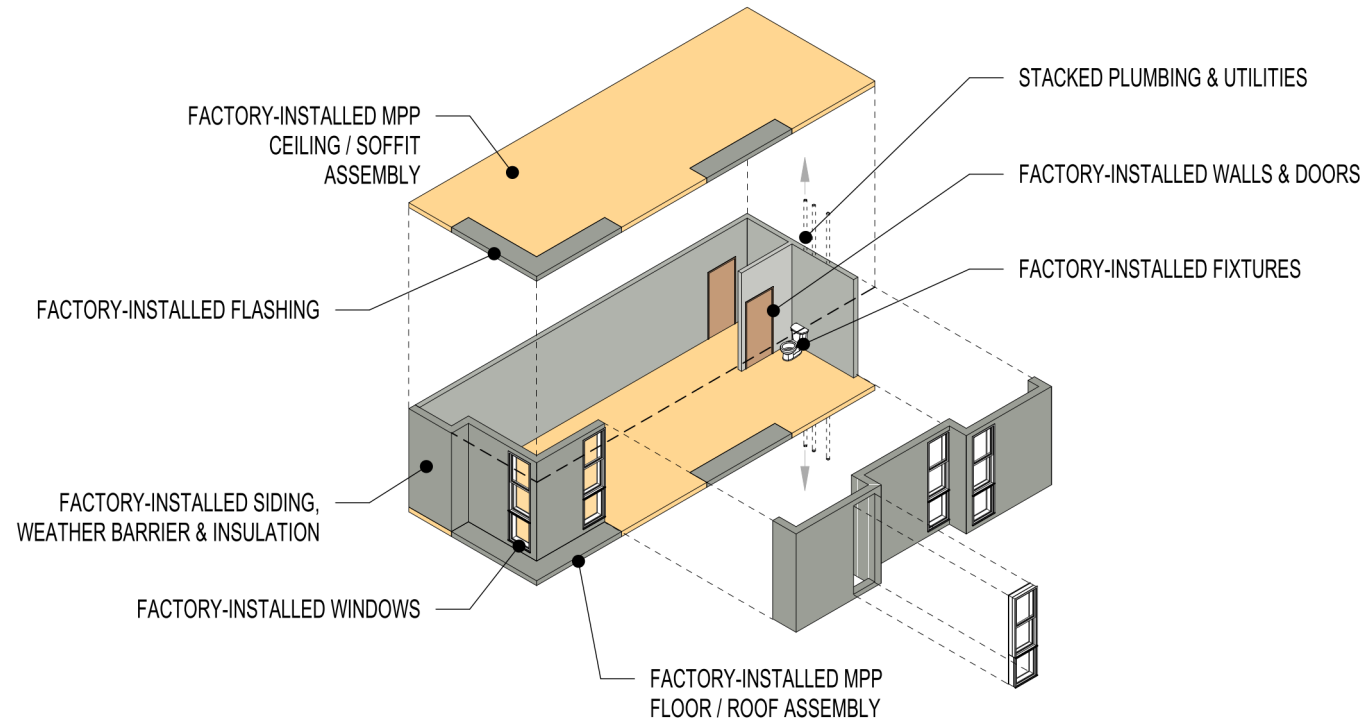
4. ACTIVATE STREET FRONTAGES & ENTRY COURT



5. LOCATE BICYCLE PARKING & ACCESS



6. LOCATE RESIDENTIAL UNITS & LOADING

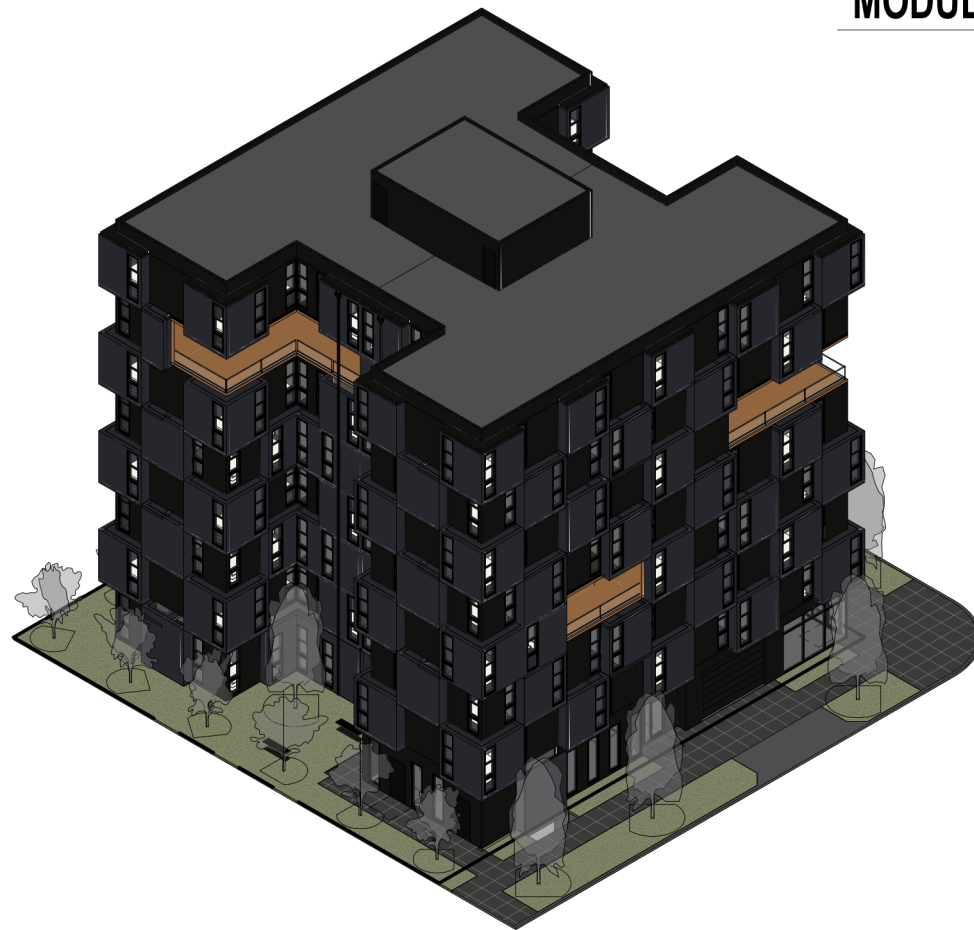


**MODULAR CONSTRUCTION**

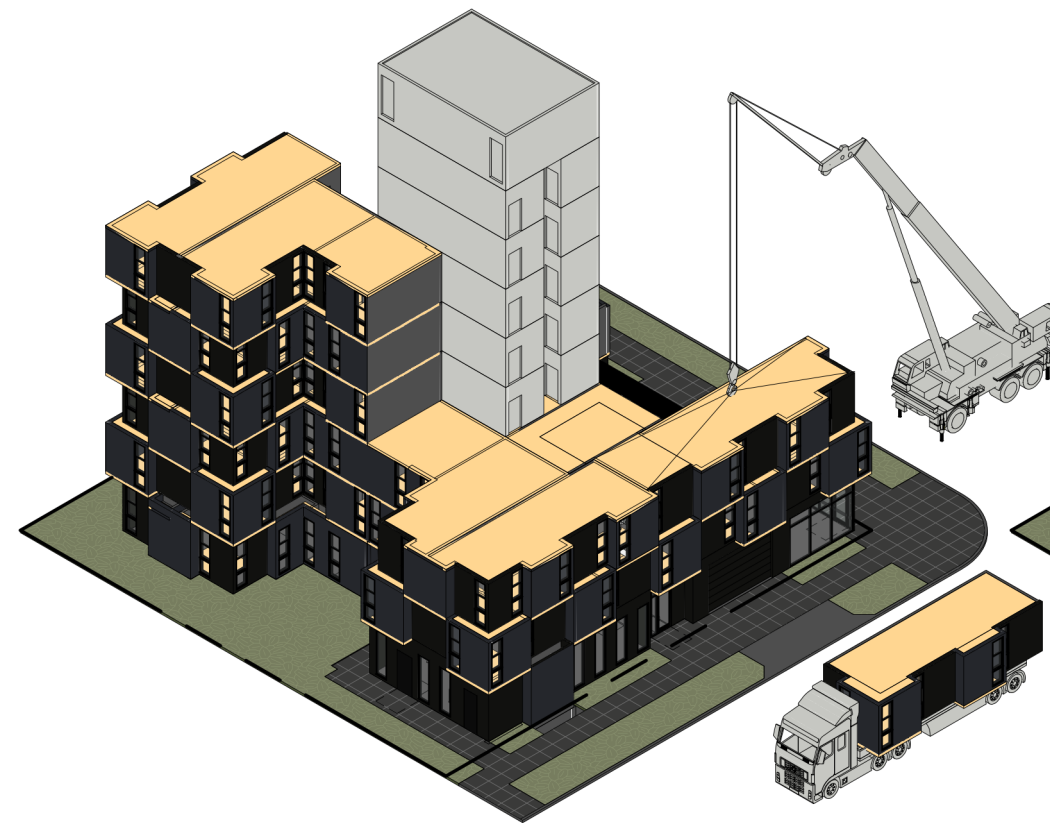
**CONSTRUCTION PHASING**

Modular construction reduces on-site activity and construction waste while speeding up the overall delivery schedule. Unlike conventional construction, each module will be constructed in a factory in optimal conditions and then delivered on site fully finished. The module forms stack with overlapping joints for a weather tight seal.

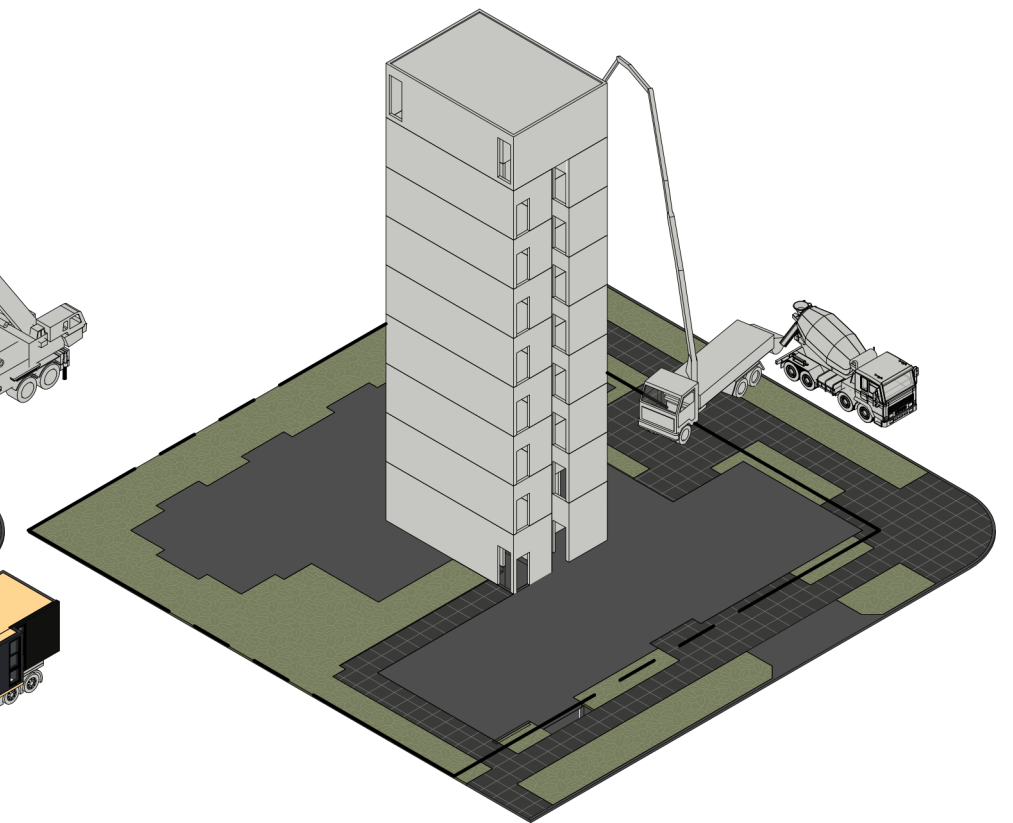
On-site construction will be limited to foundation, site and core work prior to the arrival of the module units. All utilities and concrete flatwork will also be completed before the crane arrives. After all modules are placed the landscaping, roof joints, and other minor finishes will be completed.



**LANDSCAPING & FINISHES**



**MODULAR STACKING**



**CORE & SITE WORK**

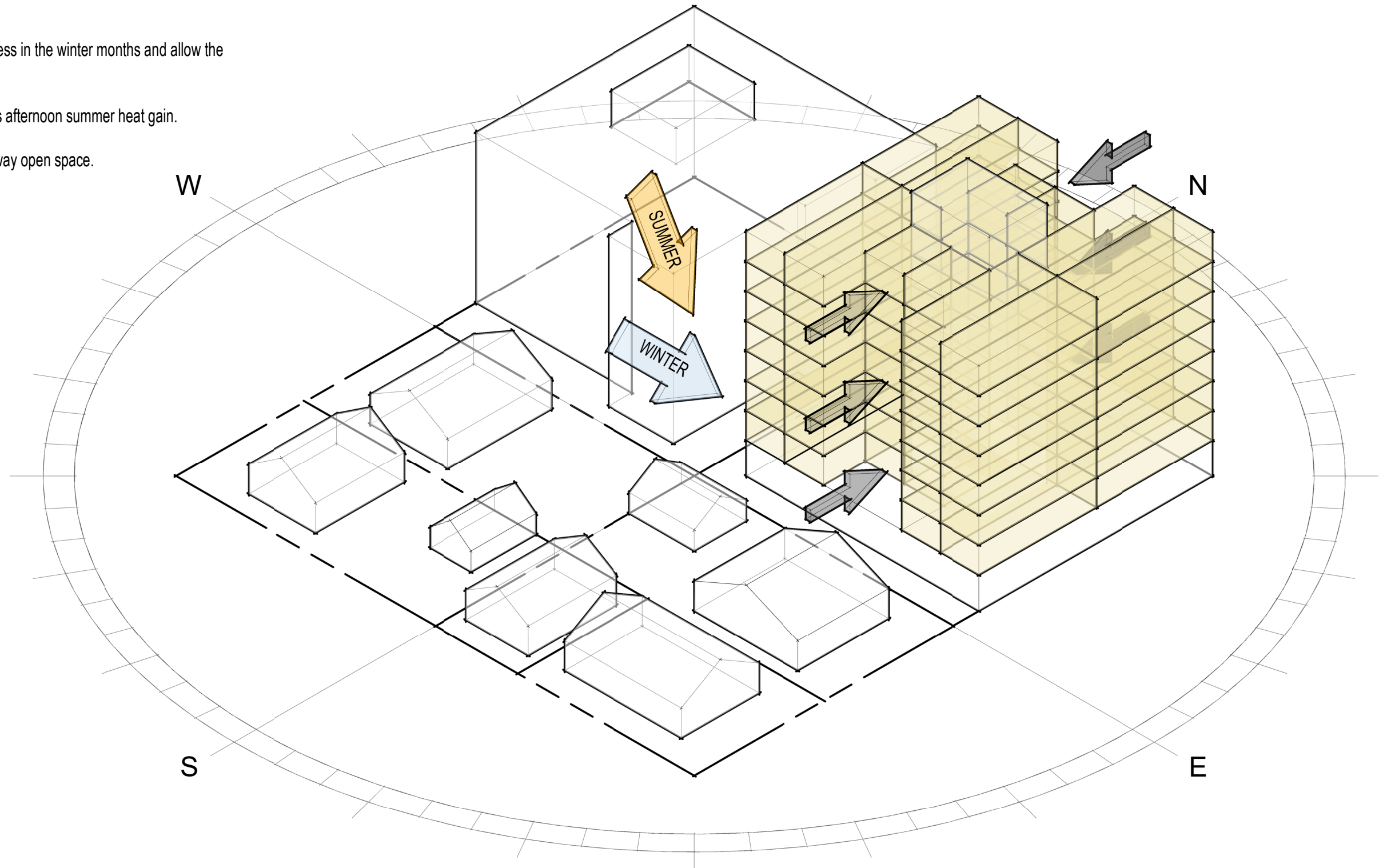
**ACCESS TO NATURAL LIGHT & AIR**

Maximize access to natural light and ventilation by recessing center court and modulating the facade.

Orient recession to South to maximize solar access in the winter months and allow the North entry court to receive summer sun.

New development to the West passively reduces afternoon summer heat gain.

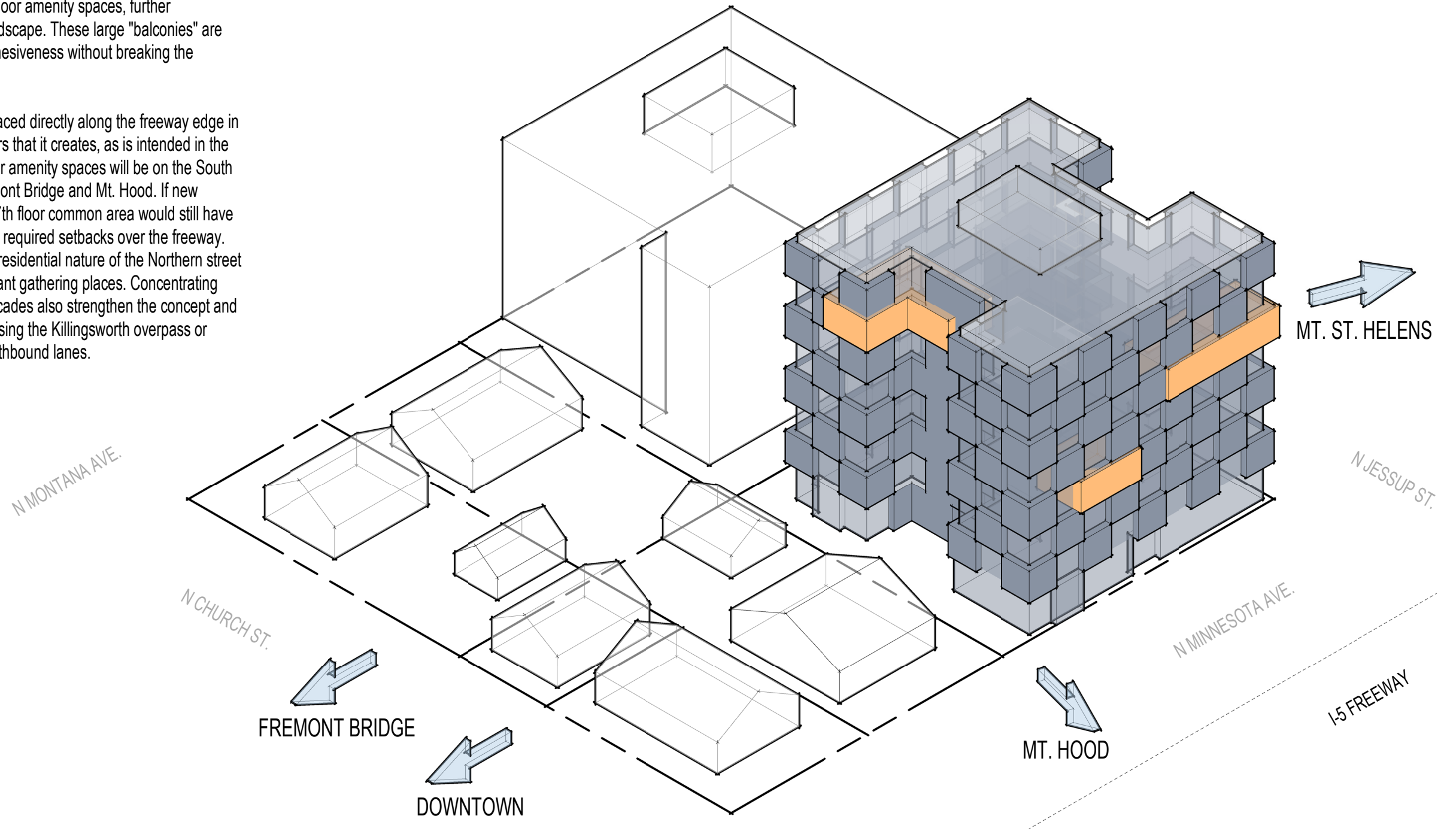
View opportunities to the East over the I-5 Freeway open space.

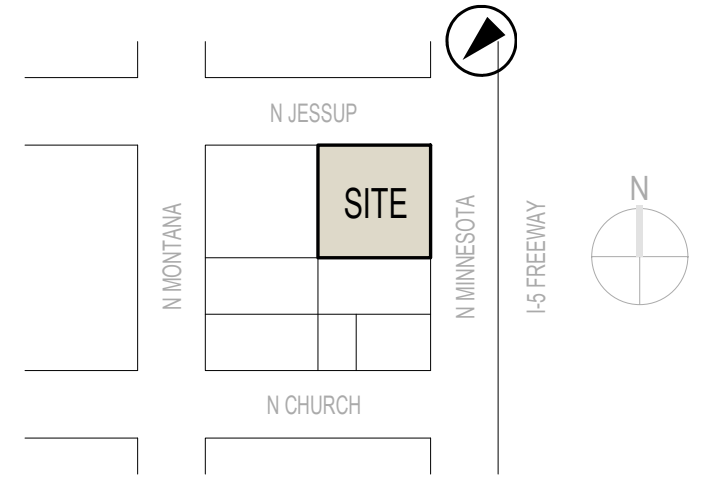


**COMMON AREAS & BUILDING FORM REFINEMENT**

The final component of the building design centers on the common areas within the building. The framework is eroded by three outdoor amenity spaces, further expressing the connection to the vernacular landscape. These large "balconies" are recessed into the grid structure, maintaining cohesiveness without breaking the modular construction stacking.

Two of the three primary amenity spaces are placed directly along the freeway edge in order to utilize the open space and view corridors that it creates, as is intended in the design guidelines. The largest of the three upper amenity spaces will be on the South facade with direct views of Downtown, the Fremont Bridge and Mt. Hood. If new development were to be built to the South, the 7th floor common area would still have unobstructed views of the mountain through the required setbacks over the freeway. New development directly to the West, and the residential nature of the Northern street preclude these facades from being ideal for tenant gathering places. Concentrating these formal moves onto the South and East facades also strengthen the concept and the building as a visible landmark for those crossing the Killingsworth overpass or traveling on I-5 below it, especially from the Northbound lanes.





NE PERSPECTIVE VIEW FROM MINNESOTA & JESSUP LOOKING SW



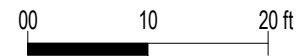
SE PERSPECTIVE VIEW FROM MINNESOTA LOOKING NORTHWEST



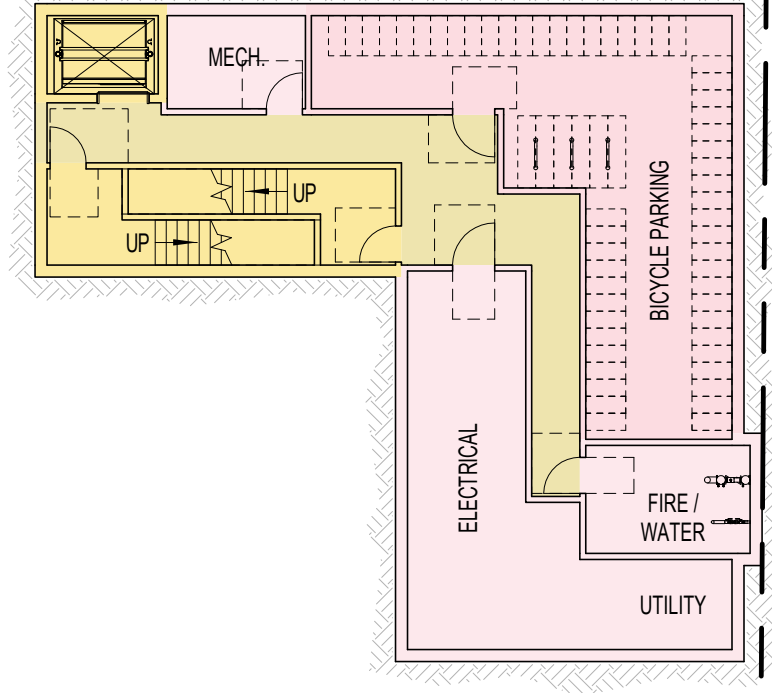
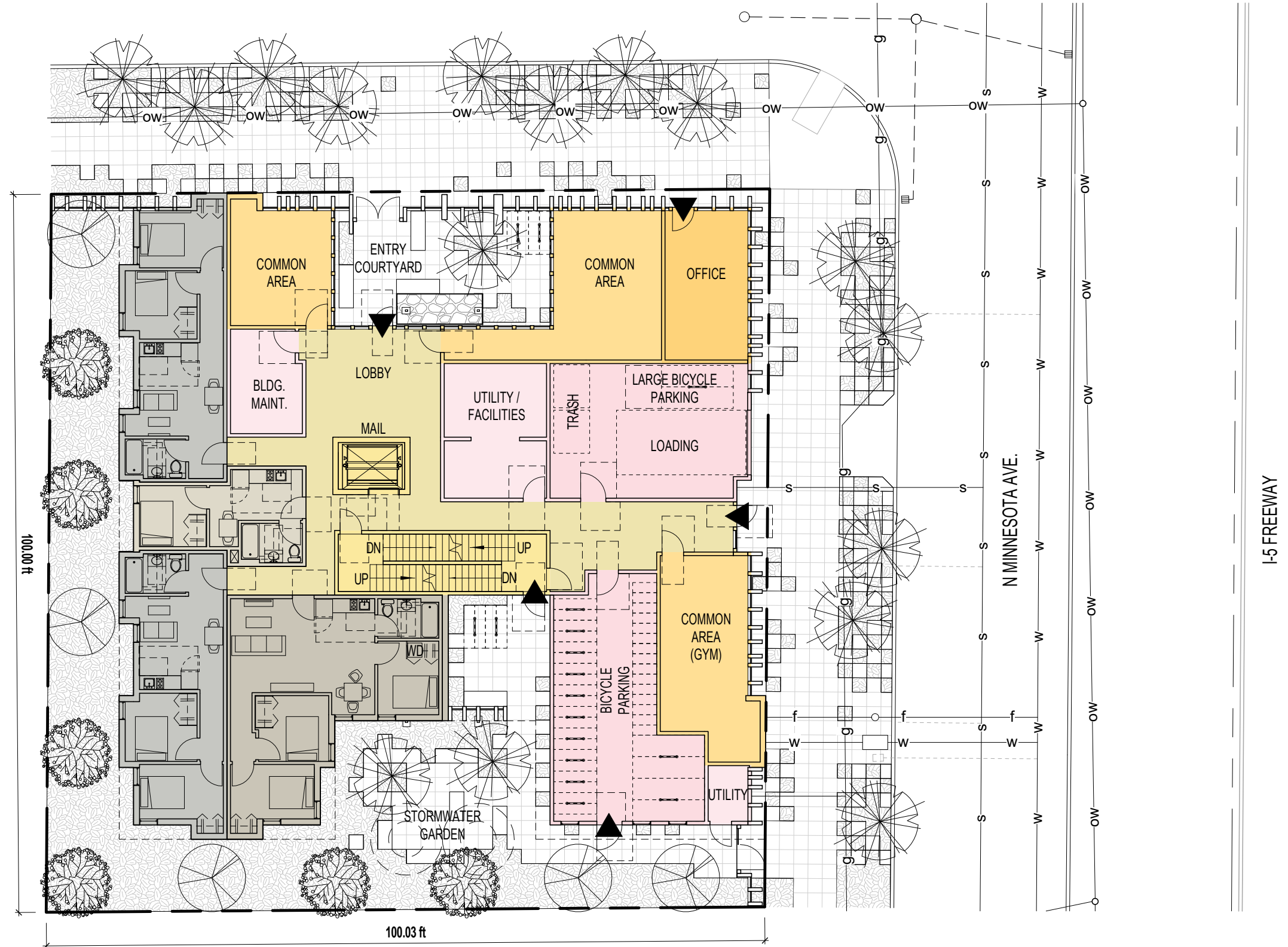
I-5 PERSPECTIVE VIEW FROM NORTHBOUND LANES LOOKING NORTHWEST

PLAN KEY:

- 1-BEDROOM UNIT
- 2-BEDROOM UNIT
- 3-BEDROOM UNIT
- RECREATIONAL / ACTIVE USE
- CIRCULATION
- USER AMENITY / UTILITY



N JESSUP ST.



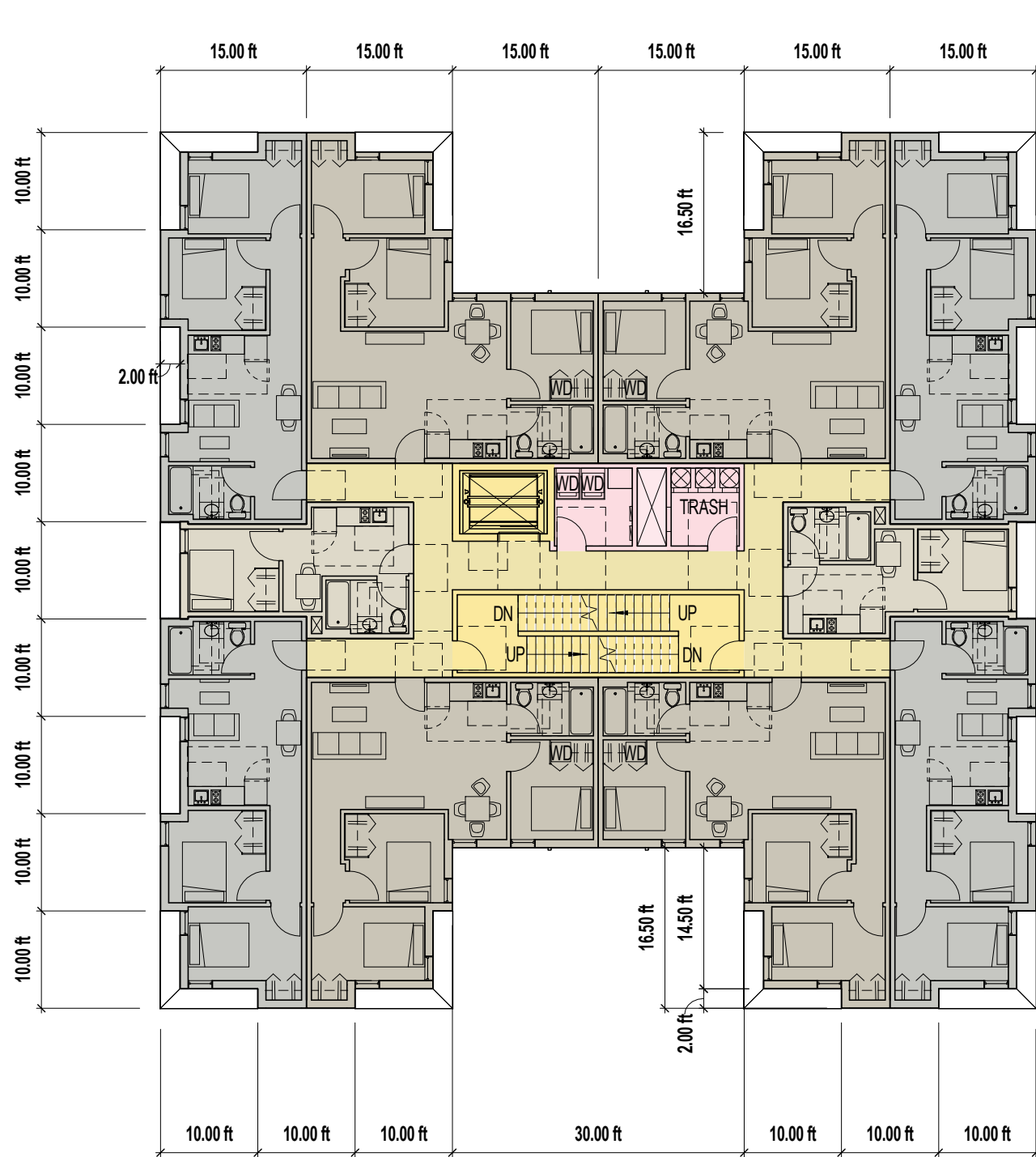
FLOOR PLAN BASEMENT

1/16" = 1'-0"

SITE PLAN & GROUND FLOOR PLAN

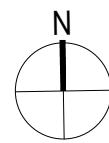
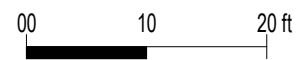
1/16" = 1'-0"





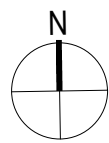
FLOOR PLAN LEVEL 3

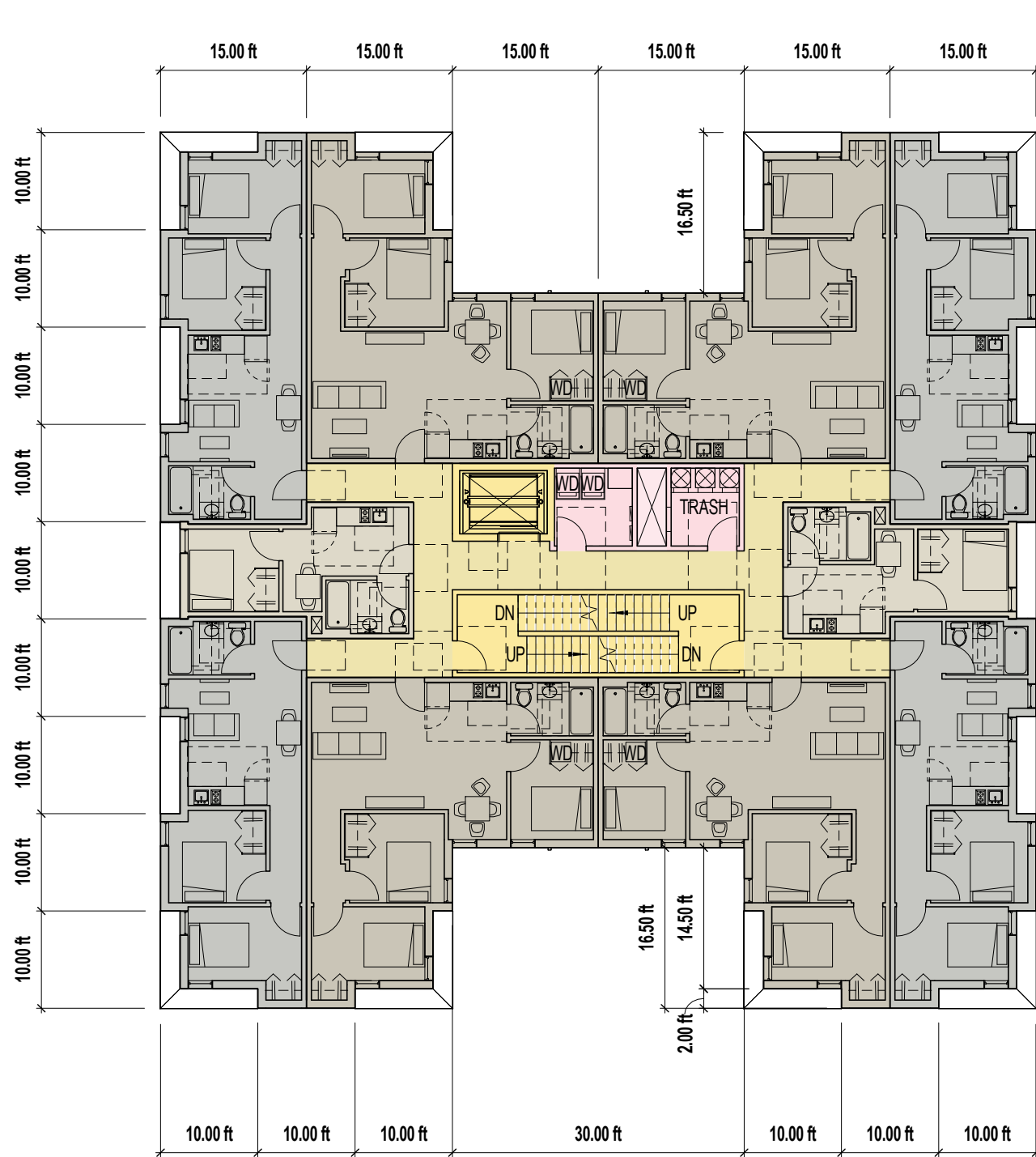
1/16" = 1'-0"



FLOOR PLAN LEVEL 2

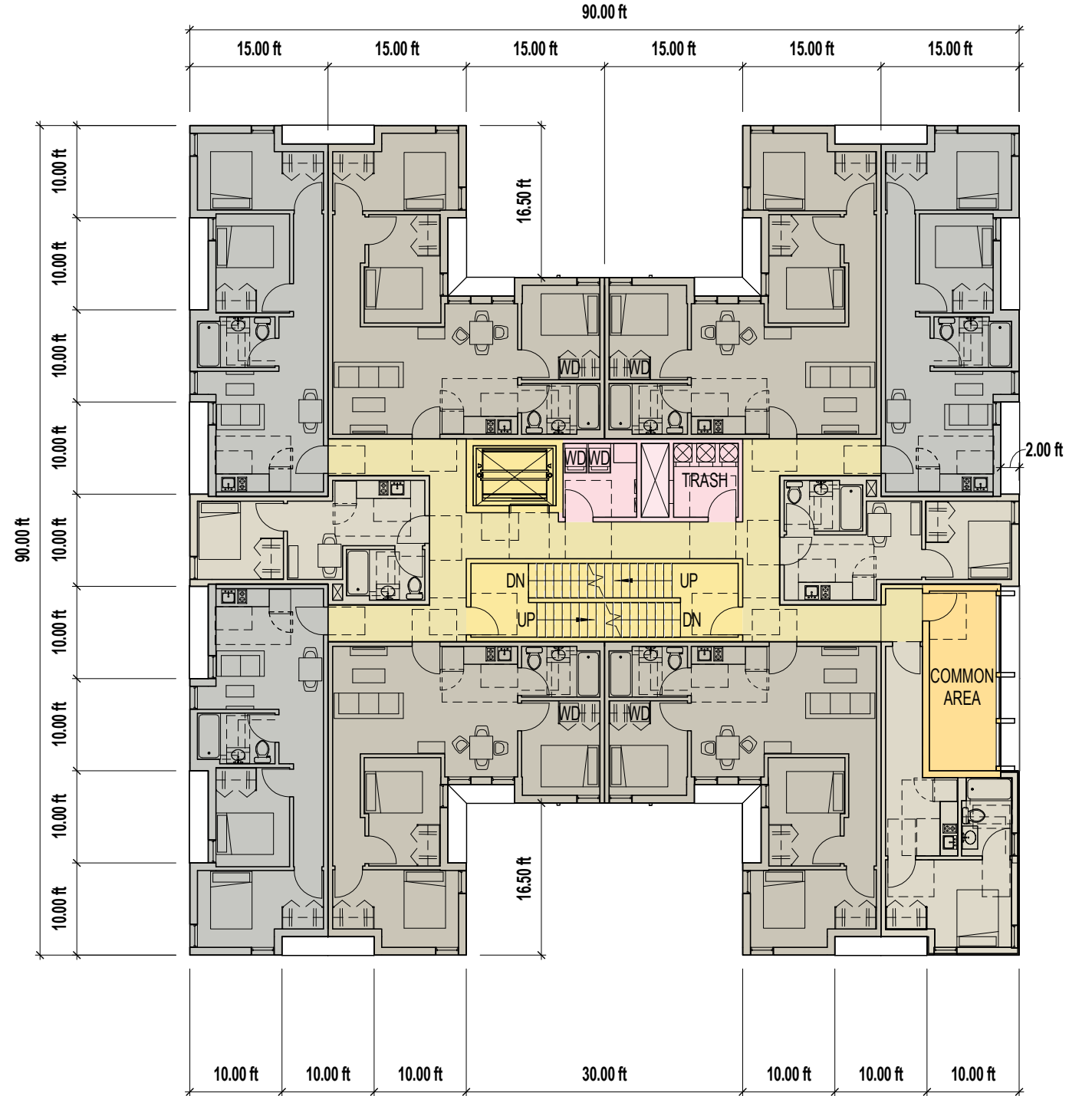
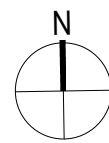
1/16" = 1'-0"





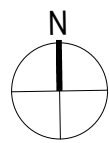
**FLOOR PLAN LEVEL 5**

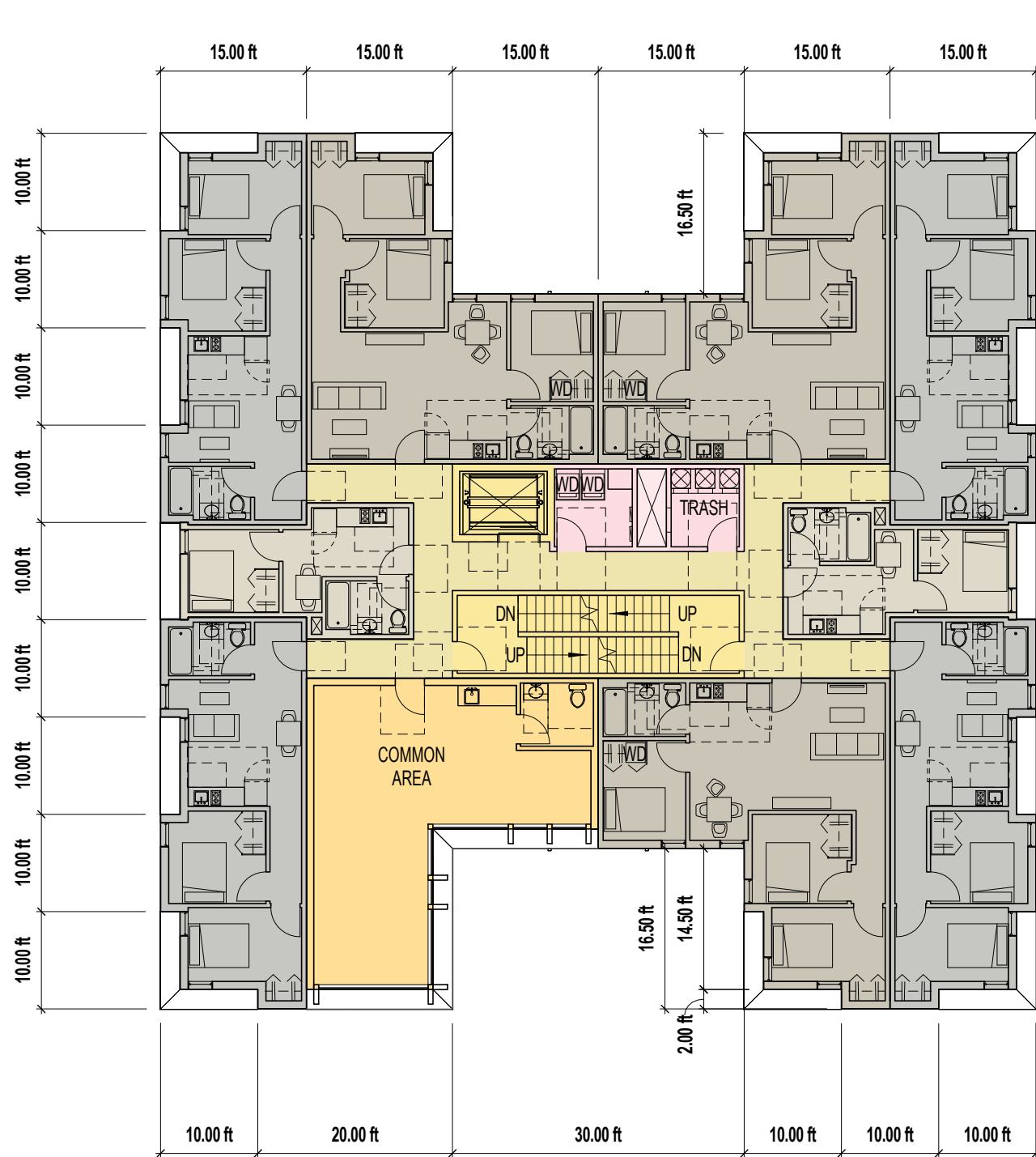
1/16" = 1'-0"



**FLOOR PLAN LEVEL 4**

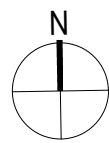
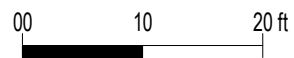
1/16" = 1'-0"





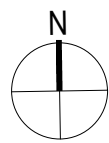
**FLOOR PLAN LEVEL 7**

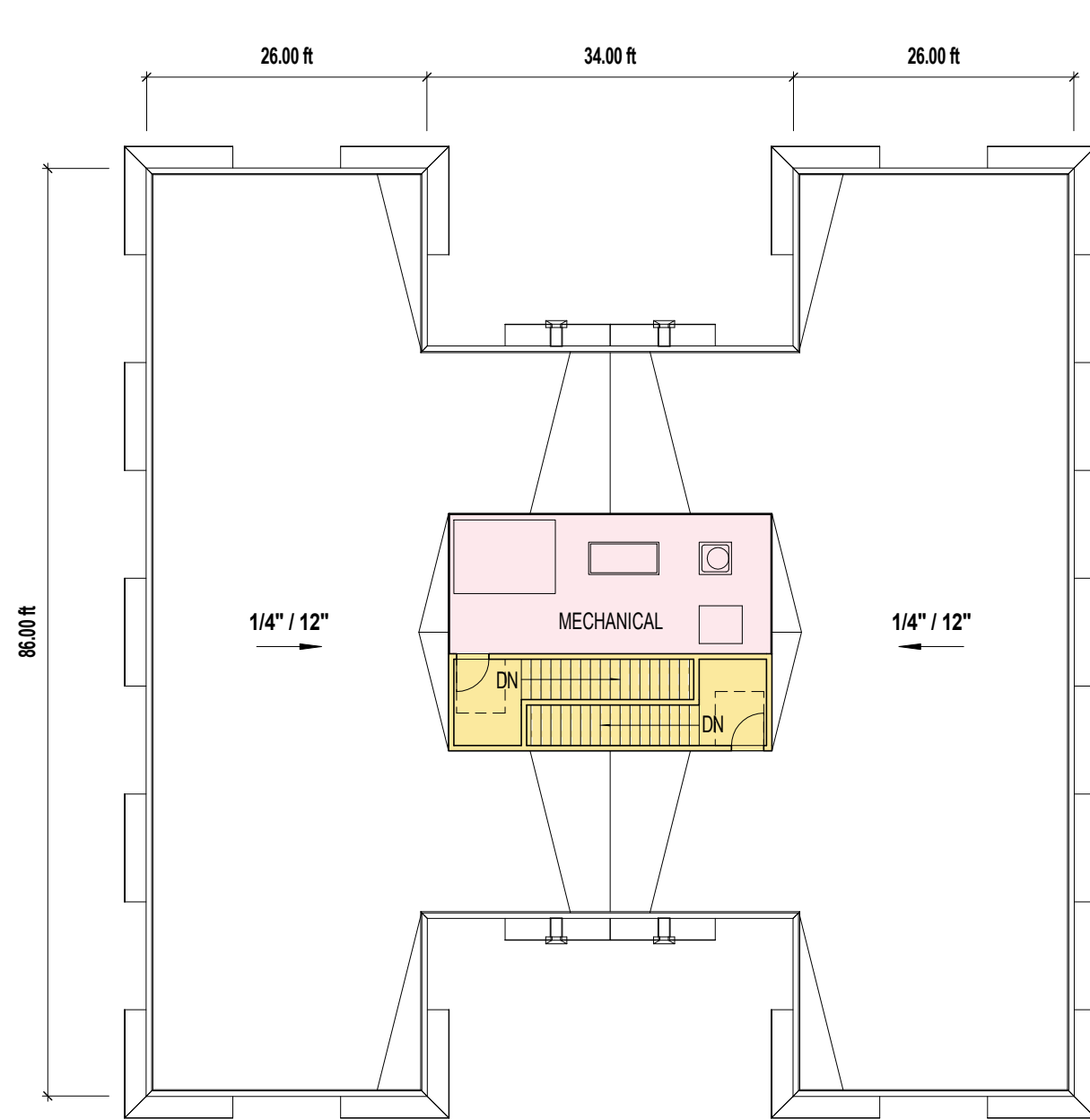
1/16" = 1'-0"



**FLOOR PLAN LEVEL 6**

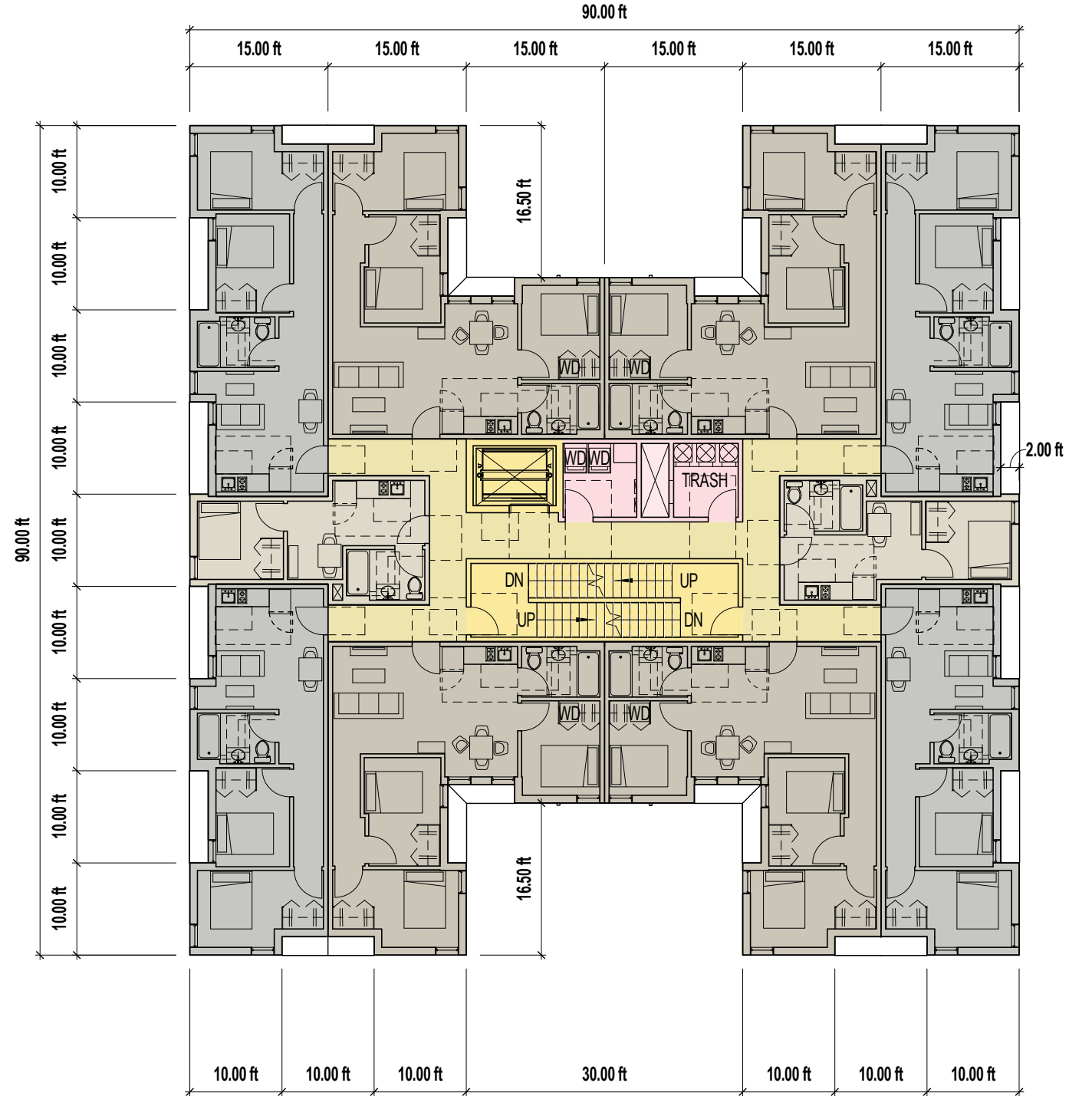
1/16" = 1'-0"





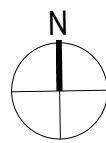
ROOF PLAN

1/16" = 1'-0"



FLOOR PLAN LEVEL 8

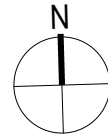
1/16" = 1'-0"





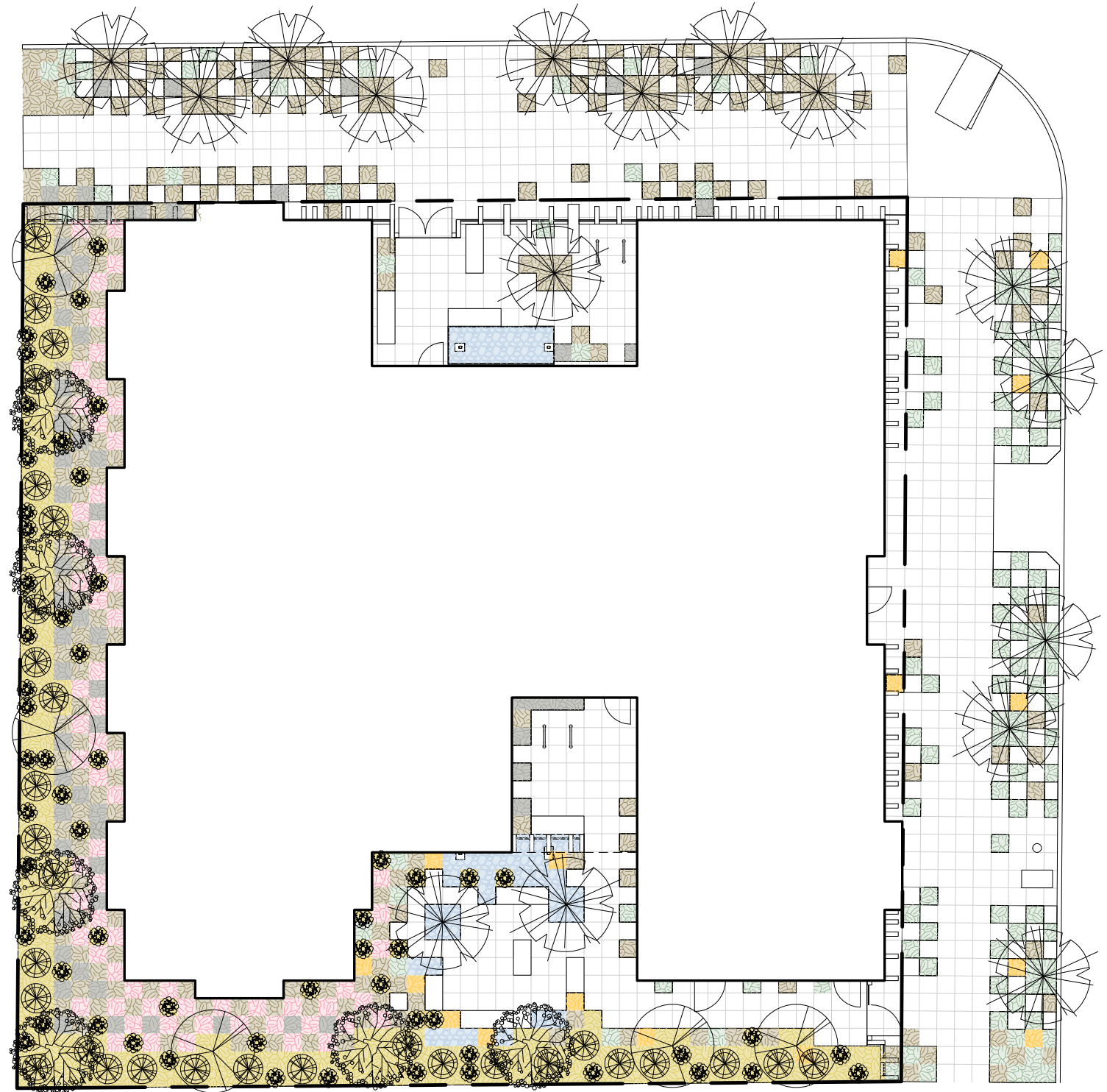
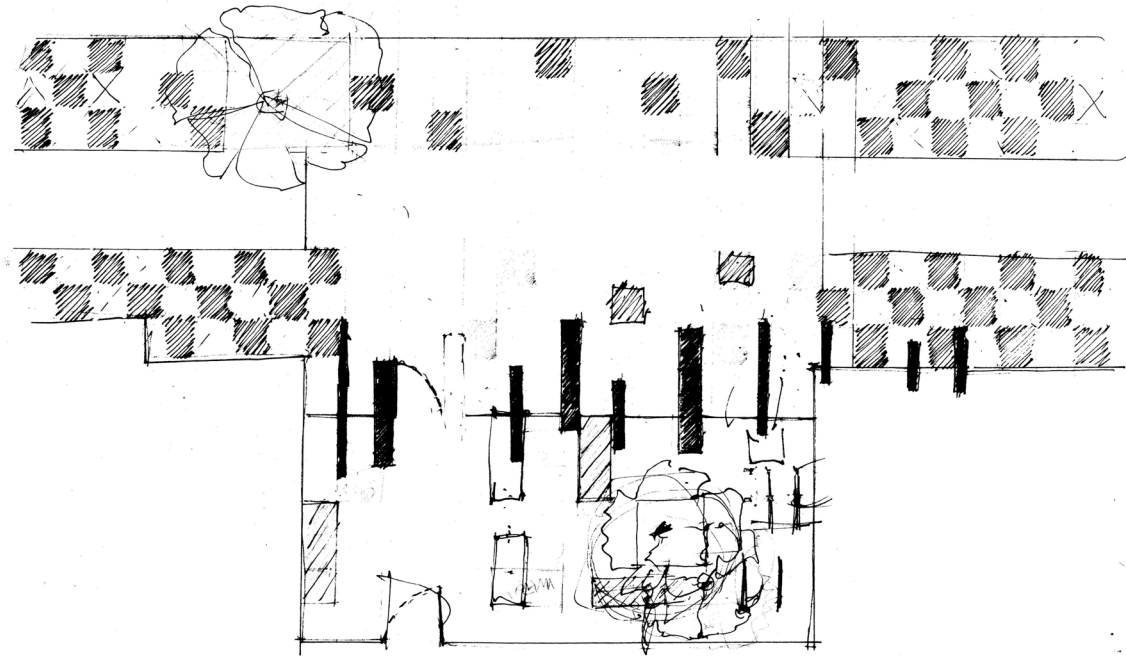


N JESSUP ST.



00 10 20 ft

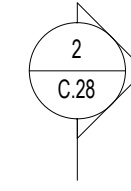
- LANDSCAPING KEY
- BLACK MONDO GRASS
  - GREEN MONDO GRASS
  - NATIVE BUNCHBERRY
  - SWORD FERN
  - FOUNTAIN GRASS
  - ROCK / STORM GARDEN
  - PERIMETER LANDSCAPING, SEE ENLARGED PLANS



N MINNESOTA AVE.

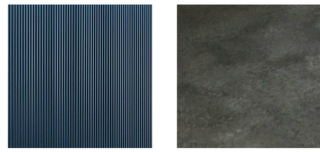
LANDSCAPING PLAN

1/16" = 1'-0"

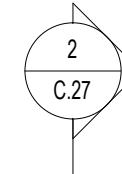
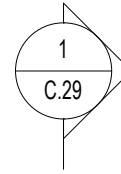
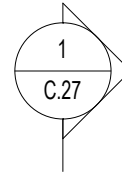


**ENLARGED EAST ELEVATION**

1/8" = 1'-0"



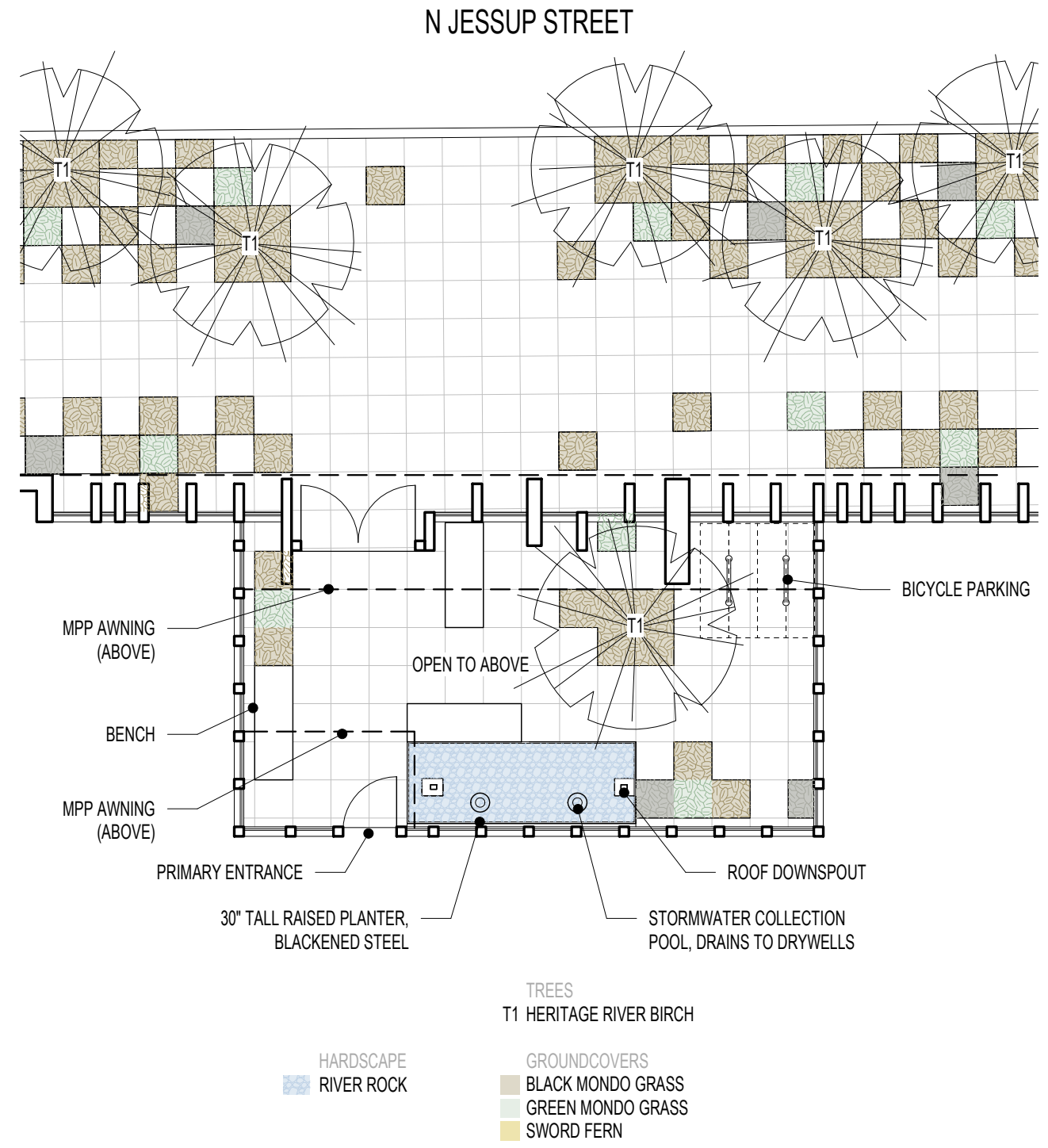
STUCCO OR METAL  
PANEL SIDING



**ENLARGED NORTH ELEVATION**

1/8" = 1'-0"





**ENLARGED ENTRY COURTYARD PLAN**

1/8" = 1'-0"

# PUBLIC REALM

22'-0"  
FINISH FLOOR LEVEL 3

12'-0"  
FINISH FLOOR LEVEL 2

0'-0"  
FINISH FLOOR LEVEL 1



UNIT INTERIOR    SET BACK 2 ft    WEST PERIMETER LANDSCAPING 10 ft    ADJACENT LANDSCAPING 6 - 14 ft (varies)    ADJACENT DEVELOPMENT

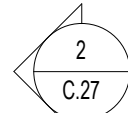


**TYPICAL LANDSCAPING SECTION - WEST PERIMETER**

1/4" = 1'-0"

# LANDSCAPING - WEST SETBACK

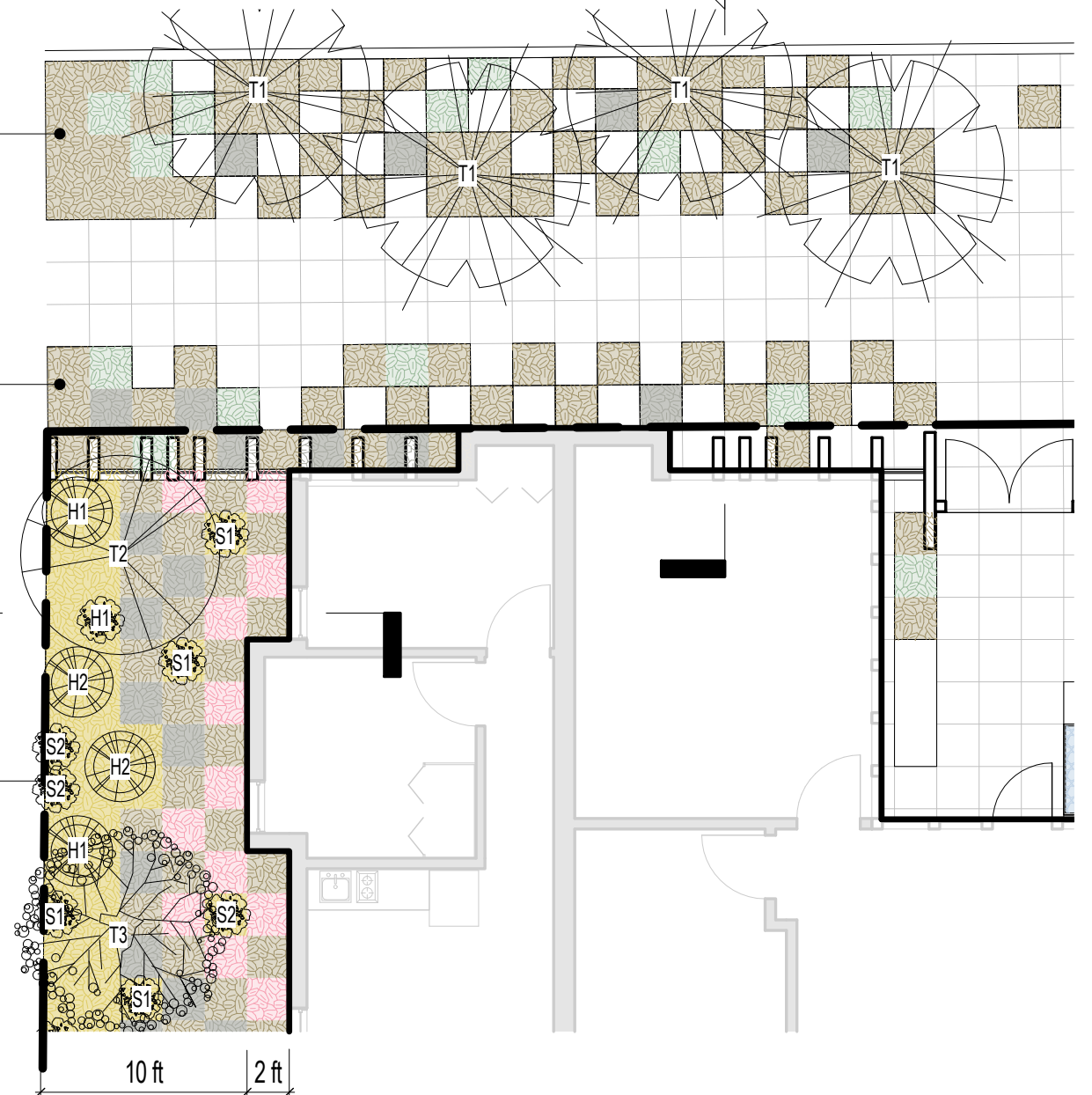
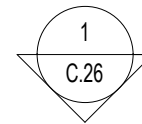
N JESSUP ST.



FURNISHING ZONE LANDSCAPING

FRONTAGE ZONE LANDSCAPING

WEST PERIMETER LANDSCAPING



- TREES**  
 T1 HERITAGE RIVER BIRCH  
 T2 MAPLE MATSUMURAE  
 T3 WHITE STAR MAGNOLIA

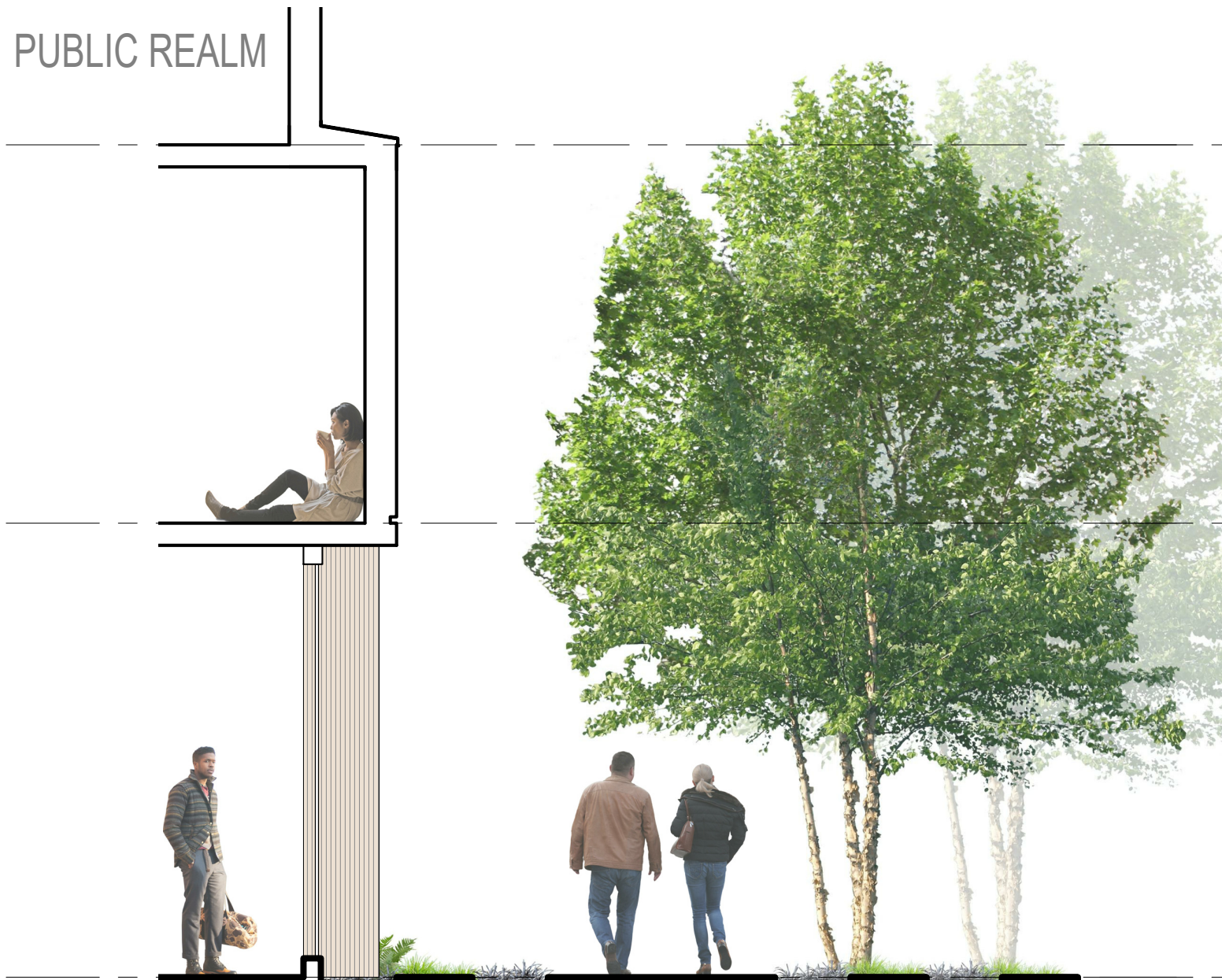
- SHRUBS**  
 H1 CAMELLIA SASANQUA  
 H2 CUTLEAF LILAC  
 S1 NATIVE SNOWBERRY  
 S2 RED HUCKLEBERRY

- GROUNDCOVERS**  
 BLACK MONDO GRASS  
 GREEN MONDO GRASS  
 NATIVE BUNCHBERRY  
 SWORD FERN

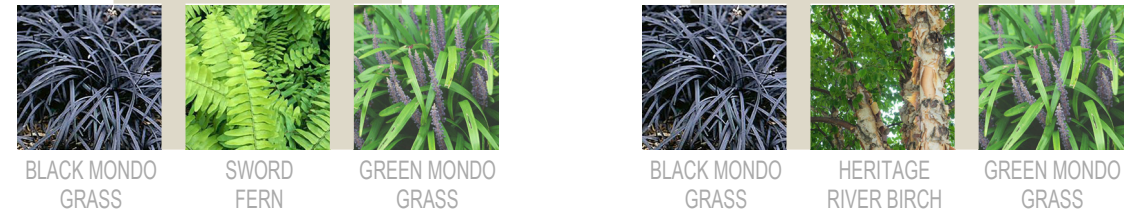
**TYPICAL LANDSCAPING PLAN - NORTH & WEST**

1/8" = 1'-0"

PUBLIC REALM



ACTIVE USE    SET BACK 2 ft    FRONTAGE ZONE 4 ft    THROUGH PEDESTRIAN 6 ft    FURNISHING ZONE 8 ft    N. JESSUP



BLACK MONDO GRASS    SWORD FERN    GREEN MONDO GRASS    BLACK MONDO GRASS    HERITAGE RIVER BIRCH    GREEN MONDO GRASS

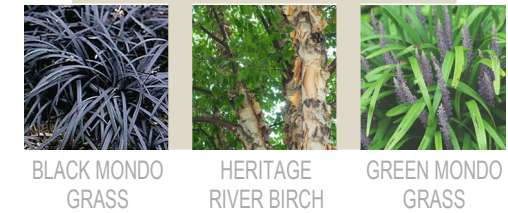
**NORTH SIDEWALK SECTION - TYPICAL SIDEWALK**

1/4" = 1'-0"

LANDSCAPING - STREET FRONTAGE



ACTIVE USE    SET BACK 2 ft    FRONTAGE ZONE 4 ft    THROUGH PEDESTRIAN 6 ft    FURNISHING ZONE 8 ft    N. JESSUP



BLACK MONDO GRASS    HERITAGE RIVER BIRCH    GREEN MONDO GRASS

**NORTH SIDEWALK SECTION - PEDESTRIAN AREA**

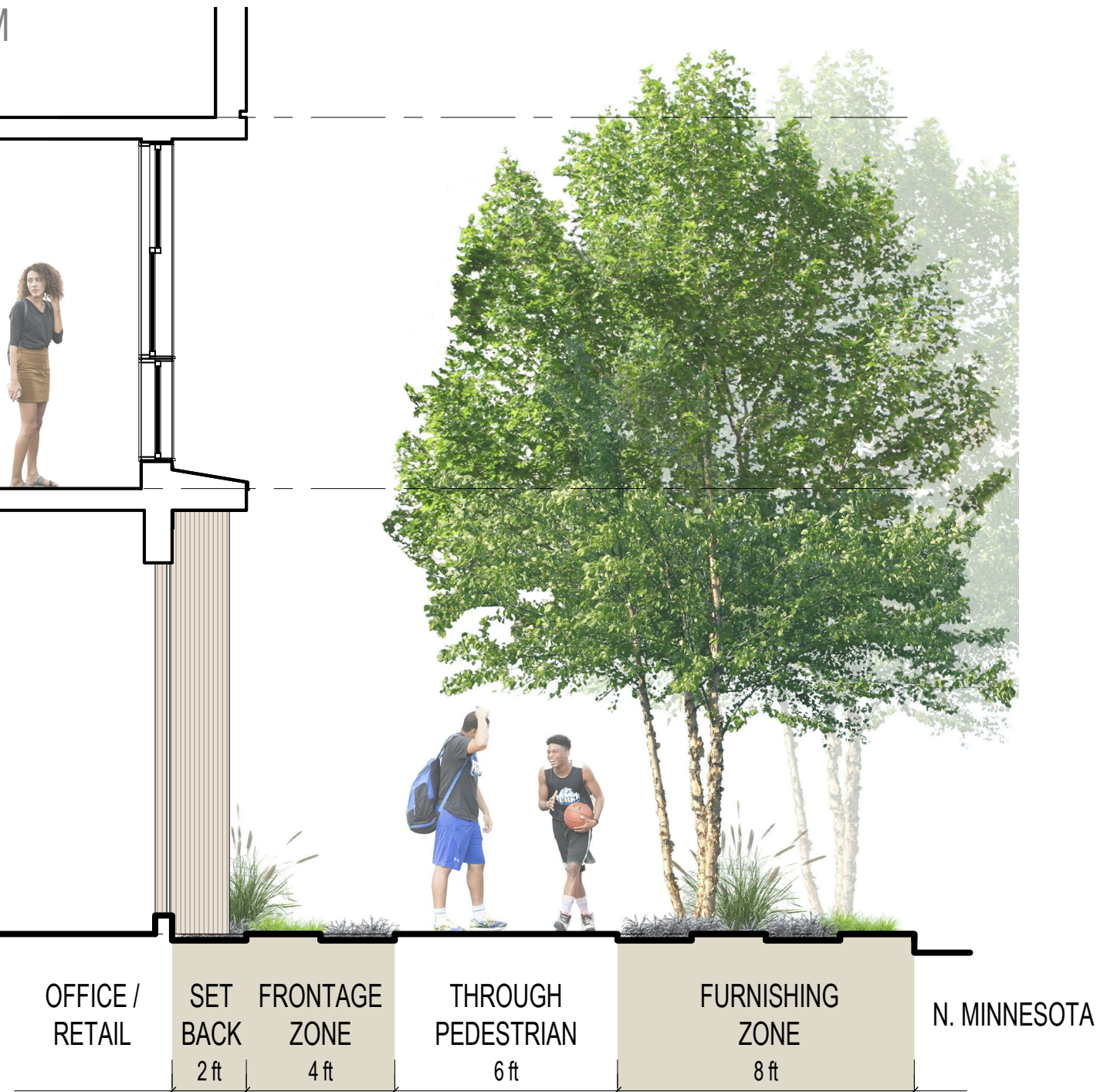
1/4" = 1'-0"

# PUBLIC REALM

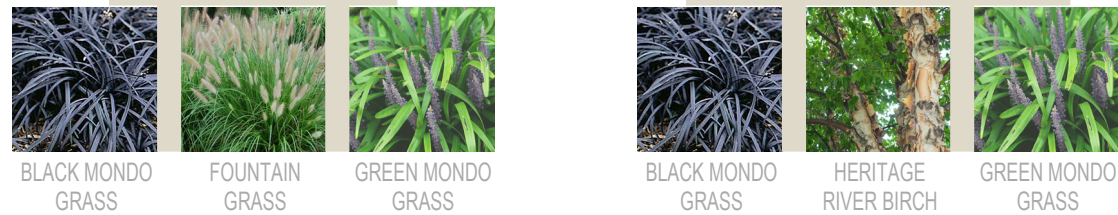
22'-0"  
FINISH FLOOR LEVEL 3

12'-0"  
FINISH FLOOR LEVEL 2

0'-0"  
FINISH FLOOR LEVEL 1



OFFICE / RETAIL    SET BACK 2 ft    FRONTAGE ZONE 4 ft    THROUGH PEDESTRIAN 6 ft    FURNISHING ZONE 8 ft    N. MINNESOTA



**EAST SIDEWALK SECTION - TYPICAL SIDEWALK**

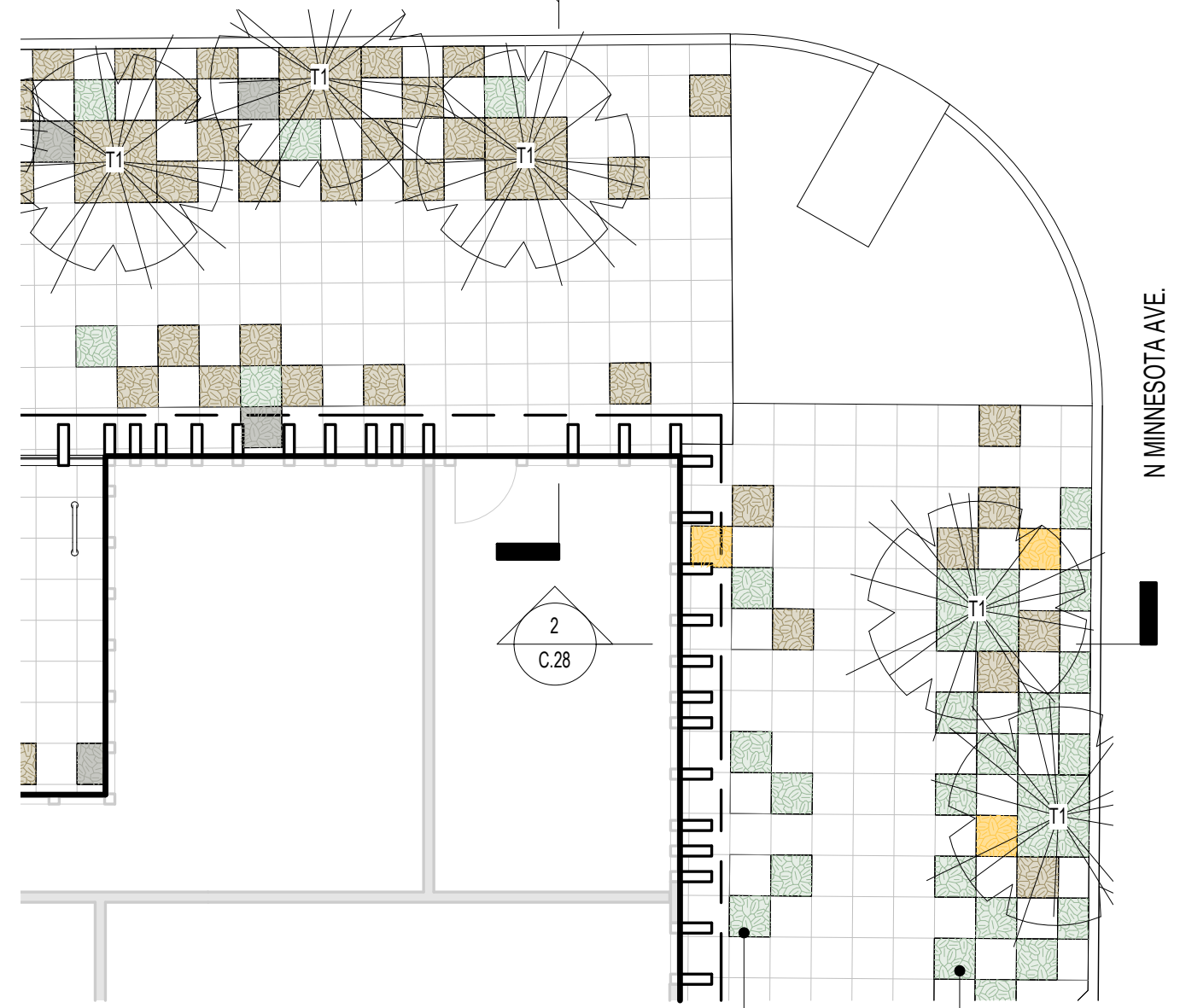
1/4" = 1'-0"

# LANDSCAPING - STREET CORNER

1  
C.27

N JESSUP ST.

N MINNESOTA AVE.



TREES  
T1 HERITAGE RIVER BIRCH

GROUNDCOVERS  
BLACK MONDO GRASS  
GREEN MONDO GRASS  
SWORD FERN  
FOUNTAIN GRASS

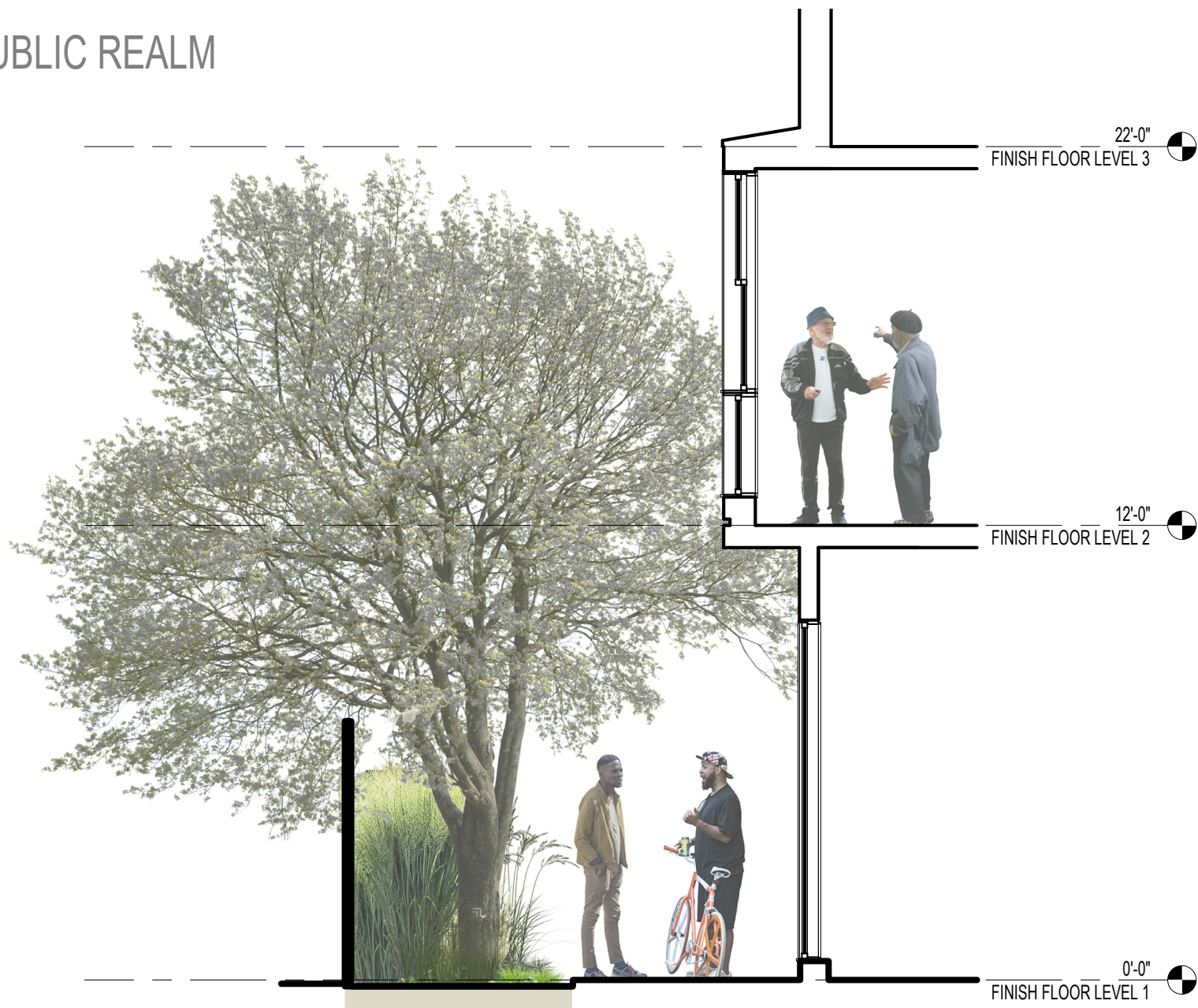
FRONTAGE ZONE LANDSCAPING

FURNISHING ZONE LANDSCAPING

**TYPICAL LANDSCAPING PLAN - EAST & SOUTH**

1/8" = 1'-0"

PUBLIC REALM

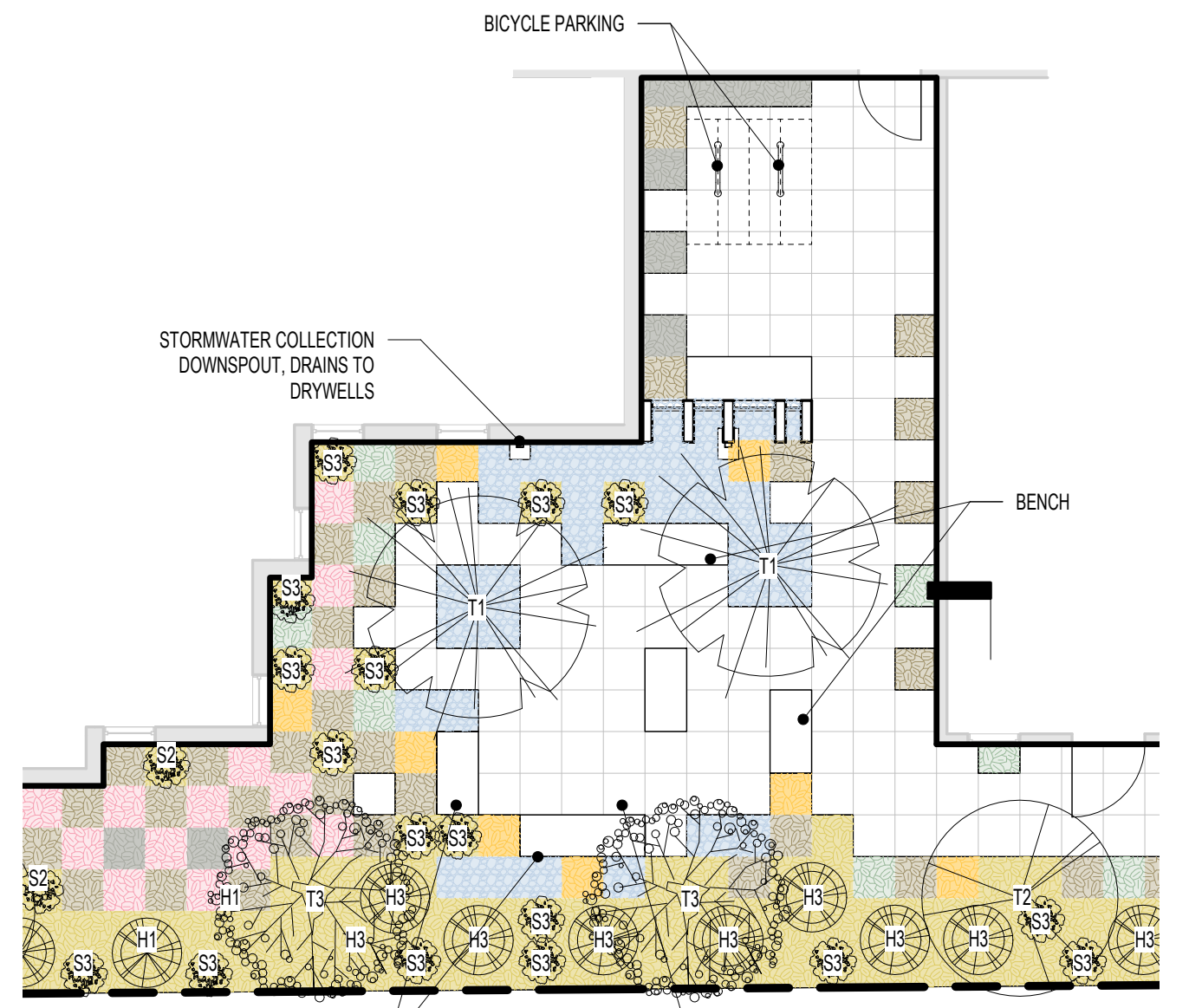


BLACK MONDO GRASS    GREEN MONDO GRASS    FOUNTAIN GRASS    TUFTED HAIRGRASS    NORTHWIND SWITCH GRASS    MAPLE MATSUMURAE    WHITE STAR MAGNOLIA

**SOUTH EGRESS COURT SECTION - TYPICAL WALKWAY**

1/4" = 1'-0"

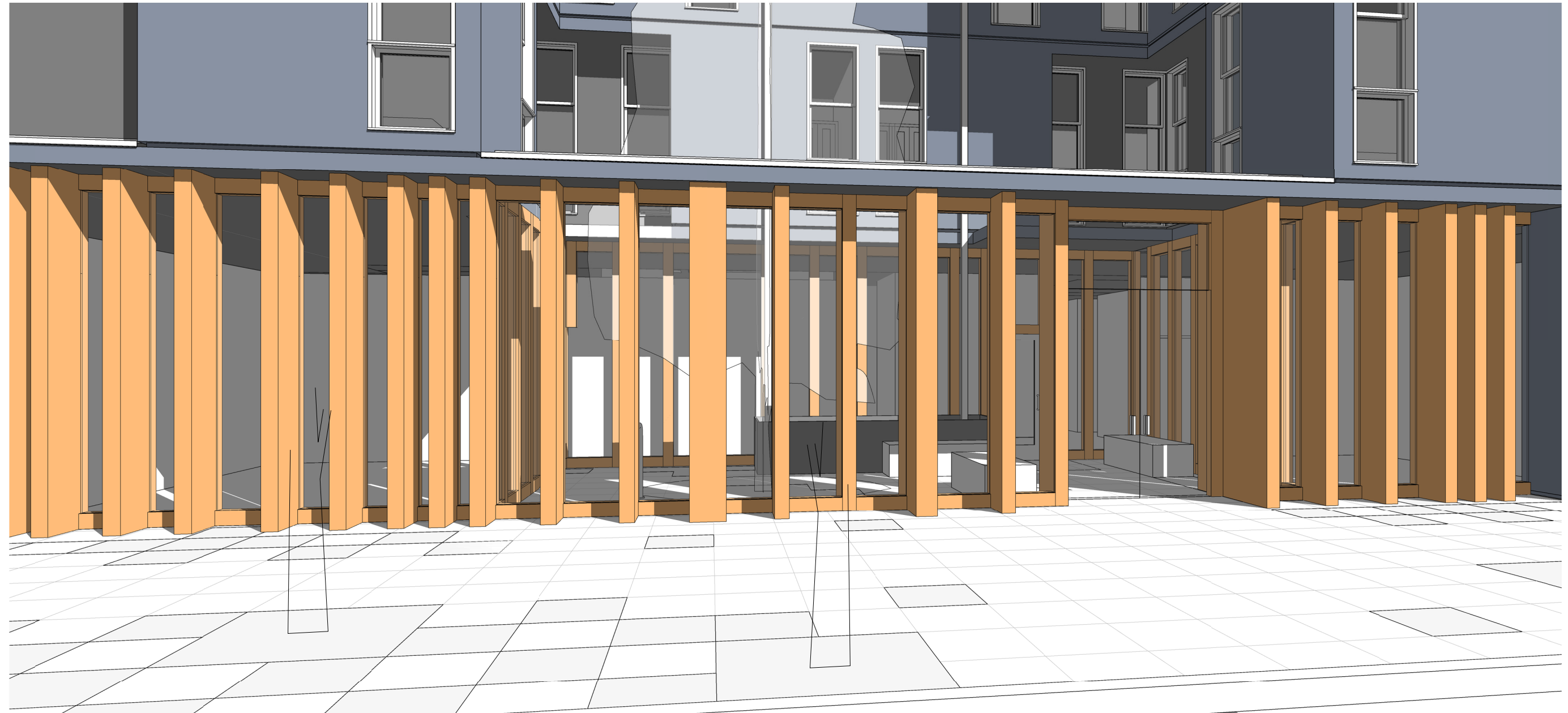
LANDSCAPING - STORMWATER GARDEN



- HARDSCAPE**
- RIVER ROCK
- TREES**
- T1 HERITAGE RIVER BIRCH
- T2 MAPLE MATSUMURAE
- T3 WHITE STAR MAGNOLIA
- SHRUBS**
- H1 CAMELLIA SASANQUA
- H3 NORTHWIND SWITCH GRASS
- S2 RED HUCKLEBERRY
- S3 TUFTED HAIRGRASS
- GROUNDCOVERS**
- BLACK MONDO GRASS
- GREEN MONDO GRASS
- NATIVE BUNCHBERRY
- SWORD FERN
- FOUNTAIN GRASS

**TYPICAL LANDSCAPING PLAN - SOUTH**

1/8" = 1'-0"



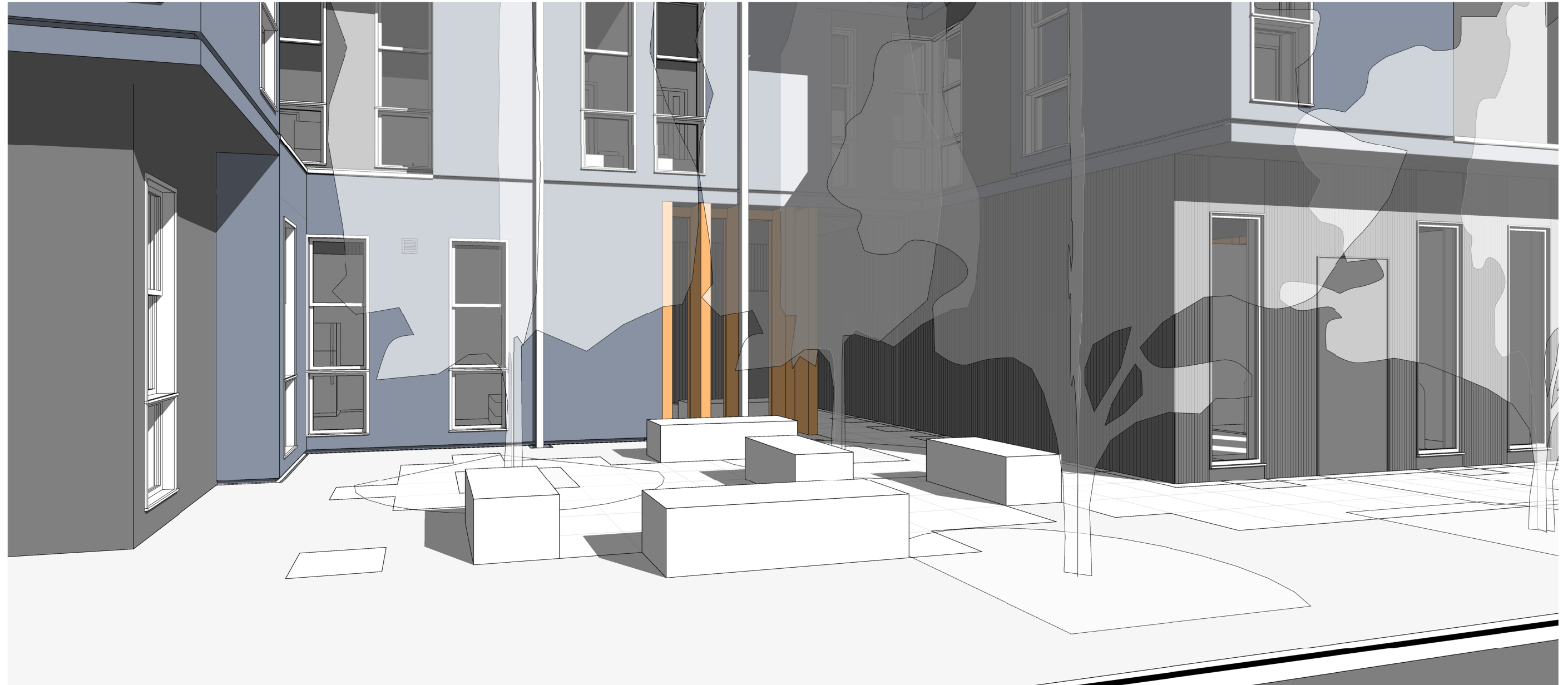
**PERSPECTIVE VIEW OF ENTRY COURTYARD**



**PERSPECTIVE VIEW OF STREET CORNER - JESSUP**



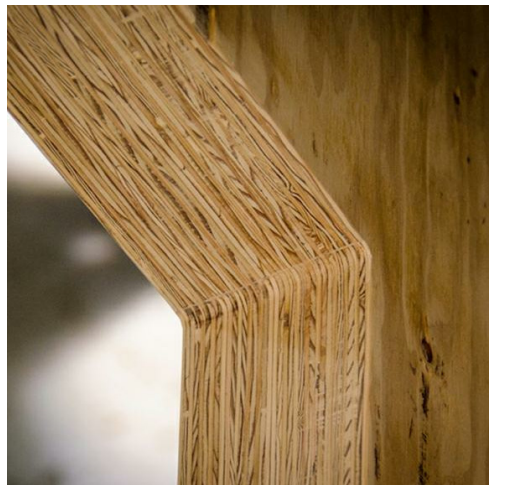
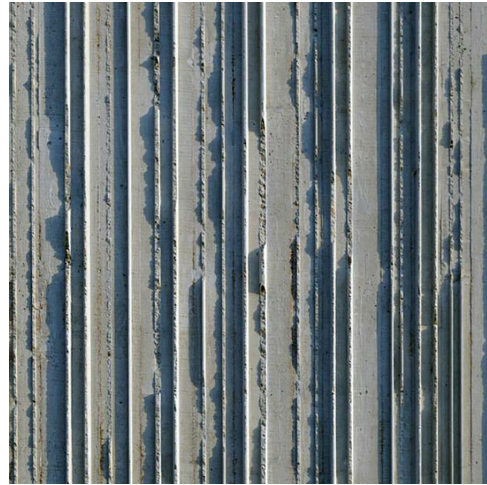
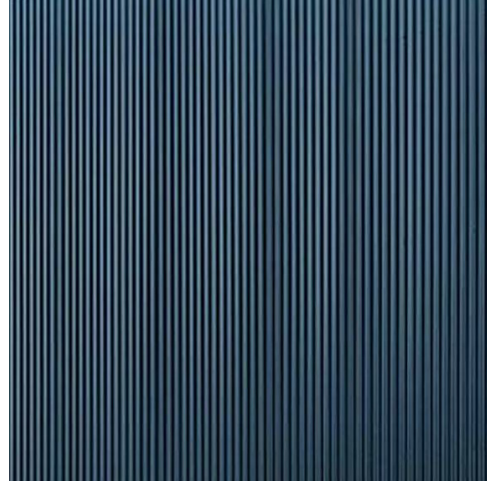
**PERSPECTIVE VIEW OF STREET CORNER - MINNESOTA**

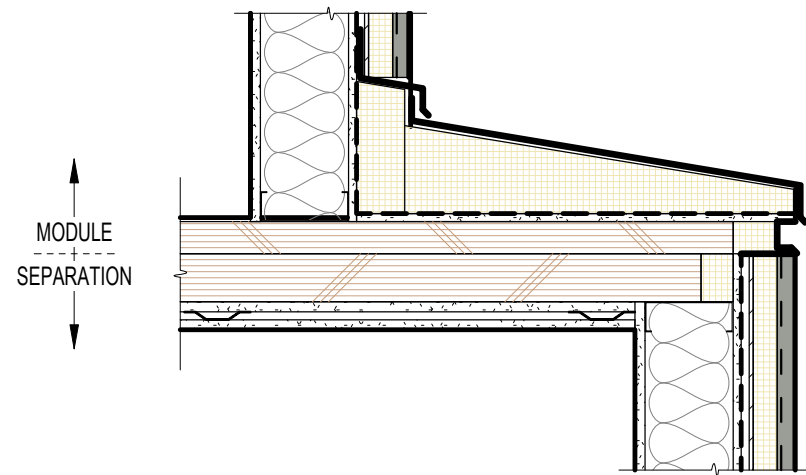


**PERSPECTIVE VIEW OF STORMWATER GARDEN**

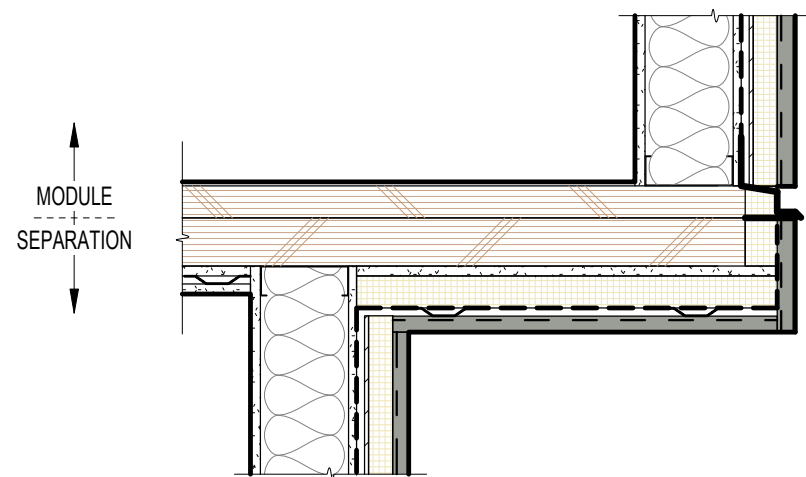


# QUALITY & PERMANENCE

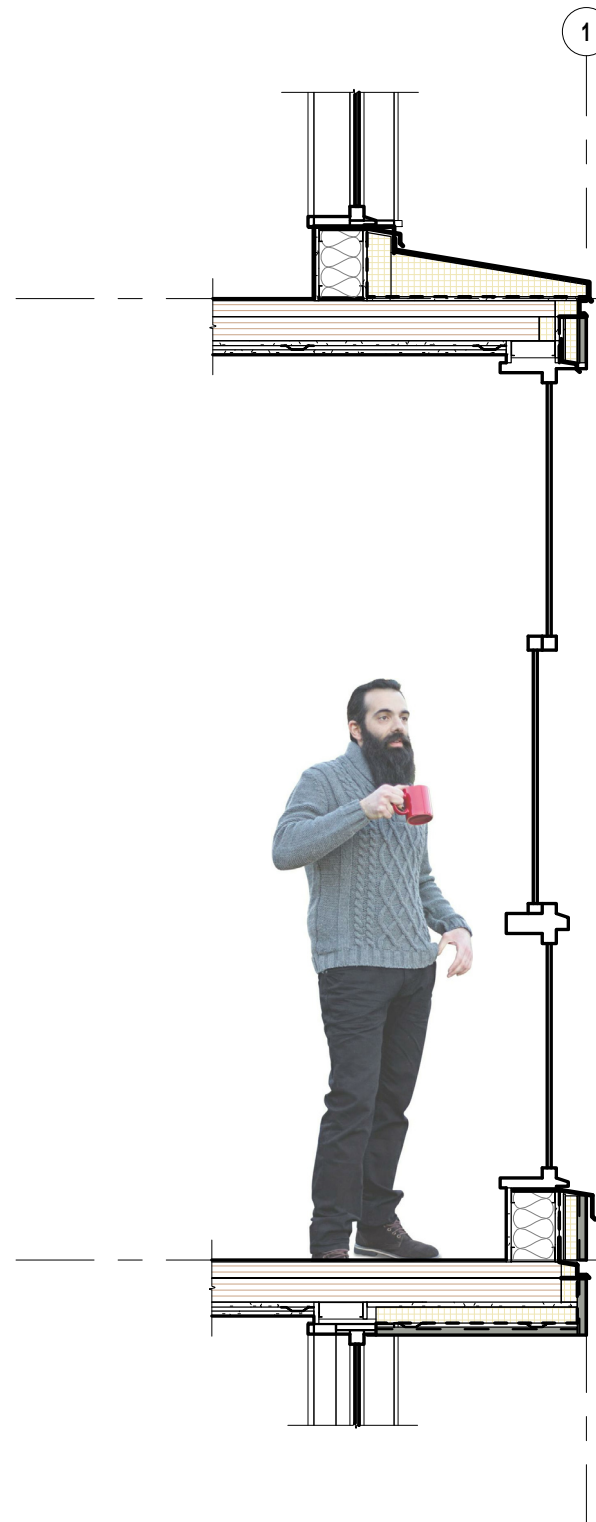




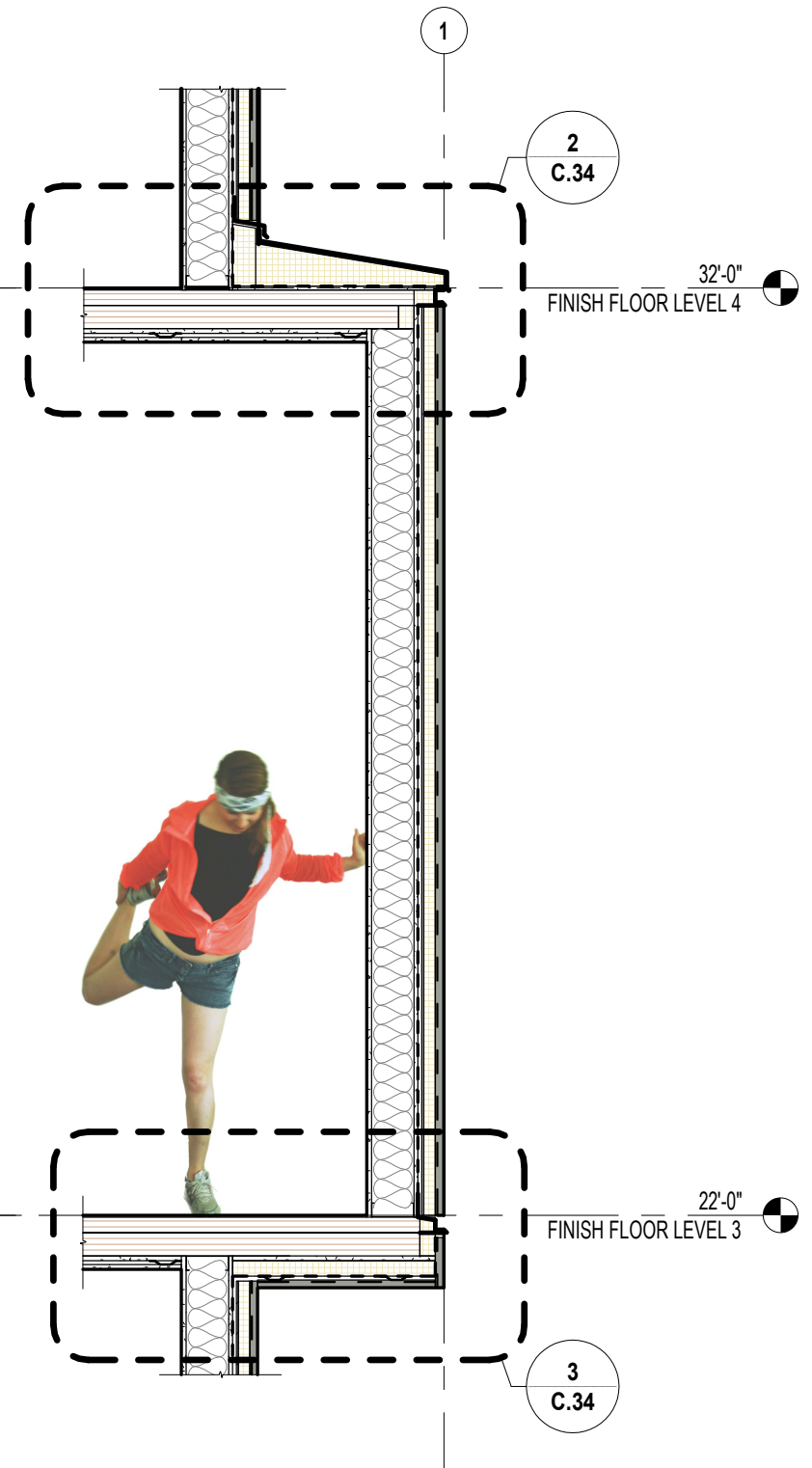
**SECTION DETAIL @ TYP. RECESS**  
1" = 1'-0"



**SECTION DETAIL @ TYP. SOFFIT**  
1" = 1'-0"



**WALL SECTION @ TYP. WINDOW**  
1/2" = 1'-0"



**WALL SECTION @ TYP. FLOOR**  
1/2" = 1'-0"

**CONTEXT - COMMUNITY DESIGN GUIDELINES**

**P1 COMMUNITY PLAN AREA CHARACTER**

A: Not applicable, outside of Killingsworth Station Concept Area.

B: Not applicable, no culturally significant buildings within the immediate vicinity of the site.

C: Not applicable, site is not at one of the specified gateways within the plan area.

D: Not applicable, no mature trees are on the site nor in the adjacent public ROW.

E: Not applicable, site is not on Montana Avenue.

F: *"In Neighborhood East, (1) creating a special developed edge along the Interstate 5 Freeway. (2) Orienting larger and/or taller buildings toward the freeway would allow these developments to (3) take advantage of views created by the freeway's open space while (4) creating a buffer for nearby residences and buildings. (5) Minnesota Avenue exists sporadically along the western edge of the freeway/sound wall, and offers a unique opportunity for access to parking areas, building loading, other service functions, and/or stormwater management facilities."*

(1) Proposal's primary, unbroken facade is oriented to the freeway.

(2) Proposal is taller than its adjacent neighbors farther from the freeway.

(3) Proposal's upper floor common areas take advantage of the views created by the freeway's open space.

(4) Proposal is designed to act as a buffer between the freeway and nearby residences.

(5) Proposal's bicycle parking areas, building loading, service functions, trash area, and utilities are oriented toward the sound wall.

G: Not applicable, site is not in Neighborhood West.

H: Not applicable, site is not on Interstate Avenue nor contains a neon sign.

**P2 HISTORIC AND CONSERVATION DISTRICTS**

Not applicable, site is not in a conservation or historic district.

**P3 GATEWAYS**

Not applicable, site is not in a designated district or city gateway.

**D7 BLENDING INTO THE NEIGHBORHOOD**

While the proposal is taller than its neighbors, the mass and proportions are broken down into smaller, more local scales. The building's overall mass is divided into 30 foot increments to the north and south, greatly reducing the bulk toward the immediate residential neighbors. Most of the existing structures in the area are similar in width. The building's facade is then further broken down into a more human scale grid of roughly 10 foot squares that are recessed two feet from each other. This articulation is proportional to a single story and to bay windows in nearby residential construction. The primary facade material is proposed to be either stucco or metal panel. Stucco has a great deal of history in the early stages of the neighborhood, whereas tight-corrugated metal panel is a contemporary material that requires less maintenance and is not prone to cracking. Both exterior siding options are proposed as a monolithic material, minimizing the need for expressed joints beyond that which are required for modular construction stacking.

## **PUBLIC REALM - COMMUNITY DESIGN GUIDELINES**

### **E1 PEDESTRIAN NETWORKS**

This proposal has three primary entrances. The main entrance is set back 18'-6" from the sidewalk in a hardscaped courtyard that faces Jessup Street. All courtyards, entries, pavers and concrete will be an elevated level of concrete specification. We propose to extend the concrete from the entrances to the right of way to enhance the pedestrian experience for residents and all citizens and visitors to this site. The entry courtyard is half-covered with a large MPP awning that will be partially exposed and finished. The secondary entrance faces the freeway soundwall across Minnesota Avenue. This entrance is recessed an additional 2'-0" from the primary ground floor facade for demarcation, which is in turn recessed 2'-0" from the primary building facade above. The third entrance is designed to be more private and is accessed only through a secure gate to the south of the property along Minnesota. A 6'-0" wide concrete path connects the sidewalk around the building to the rear entrance. The path will connect to the rear stormwater garden, the private counterpart to the more public entry courtyard, that incorporates individual poured concrete pavers.

### **E2 STOPPING PLACES**

In order to fully activate the primary entrance's courtyard and the rear stormwater garden, both will be provided with places to linger, sit and converse. These two outdoor areas will have differentiating main features yet will maintain a cohesive overall design language and shared material palette. The main entrance courtyard will have four cardinal focal points, a stormwater feature against the lobby windows, a shade tree, and two linear benches flanking the sides. The rear stormwater garden will have a much larger stormwater feature that includes swales and rain gardens. Several benches will be dispersed around a central gathering point at the center of the stormwater garden.

### **E3 THE SIDEWALK LEVEL OF BUILDINGS**

The primary exterior material will be stucco or metal panels with puncture windows, which will be installed in a prefab facility before each module arrives at the site. The non-street facing ground floor will house residential units and therefore will maintain the stucco or metal panel and puncture window motif of the floors above. The non-residential portions of the ground floor will be delineated with a different material or color palette and a continuous two foot setback except at the entry points. Two bookending moments occur at the transitions from the street-facing treatments to the more residential material language in the form of the a single grid block touching the ground where the building is about to turn away from the public realm. The main entry courtyard is proposed to have large windows directly linking the building's lobby to the exterior, and will have a prominent stormwater feature, bicycle parking, seating and an exposed MPP awning. The Minnesota entrance will be recessed a further two feet, and will be aligned with the grid pattern above. The corner office and both main entrances will have aluminum storefronts that will distinguish them from the rest of the ground floor functions.

### **E4 CORNERS THAT BUILD ACTIVE INTERSECTIONS**

The street corner of N. Jessup and Minnesota is proposed as the building's only non-residential use, a small office or retail space. The space will have a strong presence with two walls of floor-to-ceiling glazing that contrast with the more reserved concrete and puncture window elements at either side. The storefront system is intentionally extended beyond the strict limitations of the grid pattern above for further emphasis. The paved portions of the sidewalks are proposed to widen into the landscaped areas at the main entrances and the street corner to further emphasize these active spaces. The entrance door to this space is proposed to open onto the more dynamic Jessup street frontage.

### **E5 LIGHT, WIND AND RAIN**

All three primary entrances will be protected by either awnings or building recesses. The main entrance's awning is proposed as a deep Mass Plywood Panel (MPP) that will cover the entry door, two of three benches and the short-term bicycle parking. The Minnesota entrance will be recessed four feet from the floor above, and the rear entrance to the south will have a dramatic 15'-6" deep recess opening up to a 28'-6" deep stormwater garden courtyard. A selection of shade trees provide further protection around the perimeter of the building and in both courtyards.

### **D1 OUTDOOR AREAS**

This proposal has two primary ground floor outdoor areas. The main entry courtyard on the North of the property that directly connects with the Jessup Street sidewalk, and a south-facing courtyard that is accessed through a secure gate from Minnesota. The landscaped setback around the remainder of the building is not accessible for privacy reasons. Both the entry and rear courtyards are surfaced with an enhanced finish level of concrete and permeable landscaping. The entry courtyard is designed as an active space with seating, bicycle parking, and a stormwater feature. The benches and stormwater collection pool are placed in the cardinal directions from the center of the courtyard, forming focal points and cross views across the partially covered space. A large awning, made of the MPP, covers the main entrance, front gate and immediate vicinity for weather protection. A similar congregation space is found at the southern courtyard where the building itself acts as the rain and sun cover. A 6'-0" wide path connects the sidewalk around the building to the rear entrance and the primary entrances to both bicycle parking areas. The southern path also connects to the stormwater garden, an amenity space for the tenants with additional places to sit and mingle.

### **D2 MAIN ENTRANCES**

The primary entrance to the residences is located in the aforementioned northern entry courtyard, an area that is 30 feet wide and more than 18 feet deep. This setback accentuates the main entrance, and does so in a familiar way, similar to apartment building's of the late 19th and early 20th centuries. The awning canopies, which cover a third of the courtyard, provides year around weather protection for those coming and going from the building. The underside of the canopy will be well lit throughout the winter, further delineating the entrance from the surrounding north-facing facade.

### **D3 LANDSCAPE FEATURES**

Both courtyards will feature in-ground landscaping and custom benches. The perimeter of the building along the sidewalk will be planted with native low shrubs and groundcovers in a repeating grid pattern, which will turn into the main entry court in order to direct pedestrians inside. The courtyards will both house stormwater features; a collection pool to the north, and swales and rain gardens to the south. The non-street facing perimeter will be planted with a mix of small trees, large shrubs and groundcovers that will provide a privacy screen for the ground floor residences and the project's neighbors.

### **D4 PARKING AREAS AND GARAGES**

The proposal includes two types of parking areas, bicycle parking and a loading dock, both of which are oriented toward the freeway soundwall across Minnesota Avenue. Longer term bicycle parking will be found in the basement, easily accessed from all ground floor entrances. Medium term bicycle parking is located to the rear of the building, accessed through the secure gate along the southern pathway. Short term bicycle parking is located directly adjacent to the front and back entrances, and both are protected from weather directly overhead. The loading dock's overhead door will be composed of solid metal, painted to match the adjacent wall in order to minimize the large opening's appearance when closed.

### **D5 CRIME PREVENTION**

The majority of the ground floor perimeter will have at least one window every 10 feet, and most of those windows face into an active environment. The three entrances are dispersed evenly around the building, reducing the number of dead zones in the pedestrian realm. The future office or retail space at the corner provides eyes on both streets. Ground floor lighting will illuminate all accessible building recesses and along the southern path to the rear court. Both courtyards will be accessed through a security gate, and the landscaped area along the west of the property will be fully fenced in as well.

**QUALITY & PERMANENCE - COMMUNITY DESIGN GUIDELINES**

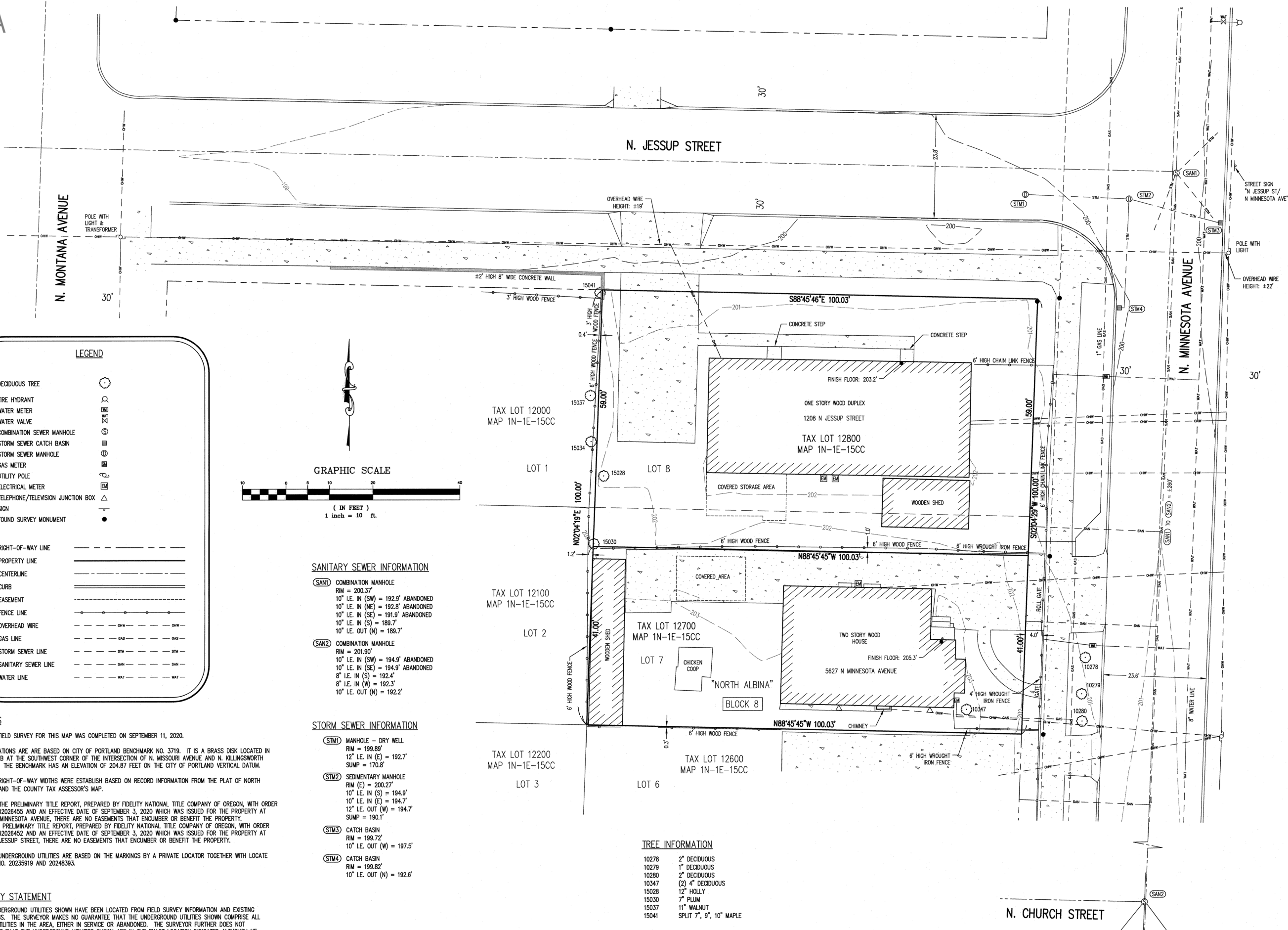
D6 ARCHITECTURAL INTEGRITY

Not applicable, not an addition or alteration.

D8 INTEREST, QUALITY AND COMPOSITION

Like great works of art, quality architecture is rooted in conceptual rigor and a set of rules. The rigor is then made more interesting and human by the playful breaking of those rules. This proposal has a strong, simple form that is brought to the vernacular and human scale with one of the most basic of regular patterns. The pattern is then eroded by locating community rooms along view corridors and the freeway. The primary building material is proposed to be a three-coat stucco or metal panel, long-lasting and durable materials that will be finished in the controlled environment of the fabrication shop. The primary structure will utilize Mass Plywood Panels (MPP), which will reduce the building's carbon footprint and construction waste.

# EXHIBIT A

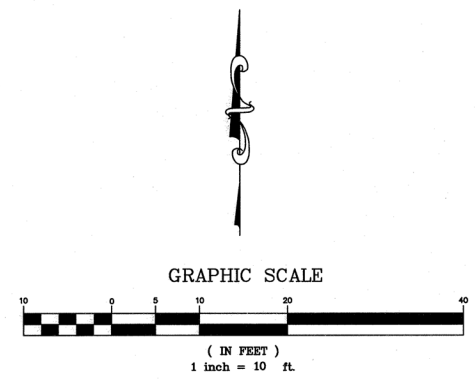


**LEGEND**

DECIDUOUS TREE	
FIRE HYDRANT	
WATER METER	
WATER VALVE	
COMBINATION SEWER MANHOLE	
STORM SEWER CATCH BASIN	
STORM SEWER MANHOLE	
GAS METER	
UTILITY POLE	
ELECTRICAL METER	
TELEPHONE/TELEVISION JUNCTION BOX	
SIGN	
FOUND SURVEY MONUMENT	

RIGHT-OF-WAY LINE	
PROPERTY LINE	
CENTERLINE	
CURB	
EASEMENT	
FENCE LINE	
OVERHEAD WIRE	
GAS LINE	
STORM SEWER LINE	
SANITARY SEWER LINE	
WATER LINE	



**SANITARY SEWER INFORMATION**

- (SAN1) COMBINATION MANHOLE  
 RIM = 200.37'  
 10" I.E. IN (SW) = 192.9' ABANDONED  
 10" I.E. IN (NE) = 192.8' ABANDONED  
 10" I.E. IN (SE) = 191.9' ABANDONED  
 10" I.E. IN (S) = 189.7'  
 10" I.E. OUT (N) = 189.7'
- (SAN2) COMBINATION MANHOLE  
 RIM = 201.90'  
 10" I.E. IN (SW) = 194.9' ABANDONED  
 10" I.E. IN (SE) = 194.9' ABANDONED  
 8" I.E. IN (S) = 192.4'  
 8" I.E. IN (W) = 192.3'  
 10" I.E. OUT (N) = 192.2'

**STORM SEWER INFORMATION**

- (STM1) MANHOLE - DRY WELL  
 RIM = 199.89'  
 12" I.E. IN (E) = 192.7'  
 SUMP = 170.8'
- (STM2) SEDIMENTARY MANHOLE  
 RIM (E) = 200.27'  
 10" I.E. IN (S) = 194.9'  
 10" I.E. IN (E) = 194.7'  
 12" I.E. OUT (W) = 194.7'  
 SUMP = 190.1'
- (STM3) CATCH BASIN  
 RIM = 199.72'  
 10" I.E. OUT (W) = 197.5'
- (STM4) CATCH BASIN  
 RIM = 199.82'  
 10" I.E. OUT (N) = 192.6'

**TREE INFORMATION**

- 10278 2" DECIDUOUS  
 10279 1" DECIDUOUS  
 10280 2" DECIDUOUS  
 10347 (2) 4" DECIDUOUS  
 15028 12" HOLLY  
 15030 7" PLUM  
 15037 11" WALNUT  
 15041 SPLIT 7", 9", 10" MAPLE

- NOTES**
- THE FIELD SURVEY FOR THIS MAP WAS COMPLETED ON SEPTEMBER 11, 2020.
  - ELEVATIONS ARE BASED ON CITY OF PORTLAND BENCHMARK NO. 3719. IT IS A BRASS DISK LOCATED IN THE CURB AT THE SOUTHWEST CORNER OF THE INTERSECTION OF N. MISSOURI AVENUE AND N. KILLINGSWORTH STREET. THE BENCHMARK HAS AN ELEVATION OF 204.87 FEET ON THE CITY OF PORTLAND VERTICAL DATUM.
  - THE RIGHT-OF-WAY WIDTHS WERE ESTABLISHED BASED ON RECORD INFORMATION FROM THE PLAT OF NORTH ALBINA AND THE COUNTY TAX ASSESSOR'S MAP.
  - PER THE PRELIMINARY TITLE REPORT, PREPARED BY FIDELITY NATIONAL TITLE COMPANY OF OREGON, WITH ORDER NO. 45142026455 AND AN EFFECTIVE DATE OF SEPTEMBER 3, 2020 WHICH WAS ISSUED FOR THE PROPERTY AT 5627 N MINNESOTA AVENUE, THERE ARE NO EASEMENTS THAT ENCUMBER OR BENEFIT THE PROPERTY.
  - PER THE PRELIMINARY TITLE REPORT, PREPARED BY FIDELITY NATIONAL TITLE COMPANY OF OREGON, WITH ORDER NO. 45142026452 AND AN EFFECTIVE DATE OF SEPTEMBER 3, 2020 WHICH WAS ISSUED FOR THE PROPERTY AT 1208 N JESSUP STREET, THERE ARE NO EASEMENTS THAT ENCUMBER OR BENEFIT THE PROPERTY.
  - THE UNDERGROUND UTILITIES ARE BASED ON THE MARKINGS BY A PRIVATE LOCATOR TOGETHER WITH LOCATE TICKET NO. 20235919 AND 20248393.

**UTILITY STATEMENT**

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.

**ORTHWEST SURVEYING, INC.**  
 1815 NW 168th PLACE, SUITE 2090  
 BEAVERTON, OR 97006  
 PH: (503) 848-2127 FAX: (503) 848-2179  
 EMAIL: nwsurveying@verizon.com

LOTS 7 AND 8, BLOCK 8, NORTH ALBINA, LOCATED IN THE SOUTHWEST ONE-QUARTER OF SECTION 15, TOWNSHIP 1 NORTH, RANGE 1 EAST, W.M., CITY OF PORTLAND, MULTNOMAH COUNTY, OREGON

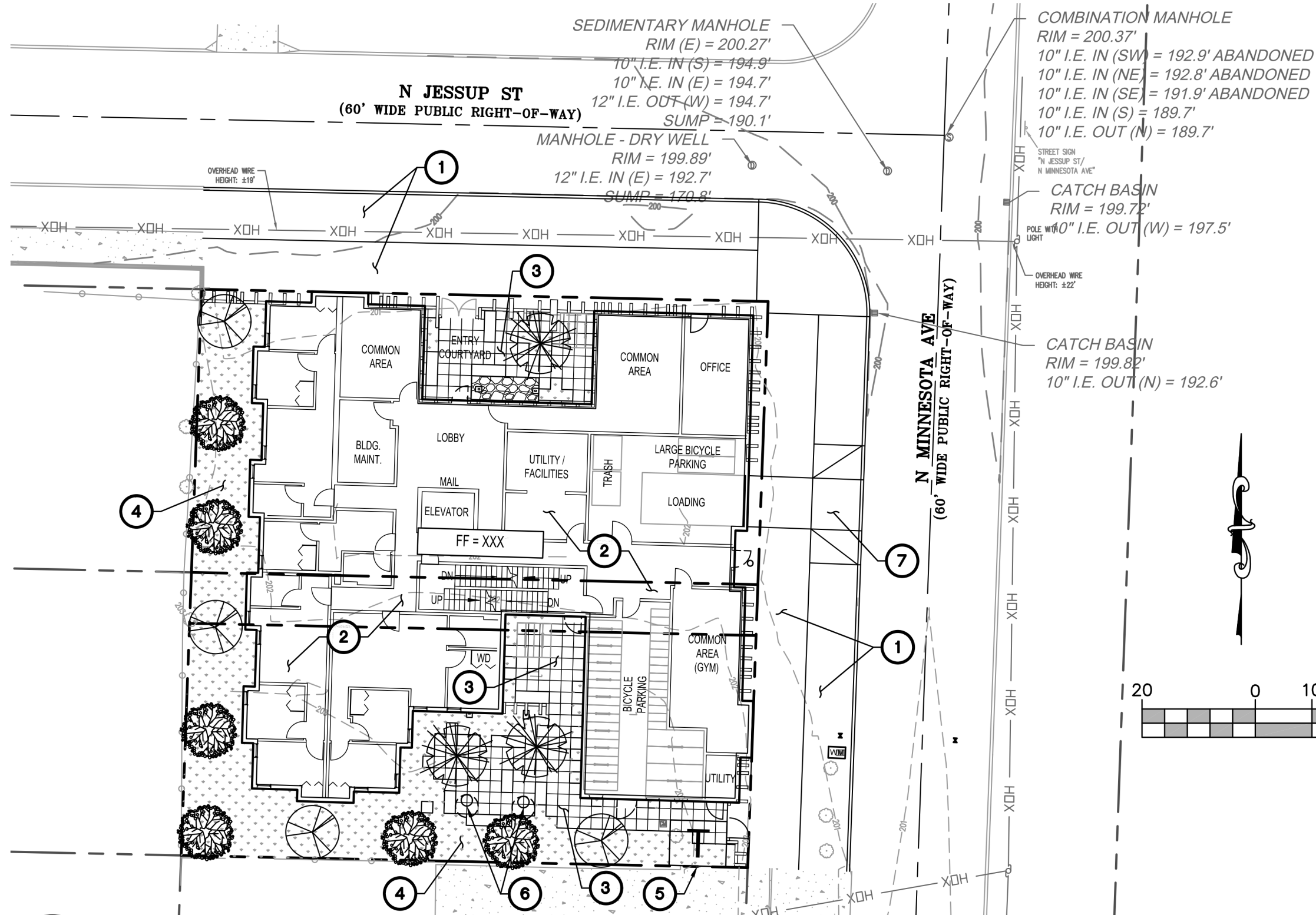
**TOPOGRAPHIC SURVEY**  
**PORTLAND, OREGON**

DRAWING NO.: 2159 TOPO  
 SCALE: AS NOTED  
 DRAWING GENERATED BY: LD2004  
 DRAWN BY: BJA  
 CHECKED BY: SF  
**PREPARED FOR:**  
 FOX SIGNATURE PROPERTIES, LLC  
 12449 NW SPRINGVILLE ROAD  
 PORTLAND, OR 97229

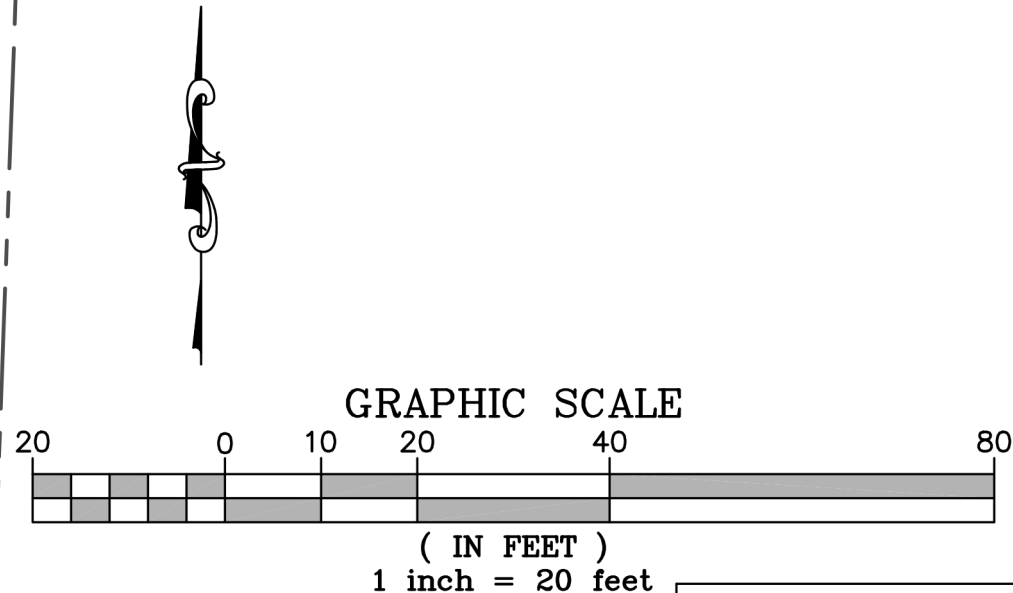
**REVISIONS:**  
 INITIAL RELEASE: SEPT. 22, 2020

REGISTERED PROFESSIONAL LAND SURVEYOR  
  
 OREGON  
 JUNE 30, 1997  
 SCOTT F. FIELD  
 2844  
 RENEWS: 12-31-2021

JOB NUMBER  
**2159**  
 SHEET  
**1 OF 1**



KEYNOTES FOR THIS SHEET	
MARK	DESCRIPTION
1	NEW PUBLIC IMPROVEMENTS UNDER SEPARATE PERMIT, TYP.
2	NEW BLDG, TYP.
3	NEW SITE WALKS AND PATIOS, TYP.
4	NEW LANDSCAPING, TYP.
5	NEW TRANSFORMER.
6	(2) NEW DRYWELLS.
7	NEW DRIVEWAY CURB CUT FOR LOADING ZONE.

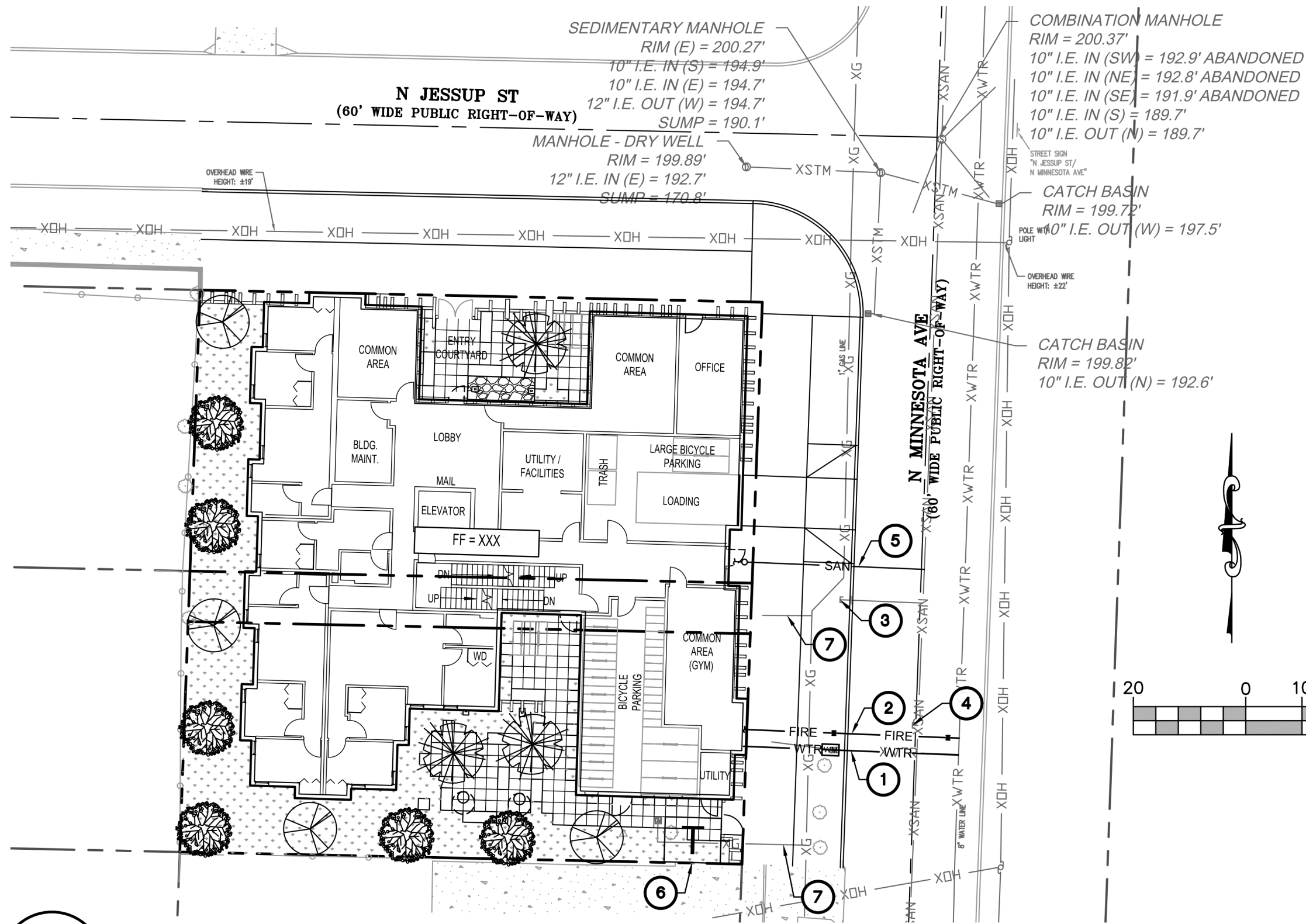


**1** **EROSION & SEDIMENT CONTROL & GRADING PLAN**  
**C2.0** SCALE: 1" = 20'-0"

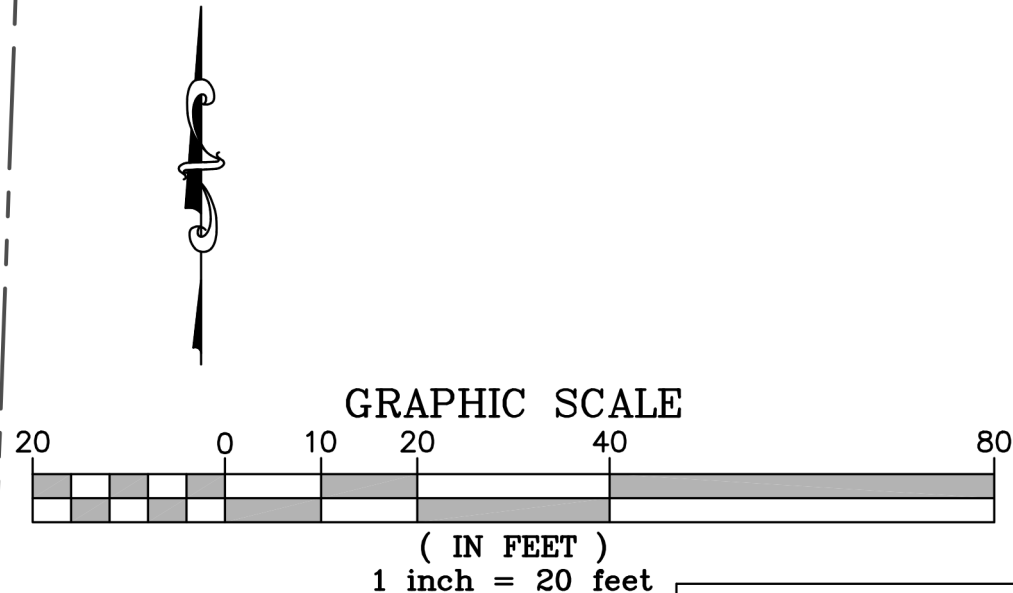
REGISTERED PROFESSIONAL ENGINEER  
**PRELIMINARY**  
 NOT FOR CONSTRUCTION  
 JUNE 30, 2016  
 J. DESLAURIERE

RENEWS: 12-31-2021

REVISIONS	EROSION & SEDIMENT CONTROL & GRADING PLAN SHEET	
	DATE MM/DD/YY	DRWN. ALM
	JOB NO. 20158	CHKD. CJD



KEYNOTES FOR THIS SHEET	
MARK	DESCRIPTION
1	PWB TO KILL EXISTING AND INSTALL NEW DOM WATER.
2	PWB TO INSTALL NEW FIRE SERVICE.
3	CAP EXIST SEWER AT BACK OF CURB.
4	CAP EXIST SEWER AT MAIN.
5	NEW PAD MOUNTED TRANSFORMER WITH UG POWER FROM EAST SIDE OF MINNESOTA AVE.
6	NEW GAS OR REUSE EXISTING.



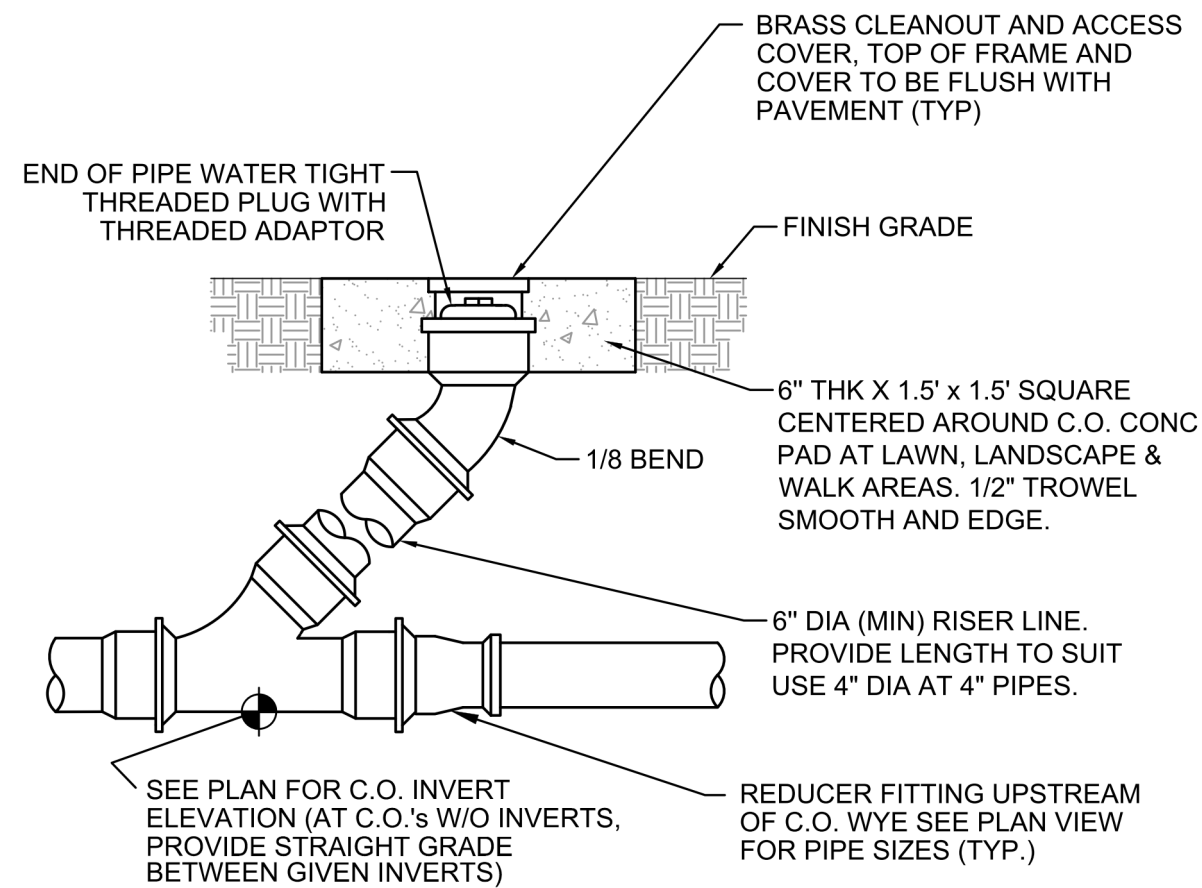
**1 UTILITY PLAN**  
**C2.1**  
SCALE: 1" = 20'-0"

REGISTERED PROFESSIONAL ENGINEER  
PRELIMINARY NOT FOR CONSTRUCTION  
80058PE  
JUNE 30, 2016  
J. DESLAURIER  
RENEWALS: 12-31-2021

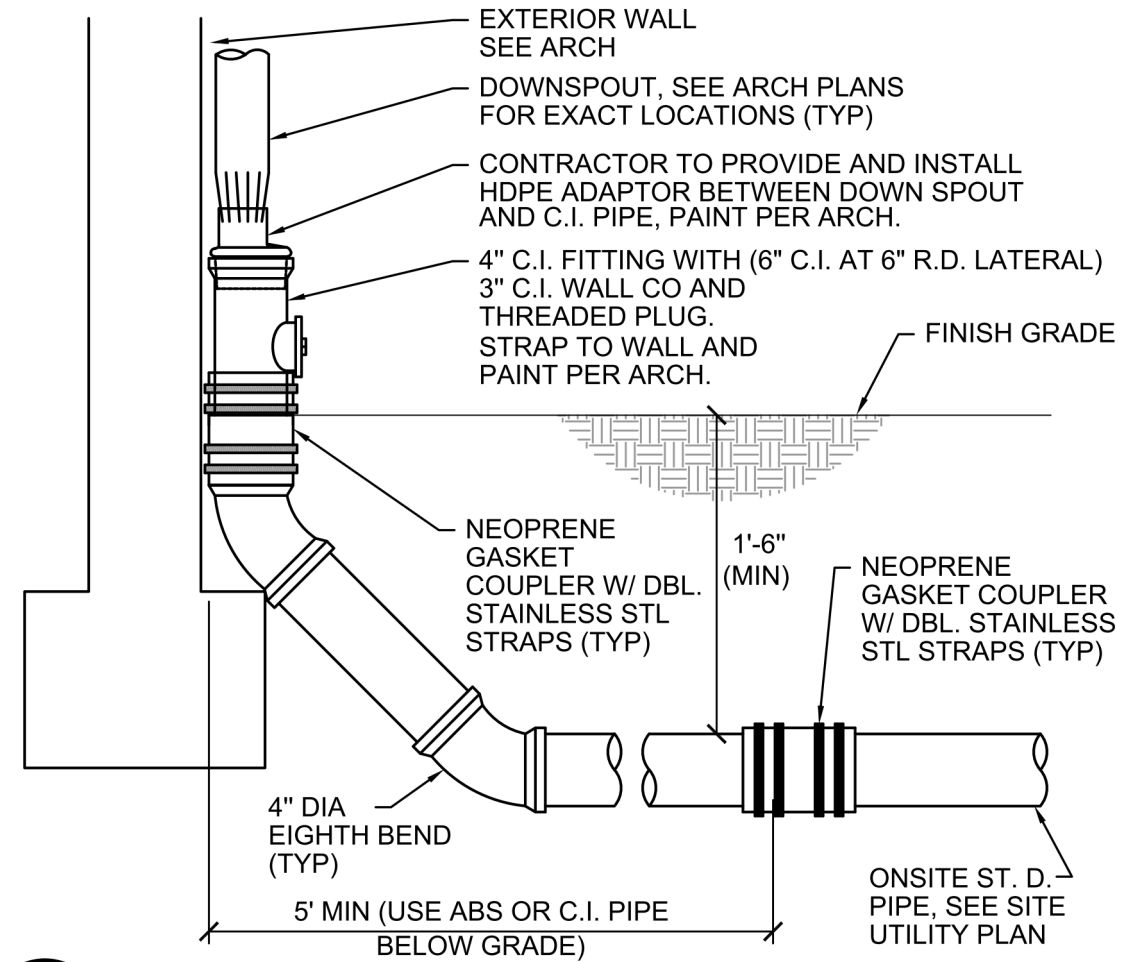
REVISIONS	DATE	MM/DD/YY	DRWN.	ALM

UTILITY PLAN  
CHKD. CJD

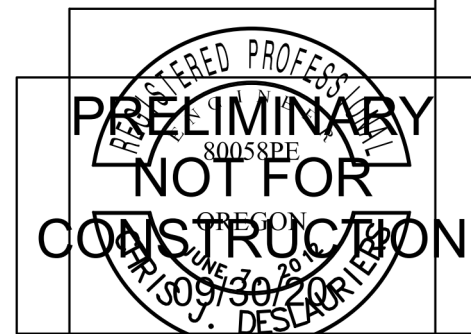




**1**  
**C3.0** **CLEANOUT AT WALKS, SLABS AND PLANTERS**  
1" = 1'-0"



**2**  
**C3.0** **TYPICAL EXTERIOR DOWN SPOUT CONNECTION**  
N.T.S.



RENEWS: 12-31-2021



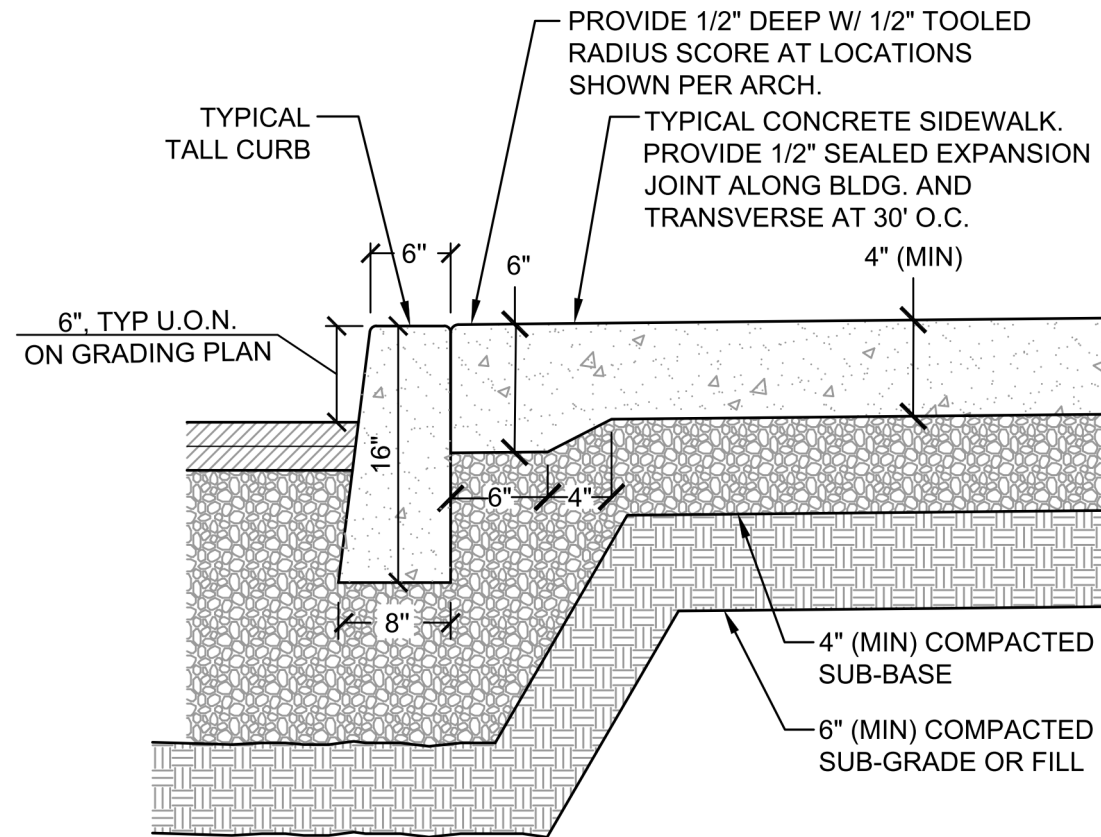
Structural · Civil Engineers

6443 SW Beaverton-Hillsdale Hwy, suite 210 Portland, OR 97221 ph:503.203.8111 fx:503.203.8122 www.wdyi.com

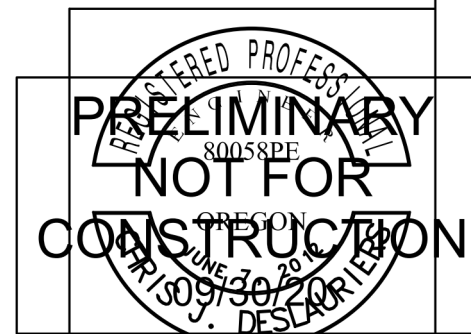
**MINNESOTA + JESSUP**  
1208 N JESSUP + 5627 N MINNESOTA  
PORTLAND, OR 97217

REVISIONS	CIVIL DETAILS	
	DATE	MM/DD/YY
	DRWN.	ALM
	JOB NO.	20158
	CHKD.	CJD

SHEET  
**C3.0**



1  
C3.1  
TYPICAL CONCRETE WALK  
N.T.S.



RENEWS: 12-31-2021



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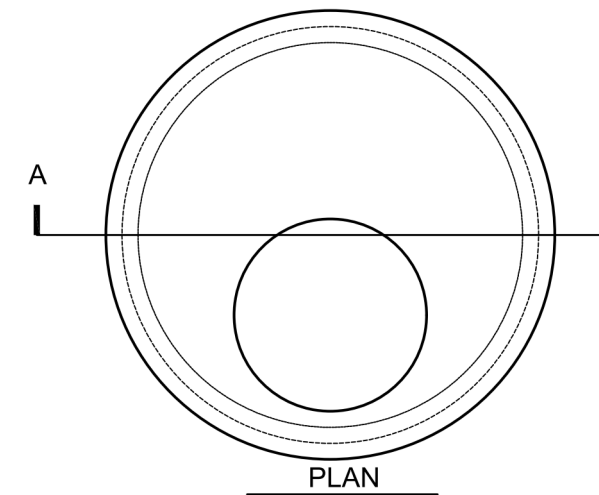
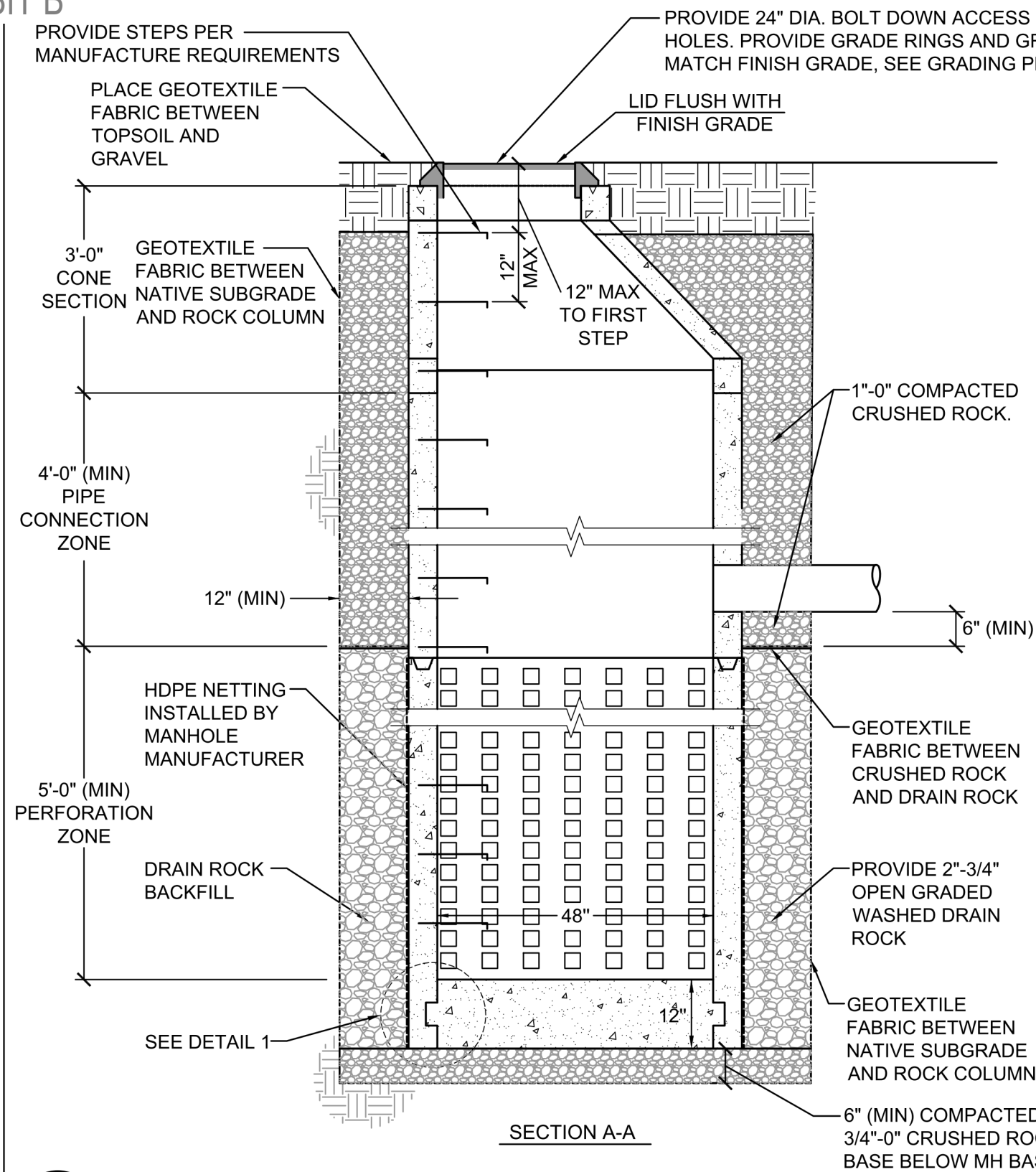
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**C3.1**

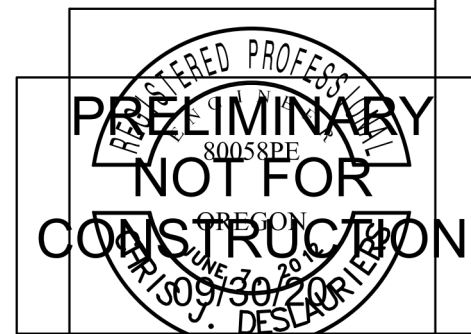
# EXHIBIT B



### NOTES:

1. ALL PRECAST SECTIONS SHALL CONFORM TO THE REQUIREMENTS OF THE CITY OF PORTLAND MANUFACTURING STANDARDS FOR PRECAST CONCRETE PRODUCTS (MSPCP), AS REVISED.
2. PROVIDE 6 INCHES CLEAN CRUSHED ROCK UNDER ALL CONNECTING PIPE.
3. DO NOT CONNECT PIPE TO ANY PERFORATED SECTION. PROVIDE 5' MIN TO PERFORATIONS BELOW FINISH SLAB ELEVATION MIN.
4. CAST-IN-PLACE CONCRETE SHALL BE COMMERCIAL GRADE. FORM BASE TO BE LEVEL AND SMOOTH. A PRECAST CONCRETE BASE MAY BE SUBSTITUTED FOR THE BASE SHOWN.
5. PROVIDE A FLEXIBLE JOINT FOR ALL CONNECTING PIPES:
  - RIGID PIPE < 36 INCHES - 18 INCHES (MAX.) FROM OUTSIDE WALL
  - FLEXIBLE PIPE - 18 INCHES (MAX.) FROM THE OUTSIDE WALL UNLESS A FLEXIBLE JOINT FITTING IS INSTALLED AND ACCEPTED.
6. PROVIDE 6 INCHES (MIN.) OF SEPARATION BETWEEN A SECTION JOINT AND THE OUTER EDGE OF ANY OPENING.

**1**  
**C3.2** PRECAST DRYWELL MANHOLE  
N.T.S.



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SHEET

**C3.2**

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