

PORTLAND PROPER HOTEL



TABLE OF CONTENTS

C.1

Project Data

C.2

Central City Design Guidelines

C.5

City Context

C.11

Plans

C.18

Elevations

C.19

Materials

C.20

Renderings

Appendix

A.2

FAR Calculations

PROJECT DATA

Site Location: NW Irving St between NW 12th Ave and NW 13th Ave

Site Area: 19,990 Square Feet

Address: 1202 NW Irving St, Portland OR 97209

Site Property ID: R140734

Adjacent Property Under Separate Ownership: Maddox Building - PropID: R140736, Pearl Parking- PropID: R140732

Zoning: EXd

Neighborhood: Pearl District

Pre-Application Conference: November 20, 2018 - EA 18-256752

APPLICATION REQUEST SUMMARY

Design Advice Request for a new approximately 170,000 GSF, 9-story full-service hotel building including ground floor lobby and amenity spaces, rooftop amenities, and one level of below-ground valet parking. The proposed building is located in the Pearl District on Irving Street between 12th and 13th Avenues. Rainwater from the eco roofs will be routed directly to a new 10-inch storm line connecting to a 21-inch combined sewer in NW Irving St. Currently the project achieves 60% eco roof coverage. If at least 60% eco roof coverage is not possible due to equipment area coverage as the design progresses, then the remaining non-eco roof areas will be captured, treated and are subject to flow control before leaving the site.

The Applicant requests Design Commission consideration of:

1. Proposed building form, massing design coherency, materials and response to context.
2. Loading strategy; Type A loading bay #1 located inside building off 12th Avenue and bay #2 located adjacent historic loading dock on 13th Avenue.

a. Adjustment will be filed to utilize public ROW for second loading bay.

b. Loading Demand study is underway.
3. Bicycle strategy - short term bike parking in the public right-of-way.
4. Valet Parking strategy - utilize public right of way as curb drop-off/pick-up point. No public access to garage.
5. Project anticipates necessity for the following:

a. Driveway Design Exception (PBOT) for overhead doors (loading and parking garage) located at property line.

b. Utility Vault Exception (PBOT) for vault in public right of way on Irving.

c. Below-grade Parking Encroachment Permit on Irving Street, 13th Ave and 12th Ave.

d. Bike Rack Encroachment Permit on Irving Street.

e. Historic Loading Dock Encroachment Permit on 12th Street.

f. Hotel Zone Permit on Irving Street.

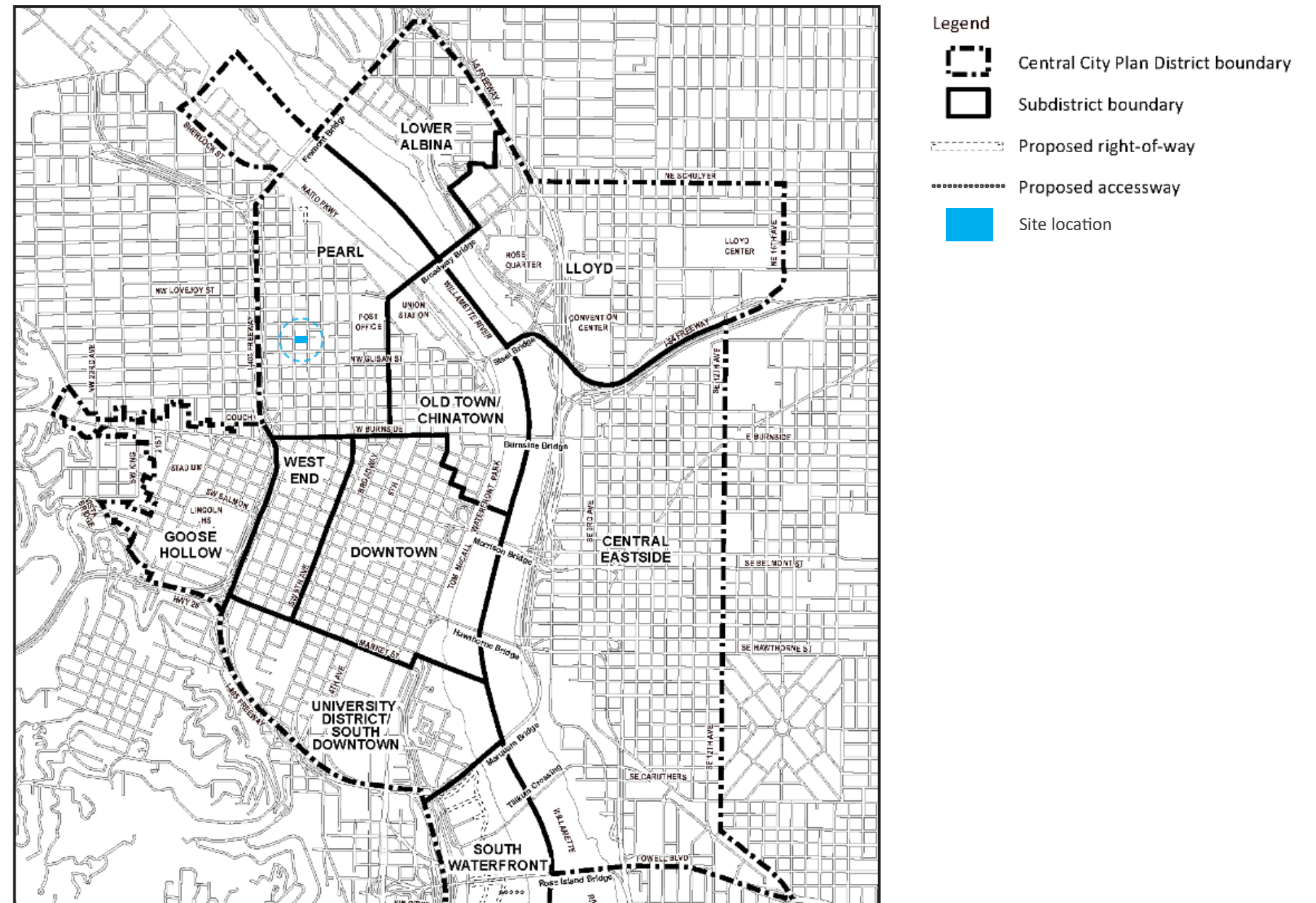
g. Rolling Valet Station Sign Permit on Irving Street.

h. Building Signage Permit

i. Loading Adjustment / Loading Demand Study

CENTRAL CITY FUNDAMENTAL DESIGN GUIDELINES

- | | |
|-----|---|
| A1 | Integrate the River |
| A2 | Emphasize Portland Themes |
| A3 | Respect the Portland Block Structures |
| A4 | Use Unifying Elements |
| A5 | Enhance, Embellish and Identify Areas |
| A6 | Reuse / Rehabilitate / Restore Buildings |
| A7 | Establish and Maintain a Sense of Urban Enclosure |
| A8 | Contribute to a Vibrant Streetscape |
| A9 | Strengthen Gateways |
| | |
| B1 | Reinforce and Enhance the Pedestrian System |
| B2 | Protect the Pedestrian |
| B3 | Bridge Pedestrian Obstacles |
| B4 | Provide Stopping and Viewing Places |
| B5 | Make Plaza Parks and Open Space Successful |
| B6 | Develop Weather Protection |
| B7 | Integrate Barrier-Free Design |
| | |
| C1 | Enhance View Opportunities |
| C2 | Promote Quality and Permanence in Development |
| C3 | Respect Architectural Integrity |
| C4 | Complement the Context of Existing Buildings |
| C5 | Design for Coherency |
| C6 | Develop Transitions Between Buildings and Public Spaces |
| C7 | Design Corners that Build Active Intersections |
| C8 | Differentiate the Sidewalk-Level of Buildings |
| C9 | Develop Flexible Sidewalk-Level Spaces |
| C10 | Integrate Encroachments |
| C11 | Integrate Roofs and Use Rooftops |
| C12 | Integrate Exterior Lighting |
| C13 | Integrate Signs |



BASE EMPLOYMENT
ZONE &
CENTRAL CITY
PLAN STANDARDS

DOCUMENTS REFERENCED: PORTLAND ZONING CODE TITLE 33 - MARCH 1, 2019, CENTRAL CITY 2035 PLAN VOLUME 2A - JANUARY 9, 2018

The site is zoned EXd (Employment with a Design “d” overlay in the River District). Applicable development standards are addressed below including standards of the Base Zone, Central City Plan, and River District.. The site is located within the Central City District following Map 510-1.

REQUIREMENT

PROPOSAL

33.140.100: Primary Uses	As defined in Table 140-1, zone allows mixed uses: high-density commercial, light industrial, institutional and residential uses.	A commercial-use building consisting of entertainment-oriented services including: hotel, events spaces, fitness, bars, and restaurant spaces is allowed within this Employment Zone.
33.614.100: Lot Size	Each lot must have a front lot line that is at least 10 feet long. There are no other required minimum lot dimensions.	The proposed building is to be built out to the lot lines on all street frontages.
33.510.200: Floor Area Ratio	The Base FAR is 5:1, as defined on Map 510-2, with a Maximum Bonus FAR of 3:1, there is no limit on the amount of FAR that can be transferred to the site.	The proposed FAR is 5:1 with two existing FAR transfers to the site resulting in a total of 173,800 sqft. of available floor area.
33.510.210: Height	Per Map 510-3, the base height is 75’ with an available Bonus Height of 250’ as described on Map 510-4.	Building height proposed is 131’-8” as measured from the base point. The site base point is at the Northeast corner, along NW Irving. The building height is measured from this base point to top of mechanical screen.
33.140.215: Minimum Building Setbacks	There is no minimum building setback.	There are no building setbacks.
33.140.220: Building Coverage	There is no limit to building coverage as defined in Table 140-2.	The Proposed Building covers 100% of the available buildable site.
33.140.225: Landscaped Areas	There is no requirement for minimum landscaped areas as defined in Table 140-2.	As 100% of the site is built out, landscape is limited to green-roof and street trees.
33.510.220: Ground Floor Windows	The proposed percentage of glazed street level facade must exceed the minimum standard of 33.510.220 section B, paragraph 2 of a minimum of 40% of the ground-level wall area measured from 2 feet to 10 feet above the finished grade.	The proposed percentage of glazed street level facade exceeds the minimum standards on each street frontage.
33.140.235: Screening	All exterior garbage cans, garbage collection areas, and recycling collection areas must be screened from the street and any adjacent properties. Screening must comply with at least the L3 or F2 standards of Chapter 33.248, Landscaping and Screening.	Garbage and recycling for the building is collected from the interior of the building via the loading dock and does not therefore does not require screening at the ground level. The mechanical units on the roof are fully screened from view.
33.140.240: Pedestrian Standards	Standards include connections, materials, and lighting.	The proposed project has a clear connection between the street and the public lobby with a bar space directly accessed upon entry into the lobby.
33.140.280: Demolitions	Demolition on a site that requires a demolition permit is subject to the tree preservation and protection requirements of Title 11, Trees. See Chapter 11.50, Trees in Development Situations.	There are no existing site trees.
33.510.242: Demolitions	Sites must be landscaped within 6 months of the demolition of a building unless there is a development that had been approved through design review. Landscaping must meet at least L1 standards of Chapter 33.248	The proposal is scheduled to complete the design review process prior to the 6 month deadline
33.140.227: Street Trees	Street trees are required for developments by the City Forester. One 2.5” caliper tree required per 25 feet of frontage. See Title 11, Chapter 11.50.	There are no existing street trees. They will be incorporated along the frontages of NW Irving St and NW 12th Ave
33.140.310: Recycling Areas	Requirements for recycling areas are regulated by the Bureau of Planning and Sustainability. See Section 17.102.270, Businesses and Multifamily Complexes Required to Recycle, of the Portland City Code.	Recycling areas will be incorporated into the garbage collection area withing the enclosed loading dock space and will not require screening. Appropriate signage will be incorporated into the design of all recycling areas.
33.266.210: Bicycle Parking	Long-Term Parking required is 2, or 1 per 20 rentable rooms (12 required). Short-Term Parking required is 2, or 1 per 20 rentable rooms (12 required) per Table 266-6, and must be within 50’ of the main entrance	The Proposed Building provides 12 Long-Term secure stalls (see Parking Level Plan), and 12 Short-Term stalls (see Level 1 Plan).

BASE EMPLOYMENT
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REQUIREMENT

PROPOSAL

33. 266.310: Loading	Two loading spaces meeting Standard A are required. The loading space must be at least 35’-0” long, 10’-0” wide, and have a clearance of 13’-0”.	One compliant loading space meeting Standard A is located within the building. Proposal seeks a modification to use existing loading space adjust to the property on 13th Street.
33.266.110: Parking	There are no minimum parking requirements.	Valet service with a portable valet station located on Public right of way is proposed.
33.510.215: Required Building Lines	As defined on Map 510-6, the building must extend to within 12 feet of the street lot line for 75 percent of the lot line.	The Proposed Building fulfills this requirement.
33.510.221: Required Windows Above the Ground Floor	Windows must cover at least 15 percent of the area of street-facing facades above the ground level wall areas.	The windows above ground level of the Proposed Building cover more than 15 percent of the area of street-facing facades above the ground level.
33.510.225: Ground Floor Active Uses	Buildings must be designed and constructed to accommodate uses such as those listed in 33.510.225, Section A, along elevations with frontage to streets as shown on Map 510-9. The distance from the finished floor to the bottom of the structure above must be at least 12 feet. The area must be at least 25 feet deep, measured from the street-facing facade.	This project meets the requirements by allocating an active food and beverage space on the West elevation along NW 13th Ave that is over 25’ deep measured from the facade and over 12’ in height.
33.510.243 Ecoroofs	Ecoroofs must cover 100 percent of viable roof area, however up to 40 percent of roof area may be taken up by applicable equipment, stairwell and elevator enclosures, or uncovered commons spaces.	The Proposed Building fulfills this requirement.
33.510.223 Bird-Safe Exterior Glazing	At least 90% of glazing on the first 60 feet of the building measured from the grade adjacent to the facade, as well as glazed fences must be treated with patterns or applications from the <i>Portland Bird Safe Windows List</i>	The proposal complies with the modified glazing requirements along the lower portion of the facade.
33.510.244 Low Carbon Buildings	New developments with a net area of over 50,000 square feet must provide a letter from the Bureau of Planning and Sustainability that verifies that the project has registered for a green building certification.	Acknowledged.
33.825.025.A.4 3D Digital Model	A three-dimensional digital model of the proposal must be submitted alongside the application for Design Review as well as before a building permit is issued.	Acknowledged.

EXTERIOR DESIGN GOALS

The Portland Proper Hotel is located in a neighborhood that is rich in history and culture. Historically, the Pearl District was home to industries and warehouses that shaped the streetscape according to the movement of freight. Over the past decade and a half, it has been slowly revitalized into an urban and cultural center of the city of Portland— home to world class art galleries, advertising agencies, software firms, breweries, coffee roasters, bakeries, restaurants, and art schools. It is now a place where craft and creation have replaced traditional industries, but the historic buildings continue to provide an ideal context for perpetually changing uses.

The project site is ideally located to take advantage of and add to the cultural offerings of the neighborhood. The building has been designed to engage the streets in both program and form. Active program is placed at the street edges of the ground floor to enhance the pedestrian experience, both at day and night. Large windows at street level allow for the activity of the hotel, restaurant and event space to be expressed and to engage the street life. Views through the facade provide visual access of activity deep in the site. Clear, low-iron glazing at the first floor allows the rich interior environments and warm accent lighting connect to the pedestrian realm.

The main entrance on Irving Street is intended to be used as the hotel entry, while the second entrance on Irving is planned to be used for events. A Restaurant and Bar located on 13th street are accessed via a new “loading dock” dock terrace that ties into the historic loading docks that line the street. The street level openings (windows and doors) are stepped back from the building face to provide a protected and intimate transition from the public right of way and to emphasize the mass of the brick façade in keeping with the neighboring historic masonry buildings.

Historically, the Pearl District was known as the “Northwest Industrial Triangle” and while the original activities of the district are slowly being replaced, the industrial heritage of the neighborhood remain in the form of rehabilitated masonry buildings, industrial lofts and loading docks repurposed for dining and retail. The simple massing of the building and the proportion of the punched windows are in keeping with the industrial lofts in the neighborhood while the divided lite windows further enhance the specific relationship of the building to the Irving Street loft’s steel windows directly across 13th. The industrial heritage is further evoked in the Hotel’s design through the use of metal canopies on 13th street that are detailed with suspension cables in keeping with existing canopies on the street.

Although new construction in the Pearl has largely been characterized by homogenous expanses of storefront glazing at street level, the historic fabric of the neighborhood is far more solid and the character of the openings far more diverse. The design of the hotel’s base emulates the varied openings of

the adjacent buildings. On NW 13th the deep rectilinear openings and simple continuous canopy over the loading dock speak to the size and proportion of the neighboring loading bays. On NW 12th the loading, parking and service entries are sized according to their use and fenestrated with solid and durable materials. The arched windows along the Irving street façade differentiate the sidewalk-level of the building and take their cues from historical masonry arches in the Pearl District and historic Hotels in the larger downtown. Due to modern seismic requirements, the proposed brick arches of the hotel are no longer a means to creating load-bearing spans like you see in historic buildings such as the Portland Armory, however the arched form still serves a strong purpose by emphasizing the scale of the ground floor, the grand public nature of the interior spaces, and brings further specificity to the pedestrian experience of Irving’s cobblestone blocks. The variable form of the ground floor openings respond to the specific context of the streets they face creating a vibrant pedestrian experience informed by neighboring historic buildings where the proportion and character of street level openings reflect the nature of their use, heighten the legibility of the ground floor program and emphasizes the important moments of entry.

The palette of building materials selected for the hotel have been chosen for their durability and aesthetic linkage to the historical character of the neighborhood. Brick was selected as the primary material to connect the building with the robustness of the historic masonry buildings in the neighborhood. While the massing of the building is simple, the proportion and articulation of the façade provide additional sense of scale and are harmonious with the context. The proposed horizontal brick banding, which is differentiated from the vertical brick piers through a combination of brick offsets and variations in brick texture and coursing, relates to the tartan quality of neighboring industrial facades and creates coherency across all building facades and levels. The design pulls many lessons from the depth and texture achieved in historic masonry buildings by adding layers of interest and detail through the introduction of textured bricks and coursing offsets at each floor, while the masonry arches and brick patterning at the ground floor put the artistry of the mason’s craft on high display.

The Proper Hotel takes its cues from the context of masonry and industrial buildings and applies the lessons of proportion, materiality and detail to design a contemporary building that will knit into the fabric of the surrounding load-bearing masonry buildings while reimagining the potential of brick in the context of modern cladding systems. In using a repertoire of traditional brick details in modern ways, the project diversifies the architectural language and enriches a palette of materials defined by the local fabric.

CITY CONTEXT



IMMEDIATE BUILDING CONTEXT
PEARL DISTRICT



1. Gann Building, 1455 NW Irving St
2. Downtown Self Storage, 1304 NW Johnson St
3. Pearl West, 1455 NW Irving St
4. Crane Flats and Lofts, 1323 NW Irving St
5. Irving Street Lofts, 1314 NW Irving St
6. Maddox Building, 1231 NW Hoyt St
7. 1306, 1306 NW Hoyt St
8. Modern Confectionary Lofts, 539 NW 13th Ave
9. The Pearl Building, 1313 NW Glisan St
10. Gerdling Edlen, 1477 NW Everett St
11. Pearl Auto Park, 1111 NW Davis St
12. The Armory, 128 NW 11th Ave
13. Halo Shoes, 230 NW 10th Ave
14. Weinhard Brewery Complex, 10 NW 12th Ave
16. Zenabe Court Apartments, 708 NW 20th Ave
19. The Regent, 1975 NW Everett St

HISTORIC (Demolished)
A. Survey 2 NW Irving 9th to 10th

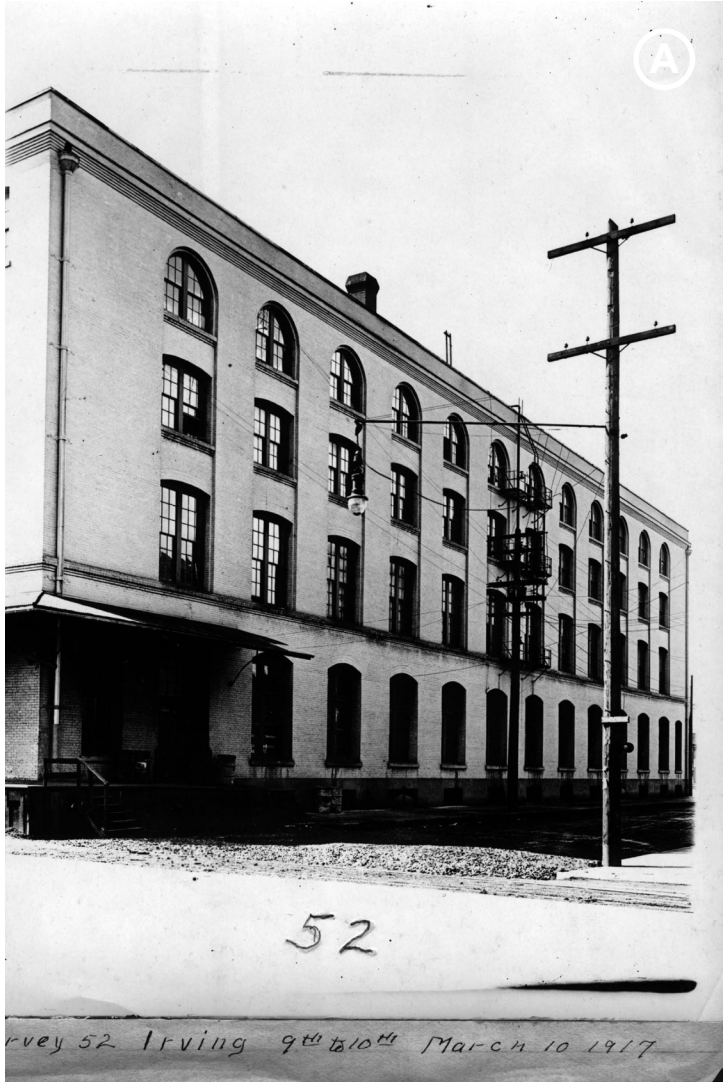
BUILDING CONTEXT PORTLAND DOWNTOWN



1. Gann Building, 1455 NW Irving St
2. Downtown Self Storage, 1304 NW Johnson St
3. Pearl West, 1455 NW Irving St
4. Crane Flats and Lofts, 1323 NW Irving St
5. Irving Street Lofts, 1314 NW Irving St
6. Maddox Building, 1231 NW Hoyt St
7. 1306, 1306 NW Hoyt St
8. Modern Confectionary Lofts, 539 NW 13th Ave
9. The Pearl Building, 1313 NW Glisan St
10. Gerdling Edlen, 1477 NW Everett St
11. Pearl Garage, 1111 NW Davis St
12. The Armory, 128 NW 11th Ave
13. Halo Shoes, 230 NW 10th Ave
14. Weinhard Brewery Complex, 10 NW 12th Ave
15. NW Irving Units, 2004 NW Irving St
16. Zenabe Court Apartments, 708 NW 20th Ave
17. Monterey Apartments, 1998-1900 NW Irving St
18. Mayfair Apartments, 1929 NW Irving St
19. The Regent, 1975 NW Everett St
20. 1532 SW Morrison St
21. Bronaugh Apartments, 1434 SW Morrison St
22. Tiffany Center, 1410 SW Morrison St
23. 1130 SW Morrison St
24. Dekum Buidling, 519 SW 3rd Ave
25. 214 SW Harvey Milk Street
26. Benson Hotel, 309 SW Broadway

- HISTORIC (Demolished)
- A. Survey 2 NW Irving 9th to 10th
 - B. The Oregonian Building, SW 6th Ave & Alder St
 - C. Congress Hotel, SW 6th Ave & Main St

ARCHES IN HISTORIC PORTLAND
BUILDINGS - HISTORIC, INCLUDING
BOTH DEMOLISHED AND EXISTING



A. Survey 2 NW Irving 9th to 10th



B. The Oregonian Building, *Lost Portland Oregon*



C. Congress Hotel, *Lost Portland Oregon*



26. Benson Hotel, *Lost Portland Oregon*

IMMEDIATE CONTEXT



2. Downtown Self Storage
View south on NW 13th Ave



3. Pearl West
4. Crane Flats and Lofts
5. Irving Street Lofts
View west on NW Irving St



4. Crane Flats and Lofts
5. Irving Street Lofts
View east on NW Irving St



8. Modern Confectionary Lofts
9. The Pearl Building
View south on NW 13th Ave

ARCHED DOORWAYS ALONG NW
IRIVING STREET



15. NW Irving Unit



15. NW Irving Unit



17. Monterey Apartments



18. Mayfair Apartments

BRICK TEXTURES PAINTED AND WEATHERED



8. Modern Confectionary Lofts



8. Modern Confectionary Lofts



9. The Pearl Building



Painted and weathered brick

BRICK TEXTURES



13. 230 NW 10th Ave



13. 230 NW 10th Ave



16. 708 NW 20th Ave



20. 1532 SW Morrison Street



Jeff Morris Fire Museum
Clinker and weathered Brick

BUILDING REFERENCE:
PORTLAND BUSCUIT COMPANY
11. PEARL AUTO PARK



Historic Image



West Facade
Diversity of floor to floor arched openings



Southwest corner signage



South Facade
Diversity of ground floor openings



East Facade
Diversity of ground floor openings

BUILDING REFERENCE:
THE ARMORY



Historic Image North East Corner



North Facade



Arched entry close up

STREET SCALE



2. Downtown Self Storage



4. Crane Flats and Lofts
West Facade Entry



4. Crane Flats and Lofts
Main West Facade Entry 2



4. Crane Flats and Lofts
Southwest corner view

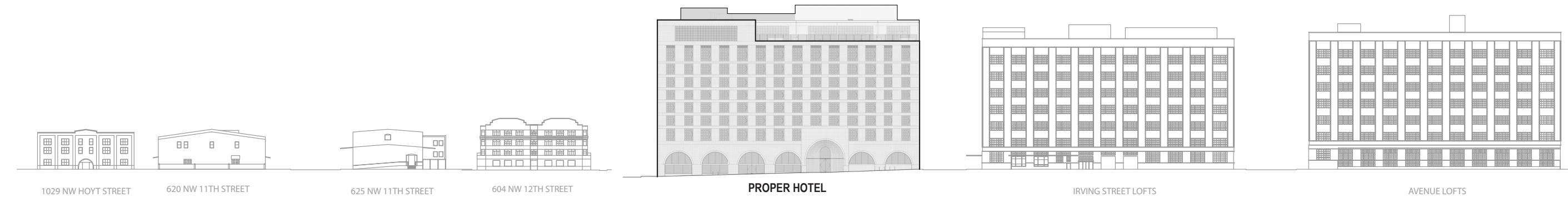


4. Crane Flats and Lofts
Striated brick pattern

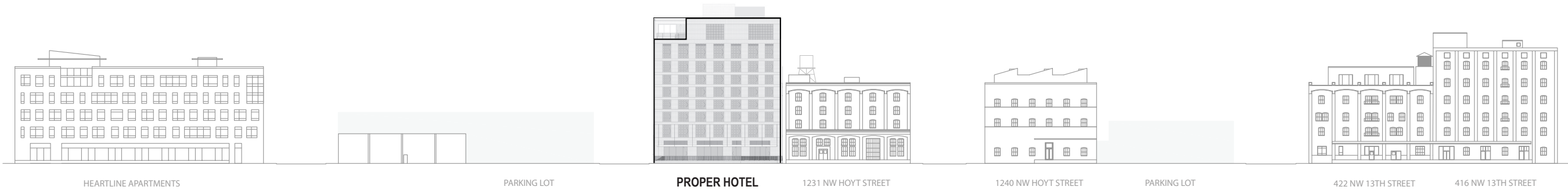
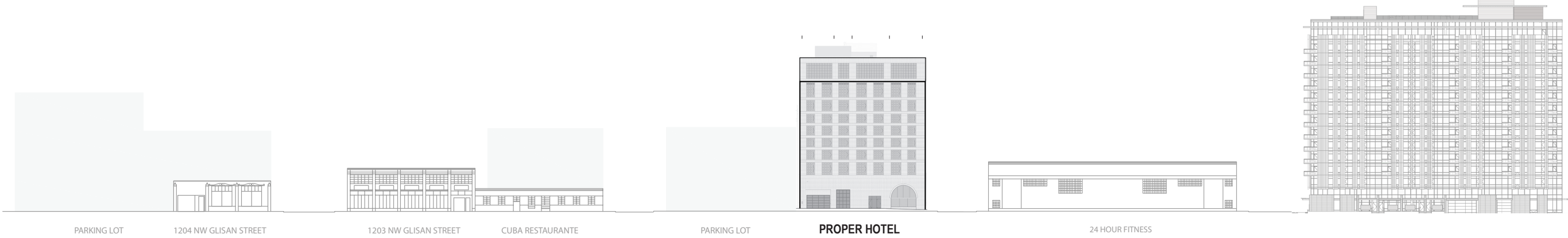


5. Irving Street Lofts

NEIGHBORHOOD // CONTEXT



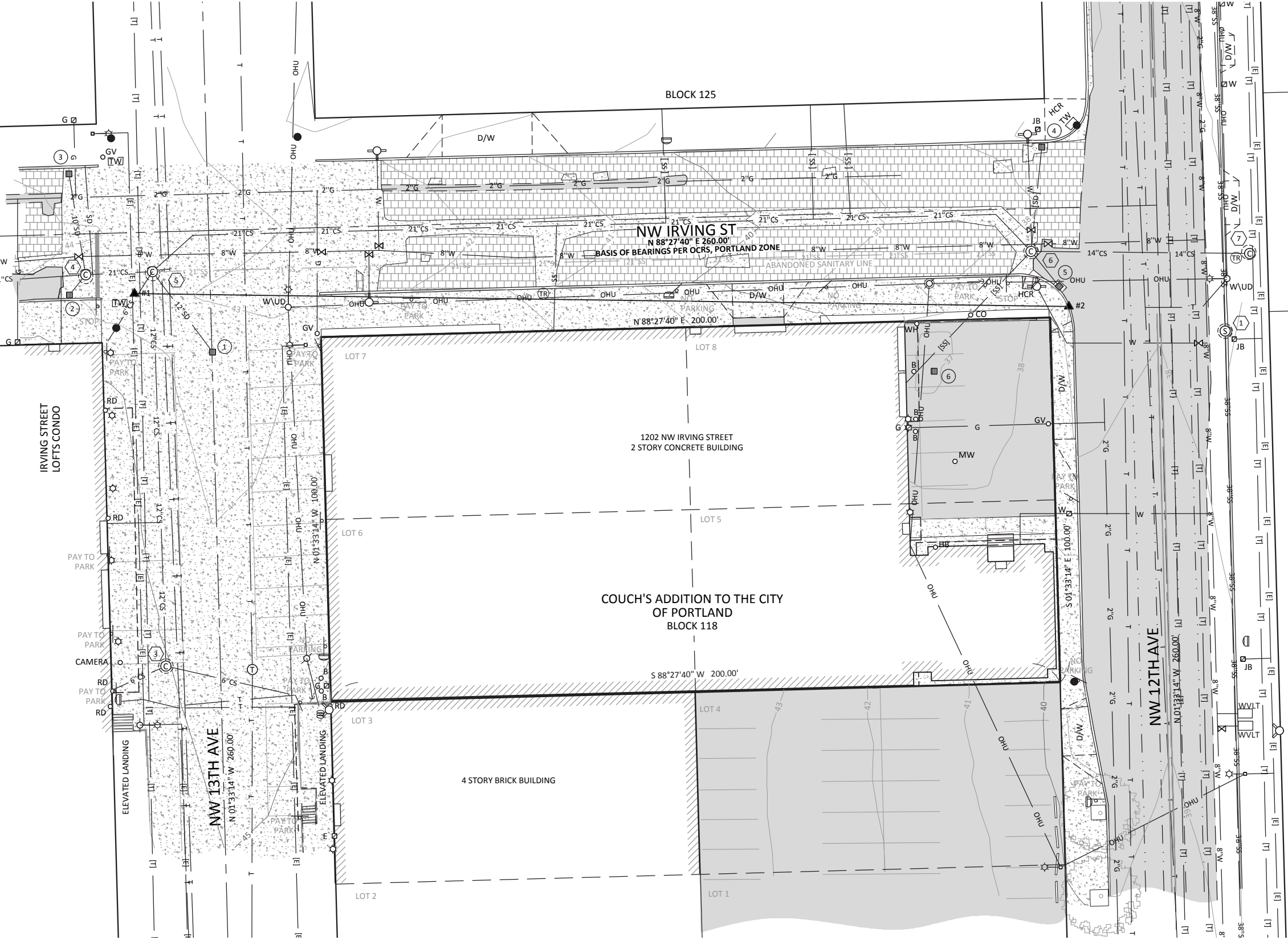
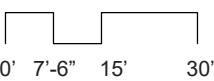
NEIGHBORHOOD // CONTEXT



C.11 PLANS

SURVEY
MAXIMUM ELEVATION CHANGE OF SITE IS 6'-0"

Scale = 1" = 30'-0"



NOTES:

- 1.) VERTICAL DATUM: CITY OF PORTLAND
BENCHMARK: 2-1/2 INCH BRASS DISK AT THE NORTHWEST QUADRANT OF THE INTERSECTION OF NW FLANDERS STREET
AND NW 12TH AVENUE
BENCHMARK NO. 3620
ELEVATION = 40.741'
- 2.) BASIS OF BEARINGS FOR THIS SURVEY IS THE OREGON COORDINATE REFERENCE SYSTEM (OCRS), PORTLAND ZONE.
THE RESULTANT BEARING OF THE CENTERLINE OF NW IRVING STREET IS NORTH 88°27'40" EAST.
- 3.) A TITLE REPORT WAS NOT PROVIDED FOR THE PURPOSE OF THIS SURVEY. EASEMENTS AFFECTING THE SUBJECT
PROPERTY MAY EXIST.
- 4.) UTILITY LOCATIONS SHOWN ARE PER FIELD LOCATED UTILITY PAINT MARKS & REFERENCE MAPS MADE AVAILABLE BY
THE VARIOUS UTILITY PROVIDERS. UNLESS INDICATED, DEPTHS OF UTILITY LINES ARE NOT AVAILABLE. ALL UTILITY
LOCATIONS SHOULD BE FIELD VERIFIED (POTHOLED) PRIOR TO CONSTRUCTION.

STORM TABLE:

- 1 CATCH BASIN
RIM = 43.30'
PIPE NOT VISIBLE
- 2 CATCH BASIN
RIM = 43.51'
IE 10" OUT (NE) = 39.41'
- 3 CATCH BASIN
RIM = 43.34'
IE 10" OUT (S) = 41.54'
- 4 CATCH BASIN
RIM = 37.37'
FULL OF DEBRIS
- 5 CATCH BASIN
RIM = 37.32'
IE 10" OUT (NW) = 35.82'
- 6 CATCH BASIN (SUMP TYPE)
RIM = 36.95'
WATER LEVEL = 36.15'

SANITARY TABLE:

- 1 SANITARY MANHOLE
RIM = 37.36'
IE 21" RCP IN (W) = 32.70'
IE 6" IN (S) = 32.26'
IE 12" RCP IN (S) = 35.40'
IE 10" OUT (NE) = 12.76'
- 2 SANITARY MANHOLE
RIM = 38.87
SHOWN PER AS-BUILTS
- 3 COMBINATION MANHOLE
RIM = 45.07'
IE 6" IN (E) = 39.57'
IE 6" IN (W) = 38.97'
IE 12" NCP OUT (N) = 38.67'
- 4 COMBINATION MANHOLE
RIM = 43.65'
IE 10" IN (NW) = 36.16'
IE 21" RCP IN (W) = 33.26'
IE 10" IN (S) = 36.06'
IE 21" RCP OUT (E) = 33.16'
- 5 COMBINATION MANHOLE
RIM = 43.40'
IE 21" RCP IN (W) = 32.70'
IE 6" IN (SW) = 32.90'
IE 12" RCP IN (S) = 35.40'
IE 21" RCP OUT (NE) = 32.70'
- 6 COMBINATION MANHOLE
RIM = 37.80'
IE 21" RCP IN (NW) = 27.75'
IE 10" IN (W) = 28.90'
IE 10" IN (SE) = 29.80'
IE 14" VSP OUT (E) = 27.45'
- 7 COMBINATION MANHOLE
RIM = 37.82'
IE 14" VSP IN (W) = 26.72'

PROJECT CONTROL:

STATION	DESCRIPTION	NORTHING	EASTING	ELEVATION
1	1-1/8" BRASS CAP "KPFF CONTROL"	174210.48	344894.67	43.32'
2	1-1/8" BRASS CAP "KPFF CONTROL"	174206.97	345150.81	38.02'
3	1-1/8" BRASS CAP "KPFF CONTROL"	173997.48	345150.65	40.15'
4	1-1/8" BRASS CAP "KPFF CONTROL"	173992.14	344948.20	45.87'

LEGEND:

	BUILDING OUTLINE WITH DOOR		ELECTRICAL METER
	CONCRETE SURFACE		GUY ANCHOR
	GRAVEL SURFACE		LUMINAIRE
	ASPHALT SURFACE		POWER POLE
	WALL		OVERHEAD LIGHT
	COBBLESTONE SURFACE		POWER POLE/OVERHEAD LIGHT
	BUILDING OVERHANG		GAS METER
	CURB LINE		GAS RISER
	EDGE OF ASPHALT		GAS VALVE
	RIGHT-OF-WAY LINE		SANITARY MANHOLE WITH STRUCTURE
	LOT LINE		COMBINED MANHOLE WITH STRUCTURE
	PROPERTY LINE		STORM MANHOLE WITH STRUCTURE
	PARTIALLY EXPOSED RAIL		CATCH BASIN
	TELECOMMUNICATIONS LINE		AREA DRAIN
	STORM LINE		SANITARY/STORM CLEAN OUT
	COMBINED SEWER LINE		SANITARY/STORM STRUCTURE #
	SANITARY SEWER LINE		TELECOMMUNICATIONS MANHOLE
	WATER LINE		TELECOMMUNICATIONS VAULT
	GAS LINE		WATER VALVE
	ELECTRIC LINE		FIRE HYDRANT
	OVERHEAD UTILITY LINES		WATER METER
	UNDERGROUND LINE PER AS-BUILTS		FIRE DEPARTMENT CONNECT
	SIGN		WATER VAULT
	BOLLARD		HOSE BIB
	DRIVEWAY ENTRY		MONITORING WELL
	HANDICAP RAMP		PLANTER BOX
	TACTILE WARNING		TRASH CAN
	BIKE RACK		DECIDUOUS TREE
	PARKING PAY STATION		-PERIMETER REPRESENTS DRIPLINE
	ROOF DRAIN		CONIFEROUS TREE
	WEEP HOLE		-PERIMETER REPRESENTS DRIPLINE
	WITH UTILITY DROP		FOUND MONUMENT
	ELECTRICAL JUNCTION BOX		PROJECT CONTROL POINT

HOYT STREET CONDOMINIUMS



1. PROPOSED FRONTAGE IMPROVEMENTS IN RIGHT-OF-WAY SHOWN FOR REFERENCE ONLY. TO BE PERMITTED UNDER SEPARATE PUBLIC WORKS PERMIT.
2. ARCHITECT TO LOCATE BUILDING. INTENT OF C3.00 IS TO SHOW THE RELATION OF THE PROPOSED BUILDING TO PUBLIC IMPROVEMENTS.
3. DEMOLITION WORK IN THE RIGHT-OF-WAY TO BE PERMITTED UNDER SEPARATE PUBLIC WORKS PERMIT #XXXX.
4. SEE SHEET C2.00 FOR BASIS OF BEARING.











ⓧ KEY NOTES

#	DESCRIPTION
1	SIDEWALK IMPROVEMENTS ARE PART OF SEPARATE PBOT PERMIT PROCESS. PBOT JOB #XXXX.
2	MATCH EXISTING SIDEWALK GRADES
3	SEE ARCHITECTURAL PLANS FOR HISTORIC LOADING DOCK, STAIRS, AND RAMP.

GRADING SHEET NOTES

1. GRADING SHOWN IN THE RIGHT-OF-WAY ARE SHOWN FOR REFERENCE ONLY AND PERMITTED UNDER SEPARATE PBOT PERMITTING PROCESS (XXXX). THE INTENT IS TO SHOW PROPOSED SIDEWALK SLOPES AS IT RELATES TO THE BUILDING DOORS.
2. SLOPES PROVIDED ON GRADE ARROW ARE FOR REFERENCE ONLY.
3. LANDINGS ON ACCESSIBLE ROUTES SHALL NOT EXCEED 2% IN ANY DIRECTION.
4. ALL ACCESSIBLE ROUTES SHALL COMPLY WITH CURRENT ADA ACCESSIBILITY GUIDELINES FOR BUILDING AND FACILITIES (ADAAG).

GRADING LABEL LEGEND

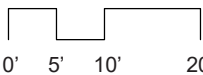
<u>CALLOUT</u>	<u>DESCRIPTION</u>
<u>X.X%</u>	GRADING SLOPE AND DIRECTION (DOWNHILL)
	SPOT ELEVATION
	DESCRIPTION LISTED BELOW.
	NO DESCRIPTION MEANS TP OR TG
	XX.XX BW BACK OF WALK (PED ZONE)
	FG FINISHED GRADE
	LP LOW POINT
	TC TOP OF CURB
	TP TOP OF PAVEMENT
	TOD TOP OF LOADING DOCK
	(XXX.X±) EXISTING GRADE
	(MATCH WHERE APPLICABLE)

SHEET LEGEND

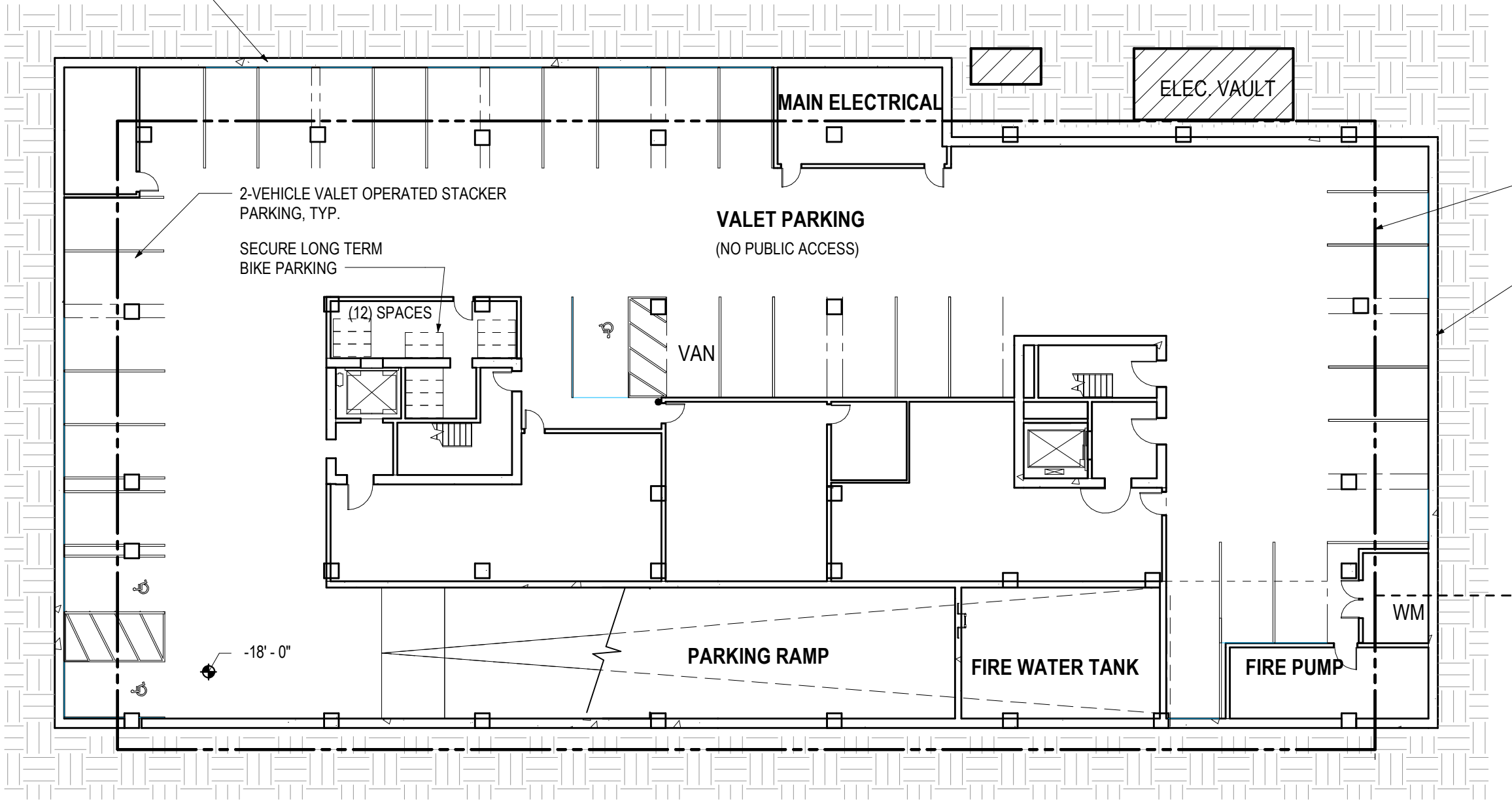
————— - - - ————— PROPERTY LINE

BELOW GRADE PLAN

Scale = 1" = 20'-0"



PARKING GARAGE
ENCROACHMENT



PROPERTY LINE

PARKING GARAGE
ENCROACHMENT

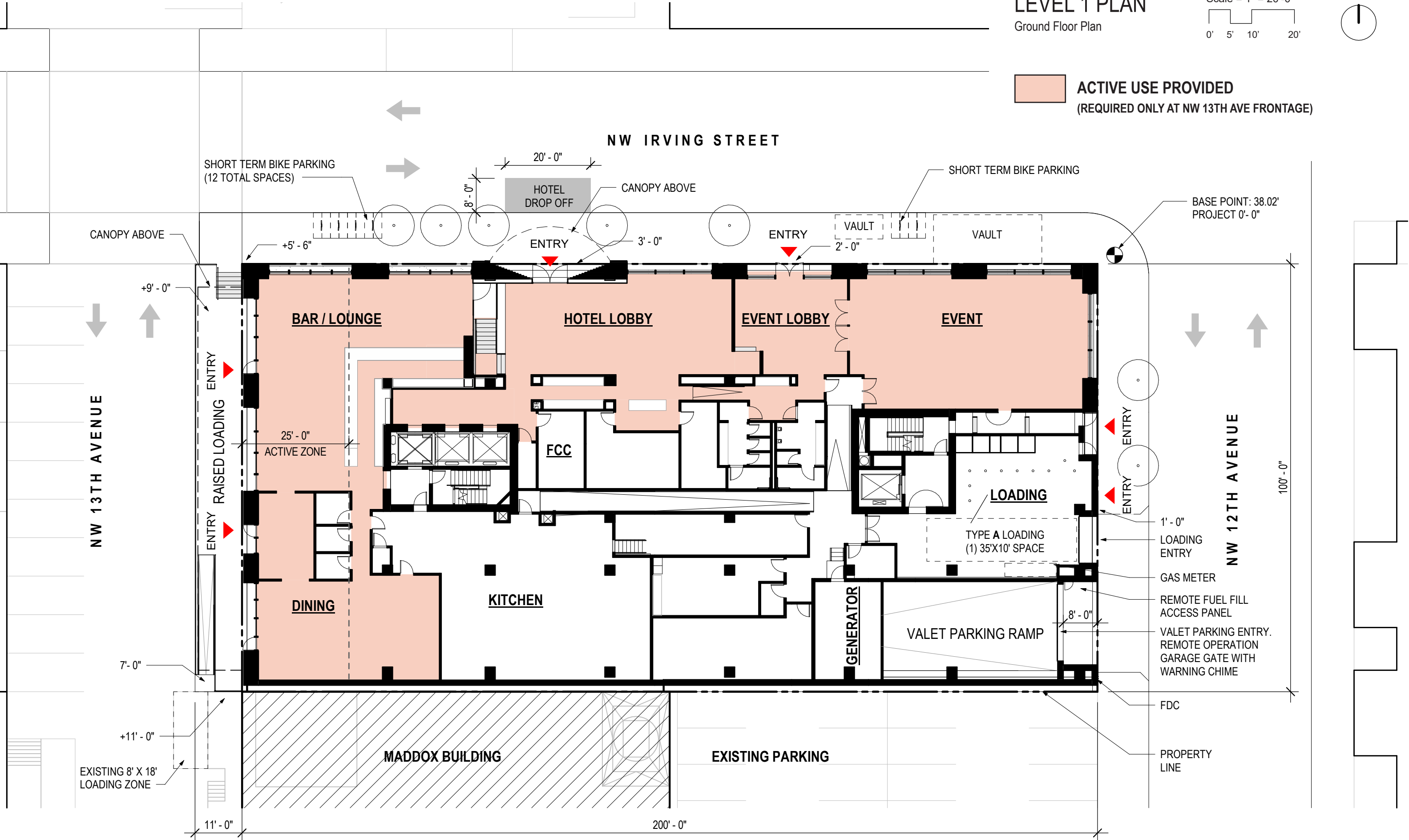
ACCESSIBLE	3
STANDARD	1
STACKER SPACES	62
TOTAL VALET SPACES	66

LEVEL 1 PLAN
Ground Floor Plan

Scale = 1" = 20'-0"

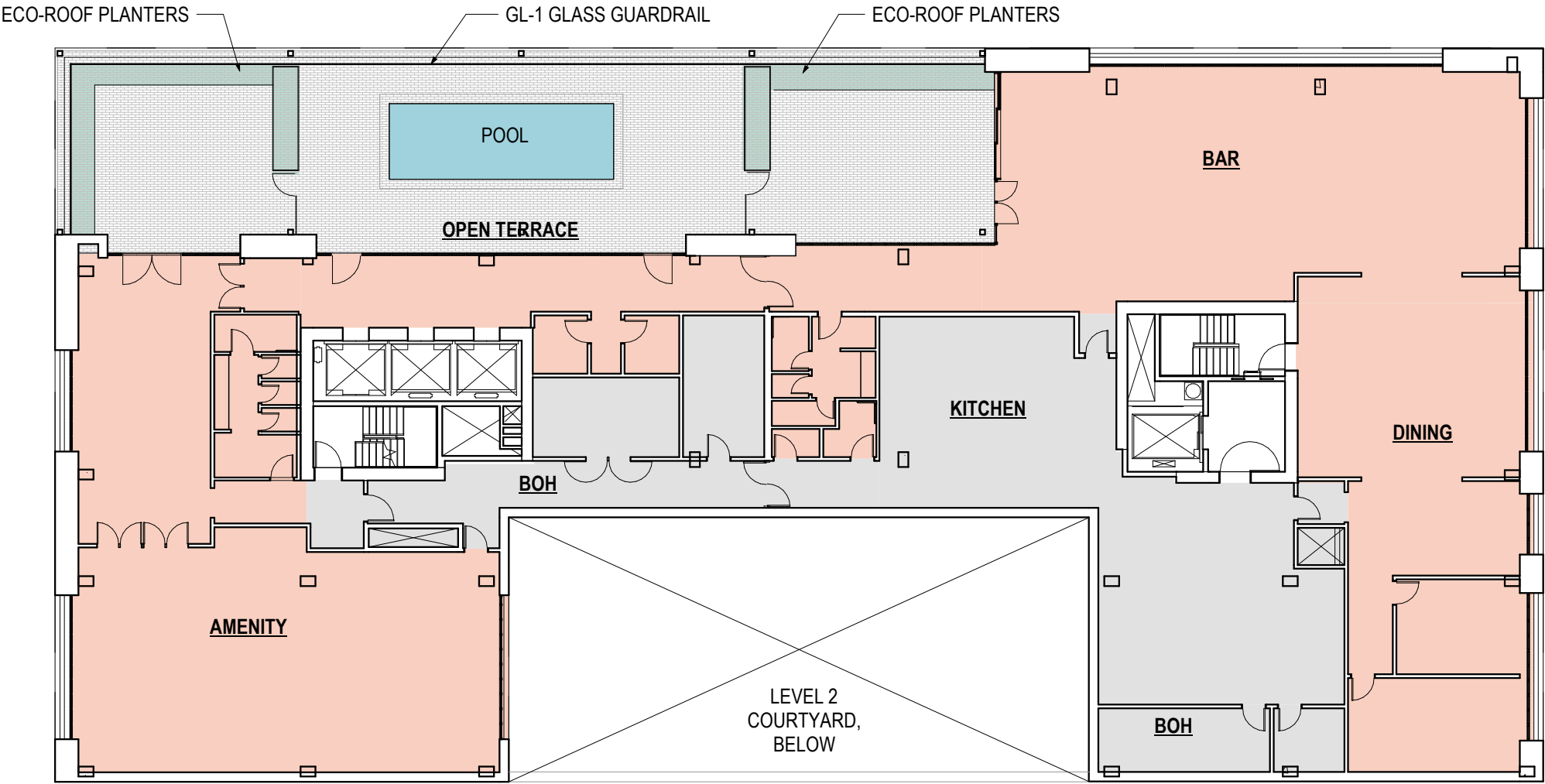


ACTIVE USE PROVIDED
(REQUIRED ONLY AT NW 13TH AVE FRONTAGE)



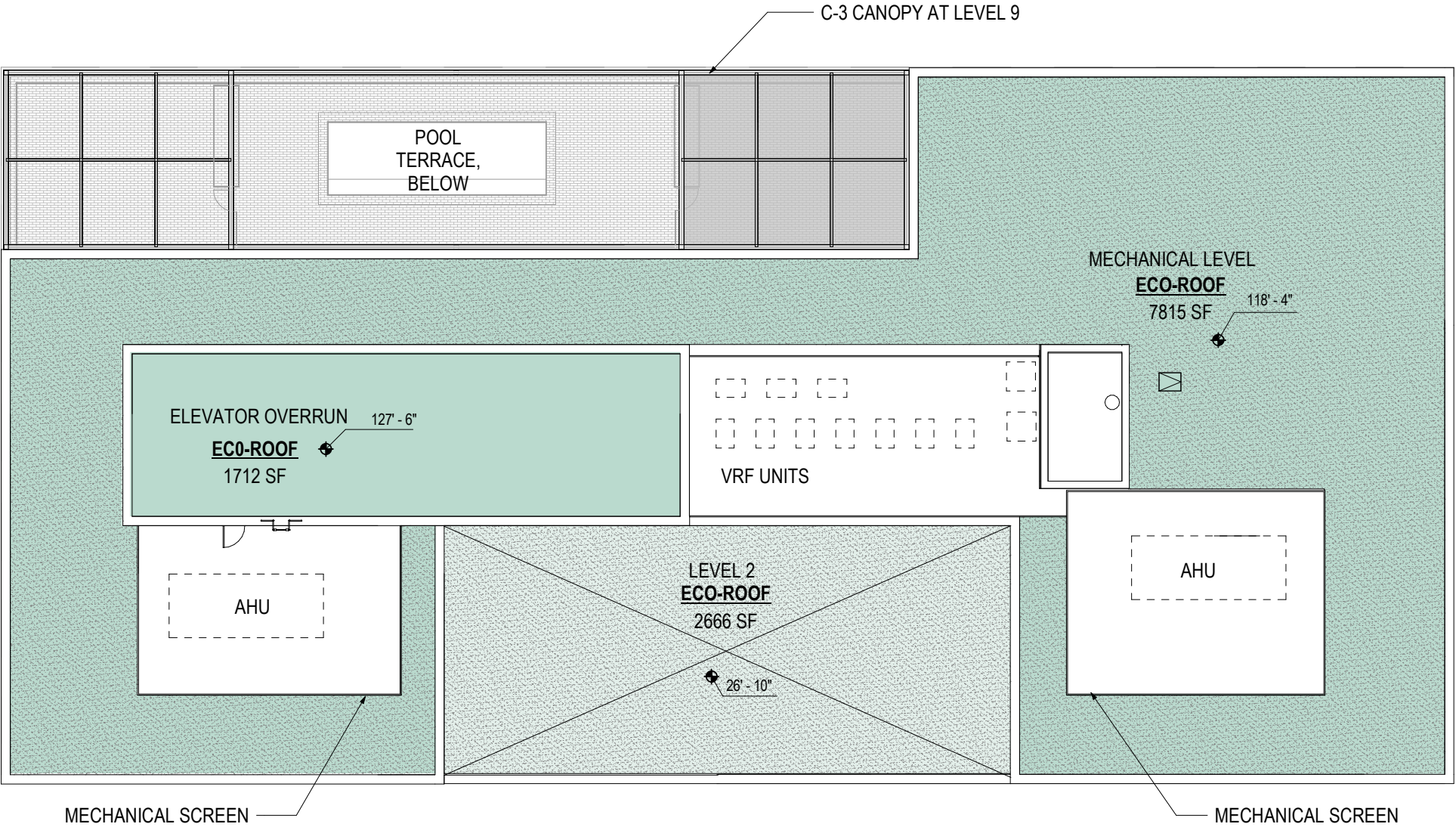
LEVEL 9 PLAN
Roof Amenity Level Floor Plan

Scale = 1" = 20'-0"



ROOF PLAN
Mechanical Roof Floor Plan

Scale = 1" = 20'-0"



TOTAL ROOF AREA:
18,406 SF

REQUIRED ECO-ROOF AREA:
60% OF 18,406 SF = 11,043 SF

PROVIDED ECO-ROOF AREA:
12,193 SF

C.18 ELEVATIONS

Scale = 1" = 20'-0"

EXTERIOR MATERIALS LEGEND

- BV-1: BRICK CAVITY WALL
- BV-2: TEXTURED BRICK CAVITY WALL
- BV-3: TEXTURED BRICK CAVITY WALL
- CMU-1: RATED CMU PARTY WALL

- W-1: ALUMINUM WINDOW SYSTEM
- W-2: FIBERGLASS WINDOW SYSTEM
- W-3: ALUMINUM WINDOW SYSTEM

- MP-1 METAL PANEL CLADDING
- MP-2 METAL PANEL MECHANICAL SCREEN

- C-1: METAL LOADING CANOPY
- C-2: METAL ENTRY CANOPY
- C-3: METAL TERRACE CANOPY

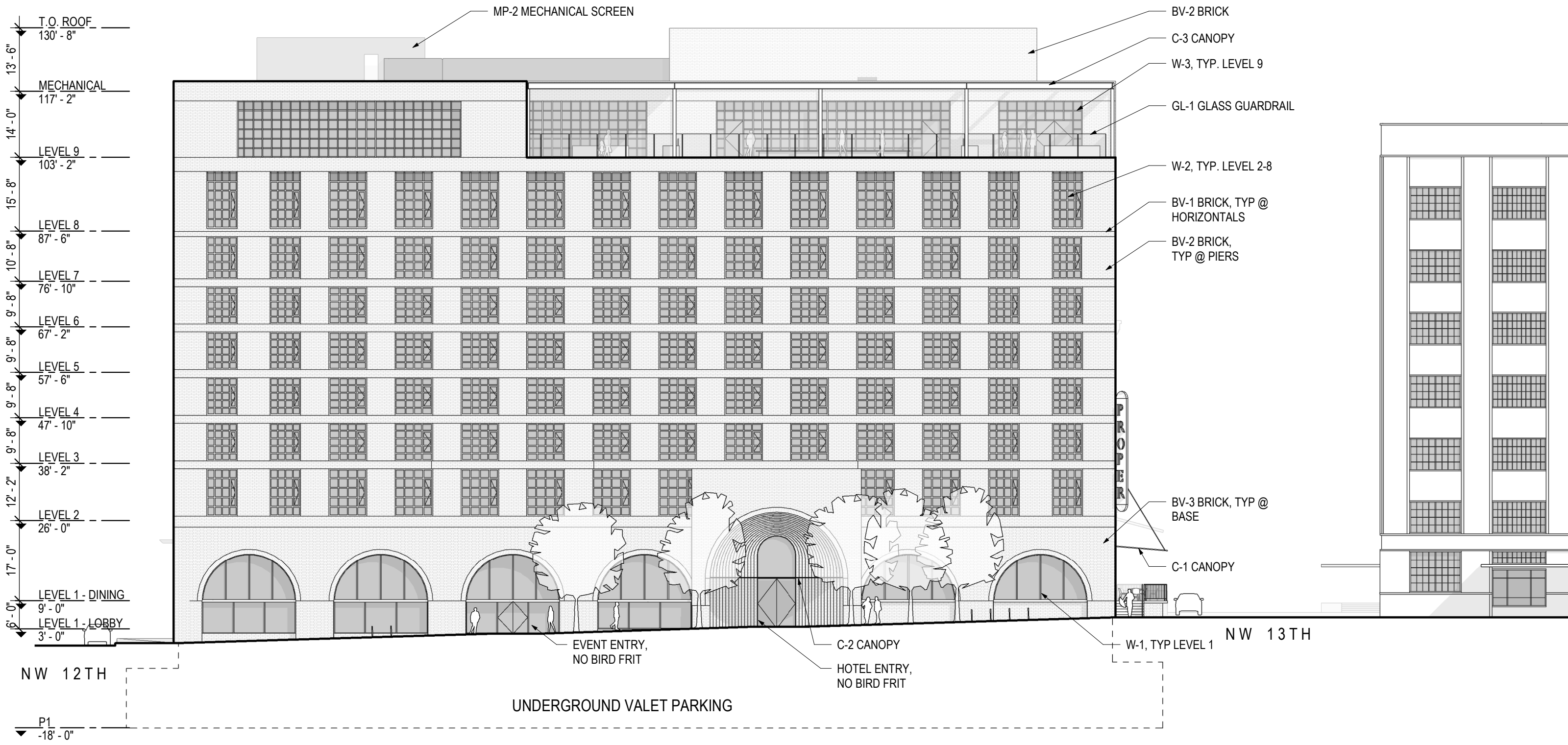
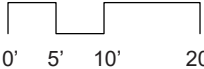
- BG-1: METAL BAR GRILLE

- GL-1: GLASS GUARDRAIL

- GR-1: GREEN-ROOF TRAY SYSTEM

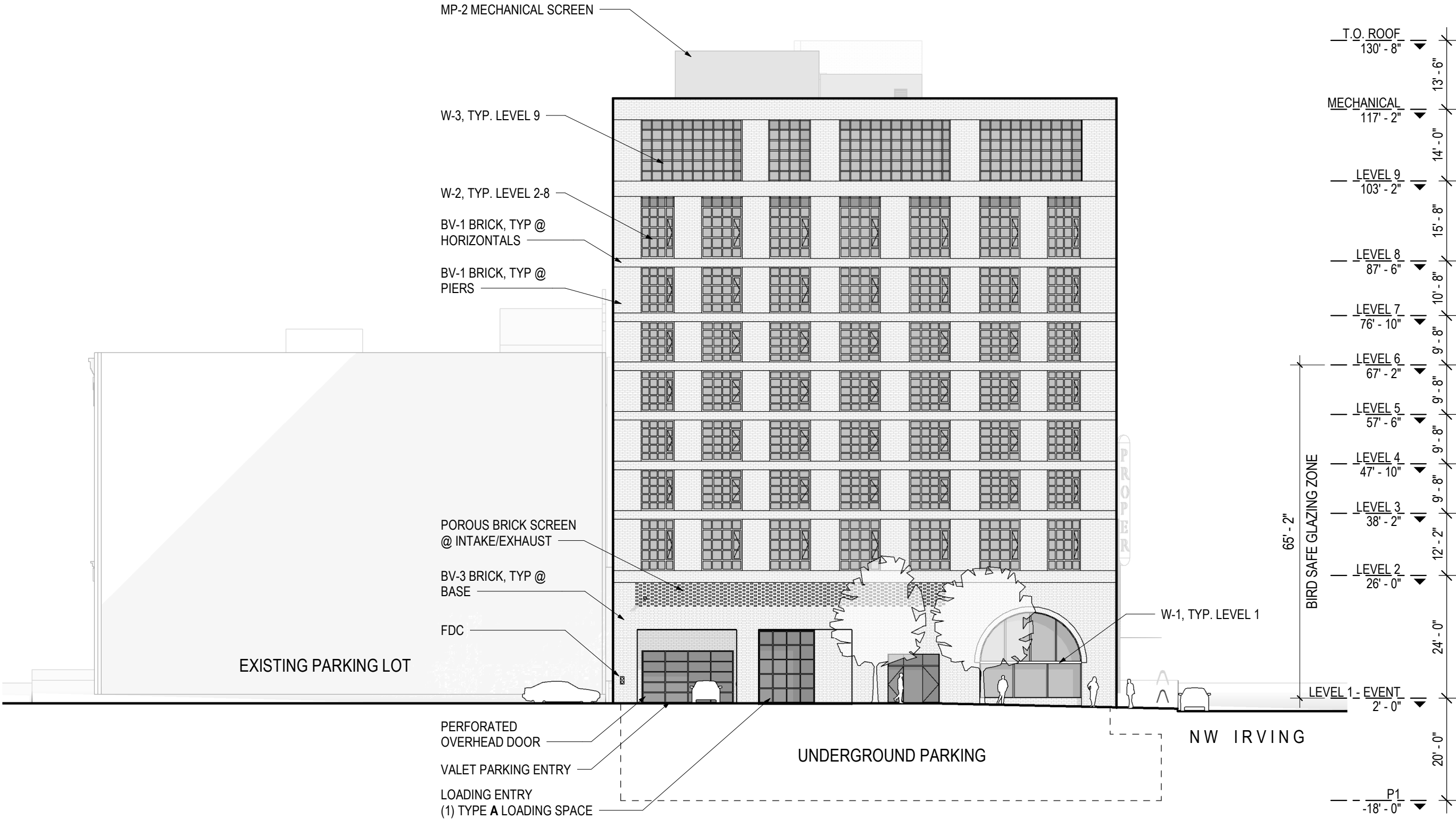
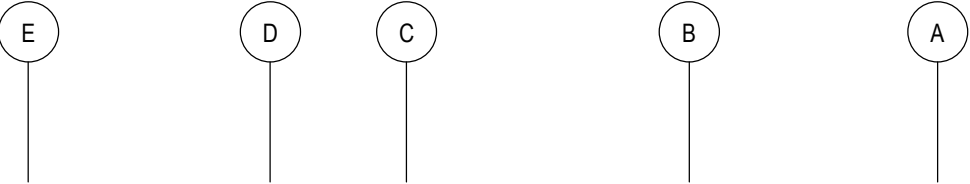
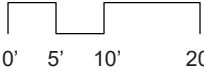
NORTH ELEVATION

Scale = 1" = 20'-0"



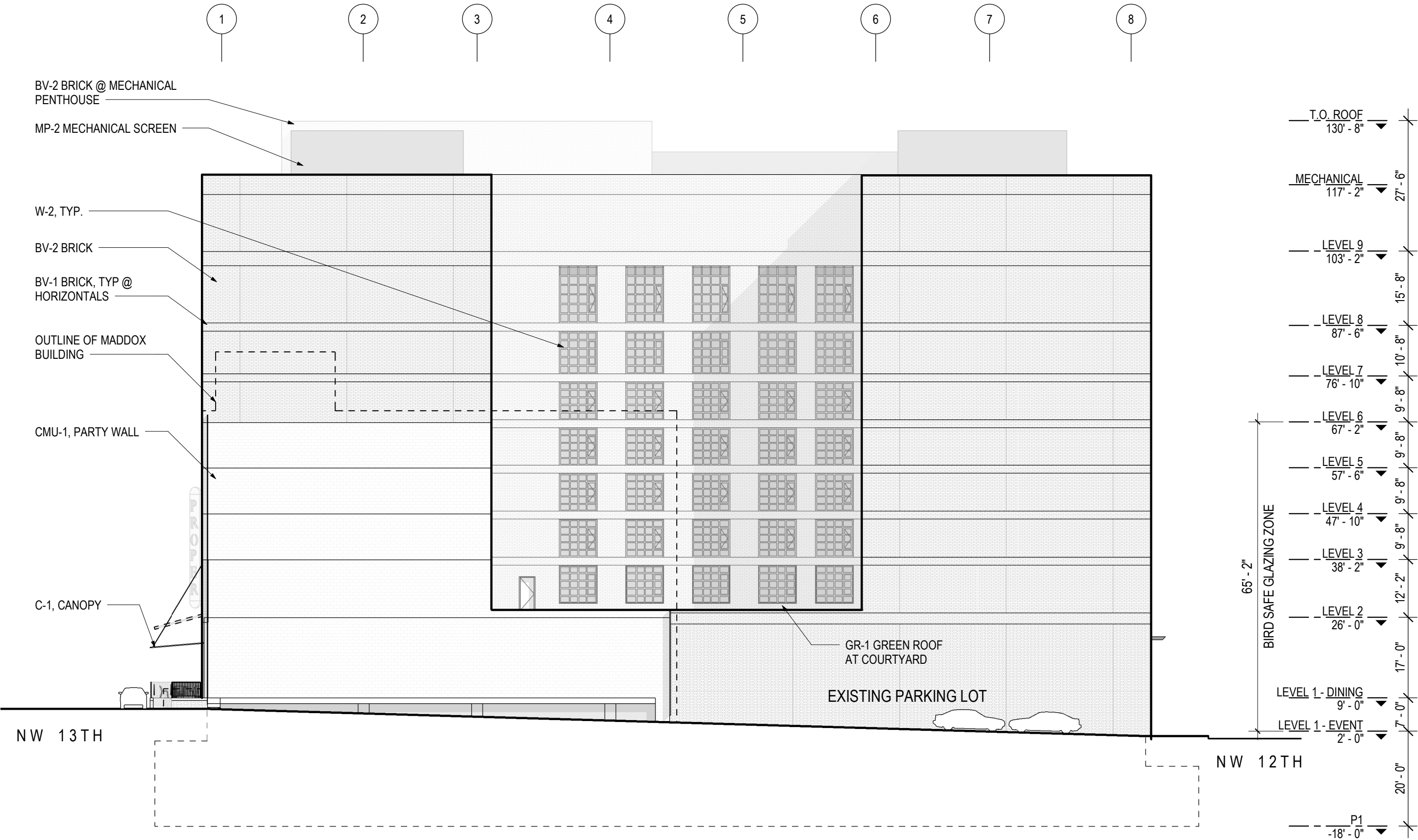
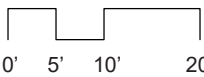
EAST ELEVATION

Scale = 1" = 20'-0"



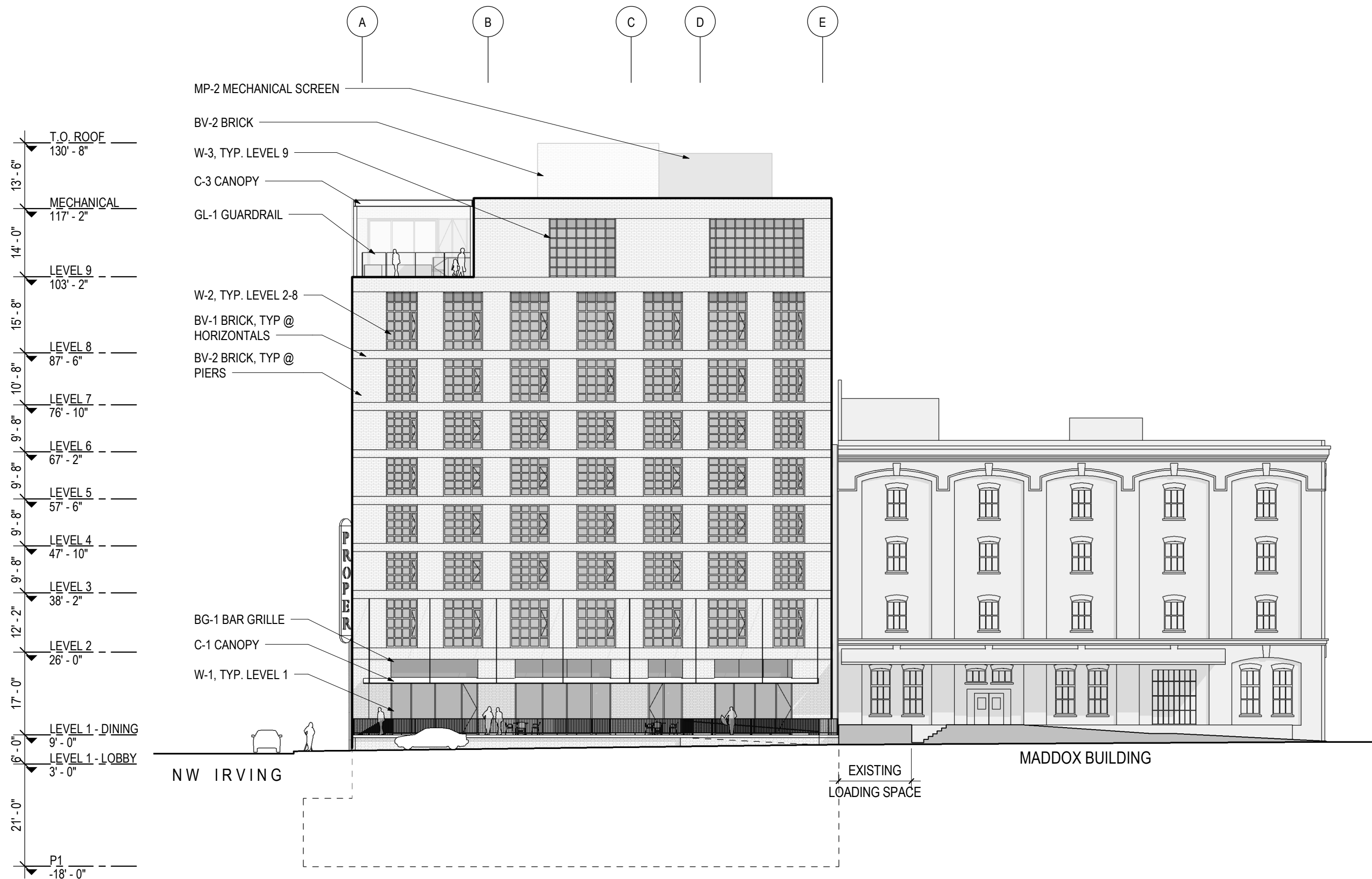
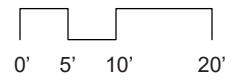
SOUTH ELEVATION

Scale = 1" = 20'-0"



WEST ELEVATION

Scale = 1" = 20'-0"



C.19 MATERIALS

EXTERIOR MATERIALS LEGEND

- BV-1: BRICK CAVITY WALL
- BV-2: TEXTURED BRICK CAVITY WALL
- BV-3: TEXTURED BRICK CAVITY WALL
- CMU-1: RATED CMU PARTY WALL

- W-1: ALUMINUM WINDOW SYSTEM
- W-2: FIBERGLASS WINDOW SYSTEM
- W-3: ALUMINUM WINDOW SYSTEM

- MP-1 METAL PANEL CLADDING
- MP-2 METAL PANEL MECHANICAL SCREEN

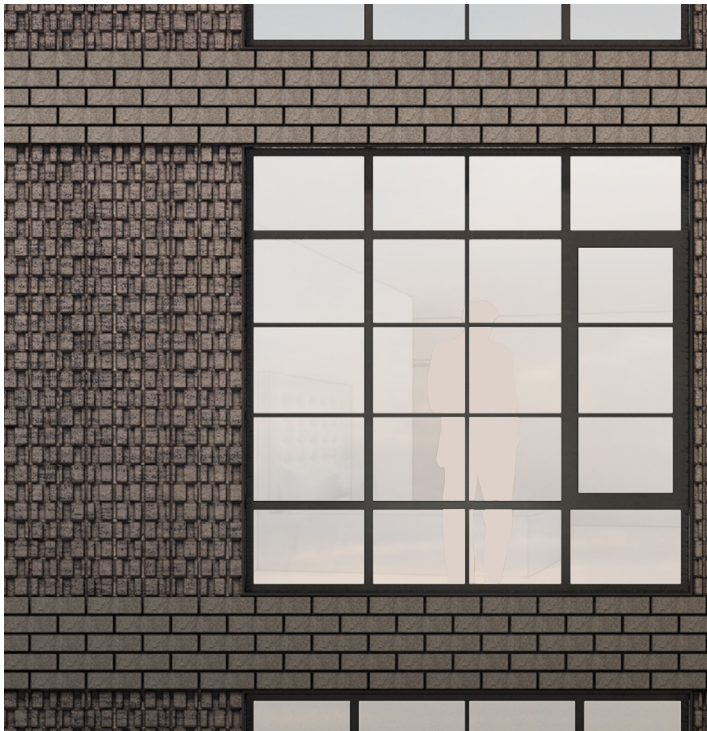
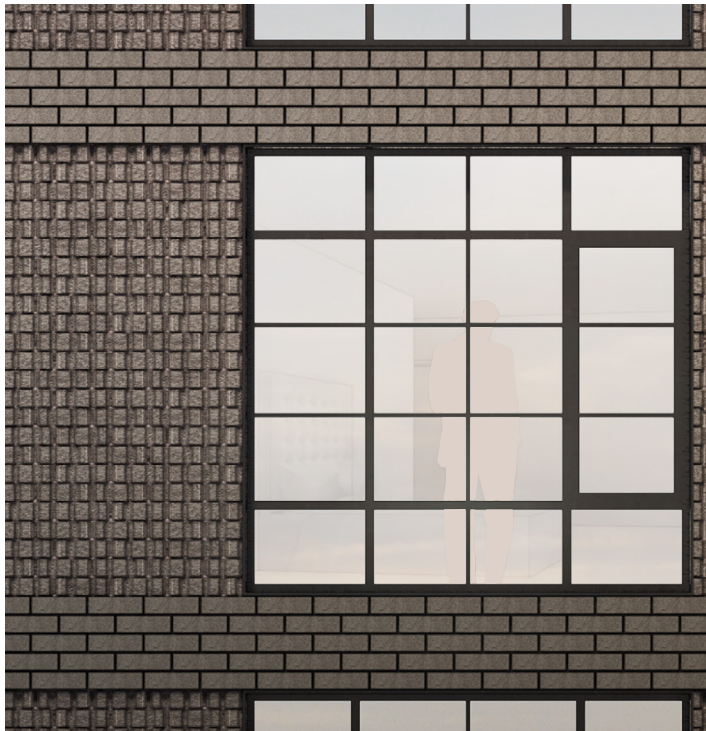
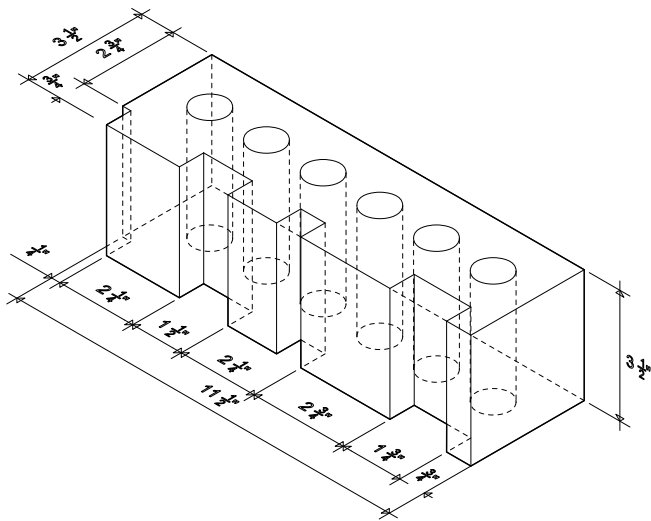
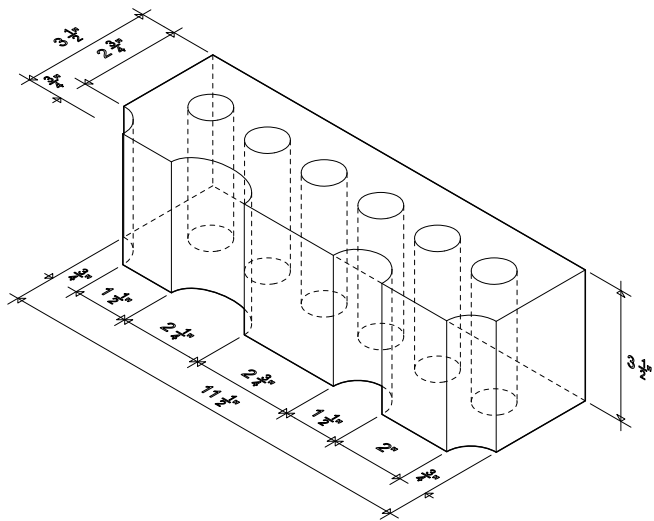
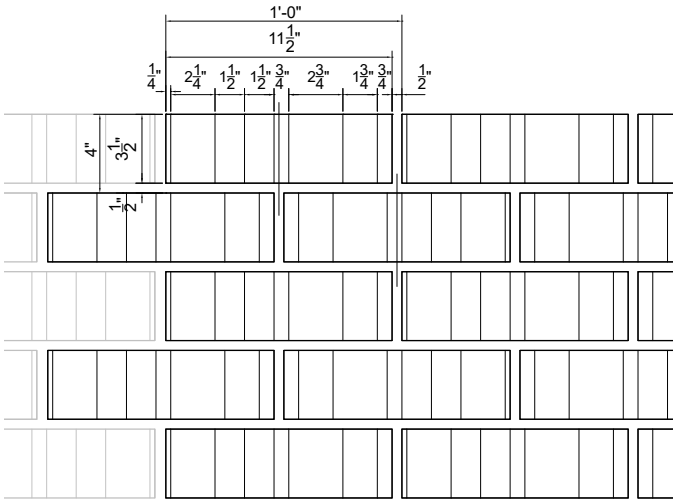
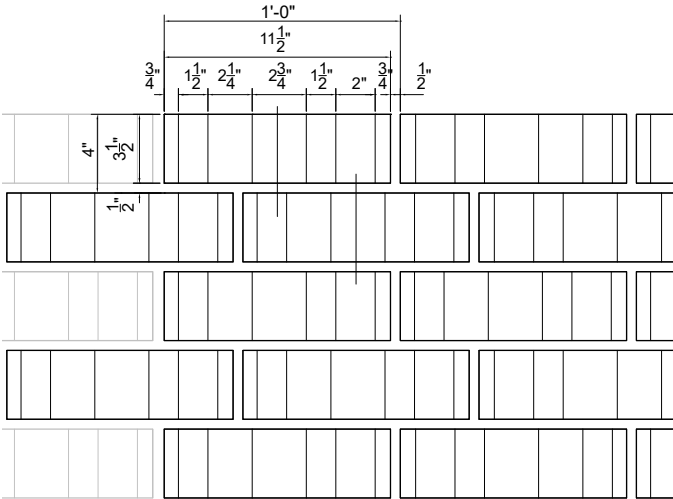
- C-1: METAL LOADING CANOPY
- C-2: METAL ENTRY CANOPY
- C-3: METAL TERRACE CANOPY

- BG-1: METAL BAR GRILLE

- GL-1: GLASS GUARDRAIL

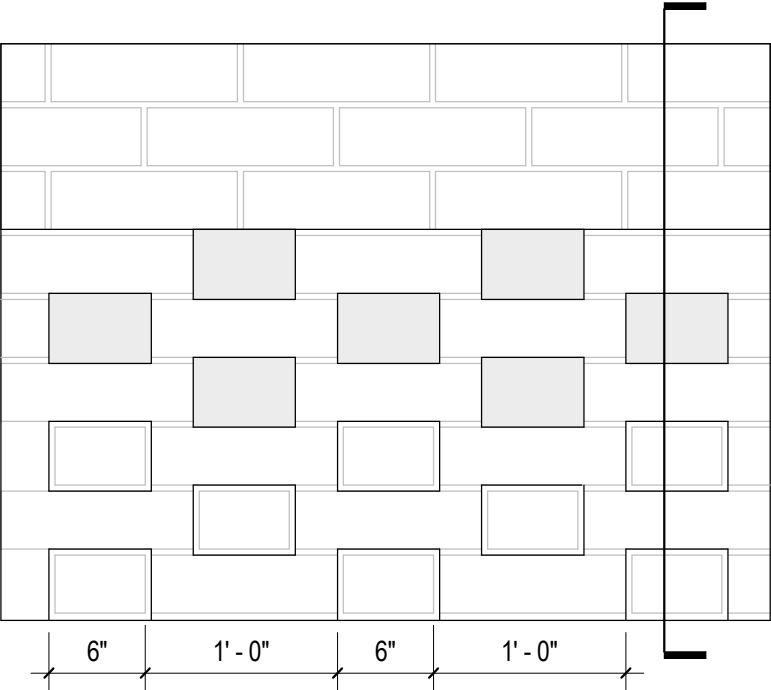
- GR-1: GREEN-ROOF TRAY SYSTEM

TEXTURED BRICK BV-2

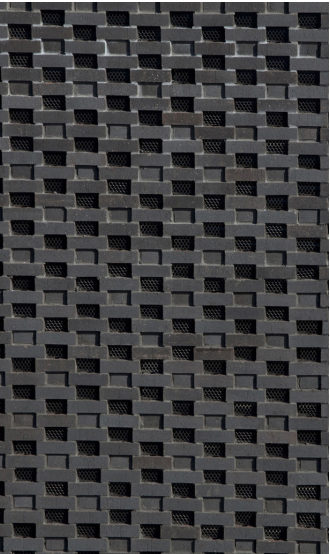
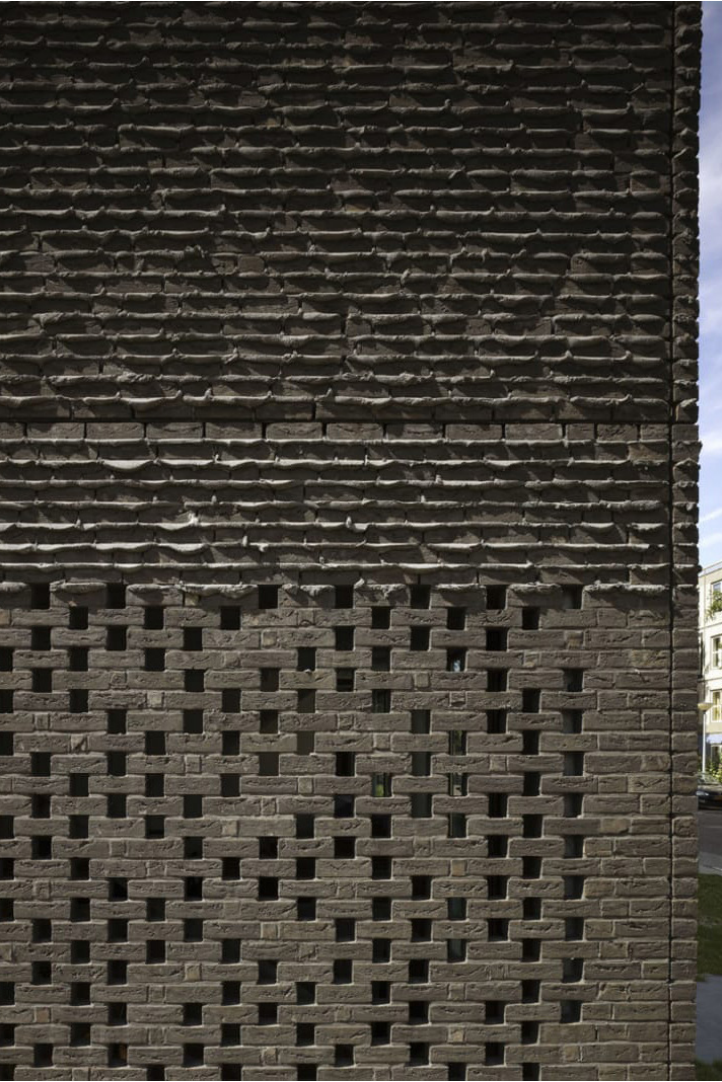
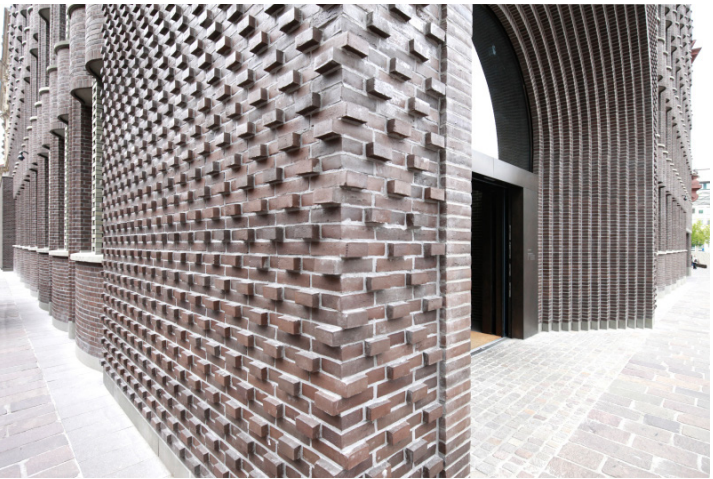
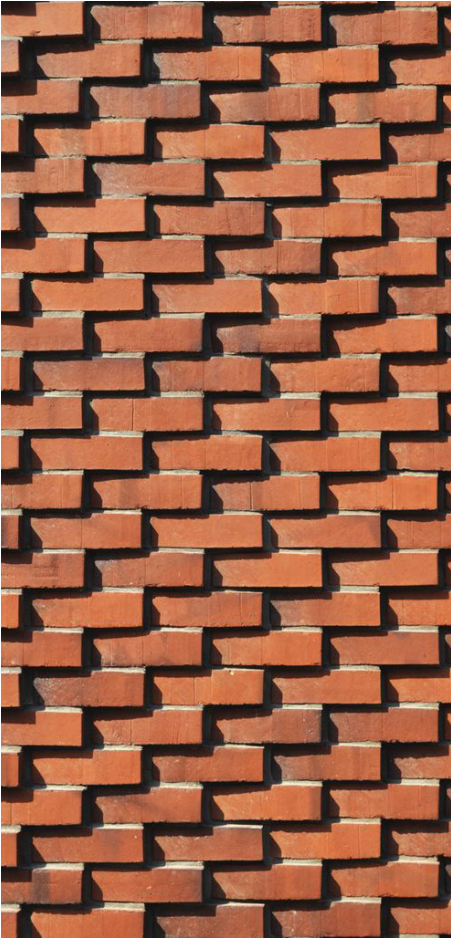
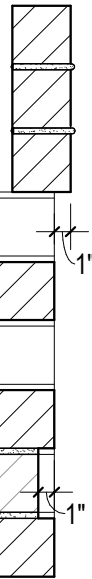


TEXTURED BRICK BV-3

ELEVATION



SECTION



C.20 RENDERINGS

**C.35 VIEW SOUTH ON 13TH
C.36 VIEW WEST ON IRVING
C.37 VIEW EAST ON IRVING
C.38 VIEW NORTH ON 13TH**



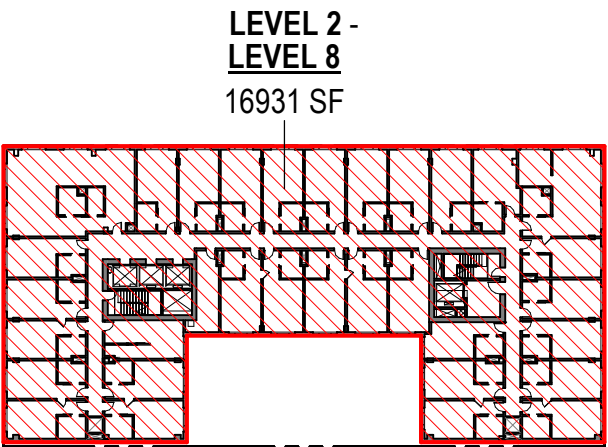
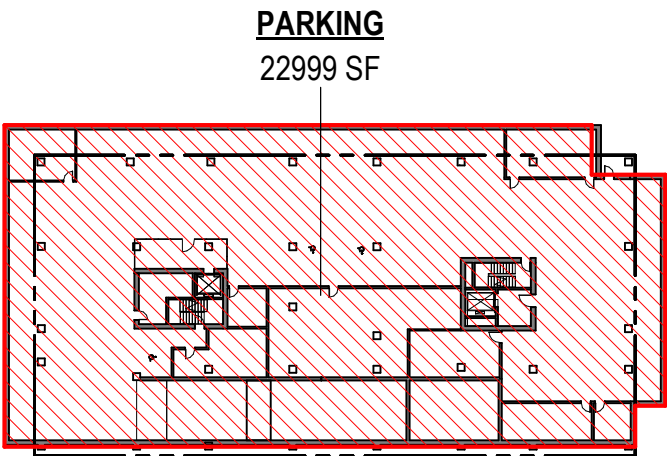
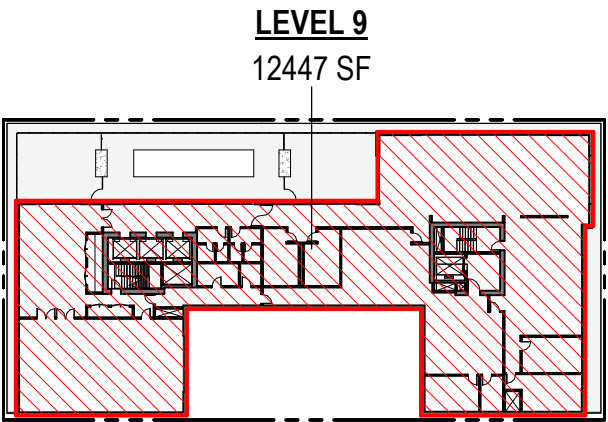
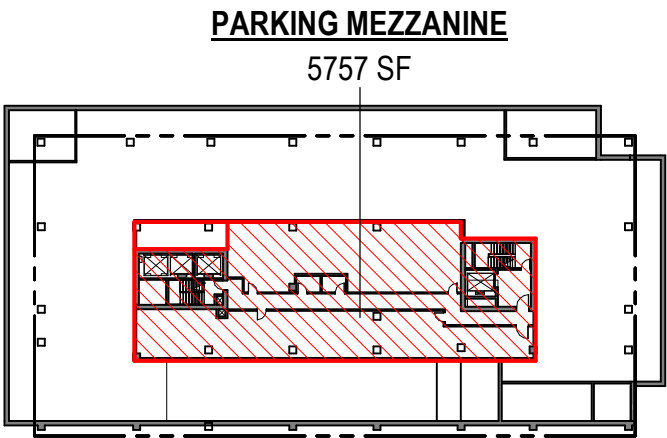
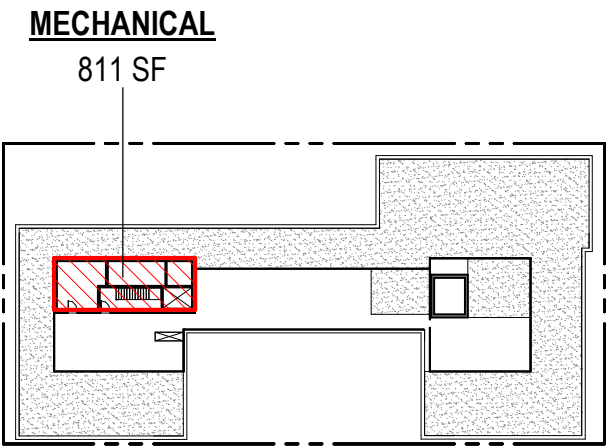
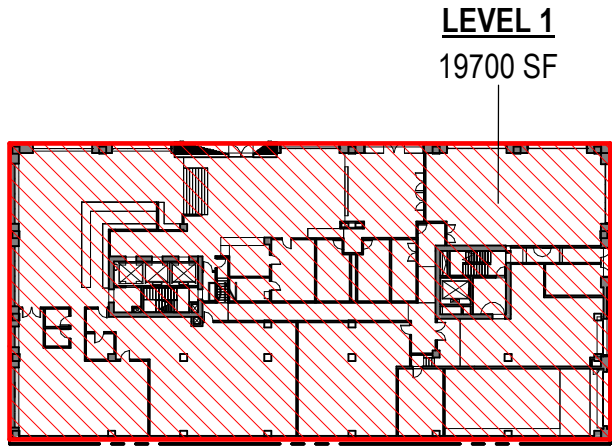






A.1 APPENDIX

FAR CALCULATIONS



Site Area= 20,000 SF
Maximum base FAR of 5:1 (Map 510-2)= 98,500 SF
FAR transferred to site (PR 18-185046) = 35,100 SF
FAR transferred to site (PR 18-230163) = 38,700 SF
Maximum Allowable Floor Area= 173,800 SF

GROSS AREA SCHEDULE	
Name	Gross Area

PARKING	22999 SF
PARKING MEZZANINE	5757 SF

Not included in FAR: 28756 SF

LEVEL 1	19700 SF
LEVEL 2	16931 SF
LEVEL 3	16931 SF
LEVEL 4	16931 SF
LEVEL 5	16931 SF
LEVEL 6	16931 SF
LEVEL 7	16931 SF
LEVEL 8	16931 SF
LEVEL 9	12447 SF
MECHANICAL	811 SF

Area included in FAR: 151475 SF
Total Gross Area: 180231 SF