



STAFF REPORT AND RECOMMENDATION TO THE DESIGN COMMISSION

CASE FILE: LU 19-119554 DZM AD
PC # 18-108160
Lincoln High School

REVIEW BY: Design Commission

WHEN: August 1, 2019 at 1:30 PM

WHERE: 1900 SW Fourth Ave., Room 2500A
Portland, OR 97201

Bureau of Development Services Staff: Staci Monroe 503-823-0624 / staci.monroe@portlandoregon.gov

GENERAL INFORMATION

Applicant: Kate Feiertag | Bora Architects
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Property Owner: Portland Public Schools
c/o Erik Gerding, Project Manager
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Portland, OR 97208
egerding@pps.net | 503-916-3902

Site Address: 1600 SW Salmon Street

Legal Description: TL 100 10.96 ACRES, SECTION 04 1S 1E

Tax Account No.: R991040010

State ID No.: 1S1E04AB 00100

Quarter Section: 3028, 3128

Neighborhood: Goose Hollow, contact planning@goosehollow.org.

Business District: Goose Hollow Business Association, contact Angela Crawford at 503-223-6376., Stadium Business District, contact Tina Wyszynski at tina@stadiumdistrictpdx.biz

District Coalition: Neighbors West/Northwest, contact Mark Sieber at 503-823-4212.

Plan District: Central City - Goose Hollow

Other Designations: Existing School is a Rank II resource on the Historic Resource Inventory

Zoning: CXd – Central Commercial zone with a Design overlay

Case Type: DZM AD – Design Review with Modifications and an Adjustment

Procedure: Type III, with a public hearing before the Design Commission. The decision of the Design Commission can be appealed to City Council.

Proposal:
The applicant seeks Design Review approval for the Lincoln High School replacement project in the Goose Hollow subdistrict of the Central City Plan District. The project consists of replacing the existing school building with a new 6-story, 102’ tall building providing 281,000 GSF of educational and support space at the western end of the site along SW 18th. The athletic field and track will also be replaced and located on the eastern portion of the site with a smaller practice field along the southern edge. The existing portable buildings will be relocated along SW 14th and repurposed as a teen/parent center. Parking and loading will be along the south and accessed off SW 17th and 16th. Aligning with SW 17th will be a large plaza space and north-south pedestrian and bike path. Other site elements include a series of internal walkways, small structures for athletic operations and bike parking, landscaping and gates and fences. Building materials include cement panel, fiberglass and aluminum storefront windows, curtain wall windows, perforated ribbed metal panel for mechanical enclosures, and glass canopies.

The following Modifications are also requested (PZC 33.825.040):

1. *Transit Street Main Entrance* – To not provide a building entry every 200’ along SW 18th that is unlocked during regular business hours and within 25’ of a Transit Street (33.130.242)
2. *Required Building Line* – For the building to not extend to the street lot lines along SW Salmon and 14th for 75% of these frontage (33.510.215).
3. *Ecoroof* – To reduce the ecoroof area requirement from 60% to 28% (PZC 33.510.243).
4. *Parking Landscape* – To not provide the perimeter landscaping along a portion of the southern edge of the surface parking lot and reduce the amount of required interior parking lot landscaping (33.266.130.G).

The following Adjustment is also requested (PZC 33.805.040):

1. *Bike Parking* – To reduce the width (from 2’ to 18”) and number of required long-term bike parking spaces (from 280 to 92) (PZC 33.266.210)

A Type 3 Design Review is required for new development in the Goose Hollow Sub District that have a value of more than \$3,266,000 per Sections 33.420.041.A and 33.825.025.A.1 (Table 825-1).

Approval Criteria:

In order to be approved, this proposal must comply with the approval criteria of Title 33, Portland Zoning Code. The applicable approval criteria are:

- Central City Fundamental Design Guidelines
- Goose Hollow Design Guidelines
- Section 33.825.040 - Modifications
- Section 33.805.040 - Adjustments

ANALYSIS

Site and Vicinity: The site is located within the area bounded by SW 18th Avenue on the west, SW 14th Ave (and the I- 405 Freeway) to the east, and SW Salmon Street to the north. Several commercial properties and the terminus of both SW 16th and SW 17th Avenues make up the abutting property line to the south. The existing development consists of the school building at the eastern end of the site, which is listed on

Portland's Historic Resource Inventory, and sports field and track on the western portion of the site. Two modular classrooms approved in 2016 occur between the school building and SW 14th. A large City designations Heritage Tree (*Juglans nigra*: Black Walnut) is located at the NE corner of the site.

Regarding area amenities, the site is within a typical 5-minute walk (a quarter of a mile distance) from a number of retail, commercial and open space resources. To the northwest is Providence Park (formerly PGE Park then Jeld-Wen Park) home of the current MLS champion Portland Timbers. Further north is West Burnside Street where there are a number of local and national retail and commercial outlets. To the southwest is Goose Hollow, also home to a number of established local restaurants and retailers. To the west it becomes much more residential in particular as one crosses into the King's Hill Historic District whose boundary is a block to the west of the site. To the east the density and height of buildings increases as one heads further into the city center.

Regarding transportation amenities adjacent to the site; the site is well served with transportation options. SW Salmon Street is designated in the Transportation Service Plan (TSP) as a Transit Access Street, SW 18th Avenue is designated as a Regional Transitway. MAX service is provided to the site on SW 18th Ave. Bus service is provided to the site by the #15, #51 and #63 on SW 18th Ave. and by the #51 and #63 on SW Salmon St. SW 18th Ave. and SW Salmon St. are both designated as City Bikeways. The site is also within the Goose Hollow pedestrian district.

Zoning: The Central Commercial (CX) zone is intended to provide for commercial development within Portland's most urban and intense areas. A broad range of uses is allowed to reflect Portland's role as a commercial, cultural and governmental center. Development is intended to be very intense with high building coverage, large buildings, and buildings placed close together. Development is intended to be pedestrian-oriented with a strong emphasis on a safe and attractive streetscape.

The "d" overlay promotes the conservation and enhancement of areas of the City with special historic, architectural or cultural value. New development and exterior modifications to existing development are subject to design review. This is achieved through the creation of design districts and applying the Design Overlay Zone as part of community planning projects, development of design guidelines for each district, and by requiring design review. In addition, design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area.

The Central City Plan District implements the Central City 2035 Plan. The regulations address the unique role the Central City plays as the region's premier center for jobs, health and human services, tourism, entertainment and urban living. The regulations encourage a high-density urban area with a broad mix of commercial, residential, industrial and institutional uses, and foster transit-supportive development, pedestrian and bicycle-friendly streets, a vibrant public realm and a healthy urban river. The site is within the Goose Hollow Subdistrict of this plan district.

Land Use History: City records indicate that prior land use reviews include:

- LUR 98-015453 DZ (reference file # 98-00147): Design review approval to install new exterior shear walls along the east facade of Lincoln High School as part of a structural seismic upgrade and a new external elevator tower.
- LUR 00-006785 CU DZ (reference file # 00-00230): Design review and conditional use approval of a proposal to construct a new seismic steel shear wall, including some new floor area, along the existing east wall between two existing wings of the school.

- LUR 01-007799 CU DZ (reference file # 01-00401): Withdraw design review and conditional use application to maintain two modular classroom additions.
- LU 06-123898 DZM: Design review approval for a new bathroom/concession facility building with Modifications to allow the structure to be constructed 425' from the Transit Street (Salmon) and 6.3' to 7.5' from the south property line.
- LU 07-105731 DZ CU: Withdrawn design review and conditional use review to keep two modular classroom buildings on the campus of Lincoln High School.
- LU 07-182752 DZ: Design review approval for a new structure to house two batting cages.
- LU 16-130866 DZM – Design Review approval for two replacement modular classrooms.

Agency Review: A “Notice of proposal in Your Neighborhood” was mailed **July 17, 2019**. The following Bureaus have responded with no issue or concerns:

- Bureau of Environmental Services – See Exhibit E.1
- Bureau of Transportation Engineering and Development Review - See Exhibit E.2
- Bureau of Parks, Urban Forestry Division - See Exhibit E.3 & E.6
- Site Development Review Section of BDS - See Exhibit E.4
- Plan Review Section of BDS - See Exhibit E.5

Neighborhood Review: A Notice of Proposal in Your Neighborhood was mailed on **July 17, 2019**. No written responses have been received from either the Neighborhood Association or notified property owners in response to the proposal.

ZONING CODE APPROVAL CRITERIA

(1) DESIGN REVIEW - CHAPTER 33.825

Section 33.825.010 Purpose of Design Review

Design review ensures that development conserves and enhances the recognized special design values of a site or area. Design review is used to ensure the conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district or area. Design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area. Design review is also used in certain cases to review public and private projects to ensure that they are of a high design quality.

Section 33.825.055 Design Review Approval Criteria

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design district guidelines.

Findings: The site is designated with design overlay zoning (d), therefore the proposal requires Design Review approval. Because of the site’s location, the applicable design guidelines are the Central City Fundamental and Goose Hollow Special Design Guidelines.

Goose Hollow District Design Guidelines and Central City Fundamental Design Guidelines

The Goose Hollow District is envisioned to be a predominantly urban residential, transit-oriented community located on the western edge of the Central City between Washington Park and Downtown Portland. When riding light rail through the West Hills tunnel to the Central City, it is the first neighborhood experienced before entering downtown Portland. The Urban Design Vision celebrates the sense of arrival from the

west at Jefferson Street Station and Collins Circle, and from the north at the Civic Stadium Station and Fire Fighter’s Park. This is done by integrating the history of the community with its special natural and formal (man-made) characteristics.

The Central City Fundamental Design Guidelines focus on four general categories. **(A) Portland Personality**, addresses design issues and elements that reinforce and enhance Portland’s character. **(B) Pedestrian Emphasis**, addresses design issues and elements that contribute to a successful pedestrian environment. **(C) Project Design**, addresses specific building characteristics and their relationships to the public environment. **(D) Special Areas**, provides design guidelines for the four special areas of the Central City.

Goose Hollow District Design Goals

The Goose Hollow District Design Goals are specific to the Goose Hollow District. These urban design goals and objectives are to:

- Enhance mixed-use, transit-oriented development around the light rail stations to make it a pedestrian-friendly station community.
- Provide open spaces to accommodate active public life.
- Strengthen connections to adjacent neighborhoods through light rail, bike and pedestrian access and assure a safe and pleasant bike/pedestrian environment.
- Preserve and enhance the community’s history and architectural character.

Central City Plan Design Goals

This set of goals are those developed to guide development throughout the Central City. They apply within all of the Central City policy areas. The nine goals for design review within the Central City are as follows:

1. Encourage urban design excellence in the Central City;
2. Integrate urban design and preservation of our heritage into the development process;
3. Enhance the character of the Central City’s districts;
4. Promote the development of diversity and areas of special character within the Central City;
5. Establish an urban design relationship between the Central City’s districts and the Central City as a whole;
6. Provide for a pleasant, rich and diverse pedestrian experience for pedestrians;
7. Provide for the humanization of the Central City through promotion of the arts;
8. Assist in creating a 24-hour Central City which is safe, humane and prosperous;
9. Ensure that new development is at a human scale and that it relates to the scale and desired character of its setting and the Central City as a whole.

Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project.

A2. Emphasize Portland Themes. When provided, integrate Portland-related themes with the development’s overall design concept.

A2-1. Recognize the Historic Tanner Creek Theme. Recognize the course of the historic Tanner Creek and emphasize the District’s connection with the Creek on site developments of 20,000 square feet or more, including and immediately adjacent to the historic course of the Creek. This guideline may be accomplished by any or all of the following:

- a. Exposing the Creek using water features and fountains; or
- b. Incorporating interpretive trails, artwork, murals or sculptures that describe and symbolize the relation between the district and the history of Tanner Creek.

Findings for A2 and A2-1: The proposal will meet these guidelines in the following ways:

- Lincoln High school has an existing Thomas Hardy sculpture of Herons (official bird of the City of Portland) which will be relocated along the SW 17th public accessway. This location will move a currently somewhat hidden element into an area of public circulation.
- The large tree canopy of the West Hills is a defining characteristic of Portland. This large canopy will continue onto the site with a row of conifers along SW Salmon Street, bringing this canopy into the Central City.
- Historic Benson Bubblers appear throughout the city. Lincoln High school currently has one on site in front of the concessions building. This Benson Bubbler will be preserved in its current location along with the concessions building.
- Lincoln High school is the oldest High School in Portland and as such it has a long history of Rosarians attending the school. These Rosarians are commemorated with plaques which will be relocated and reused along 14th Avenue. Additionally, an existing rose garden will be retained and enhanced.
- Tanner Creek is far too deep to expose. However, the project will recognize the history of Tanner Creek with the use of a paving pattern along the 17th easement that recalls the irregular planting bed configurations of the Chinese vegetable gardens that were located along Tanner Creek as it passed through the site. A stainless-steel trench drain that transitions to a stainless steel band will traverse the paving alluding to the creek. A series of stainless steel plates set along the trench drain will contain interpretative text depicting specifics about the Chinese gardens.

These guidelines have been met.

A3. Respect the Portland Block Structures. Maintain and extend the traditional 200-foot block pattern to preserve the Central City’s ratio of open space to built space. Where superblocks exist, locate public and/or private rights-of-way in a manner that reflects the 200-foot block pattern, and include landscaping and seating to enhance the pedestrian environment.

A7. Establish and Maintain a Sense of Urban Enclosure. Define public rights-of-way by creating and maintaining a sense of urban enclosure.

Findings for A3 and A7: The proposal will meet these guidelines in the following ways:

- The project establishes a north-south public pedestrian and bicycle path through the site within the original SW 17th right-of-way. Landscaping and multiple seating opportunities line the path.
- The termination of SW Main occurs mid-block on SW 18th. The block structure is marked by a mural (in collaboration with RACC) even though Main Street never extended through the site.
- The building located at the northwest corner holds this edge of the large site. The athletic field and landscaping and seating help define the edges along Salmon and 14th.

These guidelines have been met.

A4. Use Unifying Elements. Integrate unifying elements and/or develop new features that help unify and connect individual buildings and different areas.

A5. Enhance, Embellish, and Identify Areas. Enhance an area by reflecting the local character within the right-of-way. Embellish an area by integrating elements in new development that build on the area’s character. Identify an area’s special features or qualities by integrating them into new development.

A5-1. Strengthen the Identity of the Civic Stadium Station Area. This guideline may be accomplished by any or all of the following:

- a. Integrating the Civic Stadium and Multnomah Athletic Club into the station area. This can be done by making their ground level street walls more pedestrian-friendly.
- b. Incorporating the history of the Stadium and Tanner Creek in the form of art work, murals, and other design features;
- c. Strengthening the neighborhood focal point located within the station area. Street trees, awnings, lighting and other amenities can emphasize pedestrian connections to the focal point (the Park, Plaza/Station). Buildings can orient their openings and entries towards the focal point to create a sense of enclosure and enhance the sense of entry into the District; or
- d. Using architectural vocabulary and materials that maintain continuity with the existing developments and add to the character of the station area.

A5-2. Strengthen the Identity of the Jefferson Street Station Area. This guideline may be accomplished by any or all of the following:

- a. Integrating Lincoln High School and the First United Methodist Church into the Jefferson Street Station area by providing pedestrian-friendly treatment along the surrounding streets. Streets lacking the pedestrian-friendly treatment of the light rail alignment can use elements such as street trees, landscaping, street furniture, art work, awnings, seating, special lighting and textured paving to improve the pedestrian environment;
- b. Recognizing the historic “Goose” theme and incorporating it in projects within the station area, where appropriate, in the form of art work, symbols or other design features;
- c. Strengthening the neighborhood focal point located in the station area. Arcades, awnings and/or balconies can provide pedestrian scale along all developments facing the Circle and can enhance pedestrian access to the Circle. To emphasize the Circle’s importance as a focal point adjacent development can orient their entrances and ground level windows towards the Circle and garage entries can be avoided on the streets fronting it where feasible;
- d. Orienting buildings around the public plaza to create an enclosed public place and providing seating and other amenities to ensure safety and convenient pedestrian access; or
- e. Using architectural vocabulary and materials that maintain continuity with existing developments and add to the character of the station area.

A5-5. Incorporate Water Features. Incorporate water features or water design themes that enhance the quality, character, and image of the Goose Hollow District.

C4. Complement the Context of Existing Buildings. Complement the context of existing buildings by using and adding to the local design vocabulary.

Findings for A4, A5, A5-1, A5-2, A5-5 and C4: The Lincoln High School site is not located within the Civic Stadium Station area per the map on page 13 of the Goose Hollow District Design Guidelines. However, the project does have a stronger relationship to the Civic Stadium Station Area given its close proximity rather than the Jefferson Street Station Area, which it lies within but is several blocks away and around a corner. That said, the project strengthens the identity of both stations and meets these guidelines in the following ways:

- The project will maintain the consistent streetscape (paving, street trees, furnishings, etc.) that has been established for this portion of the Central City. The street trees along the eastern portion of Salmon and the northern portion of 14th will be a bit more sporadic given conflicts with the bus drop-off/pick-up and the heritage tree at the NE corner. However, a consistent row of trees is located at the back of the sidewalk along Salmon to ensure consistent canopy along the streetscape.
- The SW 17th pedestrian and bike path and plaza will be a shared public space and will serve as a unifying element. The paving pattern and trench drain details harkens back to the Chinese garden plots that once occupied the site along Tanner Creek.
- Numerous existing features will be maintained and renewed including the Thomas Hardy “Heron” sculpture, bronze plaques from the existing Trimet art wall, Benson Bubbler, as well as various commemorative planting and paving elements.
- Canopies (an established downtown design feature) will protect pedestrians and provide human scale along portions of SW Salmon and SW 18th.
- The existing fabric of buildings includes primarily modern buildings such as the Zion Lutheran Church (Pietro Belluschi), the Multnomah Athletic Club (ZGF) and the addition to Timbers Stadium (Allied Works). Lincoln High School will use materials and colors that are common to this area (precast concrete, brick colored UHPC panels, large areas of glass). The design vocabulary will be primarily modern and will be expressed in simple/minimal detailing. Verticality and two different colors of UHPC panels provide for a modern character while recognizing the design character of some of Portland’s most beloved historic high school buildings.

These guidelines have been met.

A5-6. Incorporate Works of Art. Incorporate works of art or other special design features that increase the public enjoyment of the District.

A8. Contribute to a Vibrant Streetscape. Integrate building setbacks with adjacent sidewalks to increase the space for potential public use. Develop visual and physical connections into buildings’ active interior spaces from adjacent sidewalks. Use architectural elements such as atriums, grand entries and large ground-level windows to reveal important interior spaces and activities.

B1-1. Provide Human Scale to Buildings along Walkways. Provide human scale and interest to buildings along sidewalks and walkways.

B1-2. Orient Building Entries to Facilitate Transit Connections. Orient primary building entries at pedestrian circulation points which conveniently and effectively connect pedestrians with transit services.

Findings for A5-6, A8, B1-1 and B1-2: The proposal will meet these guidelines in the following ways:

- Numerous active uses have been located along the sidewalk level of the building in order to create a vibrant streetscape. Major school elements that will be in active use at all times of the day and night will be visible from outside the building, such as the commons and the library. Although the 1st floor level drop below the sidewalk level on the southern half of SW 18th, chiro and band rooms that are interesting to view from above have been located here, with the ceiling elements (lighting, ducting and sound insulation for music) carefully considered so as not to block views. While these uses do not

have individual entrances for security purposes, large and ample windows are provided to allow for views into very active learning spaces.

- The project provides human scale and interest through the use of articulation in the façade, as well as the texture of the façade panels themselves. The articulation occurs with the sawtooth windows flanking the Main Street mural. There is also articulation of the façade at the ground floor at the corner of SW 18th and Salmon, which includes canopies, as well as at the commons Plaza. Further up the façade the articulation of the stairs extends to the top of the building. The recessed areas at the entry and commons plaza brings the scale of the building down and provides a welcoming gesture to the pedestrian. The ultra-high-performance concrete panels have a ribbed texture to them that further breaks down the scale of the façade elements.
- The main entry to the building is oriented to SW Salmon Street at SW 17th, allowing access to the Max line at SW King and Bus stops on SW Salmon.
- There are several areas where the building and site will incorporate works of art. The most significant new contribution will be a mural (in conjunction with RACC) that will be integrated into building wall at Main street termination. Various works of art from the existing high school site will be relocated and reused, including portions of the existing Trimet art wall (primarily the bronze plaques created by former students). These plaques will be deconstructed and relocated to another location on site. PPS will also maintain and relocate the existing Thomas Hardy “Heron Sculpture” to a more prominent place on the site in the SW 17th pedestrian easement. Lincoln High School currently has a Bart Simpson caricature etched into the sidewalk by Matt Groening. This will also be preserved and relocated.

These guidelines have been met.

A9. Strengthen Gateways. Develop and/or strengthen gateway locations.

A9-1. Provide a Distinct Sense of Entry and Exit. When developing at gateway locations, provide a distinct sense of entry and exit that relates to the special qualities of an area.

C7. Design Corners that Build Active Intersections. Use design elements including, but not limited to, varying building heights, changes in façade plane, large windows, awnings, canopies, marquees, signs and pedestrian entrances to highlight building corners. Locate flexible sidewalk-level retail opportunities at building corners. Locate stairs, elevators, and other upper floor building access points toward the middle of the block.

Findings for A9, A9-1 and C7: The site is not a designated gateway, however, the intersection of SW Salmon & SW 18th mark an active and important district focal point as the West Hills meet the Central City. As discussed during the prior Design Advice Requests (DAR) the building design for this corner is therefore critical in both activating and establishing a gateway feature at this intersection.

Two significant architectural elements will activate the corner of SW 18th and Salmon: At the ground floor, the Media Center (Library) will provide activity and visual interest. The use of this space frequently extends into the evening, resulting in a sense of activity that will extend beyond the school day. On floors 2-6, a large stair tower is located on the corner. This stair will be actively used as one of the primary ways students and teachers will move up and down through the building multiple times per day. Both the stair and the Media Center are enclosed in glass that wraps around the corner of the building,

creating the moment of the greatest transparency for the entire project. These two elements create a response to the “Flatiron” corner of the Butler Block mixed-use project planned for the north side of SW 18th and Salmon. Together, the two buildings will define the threshold between the West Hills and the Central City.

Two architectural elements will activate the corner of SW 17th and Salmon; the main entry to the building has been located at this corner. The entry is defined by a large, transparent recessed porch, exterior steps and seating elements. The main entry corner is emphasized with a large, raised-letter sign.

These guidelines have been met.

B1. Reinforce and Enhance the Pedestrian System. Maintain a convenient access route for pedestrian travel where a public right-of-way exists or has existed. Develop and define the different zones of a sidewalk: building frontage zone, street furniture zone, movement zone, and the curb. Develop pedestrian access routes to supplement the public right-of-way system through superblocks or other large blocks.

B2. Protect the Pedestrian. Protect the pedestrian environment from vehicular movement. Develop integrated identification, sign, and sidewalk-oriented night-lighting systems that offer safety, interest, and diversity to the pedestrian. Incorporate building equipment, mechanical exhaust routing systems, and/or service areas in a manner that does not detract from the pedestrian environment.

B3. Bridge Pedestrian Obstacles. Bridge across barriers and obstacles to pedestrian movement by connecting the pedestrian system with innovative, well-marked crossings and consistent sidewalk designs.

B5. Make Plazas, Parks and Open Space Successful. Orient building elements such as main entries, lobbies, windows, and balconies to face public parks, plazas, and open spaces. Where provided, integrate water features and/or public art to enhance the public open space. Develop locally oriented pocket parks that incorporate amenities for nearby patrons.

C6. Develop Transitions between Buildings and Public Spaces. Develop transitions between private development and public open space. Use site design features such as movement zones, landscape elements, gathering places, and seating opportunities to develop transition areas where private development directly abuts a dedicated public open space.

Findings for B1, B2, B3, B5 and C6: The proposal meets these guidelines in the following ways:

- The pedestrian system has been reinforced and enhanced around the building with canopies, lighting and covered entries. Along the site’s perimeter street trees and light in the furnishing zone, a wide sidewalk meeting the corridor standards and seating at the back of the sidewalk support the pedestrian experience. Through the site a series of pedestrian paths have been incorporated, with the largest being the north-south pedestrian and bike path that occurs with the vacated SW 17th. Other paths the run east-west and north-south on the opposite site of the field provide further connections through the large site. While not all the connections align with the original north-south vacated streets (16th and 15th specifically) due to the athletic field they still provide porosity through the site. The site will be fully secured with fences and gates. PPS has been working with City Staff (PBOT and BES) to ensure these pedestrian paths through the site are accessible to the public during non-school hours. The necessary documents to ensure public access (Public Access Easement and Memorandum of Understanding) are in their final stages. The drafts of these documents should be complete within the

next several days. A condition of approval has been added that requires both of these documents to be signed and recorded at the time of Certificate of Occupancy so that public access routes through the large site is attained once the paths are constructed.

- At the south end of the north-south pedestrian path is where the loading and service area occurs. The City worked with the applicant to come up with an urban response to the multimodal space. The maneuvering area for the loading and service vehicles has been defined using large rocks that define the pedestrian area from the vehicles. The continuous paving treatment helps to unify this space, while the seating rocks provide a function and safety measure.
- Mechanical elements have been thoughtfully screened, internalized, integrated or below-grade. Landscaping screens above grade mechanical elements near the parking area while perforated roof screens obscure the rooftop elements from street view. The service and loading area has been partially enclosed with a ground face CMU wall with trash and recycling set within a covered alcove.
- There are two significant transitions between the building and the public realm: The main entry incorporates a recessed porch, steps and seating elements that front onto an entry plaza that faces Salmon Street. Together, these elements serve as a transition from the public way to the front door of the building. The Student Commons on the east side of the building also incorporates a recessed dining porch that fronts onto an outdoor plaza. Furniture, lighting and seating walls will define the transition from the SW 17th pedestrian path to the Student Commons and Athletic Fields. At the Main Entry and the Commons, there will be large openings in the building wall to emphasize these as points of transition. In addition to this, both locations will include human scaled elements such as steps, furnishings and low seat walls.
- A variety of outdoor plaza and landscape areas occur on the site. A hardscape plaza and pedestrian path align with the vacated SW 17th street. Numerous elements within this area support the pedestrian including, a variety of seating, lighting, art and landscape planters. However, SW 17th has a BES sewer within it. In order for these pedestrian elements to be located within BES's sewer easement, an encroachment agreement is required. Therefore, a condition of approval has been included that requires the applicant obtain BES approval of all features that encroach into City of Portland sewer easements, which may include entering into a formal Encroachment Agreement with the City, to the satisfaction of BES.
- Other open spaces on the site are landscaped and allow for seating, gathering, resting or viewing the events within the athletic field.
- The main plaza and lawn areas are facing south/west allowing these spaces to receive sunlight. The main entry to the school is adjacent to the open space of the plazas and athletic fields. There are many different programs throughout the site – from athletics to academic, to neighborhood leisure, etc. A large tree canopy lines the site at SW Salmon broken by an overlook onto the athletic fields. The heritage tree at the corner of SW Salmon and SW 14th provides a shady spot to watch a game as well. Gardens along SW 14th provide further entrance. The major site elements of the practice field, the track and field and the commons plaza will receive active use at all times of day throughout the year.

As conditioned for the applicant to sign and record the Public Access Easement and Memorandum of Understanding prior to the Certificate of Occupancy and to obtain BES approval of all features that encroach into City of Portland sewer easements in SW 17th and 16th, these guidelines are met.

B7. Integrate Barrier-Free Design. Integrate access systems for all people with the building's overall design concept.

Findings for B7: The building entry and overall site has been designed to be fully accessible for people of all abilities. The design takes advantage of the slope across the site to incorporate steps and ramps in an inviting and thoughtful manner. *This guideline has been met.*

A1. Integrate the River. Orient architectural and landscape elements including, but not limited to lobbies, entries, balconies, terraces, and outdoor areas to the Willamette River and Greenway. Develop access ways for pedestrians that provide connections to the Willamette River and Greenway.

C1. Enhance View Opportunities. Orient windows, entrances, balconies and other building elements to surrounding points of interest and activity. Size and place new buildings to protect existing views and view corridors. Develop building façades that create visual connections to adjacent public spaces.

Findings for A1 and C1: The proposal meets these guidelines in the following ways:

- The site is over $\frac{3}{4}$ of a mile from the Willamette River and Greenway to the east. The proposal includes enhancing the abutting pedestrian connections through the site and surrounding area that one could utilize to access these important natural features.
- The building is designed to take advantage of views to significant features: The east side of the building has a significant amount of glass on its upper levels to take advantage of views over the athletic fields toward the Central City, Willamette River and Mt. Hood beyond. The stair tower located on SW 18th and Salmon has a significant amount of glass to take advantage of views to the West Hills and Timbers Stadium.
- There will be numerous views into educational and community spaces from the sidewalk level. Along SW 18th these spaces include the Library, Construction CTE Shop, Product Design Lab, Maker Space, Band Room, Choir Room, a Mural (in collaboration with RACC) and two different actively used stairways. Along SW Salmon, these spaces include the Library, Collaboration Lab, Main Administration waiting and reception and entry lobby. Along the pedestrian easement located in SW 17th, these spaces include the Student Commons, an actively used stairway and a community meeting room.
- The building is also oriented such that large open spaces inside the building are visible and accessible from the outdoor open space. The covered main entry has a significant amount of glass to create a visual connection with the entry plaza. The Student Commons also has a significant amount of glass and a covered outdoor dining porch that creates a strong visual connection to the Plaza, Pedestrian Easement and Athletic Fields.

These guidelines have been met.

C1-1. Integrate Parking. Design surface parking and parking garage exteriors to

visually integrate with their surroundings. This guideline may be accomplished by any or all of the following:

- a. Designing street facing parking garages to not express the sloping floors of the interior parking;
- b. Designing the sidewalk level of parking structures to accommodate active uses, display windows, public art or other features which enhance the structure's relationship to pedestrians; or
- c. Accommodating vending booths along sidewalks adjacent to parking facilities when active ground level uses are not possible.

C3-1. Locate Buildings to Provide for Future Infill on Surface Parking Areas.

Locate and shape buildings to provide for future infill development on surface parking areas.

Findings for C1-1 and C3-1: The surface parking lot has been situated in the southeast portion of the site to limit its visibility from the public streets and impact from the abutting properties. The parking lot is setback over 200' from SW 14th and behind the portable buildings of the teen/parent center with only a narrow one-way driveway accessing SW 14th. There is also a significant grade change that occurs between SW 14th & the parking lot, resulting in the parking area approximately 14' below the sidewalk level on SW 14th. Furthermore, a large portion of the parking lot is tucked underneath the grandstand. Landscaping within and particularly along the south edge, where large trees dominate the planters, soften the vehicle area.

Should the on-site parking not be needed in the future its proposed location does not preclude a future building or other non-vehicle development in its place.

These guidelines have been met.

B6. Develop Weather Protection. Develop integrated weather protection systems at the sidewalk-level of buildings to mitigate the effects of rain, wind, glare, shadow, reflection, and sunlight on the pedestrian environment.

B6-1. Provide Outdoor Lighting at Human Scale. Provide outdoor lighting at a human scale to encourage evening pedestrian activity.

C1-2. Integrate Signs and Awnings. Integrate signs and awnings to be complementary and respectful of a building's architecture. This guideline may be accomplished by any or all of the following:

- a. Placing signs and awnings to fit with and respect a building's architecture.
- b. Avoiding large, excessively illuminated or freestanding signs that contribute to visual clutter; or
- c. Integrating with a building's design an exterior sign program/system for flexible sidewalk level space that accommodates changing tenants.

C10. Integrate Encroachments. Size and place encroachments in the public right-of-way to visually and physically enhance the pedestrian environment. Locate permitted skybridges toward the middle of the block, and where they will be physically unobtrusive. Design skybridges to be visually level and transparent.

C12. Integrate Exterior Lighting. Integrate exterior lighting and its staging or structural components with the building's overall design concept. Use exterior lighting to highlight the building's architecture, being sensitive to its impacts on the skyline at night.

C13. Integrate Signs. Integrate signs and their associated structural components with the building's overall design concept. Size, place, design, and light signs to not dominate the skyline. Signs should have only a minimal presence in the Portland skyline.

Findings for B6, B6-1, C1-2, C10, C12 and C13: The proposal meets these guidelines in the following ways:

- The project includes canopies along SW 18th and SW Salmon. The canopies are at a sufficient depth (6' to 8') to provide weather protection and are well integrated into the building design by residing within window bays and using compatible materials (steel and glass).
- Additional covered areas at the main entry on SW Salmon and the outdoor dining located adjacent to student commons along the public path on SW 17th.
- In addition to the built weather protection, the large tree canopy on the site along SW Salmon and at the NE corner with the heritage tree will provide protection as well.
- As the project is a single use, the exterior signage will be minimal and will be limited to a two raised letter signs at the main entry facing SW Salmon and above the student plaza and commons facing the athletic field to the east. The non-illuminated signs will be comprised of 14" tall aluminum dimensional (1" thick) letters pin mounted to the building. The locations of the two signs are pedestrian oriented and help demarcate the main entry.
- Downward focused lighting will be integrated into canopies and soffits to highlight important building elements including the main building entry, the student commons and plaza and the main glass-enclosed stair towers located at the intersections of SW 18th and Salmon as well as SW 17th and Salmon. Wall mounted light in the loading area will provide security to these back-of-house inactive areas.
- Pedestrian scaled outdoor lighting will be provided throughout the site, including at the athletic fields and grandstands, along SW 17th, at the building entry and at the covered Commons Plaza. Pole lights (16' tall) will line primary pedestrian paths with 25' tall light poles in the parking area. A combination of other lighting elements throughout the site will ensure a safe environment for all. Approximately four 80' tall stadium lights will occur near the athletic and practice fields. An illumination study has been provided to show the lighting levels at the edges of these facilities where the public sidewalks and adjacent properties exist will not receive excessive glare or light spill over.

These guidelines have been met.

C2. Promote Quality and Permanence in Development. Use design principles and building materials that promote quality and permanence.

C5. Design for Coherency. Integrate the different building and design elements including, but not limited to, construction materials, roofs, entrances, as well as window, door, sign, and lighting systems, to achieve a coherent composition.

Findings for C2 and C5: The proposal meets these guidelines in the following ways:

- The exterior cladding material is primarily Ultra High-Performance Concrete (UHPC). An extremely durable and permanent material. The integral color is reminiscent of brick with light accents that recall historical brick and terra cotta school buildings in Portland. UHPC panels are individually formed and cast by hand by skilled craftsman. They have a handmade quality as opposed to a machine-made quality. The large panel sizes are appropriate to the scale

of the building while the ribbed detail provides a refined scale and movement on the façade. The panels will be faced-fixed with the exception of the ground level where the attachment will be behind the panel.

- The other building materials (fiberglass windows, precast concrete, box ribbed metal panel 20 gauge and ground face CMU) are durable and quality finishes.
- The use of a rich, but restrained material palette creates a cohesive and unified character to a complex building with many different parts and functions. The emphasis on verticality (panel orientation, window proportions and groupings, folding of façade elements, etc.) acts as a unifying element across the project. The differing heights of the different building volumes are unified through the use of a single material palette. The design uses a common system of proportions, recesses, folds, “sawtooth” shapes and expressions of stairs to create coherency on all sides. The sidewalk level of the project is consistently active and transparent but avoids being monotonous by creating a variety of experiences around the entire building and site.
- The existing portable buildings to be repurposed as the teen/parent center at the southeast end of the site were approved in 2016 through a Design Review and exhibit quality finishes and coherency. Their location is well below the sidewalk grade on SW 14th with significant landscape in between limiting their visibility from the street.
- The new site elements, variety of concrete and stone seating options, bike cover structures, fencing and gates and athletic field features all appears to be durable and of good quality finish.
- The metal lighting fixtures and signage complement the storefront system and other building finishes.

These guidelines have been met.

C8. Differentiate the Sidewalk-Level of Buildings. Differentiate the sidewalk-level of the building from the middle and top by using elements including, but not limited to, different exterior materials, awnings, signs, and large windows.

C9. Develop Flexible Sidewalk-Level Spaces. Develop flexible spaces at the sidewalk-level of buildings to accommodate a variety of active uses.

B4. Provide Stopping and Viewing Places. Provide safe, comfortable places where people can stop, view, socialize and rest. Ensure that these places do not conflict with other sidewalk uses.

Findings for C8, C9 and B4: The proposal meets these guidelines in the following ways:

- The ground floor of the building has a very generous floor-to-floor height (20 feet) resulting in a strong civic presence at the sidewalk level.
- The size of the windows on the ground floor are larger than the rest of the building, resulting in greater transparency, views into active learning spaces and heightened pedestrian experience at the sidewalk level.
- Canopies and precast concrete base (approximately 4’ high but variable with the slope of the sidewalk) are elements that create scale. “Sawtooth” configuration of windows at the Media Center, Music and Choir Rooms and Commons create scale and unique pedestrian experience at the sidewalk level.
- Consistent use of exterior cladding material from sidewalk level to upper level of building allows for cohesive expression of overall design.

- The canopies and setback niches created by the sawtooth footprint along SW 18th and Salmon and the benches and large entry plaza near the main entrance provide areas to stop, rest and socialize as do the numerous benches and seating cubes and seating pebbles that line the sidewalk, overlook and within the pedestrian path in SW 17th.

These guidelines have been met.

C11. Integrate Roofs and Use Rooftops. Integrate roof function, shape, surface materials, and colors with the building’s overall design concept. Size and place rooftop mechanical equipment, penthouses, other components, and related screening elements to enhance views of the Central City’s skyline, as well as views from other buildings or vantage points. Develop rooftop terraces, gardens, and associated landscaped areas to be effective stormwater management tools.

Findings: Lincoln High School is a large-scale building with varied roofline which helps reduce scale of building. The rooftop consists of PV panels over white roof, rock ballast at roof edges and three areas of green roof. All of the mechanical equipment is set within a perforated metal screen as tall as the equipment or setback. While the perforated screens allow the silhouette of the units to be perceived, they contain and unify the mechanical elements while not adding to the mass of the building. The mechanical screens made of materials that match color and panel modulation (joint pattern) of the building cladding. Rooftop access is not permitted due to security and student management concerns; therefore, roof top planted areas can be viewed, but not accessed. The green roof contributes to storm water management. The green roof location helps integrate the view of the building from the west hills.

This guideline has been met.

(2) MODIFICATIONS – CHAPTER 33.825

33.825.040 Modifications That Will Better Meet Design Review Requirements:

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the design review process. These modifications are done as part of design review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through design review may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

- A. **Better meets design guidelines.** The resulting development will better meet the applicable design guidelines; and
- B. **Purpose of the standard.** On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.

Modification #1 Transit Street Main Entrance – To not provide a building entry every 200’ along SW 18th that is unlocked during regular business hours and within 25’ of a Transit Street (33.130.242);

Purpose Statement: Locating the main entrance to a use on a transit street provides convenient pedestrian access between the use and public sidewalks and transit facilities, and so promotes walking and the use of transit.

Standard: 33.130.242, For portions of a building within the maximum building setback, at least one main entrance for each nonresidential tenant space on the ground floor must be within 25' of the transit street, allow pedestrians to both enter and exit the building, face the transit street or be at an angle of up to 45 degrees from the transit street. A minimum of one entrance is required for every 200 feet of building length and the main entrance(s) must be unlocked during regular business hours.

A. Better meets design guidelines. *The resulting development will better meet the applicable design guidelines; and*

Findings: The large, covered and setback main entrance to the school along SW Salmon better meets guideline C9 – Develop Flexible Sidewalk Level Spaces and C6 Develop Transitions Between Buildings and Public Spaces. The generous size of the entry itself and spill out area in front of it supports the large number of students accessing the building and surrounding functions (track & field, bus drop-off/pick-up and bike parking. Seating within the area supports other activities of students during the day.

B. Purpose of the standard. *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

Findings: SW 18th Avenue is classified as a Regional Transit / Major Transit Priority, and SW Salmon Street is classified as a Major Transit Priority in the Portland 2035 Transportation System Plan. Security concerns require that the proposed LHS building has a single main entrance on SW Salmon Street at SW 17th Avenue located approximately 32' from the property line. This location allows for easy access to busses that stop along SW Salmon Street and is adjacent to the site amenities like bike parking and the athletic fields. This location also allows for a continuous flow from the main entrance to the common areas of the school. The main entrance will remain unlocked during the school day. After first period starts, visitors must enter into the vestibule and check in at the front desk before continuing into the school. There is a door into the Health Clinic on SW 18th Avenue. This door will also remain unlocked during business hours. The only other door along the street is an egress door at the south end of SW 18th, thereby not meeting the one entry every 200'. While a number of the elements within this standard are not being met, the large, prominent main entry on SW Salmon is situated to support convenient pedestrian access from the bus drop off area along Salmon, the bike parking to the east of the entry and visible from Salmon where the Civic Station stop occurs. The health clinic supports pedestrian access along SW 18th also to/from the Civic Station and bus stops along SW 18th.

Therefore, this Modification merits approval.

Modification #2 Required Building Line – For the building to not extend to the street lot lines along SW Salmon and 14th for 75% of these frontage (33.510.215)

Purpose Statement: The required building line standards ensure that buildings in certain parts of the Central City are built to the sidewalk's edge unless landscaping or an extension of the sidewalk is provided. The standards support the street and

development character objectives of the Central City 2035 Plan by creating diverse street character, promoting active uses, pedestrian movement, and opportunities for stopping and gathering. Extensions of the sidewalk may incorporate trees, landscape planters, groundcover, and areas for stormwater management between the building and the sidewalk

Standard: Section 33.510.215, the building must extend to the street lot line along at least 75% of the length of the street lot line; or set back at least 6 feet from the street lot line along at least 75% of the length of the street lot line. The space between the building and the street lot line must be landscaped and exterior walls of buildings must be at least 15' high measured from the finished sidewalk at the building's edge

A. Better meets design guidelines. *The resulting development will better meet the applicable design guidelines; and*

Findings: Although there is no building along a large portion of the SW Salmon and SW 14th frontage, the extensive and diverse landscape and pedestrian amenities (pathways and seating) better meet guidelines B4 - Provide Stopping and Viewing Places and B5 - Make Plazas, Parks and Open Space Successful.

B. Purpose of the standard. *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

Findings: Because of the nature of the site (large, rectangular with a heritage tree at the NE corner) and programmatic requirements of a school (large track and athletic field), this standard will not be met along SW Salmon and SW 14th. The building holds the street lot line at the western end of SW Salmon Street, however along the remainder of the street LHS proposes a series of large conifers on the school property with a break midway through the block. This break allows for an overlook to the athletic field and track at the 50-yard line. A series of benches and seating areas occur within the landscape setback and in the overlook. This active use along SW Salmon will engage the pedestrian as well as provide an edge to the sidewalk. The layered and varied landscaping also support the desired character of the Boulevard Street classification of Salmon. Along SW 14th, the open space with large trees, pedestrian paths and seating allow for pedestrian movement, opportunities for gathering and watching the activities on the track and field.

Therefore, this Modification merits approval.

Modification #3 Ecoroof – To reduce the ecoroof area requirement from 60% to 28% (PZC 33.510.243).

Purpose Statement: Ecoroofs provide multiple complementary benefits in urban areas, including stormwater management, reduction of air temperatures, mitigation of urban heat island impacts, air quality improvement, urban green spaces, and habitat for birds, plants and pollinators. The standards are intended to:

- Maximize the coverage of ecoroofs;
- Allow for the placement of structures and other items that need to be located on roofs; and
- Support the architectural variability of rooftops in the Central City.

Standard: Section 33.510.243, Ecoroofs must cover 100% of the building roof area, except that up to 40% of the building roof area can be covered typical rooftop elements like mechanical equipment, solar panels, etc.

A. Better meets design guidelines. *The resulting development will better meet the applicable design guidelines; and*

Findings: The proposal with generous accessible landscape areas that include a variety of tree and plant species and amenities better meets guideline B5 - Make Plazas, Parks & Open Space Successful.

B. Purpose of the standard. *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

Findings: The intent of the code is to maximize the use of ecoroofs to achieve multiple benefits for sites in the Central City that typically develop lot line to lot line. Lincoln High School is a large site and the project will not be developed from lot line to lot line. Therefore, it is an atypical site within the Central City. The unique nature of this large site under single public agency ownership opens the door to potential solutions that will equally meet the intent of the code while also meeting the safety, operational and maintenance requirements of PPS.

The proposed building design has a roof area of 98,029 SF. The code requirement is for 58,817 SF of ecoroof (60% of the roof area). Ancillary structures to be located on the site (bike shelters, teen parent center, existing concessions building and press box) are all below the size threshold for the ecoroof requirement and are therefore not included in the total. The proposal is to trade a portion of the required ecoroof for dedicated landscaped areas on site on a square-foot per square-foot basis. It should be noted that the CX zoning of the site does not require a minimum landscape area, therefore the proposed mitigation landscaping is above and beyond what the zoning code requires. PPS does not desire to install ecoroof below the solar panels on the southern portions of the roof due to concerns related to cost, maintenance and energy efficiency.

The proposed building design has three areas of ecoroof:

- Area 1 is located on the roof over the second floor. It has an area of 9,934 SF. This area has been chosen because it is immediately outside of the third-floor classroom windows. This area is highly visible from the south-facing classrooms and will provide a great visual amenity for the school. It is also directly accessible from the third floor for PPS maintenance staff only. PPS prohibits roof access for students for life safety and security reasons. The roof will not be accessible to the general school population.
- Areas 2 and 3 are located on top of the 6th floor. They have areas of 1,862 SF and 4,447 SF respectively. They have been located here to mitigate the views of the building roof from the west hills.
- The proposed building design has 16,243 total SF of ecoroof (28% of the requirement).

The proposal replaces ecoroof with dedicated landscaped areas on site as follows:

- Areas A and B are located along SW Salmon Street. They have areas of 10,555 SF and 3,176 SF respectively. These areas will contain large canopy trees and groundcover in compliance with the BES Stormwater Manual.

- Area C is located on the corner of SW 14th and SW Salmon. It has an area of 20,972 SF. This area will retain several large trees including a designated heritage tree. Additionally, this area will contain wildlife habitat supportive elements and groundcover in compliance with the BES Stormwater Manual.
- Area D is located along SW 14th in the SE corner of the site. It has an area of 12,747 SF. This area will contain large canopy trees as well as wildlife habitat supportive landscaping and groundcover in compliance with the BES Storm Manual.
- The total amount of dedicated landscaping proposed as ecoroof replacement is 47,450 sf.

The combined total areas of proposed ecoroof and proposed dedicated landscape is 63,693 sf (108% of the requirement). This amount exceeds the overall code requirement for ecoroof. The proposal meets the intent of the code to provide the following benefits in urban areas:

Stormwater Management – The properly designed and landscaped areas on natural grade have an equivalent capacity for the management of stormwater to an ecoroof.

Reduction of Air Temperatures & Mitigation of Urban Heat Island Impacts - The project will utilize either a highly reflective mortar faced insulation or white rock ballast in place of the ecoroof in the areas specified on the exhibit. White or highly reflective roofs, when combined with other measures, can provide two benefits; reduction of air temperatures and mitigation of urban heat island. The dedicated areas of landscaping on natural grade will provide additional reduction of air temperatures.

Air Quality Improvement - Landscaped areas on natural grade can have a greater variety and significantly larger plants and trees than can be accommodated on an ecoroof. Larger plants and trees have a greater capacity of improving air quality than an ecoroof.

Urban Green Spaces - The landscaped areas on natural grade will be accessible to the public as well as the school population. Per PPS operational and security policies, the ecoroof will not be accessible to anyone other than maintenance staff. This prohibition of access means that the ecoroof is not truly an urban green space. The proposed modification for some of the ecoroof on-grade provides truly urban green space because it is accessible and visible to the urban population.

Habitat for Birds, Plants and Pollinators - Landscaped areas on natural grade can have a greater variety and significantly larger plants and trees than can be accommodated on an ecoroof. The larger plants and trees proposed include a wide variety of species providing a greater capacity for providing habitat for birds, plants and pollinators than an ecoroof. Large “nurse logs” within landscape area C will support smaller habitat and wildlife.

Therefore, this Modification merits approval.

Modification #4 Parking Lot Landscape – To not provide the perimeter landscaping along a portion of the southern edge of the surface parking lot and reduce the amount of required interior parking lot landscaping (33.266.130.G).

Purpose Statement: The parking area layout standards are intended to promote safe circulation within the parking area, provide for the effective management of stormwater runoff from vehicle areas, and provide for convenient entry and exit of vehicles. The setback and landscaping standards:

- Improve and soften the appearance of parking areas;
- Reduce the visual impact of parking areas from sidewalks, streets, and especially from adjacent residential zones;
- Provide flexibility to reduce the visual impacts of small residential parking lots;
- Direct traffic in parking areas;
- Shade and cool parking areas;
- Reduce the amount and rate of stormwater runoff from vehicle areas;
- Reduce pollution and temperature of stormwater runoff from vehicle areas; and
- Decrease airborne and waterborne pollution.

Standard: Section 33.266.130.G, requires a 5' deep landscape area planted to the L2 standard along the edges of parking areas abutting a street or adjacent property and interior parking areas must provide 45 SF of landscape area to the P1 standard per parking space.

A. Better meets design guidelines. *The resulting development will better meet the applicable design guidelines; and*

Findings: The proposal better meets guidelines B2 - Protect the Pedestrian in that reducing and combining the landscape at the perimeter allows a 6' wide pedestrian path through the parking area providing a safe path of travel for people.

B. Purpose of the standard. *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

Findings: The proposal includes an extensive landscape area between the surface parking lot and the building to the south, with the exception of a small section (~35') at the western end. The parking area is sunken 2' to 16' below the property to the south. The grade difference is approximately 3' at the location without the landscape perimeter. Given that the southern perimeter contains an average 10' wide landscape perimeter with a dense planting of tall conifers with an off-set in grades the purpose of the standard is met by reducing and softening the appearance of the vehicle area.

Regarding the interior parking lot landscape requirement, a total of 3,150 SF is required (based on the 70 spaces provided) with 1,776 SF provided. The interior landscaping is challenging to meet due the grandstands that cover over half of the spaces and the desire to provide pedestrian paths through portions of the parking area. The grandstands provide shade and cooling for the parking area while extra deep and densely planted perimeter and interior landscaping also provides shade and stormwater treatment meeting the purpose of the regulations.

Therefore, this Modification merits approval.

(3) ADJUSTMENTS – CHAPTER 33.805

33.805.010 Purpose

The regulations of the zoning code are designed to implement the goals and policies of the Comprehensive Plan. These regulations apply citywide, but because of the city's diversity, some sites are difficult to develop in compliance with the regulations. The adjustment review process provides a mechanism by which the regulations in the zoning code may be modified if the proposed development continues to meet the intended purpose of those regulations. Adjustments may also be used when strict application of the zoning code's regulations would preclude all use of a site. Adjustment reviews provide flexibility for unusual situations and allow for alternative ways to meet the purposes of the code, while allowing the zoning code to continue to provide certainty and rapid processing for land use applications.

Adjustment #1 Bike Parking – To reduce the width (from 2' to 18") and number of required long-term bike parking spaces (from 280 to 92) (PZC 33.266.210).

33.805.040 Approval Criteria

Adjustment requests will be approved if the review body finds that the applicant has shown that approval criteria A through F have been met:

- A.** Granting the adjustment will equally or better meet the purpose of the regulation to be modified.

Purpose Statement: Bicycle parking is required for most use categories to encourage the use of bicycles by providing safe and convenient places to park bicycles. These regulations ensure adequate short and long-term bicycle parking based on the demand generated by the different use categories and on the level of security necessary to encourage the use of bicycles for short and long stays. These regulations will help meet the City's goal that 10 percent of all trips be made by bicycle.

Standard: A 2' x 6' area must be provided for each required bicycle parking space and Schools with grade 6-12 require a space per 4 classrooms and be secured through a number of options.

Findings: The minimum required long-term bicycle parking spaces for Lincoln High School is 4 per classroom. The new Lincoln High school will have 70 classrooms making the minimum required long-term bicycle parking spaces 280. The project to provide 172 bicycle parking spaces (140 covered). This is providing the entire amount of covered parking required and a reduction of the uncovered spaces. In total, 172 spaces represents 2.5 spaces per classroom.

The reason for low ridership at Lincoln High school is illustrated in the diagram showing the Lincoln catchment area. For the majority of the residential areas of the catchment, there is significant topography and minimal bicycle infrastructure, making riding to this particular school difficult and unsafe. The zip code map shows the majority of the students are concentrated in these hilly residential areas that are not served well by bicycle lanes. An additional challenge to commuting to school via bicycle that is unique to Lincoln High School is that the available athletic facilities on site are limited to one field. This means that most Lincoln High School student athletes must travel to participate in athletics all over the city, even all the way to Delta Park. These activities extend into late evening hours making it quite impractical to carry athletic equipment by bicycle and unsafe to bike home late at night. The proposed number of bicycle parking spaces will be more than adequate to serve the current needs and will provide for a significant amount of growth in bicycle use. Lincoln High School is unique in its' catchment area, which is large and varied

in topography. The downtown location of the school is well served by public transit.

The purpose of bicycle parking is to provide in a safe and convenient location to encourage bicycling. Further it ties the minimum required amounts to the City's bicycle mode split goals. At the time of the adoption of the current bicycle parking standards the City's target bicycle mode split goal was 10%; today's target mode split goal is 25%. Summary zip code information for student and staff in the Lincoln High School catchment area show large concentrations of students and staff living in areas with significant elevations which make bicycle access difficult. Given topography issues, PBOT-ATS (Active Transportation and Safety Division) is supportive of the reduction as it meets the purpose statement by providing bicycle parking spaces for approximately 10% of the Lincoln High School population. Further, PBOT-ATS believes that the applicant's proposal to provide the required 140 covered spaces meets the purpose statement's intent of ensuring safe and convenient bicycle parking spaces to encourage greater use of bicycles. Bicycle parking that is sheltered from the elements is essential when leaving bicycles outside during the duration of a school day (6+ hours).

The project is also seeking an adjustment to the width of the bicycle parking space 2' to 18" for the staple racks throughout the site. This will allow for more bike to be parked under each prefabricated cover. The staple racks require that the two bicycles are placed such that the handlebars are on the opposite ends of the rack, therefore eliminating handlebar and pedal conflicts and meeting the intent for convenient and safe bike parking.

The bike parking will be located within the fenced campus. Gates around the site will be locked during all school hours (8 am -5 pm), however, they will be opened to allow public access during non-school hours and on weekends. Given that the bikes will meet the security requirement when the use that generates the need is in operation meets the purpose of the regulation. When the campus operates as public open space during non-school hours there is no bike parking requirement for that use.

For these stated reasons, the approval criterion is met.

- B.** If in a residential, CI1, or IR zone, the proposal will not significantly detract from the livability or appearance of the residential area, or if in an OS, C, E, I, or CI2 zone, the proposal will be consistent with the classifications of the adjacent streets and the desired character of the area.

Findings: The proposed Adjustment to the bike parking regulations does not conflict with the street classifications of the surrounding rights-of-way nor does it lessen the desired character of the Goose Hollow neighborhood. There is still an extensive amount provided in a manner than meets the intent of the code.

For these stated reasons, the approval criterion is met

- C.** If more than one adjustment is being requested, the cumulative effect of the adjustments results in a project which is still consistent with the overall purpose of the zone.

Findings: *Only 1 Adjustment is being requested, therefore this criterion does not apply.*

- D.** City-designated scenic resources and historic resources are preserved.

Findings: *There are no City designated scenic or historic resourced on the site, therefore this criterion does not apply.*

- E.** Any impacts resulting from the adjustment are mitigated to the extent practical; and

Findings: *No impacts resulting from the Adjustment have been identified therefore this criterion does not apply.*

- F.** If in an environmental zone, the proposal has a few significant detrimental environmental impacts on the resource and resource values as is practicable;

Findings: *The site is not within an environmental zone, therefore this criterion does not apply.*

DEVELOPMENT STANDARDS

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all requirements of Title 11 can be met, and that all development standards of Title 33 can be met or have received an Adjustment or Modification via a land use review, prior to the approval of a building or zoning permit.

CONCLUSIONS

The design review process exists to promote the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. The proposal meets the applicable design guidelines and modification and adjustment criteria and therefore warrants approval.

TENTATIVE STAFF RECOMMENDATION

(May be revised upon receipt of new information at any time to the Design Commission decision)

Staff recommends approval of the Design Review for the new 6-story, 102' tall school building, athletic field and track, other smaller buildings and structures, plaza and north-south pedestrian and bike path and other site elements including a series of internal walkways landscaping, gates and fences in the Goose Hollow subdistrict of the Central City Plan District

Staff recommends approval of the following Modifications:

1. *Transit Street Main Entrance* – To not provide a building entry every 200' along SW 18th that is unlocked during regular business hours and within 25' of a Transit Street (33.130.242).

- 2. *Required Building Line* – For the building to not extend to the street lot lines along SW Salmon and 14th for 75% of these frontage (33.510.215).
- 3. *Ecoroof* – To reduce the ecoroof area requirement from 60% to 28% (PZC 33.510.243).
- 4. *Parking Lot Landscape* – To not provide the perimeter landscaping along a portion of the southern edge of the surface parking lot and reduce the amount of required interior parking lot landscaping (33.266.130.G).

The following Adjustment is also requested (PZC 33.805.040):

- 1. *Bike Parking* – To reduce the width (from 2’ to 18”) and number of required long-term bike parking spaces (from 280 to 92) (PZC 33.266.210)

Staff recommends the following conditions of approval:

- A. As part of the building permit application submittal, the following development-related conditions (B through E) must be noted on each of the 4 required site plans or included as a sheet in the numbered set of plans. The sheet on which this information appears must be labeled "ZONING COMPLIANCE PAGE - Case File LU 19-119554 DZM AD". All requirements must be graphically represented on the site plan, landscape, or other required plan and must be labeled "REQUIRED."
- B. At the time of building permit submittal, a signed Certificate of Compliance form (<https://www.portlandoregon.gov/bds/article/623658>) must be submitted to ensure the permit plans comply with the Design/Historic Resource Review decision and approved exhibits.
- C. No field changes allowed.
- D. Prior to the Certificate of Occupancy, the applicant must sign and record the Public Access Easement and Memorandum of Understanding.
- E. Prior to building permit issuance, the applicant must obtain BES approval of all features that encroach into City of Portland sewer easements, which may include entering into a formal Encroachment Agreement with the City, to the satisfaction of BES.

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Procedural Information. The application for this land use review was submitted on February 12, 2019, and was determined to be complete on June 10, 2019.

Zoning Code Section 33.700.080 states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on February 12, 2019.

ORS 227.178 states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case,

the applicant waived the 120-day review period, as stated with Exhibit #G.3. Unless further extended by the applicant, **the 120 days will expire on: 6/9/2020**

Some of the information contained in this report was provided by the applicant.

As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the recommendation of the Bureau of Development Services with input from other City and public agencies.

This report is not a decision. The review body for this proposal is the Design Commission who will make the decision on this case. This report is a recommendation to the Design Commission by the Bureau of Development Services. The review body may adopt, modify, or reject this recommendation. The Design Commission will make a decision about this proposal at the hearing or will grant a continuance. Your comments to the Design Commission can be mailed, c/o the Design Commission, 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201 or faxed to 503-823-5630.

You will receive mailed notice of the decision if you write a letter received before the hearing or testify at the hearing, or if you are the property owner or applicant. You may review the file on this case by appointment at our office at 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201. Please call the file review line at 503-823-7617 to schedule an appointment.

Appeal of the decision. The decision of the Design Commission may be appealed to City Council, who will hold a public hearing. If you or anyone else appeals the decision of the Design Commission, City Council will hold an evidentiary hearing, one in which new evidence can be submitted to them. Upon submission of their application, the applicant for this land use review chose to waive the 120-day time frame in which the City must render a decision. This additional time allows for any appeal of this proposal to be held as an evidentiary hearing.

Who can appeal: You may appeal the decision only if you write a letter which is received before the close of the record for the hearing, if you testify at the hearing, or if you are the property owner/applicant. **Appeals must be filed within 14 days of the decision. An appeal fee of \$5,000.00 will be charged (one-half of the application fee for this case).**

Additional information on how to file and the deadline for filing an appeal will be included with the decision. Assistance in filing the appeal and information on fee waivers are available from the Bureau of Development Services in the Development Services Center, 1900 SW Fourth Ave., First Floor. Neighborhood associations recognized by the Office of Neighborhood Involvement may qualify for a waiver of the appeal fee provided that the association has standing to appeal. The appeal must contain the signature of the Chair person or other person authorized by the association, confirming the vote to appeal was done in accordance with the organization's bylaws.

Neighborhood associations, who wish to qualify for a fee waiver, must complete the Type III Appeal Fee Waiver Request for Organizations Form and submit it prior to the appeal deadline. The Type III Appeal Fee Waiver Request for Organizations Form contains instructions on how to apply for a fee waiver, including the required vote to appeal.

Recording the final decision.

If this Land Use Review is approved the final decision will be recorded with the Multnomah County Recorder. The applicant, builder, or a representative does not need to record the final decision with the Multnomah County Recorder.

For further information on your recording documents please call the Bureau of Development Services Land Use Services Division at 503-823-0625.

Expiration of this approval. An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Zone Change and Comprehensive Plan Map Amendment approvals do not expire.

Applying for your permits. A building permit, occupancy permit, or development permit must be obtained before carrying out this project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed here.
- All applicable development standards, unless specifically exempted as part of this land use review.
- All requirements of the building code.
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the city.

The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).

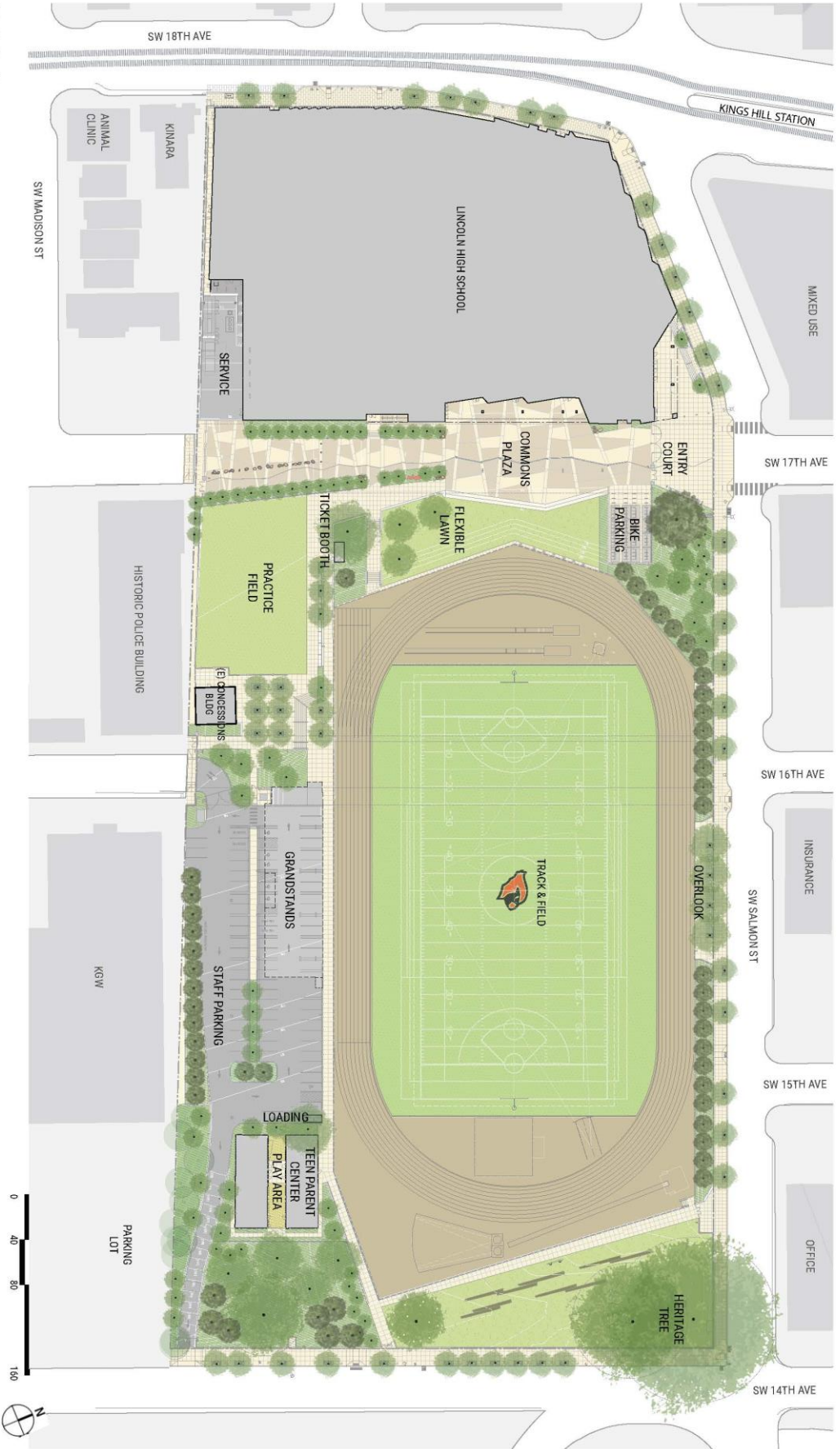
Staci Monroe
July 22, 2019

EXHIBITS – NOT ATTACHED UNLESS INDICATED

- A. Applicant’s Statement
 1. Meeting minutes dated 11/13/18 regarding Urban forestry preliminary discussion of the heritage and street trees.
 2. Project Narrative and approval criteria responses dated 4/5/19
 3. Kittleson Bike Parking Demand Analysis dated 5/9/19
 4. Interface Engineering memo dated 5/29/19 regarding compliance with Oregon Energy Code
 5. HHPR Stormwater Report dated 6/4/19
 6. HHPR Easement Encroachment memo dated 6/5/19
 7. Project Narrative and approval criteria responses dated 6/7/19
 8. Drawing Set dated 6/7/19
 9. Drawing Set dated 2/12/19
 10. PPS Bike Parking memo dated 6/14/19

11. Project Narrative and approval criteria responses dated 7/10/19
12. Project Narrative and approval criteria responses dated 7/16/19
13. Appendix sheets dated 8/1/19
- B. Zoning Map (attached)
- C. Plan & Drawings
 1. Through 125 (C.1, C.5-C.8 attached)
- D. Notification information:
 1. Request for response
 2. Posting letter sent to applicant
 3. Notice to be posted
 4. Applicant's statement certifying posting
 5. Mailed notice dated July 11, 2019
 6. Mailing list dated July 11, 2019
 7. Revised Mailed notice dated July 17, 2019
 8. Revised Mailing list dated July 17, 2019
- E. Agency Responses:
 1. Bureau of Environmental Services
 2. Bureau of Transportation Engineering and Development Review
 3. Bureau of Parks, Urban Forestry Division
 4. Site Development Review Section of BDS
 5. Plan Review Section of BDS
 6. Revised Bureau of Parks, Urban Forestry Division
- F. Letters - None
- G. Other
 1. Original LUR Application
 2. Letter of Incomplete dated 3/5/19
 3. Signed Request for an Evidentiary Hearing & Waiver of Right to a Decision within 120 days.

SITE PLAN



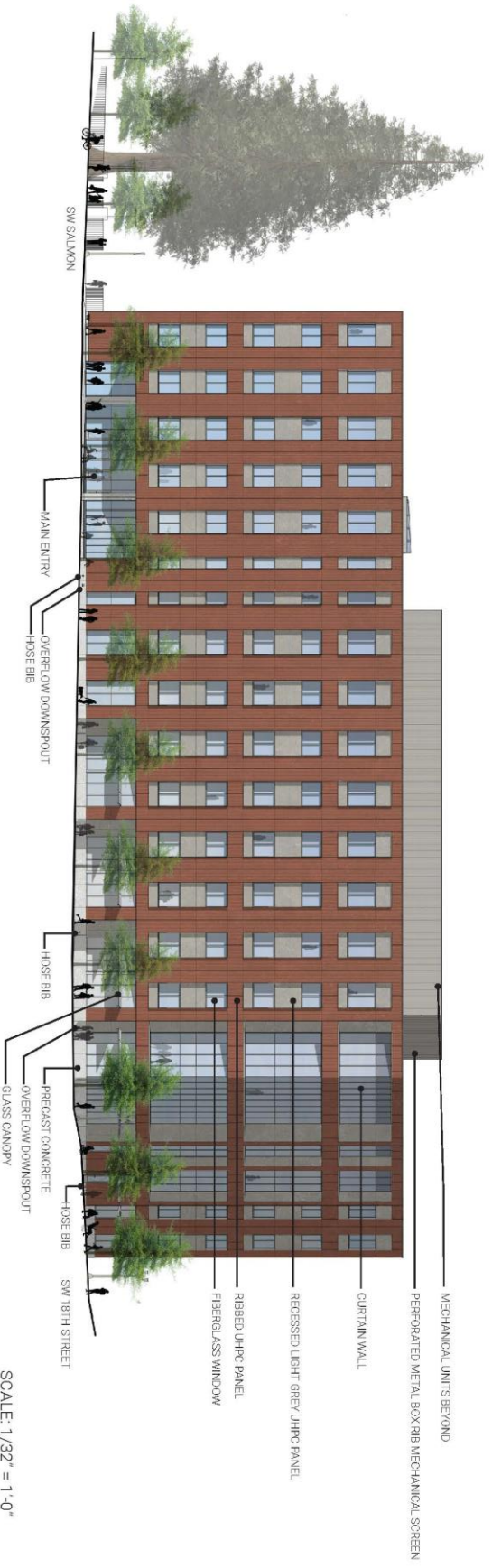
BORA Mayer/Reed

Lincoln High School Modernization Land Use Review #LU-119554 DZM

C-1

RENDERED NORTH ELEVATION

NOTE: Percentage Glazing on East facade <30%. Bird Friendly Glazing Standard does not apply



SCALE: 1/32" = 1'-0"

RENDERED WEST ELEVATION

NOTE: Percentage Glazing on East facade <30%. Bird Friendly Glazing Standard does not apply

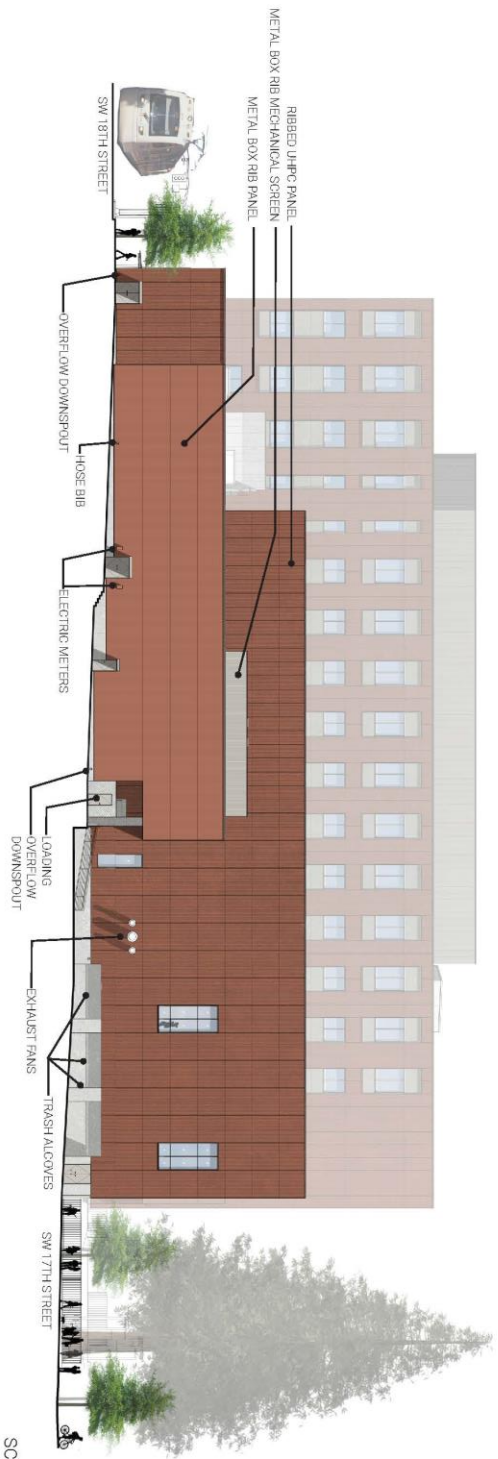


SCALE: 1/32" = 1'-0"

BORA
Mayer/Reed

Lincoln High School Modernization Land Use Review #LU-19554 DZM

RENDERED SOUTH ELEVATION



SCALE: 1/32" = 1'-0"

BORA Mayer/Reed

Lincoln High School Modernization Land Use Review #LU-119554-02M

RENDERED EAST ELEVATION

NOTE: Percentage Glazing on East facade <30%. Bird Friendly Glazing Standard does not apply

