

APPENDIX

APPENDIX: BLOCK 42 & BLOCK 45

APP.03	Renderings
APP.15	Vicinity and Context
APP.16	Block 42: Massing + Design Concept
APP.19	Block 45: Massing + Design Concept
APP.20	Modifications + Compliance Diagrams
APP.29	Civil Drawings: Block 42 & Block 45



AERIAL PERSPECTIVE LOOKING EAST



AERIAL PERSPECTIVE LOOKING WEST



BL 42 - SE CORNER



BL 42 - STREET AT SOUTH STAIR



BL 42 - NW CORNER



BL 42 - WALK UP UNITS ALONG LANE ST



BL 42 - WEST ENTRANCE



BL 45 - NORTH-EAST PLAZA AND COURTYARD



BL 45 - NE CORNER AND PLAZA



BL 45 - SW CORNER



BL 45 - RESIDENTIAL LOBBY ENTRANCE



BL 45 - WALKUP UNITS AT SOUTHWEST GASKET



- NEIGHBORHOOD FEATURES
1. ZIDELL PROPERTY

2. OHSU / CHH

3. ARDEA CONDOMINIUMS

4. JOHN ROSS CONDOMINIUMS

5. ATWATER CONDOMINIUMS

6. MERIWETHER CONDOMINIUMS

7. MIRABELLA RETIREMENT COMMUNITY

8. RIVA ON THE PARK

9. THE MATISSE APARTMENTS

10. BLOCK 49 APARTMENTS

11. ELIZABETH CARUTHERS PARK

12. SOWA GREENWAY

13. OLD SPAGHETTI FACTORY

14. HEALTHY STARTS CHILDREN'S CENTER

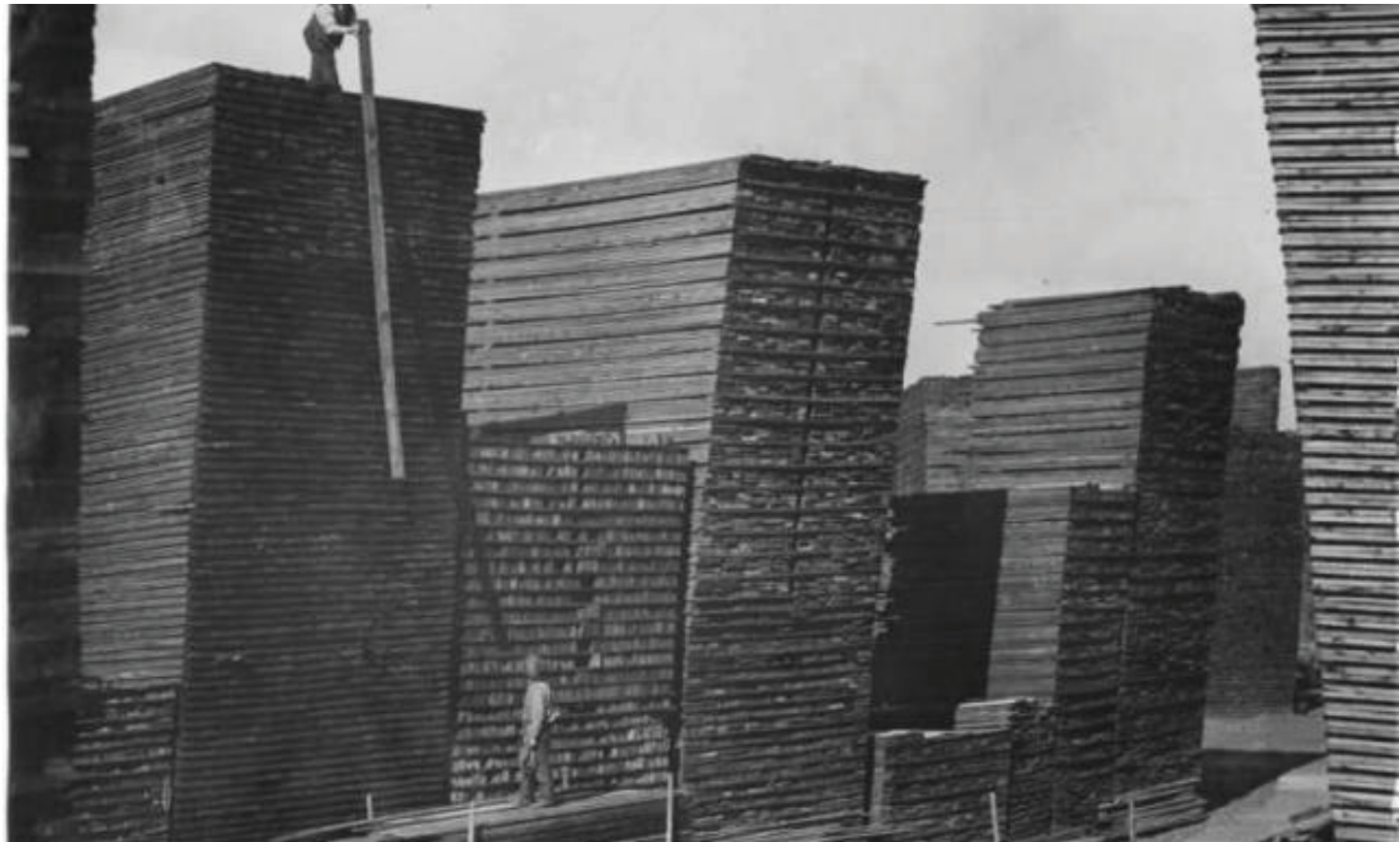
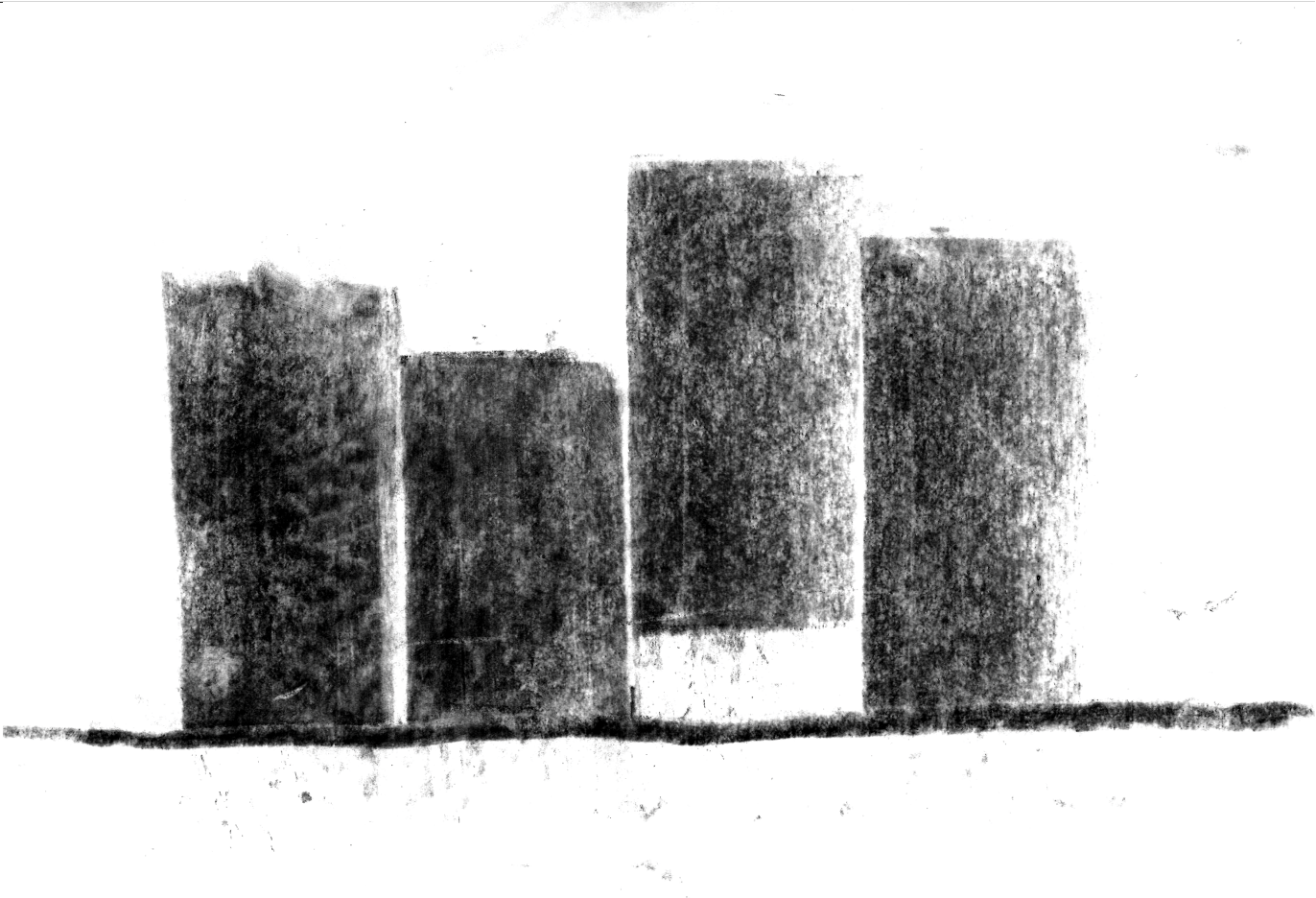
15. SOUTHWEST CHARTER SCHOOL

16. MONTESSORI CHILDREN'S HOUSE

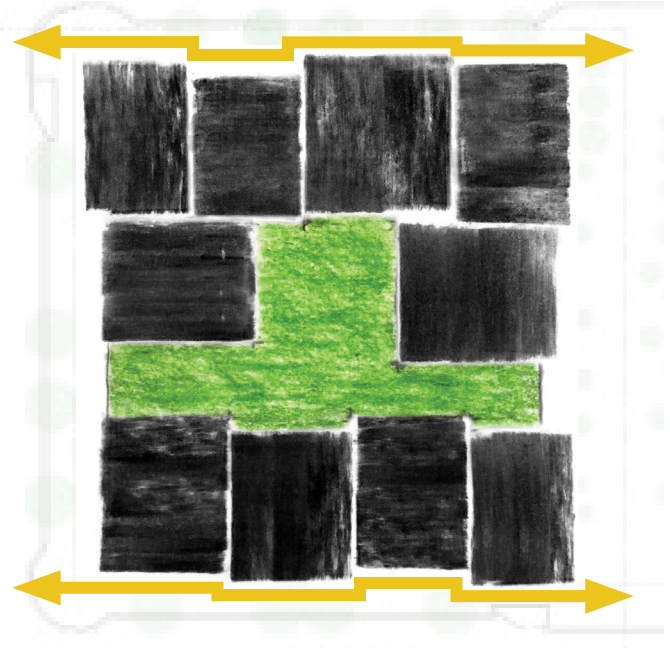
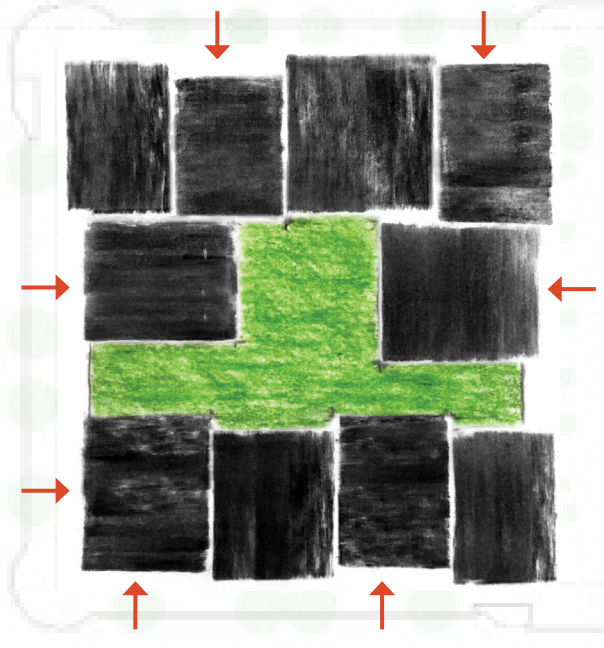
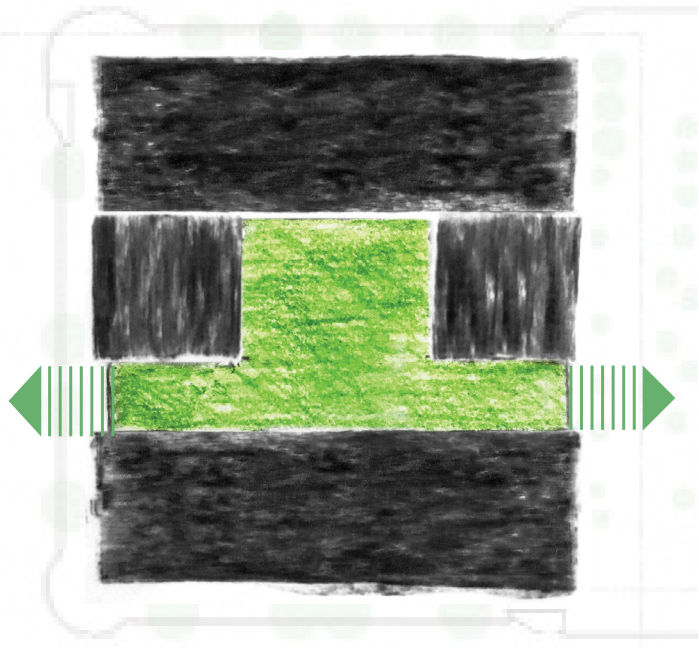
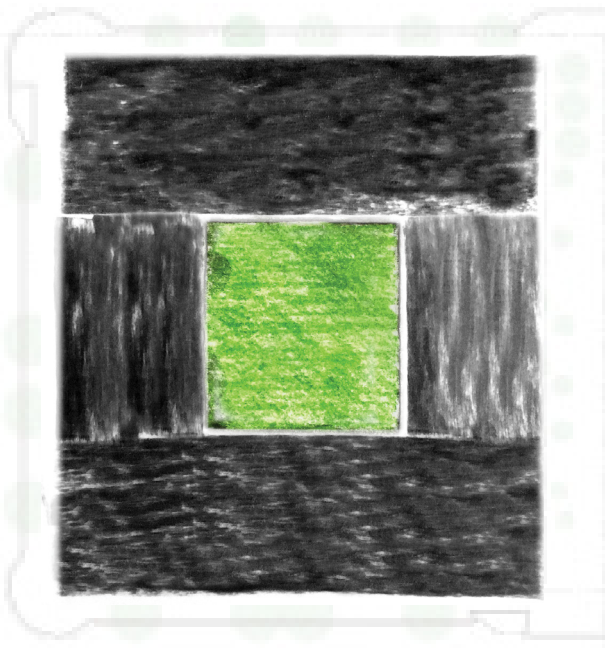
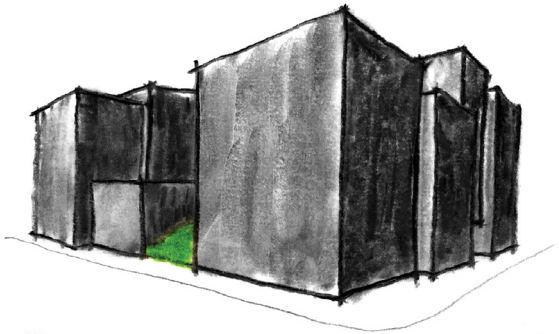
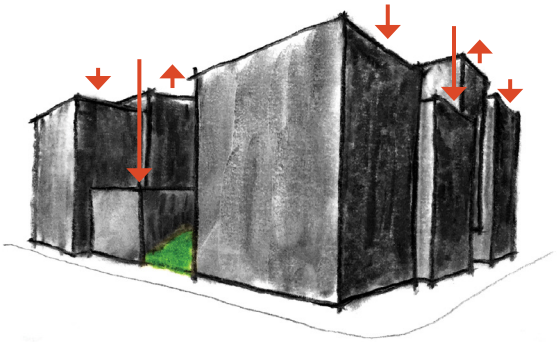
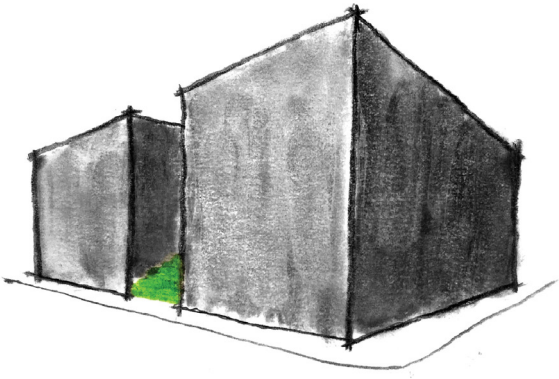
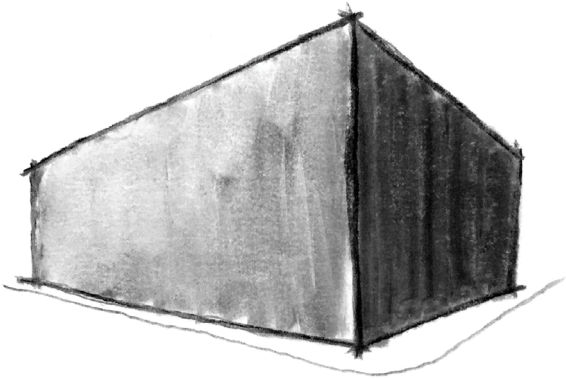
17. PROMETHEUS' PROPERTY

18. BLOCK 37 APARTMENTS

VICINITY PLAN



BL 42 - CONCEPT



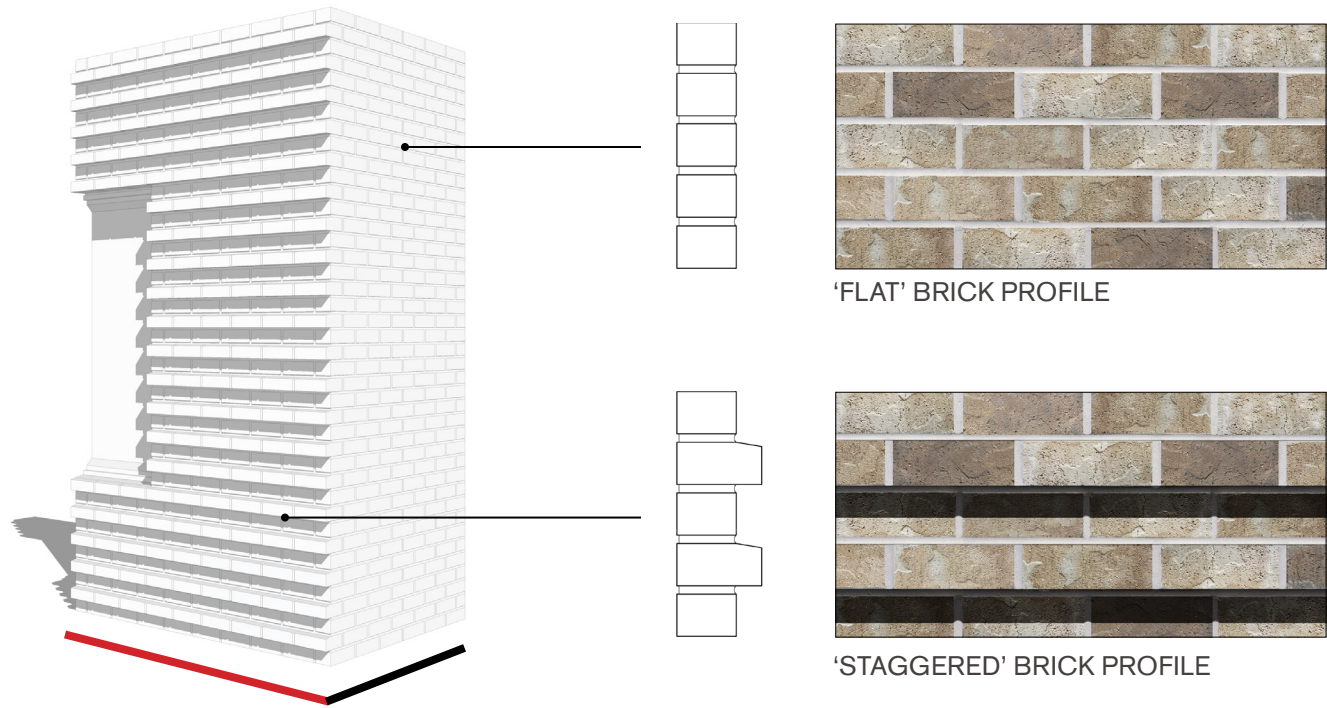
BL 42 - MASSING



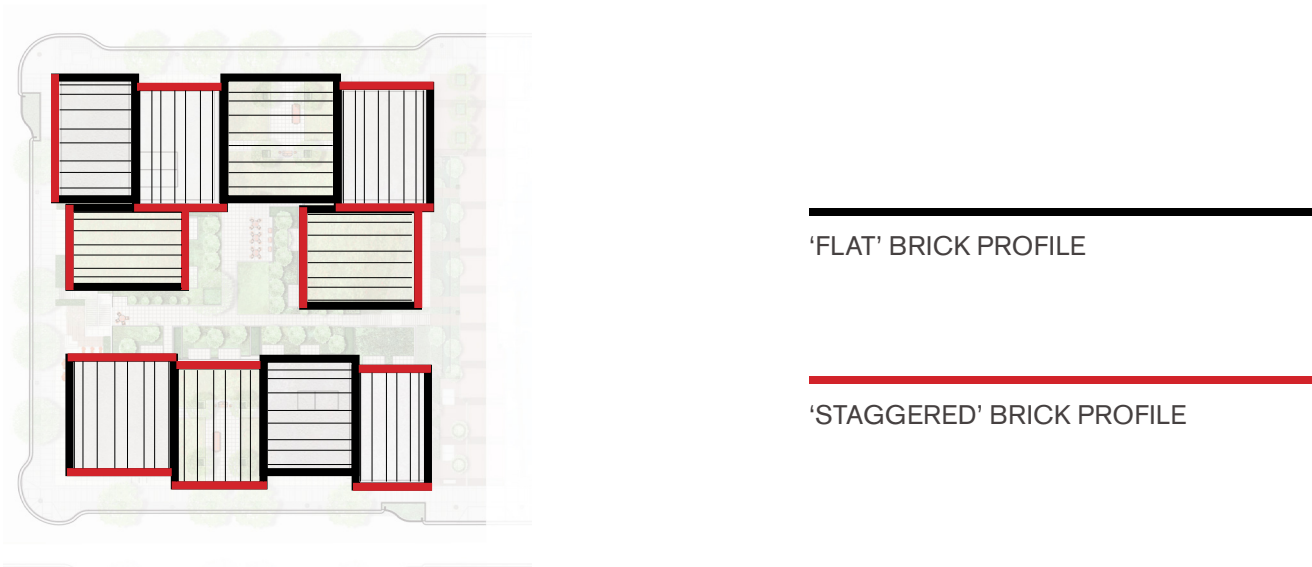
TEXTURE
Lumber stacks possess a distinct textural variation between sides, exhibiting a smooth **‘Flat’ side** and a rough **‘Staggered’ side** due to the natural variation in length of rough cut timber boards.



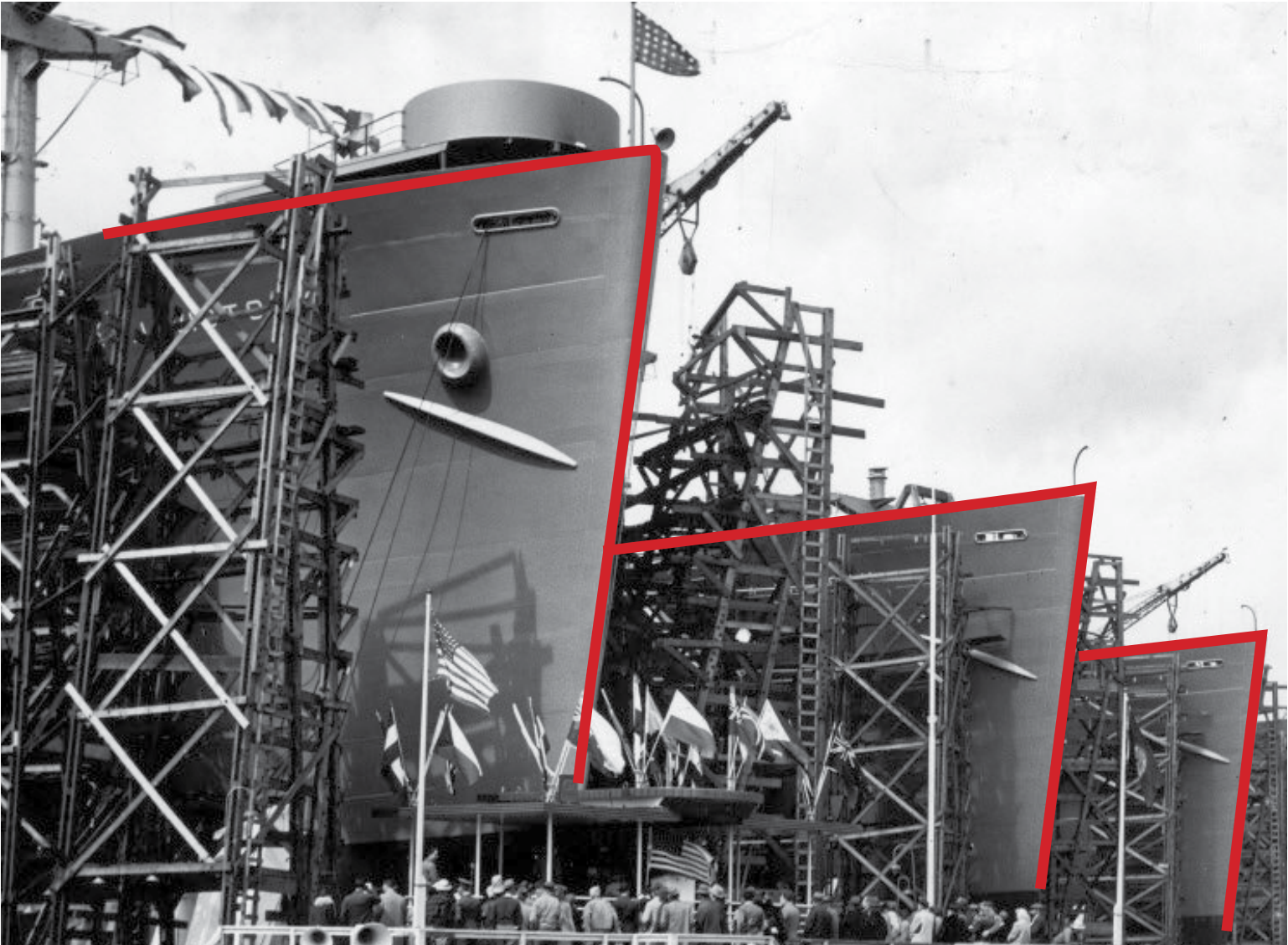
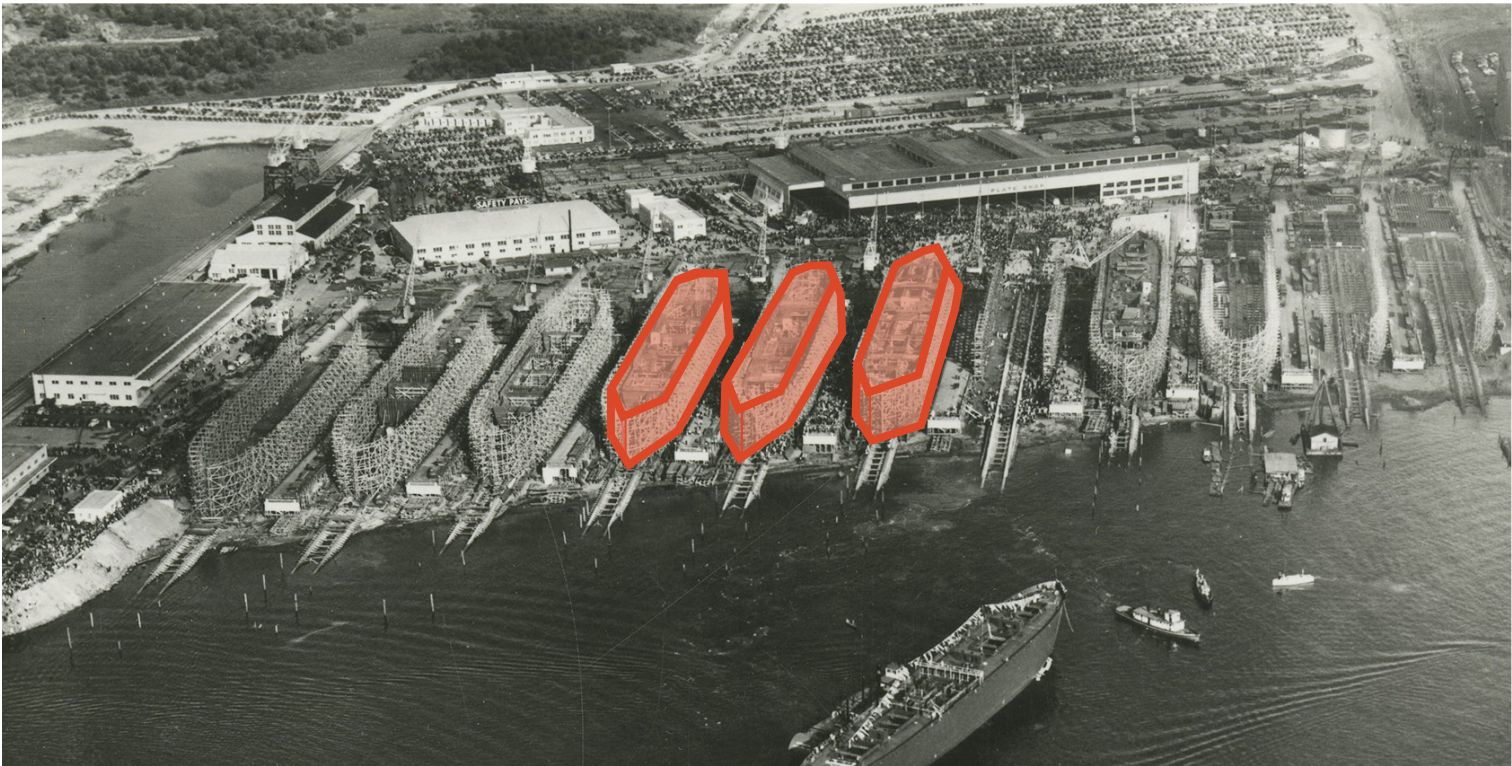
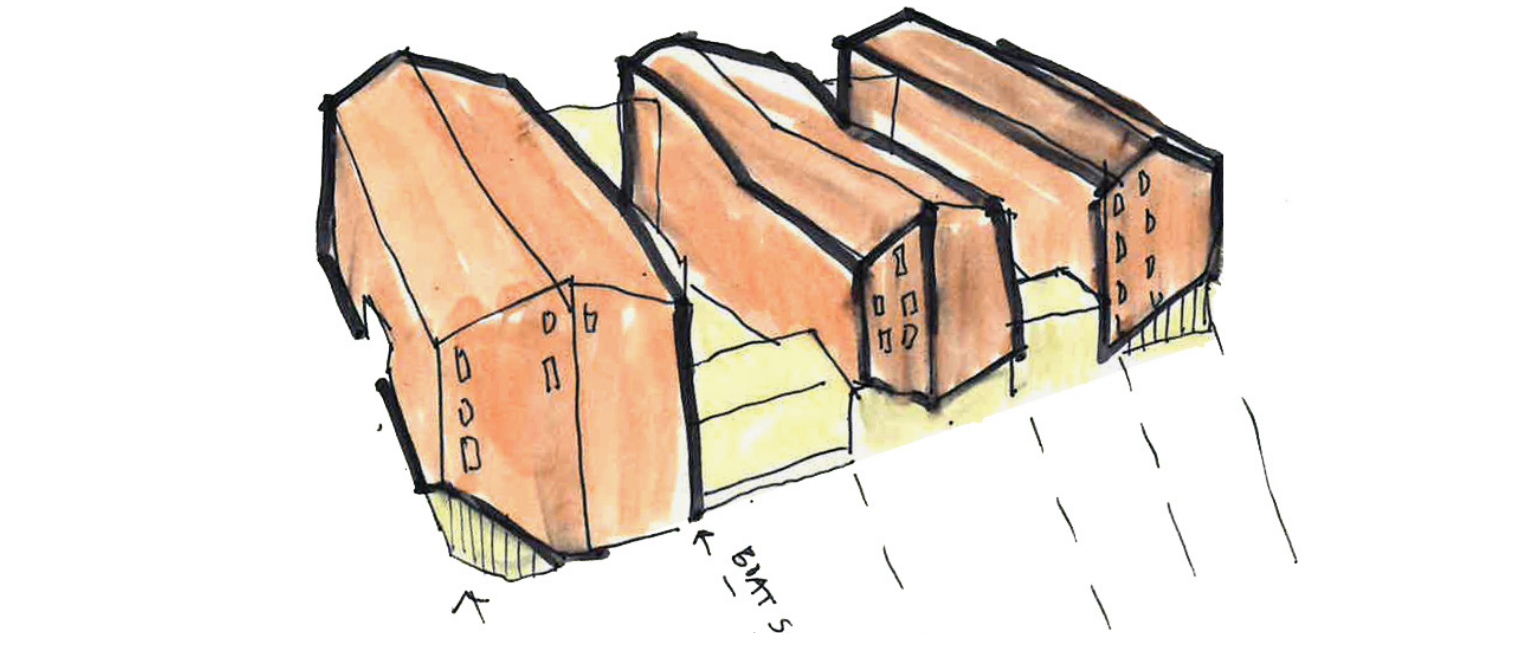
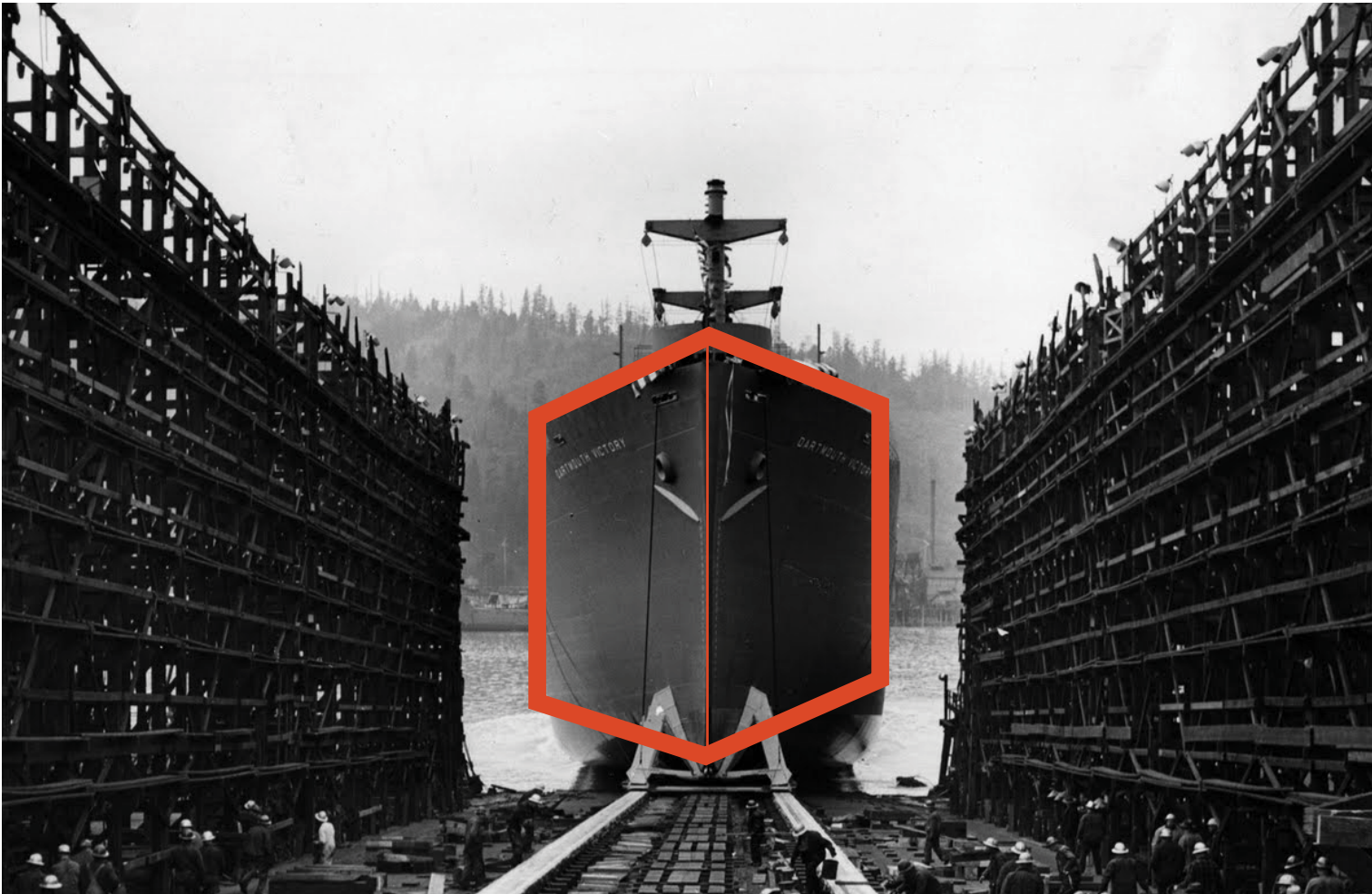
ORIENTATION
In lumber yards along the river banks the boards are characteristically piled in ‘stacks’ with varying orientations of the ‘Flat’ and ‘Staggered’ sides as they await shipping or other transport.



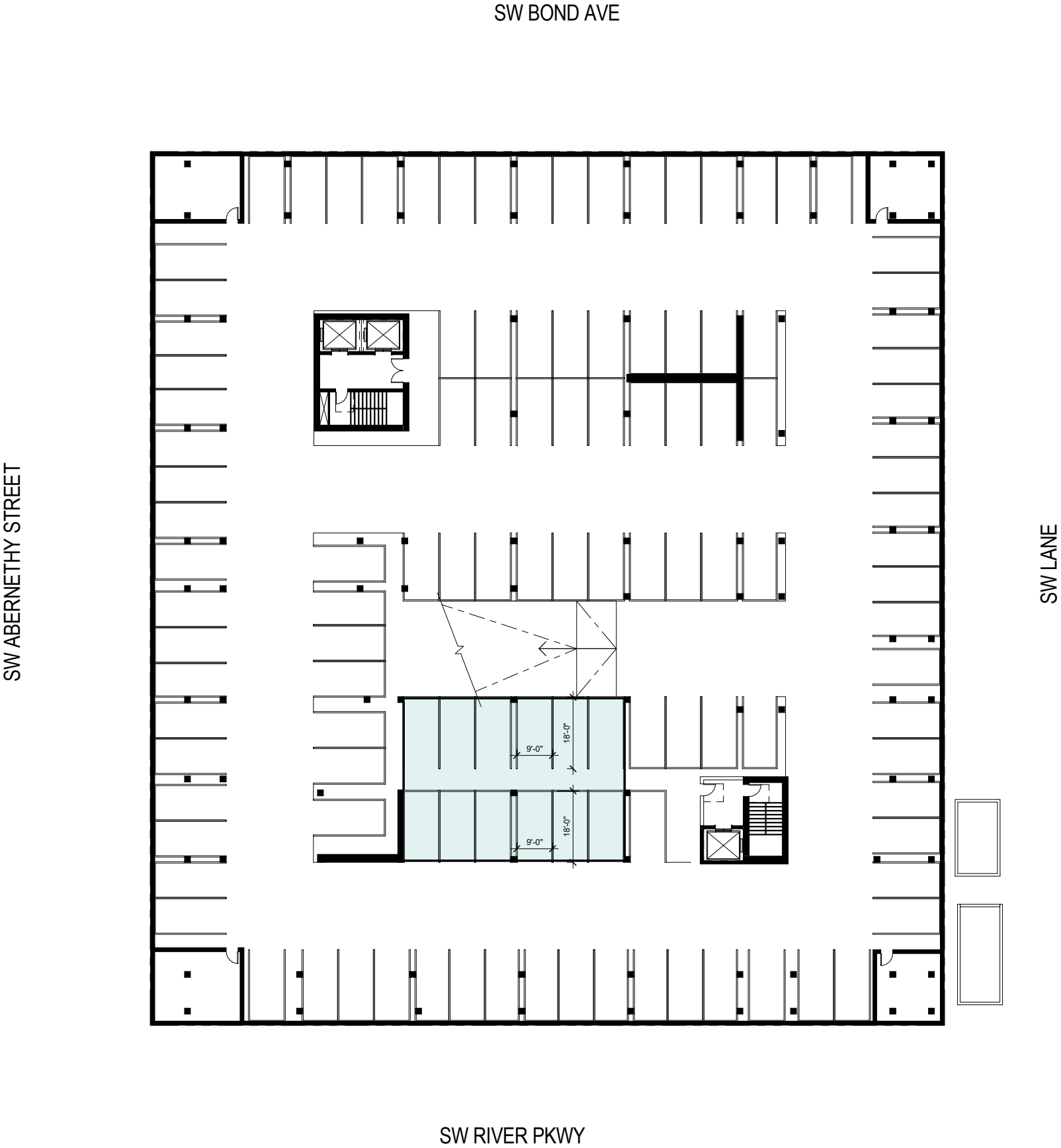
BL 42 - BRICK
This textural quality is expressed on the facade of BL 42 by establishing two distinct brick coursing strategies. A ‘flat’ standard coursing and a ‘staggered’ coursing which projects brick out every other row.



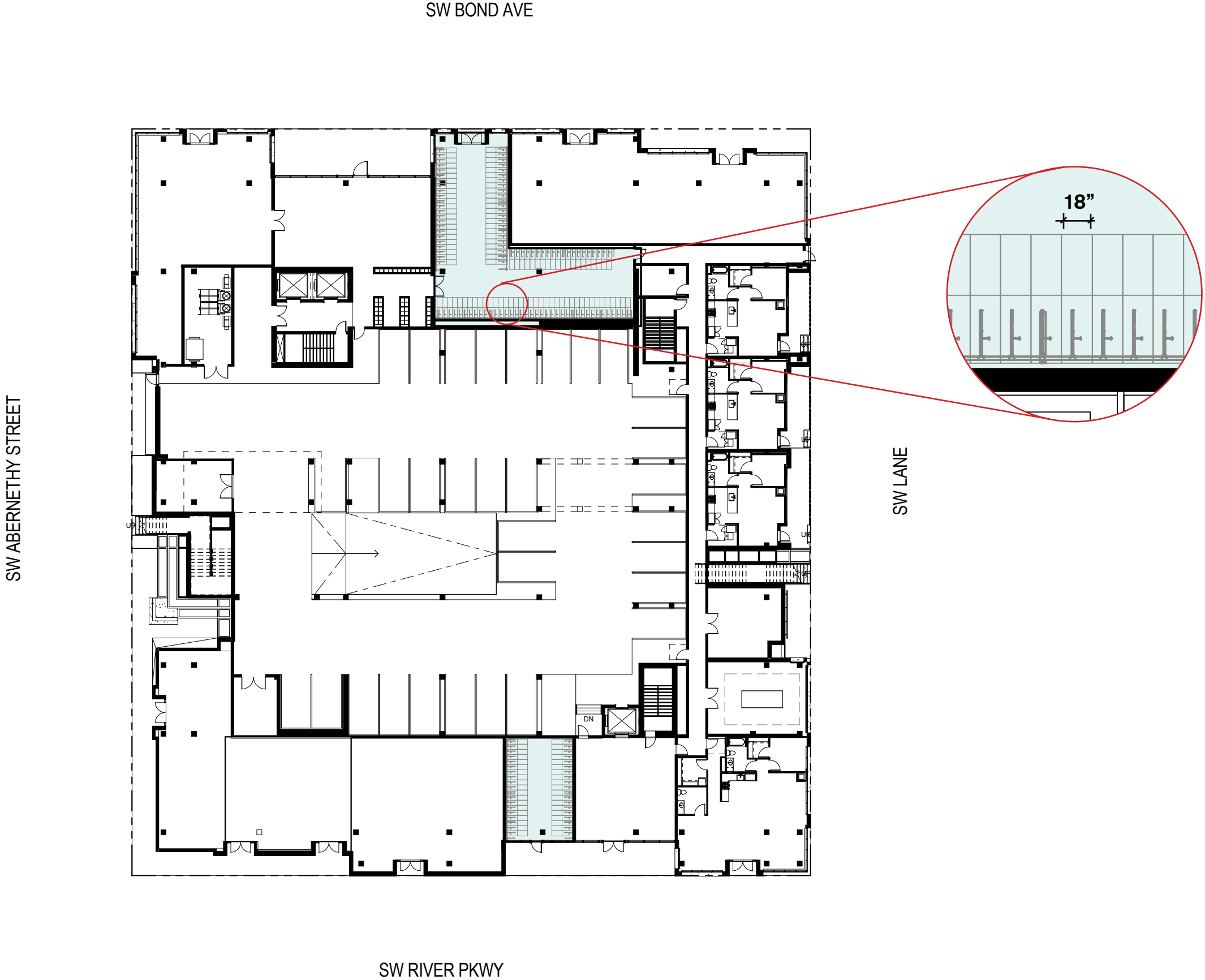
BL 42 - ‘STACK’ ORIENTATION
The conceptual massing ‘stacks’ of BL 42 vary in their orientation to create an alternating rythm of texure, shadow and light around the building.

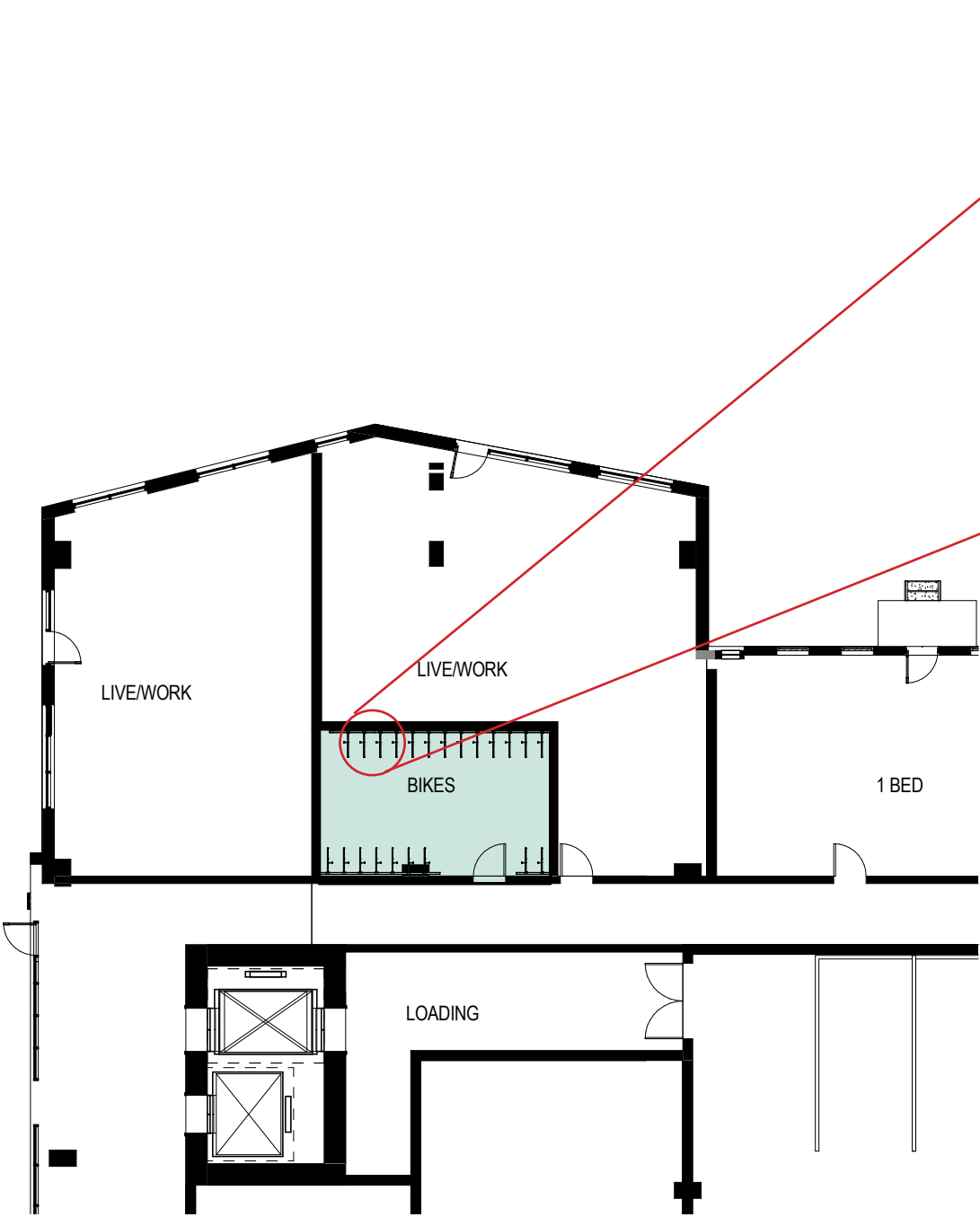


BL 45 - MASSING & DESIGN CONCEPT

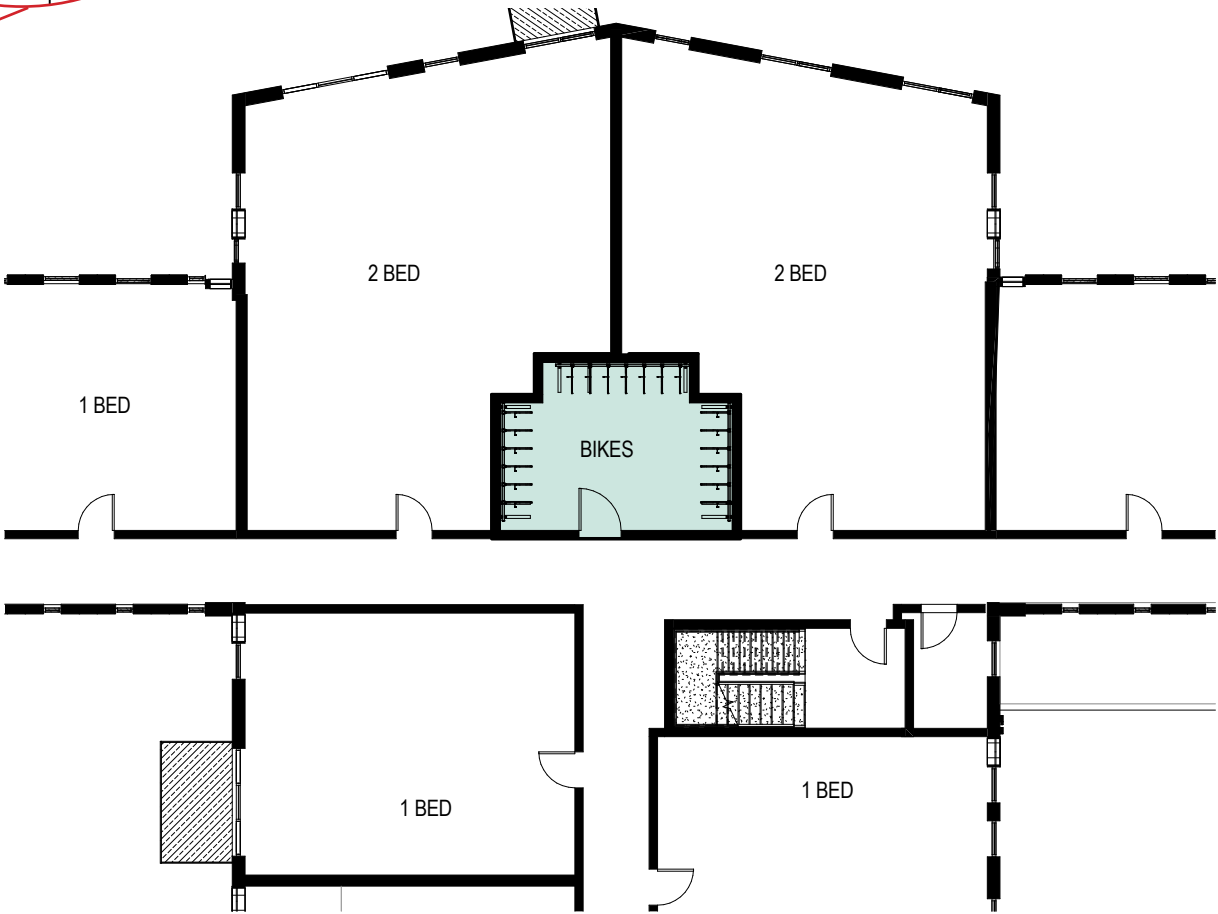
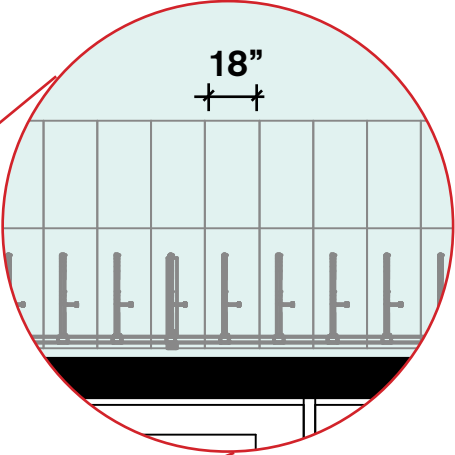


MODIFICATION #1 - BLOCK 42 - TANDEM PARKING



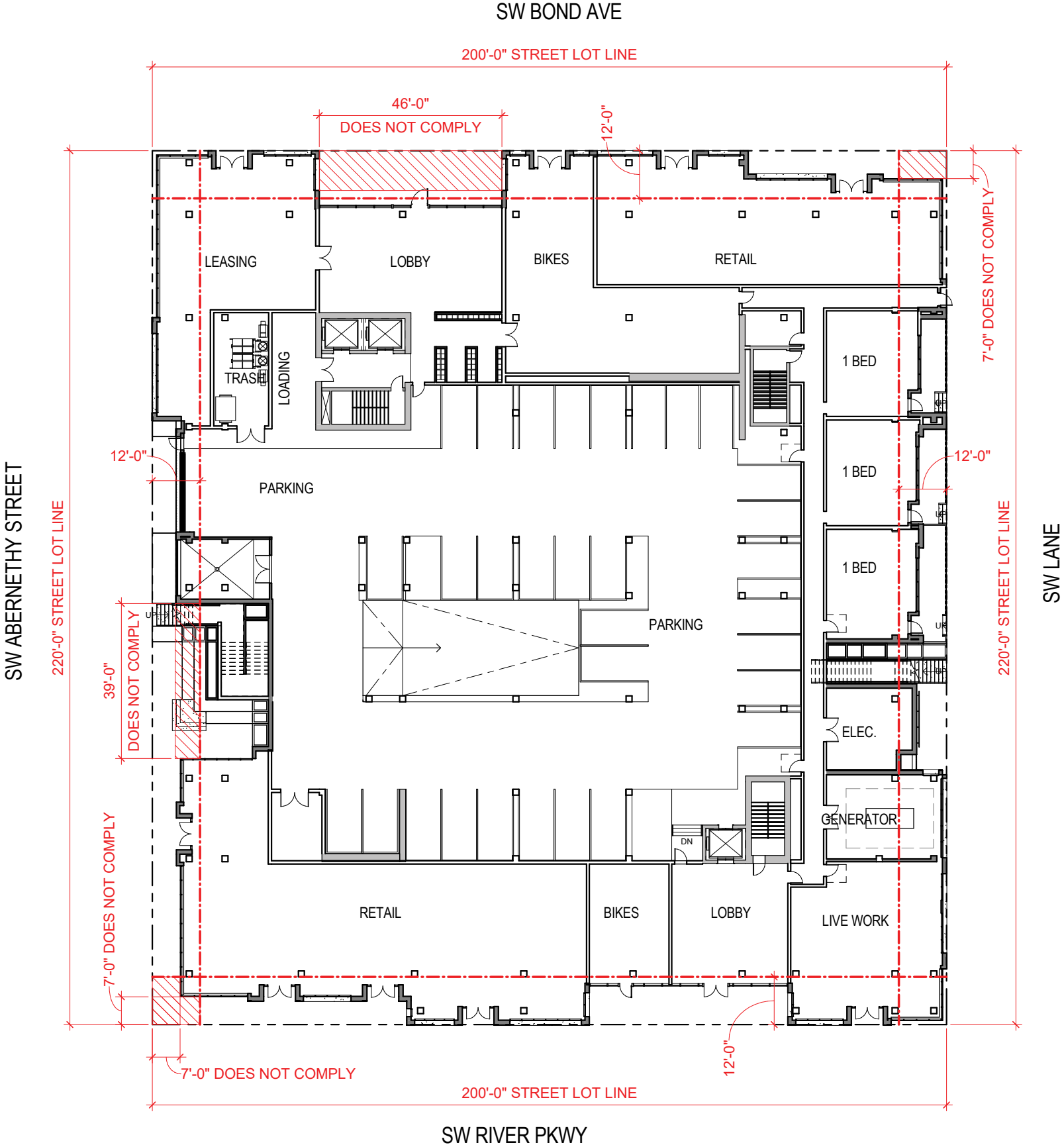


GROUND FLOOR BIKE ROOMS



RESIDENTIAL FLOOR BIKE ROOMS

MODIFICATION #2 - BLOCK 45 - BIKE STALL WIDTH



DEMONSTRATING COMPLIANCE

REQUIRED BUILDING LINES:
33.510.215
The building must extend to within 12' of the street lot line for 75% of the lot line.

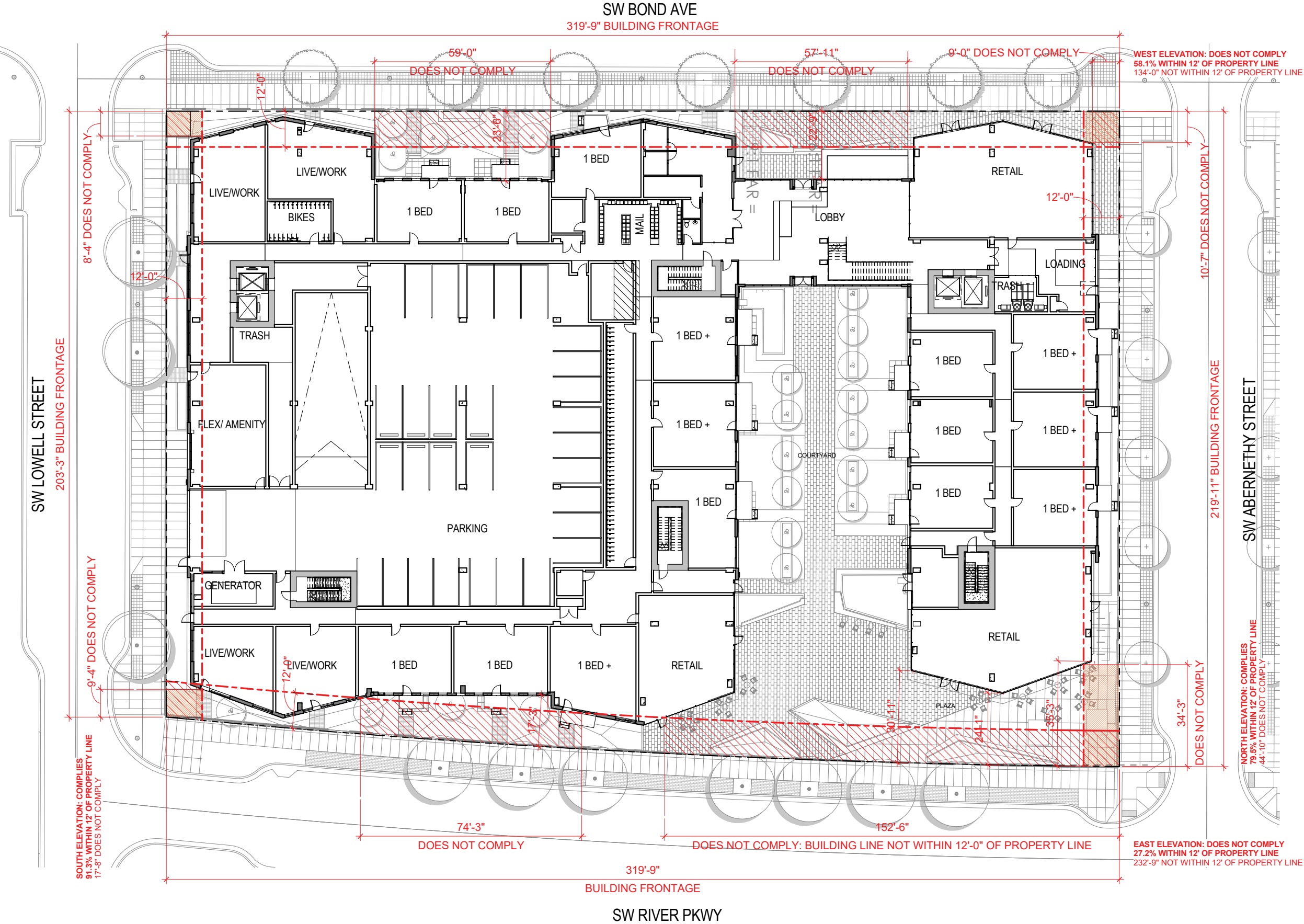
Block 42 Active Use Provided:

SW Bond:	77% of Frontage
SW River Parkway:	97% of Frontage
SW Abernethy:	79% of Frontage
SW Lane:	97% of Frontage

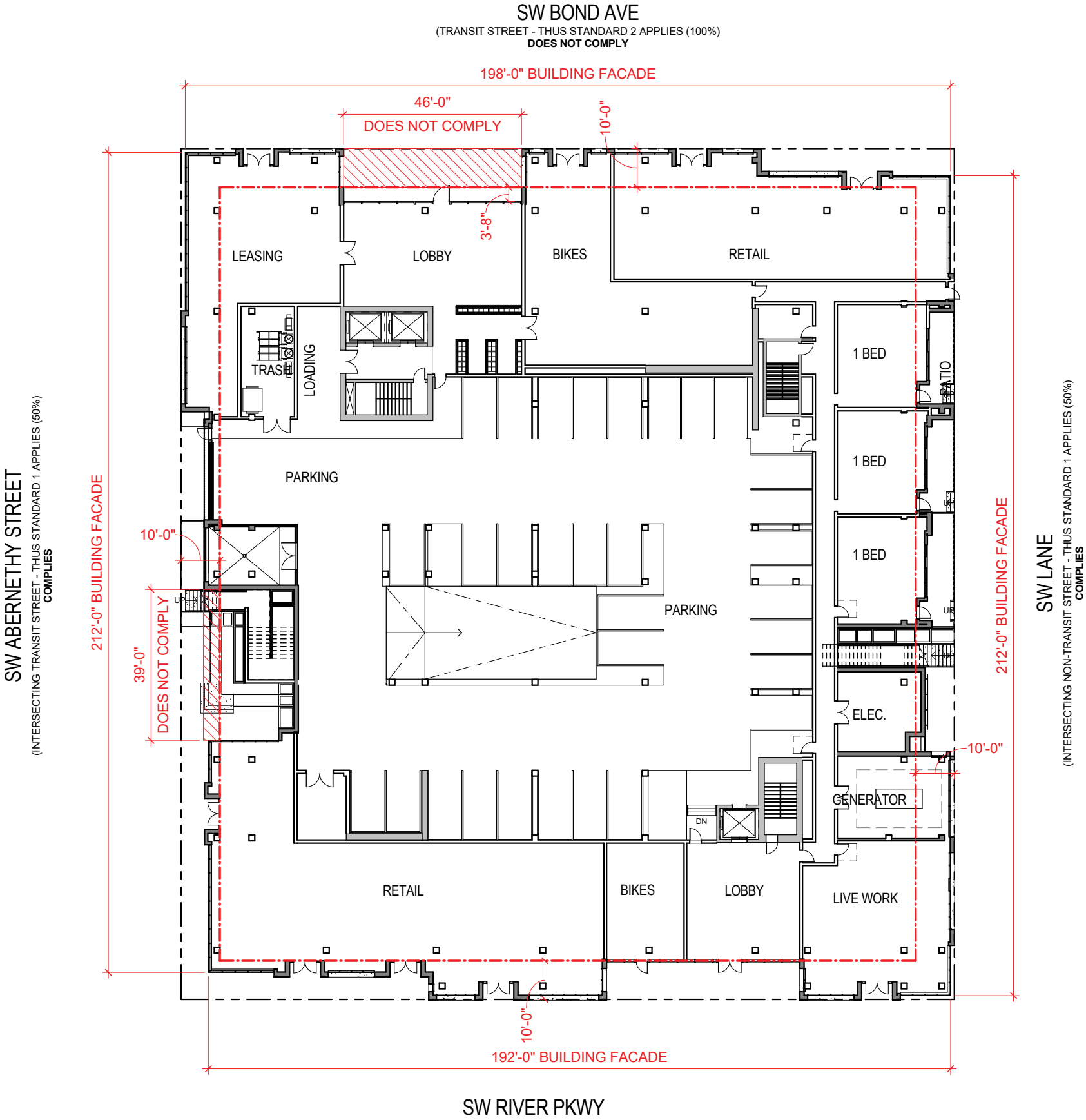
REQUIRED BUILDING LINES:
33.510.215
The building must extend to within 12' of the street lot line for 75% of the lot line.

MODIFICATION:
27.2% of the east facade of the building is within 12'-0" of the lot line.
58.1% of the west facade of the building is within 12'-0" of the lot line.
91.3% of the south facade of the buildingf is within 12'-0" of the lot line.
79.5% of the north facade of the building is within 12'-0" of the lot line.

Block 45 will be asking for a modification for both the east and west facades, which do not meet the required building lines per Section 33.510.215 and Map 510-6. 152'-6" of eastern frontage is in non-compliance to allow for the public plaza at the northeast corner. 116'-11" of western frontage is in non-compliance at residential units and at the residential lobby.



MODIFICATION #3 - BLOCK 45 - REQUIRED BUILDING LINES



DEMONSTRATING NON-COMPLIANCE
REQUESTING MODIFICATION FOR WEST FACADE

MAXIMUM BUILDING SETBACK:
(33.130.215; TABLE 130-3)

Standard 1: At least 50 percent of the length of the ground level street-facing façade of the building must be within the maximum setback;
Standard 2: 100 percent of the length of the ground level street-facing façade of the building must be within the maximum setback.

(5) One transit street and one intersecting non-transit street. Where the site is adjacent to a transit street and an intersecting non-transit street, the following standards must be met:

- *Standard 2 must be met on the frontage of the transit street,*
- *Standard 1 must be met on the intersecting non-transit street[s].*

Thus, 100% of west facade must comply (Standard 2);
50% of all other facades must comply (Standard 1).

MODIFICATION:
100% of the east facade of the building is within 10'-0" of the lot line.
77% of the west facade of the building is within 10'-0" of the lot line.
82% of the south facade of the building is within 10'-0" of the lot line.
100% of the north facade of the building is within 10'-0" of the lot line.

Block 42 is asking for a modification for the west facade, which does not meet the required maximum building setback (Standard 2) per 33.130.215.

MODIFICATION #4 - BLOCK 42 - MAXIMUM BUILDING SETBACK

``MAXIMUM BUILDING SETBACK:
33.130.215; TABLE 130-3

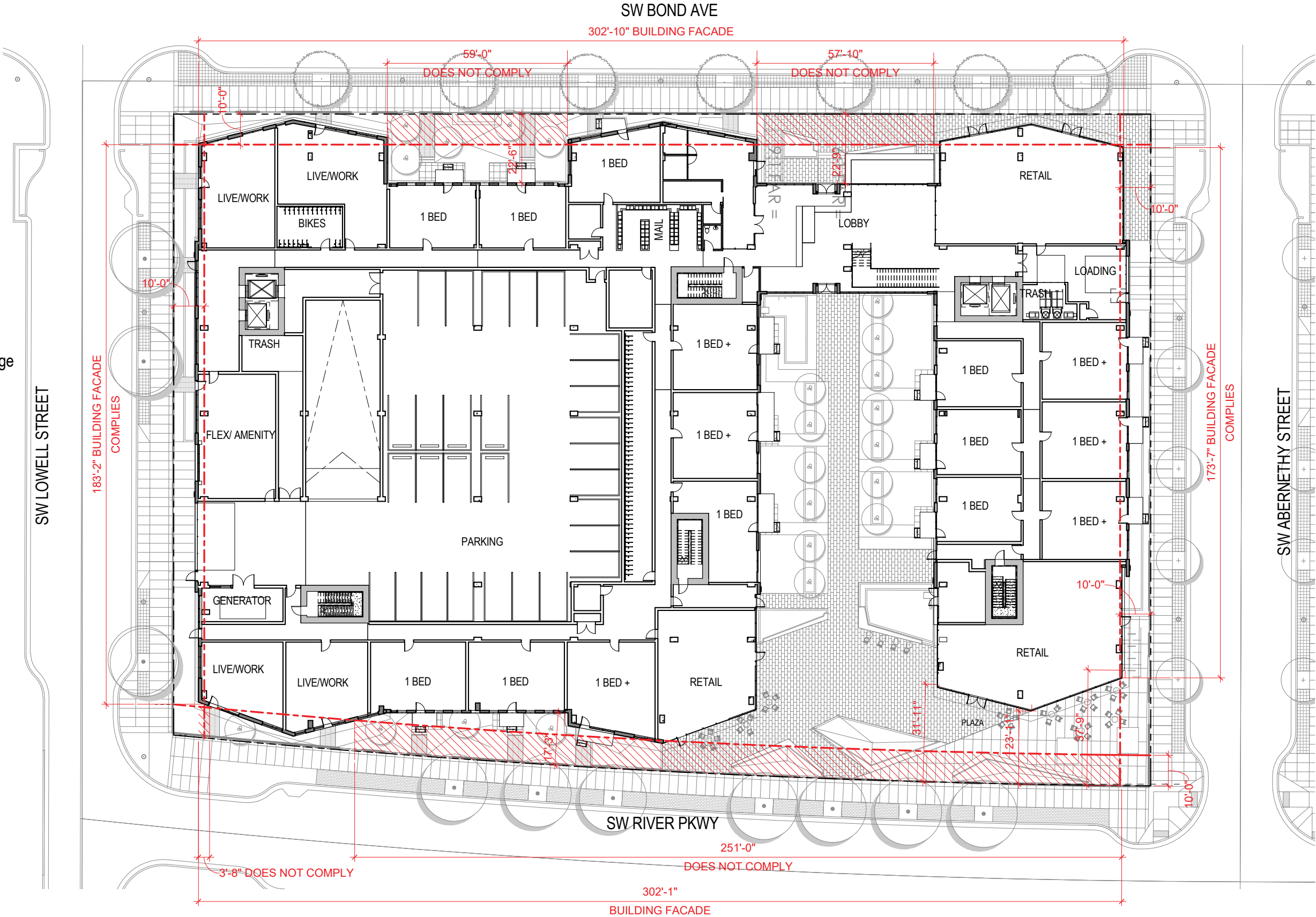
Standard 1: At least 50 percent of the length of the ground level street-facing façade of the building must be within the maximum setback;
Standard 2: 100 percent of the length of the ground level street-facing façade of the building must be within the maximum setback.

- (5) One transit street and one intersecting non-transit street. Where the site is adjacent to a transit street and an intersecting non-transit street, the following standards must be met:
- Standard 2 must be met on the frontage of the transit street,
 - Standard 1 must be met on the intersecting non-transit street[s].

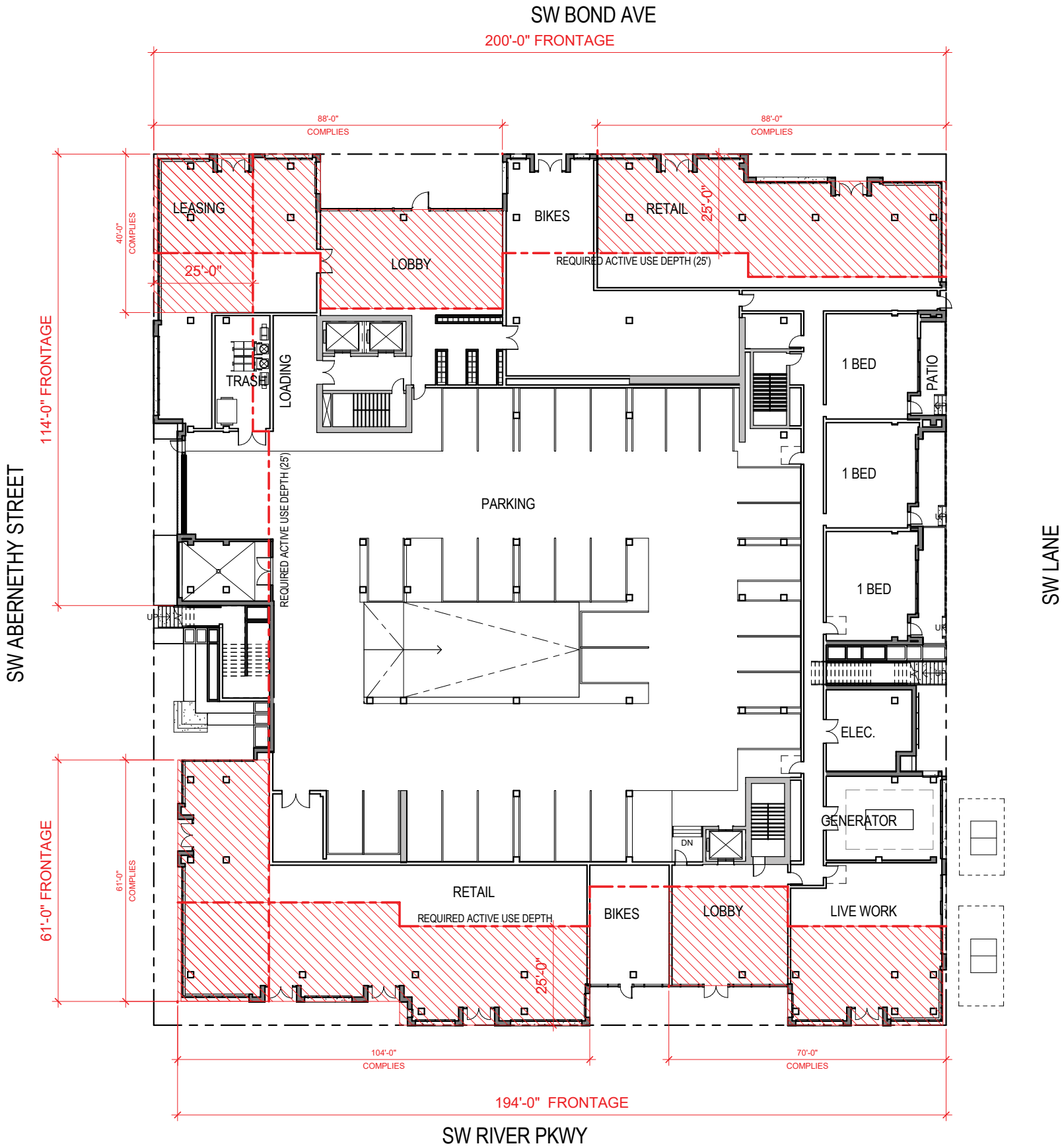
Thus, 100% of west facade must comply (Standard 2); 50% of all other facades must comply (Standard 1).

MODIFICATION:
16% of the east facade of the building is within 10'-0" of the lot line. (50% required; does not comply)
61% of the west facade of the building is within 10'-0" of the lot line. (100% required; does not comply)
100% of the south facade of the building is within 10'-0" of the lot line.
100% of the north facade of the building is within 10'-0" of the lot line.

Block 45 will be asking for a modification for both the east and west facades, which do not meet the required maximum building setback (Standard 2 at SW Bond; Standard 1 at SW River Pkwy) per 33.130.215.



MODIFICATION #4 - BLOCK 45 - MAXIMUM BUILDING SETBACK



DEMONSTRATING COMPLIANCE

GROUND FLOOR ACTIVE USE:
(33.510.225)
50% OF USES REQUIRED TO CONTAIN ACTIVE
USE 25' DEEP

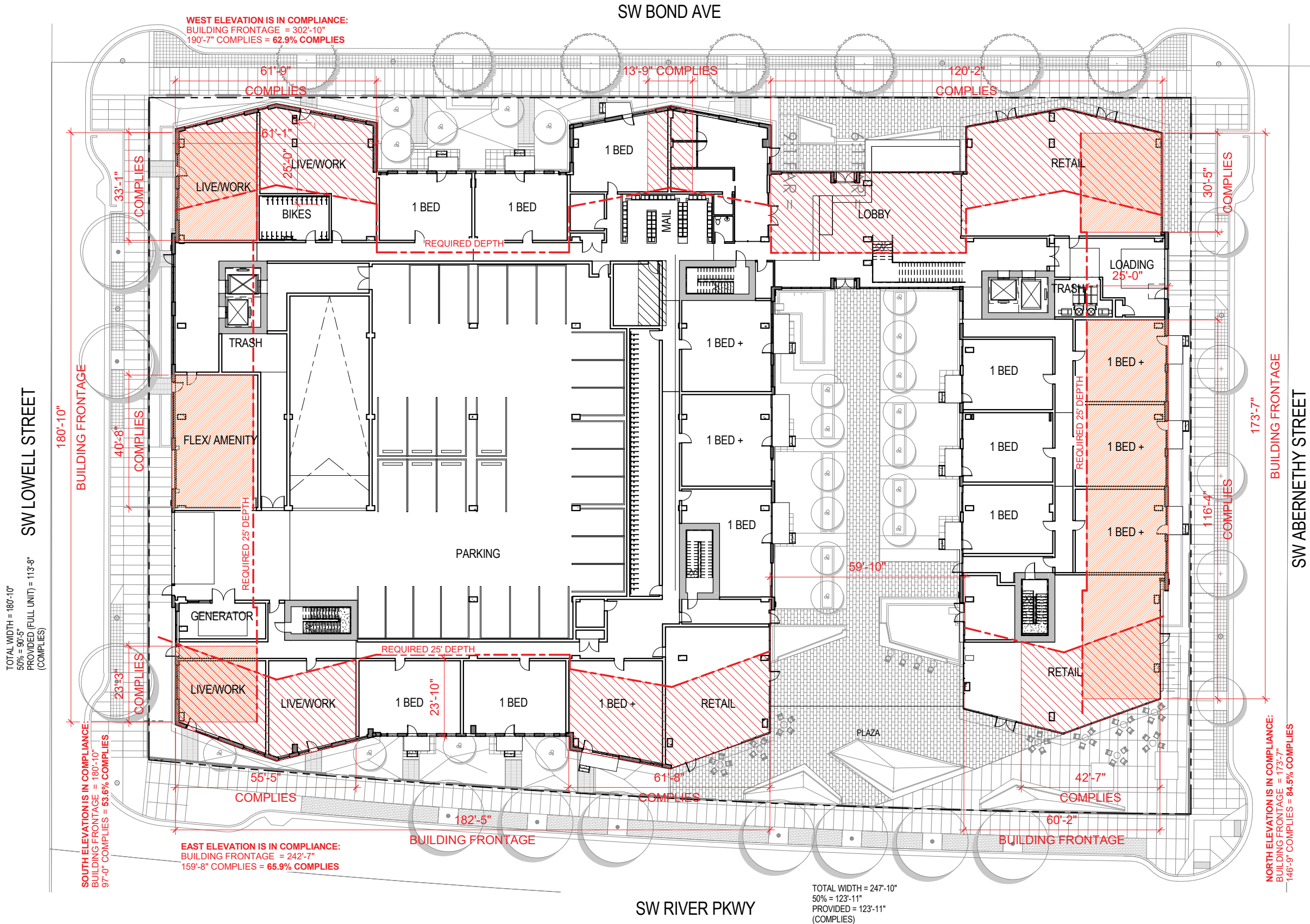
Block 42 Active Use Provided:

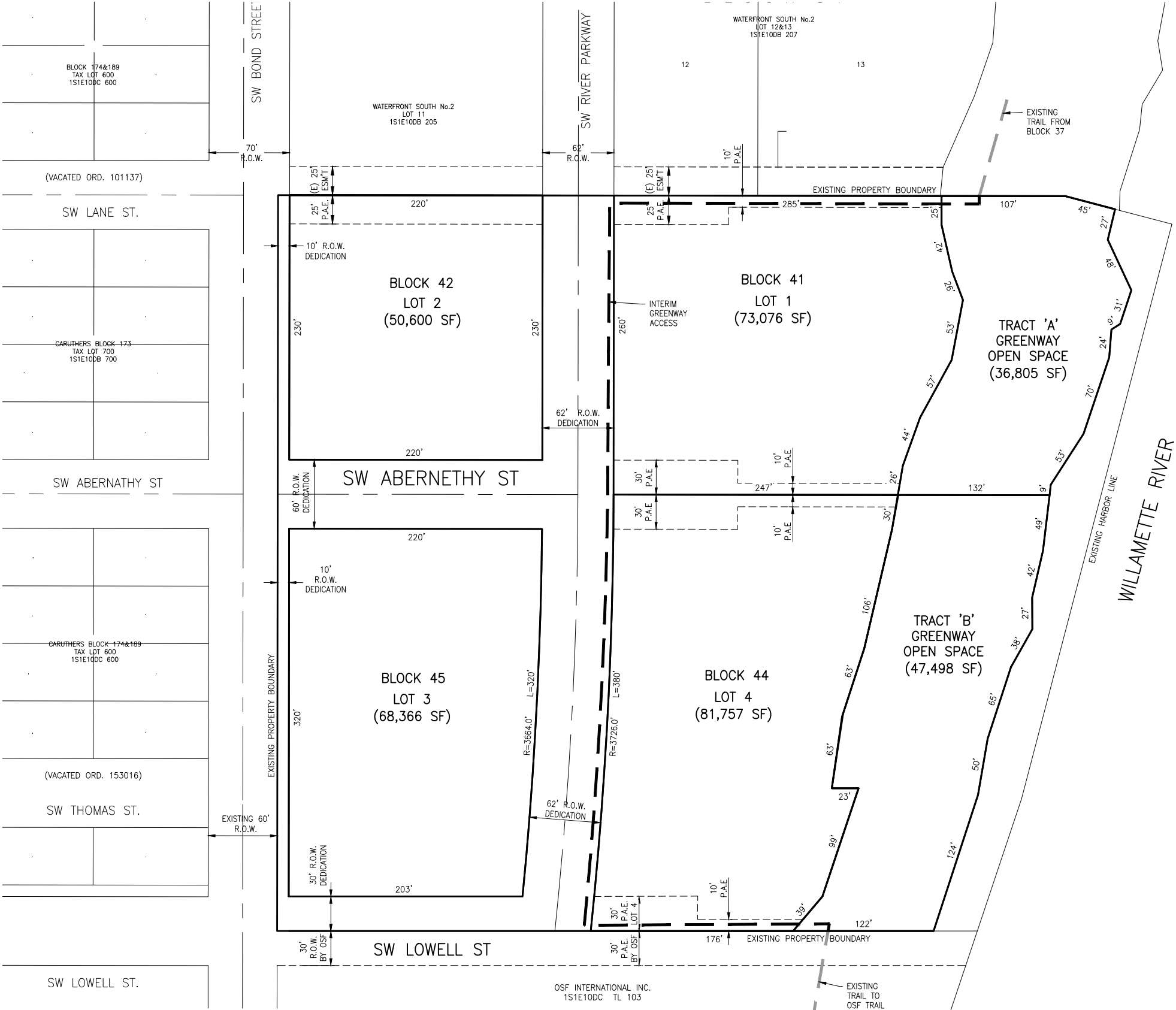
SW Bond:	88% of Frontage
SW River Parkway:	90% of Frontage
SW Abernethy:	58% of Frontage
SW Lane:	not required

GROUND FLOOR ACTIVE USE: 33,510.225
50% OF USES REQUIRED TO CONTAIN
ACTIVE USE 25' DEEP

Block 45 Active Use Provided:

SW Bond:	62.9% of Frontage
SW River Parkway:	65.9% of Frontage
SW Abernethy:	84.5% of Frontage
SW Lowell:	53.6% of Frontage



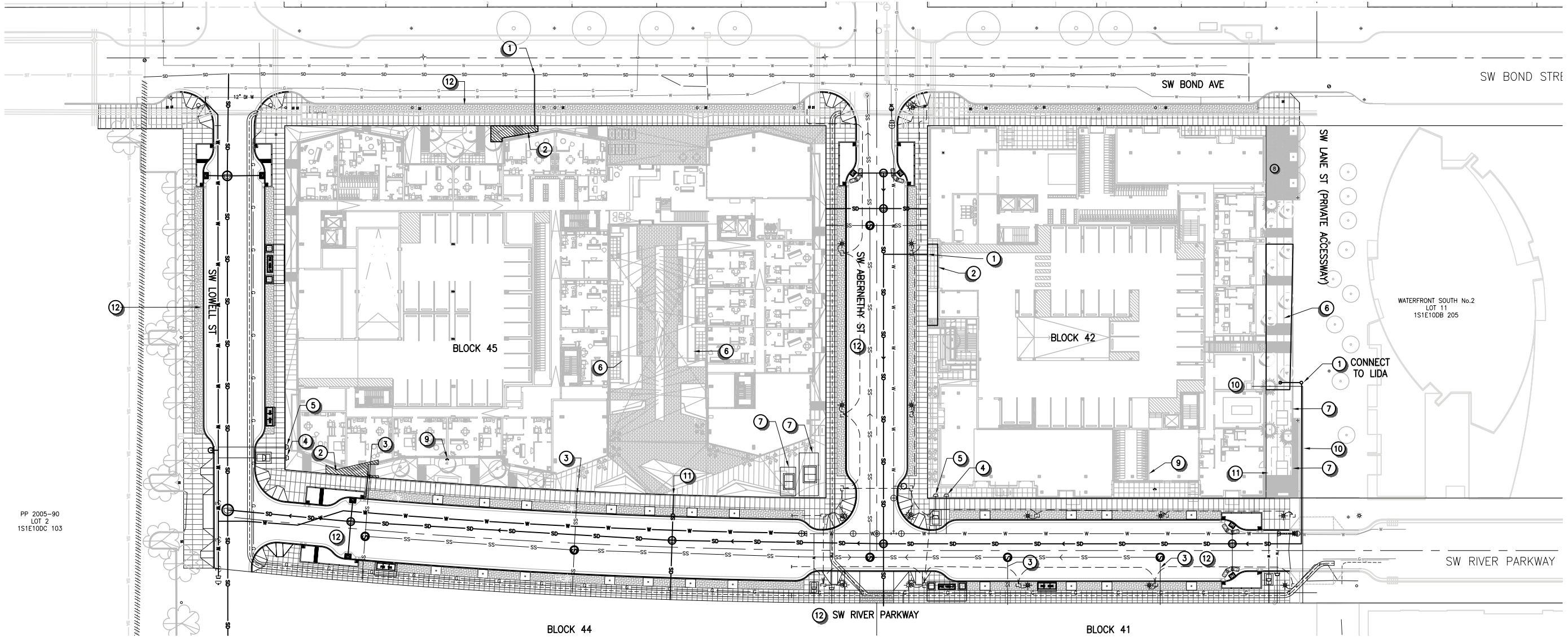


LEGEND		
RIGHT OF WAY LINE (R.O.W.)		_____
PROPOSED LOT LINE (P.L.)		_____
ROADWAY CENTERLINE (C.L.)		_____
PROPOSED ACCESS EASEMENT (P.A.E.)		-----



PRELIMINARY LAND USE PLAT





PP 2005-90
LOT 2
1S1E10DC 103

UTILITY CONTACT DETAILS

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terry.wenz@portlandoregon.gov
503-823-7171

City of Portland (Storm & Sewer)
Andre Duval
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503-823-7214

contacted by
OTAK (Civil Engineer)
Brian A. Ballerini, PE
brian.ballerini@otak.com

UTILITY CALLOUTS

- 1 PROPOSED STORM LATERAL.
- 2 PROPOSED PRIVATE FLOW THROUGH PLANTER.
- 3 PROPOSED SANITARY LATERAL.
- 4 PROPOSED DOMESTIC WATER SERVICE & METER
- 5 PROPOSED FIRE WATER SERVICE.
- 6 PROPOSED STORMWATER SWALE.
- 7 PROPOSED ELECTRICAL VAULT/CONDUIT DUCT BANK.
- 8 PROPOSED IMPERVIOUS PAVERS.
- 9 PROPOSED FDC CONNECTION.
- 10 PROPOSED GAS SERVICE.
- 11 PROPOSED PRIVATE STORM DRAIN.
- 12 IMPROVEMENTS IN RIGHT OF WAY UNDER SEPARATE PUBLIC WORKS PERMIT.

CONSTRUCTION NOTES:

- 1. PUBLIC IMPROVEMENTS SHOWN ARE FOR REFERENCE ONLY. PUBLIC IMPROVEMENTS TO BE CONSTRUCTED UNDER SEPARATE PERMIT.

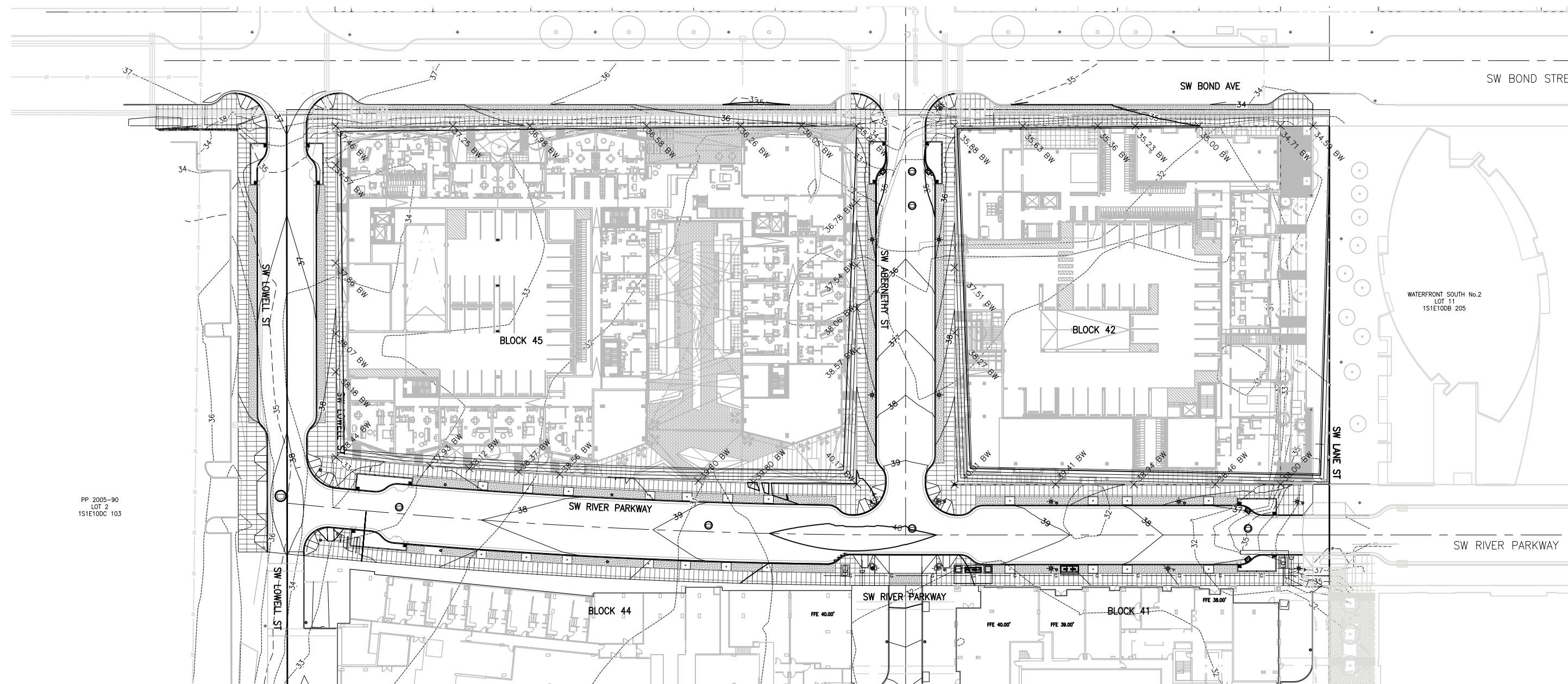


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CIVIL SITE AND COMPOSITE UTILITY PLAN





LEGEND

----- 194 ----- 1' EXISTING CONTOURS
 ----- 195 ----- 5' EXISTING CONTOURS
 ----- 194 ----- 1' PROPOSED CONTOURS
 ----- 195 ----- 5' PROPOSED CONTOURS
 ● 200.90
 XX SPOT ELEVATION



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CIVIL SITE GRADING PLAN

