



**City of Portland, Oregon**  
**Bureau of Development Services**  
**Land Use Services**  
FROM CONCEPT TO CONSTRUCTION

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## **STAFF REPORT AND RECOMMENDATION TO THE DESIGN COMMISSION – APPROVAL**

CASE FILE: LU 16-284073 DZM  
PC # 16-137537  
1440 SW Taylor St Apartment Building  
REVIEW BY: Design Commission  
WHEN: February 16, 2017, 1:30 pm  
WHERE: 1900 SW Fourth Ave., Room 2500A  
Portland, OR 97201

It is important to submit all evidence to the Design Commission. City Council will not accept additional evidence if there is an appeal of this proposal.

**Bureau of Development Services Staff:** Jeffrey Mitchem 503-823-7011 /  
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### **GENERAL INFORMATION**

**Applicant:** Robert Leeb  
Leeb Architects  
308 SW First Ave #200  
Portland, OR 97204

**Owner:** Eric Evans  
Shelter Holdings  
11624 SE 5th St Suite 210  
Bellevue WA 98005

**Site Address:** 1440 SW TAYLOR ST

**Legal Description:** BLOCK 308 FRAC LOT 5 LOT 6, PORTLAND; BLOCK 308 LOT 7&8, PORTLAND

**Tax Account No.:** R667732880, R667732900

**State ID No.:** 1N1E33DC 04300, 1N1E33DC 04200

**Quarter Section:** 3028

**Neighborhood:** Goose Hollow, contact Jerry Powell at 503-222-7173.

**Business District:** Goose Hollow Business Association, contact Angela Crawford at 503-223-6376.

**District Coalition:** Neighbors West/Northwest, contact Mark Sieber at 503-823-4212.

**Plan District:** Central City - Goose Hollow

**Zoning:** RXd, Central Residential with a Design Overlay

**Case Type:** DZM, Design Review with Modifications

**Procedure:** Type III, with a public hearing before the Design Commission.  
The decision of the Design Commission can be appealed to City Council.

**Proposal:**

**Program.** The proposal is for a new 7-story mixed use market rate apartment building on a 15,510 square foot site with frontages on SW 15<sup>th</sup> Ave (approximately 150') and SW Taylor St (approximately 100'). Key components of the development program are (approximate quantities):

- **Units.** 107 units, including five work-live units (ground level);
- **Ground Level.** 1,228 square feet of ground-level retail oriented to northwest corner, residential lobby, leasing, 2-level work-live units (5), mechanical;
- **Auto Parking.** 21 at-grade parking stalls and 2 Standard B loading stalls (10' clear height) accessed via NW Taylor St (garage door setback approximately 5'). *No minimum parking requirement (33.510.265.E);*
- **Bike Parking.** 163 long-term bike spaces (1 below-grade room). Demand for 8 short-term spaces met by paying into city parking fund. *160 long-term bike spaces required (33.266.220).*
- **Floor Area Ratio** (33.510.200.F, Map 510-2) – The Central City Plan District allowable FAR is 6:1. Bonus or Transfer of FAR from a qualifying site no greater than 3:1 is allowed for a maximum FAR of 9:1. Therefore, the 15,510 SF site allows 139,590 SF total floor area. *Proposed FAR is 5.98:1 (92,760 SF) total FAR.*
- **Height** (33.510.205, Map 510-3) – The Central City Plan District allowable height is 250'. The site is not eligible for general and housing bonuses. *Proposed Height: approximately 76' to top of parapet.*
- **Roof Deck.** Green Roof and amenity featuring seating, gathering and landscaping.

**Materials.** Exterior materials and systems:

- **Primary Cladding.** Standard brick in two colors – coal creek and autumn blend;
- **Accent Cladding.** Equitone (concealed fasteners, honeycomb-backed) as the accent material at window spandrels;
- **Windows/Storefront.** Commercial-grade vinyl windows and aluminum storefront, colors to match Equitone panel (dark gray);

**Modifications.** The following Modifications to Title 33, Portland Zoning Code standards and Design Exception are requested:

1. **Modification to Size of Parking Spaces; 33.266.130F. Table 266-4.** Standard requirement: 8.5'Wx16'L with 20' wide aisles. Proposed: reduce the space dimension from 8.5' x 16' to 7'-9" x 16' for some spaces due to structural column encroachment.
2. **Modification to Size of Bicycle Racks; 33.266.220.C3.** Standard requirement: 24" space separation; Proposed: 18" vertical staggered rack separation.
3. **Design Exception. OSCC 3202.3.2. / IBC/32#1. Window Projections into a Right-of-Way.** Width of projections are limited to 12 feet. The project proposes six projections exceeding this limit ranging from approximately 14' to 21' – two on the north elevation and four on the west elevation.

Design Review is necessary because the proposal is for new construction within a Design Overlay Zone.

**Approval Criteria:**

In order to be approved, this proposal must comply with the approval criteria of Title 33, Portland Zoning Code. The applicable approval criteria are:

- Central City Fundamental Design Guidelines
- Goose Hollow Design Guidelines
- Modification Review 33.825.040
- 33.825 Design Review

## ANALYSIS

**Site and Vicinity:** The subject property occupies approximately a 1/3-block bound by SW 15th Avenue to the east and SW Taylor Street to the north. The site currently supports a 1951 1-story commercial building to be demolished. Neighboring development includes a variety of buildings ranging in age – adjacent on SW 15th Avenue the 1924 3-story Lownsdale Apartments, to the north the recently constructed North Hollow Apartments (5 over 1 market rate apartments featuring ground level residential and retail at the corner of SW 15th Ave and Taylor St) and westward across SW 15th Ave, is the site of the recently approved 11-story 1500 SW Taylor Apartment Building. Further west, is the Brutalist Oregonian production facility built in 1972 (soon to be re-developed with a mixed use project “The Press Blocks”.) Significant community landmarks nearby include Lincoln High School one block south, Portland Timbers home pitch (Providence Park) located two blocks west and the sunken I-405 freeway one block east. SW 15th Ave is a Local Service Bikeway. SW Taylor is a City Bikeway and Traffic Access Street. SW Yamhill, a half block north, is a City Walkway and Regional Transitway and Major Transit Priority Street. SW Salmon Street, one block south, is a City Walkway and Transit Access Street.

**Zoning:** The Central Residential (RX) zone is a high-density multi-dwelling zone which allows the highest density of dwelling units of the residential zones. Density is not regulated by a maximum number of units per acre. Rather, the maximum size of buildings and intensity of use are regulated by floor area ratio (FAR) limits and other site development standards. Generally the density will be 100 or more units per acre. Allowed housing developments are characterized by a very high percentage of building coverage. The major types of housing development will be medium and high rise apartments and condominiums, often with allowed retail, institutional, or other service oriented uses. Generally, RX zones will be located near the center of the city where transit is readily available and where commercial and employment opportunities are nearby. RX zones will usually be applied in combination with the Central City plan district.

The “d” overlay promotes the conservation and enhancement of areas of the City with special historic, architectural or cultural value. New development and exterior modifications to existing development are subject to design review. This is achieved through the creation of design districts and applying the Design Overlay Zone as part of community planning projects, development of design guidelines for each district, and by requiring design review. In addition, design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area.

The Central City Plan District implements the Central City Plan and other plans applicable to the Central City area. These other plans include the Downtown Plan, the River District Plan, the University District Plan, and the Central City Transportation management Plan. The Central City plan district implements portions of these plans by adding code provisions which address special circumstances existing in the Central City area. The site is within the Goose Hollow Subdistrict of this plan district.

**Land Use History:** City records indicate there are no prior land use reviews for this site.

**Agency Review:** A *Notice of proposal in Your Neighborhood* was mailed **January 23, 2017**. The following Bureaus have responded with no issue or concerns:

- Bureau of Environmental Services (Exhibit E.1)
- Water Bureau (Exhibit E.2)

- Life Safety (Exhibit E.3)
- Fire Bureau (Exhibit E.4)
- Bureau of Transportation (Exhibit E.5)

**Neighborhood Review:** A *Notice of Proposal in Your Neighborhood* was mailed on **January 23, 2017**. No written responses have been received from either the Neighborhood Association or notified property owners in response to the proposal.

## **ZONING CODE APPROVAL CRITERIA**

### **I. DESIGN REVIEW (33.825)**

#### **Section 33.825.010 Purpose of Design Review**

Design review ensures that development conserves and enhances the recognized special design values of a site or area. Design review is used to ensure the conservation, enhancement, and continued vitality of the identified scenic, architectural, and cultural values of each design district or area. Design review ensures that certain types of infill development will be compatible with the neighborhood and enhance the area. Design review is also used in certain cases to review public and private projects to ensure that they are of a high design quality.

#### **Section 33.825.055 Design Review Approval Criteria**

A design review application will be approved if the review body finds the applicant to have shown that the proposal complies with the design district guidelines.

**Findings:** The site is designated with design overlay zoning (d), therefore the proposal requires Design Review approval. Because of the site's location, the applicable design guidelines are the Central City Fundamental and Goose Hollow Special Design Guidelines.

#### **Goose Hollow District Design Guidelines and Central City Fundamental Design Guidelines**

The Goose Hollow District is envisioned to be a predominantly urban residential, transit-oriented community located on the western edge of the Central City between Washington Park and Downtown Portland. When riding light rail through the West Hills tunnel to the Central City, it is the first neighborhood experienced before entering downtown Portland. The Urban Design Vision celebrates the sense of arrival from the west at Jefferson Street Station and Collins Circle, and from the north at the Civic Stadium Station and Fire Fighter's Park. This is done by integrating the history of the community with its special natural and formal (man-made) characteristics.

The Central City Fundamental Design Guidelines focus on four general categories. **(A) Portland Personality**, addresses design issues and elements that reinforce and enhance Portland's character. **(B) Pedestrian Emphasis**, addresses design issues and elements that contribute to a successful pedestrian environment. **(C) Project Design**, addresses specific building characteristics and their relationships to the public environment. **(D) Special Areas**, provides design guidelines for the four special areas of the Central City.

#### **Goose Hollow District Design Goals**

The Goose Hollow District Design Goals are specific to the Goose Hollow District. These urban design goals and objectives are to:

- Enhance mixed-use, transit-oriented development around the light rail stations to make it a pedestrian-friendly station community.
- Provide open spaces to accommodate active public life.

- Strengthen connections to adjacent neighborhoods through light rail, bike and pedestrian access and assure a safe and pleasant bike/pedestrian environment.
- Preserve and enhance the community's history and architectural character.

### **Central City Plan Design Goals**

This set of goals are those developed to guide development throughout the Central City. They apply within all of the Central City policy areas. The nine goals for design review within the Central City are as follows:

1. Encourage urban design excellence in the Central City;
2. Integrate urban design and preservation of our heritage into the development process;
3. Enhance the character of the Central City's districts;
4. Promote the development of diversity and areas of special character within the Central City;
5. Establish an urban design relationship between the Central City's districts and the Central City as a whole;
6. Provide for a pleasant, rich and diverse pedestrian experience for pedestrians;
7. Provide for the humanization of the Central City through promotion of the arts;
8. Assist in creating a 24-hour Central City which is safe, humane and prosperous;
9. Ensure that new development is at a human scale and that it relates to the scale and desired character of its setting and the Central City as a whole.

*Staff has considered all guidelines and has addressed only those guidelines considered applicable to this project.*

**A2. Emphasize Portland Themes.** When provided, integrate Portland-related themes with the development's overall design concept.

**A2-1. Recognize the Historic Tanner Creek Theme.** Recognize the course of the historic Tanner Creek and emphasize the District's connection with the Creek on site developments of 20,000 square feet or more, including and immediately adjacent to the historic course of the Creek. This guideline may be accomplished by any or all of the following:

- a. Exposing the Creek using water features and fountains; or
- b. Incorporating interpretive trails, artwork, murals or sculptures that describe and symbolize the relation between the district and the history of Tanner Creek.

**A5-5. Incorporate Water Features.** Incorporate water features or water design themes that enhance the quality, character, and image of the Goose Hollow District.

**Findings for A2, A2-1, and A5-5:** While Tanner Creek now runs in a channel deep below SW 16<sup>th</sup> avenue, the historic course was actually a few blocks south and west. Portland themes will primarily be integrated through the green roof and the integration of a rooftop patio which will allow residents access to the outdoors, where they can experience views of the City and surrounding landscape. *Therefore, these guidelines are met.*

**A4. Use Unifying Elements.** Integrate unifying elements and/or develop new features that help unify and connect individual buildings and different areas.

**A5. Enhance, Embellish, and Identify Areas.** Enhance an area by reflecting the local character within the right-of-way. Embellish an area by integrating elements in new development that build on the area's character. Identify an area's special features or qualities by integrating them into new development.

**C4. Complement the Context of Existing Buildings.** Complement the context of existing buildings by using and adding to the local design vocabulary.

**Findings for A4, A5, and C4:** The proposed building is designed to be a traditional expression compatible with the historic character of this eclectic part of the Goose Hollow neighborhood. The proposed building is 7-stories tall and is expressed as a masonry box with punched windows (approximately 5.5" from face of brick to face of sash and 3.5" from face of cladding to sash within window bays) paired vertically separated by dark cementitious spandrel (Equitone). The primary exterior material is brick in two colors (coal creek and autumn blend), similar to several other buildings in the vicinity, including the Lafayette and Commodore apartment buildings, as well as the Lownsdale across SW 15th Avenue and the recently constructed North Hollow Apartment across NW Taylor St. The traditional expression takes cues from nearby apartment buildings as well as the brick clad pavilion structure part of the soon-to-be-developed Press Blocks two blocks west (currently under review – LU16-273094 DZM). In addition, the two-level work-live units fronting SW 15<sup>th</sup> Ave will further embellish the streetscape with ground-level program that is both active and evocative of traditional residential character. *Therefore, these guidelines are met.*

**A7. Establish and Maintain a Sense of Urban Enclosure.** Define public rights-of-way by creating and maintaining a sense of urban enclosure.

**Findings for A7:** The proposed building will be built to the property lines and will be 7-stories high. While the zoning allows for taller buildings (250'), the proposed height is in keeping with the trend toward partial-block infill development with on-site amenity space. The proposed massing configuration will establish strong urban edges and reinforce the sense of enclosure. *Therefore, this guideline is met.*

**A8. Contribute to a Vibrant Streetscape.** Integrate building setbacks with adjacent sidewalks to increase the space for potential public use. Develop visual and physical connections into buildings' active interior spaces from adjacent sidewalks. Use architectural elements such as atriums, grand entries and large ground-level windows to reveal important interior spaces and activities.

**C6. Develop Transitions between Buildings and Public Spaces.** Develop transitions between private development and public open space. Use site design features such as movement zones, landscape elements, gathering places, and seating opportunities to develop transition areas where private development directly abuts a dedicated public open space.

**C7. Design Corners that Build Active Intersections.** Use design elements including, but not limited to, varying building heights, changes in façade plane, large windows, awnings, canopies, marquees, signs and pedestrian entrances to highlight building corners. Locate flexible sidewalk-level retail opportunities at building corners. Locate stairs, elevators, and other upper floor building access points toward the middle of the block.

**C9. Develop Flexible Sidewalk-Level Spaces.** Develop flexible spaces at the sidewalk-level of buildings to accommodate a variety of active uses.

**Findings for A8, C6, C7 and C9:** In plan, the building is configured as an L-shape orienting building walls and entries to the adjacent public streets, with a rooftop private courtyard. The main entries to the building (residential lobby and retail) emphasize the corner of SW 15<sup>th</sup> Ave and Taylor St, and are identifiable by full-height windows and continuous glass entry canopy. Additional flexibly active uses – two-level work/live units (5) – are oriented to the abutting SW 15<sup>th</sup> Ave public right-of-way. The configuration of these work-live units – open ground-level floor area (between approximately 400-600 sf) with bedroom and kitchen above – will maximize the flexibility of the ground level to function as commercial space thereby activating the streetscape. *Therefore, these guidelines are met.*

**B1. Reinforce and Enhance the Pedestrian System.** Maintain a convenient access route for pedestrian travel where a public right-of-way exists or has existed. Develop and define the different zones of a sidewalk: building frontage zone, street furniture zone, movement zone, and the curb. Develop pedestrian access routes to supplement the public right-of-way system through superblocks or other large blocks.

**B1-2. Orient Building Entries to Facilitate Transit Connections.** Orient primary building entries at pedestrian circulation points which conveniently and effectively connect pedestrians with transit services.

**Findings for B1 and B1-2:** All sidewalks will be reconstructed to City standards with street trees on each frontage. The building's ground level program reinforces pedestrian and transit orientation – retail at the site's only intersection (SW 15<sup>th</sup> Ave and Taylor St), residential lobby and entrance favoring the same corner and active floor area (leasing, reception and two-level work-live units) flanking the remainder of the SW 15<sup>th</sup> Ave frontage with access to MAX two blocks north. *Therefore, these guidelines are met.*

**B4. Provide Stopping and Viewing Places.** Provide safe, comfortable places where people can stop, view, socialize and rest. Ensure that these places do not conflict with other sidewalk uses.

**Findings for B-4:** The main lobby entry to the building is located toward the site's only intersection (40' from the intersection of SW 15<sup>th</sup> Ave and Taylor St) Ave and is accentuated with a radius canopy and floor to ceiling glazing. Retail anchors the northwest corner and the leasing office and amenity space help to activate this frontage.

The main entry door at the residential lobby is inset by approximately 4' with continuous canopy coverage. This recessed entry point is sufficiently sized to provide convenient places for pedestrians to stop, view the surroundings, socialize and rest, outside of the main pedestrian movement zone on the abutting streets. *Therefore, this guideline is met.*

**B7. Integrate Barrier-Free Design.** Integrate access systems for all people with the building's overall design concept.

**Findings for B7:** The proposed building is designed to have barrier-free access to all, including the at-grade work-live units. *Therefore, this guideline is met.*

**B2. Protect the Pedestrian.** Protect the pedestrian environment from vehicular movement. Develop integrated identification, sign, and sidewalk-oriented night-lighting systems that offer safety, interest, and diversity to the pedestrian. Incorporate building equipment, mechanical exhaust routing systems, and/or service areas in a manner that does not detract from the pedestrian environment.

**B6-1. Provide Outdoor Lighting at Human Scale.** Provide outdoor lighting at a human scale to encourage evening pedestrian activity.

**C12. Integrate Exterior Lighting.** Integrate exterior lighting and its staging or structural components with the building's overall design concept. Use exterior lighting to highlight the building's architecture, being sensitive to its impacts on the skyline at night.

**Findings for B2, B6-1, and C12:** The combined parking garage and loading access point is at the northeast corner on SW Taylor St which will limit conflicts between pedestrians and vehicles. Lighting along the project's public frontage is

shown to be incorporated as recessed soffit within the continuous canopy and wall-mounted downlights emphasizing entries and illuminate at pedestrian scale. This lighting will provide safe entry for residents, illuminate the sidewalk for pedestrians, and provide modest ground-level architectural lighting for the building.

Though the building's mechanical rooms (fire, water, electrical) directly abut the sidewalk at the northeast corner adjacent to the garage door, the wall area is minimized so as to not significantly detract from the pedestrian environment. In addition, the garage gate is a gradient perforated metal door (set back approximately 4') which, though a utilitarian expression, is sufficiently detailed to ensure that headlight glare will not adversely impact pedestrians. *Therefore, these guidelines are met.*

**B6. Develop Weather Protection.** Develop integrated weather protection systems at the sidewalk-level of buildings to mitigate the effects of rain, wind, glare, shadow, reflection, and sunlight on the pedestrian environment.

**C1-2. Integrate Signs and Awnings.** Integrate signs and awnings to be complementary and respectful of a building's architecture. This guideline may be accomplished by any or all of the following:

- a. Placing signs and awnings to fit with and respect a building's architecture.
- b. Avoiding large, excessively illuminated or freestanding signs that contribute to visual clutter; or
- c. Integrating with a building's design an exterior sign program/system for flexible sidewalk level space that accommodates changing tenants.

**C13. Integrate Signs.** Integrate signs and their associated structural components with the building's overall design concept. Size, place, design, and light signs to not dominate the skyline. Signs should have only a minimal presence in the Portland skyline.

**Findings for B6, C1-2, and C13:** All proposed entries are shown to be recessed from the sidewalk with canopies and projecting upper floors partially serving as shelter at these entries. In addition, a radius canopy is provided at the lobby entry for additional weather protection along the sidewalk. No signage is proposed, however, signage under 32 square feet is exempt from review. *Therefore, these guidelines are met.*

**C1. Enhance View Opportunities.** Orient windows, entrances, balconies and other building elements to surrounding points of interest and activity. Size and place new buildings to protect existing views and view corridors. Develop building façades that create visual connections to adjacent public spaces.

**C11. Integrate Roofs and Use Rooftops.** Integrate roof function, shape, surface materials, and colors with the building's overall design concept. Size and place rooftop mechanical equipment, penthouses, other components, and related screening elements to enhance views of the Central City's skyline, as well as views from other buildings or vantage points. Develop rooftop terraces, gardens, and associated landscaped areas to be effective stormwater management tools.

**Findings for C1 and C11:** Traditionally expressed with well-glazed upper floors and a rooftop outdoor amenity deck, the building will provide significant views to the surrounding landscape. Compatible in massing with most traditional context development, it will become part of the fabric of the rapidly growing cityscape and will not interfere with existing views and view corridors. *Therefore, these guidelines are met.*



**C1-1. Integrate Parking.** Design surface parking and parking garage exteriors to visually integrate with their surroundings. This guideline may be accomplished by any or all of the following:

- a. Designing street facing parking garages to not express the sloping floors of the interior parking;
- b. Designing the sidewalk level of parking structures to accommodate active uses, display windows, public art or other features which enhance the structure's relationship to pedestrians; or
- c. Accommodating vending booths along sidewalks adjacent to parking facilities when active ground level uses are not possible.

**C7-1. Reduce the Impact of Residential Unit Garages on Pedestrians.** Reduce the impact on pedestrians from cars entering and exiting residential unit garages by locating garage access on alleys, wherever possible, and active spaces on ground floors that abut streets.

**Findings for C1-1 and C7-1:** The access to garage, loading and service areas are co-located on east end of the SW Taylor St elevation. Concentration of these uses limits conflicts between pedestrians and vehicles, as there is only one point of potential conflict rather than multiple points. The parking and loading are located interior to the block with active floor area wrapping most of the project frontage, while long-term bike parking is provided below grade. The applicant is requesting a Modification to reduce the width of some parking spaces, thus relieving pressure on active floor-area at the ground level. *Therefore, these guidelines are met.*

**C2. Promote Quality and Permanence in Development.** Use design principles and building materials that promote quality and permanence.

**C5. Design for Coherency.** Integrate the different building and design elements including, but not limited to, construction materials, roofs, entrances, as well as window, door, sign, and lighting systems, to achieve a coherent composition.

**B1-1. Provide Human Scale to Buildings along Walkways.** Provide human scale and interest to buildings along sidewalks and walkways.

**C8. Differentiate the Sidewalk-Level of Buildings.** Differentiate the sidewalk-level of the building from the middle and top by using elements including, but not limited to, different exterior materials, awnings, signs, and large windows.

**Findings for C2, C5, B1-1, and C8:** The building is designed as a traditional tri-partite expression – a well glazed storefront base, a series of bay expressions accenting the main body of the building, and upper two levels subtly accented with pairs of recessed window planes. Overall, the building presents a coherent composition to the streetscape and will serve as a traditionally inspired infill development within this rapidly developing neighborhood featuring a mix of classically-ordered historic buildings, mid-century commercial buildings, emerging contemporary mixed-use development and surface parking lots. Generally, the proposed building features quality materials, including masonry, cementitious panel (Equitone), aluminum storefront, and commercial-grade vinyl windows.

The storefront bays are accented with deep recesses (2'-4') framed by brick column bases. Windows in the main body of the building are arranged in vertical pairs, with varied recess (approximately 5.5" from face of brick to face of sash and 3.5" within window bays). The resulting effect (punched double-height glazing volumes) will create sufficient shadow lines and help to lighten the building's overall mass, especially the 150'-long west elevation. *Therefore, these guidelines are met.*

## II MODIFICATION REQUESTS (33.825)

**33.825.040 Modifications That Will Better Meet Design Review Requirements:**

The review body may consider modification of site-related development standards, including the sign standards of Chapters 32.32 and 32.34 of the Sign Code, as part of the design review process. These modifications are done as part of design review and are not required to go through the adjustment process. Adjustments to use-related development standards (such as floor area ratios, intensity of use, size of the use, number of units, or concentration of uses) are required to go through the adjustment process. Modifications that are denied through design review may be requested as an adjustment through the adjustment process. The review body will approve requested modifications if it finds that the applicant has shown that the following approval criteria are met:

- A. **Better meets design guidelines.** The resulting development will better meet the applicable design guidelines; and
- B. **Purpose of the standard.** On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.

**Modification #1:** 33.266.130.F *Parking area layouts* – to reduce the width of some of the below-grade parking spaces from the required 8'-6" to as much as 7'-10"; and

*Purpose Statement for 33.266.130:* "The development standards promote vehicle areas which are safe and attractive for motorists and pedestrians. Vehicle area locations are restricted in some zones to promote the desired character of those zones. Together with the transit street building setback standards in the base zone chapters, the vehicle area restrictions for sites on transit streets and in Pedestrian Districts:

- Provide a pedestrian access that is protected from auto traffic; and
- Create an environment that is inviting to pedestrians and transit users.
- The parking area layout standards are intended to promote safe circulation within the parking area, provide for the effective management of stormwater runoff from vehicle areas, and provide for convenient entry and exit of vehicles.

The setback and landscaping standards:

- Improve and soften the appearance of parking areas;
- Reduce the visual impact of parking areas from sidewalks, streets, and especially from adjacent residential zones;
- Provide flexibility to reduce the visual impacts of small residential parking lots;
- Direct traffic in parking areas;
- Shade and cool parking areas;
- Reduce the amount and rate of stormwater runoff from vehicle areas;
- Reduce pollution and temperature of stormwater runoff from vehicle areas; and
- Decrease airborne and waterborne pollution.

*Standard:* 33.130.210.F.2 *Parking space and aisle dimensions.* Parking spaces and aisles must meet the minimum dimensions contained in Table 266-4. Table 266-4 states that parking spaces oriented at 90° to the drive aisle should have minimum dimensions of 8'-6" x 16'-0".

- A. **Better meets design guidelines.** The resulting development will better meet the applicable design guidelines; and

**Findings:** As noted above under C1-1 *Integrate Parking* and C7-1 *Reduce the Impact of Residential Unit Garages on Pedestrians*, reduction of the width of some parking spaces allows a greater number of parking spaces to be provided below grade, reducing the pressure for parking located on the street or at the ground level of the building.

**B. Purpose of the standard.** *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

**Findings:** The applicant is proposing approximately 21 at-grade parking spaces to serve the residential units. Approximately 14 of these spaces will be reduced up to 8 inches in width, for a total width of 7'-10" due to the presence of structural columns partially encroaching into the spaces. Staff notes that the columns are 2'-0" deep, thus compromising the width of the space for only 2'-0", while the remaining 14'-0" depth of the spaces are at the standard width. By allowing a reduced width, the applicant is able to accommodate more vehicles, which reduces the number of potential vehicles parked on the street and increases ground floor activation.

The purpose of the design standard is met and the design guidelines are better met by the proposal to reduce the width of some of the proposed at-grade parking spaces.

*Therefore, this Modification merits approval.*

**Modification #2:** 33.266.220.C.3 Standards for all bicycles – to reduce the width of all of the required 163 long-term bicycle parking spaces from the required 2'-0" to 1'-6".

*Purpose Statement for 33.266.130.C:* "These standards ensure that required bicycle parking is designed so that bicycles may be securely locked without undue inconvenience and will be reasonably safeguarded from intentional or accidental damage."

*Standard:* 33.266.C.3 Bicycle racks. The Office of Transportation maintains a handbook of racks and citing guidelines that meet the standards of this paragraph. Required bicycle parking may be provided in floor, wall, or ceiling racks. Where required bicycle parking is provided in racks, the racks must meet the following standards:

- a. The bicycle frame and one wheel can be locked to the rack with a high security, U-shaped shackle lock if both wheels are left on the bicycle;
- b. A space 2 feet by 6 feet must be provided for each required bicycle parking space, so that a bicycle six feet long can be securely held with its frame supported so that the bicycle cannot be pushed or fall in a manner that will damage the wheels or components. See Figure 266-11; and
- c. The rack must be securely anchored.

**A. Better meets design guidelines.** *The resulting development will better meet the applicable design guidelines; and*

**Findings:** By reducing the width of the bike parking, the applicant has the option of reducing the total amount of square footage devoted to bike parking, or providing additional spaces to better meet bike parking demand. Guideline C1-1 *Integrate Parking* is better met by the reduced width, as it allows for accommodation of either a greater number of bicycle parking spaces.

**B. Purpose of the standard.** *On balance, the proposal will be consistent with the purpose of the standard for which a modification is requested.*

**Findings:** With the reduction in width, the applicant will be able to accommodate a greater number of long-term bike parking spaces and ensure that demand for bike parking spaces is met. The purpose of the standard is met in that many of the bicycles will be stored privately within the units and that a greater number of spaces than required is to be provided, including space for non-traditional bicycles,

which will provide extra space for bicycles to spread out if all spaces are not occupied.

*Therefore, this Modification merits approval.*

### III. EXCEPTION TO “WINDOW PROJECTIONS INTO THE PUBLIC RIGHT-OF-WAY” IBC/32/#1

Windows that project into the public right-of-way have a maximum width of 12'. When approved through design review, the width may vary. The proposal includes a 31'-10" wide window projecting into the NW 19th Ave public right-of-way at the east elevation near the SE corner of the building.

**A. Projection.** Maximum projection of 4 feet into the right-of-way including trim, eaves and ornament.

**Findings:** The maximum projection is 1'-9". *This Criterion is met.*

**B. Clearance.** Clearance above grade as defined in Chapter 32, Section 3202.3.2 of the current Oregon Structural Specialty Code. (The 2004 edition of the Oregon Structural Specialty Code states that no projection is allowed for clearances less than 8 feet above grade. For clearances above grade greater than 8 feet, 1 inch of projection is allowed for each additional inch of clearance, provided that no such projection shall exceed a distance of 4 feet.)

**Findings:** Minimum clearance above grade is 8' and the maximum projection is 21". *This Criterion is met.*

**C. Area.** Maximum wall area of all windows which project into public right-of-way on a wall is 40% of the wall's area.

**Findings:** Projecting wall area is under 40% on both elevations. *This Criterion is met.*

**D. Wall Length.** Maximum width of any single window which projects into public right-of-way is 50% of its building wall length.

**Findings:** Projecting wall length is under 50% on both elevations. *This Criterion is met.*

**E. Window Area.** Minimum of 30% window area at the face of the projecting window element. Projections greater than 2 feet 6 inches must have windows at all sides. Required side windows must be a minimum of 10% of side walls.

**Findings:** Front-facing window area of the projecting bay window is well over 30%. At 21", the side walls are not required to be glazed. *This Criterion is met.*

**F. Width.** Maximum width of 12 feet for each projecting window element. When approved through Design Review, the width may vary provided the area of all windows on a wall which project into public right of way does not exceed 40% of the wall's area and the width of any single projecting window element does not exceed 50% of its building wall's length.

**Findings:** The proposed projections range in width from approximately 14' to 21'. This Criterion is not met but is approvable with (1) compliance with standards C and D, and (2) a favorable recommendation through Design Review. Standards C

and D are met. With regard to Design Review consideration, the building is stronger and more compelling with the proposed bay window as follows:

- typologically appropriate given the building's traditional architectural expression;
- reflective of historical context development;
- proportionally appropriate to accent the project's primary corner (SW 15<sup>th</sup> Ave and Taylor St);
- effective measure to modulate building mass to pedestrian scale;
- containing primary living/dining area which will provide eyes on the street below/
- restrained projection depth (21") with recessed glazing.

*This criterion is met.*

**G. Separation.** Minimum separation of 12 feet measured from other projecting window elements on the same elevation or plane of wall. When approved through Design Review, required separation may vary provided the area of all projecting window elements on a wall does not exceed 40% of the wall's area and the width of any single projecting window element over the right-of-way does not exceed 50% of its building wall's length.

**Findings:** All proposed Oriels are separated by more than 12'. *This criterion is met.*

*Staff recommends approval of this requested exception.*

## DEVELOPMENT STANDARDS

Unless specifically required in the approval criteria listed above, this proposal does not have to meet the development standards in order to be approved during this review process. The plans submitted for a building or zoning permit must demonstrate that all development standards of Title 33 can be met, or have received an Adjustment or Modification via a land use review prior to the approval of a building or zoning permit.

## CONCLUSIONS

The design review process exists to promote the conservation, enhancement, and continued vitality of areas of the City with special scenic, architectural, or cultural value. The proposal as revised pursuant to Design Commission comments, meets and in some case exceeds the Design Guidelines and the Zoning Code standards, and therefore warrants approval.

## TENTATIVE STAFF RECOMMENDATION

(May be revised upon receipt of new information at any time to the Design Commission decision)

Staff is recommending *approval* of the Design Review and Modifications of the 7-story apartment building located on a 15,000 sf site in the Goose Hollow Sub-District of the Central City Plan District including the following key program components (approximate quantities): 107 units, 1,200 square feet of ground-level retail, private rooftop amenity deck, 21 auto parking stalls and 2 Standard B loading stalls, 163 long-term bike spaces.

Approval per approved Exhibits C-1 through C-48, subject to the following conditions:

- A. As part of the building permit application submittal, the following development-related conditions (B) must be noted on each of the 4 required site plans or included as a sheet in the numbered set of plans. The sheet on which this information appears must be labeled "ZONING COMPLIANCE PAGE- Case File LU 16-284073 DZM. All requirements must be graphically represented on the site plan, landscape, or other required plan and must be labeled "REQUIRED."
- B. At the time of building permit submittal, a signed Certificate of Compliance form (<https://www.portlandoregon.gov/bds/article/623658>) must be submitted to ensure the permit plans comply with the Design/Historic Resource Review decision and approved exhibits.
- C. No field changes allowed.

Staff recommends approval of the following Modifications and Design Exception:

- 1. Modification to Size of Parking Spaces; 33.266.130F. Table 266-4. Standard requirement: 8.5'Wx16'L with 20' wide aisles. Proposed: reduce the space dimension from 8.5' x 16' to 7'-9" x 16' for some spaces due to structural column encroachment.
- 2. Modification to Size of Bicycle Racks; 33.266.220.C3. Standard requirement: 24" space separation; Proposed: 18" vertical staggered rack separation.
- 3. Design Exception. OSCC 3202.3.2. / IBC/32#1. Window Projections into a Right-of-Way. Width of projections are limited to 12 feet. The project proposes six projections exceeding this limit ranging from approximately 14' to 21' – two on the north elevation and four on the west elevation.

=====

**Procedural Information.** The application for this land use review was submitted on December 9, 2016, and was determined to be complete on December 27, 2016.

*Zoning Code Section 33.700.080* states that Land Use Review applications are reviewed under the regulations in effect at the time the application was submitted, provided that the application is complete at the time of submittal, or complete within 180 days. Therefore this application was reviewed against the Zoning Code in effect on December 9, 2016.

*ORS 227.178* states the City must issue a final decision on Land Use Review applications within 120-days of the application being deemed complete. The 120-day review period may be waived or extended at the request of the applicant. In this case, the applicant did not waive or extend the 120-day review period. Unless extended by the applicant, **the 120 days will expire on April 26, 2017.**

**Some of the information contained in this report was provided by the applicant.** As required by Section 33.800.060 of the Portland Zoning Code, the burden of proof is on the applicant to show that the approval criteria are met. The Bureau of Development Services has independently reviewed the information submitted by the applicant and has included this information only where the Bureau of Development Services has determined the information satisfactorily demonstrates compliance with the applicable approval criteria. This report is the recommendation of the Bureau of Development Services with input from other City and public agencies.

**This report is not a decision. The review body for this proposal is the Design Commission who will make the decision on this case.** This report is a

recommendation to the Design Commission by the Bureau of Development Services. The review body may adopt, modify, or reject this recommendation. The Design Commission will make a decision about this proposal at the hearing or will grant a continuance. Your comments to the Design Commission can be mailed, c/o the Design Commission, 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201 or faxed to 503-823-5630.

You will receive mailed notice of the decision if you write a letter received before the hearing or testify at the hearing, or if you are the property owner or applicant. You may review the file on this case by appointment at our office at 1900 SW Fourth Ave., Suite 5000, Portland, OR 97201. Please call the file review line at 503-823-7617 to schedule an appointment.

**Appeal of the decision.** The decision of the Design Commission may be appealed to City Council, who will hold a public hearing. If you or anyone else appeals the decision of the review body, only evidence previously presented to the review body will be considered by the City Council.

**Who can appeal:** You may appeal the decision only if you write a letter which is received before the close of the record for the hearing, if you testify at the hearing, or if you are the property owner/applicant. **Appeals must be filed within 14 days of the decision. An appeal fee of \$5,000.00 will be charged.**

Additional information on how to file and the deadline for filing an appeal will be included with the decision. Assistance in filing the appeal and information on fee waivers are available from the Bureau of Development Services in the Development Services Center, 1900 SW Fourth Ave., First Floor. Neighborhood associations recognized by the Office of Neighborhood Involvement may qualify for a waiver of the appeal fee provided that the association has standing to appeal. The appeal must contain the signature of the Chair person or other person authorized by the association, confirming the vote to appeal was done in accordance with the organization's bylaws.

Neighborhood associations, who wish to qualify for a fee waiver, must complete the Type III Appeal Fee Waiver Request for Organizations Form and submit it prior to the appeal deadline. The Type III Appeal Fee Waiver Request for Organizations Form contains instructions on how to apply for a fee waiver, including the required vote to appeal.

**Recording the final decision.**

If this Land Use Review is approved the final decision must be recorded with the Multnomah County Recorder. A few days prior to the last day to appeal, the City will mail instructions to the applicant for recording the documents associated with their final land use decision.

- A building or zoning permit will be issued only after the final decision is recorded.

The applicant, builder, or a representative may record the final decision as follows:

- By Mail: Send the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to: Multnomah County Recorder, P.O. Box 5007, Portland OR 97208. The recording fee is identified on the recording sheet. Please include a self-addressed, stamped envelope.
- In Person: Bring the two recording sheets (sent in separate mailing) and the final Land Use Review decision with a check made payable to the Multnomah County Recorder to the County Recorder's office located at 501 SE Hawthorne Boulevard, #158, Portland OR 97214. The recording fee is identified on the recording sheet.

For further information on recording, please call the County Recorder at 503-988-3034  
For further information on your recording documents please call the Bureau of  
Development Services Land Use Services Division at 503-823-0625.

**Expiration of this approval.** An approval expires three years from the date the final decision is rendered unless a building permit has been issued, or the approved activity has begun.

Where a site has received approval for multiple developments, and a building permit is not issued for all of the approved development within three years of the date of the final decision, a new land use review will be required before a permit will be issued for the remaining development, subject to the Zoning Code in effect at that time.

Zone Change and Comprehensive Plan Map Amendment approvals do not expire.

**Applying for your permits.** A building permit, occupancy permit, or development permit must be obtained before carrying out this project. At the time they apply for a permit, permittees must demonstrate compliance with:

- All conditions imposed here.
- All applicable development standards, unless specifically exempted as part of this land use review.
- All requirements of the building code.
- All provisions of the Municipal Code of the City of Portland, and all other applicable ordinances, provisions and regulations of the city.

**The Bureau of Development Services is committed to providing equal access to information and hearings. Please notify us no less than five business days prior to the event if you need special accommodations. Call 503-823-7300 (TTY 503-823-6868).**

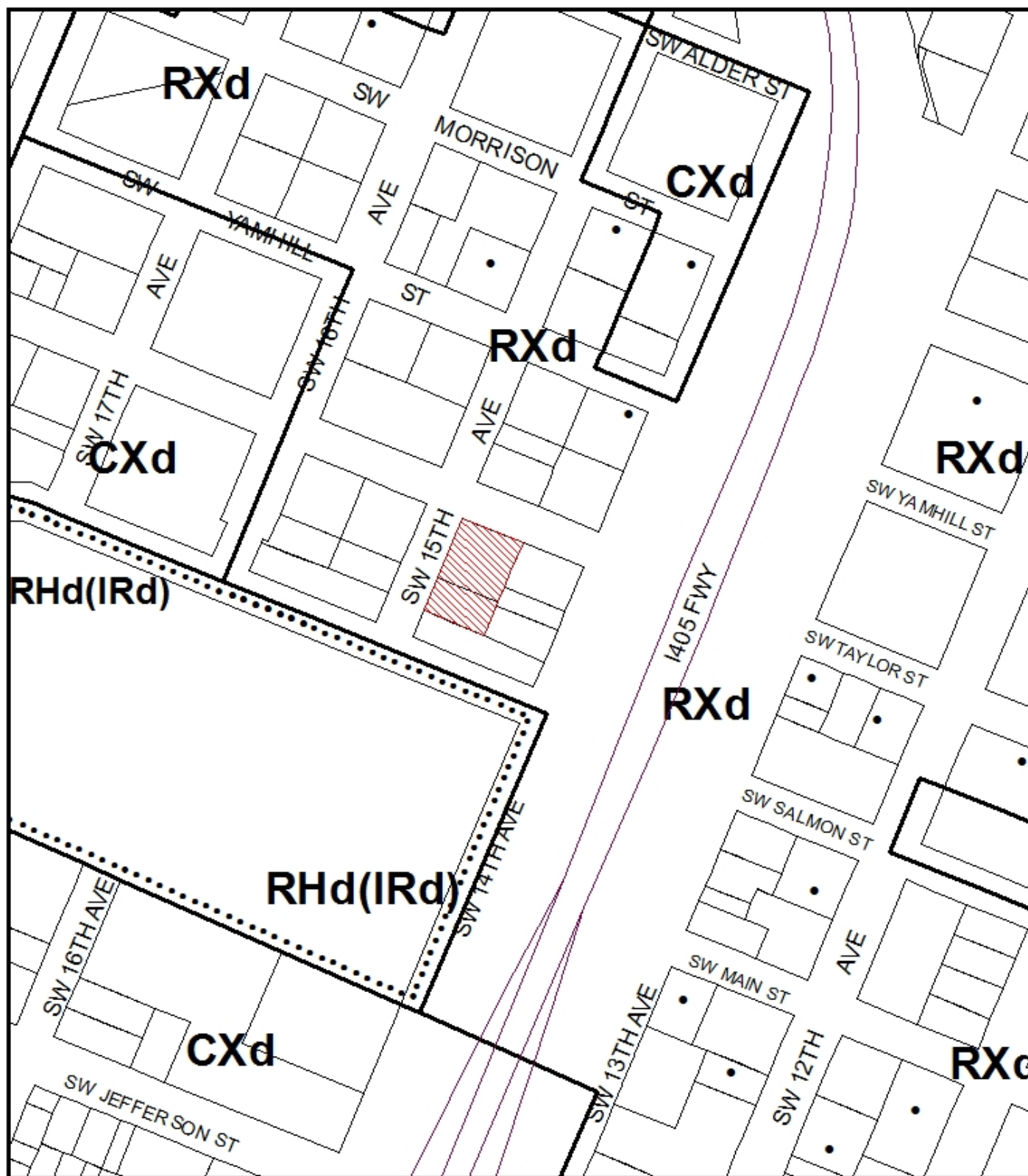
Jeffrey Mitchem  
February 10, 2017

**EXHIBITS – NOT ATTACHED UNLESS INDICATED**

- A. Applicant's Statement
- B. Zoning Map (attached)
- C. Plan & Drawings
  - 1. Design Review Drawing Set (Sheet C.1-C.47)
    - Sheet C.16 Site Plan (attached)
    - Sheet C.25 North and South Elevations (attached)
    - Sheet C.26 West Elevation (attached)
- D. Notification information:
  - 1. Request for response
  - 2. Posting letter sent to applicant
  - 3. Notice to be posted
  - 4. Applicant's statement certifying posting
  - 5. Mailed notice
  - 6. Mailing list
- E. Agency Responses:
  - 1. Bureau of Environmental Services
  - 2. Water Bureau
  - 3. Life Safety
  - 4. Fire Bureau



- 5. Bureau of Transportation
- F. Letters – None
- G. Other
  - 1. Original LUR Application
- H. Post First Hearing

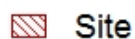


# ZONING



NORTH

This site lies within the:  
CENTRAL CITY PLAN DISTRICT  
GOOSE HOLLOW SUB DISTRICT



Site



Historic Landmark

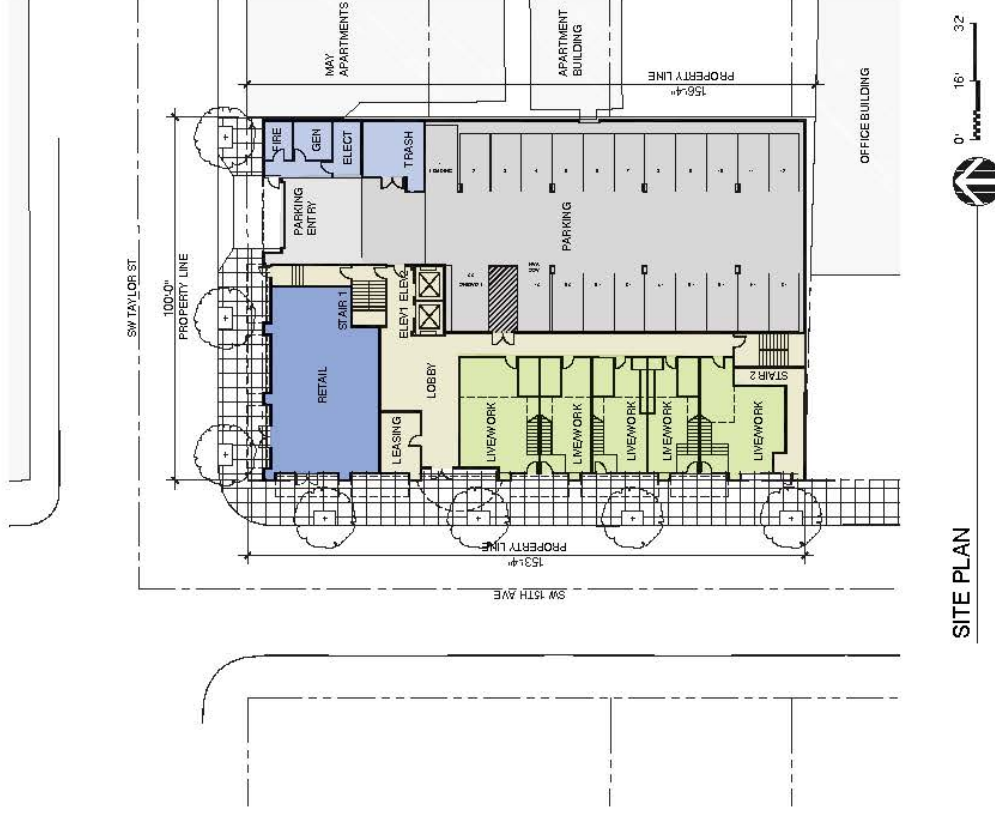
File No. LU 16-284073 DZM

1/4 Section 3028

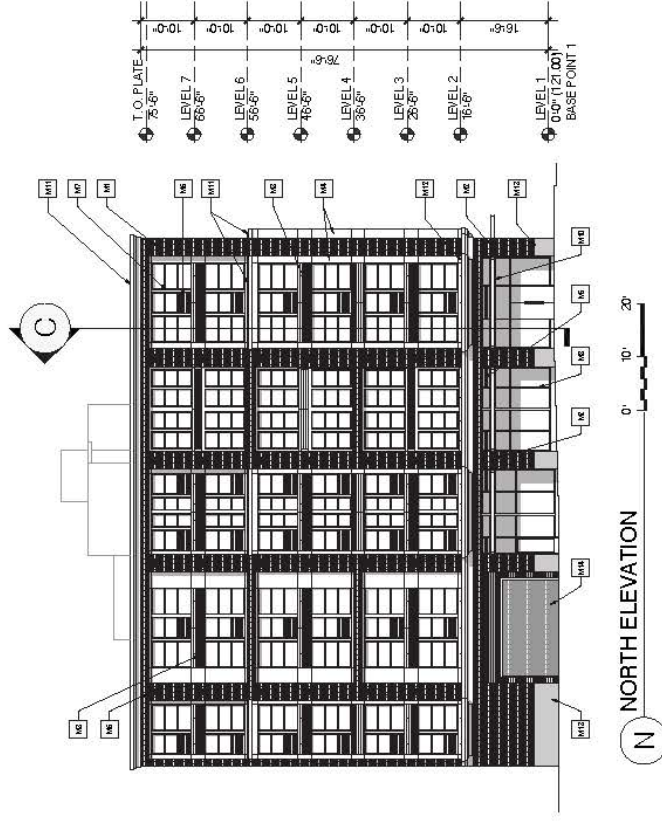
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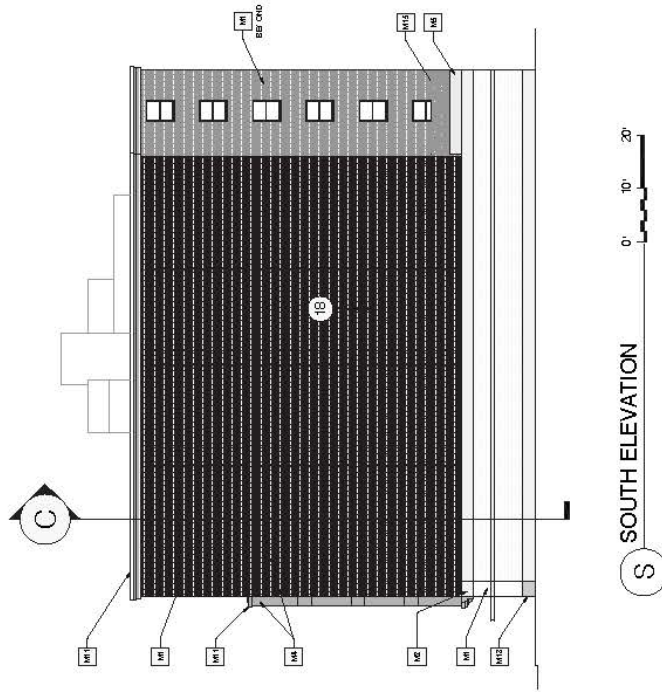
Exhibit B (Dec 15, 2016)



SITE PLAN



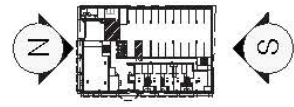
NORTH ELEVATION



SOUTH ELEVATION

MATERIAL KEYNOTE LEGEND

- |    |                          |    |  |    |                                |     |   |     |                                   |     |   |
|----|--------------------------|----|--|----|--------------------------------|-----|---|-----|-----------------------------------|-----|---|
| M1 | BRICK                    | M3 | HORIZONTAL STRIATED FIBER CEMENT PANEL | M5 | EXPOSED SMOOTH CONCRETE        | M8  | STOREFRONT GLAZING SYSTEM                                       | M12 | PRECAST CONCRETE BAND AND CORBELS | M15 | STEEL GUARDRAIL WITH EXPANDED METAL INFILL PANELS |
| M2 | BRICK, CONTRASTING COLOR | M4 | FIBER CEMENT PANEL - SMOOTH FINISH     | M6 | INTEGRAL METAL VENT LOUVER     | M9  | GLASS AND STEEL CANOPY  | M13 | PRECAST CONCRETE BASE             | M16 | STEEL FRAME WITH EXPANDED METAL INFILL PANELS     |
|    |                          |    |  | M7 | COMMERCIAL GRADE VINYL WINDOWS | M10 | STEEL CHANNEL CANOPY WITH WOOD SOFT, TYP. <b>SEE DETAIL 96.</b> | M14 | HIGH SPEED COILING DOOR           | M17 | LINE OF ADJACENT BUILDING                         |



1440 SW TAYLOR APARTMENTS

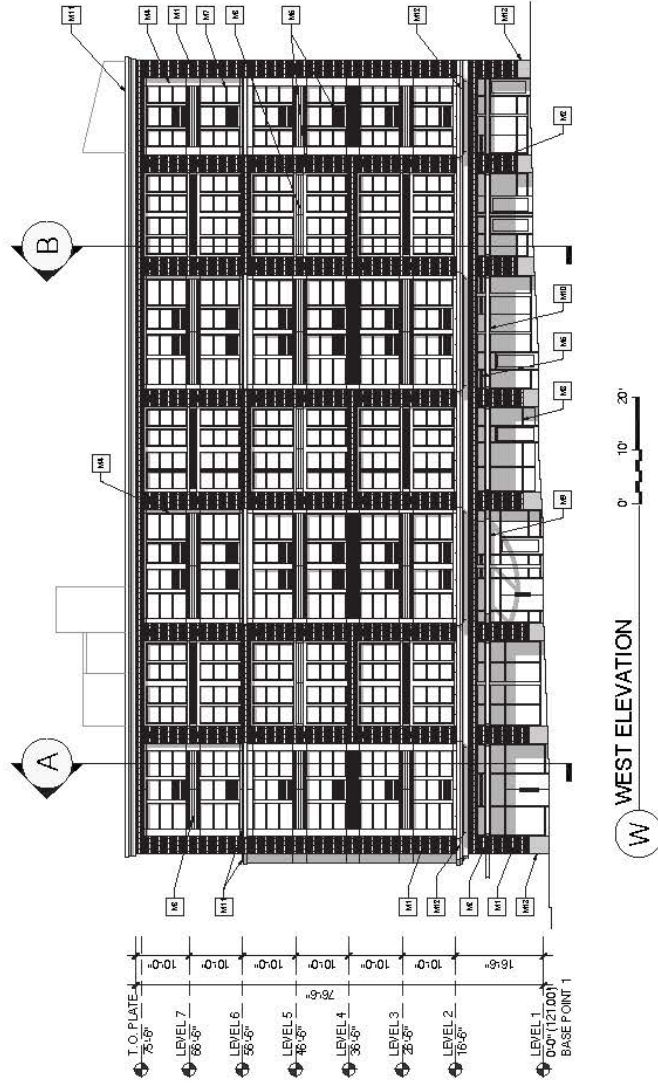
EXHIBIT  
**C25**

SHELTER HOLDINGS LLC  
Type III Design Review  
08 February 2017  
EA 16-137537

Exterior Elevations  
North + South

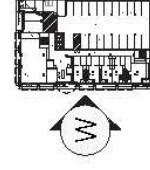
300 SW 1st Street  
Portland, OR 97204  
Tel: 503.228.2841  
www.leeb.com

**leeb** Leeb Architects



# MATERIAL KEYNOTE LEGEND

- M1 BRICK
- M2 BRICK, CONTRASTING COLOR
- M3 HORIZONTAL STRATIFIED FIBER CEMENT PANEL
- M4 FIBER CEMENT PANEL - SMOOTH FINISH
- M5 EXPOSED SMOOTH CONCRETE
- M6 INTEGRAL METAL VENT LOUVER
- M7 COMMERCIAL GRADE VINYL WINDOWS
- M8 STOREFRONT GLAZING SYSTEM
- M9 GLASS AND STEEL CANOPY
- M10 STEEL CHANNEL CANOPY WITH WOOD SOFFIT, TYP.
- M11 SHAPED METAL COPING, SEE DETAIL 98.
- M12 PRECAST CONCRETE BAND AND COFFEYS
- M13 PRECAST CONCRETE BASE
- M14 HIGH SPEED COILING DOOR
- M15 STEEL GUARDRAIL WITH EXPANDED METAL INFILL PANELS
- M16 STEEL FRAME WITH EXPANDED METAL INFILL PANELS
- M17 LINE OF ADJACENT BUILDING



1440 SW TAYLOR APARTMENTS

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Type III Design Review  
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EXHIBIT  
C26